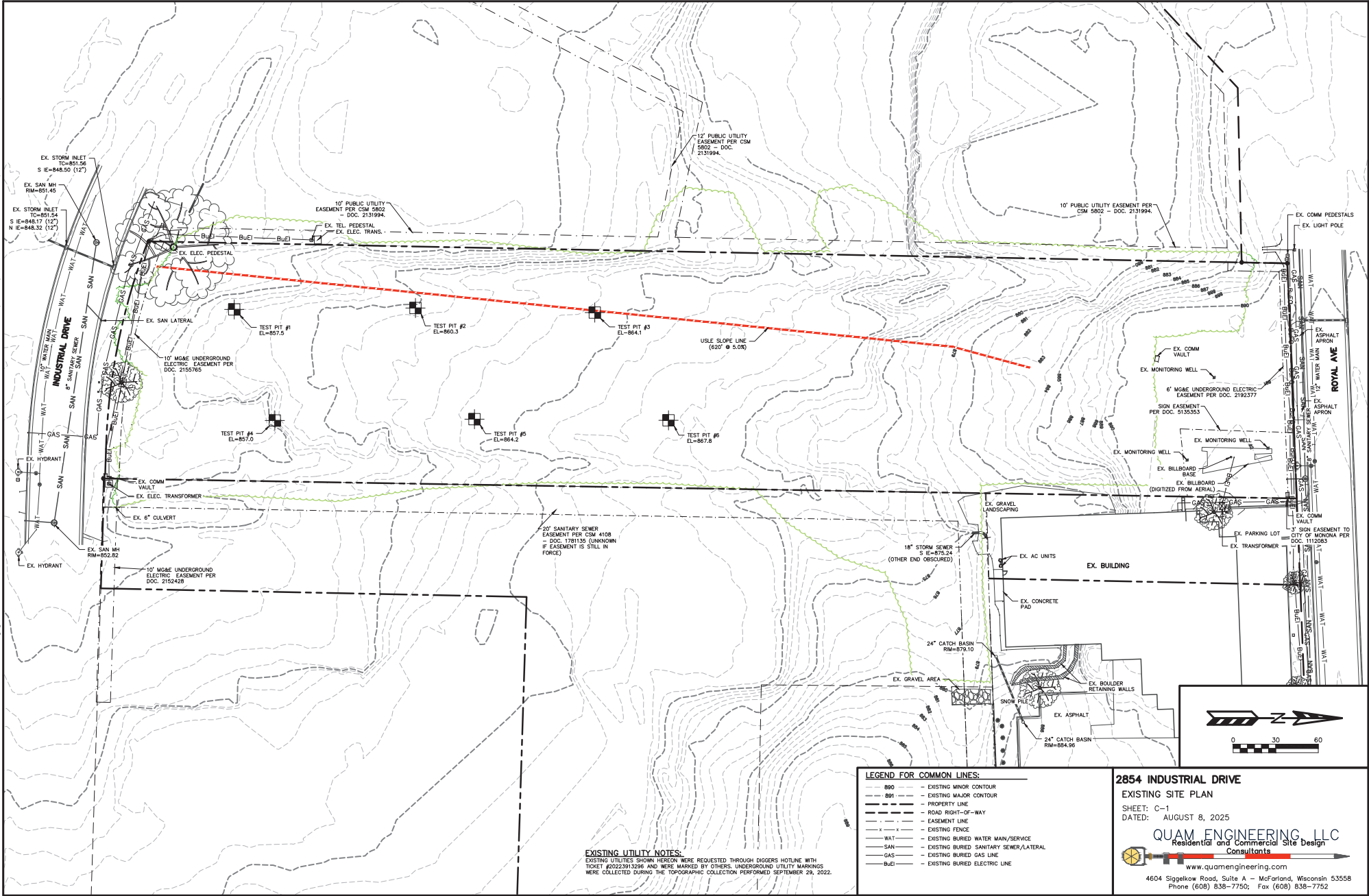


Summary of Improvements:

- Construct public sidewalk, street terrace, curb and gutter, and pavement per plan approved by the City Engineer.
- Developer shall dedicate Right of Way or grant Public Sidewalk Easement for and be responsible for the construction of a minimum five (5)-foot wide sidewalk, eight (8)-foot wide terrace, and additional one (1) foot for maintenance along Royal Ave.
- Construct new private drive entrance aprons per plans approved by the City Engineering and per the Commercial Driveway Permit approved by the City Traffic Engineer.
- Construct private storm service laterals necessary to serve the private development project per plans approved by the City Engineer.
- Relocate and install new fire hydrants as determined by Madison Water Utility.
- Coordinate project access, street closures, and street occupancy as approved by City Traffic Engineering.



EXISTING UTILITY NOTES:
EXISTING UTILITIES SHOWN HEREON WERE REQUESTED THROUGH DIGGERS HOTLINE WITH TICKET #202223913006 AND WERE MARKED BY OTHERS. UNDERGROUND UTILITY MARKINGS WERE COLLECTED DURING THE TOPOGRAPHIC COLLECTION PERFORMED SEPTEMBER 29, 2022.

- LEGEND FOR COMMON LINES:**
- 800 — — — — — EXISTING MINOR CONTOUR
 - 801 — — — — — EXISTING MAJOR CONTOUR
 - — — — — PROPERTY LINE
 - — — — — ROAD RIGHT-OF-WAY
 - — — — — EASEMENT LINE
 - x - x - EXISTING FENCE
 - - - - - EXISTING BURIED WATER MAIN/SERVICE
 - - - - - EXISTING BURIED SANITARY SEWER/LATERAL
 - - - - - EXISTING BURIED GAS LINE
 - - - - - EXISTING BURIED ELECTRIC LINE

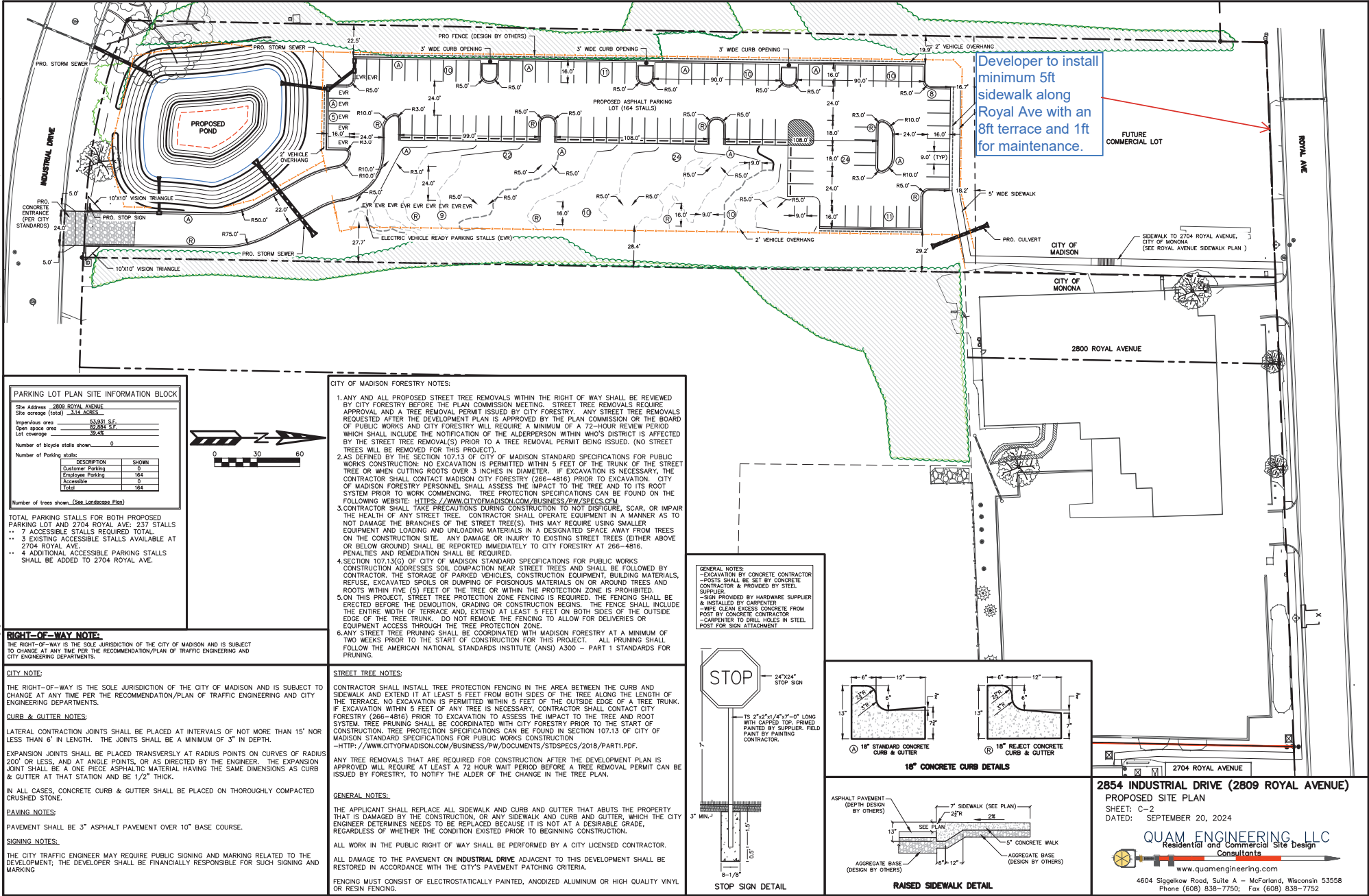
2854 INDUSTRIAL DRIVE
EXISTING SITE PLAN
SHEET: C-1
DATED: AUGUST 8, 2025

QUAM ENGINEERING, LLC
Residential and Commercial Site Design
Consultants
www.quamengineering.com

4604 Sigelkow Road, Suite A - McFarland, Wisconsin 53558
Phone (608) 838-7750; Fax (608) 838-7752

2854 INDUSTRIAL DRIVE
PROPOSED SITE PLAN - PHASE 1
 SHEET: C-2.1
 DATED: AUGUST 8, 2025

4604 Siggelkow Road, Suite A - McFarland, Wisconsin 53558
Phone (608) 838-7750; Fax (608) 838-7752



Developer to install minimum 5ft sidewalk along Royal Ave with an 8ft terrace and 1ft for maintenance.

PARKING LOT PLAN SITE INFORMATION BLOCK

Site Address: 2809 ROYAL AVENUE
Site acreage (total): 0.15 ACRES
Impervious area: 54,931 SF
Open space area: 39,424 SF
Lot coverage: 38.45%
Number of bicycle stalls shown: 0

DESCRIPTION	SHOWN
Customer Parking	0
Employee Parking	164
Accessible	0
Total	164

Number of trees shown: (See Landscape Plan)

TOTAL PARKING STALLS FOR BOTH PROPOSED PARKING LOT AND 2704 ROYAL AVE: 237 STALLS
-- 7 ACCESSIBLE STALLS REQUIRED TOTAL
-- 3 EXISTING ACCESSIBLE STALLS AVAILABLE AT 2704 ROYAL AVE
-- 4 ADDITIONAL ACCESSIBLE PARKING STALLS SHALL BE ADDED TO 2704 ROYAL AVE.

RIGHT-OF-WAY NOTE:
THE RIGHT-OF-WAY IS THE SOLE JURISDICTION OF THE CITY OF MADISON AND IS SUBJECT TO CHANGE AT ANY TIME PER THE RECOMMENDATION/PLAN OF TRAFFIC ENGINEERING AND CITY ENGINEERING DEPARTMENTS.

CITY NOTE:
THE RIGHT-OF-WAY IS THE SOLE JURISDICTION OF THE CITY OF MADISON AND IS SUBJECT TO CHANGE AT ANY TIME PER THE RECOMMENDATION/PLAN OF TRAFFIC ENGINEERING AND CITY ENGINEERING DEPARTMENTS.

CURB & GUTTER NOTES:
LATERAL CONTRACTION JOINTS SHALL BE PLACED AT INTERVALS OF NOT MORE THAN 15' NOR LESS THAN 6' IN LENGTH. THE JOINTS SHALL BE A MINIMUM OF 3" IN DEPTH.
EXPANSION JOINTS SHALL BE PLACED TRANSVERSELY AT RADIUS POINTS ON CURVES OF RADIUS 200' OR LESS, AND AT ANGLE POINTS, OR AS DIRECTED BY THE ENGINEER. THE EXPANSION JOINT SHALL BE A ONE PIECE ASPHALTIC MATERIAL HAVING THE SAME DIMENSIONS AS CURB & GUTTER AT THAT STATION AND BE 1/2" THICK.

IN ALL CASES, CONCRETE CURB & GUTTER SHALL BE PLACED ON THOROUGHLY COMPACTED CRUSHED STONE.

PAVING NOTES:
PAVEMENT SHALL BE 3" ASPHALT PAVEMENT OVER 10" BASE COURSE.

SIGNING NOTES:
THE CITY TRAFFIC ENGINEER MAY REQUIRE PUBLIC SIGNING AND MARKING RELATED TO THE DEVELOPMENT; THE DEVELOPER SHALL BE FINANCIALLY RESPONSIBLE FOR SUCH SIGNING AND MARKING

- CITY OF MADISON FORESTRY NOTES:**
1. ANY AND ALL PROPOSED STREET TREE REMOVALS WITHIN THE RIGHT OF WAY SHALL BE REVIEWED BY CITY FORESTRY BEFORE THE PLAN COMMISSION MEETING. STREET TREE REMOVALS REQUIRE APPROVAL AND A TREE REMOVAL PERMIT ISSUED BY CITY FORESTRY. ANY STREET TREE REMOVALS REQUESTED AFTER THE DEVELOPMENT PLAN IS APPROVED BY THE PLAN COMMISSION OR THE BOARD OF PUBLIC WORKS AND CITY FORESTRY WILL REQUIRE A MINIMUM OF A 72-HOUR REVIEW PERIOD WHICH SHALL INCLUDE THE NOTIFICATION OF THE ALDERPERSON WITHIN WHO'S DISTRICT IS AFFECTED BY THE STREET TREE REMOVAL(S) PRIOR TO A TREE REMOVAL PERMIT BEING ISSUED. (NO STREET TREES WILL BE REMOVED FOR THIS PROJECT).
 2. AS DEFINED BY THE SECTION 107.13 OF CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION: NO EXCAVATION IS PERMITTED WITHIN 5 FEET OF THE TRUNK OF THE STREET TREE OR WHEN CUTTING ROOTS OVER 3 INCHES IN DIAMETER. IF EXCAVATION IS NECESSARY, THE CONTRACTOR SHALL CONTACT MADISON CITY FORESTRY (266-4816) PRIOR TO EXCAVATION. CITY OF MADISON FORESTRY PERSONNEL SHALL ASSESS THE IMPACT TO THE TREE AND TO ITS ROOT SYSTEM PRIOR TO WORK COMMENCING. TREE PROTECTION SPECIFICATIONS CAN BE FOUND ON THE FOLLOWING WEBSITE: [HTTPS://WWW.CITYOFMADISON.COM/BUSINESS/PW/SPECS.CFM](https://www.cityofmadison.com/business/pw/specs.cfm)
 3. CONTRACTOR SHALL TAKE PRECAUTIONS DURING CONSTRUCTION TO NOT DISFIGURE, SCAR, OR IMPAIR THE HEALTH OF ANY STREET TREE. CONTRACTOR SHALL OPERATE EQUIPMENT IN A MANNER AS TO NOT DAMAGE THE BRANCHES OF THE STREET TREE(S). THIS MAY REQUIRE USING SMALLER EQUIPMENT AND LOADING AND UNLOADING MATERIALS IN A DESIGNATED SPACE AWAY FROM TREES ON THE CONSTRUCTION SITE. ANY DAMAGE OR INJURY TO EXISTING STREET TREES (EITHER ABOVE OR BELOW GROUND) SHALL BE REPORTED IMMEDIATELY TO CITY FORESTRY AT 266-4816. PENALTIES AND REMEDIATION SHALL BE REQUIRED.
 4. SECTION 107.13(G) OF CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION ADDRESSES SOIL COMPACTION NEAR STREET TREES AND SHALL BE FOLLOWED BY CONTRACTOR. THE STORAGE OF PARKED VEHICLES, CONSTRUCTION EQUIPMENT, BUILDING MATERIALS, REFUSE, EXCAVATED SPOILS OR DUMPING OF POISONOUS MATERIALS ON OR AROUND TREES AND ROOTS WITHIN FIVE (5) FEET OF THE TREE OR WITHIN THE PROTECTION ZONE IS PROHIBITED.
 5. ON THIS PROJECT, STREET TREE PROTECTION ZONE FENCING IS REQUIRED. THE FENCING SHALL BE ERCTED BEFORE THE DEMOLITION, GRADING OR CONSTRUCTION BEGINS. THE FENCE SHALL INCLUDE THE ENTIRE WIDTH OF TERRACE AND EXTEND AT LEAST 5 FEET ON BOTH SIDES OF THE OUTSIDE EDGE OF THE TREE TRUNK. DO NOT REMOVE THE FENCING TO ALLOW FOR DELIVERIES OR EQUIPMENT ACCESS THROUGH THE TREE PROTECTION ZONE.
 6. ANY STREET TREE PRUNING SHALL BE COORDINATED WITH MADISON FORESTRY AT A MINIMUM OF TWO WEEKS PRIOR TO THE START OF CONSTRUCTION FOR THIS PROJECT. ALL PRUNING SHALL FOLLOW THE AMERICAN NATIONAL STANDARDS INSTITUTE (ANSI) A300 - PART 1 STANDARDS FOR PRUNING.

STREET TREE NOTES:
CONTRACTOR SHALL INSTALL TREE PROTECTION FENCING IN THE AREA BETWEEN THE CURB AND SIDEWALK AND EXTEND IT AT LEAST 5 FEET FROM BOTH SIDES OF THE TREE ALONG THE LENGTH OF THE TERRACE. NO EXCAVATION IS PERMITTED WITHIN 5 FEET OF THE OUTSIDE EDGE OF A TREE TRUNK. IF EXCAVATION WITHIN 5 FEET OF ANY TREE IS NECESSARY, CONTRACTOR SHALL CONTACT CITY FORESTRY (266-4816) PRIOR TO EXCAVATION TO ASSESS THE IMPACT TO THE TREE AND ROOT SYSTEM. TREE PRUNING SHALL BE COORDINATED WITH CITY FORESTRY PRIOR TO THE START OF CONSTRUCTION. TREE PROTECTION SPECIFICATIONS CAN BE FOUND IN SECTION 107.13 OF CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION
-HTTP://WWW.CITYOFMADISON.COM/BUSINESS/PW/DOCUMENTS/STDSPEC/2016/PART1.PDF.

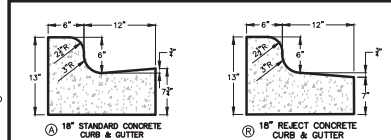
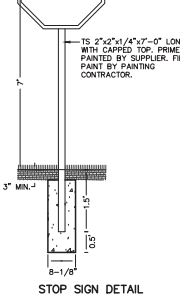
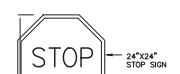
ANY TREE REMOVALS THAT ARE REQUIRED FOR CONSTRUCTION AFTER THE DEVELOPMENT PLAN IS APPROVED WILL A 72-HOUR WAIT PERIOD BEFORE A TREE REMOVAL PERMIT CAN BE ISSUED BY FORESTRY, TO NOTIFY THE ALDER OF THE CHANGE IN THE TREE PLAN.

GENERAL NOTES:
THE APPLICANT SHALL REPLACE ALL SIDEWALK AND CURB AND GUTTER THAT ABUTS THE PROPERTY THAT IS DAMAGED BY THE CONSTRUCTION, OR ANY SIDEWALK AND CURB AND GUTTER, WHICH THE CITY ENGINEER DETERMINES NEEDS TO BE REPLACED BECAUSE IT IS NOT AT A DESIRABLE GRADE, REGARDLESS OF WHETHER THE CONDITION EXISTED PRIOR TO BEGINNING CONSTRUCTION.

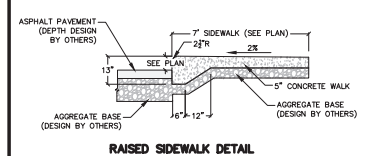
ALL WORK IN THE PUBLIC RIGHT OF WAY SHALL BE PERFORMED BY A CITY LICENSED CONTRACTOR.
ALL DAMAGE TO THE PAVEMENT ON INDUSTRIAL DRIVE ADJACENT TO THIS DEVELOPMENT SHALL BE RESTORED IN ACCORDANCE WITH THE CITY'S PAVEMENT PATCHING CRITERIA.

FENCING MUST CONSIST OF ELECTROSTATICALLY PAINTED, ANODIZED ALUMINUM OR HIGH QUALITY VINYL OR RESIN FENCING.

- GENERAL NOTES:**
-EXCAVATION BY CONCRETE CONTRACTOR -POSTS SHALL BE SET BY CONCRETE CONTRACTOR & PROVIDED BY STEEL SUPPLIER
-SIGN PROVIDED BY HARDWARE SUPPLIER & INSTALLED BY CARPENTER
-PIPE CLEAN EXCESS CONCRETE FROM POST BY CONCRETE CONTRACTOR
-CARPENTER TO DRILL HOLES IN STEEL POST FOR SIGN ATTACHMENT

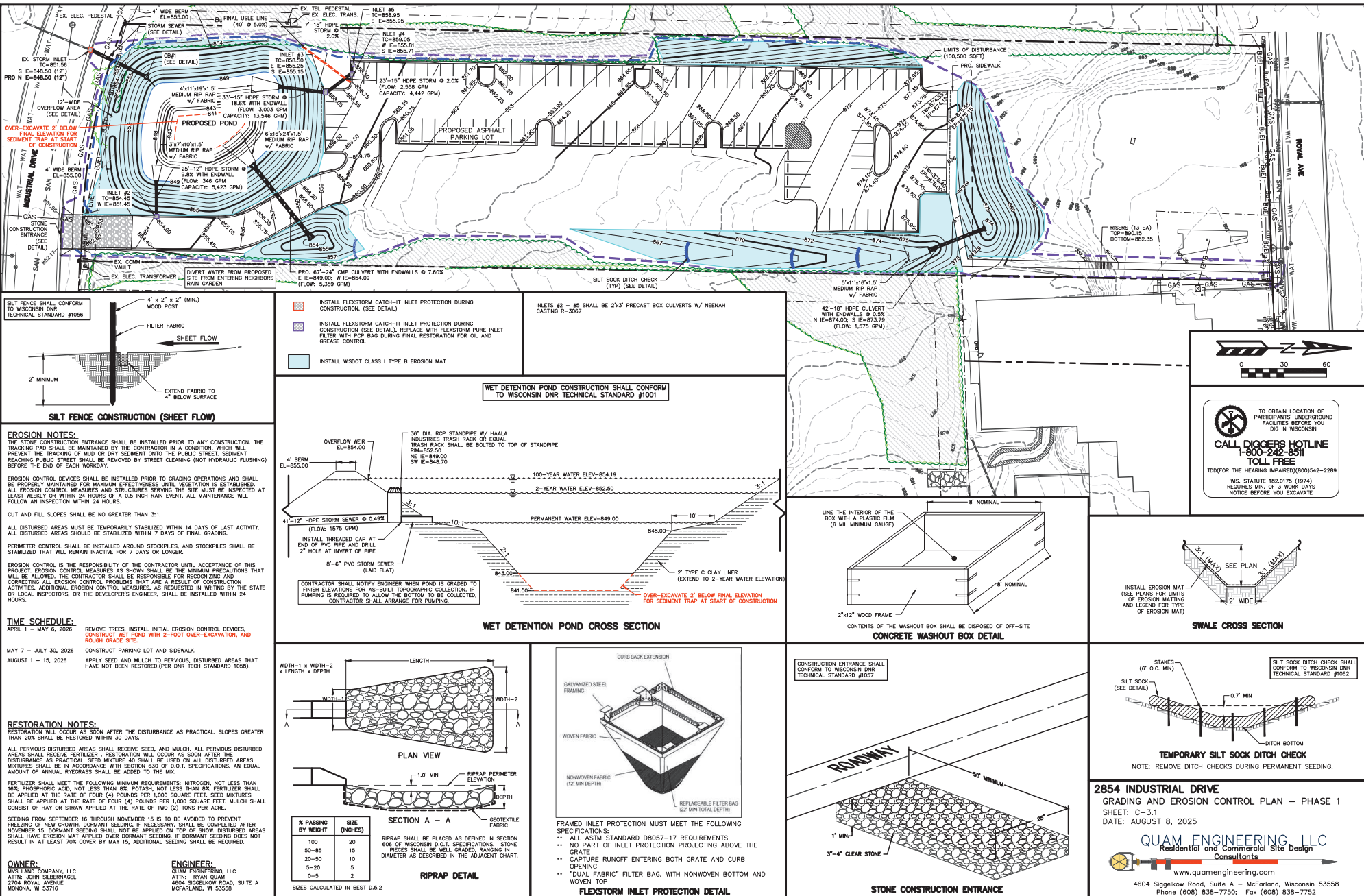


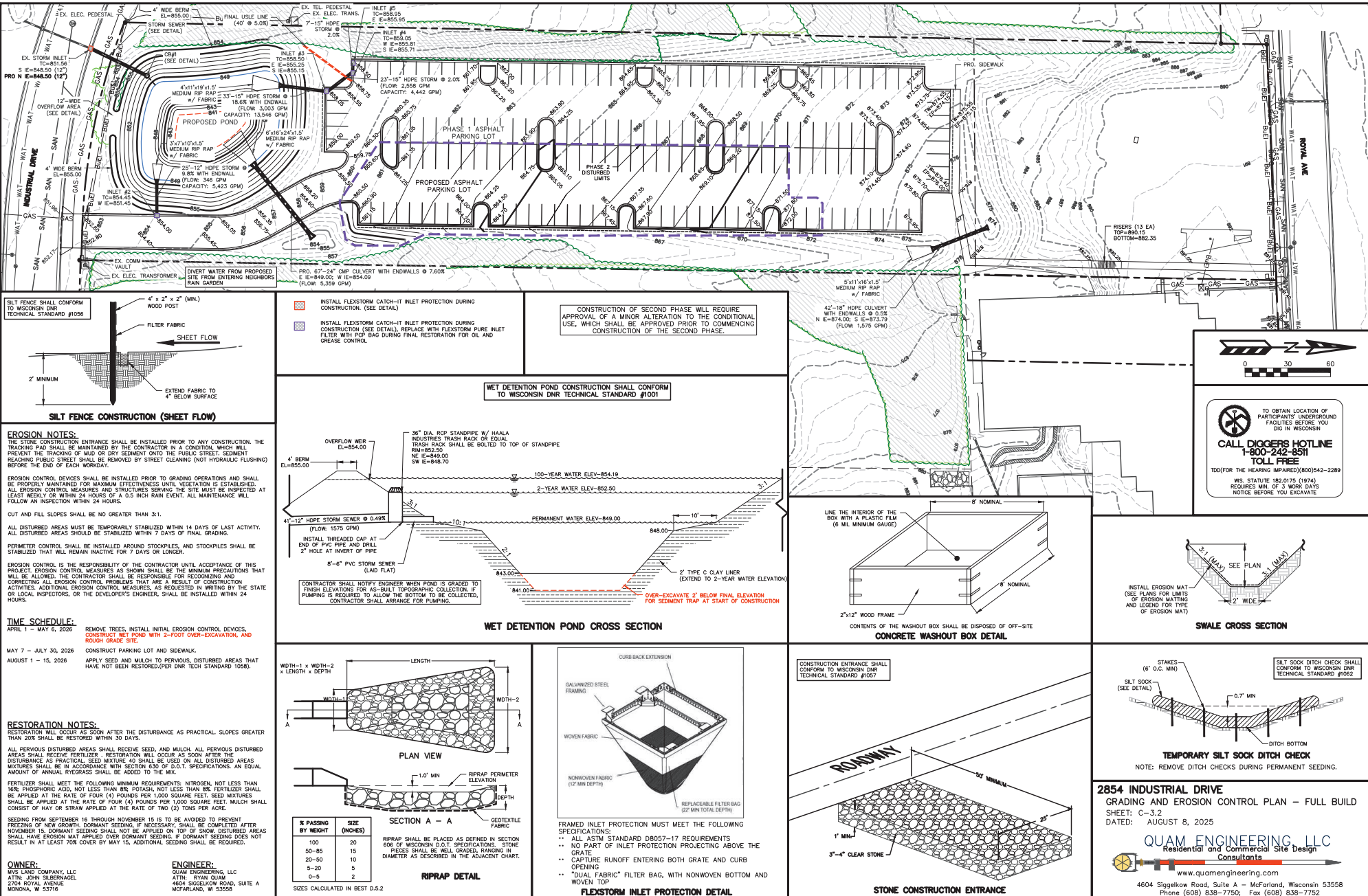
18" CONCRETE CURB DETAILS



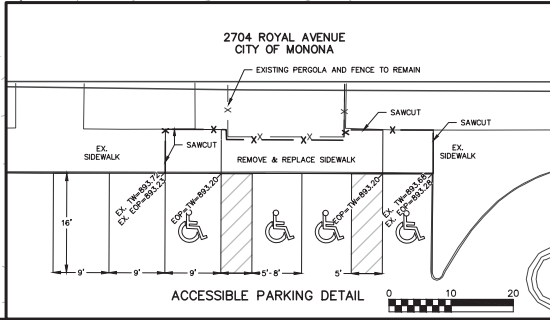
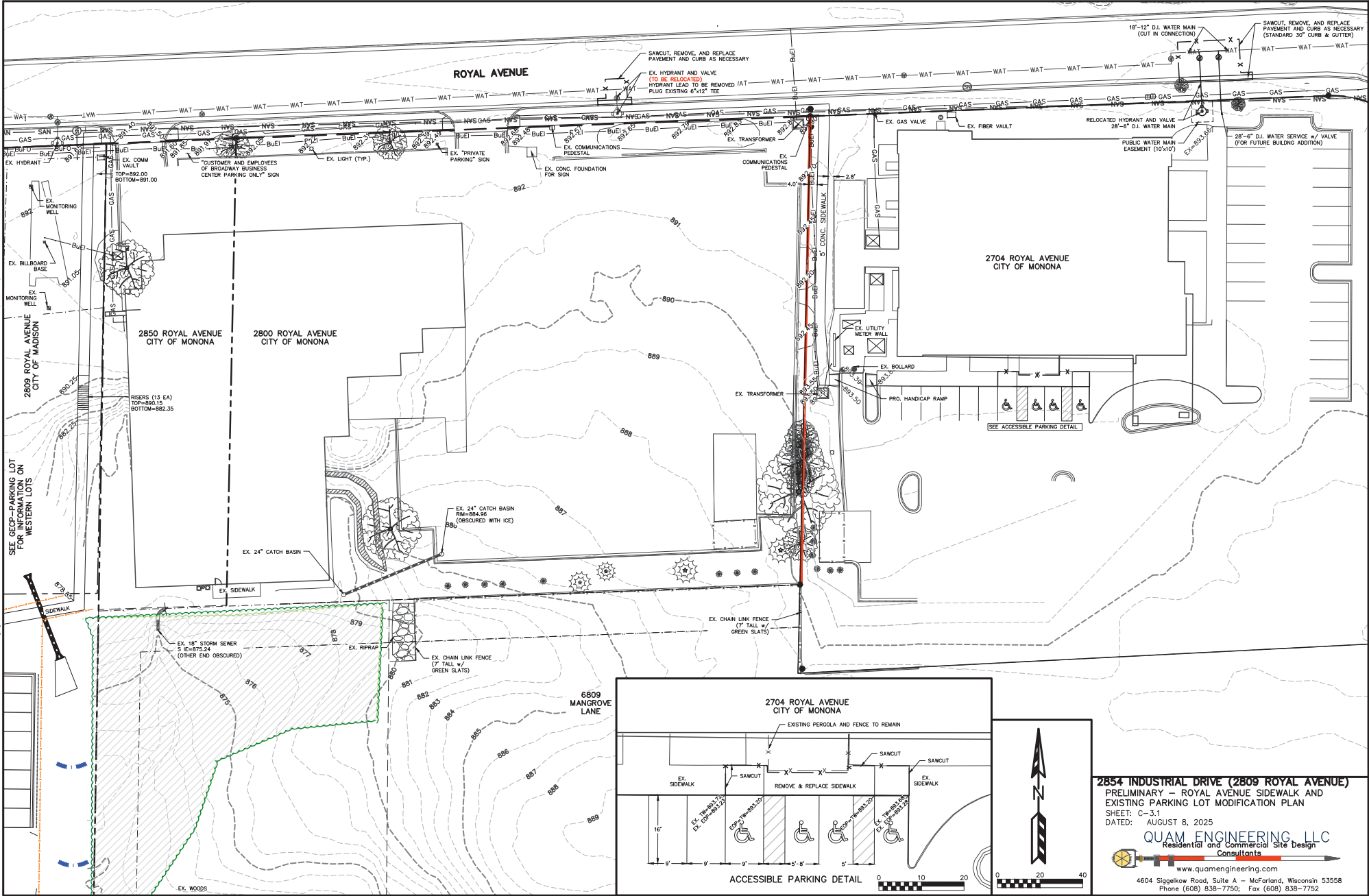
2854 INDUSTRIAL DRIVE (2809 ROYAL AVENUE)
PROPOSED SITE PLAN
SHEET: C-2
DATED: SEPTEMBER 20, 2024

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2854 INDUSTRIAL DRIVE (2809 ROYAL AVENUE)
PRELIMINARY - ROYAL AVENUE SIDEWALK AND
EXISTING PARKING LOT MODIFICATION PLAN
SHEET: C-3.1
DATED: AUGUST 8, 2025
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Consultants
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