

City of Madison Madison, WI 53703 www.cityofmadison.com

Agenda - Approved COMMON COUNCIL

Consider: Who benefits? Who is burdened?
Who does not have a voice at the table?
How can policymakers mitigate unintended consequences?

This meeting may be viewed LIVE on Charter Spectrum Channel 994, AT&T U-Verse Channel 99 or at www.madisoncitychannel.tv.

Tuesday, July 16, 2024

6:30 PM

210 Martin Luther King, Jr. Blvd. Room 201 (City-County Building)

HYBRID MEETING

The City of Madison is holding the Common Council meeting in a hybrid format. Members of the public may choose to view and/or provide comment in person or virtually at hybrid Common Council meetings.

- 1.Written Comments: You can send comments on agenda items to allalders@cityofmadison.com
- 2.Register but Do Not Speak: You can register your support or opposition to an agenda item at https://www.cityofmadison.com/MeetingRegistration
- 3.Register to Speak or to Answer Questions: If you wish to speak at the hybrid meeting on an agenda item, you must register. You can register at https://www.cityofmadison.com/MeetingRegistration. When you register to speak, you will be sent an email with the information you will need to join the hybrid meeting. You can participate in-person of virtually.
- 4. Watch the Meeting: You can call-in or watch the Common Council meeting in several ways:
- •In-person: Enter through the main doors at 210 Martin Luther King, Jr. Blvd and proceed to level 2.
- •Livestream on the Madison City Channel website:

https://media.cityofmadison.com/mediasite/showcase

•Livestream on the City of Madison YouTube channel:

https://www.youtube.com/user/CityofMadison

- •Television: Watch live on Spectrum channel 994 and AT&T U-Verse channel 99
- Listen to audio via phone: (877) 853-5257 (Toll Free)
 Webinar ID: 873 2611 6984

SPEAKING GUIDELINES

If you need an interpreter, translator, materials in alternate formats or other accommodations to access this service, activity or program, please call the phone number below immediately.

Si necesita un intérprete, un traductor, materiales en formatos alternativos u otros arreglos para acceder a este servicio, actividad o programa, comuníquese inmediatamente al número de teléfono que figura a continuación.

Yog tias koj xav tau ib tug neeg txhais lus, ib tug neeg txhais ntawv, cov ntaub ntawv ua lwm yam los sis lwm cov kev pab kom siv tau qhov kev pab, kev ua num los sis kev pab cuam no, thov hu rau tus xov tooj hauv qab no tam sim no.

Please contact the Office of the Common Council at (608) 266-4071.

Speaking Limit:

3 minutes for all items.

You must register before your item is considered by the Council.

The use of audible cell phone ringers and active use and response to cellular phone technology by the governing body, staff and members of the public is discouraged in the Council Chambers while the Council is in session.

ROLL CALL

NOTIFIED ABSENCES: None.

OPENING REMARKS

HONORING RESOLUTIONS

1. <u>84380</u> Expressing appreciation for the work and contributions of Deb Gilpin.

Sponsors: Nasra Wehelie, Satya V. Rhodes-Conway, Juliana R. Bennett, Nikki

Conklin, Jael Currie, John W. Duncan, Tag Evers, Derek Field, Yannette

Figueroa Cole, MGR Govindarajan, John P. Guequierre, Barbara

Harrington-McKinney, Isadore Knox Jr., Amani Latimer Burris, Sabrina V. Madison, Dina Nina Martinez-Rutherford, Charles Myadze, Marsha A. Rummel, William Tishler, Michael E. Verveer And Regina M. Vidaver

Legislative History

7/10/24 Council Office RECOMMEND TO COUNCIL TO ADOPT

PRESENTATIONS

Presentation of the 2024 Reverend James C. Wright Human Rights Award by the City of Madison Equal Opportunities Commission (EOC) and the

Department of Civil Rights - Equal Opportunities Division (EOD) to Reverend Dr. Marcus Allen, Sr.

Legislative History

6/25/24 EQUAL OPPORTUNITIES Refer to the COMMON COUNCIL

COMMISSION

3. <u>84386</u> Data Team presentation: Results Madison and national certification for What

Works Cities data award.

DISCLOSURES AND RECUSALS

Members of the body should make any required disclosures or recusals under the City's Ethics Code.

PRESENTATION OF CONSENT AGENDA

4. <u>83966</u> Consent Agenda Document (7/16/24)

Legislative History

6/17/24 Council Office RECOMMEND TO COUNCIL TO ACCEPT -

REPORT OF OFFICER

At this time, a consent agenda will be moved with the recommended action listed for each item EXCEPT:

1) Items which have registrants wishing to speak. 2) Items which Alder(s) have separated out for discussion/debate purposes.

PUBLIC COMMENT

Public comments will not be taken on items which are listed on the consent agenda for referral and the Council adopts the referral as part of the consent agenda OR when the Council has heard public comments on an item at a previous meeting and the item is on this agenda for discussion and action only. The Council may allow public comments in either instance by a majority vote.

STAFF PRESENTATION

5. <u>84385</u> Presentation on 2025 and Long-Range Budget Plan

PUBLIC HEARINGS

APPEALS

6. 84123 Appeal of the Plan Commission action on the conditional use request for

6610-6706 Old Sauk Road, Legistar ID 82972. (District 19)

Attachments: Link to Cond Use ID 82972

Appeal of Plan Commission Approval of Conditional Use w Enclosures.pdf

6610-6706 Old Sauk Rd Appeal Staff Memo.pdf

Public Comments.pdf

Public Comment 06-30-24.pdf

062524-070224 CC public comments.pdf Green Appeal Comments 07-02-24.pdf

Public Comment 07-05-24.pdf

Legislative History

6/21/24 Clerk's Office Referred for Introduction

Common Council Public Hearing (7/16/24)

7/2/24 COMMON COUNCIL Refer For Public Hearing to the COMMON

COUNCIL

REPORT OF ALCOHOL LICENSE REVIEW COMMITTEE

7. <u>83714</u> Public Hearing - New License

Handyspot 108 LLC • dba Handyspot 108 LLC 5551 Eastpark Blvd • Agent: Harvinder Singh

Class A Beer

Police Sector 520 (District 17)

Attachments: LICLIA-2024-00290 App.pdf

LICLIA-2024-00290 Supplemental.pdf

5551 Eastpark Blvd map.pdf

Legislative History

5/29/24 Clerk's Office Referred for Introduction

Alcohol License Review Committee- Public Hearing (6/26/24), Common Council (7/2/24)

6/4/24 COMMON COUNCIL Refer For Public Hearing to the ALCOHOL

LICENSE REVIEW COMMITTEE

6/26/24 ALCOHOL LICENSE RECOMMEND TO COUNCIL TO RE-REFER

REVIEW COMMITTEE - PUBLIC HEARING to the ALCOHOL

LICENSE REVIEW COMMITTEE

7/2/24 COMMON COUNCIL Re-refer for Recessed Public Hearing to the

ALCOHOL LICENSE REVIEW COMMITTEE

AGENDA NOTE: Alcohol License Review Committee meets 7/11/24 and a recommendation will be made from the floor.

8. <u>84234</u> Public Hearing - New License

Sriram Retails 3 LLC • dba Regent Street Mini Mart

1401 Regent St • Agent: Meenu Kaushal Estimated Capacity (in/out): 25 total

Class B Combination Liquor & Beer • 30% alcohol, 60% food, 10% other

Police Sector 205 (District 13)

Attachments: LICLIB-2024-00277 Updated App.pdf

LICLIB-2024-00277 App.pdf

LICLIB-2024-00277 Supplemental.pdf

1401 Regent St map.pdf
Alder Evers Comments.pdf

83198 VNA Comment Agenda Item 50.pdf

Fernside Comments.pdf
Outdoor capacity change.pdf
Outdoor seating area.pdf
VNA Comments.pdf
Carlson Comments.pdf

Legislative History

7/1/24 Clerk's Office Referred for Introduction

Alcohol License Review Committee- Public Hearing (7/11/24), Common Council (7/16/24)

7/2/24 COMMON COUNCIL Referred for Public Hearing to the ALCOHOL

LICENSE REVIEW COMMITTEE

AGENDA NOTE: Alcohol License Review Committee meets 7/11/24 and a recommendation will be made from the floor.

REPORT OF BOARD OF PUBLIC WORKS

9. <u>84253</u> Approving Plans, Specifications, And Schedule Of Assessments For South

Pinckney Street Resurfacing Assessment - 2024 (District 4).

Sponsors: Michael E. Verveer

Attachments: 11543EN-PinckneyResurfacing BPWExhibit.pdf

84253 BPW Mailing.pdf

11543 ExampleCCLtr Pinckney.pdf

Legislative History

7/1/24 Engineering Division Refer to the BOARD OF PUBLIC WORKS
7/10/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL TO ADOPT -

WORKS PUBLIC HEARING

REPORT OF PLAN COMMISSION

10. 83907 Creating Section 28.022-00676 of the Madison General Ordinances to change

the zoning of property located at 306 North Brooks Street from TR-U2

(Traditional Residential-Urban 2) District to RMX (Regional Mixed-Use) District.

(District 8)

Sponsors: Planning Division

COMMON COUNCIL Agenda - Approved July 16, 2024

Attachments: 83907-306 N Brooks Street.pdf

Staff Comments.pdf
Link to Demo File 83527
Link to Cond Use File 83529
Link to CSM File 83531
Staff Comments Addendum

Legislative History

6/11/24 Attorney's Office Referred for Introduction

Plan Commission (Public Hearing - 7/8/24), Common Council (7/16/24)

6/18/24 COMMON COUNCIL Referred for Public Hearing to the PLAN

COMMISSION

7/8/24 PLAN COMMISSION RECOMMEND TO COUNCIL TO ADOPT -

PUBLIC HEARING

On a motion by Ald. Guequierre, seconded by Ald. Duncan, the Plan Commission found the standards met and recommended approval of the zoning map amendment to the Common Council. The motion to recommend approval passed by voice vote/ other.

11. 83908

Creating Section 28.022-00677 of the Madison General Ordinances to approve an Amended Specific Implementation Plan of a Planned Development District for property located at 702 University Row and 5119 Silvertree Run. (District 19)

<u>Sponsors:</u> Planning Division

<u>Attachments:</u> <u>Locator Maps.pdf</u>

Application.pdf

Letter of Intent.pdf

Existing Site Plan.pdf

Staff Comments.pdf
702 University Row.pdf

Legislative History

6/11/24 Attorney's Office Referred for Introduction

Plan Commission (Public Hearing - 7/8/24), Common Council (7/16/24)

6/18/24 COMMON COUNCIL Referred for Public Hearing to the PLAN

COMMISSION

7/8/24 PLAN COMMISSION RECOMMEND TO COUNCIL TO ADOPT -

PUBLIC HEARING

On a motion by Ald. Duncan, seconded by Mendez, the Plan Commission found the standards met and recommended approval of the zoning map amendment to the Common

Council. The motion to recommend approval passed by voice vote/ other.

END OF PUBLIC HEARINGS

BUSINESS PRESENTED BY THE MAYOR

APPOINTMENTS

12. Report of the Mayor submitting resident committee appointments (introduction 7-2-2024; action 7-16-2024).

Legislative History

6/26/24 Mayor's Office Referred for Introduction

Confirm 7/16/24

7/2/24 COMMON COUNCIL Refer to a future Meeting to Confirm to the

COMMON COUNCIL

Confirm 7/16/24

BUSINESS PRESENTED BY THE PRESIDENT OF THE COMMON COUNCIL

13. 83971 Confirming the Madison Common Council meeting formats through December

10, 2024:

8/6/24 - Hybrid (Virtual & CCB 201)

8/20/24 - Virtual

9/10/24 - Hybrid (Virtual & CCB 201) 9/24/24 - Hybrid (Virtual & CCB 201)

10/8/24 - Hybrid (Virtual & CCB 201)

10/29/24 - Hybrid (Virtual & CCB 201)

11/12/24 (Budget) - Hybrid (Virtual & CCB 201)

11/13/24 (Budget, if needed) - Hybrid (Virtual & CCB 201)

11/14/24 (Budget, if needed) - Hybrid (Virtual & CCB 201)

11/26/24 - Virtual

12/10/24 - Hybrid (Virtual & CCB 201)

Legislative History

6/17/24 Council Office RECOMMEND TO COUNCIL TO ACCEPT -

REPORT OF OFFICER

REPORTS OF OFFICERS

REPORT OF BOARD OF HEALTH FOR MADISON AND DANE COUNTY

14. 83905 Authorizing a non-competitive service contract with UW-Madison's Population

Health Institute (contracted under Board of Regents of the University of Wisconsin System on behalf of the University of Wisconsin-Madison) for continued data services to support updates to the Dane County Respiratory

Pathogens dashboard

Sponsors: Yannette Figueroa Cole

Attachments: Non-Competitive Selection Request

Legislative History

6/11/24 Health Department Referred for Introduction

Board of Health for Madison and Dane County (7/15/24), Finance Committee (6/24/24),

Common Council (7/16/24)

6/18/24 COMMON COUNCIL Referred to the BOARD OF HEALTH FOR

MADISON AND DANE COUNTY

Additional referral to Finance Committee

6/18/24 BOARD OF HEALTH FOR Referred to the FINANCE COMMITTEE

MADISON AND DANE

COUNTY

6/24/24 FINANCE COMMITTEE

Return to Lead with the Recommendation for Approval to the BOARD OF HEALTH FOR MADISON AND DANE COUNTY

AGENDA NOTE: Board of Health for Madison and Dane County meets 7/15/24 and a recommendation will be made from the floor.

REPORT OF BOARD OF PUBLIC WORKS

15. 83632 SUBSTITUTE: Amending Section 10.18 and 1.08(3)(a), and creating Section

27.05(2)(dd) of the Madison General Ordinances to update the City's large item collection and electronic waste recycling ordinances, and to update the bond

schedule accordingly.

Sponsors: Satya V. Rhodes-Conway, Yannette Figueroa Cole, Derek Field And

Sabrina V. Madison

Attachments: 83632 Body-Version 2

83634 - Large Item & Electronics Resolution

070124 CC public comment.pdf

<u>070824FinanceCmte WrittenComment2.pdf</u> <u>070824FinanceCmte WrittenComment1.pdf</u>

83632 Body FC Amendment.pdf

83632-Version 1

83632 Body-Version 1

Legislative History

5/24/24	, ,,	Referred for Introduction g Code, Fire Code, Conveyance Code, and nce Committee (6/10/24), Common Council
6/4/24	COMMON COUNCIL	Refer to the BOARD OF PUBLIC WORKS e Code, Conveyance Code and Licensing Appeals
6/5/24	BOARD OF PUBLIC WORKS	Refer to the BUILDING CODE, FIRE CODE, CONVEYANCE CODE AND LICENSING APPEALS BOARD
6/5/24	BOARD OF PUBLIC WORKS	Refer to the FINANCE COMMITTEE
6/10/24	FINANCE COMMITTEE	Return to Lead with the Following Recommendation(s)

Trecommendation(3)

Re-refer to Finance Committee on 7/8/24 and Common Council on 7/16/24.

6/18/24 BUILDING CODE, FIRE Return to Lead with the Recommendation for

CODE, CONVEYANCE Approval to the BOARD OF PUBLIC

CODE AND LICENSING WORKS

APPEALS BOARD

6/26/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL WITH THE

WORKS FOLLOWING RECOMMENDATIONS -

REPORT OF OFFICER

A motion was made by Ald Guequierre, seconded by Kliems, to RECOMMEND TO COUNCIL WITH THE FOLLOWING RECOMMENDATIONS: Re-refer to Finance Committee (7/8/24) and Board of Public Works (7/10/24) - REPORT OF OFFICER. The motion passed

	by voice vote/other.		
7/2/24	FINANCE COMMITTEE	Refer to the BOARD OF PUBLIC WORKS	
7/2/24	COMMON COUNCIL Additional re-referral to Board of Public	Re-refer to the FINANCE COMMITTEE Works.	
7/8/24	FINANCE COMMITTEE Amend A motion was made by Verveer, seconded by Madison, to Amend to change the effective date in update #9 to October 1, 2024 (see Leg. File 83632 Body_FC Amendment in Attachments). The motion passed by voice vote/other.		
	The amended motion to return to lead Public Works passed by voice vote/oth	with recommendation for approval to the Board of er.	
7/8/24	FINANCE COMMITTEE	Return to Lead with the Recommendation for Approval to the BOARD OF PUBLIC WORKS	
7/10/24	BOARD OF PUBLIC WORKS	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER	

83634 Approving the Large Item Collection and Electronic Waste Recycling Policy.

Sponsors: Satya V. Rhodes-Conway, Yannette Figueroa Cole, Derek Field And

Sabrina V. Madison

<u>Attachments:</u> <u>Large Item and Electronic Waste Policy-Ver 2.0.pdf</u>

Large Item and Electronic Waste Policy

83632 - Large Item & Electronics Ordinance

070824FinanceCmte WrittenComment2.pdf

070824FinanceCmte WrittenComment1.pdf

Legislative History

16.

5/24/24	* **	Referred for Introduction ng Code, Fire Code, Conveyance Code, and ance Committee (6/10/24), Common Council
6/4/24	COMMON COUNCIL Additional referrals to Building Code, Fin Appeals Board, Finance Committee	Refer to the BOARD OF PUBLIC WORKS re Code, Conveyance Code, and Licensing
6/5/24	BOARD OF PUBLIC WORKS	Refer to the BUILDING CODE, FIRE CODE, CONVEYANCE CODE AND LICENSING APPEALS BOARD
6/5/24	BOARD OF PUBLIC WORKS	Refer to the FINANCE COMMITTEE
6/10/24	FINANCE COMMITTEE	Return to Lead with the Following Recommendation(s) to the BOARD OF PUBLIC WORKS
	Re-refer to Finance Committee on 7/8/2	4 and Common Council on 7/16/24
6/18/24	BUILDING CODE, FIRE CODE, CONVEYANCE CODE AND LICENSING APPEALS BOARD	Return to Lead with the Recommendation for Approval to the BOARD OF PUBLIC WORKS
6/26/24	BOARD OF PUBLIC WORKS	RECOMMEND TO COUNCIL WITH THE FOLLOWING RECOMMENDATIONS - REPORT OF OFFICER

A motion was made by Ald. Guequierre, seconded by Clausius, to RECOMMEND TO

COUNCIL WITH THE FOLLOWING RECOMMENDATIONS: re-refer to Finance Committee (7/8/24) and Board of Public Works (7/10/24)- REPORT OF OFFICER. The motion passed

by voice vote/other.

7/2/24 FINANCE COMMITTEE Refer to the BOARD OF PUBLIC WORKS
7/2/24 COMMON COUNCIL Re-refer to the FINANCE COMMITTEE

Additional referral to Board of Public Works.

7/8/24 FINANCE COMMITTEE Return to Lead with the Recommendation for

Approval to the BOARD OF PUBLIC

WORKS

The motion to return to lead with the recommendation for approval to the Board of Public Works passed by the following vote: 5-1. Aye: Figueroa Cole, Currie, Madison, Evers, Rummel. Nay: Verveer. Non-voting: Rhodes-Conway. Motion to return to lead with the

recommendation for approval passed.

7/10/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL TO ADOPT -

WORKS REPORT OF OFFICER

17. 83793 Assessing Benefits Halo Lane and Twilight Trail Resurfacing Assessment

District - 2023. (District 16)

<u>Sponsors:</u> BOARD OF PUBLIC WORKS <u>Attachments:</u> 13011 HaloAssessmentsFinal.pdf

Legislative History

6/4/24 Engineering Division Refer to the BOARD OF PUBLIC WORKS
6/26/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL TO ADOPT -

WORKS REPORT OF OFFICER

18. 83794 Accepting storm sewer improvements constructed by Private Contract for

413-429 Yellowstone Drive Storm Sewer Relocation, Private Contract No. 2341

(District 19).

Sponsors: BOARD OF PUBLIC WORKS

<u>Attachments:</u> Yellowstone Dr Storm Schedule A Unit Cost.pdf

Legislative History

6/4/24 Engineering Division Refer to the BOARD OF PUBLIC WORKS
6/26/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL TO ADOPT -

WORKS REPORT OF OFFICER

19. <u>83795</u> Approving plans and specifications and authorizing the Board of Public Works

to advertise and receive bids for CIPP Lining Rehabilitation - Blair Street

Sanitary Sewer Siphon.(District 4 & District 6)

Sponsors: BOARD OF PUBLIC WORKS

Attachments: 15295 BPW Exhibit 5-30-24.pdf

Legislative History

6/18/24 Engineering Division Refer to the BOARD OF PUBLIC WORKS
6/26/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL TO ADOPT -

WORKS REPORT OF OFFICER

20. 83995 Accepting street improvements (sidewalk) constructed by Private Contract 502

- 504 W. Main Street, Private Contract No. 2392 (District 4).

COMMON COUNCIL Agenda - Approved July 16, 2024

Sponsors: BOARD OF PUBLIC WORKS

Attachments: 2392 Sidewalk Schedule A.pdf

Legislative History

6/18/24 Engineering Division Refer to the BOARD OF PUBLIC WORKS
6/26/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL TO ADOPT -

WORKS REPORT OF OFFICER

21. 83996 Accepting storm sewer and street improvements (excluding bituminous surface

pavement) constructed by Private Contract In Secret Places at Siggelkow

Preserve - Phase 6, Private Contract No. 2251 (District 16).

Sponsors: BOARD OF PUBLIC WORKS

Attachments: 2251 Street Schedule A excluding bituminous.pdf

Secret Places Ph 6 Storm Schedule A Unit Cost.pdf

Legislative History

6/18/24 Engineering Division Refer to the BOARD OF PUBLIC WORKS
6/26/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL TO ADOPT -

WORKS REPORT OF OFFICER

22. <u>83997</u> Declaring the City of Madison's intention to exercise its police powers

establishing the South Pinckney Street Resurfacing Assessment District -

2024.(District 4)

Sponsors: Michael E. Verveer

Legislative History

6/18/24 Engineering Division Refer to the BOARD OF PUBLIC WORKS
6/26/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL TO ADOPT -

WORKS REPORT OF OFFICER

23. 83998 Accepting street improvements (sidewalk) constructed by Private Contract In

1109-1123 S Park Street, Private Contract No. 8926 (District 13).

Sponsors: BOARD OF PUBLIC WORKS

Attachments: 8926 Street (Sidewalk) Schedule A.pdf

Legislative History

6/18/24 Engineering Division Refer to the BOARD OF PUBLIC WORKS
6/26/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL TO ADOPT -

WORKS REPORT OF OFFICER

24. <u>84000</u> Approving plans and specifications and authorizing the Board of Public Works

to advertise and receive bids for McPike Park - Building Demolition and Site

Restoration. (District 6)

Sponsors: BOARD OF PUBLIC WORKS

<u>Attachments:</u> Contract 9174 McPike Park Building Demo Plans 06-07-24.pdf

Legislative History

6/20/24 Engineering Division Refer to the BOARD OF PUBLIC WORKS
6/26/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL TO ADOPT -

WORKS REPORT OF OFFICER

25. 84001 Awarding Public Works Contract No. 8635, Knutson Drive and Green Avenue

Assessment District - 2024. (District 18)

Sponsors: BOARD OF PUBLIC WORKS

Attachments: 9635 BidOpeningTab.pdf

8635 KNUTSON AWARD revised.pdf

Legislative History

6/20/24 Engineering Division Refer to the BOARD OF PUBLIC WORKS
6/26/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL TO ADOPT -

WORKS REPORT OF OFFICER

26. 84002 Awarding Public Works Contract No. 9522, 20 is Plenty. (Citywide)

Sponsors: BOARD OF PUBLIC WORKS

Attachments: 9522BidOpeningTab.pdf

9522 20 IS PLENTY AWARD.pdf

Legislative History

6/20/24 Engineering Division Refer to the BOARD OF PUBLIC WORKS
6/26/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL TO ADOPT -

WORKS REPORT OF OFFICER

27. 84003 Awarding Public Works Contract No. 9527, Madison Water Utility Olin Ave.

Administrative Building Roof Replacement. (District 14)

<u>Sponsors:</u> BOARD OF PUBLIC WORKS

<u>Attachments:</u> 9527 BidOpeningTab.pdf

Legislative History

6/20/24 Engineering Division Refer to the BOARD OF PUBLIC WORKS
6/26/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL TO ADOPT -

WORKS REPORT OF OFFICER

28. 84029 Approving plans and specifications and authorizing the Board of Public Works

to advertise and receive bids for West Towne Path - Phase 2a. (District 9)

Sponsors: BOARD OF PUBLIC WORKS

Attachments: 10169-WestTownePathPh2a_BPWExhibit.pdf

Legislative History

6/20/24 Engineering Division Refer to the BOARD OF PUBLIC WORKS
6/26/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL TO ADOPT -

WORKS REPORT OF OFFICER

29. 84245 Awarding Public Works Contract No. 8744, Atticus Way Assessment District

(District 10)

Sponsors: BOARD OF PUBLIC WORKS

Attachments: 8744 BidOpeningTab Jun272024.pdf

8744 award.pdf

Legislative History

7/1/24 Engineering Division Refer to the BOARD OF PUBLIC WORKS
7/10/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL TO ADOPT -

WORKS REPORT OF OFFICER

30. 84248 Awarding Public Works Contract No. 9521, 2024 Playground Improvements

(District 7, District 10, District 11, and District 20).

Sponsors: BOARD OF PUBLIC WORKS

Attachments: 9521 BidOpeningTab Jun272024.pdf

9521 BidBreakdown.pdf

Legislative History

7/1/24 Engineering Division Refer to the BOARD OF PUBLIC WORKS
7/10/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL TO ADOPT -

WORKS REPORT OF OFFICER

31. 84258 Approving plans and specifications and authorizing Change Order 1 to Contract

8717, Mineral Point Road Widen Sidewalk (for East-West Bus Rapid Transit),

to Parisi Construction LLC, for \$86,000. (District 19)

Sponsors: BOARD OF PUBLIC WORKS

Attachments: Nautilus Point Park 8717 Added Specifications and Changes.pdf

8717 Revision Plan Supplement.pdf CO 1 - 8717 REV20240708.pdf

Nautilus Park CO1 BPW Presentation.pdf

Legislative History

7/1/24 Engineering Division Refer to the BOARD OF PUBLIC WORKS
7/10/24 BOARD OF PUBLIC RECOMMEND TO COUNCIL TO ADOPT -

WORKS REPORT OF OFFICER

REPORT OF CITY CLERK

32. Report of Operator License Applications July 16, 2024. See attached report for

list of operators.

Attachments: Operator Report 7.11.2024.pdf

Legislative History

4/10/24 Clerk's Office RECOMMEND TO COUNCIL TO GRANT -

REPORT OF OFFICER

REPORT OF DEPARTMENT OF PLANNING AND COMMUNITY AND ECONOMIC DEVELOPMENT

33. 83528 Approving a Certified Survey Map of property owned by H & E Development,

LLC located at 2221 Mustang Way and 5026 Reef Court (District 16).

 Sponsors:
 Planning Division

 Attachments:
 Application.pdf

 Proposed CSM.pdf

CSM Approval Letter.pdf

COMMON COUNCIL Agenda - Approved July 16, 2024

Legislative History

7/1/24 Department of Planning and

RECOMMEND TO COUNCIL TO ADOPT UNDER SUSPENSION OF MGO 2.055 -Community and Economic

Development REPORT OF OFFICER

34. 84372 Re-Approving a Certified Survey Map of property owned by the Islamic Center of

East Madison Corporation located at 4002 Lien Road (District 17).

Planning Division Sponsors:

Originally Approved CSM 09-15-22.pdf Attachments:

Conditional Approval Letter 10-18-22.pdf

Updated CSM 07-03-24.pdf Link to Resolution ID 73824

Legislative History

7/9/24 RECOMMEND TO COUNCIL TO ADOPT Department of Planning and

> Community and Economic UNDER SUSPENSION OF MGO 2.055 -

Development REPORT OF OFFICER

The proposed Certified Survey Map has been administratively re-approved as allowed by MGO Section 16.23(4)(f) subject to the conditions included in the attached letter. Staff

recommends adoption of the resolution.

REPORT OF FINANCE COMMITTEE

35. 83723 Authorizing a non-competitive service contract between the Madison Area

Technical College and the City of Madison Fire Department for the provision of

paramedic training, 2024-2025 cohort. (Citywide)

Derek Field, John W. Duncan And Amani Latimer Burris Sponsors:

Legislative History

5/29/24 Referred for Introduction Fire Department

Finance Committee (7/8/24), Common Council (7/16/24)

7/2/24 COMMON COUNCIL Refer to the FINANCE COMMITTEE

FINANCE COMMITTEE 7/8/24 RECOMMEND TO COUNCIL TO ADOPT -

REPORT OF OFFICER

36. 83518 Requesting information, studies, and plans regarding the authorization of a local

sales tax.

Amani Latimer Burris Sponsors:

Transportation Committee Memo for Resolution 83518.pdf Attachments:

Part4Powerpoint.pdf

2025 Operating Budget Update to FC -- 6-10-2024 SB slide added.pdf

Legislative History

5/17/24 Council Office Referred for Introduction

Finance Committee (6/24/24), Common Council Executive Committee (6/4/24), Committee

on Employee Relations (6/11/24), Transportation Commission (6/12/24), Equal

Opportunities Commission (6/13/24), Common Council (7/2/24)

5/21/24 COMMON COUNCIL Refer to the FINANCE COMMITTEE

> Additional referrals to Common Council Executive Committee (6/4/24), Committee on Employee Relations (6/11/24), Transportation Commission (6/12/24), and Equal

	Opportunities Commission (6/13/24)	
5/21/24	FINANCE COMMITTEE	Referred to the COMMON COUNCIL EXECUTIVE COMMITTEE
5/21/24	FINANCE COMMITTEE	Referred to the COMMITTEE ON EMPLOYEE RELATIONS
5/21/24	FINANCE COMMITTEE	Referred to the TRANSPORTATION COMMISSION
5/21/24	FINANCE COMMITTEE	Referred to the EQUAL OPPORTUNITIES COMMISSION
6/4/24		Return to Lead with the Recommendation to Place on File without Prejudice to the FINANCE COMMITTEE ion to Place on File without Prejudice to the
	-	assed 6-0 by the following roll call vote: Govindarajan, Guequierre, Harrington-McKinney:
6/12/24	COMMITTEE ON EMPLOYEE RELATIONS	Return to Lead with the Following Recommendation(s) to the COMMON COUNCIL
	Unanimous motion by CER to make no	o recommendation.
6/12/24	TRANSPORTATION COMMISSION	Return to Lead with the Following Recommendation(s) to the FINANCE COMMITTEE
	Accept the report that was sent to the	Transportation Commission
6/24/24	FINANCE COMMITTEE	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER
		seconded by Rummel, to RECOMMEND TO OFFICER. The motion failed by the following vote:
	Aye: Rummel Noes: Figueroa Cole, Evers, Madison Excused: Currie, Verveer Non Voting: Rhodes Conway	
	Due to failure of the motion to adopt, th WITHOUT PREJUDICE.	ne resultant recommendation is to PLACE ON FILE
7/2/24	COMMON COUNCIL	Refer to the EQUAL OPPORTUNITIES COMMISSION

AGENDA NOTE: Equal Opportunities Commission meets 7/11/24 and a recommendation will be made from the floor.

37. <u>83872</u> Authorizing the execution of agreements and associated easements with

Madison Gas and Electric Company relating to the installation, operation, maintenance and repair of electric vehicle charging stations at the Yahara Hills

Golf Course. (District 16)

Sponsors: Yannette Figueroa Cole

Attachments: Yahara Hills Golf Course Charging MGE redlines 06 06 24.pdf

<u>Master</u>

COMMON COUNCIL Agenda - Approved July 16, 2024

Legislative History

6/10/24	Parks Division Finance Committee (6/24/24), Board of (7/16/24)	Referred for Introduction Park Commissioners (7/10/24), Common Council
6/18/24	COMMON COUNCIL Additional referral to Board of Park Com	Referred to the FINANCE COMMITTEE missioners
6/18/24	FINANCE COMMITTEE	Referred to the BOARD OF PARK COMMISSIONERS
6/24/24	FINANCE COMMITTEE	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER
7/10/24	BOARD OF PARK COMMISSIONERS	Return to Lead with the Recommendation for Approval to the Finance Department

38. <u>83887</u>

Authorizing the Mayor and City Clerk to amend the contract for Purchase of Services (Architect) with Engberg Anderson, Inc. to provide extra professional architectural and engineering consultant design services and construction administrative services for the expanded design and construction scope at the existing Warner Park Community Recreation Center located at 1625 Northport Drive. (District 12)

Sponsors: Yannette Figueroa Cole, Amani Latimer Burris And Charles Myadze

Attachments: 9170 Contract Amendment 02 052324.pdf

9170 Attachment A.pdf

Legislative History

6/11/24	Engineering Division Finance Committee (7/8/24), Board of F	Referred for Introduction Public Works (6/26/24), Common Council (7/16/24)
6/18/24	COMMON COUNCIL	Referred to the BOARD OF PUBLIC WORKS
	Additional referral to Finance Committee	e
6/18/24	BOARD OF PUBLIC WORKS	Referred to the FINANCE COMMITTEE
6/26/24	BOARD OF PUBLIC WORKS	Return to Lead with the Recommendation for Approval to the Finance Department
7/8/24	FINANCE COMMITTEE	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER

39. 84167

Establishing rules and procedures for Common Council deliberations on the City of Madison's 2025 Capital and Operating Budgets.

Sponsors: Yannette Figueroa Cole

Attachments: 2025 Budget Calendar (File 84167).pdf

Legislative History

6/25/24	Finance Department Finance Committee (7/8/24), Commor	Referred for Introduction Council (7/16/24)
7/2/24	COMMON COUNCIL	Refer to the FINANCE COMMITTEE
7/2/24	COMMON COUNCIL	Add Referral(s)
7/8/24	FINANCE COMMITTEE	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER

40. 84174

Amending the 2024 Adopted Capital Budget of Engineering-Other to appropriate \$200,000 of Landfill Reserves to fund a new project for Landfill Generators at the Greentree and Mineral Point Landfills. (District 19 & District 20)

Sponsors: John P. Guequierre And Barbara Harrington-McKinney

Legislative History

6/26/24 Engineering Division Referred for Introduction

Finance Committee (7/8/24), Common Council (7/16/24)

7/2/24 COMMON COUNCIL Refer to the FINANCE COMMITTEE

7/8/24 FINANCE COMMITTEE RECOMMEND TO COUNCIL TO ADOPT

(15 VOTES REQUIRED) - REPORT OF

OFFICER

41. 84175

Authorizing the Transportation Department, to submit a grant application to the Wisconsin Department of Transportation for up to \$500,000 dollars, accept grant funding if awarded, and authorizing the Mayor and Clerk to sign the resulting grant agreement.

Sponsors: MGR Govindarajan, Marsha A. Rummel, Barbara Harrington-McKinney,

Michael E. Verveer And Derek Field

Attachments: 070824FinanceCmte_WrittenComment.pdf

Legislative History

6/26/24 Transportation Department Referred for Introduction

Finance Committee (7/8/24), Common Council (7/16/24)

7/2/24 COMMON COUNCIL Refer to the FINANCE COMMITTEE

7/8/24 FINANCE COMMITTEE RECOMMEND TO COUNCIL TO ADOPT -

REPORT OF OFFICER

42. 84178

Authorizing the City of Madison Traffic Engineering Division to submit, and if awarded, to accept a grant of up to \$920,000 from the U.S. Department of Transportation Federal Highway Administration to develop a plan and design for a preferred alternative for a north side, all-ages, all-ability, bicycle and pedestrian facility and authorizing the Mayor and Clerk to sign the resulting grant agreement (District 12 & District 18).

Sponsors: Satya V. Rhodes-Conway, Dina Nina Martinez-Rutherford, Charles Myadze

And Amani Latimer Burris

Attachments: 070824FinanceCmte WrittenComment.pdf

Legislative History

6/26/24 Traffic Engineering Division Referred for Introduction

Finance Committee (7/8/24), Common Council (7/16/24)

7/2/24 COMMON COUNCIL Refer to the FINANCE COMMITTEE

7/8/24 FINANCE COMMITTEE RECOMMEND TO COUNCIL TO ADOPT -

REPORT OF OFFICER

43. 84277

Amending the 2024 Adopted Capital Budget for Traffic Engineering - Traffic Safety Infrastructure to accept a \$177,584 grant from the Wisconsin Department of Transportation, and authorizing the Mayor and City Clerk to execute the updated agreement, Project I.D. 5992-22-12 Safe Routes to School Plan.

Sponsors: Yannette Figueroa Cole And Satya V. Rhodes-Conway

Legislative History

7/2/24 COMMON COUNCIL Refer to the FINANCE COMMITTEE

7/2/24 Traffic Engineering Division Referred for Introduction

Finance Committee (7/8/24), Common Council (7/16/24)

7/8/24 FINANCE COMMITTEE RECOMMEND TO COUNCIL TO ADOPT

(15 VOTES REQUIRED) - REPORT OF

OFFICER

REPORT OF PLAN COMMISSION

44. 83530 Approving a Certified Survey Map of property owned by LZ, LLC located at 521

E Washington Avenue (District 6).

<u>Sponsors:</u> Planning Division

<u>Attachments:</u> <u>Locator Maps.pdf</u>

Application.pdf
Proposed CSM.pdf
Staff Comments.pdf

Link to Demo Permit File 83525
Link to Cond Use File 83526
Public Comments 07-08-24.pdf

Legislative History

7/8/24 PLAN COMMISSION RECOMMEND TO COUNCIL TO ADOPT

UNDER SUSPENSION OF MGO 2.055 -

REPORT OF OFFICER

On a motion by Solheim, seconded by Ald. Guequirre, the Plan Commission found that the standards were met and recommended approval of the Certified Survey Map subject to the comments and conditions contained in the Plan Commission materials. The motion to

approve passed by voice vote/ other.

45. 83531 Approving a Certified Survey Map of property owned by LZ Ventures, LLC

located at 306 N Brooks Street (District 8).

<u>Sponsors:</u> Planning Division

<u>Attachments:</u> <u>Locator Maps.pdf</u>

Application.pdf
Proposed CSM.pdf
Staff Comments.pdf

Link to Demo Permit File 83527
Link to Rezoning File 83907
Link to Cond Use File 83529

Legislative History

7/8/24 PLAN COMMISSION RECOMMEND TO COUNCIL TO ADOPT

UNDER SUSPENSION OF MGO 2.055 -

REPORT OF OFFICER

On a motion by Ald. Guequierre, seconded by Ald. Duncan, the Plan Commission found that the standards were met and recommended approval of the Certified Survey Map subject to the comments and conditions contained in the Plan Commission materials. The

motion to approve passed by voice vote/ other.

46. 83902 Re-approving the final plat of *The American Center Eastpark Fifth Addition* subdivision on land generally addressed as 4846 Eastpark Boulevard (District

17).

<u>Sponsors:</u> Planning Division

<u>Attachments:</u> <u>Locator Maps.pdf</u>

Subdivision Application.pdf

Letter of Intent.pdf
Proposed Final Plat.pdf
2023 Plat Approval Letter.pdf

Staff Memo.pdf

Link to Plat Resolution ID 74911

Legislative History

6/11/24 Planning Division Referred for Introduction

Plan Commission (7/8/24), Common Council (7/16/24)

6/18/24 COMMON COUNCIL Referred to the PLAN COMMISSION
7/8/24 PLAN COMMISSION RECOMMEND TO COUNCIL TO ADOPT

WITH CONDITIONS - REPORT OF

OFFICER

On a motion by Ald. Duncan, seconded by Mendez, the Plan Commission found the standards met and recommended re-approval of the final plat subject to the comments and conditions contained in the Plan Commission materials. The motion passed by voice vote/other.

REPORT OF RISK MANAGER

47.	<u>83934</u>	B. Coon - Property Damage - \$104.34

Legislative History

7/2/24 COMMON COUNCIL Referred to the Risk Manager

7/10/24 Risk Manager RECOMMEND TO COUNCIL TO DISALLOW

- REPORT OF OFFICER

48. 83937 J. Vendette - Vehicle Damage - \$227.00

Legislative History

7/2/24 COMMON COUNCIL Referred to the Risk Manager

7/10/24 Risk Manager RECOMMEND TO COUNCIL TO DISALLOW

- REPORT OF OFFICER

49. 83981 H. Frank - Property Damage - \$2,101.99

Legislative History

7/2/24 COMMON COUNCIL Referred to the Risk Manager

7/10/24 Risk Manager RECOMMEND TO COUNCIL TO DISALLOW

- REPORT OF OFFICER

50. 83984 S. Maughan - Property Damage - \$400.00

		<u>Legislative l</u>	<u> History</u>	
		7/2/24	COMMON COUNCIL	Referred to the Risk Manager
		7/10/24	Risk Manager	RECOMMEND TO COUNCIL TO DISALLOW - REPORT OF OFFICER
51.	<u>83988</u>	K. Kufahl -	· Property Damage - \$1,000.	00
		Legislative l		
		7/2/24	COMMON COUNCIL	Referred to the Risk Manager
		7/10/24	Risk Manager	RECOMMEND TO COUNCIL TO DISALLOW - REPORT OF OFFICER
52.	<u>83994</u>	Ryan Fune	eral Home - Property Damag	e - \$36,038.33
		<u>Legislative</u> l	<u>History</u>	
		7/2/24	COMMON COUNCIL	Referred to the Risk Manager
		7/10/24	Risk Manager	RECOMMEND TO COUNCIL TO DISALLOW - REPORT OF OFFICER
53.	<u>84121</u>	T. Goodwi	n - Property Damage - \$1,10	07.00
		Legislative l		
		7/2/24	COMMON COUNCIL	Referred to the Risk Manager
		7/10/24	Risk Manager	RECOMMEND TO COUNCIL TO DISALLOW - REPORT OF OFFICER
54.	84127	V. Quach	- Vehicle Damage - \$5,801.4	0
		Legislative I	-	
		7/2/24	COMMON COUNCIL	Referred to the Risk Manager
		7/10/24	Risk Manager	RECOMMEND TO COUNCIL TO DISALLOW - REPORT OF OFFICER
55.	<u>84131</u>	E. Bember \$25,000.0		Funeral Home - Property Damage -
		Legislative I	<u>History</u>	
		7/2/24	COMMON COUNCIL	Referred to the Risk Manager
		7/10/24	Risk Manager	RECOMMEND TO COUNCIL TO DISALLOW - REPORT OF OFFICER
56.	84220	S. Nelson	w/ Acuity - Property Damage	e - \$35,000.00
		<u>Legislative l</u>		
		7/10/24	Risk Manager	RECOMMEND TO COUNCIL TO DISALLOW - REPORT OF OFFICER

ITEMS REFERRED TO THIS MEETING

RESOLUTIONS

57. <u>83820</u> Authorizing the Mayor to sign a Memorandum of Understanding with the

President and Fellows of Harvard College acting through the Bloomberg Harvard City Leadership Initiative at the John F. Kennedy School of Government ("Harvard") for the placement of a Bloomberg Harvard City Hall Fellow in the Mayor's Office

Sponsors: Satya V. Rhodes-Conway, John P. Guequierre, Nikki Conklin And MGR

Govindarajan

Legislative History

6/5/24 Mayor's Office Referred for Introduction

Common Council (7/16/24)

7/2/24 COMMON COUNCIL Refer to a future Meeting to Adopt to the

COMMON COUNCIL

Adopt 7/16/24

58. 84168 Authorizing the Planning Division to continue the Artist at Work program,

focusing on destination marketing for Madison's music sector, and to select and

contract non-profit partners as grantees.

Sponsors: Marsha A. Rummel

Legislative History

6/25/24 Department of Planning and Referred for Introduction

Community and Economic

Development

Common Council (7/16/24)

7/2/24 COMMON COUNCIL Refer to a future Meeting to Adopt to the

COMMON COUNCIL

Adopt 7/16/24

INTRODUCTION OF NEW BUSINESS FOR REFERRAL WITHOUT DEBATE

ORDINANCES

59. 84329 Amending Section 28.045 of the Madison General Ordinances to change

minimum dimensional requirements in the TR-C4 District

<u>Sponsors:</u> Tag Evers <u>Attachments:</u> 84329Body.pdf

Legislative History

7/5/24 Attorney's Office Referred for Introduction

Plan Commission- Public Hearing (8/26/24) ,Common Council (9/10/24)

60. 84382 Adopting an amendment to the Comprehensive Plan related to the Northeast

Area Plan.

Sponsors: Derek Field And Sabrina V. Madison

Attachments: NEAP Adopting Cover Letter

Northeast Area Plan Introduction Draft

Resolution 84379-Adopting Northeast Area Plan

Legislative History

7/10/24 Attorney's Office Referred for Introduction

Plan Commission (8/26/24), Economic Development Committee (7/17/24), Transportation Commission (7/31/24), Community Development Block Grant Committee (8/1/24), Landmarks Commission (8/5/24), Board of Parks Commissioners (8/14/24), Urban Design

Commission (8/14/24), Common Council (9/10/24)

61. 84383 Adopting an amendment to the Comprehensive Plan related to the West Area

Plan.

John P. Guequierre Sponsors:

West Area Plan Introduction Draft Attachments:

> West Plan Phase 1 Feedback Summary West Plan Phase 2 Feedback Summary West Plan Phase 3 Feedback Summary West Plan Phase 4 Feedback Summary Resolution 84377-Adopting West Area Plan

West Area Adopting Memo.pdf

Legislative History

7/10/24 Referred for Introduction Attorney's Office

> Plan Commission- Public Hearing (8/26/24), Economic Development Committee (7/17/24), Transportation Commission (7/31/24), Landmarks Commission (8/5/24), Board of Parks Commissioners (8/14/24), Urban Design Commission (8/14/24), Common Council (9/10/24)

RESOLUTIONS

62. Approving the Negotiated Agreement Relating to the Dane County Landfill No. 3

and the First Amendment to the Solid Waste Agreement with Dane County.

(District 16)

Satya V. Rhodes-Conway Sponsors:

Negotiated Agreement Relating to Dane County Landfill No. 3.pdf Attachments:

First Amendment to Solid Waste Agreemernt.pdf

Legislative History

7/5/24 Attorney's Office Referred for Introduction

Finance Committee (8/26/24), Board of Public Works (7/24/24), Common Council (9/10/24)

63. 84330 Approving a Certified Survey Map of property owned by 126 Langdon St, LLC

located at 126 Langdon Street (District 2).

Planning Division Sponsors: **CSM Application.pdf** Attachments:

Project Letter of Intent.pdf

Proposed CSM.pdf

Legislative History

7/8/24 Planning Division Referred for Introduction

Plan Commission (7/29/24), Common Council (8/6/24)

64. 84337 Delete the classification of Transit Safety Coordinator in CG44, R05. Create the

classifications of Transit Safety Coordinator 1 in CG44, R05, and Transit Safety

Coordinator 2 in CG44, R07. Recreate position #4850 of Transit Safety Coordinator as Transit Safety Coordinator 2 and reallocate the employee, J. Maki, to the new classification.

Sponsors: Director of Human Resources

Attachments: PB memo Transit Safety Coordinator 6.27.24.pdf

<u>Transit Safety Coordinator 1 Class Spec 2024 DRAFT.pdf</u> <u>Transit Safety Coordinator 2 Class Spec 2024 DRAFT.pdf</u>

Legislative History

7/8/24 Human Resources Referred for Introduction

Department

Finance Committee (7/22/24), Personnel Board (7/17/24), Common Council (8/6/24)

Reallocate the employee of position #3938 from a Civil Technician 2 (CG16,

R14) to the position of Engineering Program Specialist 1 (CG16, R17).

<u>Sponsors:</u> Director of Human Resources

<u>Attachments:</u> <u>PB Memo TE Civil Tech 2.pdf</u>

PD TS.pdf

Legislative History

7/8/24 Human Resources Referred for Introduction

Department

Finance Committee (7/22/24), Personnel Board (7/17/24), Common Council (8/6/24)

66. 84340 Awarding Public Works Contract 9502, Warner Park Community Recreation

Center Expansion (District 12) and amending the 2024 Adopted Capital

Budgets for Parks Division and Engineering-Facilities Divisions to complete the

project (District 12)

Sponsors: Amani Latimer Burris And Charles Myadze

Attachments: 9502 BidOpeningTab.pdf

9502 award.pdf

Legislative History

7/8/24 Engineering Division Referred for Introduction

Finance Committee (7/22/24), Board of Public Works (7/24/24), Common Council (8/6/24)

67. 84351 Submitting the appointment of Eric M. Knepp for confirmation of a five-year term

as Parks Superintendent.

<u>Sponsors:</u> Satya V. Rhodes-Conway <u>Attachments:</u> <u>Knepp Contract - Final.pdf</u>

Legislative History

7/9/24 Human Resources Referred for Introduction

Department

Finance Committee (7/22/24), Common Council (8/6/24)

68. 84358 Authorizing the assignment of an existing Purchase and Sale Agreement and

previously allotted Affordable Housing Fund loan to KBJ Develop LLC, pertaining to a permanently affordable owner-occupied housing development project in the

Owl Creek neighborhood. (District 16)

Sponsors: Jael Currie

COMMON COUNCIL Agenda - Approved July 16, 2024

Legislative History

7/9/24 Economic Development Referred for Introduction

Division

Finance Committee (7/22/24), Common Council (8/6/24)

69. 84359 Naming Justin Festge-Russell as Madison's 2024-25 Youth Poet Laureate.

Sponsors: Marsha A. Rummel

Legislative History

7/9/24 Department of Planning and

Referred for Introduction

Community and Economic

Development Common Council (8/6/24)

70. 84360 BY TITLE ONLY: Authorizing the Issuance and Establishing the Parameters for

the Sale of Not to Exceed \$[____] General Obligation Promissory Notes, of the City of Madison, Wisconsin, and Directing the Final Approval of the

Terms Thereof.

Sponsors: Satya V. Rhodes-Conway

Legislative History

7/9/24 Attorney's Office Referred for Introduction

Finance Committee (7/22/24), Common Council (8/6/24)

71. 84373 Authorizing City of Madison, on behalf of the Greater Madison MPO, to submit a

grant application to the United States Department of Transportation for up to \$900,000 for the Greater Madison MPO Regional Safe Streets- Community Safety Enhancement Partnership project, amending the 2025 adopted operating budget, to accept grant funding, if awarded, and authorizing the Mayor and Clerk

to sign the resulting grant agreement.

Sponsors: Derek Field And Barbara Harrington-McKinney

Legislative History

7/9/24 Department of Planning and Referred for Introduction

Community and Economic

Development

Finance Committee (7/22/24), Common Council (8/6/24)

72. 84374 Authorizing a noncompetitive contract amendment with Electric Citizen LLC to

rebuild the Madison-area Out-of-School Time (MOST) website on the new City

website platform.

Sponsors: Satya V. Rhodes-Conway And Yannette Figueroa Cole

Attachments: Electric Citizen Non-Compete 07.2024.pdf

Legislative History

7/9/24 Department of Information Referred for Introduction

Technology

Finance Committee (7/22/24), Common Council (8/6/24)

73. 84375 Authorizing the Mayor and City Clerk to enter into a 3-year, competitively

selected Microsoft Enterprise Enrollment Agreement with Microsoft Corporation,

and a 3-year, competitively selected services agreement with CDW Government, LLC, a reseller of Microsoft and other software.

<u>Sponsors:</u> Yannette Figueroa Cole And Satya V. Rhodes-Conway

Legislative History

7/9/24 Department of Information Referred for Introduction

Technology

Finance Committee (7/22/24), Common Council (8/6/24)

74. 84377 Adopting the West Area Plan, adopting an amendment to the Comprehensive

Plan and archiving underlying plans.

Sponsors: John P. Guequierre

Attachments: West Area Plan Introduction Draft

West Plan Phase 1 Feedback Summary
West Plan Phase 2 Feedback Summary
West Plan Phase 3 Feedback Summary
West Plan Phase 4 Feedback Summary
Ordinance 84383-Adopt the West Area Plan

West Area Adopting Memo.pdf

Legislative History

7/9/24 Department of Planning and Referred for Introduction

Community and Economic

Development

Plan Commission (8/26/24), Economic Development Committee (7/17/24), Transportation Commission (7/31/24), Landmarks Commission (8/5/24), Board of Parks Commissioners

(8/14/24), Urban Design Commission (8/14/24), Common Council (9/10/24)

75. 84378 BY TITLE ONLY: Mid Year Appropriation Resolution

Sponsors: Satya V. Rhodes-Conway

Legislative History

7/10/24 Finance Department Referred for Introduction

Finance Committee (7/22/24), Common Council (8/6/24)

76. 84379 Adopting the Northeast Area Plan, adopting an amendment to the

Comprehensive Plan and archiving underlying plans.

Sponsors: Derek Field And Sabrina V. Madison

Attachments: NEAP Adopting Cover Letter

Northeast Area Plan Introduction Draft

Ordinance 84382-Adopt Northeast Area Plan

Legislative History

7/10/24 Department of Planning and Referred for Introduction

Community and Economic

Development

Plan Commission (8/26/24), Economic Development Committee (7/17/24), Transportation Commission (7/31/24), Community Development Block Grant Committee (8/1/24), Landmarks Commission (8/5/24), Board of Parks Commissioners (8/14/24), Urban Design

Commission (8/14/24), Common Council (9/10/24)

PRESENTATION OF CLAIMS AGAINST THE CITY OF MADISON

CLAIMS - REFER TO RISK MANAGER

77.	<u>84216</u>	J. Martin - Vehicle Damage - \$6,000.00
78.	84239	S. Abbas - Vehicle Damage - \$3,000.00
79.	84244	A. Moriarty - Vehicle Damage - \$675.00
80.	84273	J. Owens - Vehicle Damage - \$500.00
81.	84276	S. Riddle - Property Damage - \$1,740.00
82.	<u>84282</u>	S. Caya and E. Tenebruso of Nowlan Law LLP for N. Sarenac - Personal Injury - \$50,000.00
83.	84323	L. Albert - Vehicle Damage - \$13,387.38
84.	84345	M. Gerding of Majestic Building LLC - Property Damage - \$53,898.00
85.	<u>84354</u>	C. Eakins - Property Damage - \$539.83
86.	<u>84357</u>	T. Germanotta of LawnCare Plus, Inc Vehicle Damage - \$12,271.13
87.	<u>84365</u>	J. Ward - Property Damage - \$650.00

ANNOUNCEMENTS & INTRODUCTION OF ITEMS FROM THE FLOOR

ADJOURNMENT



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84380

File ID: 84380 File Type: Resolution Status: Honoring

Resolution

Version: 1 Controlling Body: Council Office Reference:

File Created Date: 07/10/2024

Effective Date:

Published Date:

File Name: Expressing appreciation for the work and Final Action:

contributions of Deb Gilpin.

Title: Expressing appreciation for the work and contributions of Deb Gilpin.

Notes:

Sponsors: Nasra Wehelie, Satya V. Rhodes-Conway, Juliana R.

Bennett, Nikki Conklin, Jael Currie, John W. Duncan, Tag Evers, Derek Field, Yannette Figueroa Cole, MGR Govindarajan, John P. Guequierre, Barbara Harrington-McKinney, Isadore Knox Jr., Amani Latimer Burris, Sabrina V. Madison, Dina Nina Martinez-Rutherford, Charles Myadze, Marsha A. Rummel, William Tishler, Michael E. Verveer And

Regina M. Vidaver

Attachments: **Enactment Number:**

Author: Ald. Nasra Wehelie **Hearing Date:** Entered by: imatthias@cityofmadison.com

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Council Office	07/10/2024	RECOMMEND TO COUNCIL TO ADOPT				
	Action Text:	This Resolution was RE	COMMEND TO COU	NCIL TO ADOPT			

Text of Legislative File 84380

Fiscal Note

No fiscal impact.

Title

Expressing appreciation for the work and contributions of Deb Gilpin.

WHEREAS, Deb Gilpin, the President and CEO of the Madison Children's Museum (MCM), announced that she would retire in the summer of 2024 following 11 years at the helm; and,

WHEREAS, Deb Gilpin was born and raised in the Madison area; and,

WHEREAS, Gilpin helped open two museums in other states before she returned to Madison in 2013 to take the position of President and CEO of the Madison Children's Museum; and,

WHEREAS, under Gilpin's leadership, the MCM has served 2 million visitors, built several new exhibits, improved its financial position, increased employee pay and retention, grew community partnerships, and continued the museum's history of innovation and leadership in the children's museum field; and,

WHEREAS, in particular, Gilpin worked to increase sustainability and break down barriers to access under her leadership at the MCM; and,

WHEREAS, Deb Gilpin oversaw the launch of several programs at the MCM, including, but not limited to, the Teen Workforce program to improve the job opportunity gap between teens of color and their peers, the First-Time Parent Membership, and the first pay-what-you-can restaurant in a museum, Little John's Lunchbox; and,

WHEREAS, the Access for Everyone program, which reduces barriers to museum admission, increased in usage from 17% to 35% under her leadership, in part due to the decision to remove documentation requirements from Access for Everyone membership and admission programs; and,

WHEREAS, in addition to her work at the Madison Children's Museum, Gilpin served on the Aldo Leopold Nature Center Board of Directors for three terms upon joining in May of 2014; and,

WHEREAS, Gilpin chaired numerous committees, including the Strategic Planning Task Force, the Compensation Sub Committee, the Program Curriculum Committee, and the Executive Search Committee; and.

NOW, THEREFORE, BE IT RESOLVED that the Mayor and Common Council express their appreciation for the work and contributions of Deb Gilpin.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84172

File ID: 84172 File Type: Presentation Status: In Committee

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 06/25/2024

File Name: Final Action:

Title: Presentation of the 2024 Reverend James C. Wright Human Rights Award by the City of Madison Equal Opportunities Commission (EOC) and the Department of Civil Rights - Equal Opportunities Division (EOD) to Reverend Dr. Marcus Allen,

Sr.

Notes:

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: mgibson@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	EQUAL OPPORTUNITIES COMMISSION	06/25/2024	Refer	COMMON COUNCIL			
		sentation was R	efer to the COMMON Co	OUNCIL			

Text of Legislative File 84172

Title

Presentation of the 2024 Reverend James C. Wright Human Rights Award by the City of Madison Equal Opportunities Commission (EOC) and the Department of Civil Rights - Equal Opportunities Division (EOD) to Reverend Dr. Marcus Allen, Sr.

Body

The Reverend James C. Wright Human Rights Award honors the late Reverend James C. Wright, who served as Executive Director of the Equal Opportunities Commission from its inception in 1968 until his retirement in 1992. The award is given to an individual who best exemplifies Reverend Wright's dedication and compassion for civil and human rights and conducts their daily life consistent with these values. The Equal Opportunities Commission has selected Reverend Dr. Marcus Allen, Sr. as the recipient of the 2024 award.

Reverend Allen's tireless efforts in various initiatives have had a profound impact on the lives of many individuals, especially the youth. One of Reverend Allen's notable achievements is the creation of the T.R.Y. Academy (Teaching and Reaching our Youth) summer program, tailored for youth ages 12-17 who have been involved in the Juvenile Justice System. This program, born out of necessity, provides essential classes on leadership, finance, apprenticeship certification, meditation, self-regulation, and more. What sets this program apart is not only its educational aspect but also the community engagement opportunities it offers, such as visits to local businesses, libraries, and participation in volunteer activities.

Furthermore, Reverend Allen's leadership extends beyond youth programs. He has played a pivotal role in organizing marches for justice, fostering open dialogues within the faith community, and establishing partnerships with organizations like Dane County to mentor teens aging out of foster care. Additionally, under his guidance, Mt. Zion Baptist Church now hosts a successful Behavioral Health Clinic, providing crucial mental health services to the community.

Reverend Allen's community involvement is vast and impactful, ranging from collaborations with various initiatives like the Collaboration Project to participating in town halls addressing community policing. His efforts have not gone unnoticed, as evidenced by his leadership roles in organizations like the African American Council of Churches and the Black Leadership Council.

In conclusion, Reverend Allen's unwavering dedication, leadership, and impactful contributions to the community make him a deserving candidate for the Reverend James C. Wright Award. With this award and for keeping in the tradition of what Reverend Wright stood for in our community, the Equal Opportunities Commission and the Department of Civil Rights - Equal Opportunities Division would like honor Reverend Dr. Marcus Allen, Sr. and say thank you for service to our community.

Past recipients include: Jacqueline Wright, 1996; Dr. Richard H. Harris, 1997; Chief Justice Shirley Abrahamson, 1998; Helen Vukelich, 1999; Anthony "Nino" Amato, 2000; John Noreika Sr., 2001; Dr. John Y. Odom, 2002; Peter Muñoz, 2003; Earnestine Moss, 2004; Jonathan "Jon" Gramling, 2005; Agnes Gutierrez Cammer, 2006; Jeffery Erlanger, 2007; Dr. Richard Davis, 2008; Darlene Hancock, 2009; Alfonso Studesville, 2010; Richard V. Brown Sr., 2011; Colleen Butler, 2012; Charlestine "Charlie" Daniel, 2013; Dr. Floyd Rose, 2014; Anthony Timmons, 2015; Annette Miller 2016; Brandi Grayson, 2019; Tamora (Tami) Fleming, 2020; Carmella Harris, 2021; Wayne Strong, 2022; and Emmanuel Scarbrough, 2023.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84386

File ID: 84386 File Type: Presentation Status: Presentation

Version: 1 Reference: Controlling Body: Council Office

File Created Date: 07/10/2024

Final Action:

File Name: Data Team presentation: Results Madison and

national certification for What Works Cities data

award.

Title: Data Team presentation: Results Madison and national certification for What

Works Cities data award.

Notes:

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: lwindsor-engnell@cityofmadison.com Published Date:

History of Legislative File

 Ver- Acting Body:
 Date:
 Action:
 Sent To:
 Due Date:
 Return
 Result:

 sion:
 Date:

Text of Legislative File 84386

Title

Data Team presentation: Results Madison and national certification for What Works Cities data award.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83966

File ID: 83966 File Type: Report Status: Consent Agenda

Version: 1 Reference: Controlling Body: Council Office

File Created Date: 06/17/2024

File Name: Consent Agenda Document (7/16/24) Final Action:

Title: Consent Agenda Document (7/16/24)

Notes:

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Council Office	06/17/2024	RECOMMEND TO COUNCIL TO ACCEPT - REPORT OF OFFICER				
	Action Text:	This Report was RECOM	MMEND TO COUNCIL	TO ACCEPT - REP	ORT OF OFFICER		

Text of Legislative File 83966

Title

Consent Agenda Document (7/16/24)



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84385

	File ID:	84385	File Type:	Presentation	Status:	Presentation		
	Version:	1	Reference:		Controlling Body:	FINANCE COMMITTEE		
					File Created Date :	07/10/2024		
	File Name:				Final Action:			
	Title: Presentation on 2025 and Long-Range Budget Plan							
•	Notes:							
	Sponsors:				Effective Date:			
Attachments:					Enactment Number:			
	Author:				Hearing Date:			
	Entered by:	mmcclain@cityofmadis	son.com		Published Date:			
listo	ory of Legisl	ative File						
	,							

Sent To:

Due Date:

Return

Date:

Result:

Text of Legislative File 84385

Title

Acting Body:

Presentation on 2025 and Long-Range Budget Plan

Date:

Action:



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84123

File ID: 84123 File Type: Appeal Status: Public Hearing

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 06/21/2024

File Name: Final Action:

Title: Appeal of the Plan Commission action on the conditional use request for

6610-6706 Old Sauk Road, Legistar ID 82972. (District 19)

Notes:

Sponsors: Effective Date:

Attachments: Link to Cond Use ID 82972, Appeal of Plan Enactment Number:

Commission Approval of Conditional Use w Enclosures.pdf, 6610-6706 Old Sauk Rd Appeal Staff Memo.pdf, Public Comments.pdf, Public

Comment 06-30-24.pdf,

062524-070224_CC_public_comments.pdf, Green Appeal Comments_07-02-24.pdf, Public Comment

07-05-24.pdf

Author: Hearing Date:

Entered by: echristianson@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Clerk's Office	06/21/2024	Referred for Introduction				
	Action Text:	This Appeal was Referred for Introduction					
	Notes:	Common Council Public He	aring (7/16/24)				
1	COMMON COUN	NCIL 07/02/2024	Refer For Public	COMMON			Pass
			Hearing	COUNCIL			
	Action Text:	ction Text: A motion was made by Figueroa Cole, seconded by Duncan, to Refer For Public Hearing to the COMMON COUNCIL meeting of 7/16/24. The motion passed by voice vote/other.					

Text of Legislative File 84123

Title

Appeal of the Plan Commission action on the conditional use request for 6610-6706 Old Sauk Road, Legistar ID 82972. (District 19)

AXLEYATTORNEYS

Christopher T. Nelson cnelson@axley.com 608.283.6707

VIA HAND DELIVERY AND E-MAIL

June 18, 2024

Matt Wachter Secretary of the Plan Commission City of Madison, Planning Division 215 Martin Luther King Jr. Blvd. Madison, WI

RE: Appeal of Plan Commission's Approval of Conditional Use at 6610-6706 Old Sauk Road Legislative File No. 82972

Dear Mr. Wachter:

Please find attached to this letter an appeal of the Plan Commission's decision at its meeting on June 10, 2024, to approve a conditional use at 6610-6706 Old Sauk Road, File No. 82972, pursuant to City of Madison Code of Ordinances, Chapter 28, Subchapter 28L, Section 28.183(5)(b).

Enclosed with this letter is the appeal, signatures for more than 20% of the property owners receiving notice of the proposed conditional use, and the list of persons receiving notice of the proposed conditional use, which was provided to me by the City of Madison Planning Division.

In total, 40 properties received notice of Stone House Development's proposed conditional use (although, three appear to be duplicative). The enclosed appeal includes signatures of property owners for 15 of the noticed properties.

Axley Brynelson, LLP represents four of the signatories to this appeal: Paul and Mary Umbeck, and Jeff and Kathy Western. If the City directs any communications to the appellants, please direct communications to the Umbecks and Westerns to me.

Please do not hesitate to contact me to further discuss this matter.

AXLEYATTORNEYS

Appeal of Plan Commission's Approval of Conditional Use June 18, 2024

Page 2

Sincerely,

AXLEY BRYNELSON, LLP

Christopher T. Nelson

CTN: eel

cc. Timothy M. Parks, City of Madison Planning Division, *Via Email*Bill Fruhling, City of Madison Planning Division, *Via Email*John P. Guequierre, City of Madison, Alder for Ward 19, *Via Email*Jeff and Kathy Western, *Via Email*Paul and Mary Umbeck, *Via Email*

Enclosures

APPEAL OF LEGISLATIVE FILE NO. 82972

Action Appealed: Approval of a conditional use in the [Proposed] Traditional Residential-Urban 2 (TR-U2) District for a multi-family dwelling with greater than 60 units and Approval of a conditional use in the TR-U2 District for outdoor recreation, all to allow construction of a three store, 138-unit apartment building with an accessory outdoor pool.

Background: On June 10, 2024, the Plan Commission approved Legislative File No. 82972, which permits applicant Stone House Development to construct an apartment complex at 6610-6707 Old Sauk Road, at a density greater than permitted under the zoning code. Section 28.051 of the City Code of Ordinances permits multi-family apartments at a density of up to 36 units on a TR-U2 zoned property. With its conditional use application, Stone House Development requested to exceed the permitted use unit-limit in the TR-U2 District and construct a multi-family apartment complex totaling 138 units.

The following findings were made by the Plan Commission according to the draft minutes for the meeting held on June 10, 2024:

On a motion by Commissioner Solheim, seconded by Alder Duncan, the Plan Commission found that the standards were met and approved the conditional use requests subject to the comments and conditions contained in the Plan Commission materials. The motion to approve passed by voice vote/other.

In finding the standards for conditional use approval met, members of the Plan Commission specifically cited:

- The stormwater information provided, the traffic impact analysis, and the proposed conditions of approval to add a crossing of Old Sauk Road in finding that standard 1 was met;
- That there are adequate services in finding that standard 2 was met;
- That the significant setbacks and screen proposed and the findings in the traffic impact analysis in finding that standard 3 was met;
- That there was nothing that would suggest that standard 4 was not met;
- That the findings in the traffic impact analysis and proposed conditions of approval allow standard 5 to be met; and
- That the conditions of approval for the land use and Certified Survey Map should help to address the many concerns raised in public comments and during testimony at the meeting.

A motion was made by Solheim, seconded by Duncan to Approve. The motion passed by voice vote/other.

Grounds for Appeal: Madison City Ordinances Section 28.183(6)(a) articulates approval standards for conditional uses. Under the ordinance, "[n]o application for a conditional use shall be granted by the Plan Commission unless it finds that all of the following conditions are present:

- 1. The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, or general welfare.
- 2. The City is able to provide municipal services to the property where the conditional use is proposed, given due consideration of the cost of providing those services.
- 3. The uses, values and enjoyment of other property in the neighborhood for purposes already established will not be substantially impaired or diminished in any foreseeable manner.
- 4. The establishment of the conditional use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.
- 5.Adequate utilities, access roads, drainage, internal circulation improvements for pedestrians, bicyclists, public transit and vehicles, parking supply (in cases with minimum parking requirements) and other necessary site improvements have been or are being provided.

The Plan Commission erred in finding that Stone House Development's proposed 138-unit apartment complex satisfied factors 1-5. The record before the Plan Commission provides substantial evidence that Stone House Development's proposal fails to satisfy conditional use factors 1, 2, 3, and 5.

Stone House Development's project will have a detrimental effect and endanger public health safety and welfare of the surrounding properties because the stormwater management plan proposed by Stone House Development will exacerbate already existing surface water issues in the neighborhood.

At its meeting on June 10, 2024, the Plan Commission heard from Chuck Nahn, P.E., and Professor John Norman of the University of Wisconsin-Madison Soil Sciences Department. The Plan Commission also received written comments from Mr. Nahn and Professor Norman, which are in the record. Mr. Nahn and Professor Norman articulated in detail why the underground stormwater tanks proposed by Stone House Development in order to meet the requirements of Madison Code of Ordinances Ch. 37, are likely to fail.

The Plan Commission further heard from neighboring property owners about the extent of stormwater issues in the neighborhood, including from neighbors already running two sump pumps to prevent flooding.

The evidence before the Plan Commission establishes that Stone House Development's proposal is likely to have a detrimental impact on neighboring established uses. Accordingly, Section 28.183(6)(a) factors 1 and 3 are not satisfied.

Additionally, factors 2 and 5 are not satisfied because the City presently lacks sufficient infrastructure to handle the increased stormwater issues caused by the Stone House Development. Rather than routing stormwater into City storm sewers, Stone House Development proposes novel and theoretical solutions to handle stormwater on its property. These include the underground storage tanks, which a professional engineer and professor

For their Appeal, Appellants rely on the record before the Plan Commission at its meeting on June 10, 2024, and all the comments made during the public hearing on this matter.

Name (PRINT)	Signature	Address

For their Appeal, Appellants rely on the record before the Plan Commission at its meeting on June 10, 2024, and all the comments made during the public hearing on this matter.

Name (PRINT)	Signature	Address	
Kathleen L Western	Kathlan Lilesten	25 St. Andrews Cir.	MADISON
Jeffrey Western	MUM	25 Saint Andrews Cin Matison, WE	
Deborah Forrestal	Doborah Forestal	21 St. Andrews Cir, -M 126 Sank Ridge Trail	adison Modeson
SHAONITS ABOL S	Shap and	1833 Block River Road,	151010
SHAUNT-SABOL	Spefall	7833 Block River Road	Vereva
JUDITH A. MEYERS	17 St. Andrews	MADISON WY 53717	
FRANK J. MEYEIZS	decensed.	17 ST. ANDREWS CIR MADISON WI 53717	7
JUDITH A. MEYERS	Judos a. Mayor	MADISON WI 53717	
Carrie E. Grehn	Courie Dahn	ZZ St Andrews Cir Madeson, W153717	
WILLIAM M. GRAHN	U	22 St. Andrews Cir. Madison, W1 53717	

For their Appeal, Appellants rely on the record before the Plan Commission at its meeting on June 10, 2024, and all the comments made during the public hearing on this matter.

Name (PRINT)	Signature	Address
	36	67090LD SAUK B
1/ 1 .		May 50 N 53705
MOHAEL A, GREEN	Meni	6709 Old Sauk Rd.
Lynn Mary Green	Symm May Treen	Madison, WI 53705
		22 EspyglassCt
Dana Meicher-Werwie	Dona Meicher Louvil	Mudison, WI 53717
portarie Die Fierre	122 01 11	25 E. Spyglass Ct Madison WI 53717
Mary A. Umbeck	Mary a Lunbech	Madison WI 33117
	C 0	25 E. Spygass Ct
Paul F. Umbeck	Harden -	madisen, W153717
1 cm 11. Min Bar	2 00	1785 AUGIOSSUT
HOTA KOMINSON	bothadows	MACISON WI53/11
OCIVI POLICE	000	11E 5P4 9(055C) ~
Sough & Kaluna	Sarah E. Kobuson	Mad son WI53/11
Jana C. Course	12113	17 E Spyglass Ct
Timothy J Burn		- Madison WI 5371/
Time quy s vaca	7 / /	

For their Appeal, Appellants rely on the record before the Plan Commission at its meeting on June 10, 2024, and all the comments made during the public hearing on this matter.

Name (PRINT)	Signature	Address
Dan Jacger Karen Gibson	On Tracy	14 Torrey Pines Ct. Hadison, w/ 53717 14 TORREY PINES CT MADISON WI 53717
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For their Appeal, Appellants rely on the record before the Plan Commission at its meeting on June 10, 2024, and all the comments made during the public hearing on this matter.

Name (PRINT)	Signature	Address
Daniel Stier	Variet The	Medison, WI 53905
DIANE SORENSEN	Disne Toyeusen	Gob San Juan Trail Madison, WI 53705
		Madison, WI 53105 6613 OLD SOVE RD
SUSAN NIESAN	Susa a Mesi	MADISON WI 53705
	Call	6713 old Six Rd
Chris Pajerski	eger	Madison WI 53705
-		
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	,	
		7.4

name1	name2	mailing_address	city	state	zip
MAKKENA, YASHASWINI	& ASHUTOSH ARYAL	6714 OLD SAUK RD	MADISON	WI	53705-2427
PAPPAS, MICHAEL N		6718 OLD SAUK RD	MADISON	WI	53705
BURNS, TIMOTHY	SARAH ELIZABETH ROBINSON	17 E SPYGLASS CT	MADISON	WI	53717
UMBECK JR, PAUL F	& MARY ACKERMAN UMBECK	25 E SPYGLASS CT	MADISON	WI	53717-1143
MEICHER-WERWIE, DONA J		22 E SPYGLASS CT	MADISON	WI	53717
LAND, ANDREA E	JOSHUA A LAND	18 E SPYGLASS CT	MADISON	WI	53717-1143
WOREL LIVING TRUST		717 PEBBLE BEACH DR	MADISON	WI	53717-1139
GREUNKE, DAVID R		6 TORREY PINES CT	MADISON	WI	53717-1153
MARTY, TRENTEN & KATHRYN		10 TORREY PINES COURT	MADISON	WI	53717-1153
JAEGER/GIBSON FAMILY TR		14 TORREY PINES CT	MADISON	WI	53717
BRADLEY, LEONARD & DIANNE		18 TORREY PINES CT	MADISON	WI	53717-1153
MORRISON, LISA A	& BRIAN L BEARD	21 TORREY PINES CT	MADISON	WI	53717
LEEMKUIL REVOCABLE TRUST		17 TORREY PINES CT	MADISON	WI	53717-1153
WINNIG, PAULA JAYNE		18 ST ANDREWS CIR	MADISON	WI	53717-1154
GRAHN, WILLIAM M	& CARRIE E GRAHN	22 ST ANDREWS CIR	MADISON	WI	53717-1154
WESTERN, JEFFREY L	& KATHLEEN L WESTERN	25 ST ANDREWS CIR	MADISON	WI	53717-1154
FORRESTAL SURVIVOR'S TR,	DEBORAH L	21 ST ANDREWS CIR	MADISON	WI	53717
MEYERS, FRANK & JUDITH		17 ST ANDREWS CIR	MADISON	WI	53717-1154
PIERSTORFF, ROBERT		6610 OLD SAUK RD	MADISON	WI	53705
PIERSTORFF, ROBERT W		6610 OLD SAUK RD	MADISON	WI	53705
UNIT OWNERS SAUKBOROUGH	SQ CDM	6417 NORMANDY LN	MADISON	WI	53719
NORMANDY PARTNERS LLC		725 E JOHNSON ST	MADISON	WI	53703
SABOL, SHAUN T		7833 BLACK RIVER RD	VERONA	WI	53593
SABOL, SHAUN T		7833 BLACK RIVER RD	VERONA	WI	53593-8400
NORMANDY PARTNERS LLC		725 E JOHNSON ST	MADISON	WI	53703
SAUKBOROUGH PROPERTY	OWNRS ASSOC % S.PACKWOOD	5 COURT OF BRIXHAM	MADISON	WI	53705
ROELKE IRREV TRUST, C M		6621 OLD SAUK RD	MADISON	WI	53705-2424
SCHUCK, HOWARD & DIANE		6617 OLD SAUK RD	MADISON	WI	53705-2424
NIESEN TRUST		6613 OLD SAUK RD	MADISON	WI	53705-2424
MO, LUOSHENG	& XUDONG ZHU	6605 OLD SAUK RD	MADISON	WI	53705
HARRIS, ZE'EV		6601 OLD SAUK RD	MADISON	WI	53705
JOSVAI, SAM		609 EVERGLADE DR	MADISON	WI	53717-1118
SORENSEN, DIANE M	DANIEL D STIER	606 SAN JUAN TRL	MADISON	WI	53705
JORANLIEN, ERIK	GRACE RIEDLE	610 SAN JUAN TRL	MADISON	WI	53705-2463
GREEN, MICHAEL A & LYNN M		6709 OLD SAUK RD	MADISON	WI	53705-2426
GREEN, MICHAEL ANTHONY	AND LYNN MARY GREEN	6709 OLD SAUK RD	MADISON	WI	53705-2426
PAJERSKI, CHRISTOPHER A		6713 OLD SAUK RD	MADISON	WI	53705-2426
Occupant		10 TORREY PINES CT	MADISON	WI	53717
Occupant		6706 OLD SAUK RD	MADISON	WI	53705
Occupant		738 SAUK RIDGE TRL UNIT B	MADISON	WI	53705
Occupant		738 SAUK RIDGE TRL UNIT C	MADISON	WI	53705
Occupant		738 SAUK RIDGE TRL UNIT D	MADISON	WI	53705
Occupant		736 SAUK RIDGE TRL UNIT A	MADISON	WI	53705

Occupant	736 SAUK RIDGE TRL UNIT B	MADISON	WI	53705
Occupant	736 SAUK RIDGE TRL UNIT C	MADISON	WI	53705
Occupant	736 SAUK RIDGE TRL UNIT D	MADISON	WI	53705
Occupant	738 SAUK RIDGE TRL UNIT A	MADISON	WI	53705
Occupant	732 SAUK RIDGE TRL UNIT B	MADISON	WI	53705
Occupant	732 SAUK RIDGE TRL UNIT C	MADISON	WI	53705
Occupant	732 SAUK RIDGE TRL UNIT D	MADISON	WI	53705
Occupant	734 SAUK RIDGE TRL UNIT A	MADISON	WI	53705
Occupant	734 SAUK RIDGE TRL UNIT B	MADISON	WI	53705
Occupant	734 SAUK RIDGE TRL UNIT C	MADISON	WI	53705
Occupant	734 SAUK RIDGE TRL UNIT D	MADISON	WI	53705
Occupant	732 SAUK RIDGE TRL UNIT A	MADISON	WI	53705
Occupant	728 SAUK RIDGE TRL UNIT A	MADISON	WI	53705
Occupant	728 SAUK RIDGE TRL UNIT B	MADISON	WI	53705
Occupant	728 SAUK RIDGE TRL UNIT C	MADISON	WI	53705
Occupant	728 SAUK RIDGE TRL UNIT D	MADISON	WI	53705
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Occupant	724 SAUK RIDGE TRL UNIT C	MADISON	WI	53705
Occupant	724 SAUK RIDGE TRL UNIT D	MADISON	WI	53705
Ald. John Guequierre	887 N High Point Rd	MADISON	WI	53717



Department of Planning & Community & Economic Development

Planning Division

William Fruhling, Interim Director

Madison Municipal Building, Suite 017 215 Martin Luther King Jr. Boulevard Madison, Wisconsin 53703 Phone: (608) 266-4635 www.cityofmadison.com

TO: Mayor Satya Rhodes-Conway

Madison Common Council

FROM: William Fruhling, AICP, Secretary of the Plan Commission (Interim)

DATE: June 20, 2024

SUBJECT: Appeal of Plan Commission Decision on Conditional Use (Legistar ID #82972)

At its June 10, 2024 meeting, the City of Madison Plan Commission unanimously approved the conditional use requested by Helen H. Bradbury of Stone House Development to allow construction of a three-story, 138-unit apartment building with an accessory outdoor pool at 6610-6706 Old Sauk Road in a TR-U2 (Traditional Residential-Urban 2) district.

Section 28.183(5)(b) of the Zoning Code provides the following regarding the filing of appeals for conditional use approvals (excerpt):

- "1. The Plan Commission's decision [on a conditional use] is appealable to the Common Council.
- 2. The appeal may be filed by:
 - a. The applicant, or
 - b. The Alderperson of the district in which the use is located, or
 - c. Twenty percent (20%) or more of the property owners entitled to notice who object to the establishment of the conditional use.
- 3. The appeal shall specify the grounds with specific reference to the findings of the Plan Commission.
- 4. The appeal shall be filed with the Secretary of the Plan Commission within ten (10) days of the final action of the Plan Commission.
- 5. The Secretary of the Plan Commission or his/her designee shall transmit the appeal to the City Clerk, who shall file the appeal with the Common Council.
- 6. The Common Council shall fix a reasonable time for hearing the appeal, and decide the appeal within a reasonable time."

On June 18, 2024, a petition was submitted to appeal the Plan Commission's approval of the conditional uses to the Common Council. Upon examination by Planning Division staff and in consultation with the City Attorney's Office, the appeal was signed by 20% or more of the property owners entitled to notice, pursuant to the requirements in Section 28.183(5)(b) of the Zoning Code.

Because the threshold for appealing the Plan Commission's decision to the Common Council has been met, staff recommended that the appeal be introduced at the July 2 Common Council meeting for

ID #82972 Conditional Use Appeal 6610-6706 Old Sauk Rd June 20, 2024 Page 2

referral to July 16, 2023. As noted in Section 28.183(5)(b)8, the action of the Plan Commission shall be upheld unless it is reversed or modified by a favorable vote of 2/3 of the members of the Common Council.

Thank you, and please feel free to contact me with any questions.

Sincerely,

William Fruhling

Cc (via e-mail):

Maribeth Witzel-Behl, City Clerk
Michael Haas, City Attorney
Matthew Wachter, Director, Department of Planning, Community, & Economic Development
Kate Smith, Assistant City Attorney
Katie Bannon, Zoning Administrator
Kevin Firchow, AICP, Principal Planner, Planning Division
Tim Parks, Planner, Planning Division
Helen H. Bradbury, Stone House Development

From: Greg Bell

To: <u>Plan Commission Comments</u>

Subject: File#82972

Date: Tuesday, June 25, 2024 2:17:18 PM

[You don't often get email from tgregbell@tds.net. Learn why this is important at https://aka.ms/LearnAboutSenderIdentification]

Caution: This email was sent from an external source. Avoid unknown links and attachments.

Dear Members of the PC Board and the City Council:

This appeal is concerning a serious and critical issue related to the Stone House Development project just off Old Sauk Rd., File #82972, but backing up to a number of properties who will likely be injured after it is completed. Please consider this serious impediment to a successful housing development so you can avoid millions of dollars in suits from injured home owners in the future if you fail to resolve this issue. There is no need to rush your judgement on this project. Do it right the first time!, please, we beg you.

T.Greg-Pam Bell, 11 Court of Brixham. 53705

--

T. Greg Bell 11 Court of Brixham Madison, WI 53705 tgregbell@tds.net 608-836-9547

From: Connie Brown

Plan Commission Comments To: Subject: 6610-6706 Old Sauk Rd

Date: Monday, June 24, 2024 5:41:12 PM

You don't often get email from cmbrown710@gmail.com. Learn why this is important

Caution: This email was sent from an external source. Avoid unknown links and attachments.

We are so thankful for this appeal. We feel as if no one is hearing our concerns.

An Appeal has been submitted to the Secretary of the Plan Commission because the Plan Commission has failed to consider overwhelming evidence that:

- a) Stone House Development's proposed novel stormwater management plan will have a detrimental effect on public health, safety and welfare of the surrounding properties; and
- b) The City presently lacks an adequate stormwater infrastructure to handle the increased stormwater issues caused by the Stone House development.

Connie Brown

Sent from my iPad

From: <u>Jeff Brown</u>

To: <u>Plan Commission Comments</u>

Subject: 6610-6706

Date: Monday, June 24, 2024 5:51:24 PM

You don't often get email from brownpianoworkshop@gmail.com. Learn why this is important

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Jeffrey Brown

From: Steve Mason

To: Plan Commission Comments

Subject: Appeal

Date: Monday, June 24, 2024 7:15:29 PM

[You don't often get email from smason65@gmail.com. Learn why this is important at https://aka.ms/LearnAboutSenderIdentification]

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There are very serious problems with the proposed drainage and water retention issues with the stone house proposal. These cannot be brushed off without being addressed point by point. City staff doesn't see a problem.... Are they blind?

From: <u>Kathleen stark</u>

To: <u>Plan Commission Comments</u>

Subject: Opposition to Stone House Development proposal

Date: Monday, June 24, 2024 5:13:06 PM

You don't often get email from strk79automatic@gmail.com. Learn why this is important

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Once again as citizens of Id Sauk neighborhood we strongly oppose the following agenda items:

2950 ... 6610-6706 Old Sauk Road (District 19): Consideration of a demolition permit to demolish two single-family residences and a two-family residence.

24) 83477 ... Creating Section 28.022-00672 of the Madison General Ordinances to change the zoning of property located at 6610-6706 Old Sauk Road from SR-C1 (Suburban Residential-Consistent 1) District and SR-C3 (Suburban Residential-Consistent 3) District to TR-U2 (Traditional Residential-Consistent 2) District. (District 19)

25) 82972 ... 6610-6706 Old Sauk Road (District 19): Consideration of a conditional use in the [Proposed] Traditional Residential-Urban 2 (TR-U2) District for a multi-family dwelling with greater than 60 units and consideration of a conditional use in the TR-U2 District for outdoor recreation, all to allow construction of a three-story, 138-unit apartment building with an accessory outdoor pool.

26) 82979 ... Approving a Certified Survey Map of property owned by Stone House Development, Inc. located at 6610-6706 Old Sauk Road (District 19)

Thomas and Kathleen Stark 809 Sauk Ridge Trail Madison, WI 53717 From: Fun to Build

To: Mayor, Figueroa Cole, Yannette, All Alders, Plan Commission Comments

Subject: Please Post as Public Comments for 82950, 82972, 83477, 82979 and 84123, 6610-6706 Old Sauk Rd

Date: Sunday, June 30, 2024 6:00:23 PM

Some people who received this message don't often get email from foster07cn@gmail.com. Learn why this is

important

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Dear Mayor Rhodes-Conway, President Cole and All Alders,

We ask that you reverse your decision as appealed and defer approval of the Stone House Development proposal at 6610-6706 Old Sauk Rd until: a) a Stormwater Plan is approved by City Engineering and b) the City can provide an adequate stormwater infrastructure to accommodate increased storm runoff caused by the Stone House Development project.

Additionally, we find it Unbelievable that the Plan Commission and a developer teamed up in advance and conspired together to radically change the zoning of our neighborhood and prior to any public comment period or neighborhood consideration.

We find it Unbelievable, despite 100% opposition from adjacent property owners and overwhelming public opposition, a decision was made at the 6/10/24 Plan Commission meeting to approve the Stone House Development proposal with zero, let us repeat, zero discussion.

We find it Unbelievable that our recently selected alder used the Plan Commission meeting time to pitch softball questions to both Stone House Development and its engineer, Wyser Engineering when they had previously exceeded their public comment time limit. He did not afford the same luxury to residents who were also cut off when speaking.

We find it Unbelievable at this flood risk location as declared by the City on their Flood Risk Map why Stone House Development choose to start with a large building design and left a small amount of room for a stormwater system, it should be the other way around, first figure out the room needed for a solid performing stormwater design and then design the building. Because Wyser Engineering's stormwater design (revised 5/24/24) does not include spare reserve capacity where flow can be diverted to in an emergency or to perform cleaning maintenance, does not include a water level or water flow monitoring system, is relying on infiltration into soils with subpar percolation rates, does not include confined space entry into the underground infiltration basins for inspection and cleaning and to our knowledge where there are no local confined space vessel cleaning services available, their design will be not be successful and achieve 100% performance, 100% of the time.

We find it Unbelievable that there is no known discussion about modifications to the City's outdated and woefully undersized stormwater sewer system that could accommodate the increased storm runoff caused by the Stone House Development project.

And lastly, we find it Unbelievable that neither the Staff, the Plan Commission or the Common Council is raising any concerns or expressing significant challenges to these issues,

only approving without question.

Sincerely, Gary and Barb Foster 6506 Old Sauk Rd

From: Greg Bell
To: All Alders

Subject: Serious appeal to Board & Council **Date:** Tuesday, June 25, 2024 2:08:55 PM

[Some people who received this message don't often get email from tgregbell@tds.net. Learn why this is important at https://aka.ms/LearnAboutSenderIdentification]

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T. Greg Bell 11 Court of Brixham Madison, WI 53705 tgregbell@tds.net 608-836-9547

From: Jeff Brown
To: All Alders

Subject: 6610-6706 Old Sauk Rd

Date: Monday, June 24, 2024 5:53:51 PM

Some people who received this message don't often get email from brownpianoworkshop@gmail.com. <u>Learn why</u> this is important

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Jeffrey Brown

From: Connie Brown
To: All Alders

Subject: 6610-6706 Old Sauk Rd.

Date: Monday, June 24, 2024 5:45:18 PM

Some people who received this message don't often get email from cmbrown710@gmail.com. Learn why this is important

Caution: This email was sent from an external source. Avoid unknown links and attachments.

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Connie Brown Sent from my iPad

From: Fun to Build

To: Mayor; Figueroa Cole, Yannette; All Alders; Plan Commission Comments

Subject: Please Post as Public Comments for 82950, 82972, 83477, 82979 and 84123, 6610-6706 Old Sauk Rd

Date: Sunday, June 30, 2024 6:00:23 PM

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important

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Sincerely, Gary and Barb Foster 6506 Old Sauk Rd

From: <u>the-greens31@charter.net</u>
To: <u>Wachter, Matthew</u>

Cc: Parks, Timothy; Fruhling, William; Guequierre, John; Madison Mayor; All Alders

Subject: Appeal (Legistar 84123) by The Greens **Date:** Tuesday, July 2, 2024 2:01:55 PM

Attachments: <u>image001.png</u>

Appeal (Legistar 84123) Submitted 20240702 by Green.pdf

Some people who received this message don't often get email from the-greens31@charter.net. <u>Learn why this is important</u>

Caution: This email was sent from an external source. Avoid unknown links and attachments.

{Please file this correspondence, and its attachment, under Legistar Numbers: 84123 (Appeal), 82972 (Conditional Use), 83477 (Rezoning), 82979 (CSM), and 82950 (Demolition)}

To: Secretary of the Plan Commission, Matt Wachter

In the attachment is the substance of our support in favor of the Appeal (Legistar 84123) of the Plan Commission actions of 10 June 2024 regarding the Conditional Uses (Legistar 82972) of the proposed development at 6610-6706 Old Sauk Road.

Hopefully, I have this routed correctly. If not, please advise.

Thank you, Mike & Lynn Green 6709 Old Sauk Rd District 19 In Support of the Appeal {Legistar 84123} of the Plan Commission Actions of 10 June 2024 Regarding the Conditional Uses {Legistar 82972} of the Proposed Development at 6610-6706 Old Sauk Rd

Submitted: 2 July 2024 By: Michael A. Green

BACKGROUND

- The (virtual) Plan Commission meeting of 10 June 2024 acted upon these Agenda Items
 - Item 23, Legistar 82950, Demolition Permit
 - Item 24, Legistar 83477, Rezoning Excessive and proactive; not covered in this Appeal
 - Item 25, Legistar 82972, Conditional Use (CU) Covered in this Appeal
 - O Item 26, Legistar 82979, CSM
- The foundation of this meeting was a 24 page Planning Division Staff Report [Ref 1, Appendix 4] released to the public on 7 June giving residents a long weekend to review its contents.

PURPOSE

• In support of this Appeal we oppose the Plan Commission (PC) approvals of Conditional Use as described in the following.

NATURE OF THE APPEAL

<u>PC Review Submission</u> – Immediately following the PC meeting, on 13 June, a Review of that meeting was submitted that was sharply negative both as to its Process and a general disregard of its own judgement criteria and their interpretation. Unfortunately, without reference to a Legistar number, those comments cannot be found on the Internet or referenced to that meeting. This appears as Reference 2, Appendix 1.

This reference concludes that close partnership existed between the City and the developer over a long period of time; and the outcome was basically predetermined. The public was allowed to speak on a metered basis, unless there were followup questions ... of which there were none. By contrast, questions were asked, in favor of the developer, which deferentially extended their speaking time. This pattern is directly mirrored, and evidenced by unanimous (the default!!!) passage of all Agenda Items.

It should be noted that for District 19, across all agenda items, there were 420 opposing votes and 30 supporting votes.

• Specifics of the Appeal – The Staff Report, and its careless, no-resistance acceptance by the PC, fed forward to comments [Ref 3, Appendix 2] presented to the Common Council (CC) meeting of 18 June [Ref 4]. Legistar 82972 (Conditional Use), critical to development approval, and the most fraught with problems, was not on the Agenda, and it therefore contributed considerably to that presentation.

The Staff Report had three Judgement Criteria:

- 1. "Additionally, state law requires that conditional use findings must be based on 'substantial evidence' that directly pertains to each standard and not based on personal preference or speculation.
- 2. CU Standard #8 "... the Plan Commission shall find that the project creates an environment of sustained aesthetic desirability compatible with the existing or intended character of the area ..."
- 3. "The City Plan Commission shall not approve a conditional use without due consideration of the recommendations in the City of Madison Comprehensive Plan ..." which reads "... newly developing LMR areas should be seamlessly integrated with surrounding development".

These are the Conditional Uses, from the Staff Report, with comments:

- 1. The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, or general welfare.
 - Specific to this appeal is that there is definite, quantifiable, demonstrably tangible risk of flooding to neighbors immediately to the north of the proposed development. This has been the subject of considerable documentation (cf. Ref 5) and needs no repetition here. While acknowledging these concerns the Report then somewhat glibly says "... staff does not believe that the information provided in these comments suggest that this standard cannot be met." This is not the same as saying, categorically, that they can be met. Thus, and using the Staff's word "believe" it can, at best, be said CU #1 is incomplete and unresolved.
- 2. The City is able to provide municipal services to the property where the conditional use is proposed, given due consideration of the cost of providing those services.
 - The Report says "... nothing out of the ordinary in providing municipal services to this property because of the proposed development". Given the exposure that the City has incurred from CU #1 [Ref 6], it is not at all obvious if, in the present case (stormwater), this statement is true or not; it defaults, too, to incomplete and unresolved.
- 3. The uses, values and enjoyment of other property in the neighborhood for purposes already established will not be substantially impaired or diminished in any foreseeable manner.
 - As the Report says: "This is often the most difficult standard ... to address". It involves input (which has been near-unanimously negative) from neighborhood residents or property owners; it also acknowledges that "The proposed building has elicited a significant amount of correspondence from nearby residents ..." There is no known, first-hand knowledge of (monetary) value ever coming up in local discussions. However, "enjoyment" is subjective (for either residents or the City). To residents: noise, lighting, added traffic, on-street parking, and the profound loss of green space will all impair and diminish enjoyment ... no question. So, did the PC approve this based on "substantially", or not? How does one satisfy the "substantial evidence" Criterion given the criterion for "substantial evidence"? The "substantial evidence" favors residents in this case.

4. The establishment of the conditional use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.

The Report does say: "... the proposed building represents a significantly different building form compared to what currently exists on the subject site and on surrounding properties ..." Although this thought carries forward, there is no other particular comment.

5. Adequate utilities, access roads, drainage, internal circulation improvements for pedestrians, bicyclists, public transit and vehicles, parking supply (in cases with minimum parking requirements) and other necessary site improvements have been or are being provided.

Regarding drainage (see CU #1 comments, above), this issue is still incomplete and unresolved. As to parking, it is very hard to believe that this development will not result in on street parking. This could easily be by residents not wanting to pay monthly underground parking fees or wanting to beat rush hour egress or ingress (on a single, internal driveway).

6. The conditional use conforms to all applicable regulations of the district in which it is located.

No comment.

- 7. Does not apply.
- 8. When applying the above standards to any new construction of a building or an addition to an existing building the Plan Commission shall find that the project creates an environment of sustained aesthetic desirability compatible with the existing or intended character of the area and the statement of purpose for the zoning district. In order to find that this standard is met, the Plan Commission may require the applicant to submit plans to the Urban Design Commission for comment and recommendation.

This is far harder than CU #3 to be justified in compliance by the PC; notably, they made no mention of this in their Minutes. Here, Judgement Criteria #2 & #3 pertain, which use phrasings "creates an environment of sustained aesthetic desirability compatible with the existing or intended character of the area ...", and "seamlessly integrated with surrounding development".

First, note the Reports' comment on "significantly different building form" under CU #4, above. Then, referring to the Table at the end of Ref. 2, there is a comparison with what should be the developer's best case comparable, viz. the adjacent Settlers Woods apartments. That Table summarizes the comparison of properly-scaled vertical height (top panel) and horizontal length (bottom panel); also included is curb setback from Old Sauk Road based on data from the developer and Google Earth. This is the comparison:

- Settlers Woods is somewhat shorter in height 2 floors with a standard, pitched roof falling away from the viewer.
- Curb setback
 - Settlers Wood 84 feet
 - New Development 35 feet

- Apparent (angular, above horizon) Height Owing to shorter height and greater setback, Settlers Woods is 2-3 times shorter than the proposed development
- Length
 - Settlers Woods 100 feet
 - New Development 400 feet
- Dwelling Units per Acre
 - Settlers Woods 14.4 (derived from City records)
 - New Development 36.6 (from developer)

Bearing in mind that the surrounding neighborhood is R1, the comparisons (see figures in Ref 7) weigh even more unfavorably against the new development. Again, does the PC approve this based on "aesthetic desirability" and "seamlessly integrated with surrounding development", or not? How does one satisfy the "substantial evidence" Criterion? Put this to a neighborhood poll (as opposed to the top-down model of governance): Would any reasonable, unbiased poll sustain the City's viewpoint or that of the neighboring community? Objectively, numerical comparisons are quite against this development; and, subjectively, a neighborhood poll would come to the same conclusion.

9. - 16. Do not apply.

• Conclusion – Conditional Uses #1, #2, and #5 are incomplete and unresolved with a default of not satisfied. CU #3 is not satisfied. CUs #4 & #6 have no comment. CUs #7 and #9-16 do not apply. Conditional Use #8 is demonstrably non-compliant with any reasonable interpretation of the Judgement Criteria.

These insufficiencies are the basis for our Appeal. It should be emphasized that fundamental problems in the Staff Report were summarily, and unanimously rubber-stamped by the Plan Commission, oblivious to residential input, thus approving Conditional Uses which do not then come before the Common Council per se; that is, everything downstream from the Staff Report is similarly flawed. Adding grievous insult-to-injury the Common Council then proactively, and excessively upzoned the properties, a practice which the City should know is not well received by neighborhoods as it leverages its position into the future.

References:

- 1) Planning Division Staff Report; see Appendix 4
- 2) Personal critique of the 10 June 2024 PC meeting; see Appendix 1
- 3) Personal CC presentation outline form; see Appendix 2
- 4) Agenda Items 13 (Legistar 83477, Rezoning) and 49 (Legistar 82979, CSM)
- 5) Under <u>Legistar 82972</u> See discussions in "<u>Public Comments June 2024.pdf</u>", pgs 36-37, 144-147, and 200-203
- 6) See, for example, greyed, Major/Non-Standard Conditions on pgs 15 & 16 of the Planning Division Staff Report; Appendix 4
- Opposition Stone House Dev 6610-6706 Old Sauk Rd (Mike & Lynn Green).pdf;
 Appendix 3

Appendix 1

the-greens31@charter.net

From: the-greens31@charter.net < the-greens31@charter.net >

Sent: Thursday, June 13, 2024 1:05 PM

 $'npollack@madison.com' < \underline{npollack@madison.com} >; 'pfanlund@captimes.com' < \underline{pfanlund@captimes.com} >; 'mtreinen@captimes.com' < \underline{pfanlund@captimes.co$

 $<\!\!\underline{mtreinen@captimes.com}\!\!>; 'faye.parks@wortfm.org' <\!\!\underline{faye.parks@wortfm.org}\!\!>$

Cc: 'pccomments@cityofmadison.com' < pccomments@cityofmadison.com>

Subject: Comments on 10 June Plan Comm Mtg OR Madison's Future

Good afternoon

Attached is a review of the process at the Plan Commission meeting at 5:30 pm on Monday 10 June that considered the proposed development at 6610-6706 Old Sauk Rd.

This is a synthesis of how our family members perceived that meeting. Nevertheless, it likely approximates what others in our opposition would say as well. It is meant to give feedback that will hopefully improve the process.

I wish the circumstances were otherwise and this review were very different; at the heart of this matter is politics and top-down governance versus bottom-up policy that begins with neighborhood communities.

In the future, I look forward to seeing this reversal, possibly with different leadership. It's one thing to chronicle talking to residents, to show "citizen involvement", but it is entirely different if that box is checked and the input ignored.

There also needs to be a truly long-term discussion about what makes Madison what it is, what it takes to preserve that "charm", and set realistic limitations on what we can and want to achieve without eventually diminishing what we love; this needs to be written into the Area Plans and the Comprehensive Plan. Then, the current, short-term impetus to densify needs be consistent with some notion of boundaries written into those Plans.

There is a larger picture here.

Thank you, Michael A. Green 6709 Old Sauk Rd.

Madison

Re: Virtual Public Hearing, Plan Commission Meeting, 5:30 pm on 10 June 2024 Agenda Items #23, 24, 25, 26 Concerning the Proposed Development at 6610-6706 Old Sauk Rd

A Citizen's Experience

Disclaimer: Our position was in opposition to the proposed development. On all points, we lost.

Purpose, to Describe: First, how the process was conducted, or "how the game was played".

Second, there are arbitrary, imprecise, qualitative judgement "features" in the process that can, depending on how they are selected, or spun, toggle the outcome to the one desired.

Impression: It was a thoroughly miserable experience in local civics and citizen involvement in the discussion of this proposal and of Madison's housing crisis. In a top-down policy environment the neighborhood is marginalized, rather than coming first.

To Begin:

- 1. Accessibility
 - a. There were Zoom login problems because of incorrect instructions to viewers or IT setup. I had to intervene, when another speaker was called, to bring this up as the problem became known from neighbors; this got a nod of recognition, but no redress or apology from the Plan Commission (PC). Had the public's input been valued, this could have been remedied *before* continuation; that was not the case.
 - b. The answer to any of the following complaints is/was/or_will_be that this is not how PC meetings are run; this is not helpful if this is your first, virtual PC meeting.
 - i. Never have I seen a Zoom setup like this ... and I've been to too many virtual meetings:
 - (1) No chat function (useful to ask moderator a question offline)
 - (2) No video function (my presentation relied on being able to hold up an exhibit and I was unable to do so); a possible counter argument was that my graphic could have been sent to the PC ahead of meeting. I didn't do this since I was unfamiliar with their procedures and setup and I wanted to be able to rehearse and time optimize my presentation
 - (3) No participants icon that would/could have indicated login problems
 - (4) Screen so sparse of detail one couldn't even tell if logged in (to speak) or not
 - ii. With no "time expires in xxx seconds" messaging to speakers, speakers were just cutoff even in mid-sentence.

2. Communication

- a. There was an unmistakable, deferential camaraderie between the PC and the developer. The public are definitely on the "outside looking in" of that relationship.
- b. Public input was metered, but not uniformly. Presentation cutoff times varied from 3-3.5 minutes.
- c. By contrast, the developer could be, and was, granted more, unlimited time by virtue of

- simply being asked a leading, soft-ball question by a PC member. There were no instances of the public or its experts being asked any questions or in any way being engaged in dialog. The tone of the meeting was clearly in favor of the developer.
- d. The PC only had discussion about approval, justification, and praise for developer.
- e. The PC's appreciation to public attendees that "your voice was heard" and "we know how difficult this process can be", etc. came across as hollow and disingenuous on the verge of becoming insulting.
- f. Finally, all four Items were summarily passed in oblivious disregard for the public's input.

3. PC Decision Making

- a. The outcome was clearly pre-ordained and never in doubt; the default on every motion was always "unanimous consent assumed unless a hand is raised" (by a PC member) ... there was never any discussion, or raised hands: every motion was systematically unanimous. The cruxes of public feedback summarily vanished.
- b. In this case, at least, the developer worked with the PC for months to reach a mutually desirable outcome; a Staff Report from the Plan Division had gone to the PC a few days before the Meeting where it is given a "public hearing". Since the public's voice is not listened to, absorbed, thought about, questioned, and/or assimilated, "public hearing" essentially means the public gets to hear, but not interfere with, the agreed-to plan.
- 4. The Staff Report shown in blue are examples of critical measures spun for a desired result
 - a. Pg 12 is a mess Under Recommendations the 1st & 3rd bullets are from another development. Was ignoring these obvious, major written gaffes called for, or was there important text that should have been there and made available to the public?
 - b. There are qualitative, subjective, and tentative wordings such as "could find", "believes", "feels" in critical instances in the absence of more quantitative, objective, and certain measures. This was invariably replaced by either language that discounted negative assertions or resulted in recommended "fact".
 - c. The development's frontal view is nowhere close to the Comprehensive Plan's wording "... newly developing LMR areas should be seamlessly integrated with surrounding development". This has previously been included in written, public comments including graphics; this would have been shown except that the PC does not allow video. But no matter: the developer justified (to itself and the PC) the proposed building's height and massing by comparison to another, higher complex over a mile away that happens to be in a more appropriate zoning setting and has BRT. The best comparable is the apartments immediately to the east of the proposed development; had they been used for comparison it is immediately apparent that it is vastly exceeded by the proposed building. See attachment.
 - d. Rebuttal of storm water issues was incomplete at best; possibly incorrect at worst. New concepts became apparent in the Staff Report that suggest a storm water easement, and which the City will acquire if the developer can't. This is a new chapter in the discussion.
 - e. Select conditions These were spun to justify additional upzoning (in this case du/ac) but also setting future precedent for much greater scale and density (a process the City terms proactive rezoning) elsewhere. Staff and PC stated that arterial status and bus availability were "... most significant factors as to why the proposed development may be approved." despite the complex not meeting at least 3 other, more significant factors. This is arbitrary cherry-picking favorable to a desired outcome.

Here is a table of those factors, with various points of view; red is negative, green is positive, and brown is in between.

		PC	
Factor	Opposition's Position	Position	Resolve
Relationships between proposed buildings and their surroundings	Totally Negative Simply not consistent with Comprehensive Plan wording	Negative: Staff acknowledges that the scale and mass of the proposed building will be unlike any other residential building in the surrounding area. But then side with the developer's efforts	Pass
Amenities	Negative: only has meaning if within walking distance Other than or Unsubstanti		Pass
Urban Service	None (other than bus which is double counting)	Other than bus (already included) – unsubstantiated	Pass
Arterial Street	Negative: 2-lane, at capacity; don't use to leverage more usage; overflow parking problem especially in winter; OSR is a minor arterial road	Say fulfilled	Pass
Transit	Bus line; leveraged by "arterial" street designation; little usage at present	Overplay	Pass
Natural features	Arguable since not defined. There are trees, wildlife, and good soil. There is a historic barn, likely the last in Madison and one of the few in Dane County	Say fulfilled	Pass
Park	Say fulfilled	Say fulfilled	Pass

In conclusion: This process cannot be distinguished from being political with enough arbitrariness to produce a desired outcome. Is there any monitoring, oversight, check or balance of the Plan Commission procedures? How is meaningful, bottom-up neighborhood feedback and dialog restored to this process?



Side-by-Side Comparison: Top – illustrates height; Bottom – best illustrates frontal length and overall comparison.

BEST, IMMEDIATELY NEIGHBORING, COMPARABLE COMPARISON

Parameter	Proposed Building	Settlers Woods
Frontal Length	400 ft	100 ft
Setback from Curb	35	84
Height	More	Less
Ratio, Apparent (Angular) Height from Curb	2-3 to) 1
Dwelling Units / Acre	36.6	14.4

Appendix 2 Public Hearing, CC 18 June

These remarks oppose the proposed development of the Pierstorff century farm.

Last week the Plan Commission acted on a Staff Report, and proposed demolition, conditional use, rezoning, and CSM. The last two appear as items #13 & #49 on tonight's Agenda.

Last Friday I submitted a critique of that meeting's process, parts of which are addressed here.

- We oppose 3 aspects, in particular
 - 1st Stormwater concerns from vastly increased impervious land coverage and likely climate change ... this is addressed separately;
 - 2nd Overbearing massing
 - 3rd Proliferation of rental-only apartments that rule out owner-occupied missing middle housing.
- Some Specifics of this Process
 - Presentation of storm water issues was incomplete at best; possibly incorrect at worst.
 - As to Massing
 - Judgement Criteria
 - "... findings must be based on substantial evidence (Staff Report)
 - Applicable Conditional Use Standard #8 reads "... Plan Commission shall find that the project creates an environment of sustained aesthetic desirability compatible with the existing or intended character of the area ..."

- "... conditional use shall ... consider ... the recommendations in the ... Comprehensive Plan ... which reads "... newly developing LMR areas should be seamlessly integrated with surrounding development".
- The Plan Commission sustained development
 - Citing "significant setbacks" (from the Plan Commission Minutes)
 - "Despite the proposed building being notably larger than those in the surrounding area, staff *feels* that the building ... etc. can create an environment of sustained aesthetic desirability." (from the Staff Report)
 - Notably, no mention made of Standard #8
- Developer's comparable was over a mile away instead of the adjacent Settlers Woods apartments.
- Picture + other comparisons
- The development's street view is nowhere close to words like aesthetics, seamless, or integrated.
- As to zoning & land use there are the Select Conditions Despite all conditions not being met, including 3 of greater significance, the findings were
 - 1st ... arterial status and bus availability " are the most significant factors as to why the proposed development may be approved."
 - 2nd Spun to justify and leverage additional upzoning and setting precedent for much greater scale and density elsewhere in the future.

In conclusion: This process cannot be distinguished from being political with enough arbitrariness to produce a desired outcome. Please consider how meaningful, bottom-up neighborhood feedback and dialog can be restored to this process.

- There are qualitative, subjective, and tentative wordings such as "could find", "believes", "feels" in critical instances in the absence of more quantitative, objective, and certain measures. Such passages invariably ended in discounting opposing assertions or resulted in recommended "fact".
- Staff Report, Pg 12 "a mess" if it had mattered, it would never have been released as written
- My professional career involved definitive measures, numerical and graphic analysis, and weighted factors in multi-factor requirements. Not nearly so in the case of the Staff Report and its judgement which involved subjective findings and cherry-picked selection criteria. This was a pre-ordained, political exercise.
- There was an unmistakable, deferential camaraderie between the PC and the developer. The public are definitely on the "outside looking in" of that relationship.
- In this case, at least, the developer worked with the PC for months to reach a mutually desirable outcome; a Staff Report from the Plan Division had gone to the PC a few days before the Meeting where it is given a "public hearing". Since the public's voice is not listened to, absorbed, thought about, questioned, and/or assimilated, "public hearing" essentially means the public gets to hear, but not interfere with, the agreed-to plan.
- With 12 members on that Commission, what's striking is that without meaningful discussion all votes are presumed unanimous (by default) ... unless someone raises a hand. Incomplete idea of members present. Unanimity strongly suggests prior discussion and approval; i.e. the outcome is pre-approved outside of and before the "Public Hearing".
- The outcome was clearly pre-ordained and never in doubt; the default on every motion was always "unanimous consent assumed unless a hand is raised" (by a PC member) ... there was never any discussion, or raised hands: every motion was systematically unanimous. The cruxes of public feedback summarily vanished.
- All four Items were summarily passed in oblivious disregard for the public's input.
- This political process features strictly top-down determination with window-dressing community input on consequential issues.
- Public input metered to 3 minutes. By contrast, the developer could be, and was, granted more, unlimited time by virtue of simply being asked a leading, soft-ball question by a PC member. There were no instances of the public or its experts being asked any questions or in any way being engaged in dialog. The tone of the meeting was clearly in favor of the developer. The PC only had discussion about approval, justification, and praise for developer.

Zoom

- Login accessibility: links caused problems.
- No participants icon that would have indicated problems
- No chat; no video (couldn't show graphic)
- Screen so sparse with information couldn't readily tell if logged in
- Last minute release of the Staff Report

Appendix 3

Position Against
Proposed Stone House Development of the
Pierstorff Farm, 6610-6706 Old Sauk Road
Mike and Lynn Green

44 Year Residents at 6709 Old Sauk Rd, Opposite the Proposed Development
5 June 2024

We are firmly against this Proposal as it stands. We are not against change, development, some increase in density, residents of any ethnicity/race or economic status, or proper use. This Proposal has major deficiencies that are technical, that include overbearing size, and that are inappropriate in use as described below.

Originally, Stone House Development (SHD) showed an interest in community/neighborhood feedback. That feedback has consistently been negative. As planning and development progressed, mutual interaction with SHD faded and that with City Planning was most disheartening both for this project and, so far, for the evolving West Side Plan. The developer is out to make money while following the City's lead. As to the latter, there is a stark difference between present City policies and those of past administrations regarding the evolution of Madison. Previously, Madison housing had bottom-up, neighborhood/community driven policies; now that is reversed with top-down policy that marginalizes local involvement. Rationale for current policy is overly weighted, to dominated, by a projected massive influx of new residents over the next few decades; that will come at the expense of current residents with differing values, vision, and preferred use. But, this is a topic in its own right that is being developed elsewhere [Ref 1]. The fundamental point is that there should be a mutual discussion of these values, and not a monolog on our part that is unheard by the City, before a massive, and yet another, rental-only apartment complex is built.

Specifics of Opposition – There are many issues of which these are the most significant.

- STORMWATER MITIGATION Homes immediately to the north, and downhill from the proposed development suffered damage from the "1000 year" rainfall in August 2018; and that was from farmland that could absorb water. This situation will likely/possibly get worse either from climate change or that the real Recurrence Interval for similar storms is actually much less than 1000 years. The problem gets even worse when the site becomes 60% impervious because of construction. These north-border residents have vivid recollections of flooding damage, the heightened likelihood of worsened conditions, and thus major concern for the proposed development.
- MASSING LMR land use permits 3 stories and 30 dwelling units per acre (du/ac). This development is 3 stories and 36 du/ac which would require escalation for "special conditions". First, the escalation increases capacity/density by roughly 20%, which is to say, areal coverage by the same amount. But, not allowing that escalation reduces the building footprint which has two beneficial effects. The first effect is to reduce the storm water problem (above) and the second enables further increasing setback(s) for an already offensive structure.
 - The developer shows what are taken to be "comparables" in the area [Ref 2] but does not

- show them juxtaposed with the proposed development. Some of these (not cherry-picked) comparables are shown side-by-side in [Ref 3] with comparison to neighborhood housing and a nearby apartment complex.
- Starting with the comparison most favorable to the developer, the nearby Settlers Woods apartments, one observes a much shorter extent along Old Sauk Road (roughly 100 ft vs 400 ft) and shorter height. But, the most noticeable difference is the setback from the curb: roughly 87 ft vs 37 ft which is to say the "apparent" height of the new development is more than twice that of its nearest "comparable" besides being 4 times longer.
- O Comparison (height and frontal length) of the new development to its surrounding [houses in Ref 3] highlights how <u>incongruous this structure actually is</u>; and in the length comparison bear in mind that the apartment is an unbroken, continuous "wall".
- O The Comprehensive Plan states "... newly developing LMR areas should be seamlessly integrated with surrounding development" with which the Plan Commission is supposed to be consistent. A reasonable comparison of this development to its surroundings shows it is neither seamless or integrated, either in height or frontal extent. This development is literally and figuratively "in your face". On this single, basis alone this proposal should be rejected. Subjectively, it is appalling.
- USE Whereas much is made of the "housing crisis", there is an acknowledged crisis-within-a-crisis in terms of housing alternative to rental, apartment-only construction. This alternative, "Missing Middle" housing offers occupant ownership with several benefits. Renting means landlord control. Rental rate increases are the highest in the country [Ref 4]. Skyrocketing rental rates increase owner profits ... indefinitely. Rentals are already 60% of Madison housing; substantially increasing to more and more apartments from influx exacerbates all of these negatives. It does not appear to be providing, nor is it likely to provide "affordable housing". Non-rental, Missing Middle housing is the needed alternative which must be enabled. Further, and more importantly for the community, ownership provides investment not just financially but also in the neighborhood. Owners are likely to be longer-term residents with families who participate in local, civic activities, send their kids to local schools, and become active and vibrant neighbors that thrive and grow in this housing type. Present understanding is that the Stone House apartment proposal is neither family-oriented nor affordable (especially to families).

City Leveraging – There is another problem at play as well, and that is the City leveraging its position on Old Sauk Road (OSR). This is a two lane road with few crosswalks (three now, it used to be only one at Crestwood School) in the 1.2 mile stretch between Old Middleton Road and Gammon Road. It is a very busy road, with often speeding traffic (passing over the center line or in the parking lane) and scant speed enforcement that, to a resident on OSR, is already at capacity. The SHD proposal will double to triple the number of dwelling units in that stretch of road. Further, the City with its Proactive Zoning philosophy has aspirations to build more higher density units just east of here. All of this is just "piling-on" (leveraging), by the City, to a saturated corridor.

Timing – These comments come ahead of the Plan Commission's Public Review of the SHD Proposal on 10 June. That Review will cover Re-zoning and Conditional Uses but the Staff

Report covering the "specific standards" against which the Proposal will be judged are not available until noon on Friday, 7 June. As a result, comments, above are necessarily incomplete as not only the "specific standards" but the parameters to be judged are not yet spelled out or available. Further, and worst of all, is that there are only a few days over the weekend for citizens to read over the objective details of the Proposal before the Public Review. This simply is grossly unfair to the public reviewers.

Finally, review, and possible passage of the SHD come at a time when other, relevant and possibly consequential meetings are occurring. One such is the series of the Housing Strategy Subcommittee which, in part, is looking into timely solutions for Missing Middle housing; it is believed that results from that study should be released this summer. Additionally, there is the ongoing and maturing West Area Plan meetings and drafts. The property addressed in the Proposal is in the West Area and would, or should, be subject to its recommendations. Both of these series concern getting-it-right where new development is concerned. The City's development polices should reflect, and give substantial weight to, these ongoing studies in lieu of maximizing apartment construction (present form of densification).

References

- [1a] March 8, 2024 [Fanlund, Cap Times] "City hall is taking aim at Madison homeowners' neighborhoods"
- [1b] March 16, 2024 [Soglin, Cap Times] "Madison zoning plan stinks, and so does its implementation"
- [1c] March 25, 2024 [Fanlund, Cap Times] "Does zoning furor suggest Madison is becoming two cities?"
- [1d] March 29, 2024 [Soglin, Cap Times] "Zoning proposals would erode Madison's sense of place"
- [1e] April 1, 2024 [Fanlund, Cap Times] "Historian Mollenhoff laments power shift to Madison planners"
- [1f] May 24, 2024 [Fanlund, Cap Times] "The common narrative around Madison rezoning is misleading"
- [2] Pg 18, Project Plans, #3, Legistar 82972 Version 1
- [3] See side-by-side comparisons (attached)
- [4] March 28, 2023 [Channel 3] "Madison year-over-year rent increases are the highest in the country, study finds"

















0' 8' 16' 32'

























2 SOUTH ELEVATION
A201 1/16" = 1'-0"

0' 8' 16' 32'

Appendix 4

PLANNING DIVISION STAFF REPORT

June 10, 2024

Project Addresses: 6610-6706 Old Sauk Road

Application Type: Demolition Permit, Zoning Map Amendment, Conditional Uses, and

Certified Survey Map Referral

Legistar File ID # 82950, 83477, 82972, and 82979

Prepared By: Timothy M. Parks, Planning Division

Report includes comments from other City agencies, as noted

Reviewed By: Kevin Firchow, Planning Division

Bill Fruhling, Interim Planning Division Director

Summary

Applicant: Helen H. Bradbury, Stone House Development; 1010 E Washington Avenue, Suite 101; Madison.

Property Owner: Robert Pierstorff; 6610 Old Sauk Road; Madison.

Surveyor: Zach Reynolds, Wyser Engineering, LLC; 300 E Front Street; Mount Horeb.

Requested Actions:

ID <u>82950</u> – Consideration of a demolition permit for 6610-6706 Old Sauk Road to demolish two single-family residences and a two-family residence;

- ID <u>83477</u> Consideration of a request to rezone 6610-6706 Old Sauk Road from SR-C1 (Suburban Residential–Consistent 1 District) and SR-C3 (Suburban Residential–Consistent 3 District) to TR-U2 (Traditional Residential–Urban 2 District);
- ID <u>82972</u> Consideration of a conditional use in the [Proposed] TR-U2 (Traditional Residential-Urban 2 District) for a multi-family dwelling with greater than 60 units and consideration of a conditional use in the TR-U2 District for outdoor recreation, all to allow construction of a three-story, 138-unit apartment building with an accessory outdoor pool; and
- ID <u>82979</u> Approval of a Certified Survey Map (CSM) to create one lot for the proposed residential development.

Proposal Summary: The applicant is seeking approvals to redevelop two parcels located at 6610 and 6706 Old Sauk Road with a three-story, 138-unit apartment building with an outdoor pool following the demolition of a single-family residence at 6610 Old Sauk Road, a two-family residence at 6612-6614 Old Sauk, and a single-family residence at 6706 Old Sauk. The proposed apartment building will include parking for 143 automobiles underground and in 25 outdoor stalls, and a total of 154 bike parking stalls. The two parcels will be combined into one lot by CSM. The letter of intent indicates that construction will commence as soon as all regulatory approvals, with completion anticipated in September 2025.

Applicable Regulations & Standards: Section 28.182 of the Zoning Code provides the process for zoning map amendments. Table C-1 in Section 28.032(1) identifies a multi-family dwelling with greater than 60 units and outdoor recreation as conditional uses in the proposed TR-U2 (Traditional Residential—Urban 2) zoning district. Section 28.183 provides the process and standards for the approval of conditional use permits. Section 28.185 provides the process and standards for the approval of demolition and removal permits. The subdivision process is outlined in Section 16.23(4)(f) of the Subdivision Regulations.



Review Required By: Plan Commission and Common Council.

Summary Recommendation: if the Plan Commission can find the applicable standards are met, the Planning Division recommends the following actions to the Plan Commission:

- That the Plan Commission find that the standards for demolition permits are met to **approve** demolition of the three residences located at 6610-6706 Old Sauk Road;
- That the Plan Commission forward Zoning Map Amendment ID 28.022–00672, rezoning 6610-6706 Old Sauk Road from SR-C1 and SR-C3 to TR-U2, to the Common Council with a recommendation of **approval**;
- That the Plan Commission find the standards for conditional uses are met to approve a three-story, 138unit apartment building and pool, subject to input at the public hearing and the conditions from reviewing agencies beginning on page 12; and
- That the Plan Commission forward the Certified Survey Map to combine 6610-6706 Old Sauk Road into
 one lot to the Common Council with a recommendation of approval subject to the conditions from
 reviewing agencies beginning on page 20.

Background Information

Parcel Location: Two parcels totaling 3.77 acres located on the north side of Old Sauk Road opposite San Juan Trail; Alder District 19 (Guequierre); Madison Metropolitan School District.

Existing Conditions and Land Use:

- 6610 Old Sauk Road is developed with a single-family residence (6610) and two-family residence (6612-6614), zoned SR-C3 (Suburban Residential–Consistent 3 District);
- 6706 Old Sauk Road is developed with a single-family residence and accessory barn, zoned SR-C1 (Suburban Residential–Consistent 1 District).

Surrounding Land Uses and Zoning: The subject site is bordered on the north, south, and west by single-family residences in SR-C1 (Suburban Residential—Consistent 1 District) zoning. On the east, the site adjoins Saukborough Square, a four-building complex of eight-unit multi-family buildings, and Settlers Woods Condominiums, a complex of single-family residences and a two-family residence; both complexes are zoned PD (Planned Development District).

Adopted Land Use Plan: The 2023 Comprehensive Plan identifies the subject site and parcels to the east for Low-Medium Residential (LMR). The single-family residences otherwise surrounding the site are recommended for Low Residential (LR).

Zoning Summary: The subject site will be zoned TR-U2 (Traditional Residential–Urban 2 District), which will be reviewed in the following sections.

Requirements	Required	Proposed
Lot Area	350 sq. ft. (48,300 sq. ft.)	161,024 sq. ft.
Lot Width	50′	553′
Minimum Front Yard Setback	15′	15' (Open porches: 11')
Maximum Front Yard Setback	30′	

	Requirements	Required	Proposed
	Side Yard Setback	10'	66' (East) 54' (West)
Rear Yard		20'	62'
Maximum Lot Coverage		80%	56%
Usable Open Space		40 sq. ft./unit (5,520 sq. ft.)	11,000 sq. ft
Maximum Building Height		6 stories/ 78'	3 stories/ 36.2'
Auto Parking		1 per dwelling unit (138 total)	143 enclosed/ garage; 25 surface (168 total)
Electric Vehicle (EV) Stalls		EV Ready: 10%; 14 stalls	19 EV Ready
	Accessible Stalls	7	7
	Bike Parking	1 per unit up to 2-bedrooms, half- space per add. bedroom (140); 1 guest space per 10 units (14) (154 total)	154
	Loading	None	0
	Building Forms	Large Multi-Family Building	Will comply (See Zoning Conditions)
Other	Critical Zoning Items		
Yes:	Utility Easements		
No:	Barrier Free, Urban Design, Transit-Oriented Development Overlay, Wellhead Protection, Waterfront Development, Wetlands, Floodplain, Adjacent to Park, Landmarks		
	•	Prepared by: Jacob Mc	oskowitz, Assistant Zoning Administrator

Environmental Corridor Status: The subject site is not located in a mapped environmental corridor.

Public Utilities and Services: The site is currently served by a full range of urban services, including Metro Transit, which operates seven-day service with trips at least every 30 minutes along Old Sauk Road (Route R). Metro Transit would initially estimate the following counts of potentially eligible trips towards US Green Building Council/LEED Quality Access to Transit points: 37 weekday and 33 weekend. Please contact Metro Transit if additional analysis would be of interest.

Project Description

The applicant is requesting approval of a demolition permit to demolish three residences located on two parcels at 6610 and 6706 Old Sauk Road and to rezone the parcels from SR-C3 and SR-C1, respectively, to TR-U2 to facilitate redevelopment of the property with a three-story, 138-unit apartment building with outdoor pool. Additionally, a Certified Survey Map (CSM) is proposed to combine the underlying parcels into one lot.

From east to west, the buildings to be demolished are:

A one-story single-family residence addressed as 6610 Old Sauk Road on the eastern half of the 37,948.2 square-foot (0.87-acre) (per City records) parcel of the same address. The ranch-style single-family residence was constructed in 1956 per City records and contains three bedrooms, one bathroom, and a two-stall attached garage, with a carport adjacent to the eastern wall. The residence is set back approximately 115 feet from the southern property line at Old Sauk Road.

- A split level two-family residence addressed as 6612-6614 Old Sauk, which occupies the western half of the 6610 Old Sauk parcel. The ranch duplex was constructed in 1970 and contains four bedrooms, two bathrooms, and dual one-car basement garages, and is set back 55 feet from the southern property line.
- A one-story single-family residence located in the northwestern corner of the 2.9-acre parcel at 6706 Old Sauk Road. According to City records, the ranch-style residence was built in 1970 and contains three bedrooms, two bathrooms, and a two-car attached garage. The demolition plan also notes a 220 square-foot detached garage located next to the house along the northern property line, and a two-story stone and masonry barn and two silos located southeast of the residence. The residence is located approximately 275 feet from the southern property line and 15 feet at its closest point from the northern property line, while the barn is set back 215 feet and 50.7 feet from those respective property lines.

Photos of the interior and exteriors of the three principal buildings and the accessory barn are included in the materials submitted for the demolition permit. Additionally, a demolition plan is included in the application materials, which highlights the salient features of the 3.77-acre site. Generally, the two single-family residences are located on the high points of the site, with the grade of the property falling towards the property lines and a low-laying area in the center of the site. Additionally, the subject site features considerable tree cover across most of the property, as noted on the demolition plans, including a line of canopy trees located adjacent to curb along the north side of Old Sauk Road. The project team has submitted a report prepared by an ecological consultant and arborist on the condition of the trees located on the perimeter of the site, including in the right of way of Old Sauk Road. The tree report is attached to the conditional use file for the project, ID 82972.

The proposed apartment building will feature three north-south wings of varying depth organized along a single east-west central spine, which will create two north-facing courtyards and two south-facing courtyards. The main entrance to the building will be located along the northern wall of the central wing and be accessed from a surface parking lot that will extend along the northern and eastern walls of the building. A lobby, community room, and tenant amenities will be located on the first floor adjacent to the northern entrance, while a coworking space for tenants will be located along the southern wall of the central wing adjacent to a secondary entrance that will provide direct pedestrian access to Old Sauk Road. The center wing of the building will be roughly centered on San Juan Trail, a local street that intersects Old Sauk Road opposite the subject site. A pool, hot tub, and sauna are proposed in the northeast courtyard of the building, while a fire pit and bocce court are proposed in the northwest courtyard. The south-facing courtyards will be open grass and landscaping. The 138 dwelling units proposed will include 25 studio units, 66 one-bedroom units, 43 two-bedroom units, and four (4) three-bedroom units. Parking for 143 automobiles and 140 bicycles will be provided in an under-building garage, with 25 parking spaces for autos and 14 bike stalls located around the perimeter of the building.

The proposed building will stand approximately 36 feet in height and be topped by a flat roof. The building will be clad with a combination of light brown-colored brick and gray fiber cement siding. Patios and balconies are proposed for all of the units, including patios that will open onto the courtyards for the abutting first floor units and patios connected to the Old Sauk Road sidewalk for the five first floor units to be located along the southern ends of the three wings.

In addition to combining the underlying parcels into one lot for the proposed multi-family development, the proposed CSM will dedicate a total of 40 feet of right of way as measured from the centerline of Old Sauk Road.

Supplemental Regulations

The following supplemental regulations in Section 28.151 of the Zoning Code apply to Outdoor Recreation:

- (a) A minimum 25-foot setback area maintained as open space shall be provided along the perimeter of the site wherever it abuts a residential district.
- (b) If the use will be available to the general public, an arterial or collector street of sufficient capacity to accommodate the traffic that the use will generate shall serve the site. Ease of access to the site by automobiles, transit, bicycles, and pedestrians shall be considered as a factor in the review of any application.
- (c) The site shall be designed in such a way as to minimize the effects of lighting and noise on surrounding properties. Hours of operation may be restricted and noise and lighting limits imposed as part of the conditional use approval.
- (d) An appropriate transition area between the use and adjacent property may be required, using landscaping, screening, and other site improvements consistent with the character of the neighborhood.

Analysis

The applicant is requesting approval of a zoning map amendment to rezone two parcels totaling 3.77 acres from SR-C1 and SR-C3 to TR-U2 to facilitate redevelopment of the site with a three-story, 138-unit apartment building following demolition of three existing residences and a variety of accessory buildings. Additionally, the applicant is seeking approval of a one-lot CSM to combine the parcels for the proposed multi-family redevelopment.

Consistency with Adopted Plans

The subject site is not currently located within the boundaries of an adopted neighborhood, sub-area, or area plan. The 2023 <u>Comprehensive Plan</u> generalized future land use plan recommends the subject site and parcels to the east for Low-Medium Residential (LMR), while the single-family residences otherwise surrounding the site are recommended for Low Residential (LR).

According to the <u>Comprehensive Plan</u>, LMR areas are made up of any or all of the following types of housing: small-lot single-family development, two-unit buildings, three-unit buildings, rowhouses, and small multi-family buildings. LMR areas are largely characterized by what is sometimes referred to as the "Missing Middle" of housing development: the range of multi-unit or clustered housing types that fall between the extremes of detached single-family homes and large apartment buildings. Building forms present in LMR are generally compatible in scale with single-family homes, and may therefore be intermixed with small-lot single-family development or used as a transition from more intense development to lower intensity areas comprised primarily of single-family development. LMR areas should be characterized by a walkable, connected street network to meet the growing demand for walkable urban living. Existing, isolated LMR areas should be better connected with their surroundings when opportunities arise, and newly developing LMR areas should be seamlessly integrated with surrounding development. Development in the LMR category should range in density from 7-30 units per acre and buildings should be up to three stories tall.

However, the 'Residential Future Land Use Categories' table on page 20 of the Growth Framework in the Plan includes a provision that allows large and courtyard multi-family buildings to be considered appropriate on properties recommended for LMR in "select conditions" at up to 70 dwelling units an acre and four stories of

height. The factors to be considered include relationships between proposed buildings and their surroundings, natural features, lot and block characteristics, and access to urban services, transit, arterial streets, parks, and amenities. These factors were expanded with the updates to the <u>Comprehensive Plan</u> adopted by the Common Council on December 5, 2023; previously, the only consideration was whether the LMR site was located along an arterial roadway. The effect of the wording change with the 2023 amendment allows more factors to be considered when determining whether the building forms more commonly associated with the more intensive Medium Residential (MR) land use category are appropriate in LMR and to make those forms possible at more locations compared to the prior language, which effectively limited them to sites on arterial roadways.

Consideration of Zoning Map Amendment Standards

The standards for zoning map amendments found in Section 28.182(6) of the Zoning Code state that such amendments are legislative decisions of the Common Council that shall be based on public health, safety, and welfare, shall be consistent with the <u>Comprehensive Plan</u>, and shall comply with Wisconsin and federal law. Wis. Stats. Section 66.1001(3) requires that zoning map amendments approved after January 1, 2010 be consistent with the City's <u>Comprehensive Plan</u>. 2010 Wisconsin Act 372 clarified "consistent with" as "furthers or does not contradict the objectives, goals, and policies contained in the comprehensive plan."

The statement of purpose for the TR-U zoning districts (TR-U1 and TR-U2) are established to "stabilize and protect and encourage the essential characteristics of high-density residential areas and to accommodate a full range of life-cycle housing." Other stated purposes of the TR-U districts include insuring that new buildings and additions to existing buildings are designed with sensitivity to their context in terms of building placement, facade width, height and proportions, garage and driveway placement, landscaping, and similar design features; maintaining and improving the viability of existing housing of all types, while providing for updating of older housing in a context-sensitive manner; maintaining or increasing compatibility between residential and other allowed uses, and between different housing types, where permitted, by maintaining consistent building orientation and parking placement and screening; and facilitating the preservation, development or redevelopment goals of the Comprehensive Plan and of adopted neighborhood, corridor or special area plans.

The TR-U2 zoning district requested allows for multi-family dwellings to be developed at a density of 124 units an acre (350 square feet of lot area per unit) and 40 square feet of usable open space per unit, with an allowed height of six stories and 78 feet. [Note: The TR-U1 zoning district allows less than half the density of TR-U2 (58 units per acre) and has similar setback requirements. However, TR-U1 requires four times the usable open space (160 square feet per multi-family unit) as TR-U2 (40 square feet), hence the district request.]

The proposed three-story apartment building meets the criteria for both a 'large multi-family building' and a 'courtyard multi-family building' in the Residential Building Form Standards in Section 28.172 of the Zoning Code. The development proposes a net density of 36.6 units per acre based on 138 units on the 3.77-acre site (lest Old Sauk Road right of way.)

In order to find the proposed rezoning consistent with the <u>Comprehensive Plan</u>, the above mentioned select conditions should be considered in turn:

• Relationships between proposed buildings and their surroundings and lot and block characteristics: The proposed apartment building will occupy approximately 56% of the 3.77-acre site, which has 553 feet of frontage along Old Sauk Road. Properties on three sides of the subject site are single-family residences on lots generally created between 1979 and 1988 north of Old Sauk Road and 1965-1978 south of Old Sauk. The subject site is identified as 'Lands' on the plats of Woodland Hills and First Addition to Woodland Hills, which

form the western and northern edges of the site, respectively, and the plat of Saukborough, which forms the eastern line of the site. The Woodland Hills plats feature lots on cul-de-sacs, which back up to the site and afford no opportunity for connectivity. Development in Saukborough includes a variety of single- and two-family residences located on private courts off of Sauk Ridge Trail, and Saukborough Square, a four-building complex of eight-unit multi-family buildings. Like to the north and west, there is no opportunity for development of the subject site to connect to the development to the east. The development pattern surrounding the site effectively limits its development to one that would be exclusively accessed from and primarily oriented to Old Sauk Road.

Staff acknowledges that the scale and mass of the proposed building will be unlike any other residential building in the surrounding area. However, despite the scale of the project, staff feels that efforts have been made to limit the differences in scale between the building and lower-scale and density surrounding uses, most particularly the use of significant building setbacks where the building abuts those uses. The proposed building will be set back over 50 feet at its nearest points from the side and rear property lines, which is well in excess of the minimum setbacks required by zoning. The actual distance between the proposed building and the surrounding buildings will be greater once the setbacks of the existing buildings are considered. The 15-foot front setback along Old Sauk Road will be less than the setbacks of some, but not all of the buildings on the north side of the street, but again, the use of the north-south wings of the building and the courtyards in between should result in scale and massing along Old Sauk Road that is more in keeping with the pattern of buildings along the road.

Staff also believes that the combination of the mass being centered in the site, the relatively short lengths of wall sections, the use of six-foot privacy fencing along the side and rear lot lines, and the use of a lower-profile design for the three-story building that features a flat roof rather than a pitched roof and modest floor-to-floor heights should all help to reduce the appearance of the scale of the building.

- Natural features: Staff does not believe that there are any natural features on the site or on the surrounding
 properties that would suggest that the building should not be built as proposed. While not defined in the
 Comprehensive Plan, staff believes that it would be reasonable for the Plan Commission and Common Council
 to consider "natural features" as those topographic features commonly identified in plans and environmental
 corridor mapping where urban development may not be appropriate, including wetlands, floodplains,
 waterways, and areas of steep slopes. None of those features are present on the site or on surrounding
 parcels.
- Access to urban services, transit, arterial streets, parks, and amenities: Old Sauk Road is classified as a minor arterial roadway according to the Madison Metropolitan Planning Organization and includes marked on-street bike lanes. Metro Transit provides daily service at least every 30 minutes on route R along Old Sauk, with stops west and east of the subject site. The sidewalk network along the north side of Old Sauk Road, however, is incomplete, which will require pedestrians to cross to the south side of the street to where the sidewalk network is fully developed between Old Middleton Road and N Gammon Road. To aid that crossing, the Traffic Engineering Division is requiring a rapid flashing beacon (RRFB), refuge island, and continental crosswalk to be installed by the developer as a condition of approval for the development.

The site is less than a quarter-mile from Everglade Park on Everglade Drive south of Old Sauk Road, while the larger Woodland Hills Park located northwest of the site is closer to a half mile walking distance. Crestwood Elementary School is located three-quarters of a mile east of the site. The site and surrounding neighborhoods are located in an area of the City that does not currently have neighborhood-serving commercial businesses within a reasonable walking distance, thereby requiring that autos, bikes or transit be used to access retail and service businesses located elsewhere. However, two of the four quadrants of the Old Sauk Road-N Gammon Road intersection are recommended for Neighborhood Mixed-Use (NMU) in the Comprehensive

<u>Plan</u>. Those NMU parcels may transition over time into higher density mixed-use developments that may include some amount of neighborhood serving commercial uses. The subject site is just over a quarter-mile east of the Old Sauk-Gammon intersection and accessible by sidewalks on both sides of Old Sauk.

On balance, the Planning Division believes it is possible that the Plan Commission and Common Council could find that the development of a three-story, 138-unit multi-family dwelling on the subject site is consistent with the factors listed in the Comprehensive Plan for large multi-family buildings and courtyard multi-family buildings in the LMR land use category. Staff does not believe that all of the select conditions enumerated in the plan have to be present in order for the larger and denser building form to be allowed. While the proposed building is both a larger building form and denser than what is located in the surrounding area, the height and density (three stories and 36.6 dwelling units per acre) is within the range discussed in the plan should it be found the project meets the select conditions described above. The site's location along a minor arterial and the availability of daily and relatively frequent bus service are the most significant factors as to why the proposed development may be approved. The proposed development is also consistent with other goals and objectives in the Comprehensive Plan that encourage development of a wider mix of housing types, sizes, and costs throughout the City, and to increase the amount of housing available by allowing more housing in more places. Staff will note that the TR-U2 district is the least intensive conventional residential district that could implement this proposal when the amount of lot area and the amount of usable open space required are considered. However, the TR-U2 district does allow for greater intensities than those currently proposed and those supported in the LMR category for large and courtyard multi-family buildings. As a reference, any development exceeding 36 units would require a conditional use consideration from the Plan Commission, though heights up to six stories are permitted.

Consideration of Demolition Permit Standards

In order to approve a demolition request, the Plan Commission shall consider the factors and information specified in Section 28.185(9)(c) and find that the proposed demolition or removal is consistent with the statement of purpose of the demolition permits section and with the health, prosperity, safety, and welfare of the City of Madison. The standards for demolition approval state that the Plan Commission shall consider the report of the City's historic preservation planner regarding the historic value of the property as well as any report that may be submitted by the Landmarks Commission. On April 15, 2024, the Landmarks Commission recommended to the Plan Commission that the residences at 6610, 6612(-6614), and 6706 Old Sauk Road had no known historic value. A member of the Landmarks Commission expressed regret about the demolition of the barn on 6706 Old Sauk Road; however, as an accessory building, it is not subject to the Landmarks Commission's or Plan Commission's purview.

In approving a demolition permit, the Plan Commission may stipulate conditions and restrictions on the proposed building demolition as deemed necessary to promote the public health, safety and general welfare of the community, and to secure compliance with the standards of approval. The proposed conditions for this demolition may be found in the 'Recommendation' section of the report, which follows.

Consideration of the Conditional Use Standards

A conditional use is defined in the Zoning Code as "a use which, because of its unique or varying characteristics, cannot be properly classified as a permitted use in a particular district." The Plan Commission shall not approve an application for a conditional use unless it can find that all of the standards found in Section 28.183(6)(a), Approval Standards for Conditional Uses, are met. That section states: "The City Plan Commission shall not approve a conditional use without due consideration of the recommendations in the City of Madison Comprehensive Plan and any applicable, neighborhood, neighborhood development, or special area plan,

including design guidelines adopted as supplements to these plans. No application for a conditional use shall be granted by the Plan Commission unless it finds that all of the [standards for approval in Section 28.183(6) are met]." Before granting a conditional use, the Plan Commission may stipulate conditions and restrictions on the establishment, location, construction, maintenance and operation of the conditional use. Additionally, state law requires that conditional use findings must be based on "substantial evidence" that directly pertains to each standard and not based on personal preference or speculation.

A review of the standards that apply to the proposed multi-family dwelling and outdoor recreation conditional uses follows.

1. The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, or general welfare.

Unlike some other conditional uses allowed in various zoning districts throughout the Zoning Code, the construction of new residential on lands recommended in adopted plans for residential development, whether in newly developing areas or as infill in established areas, would typically be assumed to meet this standard for approval. This request has been reviewed by various City reviewing agencies who have provided comments and recommended conditions. Planning staff does not believe that the information provided in these comments suggest that this standard cannot be met.

Several public comments have been received for the project expressing various concerns, including comments related to the proposed development worsening existing drainage issues in the neighborhoods surrounding the subject site, to which staff feels this standard is most relevant. Among the comments received are two reports prepared by an independent civil engineer (Nahn) and comments submitted by a soils scientist (Norman) responding to the preliminary stormwater management plans that have been submitted by the development team (dated April 8 and May 24, 2024). It is rare in staff's experience for a stormwater management plan to be submitted prior to the Plan Commission's consideration of a project like the one proposed, let alone for the applicant's civil engineer to provide an amended plan in response to comments received. Additionally, the Plan Commission should note the four-page memo submitted by Assistant City Engineer Greg Fries dated May 31, 2024, which provides an initial staff review of the plans submitted by the development team for conformance with MGO Chapter 37, The Public Stormwater System Including Erosion Control.

It is the opinion of City staff that a residential development like the one proposed, which complies with the requirements in MGO Chapter 37, can meet conditional use standard 1 as it pertains to impacts from storm drainage and erosion. As noted in the Fries memo and in the conditions recommended in the City Engineering Division section of the 'Recommendations' section of this report, the applicants will be required to submit a final stormwater management plan and erosion control plan for approval by the City Engineer before the conditional use plans could be signed-off and permits issued for the project.

2. The City is able to provide municipal services to the property where the conditional use is proposed, given due consideration of the cost of providing those services.

The comments and recommended conditions of approval received from reviewing departments and included in the last section of this report, including conditions from the City Engineering Division, Traffic Engineering Division, and Madison Fire Department, suggest nothing out of the ordinary in providing municipal services to this property because of the proposed development.

3. The uses, values and enjoyment of other property in the neighborhood for purposes already established will not be substantially impaired or diminished in any foreseeable manner.

This standard is often the most difficult standard for the Plan Commission to address in the process of reviewing conditional use applications. Information provided by residents or property owners in the neighborhood at the Plan Commission hearing usually provides additional information for the Commission to use to determine whether this standard has been met or not. The proposed building has elicited a significant amount of correspondence from nearby residents, which has been provided for consideration as part of the approval of the project. The Plan Commission will need to weigh the application materials, the comments and conditions submitted by reviewing agencies, the recommendations of the Comprehensive Plan, and the public input it receives in order to determine whether this standard is met.

Planning staff believes that the supplemental regulations for outdoor recreation can be met for the tenant amenities located in the northern courtyards of the proposed building. The pool, hot tub, and other amenities will exceed the 25-foot setback for outdoor recreation, and the amenities should be adequately screened from nearby properties. However, in order to limit impacts on adjacent properties from the outdoor recreation, staff believes that it would appropriate for the Plan Commission to require the applicant to submit proposed hours for the outdoor recreation uses for approval by the Planning Division in consultation with the district alder prior to final sign-off of the conditional uses for the project.

- 4. The establishment of the conditional use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.
 - Staff does not believe that construction of the new building will impede the normal and orderly development or improvement of surrounding properties or preclude the development of other nearby properties in a manner consistent with the LR recommendations in the <u>Comprehensive Plan</u> and zoning of those properties. While the proposed building represents a significantly different building form compared to what currently exists on the subject site and on surrounding properties, staff does not foresee how construction of the apartment building will cause the surrounding residential neighborhoods to not continue in much the same fashion as the area has functioned historically.
- 5. Adequate utilities, access roads, drainage, internal circulation improvements for pedestrians, bicyclists, public transit and vehicles, parking supply (in cases with minimum parking requirements) and other necessary site improvements have been or are being provided.
 - Staff believes that the Plan Commission can find that this standard is met overall. As noted above in the discussion of standard 1, City staff feels that the drainage component of standard 5 can be met subject to its compliance with MGO Chapter 37 as administered by the City Engineer.

Regarding the adequacy of the utilities to serve the project, the developer will be required to provide wastewater flow calculations for the development that demonstrate that adequate sanitary sewer capacity exists to serve the project, as is typical for projects that propose a significant increase in dwelling units compared to the existing conditions. If additional capacity is needed, it may be the development team's responsibility to construct off-site sanitary sewer improvements to provide the needed capacity. No comments have been received from the Madison Water Utility that would suggest that there is not adequate water capacity to serve the development.

Finally, staff is aware of concerns by some residents of the surrounding area about the potential for the proposed development to create a significant amount of additional traffic along Old Sauk Road. However, the Traffic Engineering Division has not expressed significant concerns with the proposal and has accepted the traffic impact analysis submitted by the applicants, which is attached to the conditional use legislative file, ID 82972 for reference. As noted elsewhere in the Analysis section of this report, the project will be required to construct improvements to Old Sauk Road to improve pedestrian safety related to the project,

including constructing public sidewalk along the frontage and a pedestrian refuge island, and installing a rapid flashing beacon.

- 6. The conditional use conforms to all applicable regulations of the district in which it is located.
 - The Zoning Administrator has reviewed the project and determined that it will comply with the requirements of the proposed TR-U2 district.
- 8. When applying the above standards to any new construction of a building or an addition to an existing building the Plan Commission shall find that the project creates an environment of sustained aesthetic desirability compatible with the existing or intended character of the area and the statement of purpose for the zoning district. In order to find that this standard is met, the Plan Commission may require the applicant to submit plans to the Urban Design Commission for comment and recommendation.

Staff believes that the Plan Commission can find this standard is met. Despite the proposed building being notably larger than those in the surrounding area, staff feels that the building can create an environment of sustained aesthetic desirability. The impacts on surrounding properties will be moderated by the proposed mass being centered on the site and through the use of narrow north-south wings to form courtyards in an effort to limit the presence of the building when viewed from the north and along Old Sauk Road. Staff believes that the project is well designed and that review by the Urban Design Commission is unnecessary. However, following the public hearing, the Plan Commission has the option to refer the project to the Urban Design Commission should it feel its input is needed in order to find standard 8 met.

[Note: Standards 7 and 9-16 do not apply to the conditional use requests for 6610-6706 Old Sauk Road.]

As with any conditional use, the Plan Commission retains continuing jurisdiction in the event that complaints are received about the multi-family dwelling and accessory outdoor recreation, which could result in more restrictive conditions being applied if deemed necessary following an investigation and public hearing.

Criteria for Certified Survey Map

Finally, if the Plan Commission finds that the related land use approvals meet the standards for approval, it may also find that the proposed one-lot Certified Survey Map meets the standards and criteria for approval subject to the conditions in the Recommendations section of the report.

Conclusion

The applicant proposes to demolish the former two single-family residences and a two-family residence to allow redevelopment of the 3.77-acre site with a three-story, 138-unit apartment building in TR-U2 zoning. The requests are subject to the standards for approval for demolition permits, zoning map amendments, conditional uses, and land divisions.

The subject site is recommended for Low-Medium Residential (LMR) by the <u>Comprehensive Plan</u>, which is a land use category intended to primarily encourage development of small-lot single-family, two-unit buildings, three-unit buildings, rowhouses, and small multi-family buildings at densities between 7-30 units an acre. However, the applicants are requesting approval using a provision in the <u>Comprehensive Plan</u> that allows large multi-family buildings and courtyard multi-family buildings up to four stories in height and at densities up to 70 units an acre

to be considered appropriate in LMR under select conditions. On balance, the Planning Division believes that the Plan Commission and Common Council could find that proposed development is consistent with the select conditions to allow large multi-family and courtyard multi-family building forms in LMR. Specifically, the site's location along a minor arterial roadway that has relatively frequent daily bus service as the most significant factors as to why the proposed development may be approved. Additionally, staff believes that the conditional uses required for the development can meet the applicable standards for approval subject to the conditions in the following section.

In reviewing the project, the Plan Commission should carefully consider the dozens of public comments received since the development was first made public last fall. Those comments are attached to the legislative files associated with the project.

Recommendation

Planning Division Recommendation (Contact Timothy M. Parks, (608) 261-9632)

If the Plan Commission can find the applicable standards are met, the Planning Division recommends the following to the Plan Commission:

- That the Plan Commission that the standards for demolition permits are met to **approve** demolition of the two-story office building located at 1617 Sherman Avenue;
- That the Plan Commission forward Zoning Map Amendment ID 28.022–00621, rezoning 6610-6706 Old Sauk Road from SE to TR-U2, to the Common Council with a recommendation of approval;
- That the Plan Commission find the standards for conditional uses are met to approve a residential building
 complex containing approximately 310 apartments in two five-story buildings and a residential building
 complex containing 20 townhouse units in three two-story building, subject to input at the public hearing,
 final approval by the Urban Design Commission, and the conditions from reviewing agencies that follow;
 and
- That the Plan Commission forward the Certified Survey Map to divide 6610-6706 Old Sauk Road into three
 lots and one outlot for stormwater management to the Common Council with a recommendation of
 approval subject to the conditions from reviewing agencies beginning on page 23.

Recommended Conditions of Approval

Major/Non-Standard Conditions are Shaded

Planning Division

- Provide hours of operation for the proposed outdoor recreation for approval by the district alder and Planning
 Division director prior to issuance of building permits for the apartment building. Any revision to the approved
 hours of operation shall require approval of an alteration to the conditional use to be approved by the district
 alder and Director of the Planning Division or the Plan Commission.
- 2. Revise Sheet C100 to show the side and rear yard setback dimensions.

<u>City Engineering Division</u> (Contact Tim Troester, (608) 2671-995)

- 3. The applicant shall provide projected wastewater flow calculations to Mark Moder (mmoder@cityofmadison.com). The proposed development may result in off-site sanitary sewer improvements being required of the developer as a condition of development.
- 4. The area adjacent to this proposed development has been determined by the City Engineering Division to have a known flooding risk. City Engineering has set the minimum protective lowest entrance elevation opening at an elevation of 821.30. This standard is not intended to be protective in all cases. The developer is strongly encouraged to complete their own engineering analysis to determine and meet a protective elevation which they are comfortable with. In no case shall the protective elevation be set below the minimum threshold determined by the City Engineering Division.
- 5. Enter into a City / Developer agreement for the required infrastructure improvements. The agreement shall be executed prior to sign off. Allow 4-6 weeks to obtain agreement. Contact the City Engineering Division to schedule the development and approval of the plans and the agreement.
- 6. Construct sidewalk, terrace, curb and gutter, and pavement along the Old Sauk Road frontage to a plan approved by the City Engineer. Note: In order to save trees, a public limited easement may be required.
- 7. Madison Metropolitan Sewerage District (MMSD) charges are due and payable prior to City Engineering Division sign-off, unless otherwise collected with a Developer's / Subdivision Contract. Contact Mark Moder ((608) 261-9250) to obtain the final MMSD billing a minimum of two (2) working days prior to requesting City Engineering signoff.
- 8. Obtain a permanent sewer plug permit for each existing sanitary sewer lateral serving a property that is not to be reused and a temporary sewer plug permit for each sewer lateral that is to be reused by the development.
- 9. An Erosion Control Permit is required for this project.
- 10. A Storm Water Management Report and Storm Water Management Permit is required for this project.
- 11. A Storm Water Maintenance Agreement (SWMA) is required for this project.
- 12. This site appears to disturb over one (1) acre of land and requires a permit from the Wisconsin Department of Natural Resources (WDNR) for stormwater management and erosion control. The City of Madison has been required by the WDNR to review projects for compliance with NR-216 and NR-151; however, a separate permit submittal is still required to the WDNR for this work. The City of Madison cannot issue its permit until concurrence is obtained from the WDNR via their NOI or WRAPP permit process. Contact Eric Rortvedt at (608) 273-5612 of the WDNR to discuss this requirement. The applicant is notified that the City of Madison is an approved agent of the Department of Safety and Professional Services (DSPS) and no separate submittal to this agency or the Capital Area Regional Planning Commission (CARPC) is required for this project to proceed.
- 13. Revise the plans to show a proposed private internal drainage system on the site. Include the depths and locations of structures and the type of pipe to be used.

- 14. The proposed development proposes to construct underground parking. The proposed entrance to the underground parking is adjacent to a street low point. The applicant shall provide at a minimum of one (1) foot of rise from the adjacent back of walk in the driveway before breaking grade to the down ramp to the underground parking to protect the underground parking from inundation. The stated elevation is intended to be protective but does not guarantee a flood proof structure. The developer/owner are strongly encouraged to complete their own calculations and determine an elevation that protects their property to a level of service that they are comfortable with.
- 15. Provide additional detail how the enclosed depression(s) created by the parking entrance(s) to the below building parking area(s) is/are served for drainage purposes. The building must be protected from receiving runoff up through the 100-year design storm that is current in MGO Chapter 37. If the enclosed depression(s) is/are to be served by a gravity system provide calculations stamped by a Wisconsin P.E. that show inlet and pipe capacities meet this requirement. If the enclosed depression(s) is/are to be served by a pump system provide pump sizing calculations stamped by a Wisconsin P.E. or licensed Plumber that show this requirement has been met.
- 16. This project falls in the area subject to increased erosion control enforcement as authorized by the fact that it is in a TMDL ZONE and therefore will be regulated to meet a higher standard.
- 17. This project will disturb 20,000 square feet or more of land area and require an Erosion Control Plan. Please submit an 11" x 17" copy of an erosion control plan (pdf electronic copy preferred) to Megan Eberhardt (west) at meberhardt@cityofmadison.com, or Daniel Olivares (east) at daolivares@cityofmadison.com, for approval.
- 18. Demonstrate compliance with MGO Sections 37.07 and 37.08 regarding permissible soil loss rates. Include Universal Soil Loss Equation (USLE) computations for the construction period with the erosion control plan. Measures shall be implemented in order to maintain a soil loss rate below 5.0 tons per acre per year.
- 19. Complete weekly self-inspection of the erosion control practices and post these inspections to the City of Madison website as required by MGO Chapter 37.
- 20. Prior to approval, this project shall comply with Chapter 37 of Madison General Ordinances regarding stormwater management. Specifically, this development is required to submit a Storm Water Management Permit application, associated permit fee, Stormwater Management Plan, and Storm Water Management Report to City Engineering. The Storm Water Management Plan & Report shall include compliance with the following:

Submit prior to plan sign-off, a stormwater management report stamped by a P.E. registered in the State of Wisconsin.

Provide electronic copies of any stormwater management modeling or data files including SLAMM, RECARGA, TR-55, HYDROCAD, Sediment loading calculations, or any other electronic modeling or data files. If calculations are done by hand or are not available electronically, the hand copies or printed output shall be scanned to a PDF file and provided to City Engineering.

Detain the 2-, 5-, 10-, 100-, and 200-year storm events, matching post development rates to predevelopment rates and using the design storms identified in MGO Chapter 37.

If the development has an enclosed area that provides existing storage, the existing storage will need to be accounted for in addition to meeting the requirements for detention.

Provide infiltration of 90% of the pre-development infiltration volume.

Reduce TSS by 80% (control the 5-micron particle) off of newly developed areas compared to no controls.

Treat the first half-inch of runoff over the proposed parking facility.

Provide onsite volumetric control limiting the post construction volumetric discharge to the predevelopment discharge volume as calculated using the 10-year storm event.

The applicant shall demonstrate that water can leave the site and reach the public right of way without impacting structures during a 100-year event storm. This analysis shall include reviewing overflow elevations and unintended storage occurring on site when the storm system has reached capacity.

Submit a draft Stormwater Management Maintenance Agreement (SWMA) for review and approval that covers inspection and maintenance requirements for any best management practices (BMP) used to meet stormwater management requirements on this project.

21. Submit, prior to plan sign-off but after all revisions have been completed, digital PDF files to the Engineering Division. Email PDF file transmissions are preferred to: bstanley@cityofmadison.com (East) or ttroester@cityofmadison.com (West).

<u>City Engineering Division – Mapping Section</u> (Contact Julius Smith, (608) 264-9276)

- 22. Pending the final design for the public sidewalk improvements, a public sidewalk easement may be required for this project to protect existing trees. If so required this may be done with so on the face of the concurrent CSM. Contact Jule Smith (jsmith4@cityofmadison.com) for the required language to be included on the face of the CSM.
- 23. It is anticipated that the public improvements required to serve this proposed development will require additional right of way and/or easements located beyond the project boundary. The developer shall acquire the right of way and/or easements as required by the City at the developer's expense. In the event that the developer is unable to acquire the right of way and/or easements required, the City shall proceed to acquire the easements. The developer shall reimburse the City for all costs associated with the acquisition, including attorney's fees and any and all costs associated with court ordered awards. The developer shall provide a deposit at the time of contract execution to cover the estimated City staff expenses and easement cost for the acquisition. The developer shall note that separate, additional surety in an amount estimated to cover any potential court ordered awards shall be retained by the City until such time as appeal rights have expired. The additional surety shall be provided prior to the City making an offer for the easement.

The purpose of Outlot 1 of Woodland Hills, recorded as Document No. 1623944, was never stated on the plat. This outlot appears to function a public utility routing for sanitary sewer and drainage way for the overflow route from Old Sauk Road to East Spyglass Court at the time it was platted. It is not fully known why this outlot was not dedicated, or an easement was given for these purposes when it was platted, as it seems to be the intention of the outlot. Currently the outlot is privately owned by the owner of Lot 13 subject to the public facilities conditions without recorded rights.

The subject site accepts the drainage overflow of Old Sauk Road and transfers it to the outlot. Therefore, a public easement for stormwater drainage should be established over the path of the drainage on Outlot 1. The proposed development may add to the discharge to the outlot.

Additionally, while this easement is being acquired for stormwater drainage for the project, additional easement rights for the sanitary sewer shall be acquired across the outlot.

- 24. Any portion(s) of a public easement that is intended to be released shall be released by separate document prepared by City Office of Real Estate Services. Contact Jule Smith of Engineering Mapping (jsmith4@cityofmadison.com, ((608) 264-9276) to coordinate the Real Estate project, and associated information and fees required. If any release is required prior to recording of the plat, acknowledgement of the release and document number shall be noted on the face of the plat. Provide Fee Legal description and Exhibit for the Portions Sanitary Sewer Easements Document Nos. 1275466 and 1275467 that are being requested to be released with the development. The final area to be released are to be approved by the City Engineering Division.
- 25. Coordinate and request from the utility companies serving this area the easements required to serve this development. Those easements shall be properly shown, dimensioned and labeled on the CSM.
- 26. The address of the proposed apartment building is 6624 Old Sauk Road. The site plan shall reflect a proper street address of the property as reflected by official City of Madison Assessor's and Engineering Division records.
- 27. The pending Certified Survey Map application for this property shall be completed and recorded with the Dane County Register of Deeds, the new parcel data created by the Assessor's Office, and the parcel data available to Zoning and Building Inspection staff prior to issuance of building permits and an early start permits for new construction.
- 28. Submit a site plan and a complete building Floor Plan in PDF format to Lori Zenchenko (Izenchenko@cityofmadison.com) that includes a floor plan of each floor level on a separate sheet/page for the development of a complete interior addressing plan. Also, include a unit matrix for apartment buildings that shows the number of apartments on each floor. The Addressing Plan for the entire project shall be finalized and approved by Engineering (with consultation and consent from the Fire Marshal if needed) PRIOR to the verification submittal stage of this LNDUSE with Zoning. The final approved Addressing Plan shall be included in said Site Plan Verification application materials. Per 34.505 MGO, a full copy of the approved addressing plan shall be kept at the building site at all times during construction until final inspection by the Madison Fire Department. For any changes pertaining to the location, deletion or addition of a unit, or to the location of a unit entrance, (before, during, or after construction), a revised Address Plan shall be resubmitted to Lori Zenchenko to review addresses that may need to be changed and/or reapproved.

Traffic Engineering Division (Contact Sean Malloy, (608) 266-5987)

- 29. The applicant shall be responsible for the construction of a traffic island, marked continental crosswalk, pedestrian ramps, and a Rectangular Rapid Flashing Beacons (RRFB) to facilitate pedestrian crossings of Old Sauk Road.
- 30. Items in the right of way are not approvable through the site plan approval process. The right of way is the sole jurisdiction of the City of Madison and is subject to change at any time per the recommendation/plan of Traffic Engineering and City Engineering Divisions.
- 31. The applicant shall dedicate right of way or grant a public sidewalk easement for and be responsible for the construction of a minimum five (5)-foot wide sidewalk along their site's frontage of Old Sauk Road.

- 32. Note: The applicant has submitted the Traffic Impact Analysis study requested by the Traffic Engineering Division; the study has been reviewed and accepted by Traffic Engineering.
- 33. The applicant shall submit a waste removal plan for review by the City Traffic Engineer, which shall include vehicular turning movements.
- 34. The applicant shall submit one contiguous plan showing proposed conditions and one contiguous plan showing existing conditions for approval. The plan drawing shall be scaled to 1" = 20' and include the following, when applicable: existing and proposed property lines; parcel addresses; all easements; pavement markings; signing; building placement; items in the terrace such as signs, street light poles, hydrants; surface types such as asphalt, concrete, grass, sidewalk; driveway approaches, including those adjacent to and across street from the project lot location; parking stall dimensions, including two (2) feet of vehicle overhang; drive aisle dimensions; semitrailer movement and vehicle routes; dimensions of radii; and percent of slope.
- 35. The developer shall post a security deposit prior to the start of development. In the event that modifications need to be made to any City owned and/or maintained traffic signals, street lighting, signing, pavement marking and conduit/handholes, the Developer shall reimburse the City for all associated costs including engineering, labor and materials for both temporary and permanent installations.
- 36. The City Traffic Engineer may require public signing and marking related to the development; the developer shall be financially responsible for such signing and marking.
- 37. All parking facility design shall conform to the standards in MGO Section 10.08(6).
- 38. All bicycle parking adjacent pedestrian walkways shall have a two (2)-foot buffer zone to accommodate irregularly parked bicycles and/or bicycletrailers.
- 39. Per MGO Section 12.138 (14), this project is not eligible for residential parking permits. It is recommended that this prohibition be noted in the leases for the residential units.
- 40. The applicant shall adhere to all vision triangle requirements as set in MGO Section 27.05 (no visual obstructions between the heights of 30 inches and 10 feet at a distance of 25 feet behind the property line at streets and 10 feet at driveways.). Alteration necessary to achieve compliance may include but are not limited to; substitution to transparent materials, removing sections of the structure and modifying or removing landscaping elements. If applicant believes public safety can be maintained they shall apply for a reduction of MGO Section 27.05(2)(bb), Vision Clearance Triangles at Intersections Corners. Approval or denial of the reduction shall be the determination of the City Traffic Engineer.
- 41. The applicant shall provide a clearly defined five-foot walkway from the front door to the public right of way clear of all obstructions to assist citizens with disabilities, especially those who use a wheelchair or are visually impaired. Obstructions include but are not limited to tree grates, planters, benches, parked vehicle overhang, signage and doors that swing outward into walkway.
- 42. The applicant shall show the dimensions for the proposed Class III driveway including the width of the drive entrance, width of the flares, and width of the curb cut.

43. All existing driveway approaches on which are to be abandoned shall be removed and replaced with curb and gutter and noted on the plan.

Parking Division (Contact Trent W. Schultz, (608) 246-5806)

44. The applicant shall submit a revised Transportation Demand Management (TDM) Plan to tdm@cityofmadison.com. The revised TDM Plan shall include: a) Site TDM Coordinator contact information; b) Selected TDM measures, totaling the required TDM point value (15). Applicable fees will be assessed after the revised TDM Plan is reviewed by staff.

Zoning Administrator (Contact Jacob Moskowitz, (608) 266-4560)

- 45. Section 28.185(9) requires that every applicant for a demolition or removal approval that requires approval by the Plan Commission is required to get a Reuse and Recycling Plan approved by the City Recycling Coordinator, Bryan Johnson at streets@cityofmadison.com prior to receiving a raze permit. Every person who is required to submit a reuse and recycling plan pursuant to Section 28.185(9) shall submit documents showing compliance with the plan within 60 days of completion of demolition. A demolition or removal permit is valid for two (2) years from the date of the Plan Commission approval.
- 46. Provide adequate development frontage landscaping per Section 28.142(5) Development Frontage Landscaping. Landscaping and/or ornamental fencing shall be provided between buildings or parking areas and the adjacent street(s), except where buildings are placed at the sidewalk. One overstory deciduous tree and five shrubs shall be planted for each 30 lineal feet of lot frontage. Two ornamental trees or two evergreen trees may be used in place of one overstory deciduous tree. In cases where development frontage landscaping cannot be provided due to site constraints, the zoning administrator may waive the requirement or substitute alternative screening methods for the required landscaping. Note that landscaping must be installed on the private property.
- 47. Provide details demonstrating compliance with bird-safe glass requirements Section 28.129. For building façades where the first 60 feet from grade are comprised of less than 50% glass, at least 85% of the glass on glass areas 50 square feet or over must be treated. Of all glass areas over 50 square feet, any glass within 15 feet of a building corner must be treated. Identify which glass areas are 50 square feet or greater and which glass areas will be treated. Provide a detail of the specific treatment product that will be used.

<u>Fire Department</u> (Contact Matt Hamilton, (608) 266-4457)

- 48. Provide fire apparatus access as required by IFC 503 2021 edition, MGO Section 34.503. Provide plan documenting fire access. A Fire Apparatus Access and Fire Hydrant Worksheet is available on the MFD website to assist in development.
- 49. Per MGO Section 34.503/IFC 503 Appendix D105, Provide an aerial apparatus access fire lane that is at least 26 feet wide, if any part of the building is over 30 feet in height. The near edge of the aerial fire lane shall be within 30 xxfeet and not closer than 15 feet from the structure, and parallel to one entire side. The aerial fire lane shall cover not less than 25% of the building perimeter.
- 50. A dead-end fire lane that is longer than 150 feet shall terminate in a turnaround. Provide an approved turnaround (cul-de-sac, 45-degree wye, 90-degree tee) at the end of a fire lane that is more than 150 feet in length.

51. Fire access lanes shall be designed to support 85,000 lbs.

Parks Division (Contact Ann Freiwald, (608) 243-2848)

52. Park Impact Fees (comprised of the Park Infrastructure Impact Fee, per MGO Sec. 20.08(2)), and Park-Land Impact Fees, per MGO Sec. 16.23(8)(f) and 20.08(2) will be required for all new residential development associated with this project. This development is within the West Park-Infrastructure Impact Fee district. Please reference ID# 24025 when contacting Parks Division staff about this project.

Water Utility (Contact Jeff Belshaw, (608) 261-9835)

53. A Water Service Application Form and fees must be submitted before connecting to the existing water system. Provide at least two working days' notice between the application submittal and the requested installation or inspection appointment. Application materials are available on the Water Utility's Plumbers & Contractors website (http://www.cityofmadison.com/water/plumbers-contractors), otherwise they may be obtained from the Water Utility Main Office at 119 E Olin Avenue. A licensed plumber signature is required on all water service applications. For new or replacement services, the property owner or authorized agent is also required to sign the application. A Water Meter Application Form will subsequently be required to size & obtain a water meter establish a Water Utility customer account and/or establish a Water Utility fire service account. If you have questions regarding water service applications, please contact Madison Water Utility at (608) 266-4646.

Forestry Section (Contact Brandon Sly, (608) 266-4816)

- 54. An existing inventory of street trees located within the right of way shall be included on the site, demo, utility, landscape, grading, fire aerial apparatus and street tree plan sets. The inventory shall include the following: location, size (diameter at 4 1/2 feet), and species of existing street trees. The inventory should also note if a street tree is proposed to be removed and the reason for removal.
- 55. All proposed street tree removals within the right of way shall be reviewed by City Forestry before the Plan Commission meeting. Street tree removals require approval and a tree removal permit issued by City Forestry. Any street tree removals requested after the development plan is approved by the Plan Commission or the Board of Public Works and City Forestry will require a minimum of a 72-hour review period which shall include the notification of the Alderperson within who's district is affected by the street tree removal(s) prior to a tree removal permit being issued. Add as a note on the street tree plan set.
- 56. Contractor shall take precautions during construction to not disfigure, scar, or impair the health of any street tree. Contractor shall operate equipment in a manner as to not damage the branches of the street tree(s). This may require using smaller equipment and loading and unloading materials in a designated space away from trees on the construction site. Any damage or injury to existing street trees (either above or below ground) shall be reported immediately to City Forestry at (608) 266-4816. Penalties and remediation shall be required. Add as a note on the site, grading, utility, demolition, and street tree plan set.
- 57. As defined by the Section 107.13 of City of Madison Standard Specifications for Public Works Construction: No excavation is permitted within 5 feet of the trunk of the street tree or when cutting roots over 3 inches in diameter. If excavation is necessary, the Contractor shall contact Madison City Forestry at (608) 266-4816 prior to excavation. City of Madison Forestry personnel shall assess the impact to the tree and to its root system prior to work commencing. Add as a note on the site, grading, utility, demolition and street tree plan sets.

- 58. Section 107.13(g) of City of Madison Standard Specifications for Public Works Construction addresses soil compaction near street trees and shall be followed by the contractor. The storage of parked vehicles, construction equipment, building materials, refuse, excavated spoils or dumping of poisonous materials on or around trees and roots within five (5) feet of the tree or within the protection zone is prohibited. Add as a note on both the site and street tree plan sets.
- 59. On this project, street tree protection zone fencing is required. The fencing shall be erected before the demolition, grading or construction begins. The fence shall include the entire width of terrace and extend at least 10 feet on both sides of the outside edge of the tree trunk. Do not remove the fencing to allow for deliveries or equipment access through the tree protection zone. Add as a note on both the site and street tree plan sets.
- 60. Street tree pruning shall be coordinated with City Forestry at a minimum of two weeks prior to the start of construction for this project. Contact City Forestry at (608) 266-4816. All pruning shall follow the American National Standards Institute (ANSI) A300 Part 1 Standards for pruning. Add as a note on both the site and street plan sets.
- 61. The developer shall submit a Street Tree Report performed by International Society of Arboriculture Certified Arborist prior to the Plan Commission meeting for City Forestry's review of project. This report shall identify all street trees on proposed project site, species type, canopy spread, tree condition, proposed tree removals, the impacts of proposed construction, and any requested pruning.
- 62. The developer shall post a security deposit prior to the start of the development to be collected by City Engineering as part of the developer's agreement. In the event that street trees are damaged during the construction process, City Forestry will draw from this deposit for damages incurred.
- 63. Additional street trees are needed for this project. Tree planting specifications can be found in Section 209 of *City of Madison Standard Specifications for Public Works Construction*. All street tree planting locations and tree species within the right of way shall be determined by City Forestry. A landscape plan and street tree planting plan shall be submitted in PDF format to City Forestry for approval of planting locations within the right of way and tree species. All available street tree planting locations shall be planted within the project boundaries. Add following note on both the landscape and street tree plan sets: At least one week prior to street tree planting, Contractor shall contact City Forestry at (608) 266-4816 to schedule inspection and approval of nursery tree stock and review planting specifications with the landscaper.

Metro Transit (Contact Tim Sobota, (608) 261-4289)

This agency has reviewed the request and recommended no conditions of approval.

Certified Survey Map – Recommended Conditions of Approval

Major/Non-Standard Conditions are Shaded

<u>City Engineering Division</u> (Contact Tim Troester, (608) 267-1995)

1. A Phase 1 environmental site assessment (per ASTM E1527-21), is required for lands dedicated to the City. Provide one (1) digital copy and staff review will determine if a Phase 2 ESA is also required. Submit report(s) to Brynn Bemis (608-267-1986, bbemis@cityofmadison.com).

- 2. Enter into a City / Developer agreement for required infrastructure improvements. The agreement shall be executed prior to sign off. Allow 4-6 weeks to obtain agreement. Contact the City Engineering Division to schedule the development and approval of the plans and the agreement.
- 3. Construct sidewalk, terrace, curb and gutter, and pavement along the Old Sauk Road frontage to a plan approved by the City Engineer. Note: In order to save trees, a public limited easement may be required.
- 4. Madison Metropolitan Sewerage District (MMSD) charges are due and payable prior to City Engineering Division sign-off, unless otherwise collected with a Developer's / Subdivision Contract. Contact Mark Moder (261-9250) to obtain the final MMSD billing a minimum of two (2) working days prior to requesting City Engineering signoff.
- 5. A minimum of two (2) working days prior to requesting City Engineering signoff on the CSM, contact either Tim Troester (West) at (608) 261-1995 (ttroester@cityofmadison.com) or Brenda Stanley (East) at (608) 261-9127 (bstanley@cityofmadison.com) to obtain the final stormwater utility charges that are due and payable prior to sub-division of the properties. The stormwater utility charges (as all utility charges) are due for the previous months of service and must be cleared prior to the land division (and subsequent obsolesces of the existing parcel).

<u>City Engineering Division – Mapping Section</u> (Contact Julius Smith, (608) 264-9276)

- 6. Pending the final design for the public sidewalk improvements, a public sidewalk easement may be required for this project to protect existing trees. If required, this may be done with so on the face of the CSM. Contact Jule Smith (jsmith4@cityofmadison.com) for the required language to be included on the face of the CSM.
- 7. Any portion(s) of a public easement that is intended to be released shall be released by separate document prepared by City Office of Real Estate Services. Contact Jule Smith of Engineering-Mapping (jsmith4@cityofmadison.com, ((608) 264-9276) to coordinate the Real Estate project, and associated information and fees required. If any release is required prior to recording of the plat, acknowledgement of the release and document number shall be noted on the face of the plat. Provide fee legal description and exhibit for the portions sanitary sewer easements Document Nos. 1275466 and 1275467 that are being requested to be released with the development and shown on the CSM. The final area to be released are to be approved by the City Engineering Division.
- 8. The applicant shall dedicate the existing 33 feet of right of way and an additional 7 feet for Old Sauk Road as shown on the CSM.
- 9. Coordinate and request from the utility companies serving this area the easements required to serve this development. Those easements shall be properly shown, dimensioned and labeled on the final CSM.
- 10. Update the Madison Common Council certificate to include the current standard language as required by the Office of Real Estate Services. This will include the required acceptance language for the dedications included in the CSM.
- 11. Wisconsin Administrative Code A-E 7.08 identifies when Public Land System (PLS) tie sheets must be filed with the Dane County Surveyor's office. The Developer's Surveyor and/or Applicant must submit copies of required

tie sheets or monument condition reports (with current tie sheet attached) for all monuments, including center of sections of record, used in this survey, to Julius Smith, City Engineering (jsmith4@cityofmadison.com).

- 12. In accordance with Section s. 236.18(8), Wisconsin Statutes, the applicant shall reference City of Madison WCCS Dane Zone, 1997 Coordinates on all PLS corners on the Certified Survey Map in areas where this control exists. The surveyor shall identify any deviation from City Master Control with recorded and measured designations.
- 13. Prior to Engineering final sign-off by main office for Certified Survey Maps, the final CSM shall be submitted in PDF format by email transmittal to Engineering Land Records Coordinator Jule Smith (jsmith4@cityofmadison.com) for final technical review and approval. This submittal must occur a minimum of two working days prior to final Engineering Division sign-off.
- 14. Old Sauk Road does not vary. Remove 'Varies' from the description of the existing right of way.
- 15. Make the building labels and dimensions legible on sheet 3 of 5.
- 16. Correct the area for Lot 1. The overall lands appear to be listed for Lot 1 with the dedications included. Lot 1 should be about 161,000 square feet, +/- 3.70 acres.
- 17. Confirm the proper wording and page numbers on Note 6 on Sheet 1, Note 3 on Sheet 3, and Note 2 on Sheet 3.
- 18. Revise Sheet 3 to be Sheet 3 of 6.
- 19. The pending Certified Survey Map application for this property shall be completed and recorded with the Dane County Register of Deeds, the new parcel data created by the Assessor's Office, and the parcel data available to Zoning and Building Inspection staff prior to issuance of building permits for new construction or an early start permits for new construction.
- 20. The applicant shall submit to Jeff Quamme, prior to final Engineering sign-off of the subject CSM, one (1) digital CADD drawing in a format compatible with AutoCAD. The digital CADD file(s) shall be referenced to the Dane County Coordinate System and shall contain, at minimum, the list of items stated below, each on a separate layer/level name. The line work shall be void of gaps and overlaps and match the final recorded CSM: right of way lines (public and private); lot lines; lot numbers; lot/plat dimensions; street names, and; easement lines (including wetland and floodplain boundaries).
 - *This transmittal is a separate requirement than the required submittals to Engineering Streets Section for design purposes. The Developer/Surveyor shall submit new updated final plat, electronic data and a written notification to Engineering Mapping for any changes to the plat which occur subsequent to any submittal.

<u>Traffic Engineering Division</u> (Contact Sean Malloy, (608) 266-5987)

21. The applicant shall dedicate seven (7) feet of right of way along their frontage of Old Sauk Road for a total of 40 feet from the centerline.

Zoning Administrator (Contact Jenny Kirchgatter, (608) 266-4429)

This agency has reviewed the request and recommended no conditions of approval.

Fire Department (Contact Bill Sullivan, (608) 261-9658)

This agency has reviewed the request and recommended no conditions of approval.

Water Utility (Contact Jeff Belshaw, (608) 261-9835)

This agency has reviewed the request and recommended no conditions of approval.

Metro Transit (Contact Tim Sobota, (608) 261-4289)

This agency has reviewed the request and recommended no conditions of approval.

Parks Division (Contact Ann Freiwald, (608) 243-2848)

This agency has reviewed the request and recommended no conditions of approval.

Office of Real Estate Services (Andy Miller, (608) 261-9983)

- 22. Prior to approval sign-off by the Office of Real Estate Services ("ORES"), the Owner's Certificate(s) on the CSM shall be executed by all parties of interest having the legal authority to do so, pursuant to Wis. Stats. 236.21(2)(a). Said parties shall provide documentation of legal signing authority to the notary or authentication attorney at the time of execution. The title of each certificate shall be consistent with the ownership interest(s) reported in the most recent title report. When possible, the executed original hard stock recordable CSM shall be presented at the time of ORES approval sign-off. If not, the City and the Register of Deeds are now accepting electronic signatures. A PDF of the CSM containing electronic signatures shall be provided to ORES to obtain approval sign-off.
- 23. Prior to CSM approval sign-off, an executed and notarized or authenticated certificate of consent for all mortgagees/vendors shall be included following the Owner's Certificate(s). If a mortgage or other financial instrument is reported in record title, but has been satisfied or no longer encumbers the lands or ownership within the CSM boundary, a copy of a recorded satisfaction or release document for said instrument shall be provided prior to CSM approval sign-off.
- 24. As of May 31, 2024, real estate taxes are paid for the subject property. Per 236.21(3) Wis. Stats. and MGO Section 16.23(4)(f), the property owner shall pay all real estate taxes that are accrued or delinquent for the subject property prior to CSM recording. Receipts from the City of Madison Treasurer are to be provided before or at the time of sign-off.
- 25. As of May 31, 2024, there are no special assessments reported. All known special assessments are due and payable prior to CSM approval sign-off. If special assessments are levied against the property during the review period and prior to CSM approval sign-off, they shall be paid in full pursuant to MGO Section 16.23(4)(f).
- 26. A standard 60-year report of title was not received by Office of Real Estate Services with the CSM application materials. Pursuant to MGO Section 16.23(4)(f), the owner shall furnish a 60-year title report via email to Andy Miller in the ORES (acmiller@cityofmadison.com), as well as the survey firm preparing the proposed CSM. The

ID #82950, 83477, 82972 & 82979 6610-6706 Old Sauk Road June 10, 2024 Page 24

report shall include copies of all associated documents of record. A title commitment may be provided, but will be considered only as supplementary information to the title report update. The surveyor shall update the CSM with the most recent information reported in the title update. ORES reserves the right to impose additional conditions of approval in the event the title report contains changes that warrant revisions to the CSM.

- 27. Depict, dimension, name, note and/or identify by document number all relevant easements, declarations, plans, conditions, agreements, and other documents cited in record title and the updated title report, and include relevant notes from plats or CSMs of record. If documents included in the December 23, 2019 title report do not apply to the area within the proposed CSM, have them removed from the updated title report.
- 28. Depict and dimension public easements for utilities and storm water drainage rights of way to be dedicated on the proposed CSM where necessary.

From: <u>Diane Sorensen</u>
To: <u>Mayor; All Alders</u>

Subject: Public Comment for Agenda item 119, Legistar file 84123

Date: Tuesday, July 2, 2024 10:55:22 AM
Attachments: Conditional Use Appeal - F.pdf
Conditional Use Appeal - F.docx

Opposition - Stone House Dev 6610-6706 Old Sauk Rd (Mike & Lynn Green).pdf

Caution: This email was sent from an external source. Avoid unknown links and attachments.

Dear Mayor Rhodes Conway and Members of the City Council,

Attached please find my comments on the Appeal of the Plan Commission action on the conditional use request for 6610-6706 Old Sauk Road, Legister ID 82972 (District 19).

I ask that my comments be filed in the Public Comments for Legistar file 84123 and distributed to Mayor Rhodes Conway and all Common Council members.

In addition, if possible, I also ask that these comments replace my June 23, 2024 Erroneous Conditional Use Decision..pdf submission or be attached as a later version or otherwise added to that file.

Thank you,

Diane Sorensen

Dear Mayor Rhodes-Conway and Madison Alders,

I am a party to the appeal of the Plan Commission's approval of conditional use to the Stone House Development for a 3 story, 138 unit apartment complex at 6610 - 6706 Old Sauk Road. I write in support of the appeal. I request that my and argument be filed as a public comment in the appeal file, Legistar Files No. 84123, and the linked file, Legistar No. 82972.

As shown below, conditional use standards 1, 3, 5 and 8 are not met, therefore, the Plan Commissions conditional use approval was arbitrary and capricious..

1 and 5. The establishment, maintenance, or operation of the conditional use will not be detrimental to or endanger the public health, safety or general welfare. 5. Adequate utilities drainagehave been or are being provided.

The project brings numerous problems in its wake, but by far the most serious is the threat of flooding. The staff report slides right over the serious storm water flooding threat posed by this excessively large development, its inadequate, untested storm water management system and the inadequate stormwater infrastructure provided by the City. I refer the Council to the reports of Engineer Chuck Kahn, Prof. Emeritus John Norman and the summary letter of Christopher Nelson, Axley Attorneys for Jeff and Kathy Western and Paul and Mary Umbeck and the information provided by the Westerns and the Umbecks. These materials make it clear that this conditional use will be detrimental to the public health, safety and general welfare of those who reside to the north of this development.

The Commission, in its haste to build, build, build, ignored all of this expert advice, planting a time bomb across the fence from these homeowners. To quote Dr. Norman, "I see no way for the designers to escape the fact that not only will the underground storage basins fail in a year or two, but the infiltration dry-pond basin, which serves as a backup drainage for either underground basins as well as infiltration from pervious areas is also very likely to fail. ... Delaying this development NOW is critical because it is certain that this design will fail seriously long before its design life."

Because of the exacerbated flooding problems caused by this development which neither the City nor the Stone House Development has plans to adequately remedy, standards 1 and 5 are not met. The Plan Commission decision to the contrary is arbitrary and capricious.

3. The uses, values and enjoyment of other property in the neighborhood for purposes already established will not be substantially impaired or diminished in any foreseeable manner.

If 1 person comes to a doctor and complains that a pill prescribed by the doctor is causing him great stomach pain, the doctor may reasonably question whether the pain exists and whether his pill caused it. However, if 279 people complain of a stomach

ache and each of these people was prescribed and swallowed the same pill, the doctor must accept the fact that the pill she prescribed is causing pain. No doubt the medical community would listen. Our city politicians should too.

To state the obvious, each of the 279 co-petitioners opposes the development because the development it impairs/diminishes his or her use, value and enjoyment of his or her property. The losses of the co-petitioners are foreseen and described in the many letters opposing this development. These letters, many of which include personal stories that detail the personal angst experienced, speak loudly and clearly about the common losses that a too-big development causes: the loss of neighborhood cohesion, safety, peace, order, beauty and respite. In addition, as shown above, residents to the immediate north of the development face losses caused by their proximity to the site, including flooding, invasion of privacy and noise and light pollution.

The Plan Commission's finding that this standard was met is infuriating. How dare Commissioner Solheim toss out the statements of 279 residents as though we are idiots. How dare the Plan Commission ignore our many authentic and individualized descriptions of lost and impaired uses, values and enjoyment and substitute its preordained, density-driven finding that there is no loss. This is a prime example of the city refusing to listen to its residents and issuing an arbitrary decision in order to achieve high density housing.

PLEASE READ THE LETTERS IN LEGISTAR FILE NO. 82972.

I want to add that I don't want to live on East Washington Avenue or in any other high density area. I live here for the lower volume, slower pace and greater, greener space. The proposed apartment complex impairs that too. Yes, it is a privilege to live here with space and green and ease and I guess that makes me a "privileged" person. I can think of nothing finer than sharing my "privileged" life here with others who crave the same lifestyle, but that's only possible if here is here. It won't be if the Stone House development goes in.

The Plan Commission's finding that Standard 3 is not met is contrary to fact and, therefore, arbitrary and capricious. The conditional use approval must be reversed.

8. .. When applying these standards to any new construction of a building The Plan Commission shall find that the project creates an environment of sustained aesthetic desirability compatible with the existing or intended character of the area and the statement of purpose in the zoning code......

The Plan Commission conveniently skipped over this standard and for good reason: the proposed development is incompatible with the character of the area. Even the staff report concedes that "the building is unlike any other residential building in the surrounding area." The staff report then turns to the fact that "efforts have been made to limit the differences in scale". With all due respect to this novel argument, you can't

put a quart into a pint jar. It does not fit. It will not work. It makes a mess. And that's the result here.

This monstrous building does not belong in this neighborhood. It looks much like another institution designed by the same architectural firm (Law, Law & Potter) - the Lake View Sanitorium, except that the Lake View Sanitorium is a much smaller building on a much larger parcel (48 acres), making the building to grounds setting more aesthetically pleasing and balanced. There is no comparable forested hilltop setting for the Stone House apartment complex - instead its close-up, in-your-face like the neighborhood bully.

Stone House Development.



Lake View Sanitorium.





There's the lack of set back from Old Sauk Road, the loss of trees and other vegetation, the monstrous 425 foot spread, the complete lack of any warmth or welcome, but it will make no difference if the Council refuses to acknowledge the elephant in the room. This apartment complex is the elephant in the room. It's the elephant in the

neighborhood. If you do open your eyes and your minds, you will see that.** .Once seen, it cannot be unseen and you must recognize that the proposed apartment complex does not meet the standard 8 requirement that it creates and atmosphere of sustained aesthetic desirability compatible with the surrounding area.*. For this reason, too, the Plan Commission's approval of a conditional use is an error.

In conclusion, the Plan Commission's decision approving this conditional use was arbitrary and capricious because standards Madison City Ordinances Section 28.183(6) (a) 1, 2, 3, 5 and 8. The Common Council must reverse this decision.

Thank you for your careful consideration of my views.

Diane Sorensen

- *. (For additional evidence of the incompatibility of this development with other structures in the area, see the photographs attached to the Opposition Paper filed by Mike and Lynn Green on June 5, 2024, which I have attached to my cover email for your convenience.)
- **. One reason that the many aesthetic, scientific and practical problems around this development have not been the topic of serious discussion on the Council floor is that our alder is a developer whose "values and priorities" are those of a developer and who is solidly in the developers' corner. Indeed, he has spent all of his alder time at the Plan Commission and on the Council floor inviting further comment from Helen Bradbury, the Stone House engineer and city engineer in an effort to help them pitch their case. In contrast, he has not, at any point, invited any of the many opposition registrants to complete or further explain their remarks. Why would he call upon any of the hundreds of residents who oppose extreme density when, according to him, we are people motivated by our love of "treasures piled up on this earth" (that is to say, materialist), our fear of tenants (that is to say, elitist) and our desire to remain comfortably isolated from less propertied individuals (that is to say, privileged and classist). When Lynn Green spoke out on the Council floor about the demeaning slurs we experienced, this is what she was talking about. When the election winner, Kristen Slack (65.3%) had to step down, the city happily replaced her with the election loser, John Gueguierre (34.4%), who recently stated, "For better or worse, District 19 has ended up with an alder with over 50 years of experience in construction and development," One thing for sure, that's better for developers.

Position Against Proposed Stone House Development of the Pierstorff Farm, 6610-6706 Old Sauk Road Mike and Lynn Green 44 Year Residents at 6709 Old Sauk Rd, Opposite the Proposed Development 5 June 2024

We are firmly against this Proposal as it stands. We are not against change, development, some increase in density, residents of any ethnicity/race or economic status, or proper use. This Proposal has major deficiencies that are technical, that include overbearing size, and that are inappropriate in use as described below.

Originally, Stone House Development (SHD) showed an interest in community/neighborhood feedback. That feedback has consistently been negative. As planning and development progressed, mutual interaction with SHD faded and that with City Planning was most disheartening both for this project and, so far, for the evolving West Side Plan. The developer is out to make money while following the City's lead. As to the latter, there is a stark difference between present City policies and those of past administrations regarding the evolution of Madison. Previously, Madison housing had bottom-up, neighborhood/community driven policies; now that is reversed with top-down policy that marginalizes local involvement. Rationale for current policy is overly weighted, to dominated, by a projected massive influx of new residents over the next few decades; that will come at the expense of current residents with differing values, vision, and preferred use. But, this is a topic in its own right that is being developed elsewhere [Ref 1]. The fundamental point is that there should be a mutual discussion of these values, and not a monolog on our part that is unheard by the City, before a massive, and yet another, rental-only apartment complex is built.

Specifics of Opposition – There are many issues of which these are the most significant.

- STORMWATER MITIGATION Homes immediately to the north, and downhill from the proposed development suffered damage from the "1000 year" rainfall in August 2018; and that was from farmland that could absorb water. This situation will likely/possibly get worse either from climate change or that the real Recurrence Interval for similar storms is actually much less than 1000 years. The problem gets even worse when the site becomes 60% impervious because of construction. These north-border residents have vivid recollections of flooding damage, the heightened likelihood of worsened conditions, and thus major concern for the proposed development.
- MASSING LMR land use permits 3 stories and 30 dwelling units per acre (du/ac). This development is 3 stories and 36 du/ac which would require escalation for "special conditions". First, the escalation increases capacity/density by roughly 20%, which is to say, areal coverage by the same amount. But, not allowing that escalation reduces the building footprint which has two beneficial effects. The first effect is to reduce the storm water problem (above) and the second enables further increasing setback(s) for an already offensive structure.
 - The developer shows what are taken to be "comparables" in the area [Ref 2] but does not

- show them juxtaposed with the proposed development. Some of these (not cherry-picked) comparables are shown side-by-side in [Ref 3] with comparison to neighborhood housing and a nearby apartment complex.
- Starting with the comparison most favorable to the developer, the nearby Settlers Woods apartments, one observes a much shorter extent along Old Sauk Road (roughly 100 ft vs 400 ft) and shorter height. But, the most noticeable difference is the setback from the curb: roughly 87 ft vs 37 ft which is to say the "apparent" height of the new development is more than twice that of its nearest "comparable" besides being 4 times longer.
- O Comparison (height and frontal length) of the new development to its surrounding [houses in Ref 3] highlights how <u>incongruous this structure actually is</u>; and in the length comparison bear in mind that the apartment is an unbroken, continuous "wall".
- O The Comprehensive Plan states "... newly developing LMR areas should be seamlessly integrated with surrounding development" with which the Plan Commission is supposed to be consistent. A reasonable comparison of this development to its surroundings shows it is neither seamless or integrated, either in height or frontal extent. This development is literally and figuratively "in your face". On this single, basis alone this proposal should be rejected. Subjectively, it is appalling.
- USE Whereas much is made of the "housing crisis", there is an acknowledged crisis-within-a-crisis in terms of housing alternative to rental, apartment-only construction. This alternative, "Missing Middle" housing offers occupant ownership with several benefits. Renting means landlord control. Rental rate increases are the highest in the country [Ref 4]. Skyrocketing rental rates increase owner profits ... indefinitely. Rentals are already 60% of Madison housing; substantially increasing to more and more apartments from influx exacerbates all of these negatives. It does not appear to be providing, nor is it likely to provide "affordable housing". Non-rental, Missing Middle housing is the needed alternative which must be enabled. Further, and more importantly for the community, ownership provides investment not just financially but also in the neighborhood. Owners are likely to be longer-term residents with families who participate in local, civic activities, send their kids to local schools, and become active and vibrant neighbors that thrive and grow in this housing type. Present understanding is that the Stone House apartment proposal is neither family-oriented nor affordable (especially to families).

City Leveraging – There is another problem at play as well, and that is the City leveraging its position on Old Sauk Road (OSR). This is a two lane road with few crosswalks (three now, it used to be only one at Crestwood School) in the 1.2 mile stretch between Old Middleton Road and Gammon Road. It is a very busy road, with often speeding traffic (passing over the center line or in the parking lane) and scant speed enforcement that, to a resident on OSR, is already at capacity. The SHD proposal will double to triple the number of dwelling units in that stretch of road. Further, the City with its Proactive Zoning philosophy has aspirations to build more higher density units just east of here. All of this is just "piling-on" (leveraging), by the City, to a saturated corridor.

Timing – These comments come ahead of the Plan Commission's Public Review of the SHD Proposal on 10 June. That Review will cover Re-zoning and Conditional Uses but the Staff

Report covering the "specific standards" against which the Proposal will be judged are not available until noon on Friday, 7 June. As a result, comments, above are necessarily incomplete as not only the "specific standards" but the parameters to be judged are not yet spelled out or available. Further, and worst of all, is that there are only a few days over the weekend for citizens to read over the objective details of the Proposal before the Public Review. This simply is grossly unfair to the public reviewers.

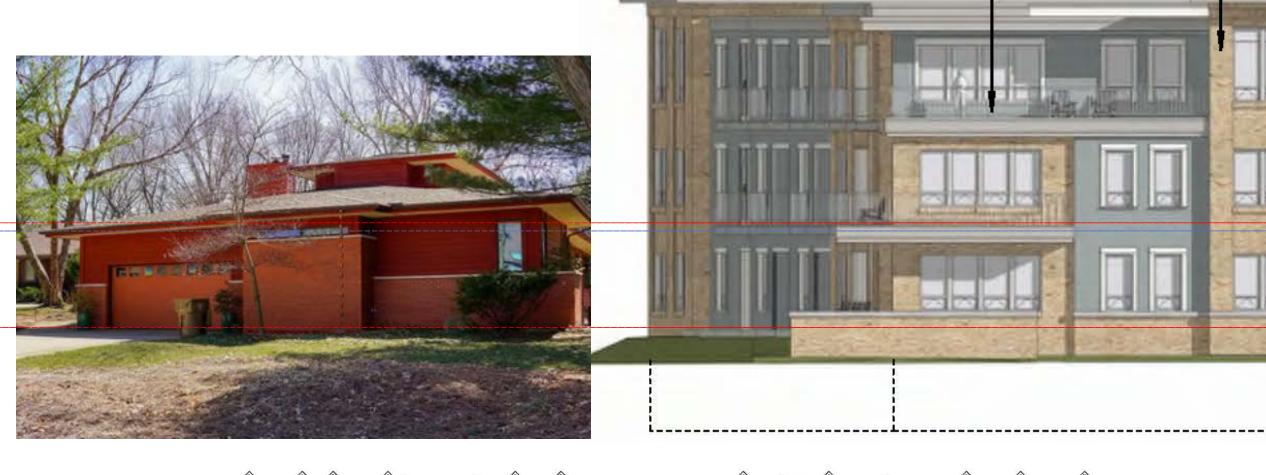
Finally, review, and possible passage of the SHD come at a time when other, relevant and possibly consequential meetings are occurring. One such is the series of the Housing Strategy Subcommittee which, in part, is looking into timely solutions for Missing Middle housing; it is believed that results from that study should be released this summer. Additionally, there is the ongoing and maturing West Area Plan meetings and drafts. The property addressed in the Proposal is in the West Area and would, or should, be subject to its recommendations. Both of these series concern getting-it-right where new development is concerned. The City's development polices should reflect, and give substantial weight to, these ongoing studies in lieu of maximizing apartment construction (present form of densification).

References

- [1a] March 8, 2024 [Fanlund, Cap Times] "City hall is taking aim at Madison homeowners' neighborhoods"
- [1b] March 16, 2024 [Soglin, Cap Times] "Madison zoning plan stinks, and so does its implementation"
- [1c] March 25, 2024 [Fanlund, Cap Times] "Does zoning furor suggest Madison is becoming two cities?"
- [1d] March 29, 2024 [Soglin, Cap Times] "Zoning proposals would erode Madison's sense of place"
- [1e] April 1, 2024 [Fanlund, Cap Times] "Historian Mollenhoff laments power shift to Madison planners"
- [1f] May 24, 2024 [Fanlund, Cap Times] "The common narrative around Madison rezoning is misleading"
- [2] Pg 18, Project Plans, #3, Legistar 82972 Version 1
- [3] See side-by-side comparisons (attached)
- [4] March 28, 2023 [Channel 3] "Madison year-over-year rent increases are the highest in the country, study finds"















0' 8' 16' 32'







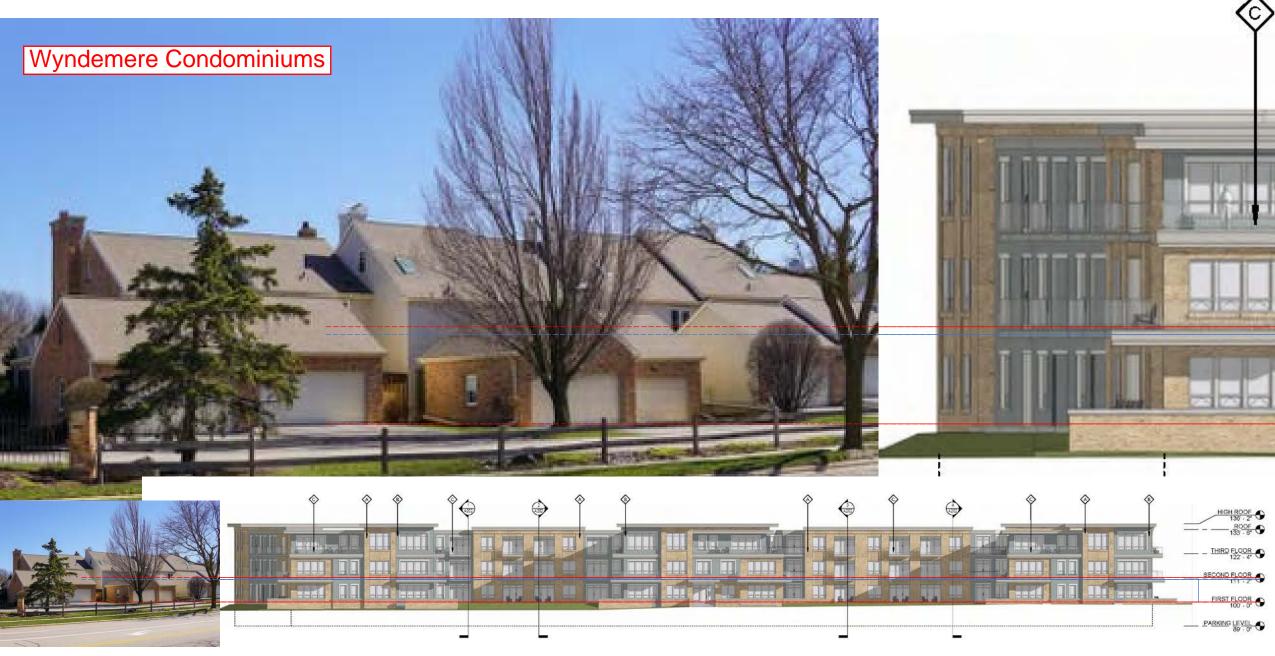
















2 SOUTH ELEVATION
4201 1/16" = 1"-0"
0' 8' 16' 32'

From: Kathleen stark
To: All Alders

Subject: Stone House Development Proposal **Date:** Monday, June 24, 2024 5:11:07 PM

Some people who received this message don't often get email from strk79automatic@gmail.com. <u>Learn why this is important</u>

Caution: This email was sent from an external source. Avoid unknown links and attachments.

Again we emphasize that as citizens of Id Sauk neighborhood we strongly oppose the following agenda items:

- **2950** ... 6610-6706 Old Sauk Road (District 19): Consideration of a demolition permit to demolish two single-family residences and a two-family residence.
- **24) 83477** ... Creating Section 28.022-00672 of the Madison General Ordinances to change the zoning of property located at 6610-6706 Old Sauk Road from SR-C1 (Suburban Residential-Consistent 1) District and SR-C3 (Suburban Residential-Consistent 3) District to TR-U2 (Traditional Residential-Consistent 2) District. (District 19)
- **25) 82972** ... 6610-6706 Old Sauk Road (District 19): Consideration of a conditional use in the [Proposed] Traditional Residential-Urban 2 (TR-U2) District for a multi-family dwelling with greater than 60 units and consideration of a conditional use in the TR-U2 District for outdoor recreation, all to allow construction of a three-story, 138-unit apartment building with an accessory outdoor pool.
- **26) 82979** ... Approving a Certified Survey Map of property owned by Stone House Development, Inc. located at 6610-6706 Old Sauk Road (District 19)

Thomas and Kathleen Stark 809 Sauk Ridge Trail Madison, WI 53717 In Support of the Appeal {Legistar 84123} of the Plan Commission Actions of 10 June 2024 Regarding the Conditional Uses {Legistar 82972} of the Proposed Development at 6610-6706 Old Sauk Rd

Submitted: 2 July 2024 By: Michael A. Green

BACKGROUND

- The (virtual) Plan Commission meeting of 10 June 2024 acted upon these Agenda Items
 - o Item 23, Legistar 82950, Demolition Permit
 - Item 24, Legistar 83477, Rezoning Excessive and proactive; not covered in this Appeal
 - Item 25, Legistar 82972, Conditional Use (CU) Covered in this Appeal
 - O Item 26, Legistar 82979, CSM
- The foundation of this meeting was a 24 page Planning Division Staff Report [Ref 1, Appendix 4] released to the public on 7 June giving residents a long weekend to review its contents.

PURPOSE

• In support of this Appeal we oppose the Plan Commission (PC) approvals of Conditional Use as described in the following.

NATURE OF THE APPEAL

<u>PC Review Submission</u> – Immediately following the PC meeting, on 13 June, a Review of that meeting was submitted that was sharply negative both as to its Process and a general disregard of its own judgement criteria and their interpretation. Unfortunately, without reference to a Legistar number, those comments cannot be found on the Internet or referenced to that meeting. This appears as Reference 2, Appendix 1.

This reference concludes that close partnership existed between the City and the developer over a long period of time; and the outcome was basically predetermined. The public was allowed to speak on a metered basis, unless there were followup questions ... of which there were none. By contrast, questions were asked, in favor of the developer, which deferentially extended their speaking time. This pattern is directly mirrored, and evidenced by unanimous (the default!!!) passage of all Agenda Items.

It should be noted that for District 19, across all agenda items, there were 420 opposing votes and 30 supporting votes.

• Specifics of the Appeal – The Staff Report, and its careless, no-resistance acceptance by the PC, fed forward to comments [Ref 3, Appendix 2] presented to the Common Council (CC) meeting of 18 June [Ref 4]. Legistar 82972 (Conditional Use), critical to development approval, and the most fraught with problems, was not on the Agenda, and it therefore contributed considerably to that presentation.

The Staff Report had three Judgement Criteria:

- 1. "Additionally, state law requires that conditional use findings must be based on 'substantial evidence' that directly pertains to each standard and not based on personal preference or speculation.
- 2. CU Standard #8 "... the Plan Commission shall find that the project creates an environment of sustained aesthetic desirability compatible with the existing or intended character of the area ..."
- 3. "The City Plan Commission shall not approve a conditional use without due consideration of the recommendations in the City of Madison Comprehensive Plan ..." which reads "... newly developing LMR areas should be seamlessly integrated with surrounding development".

These are the Conditional Uses, from the Staff Report, with comments:

- 1. The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, or general welfare.
 - Specific to this appeal is that there is definite, quantifiable, demonstrably tangible risk of flooding to neighbors immediately to the north of the proposed development. This has been the subject of considerable documentation (cf. Ref 5) and needs no repetition here. While acknowledging these concerns the Report then somewhat glibly says "... staff does not believe that the information provided in these comments suggest that this standard cannot be met." This is not the same as saying, categorically, that they can be met. Thus, and using the Staff's word "believe" it can, at best, be said CU #1 is incomplete and unresolved.
- 2. The City is able to provide municipal services to the property where the conditional use is proposed, given due consideration of the cost of providing those services.
 - The Report says "... nothing out of the ordinary in providing municipal services to this property because of the proposed development". Given the exposure that the City has incurred from CU #1 [Ref 6], it is not at all obvious if, in the present case (stormwater), this statement is true or not; it defaults, too, to incomplete and unresolved.
- 3. The uses, values and enjoyment of other property in the neighborhood for purposes already established will not be substantially impaired or diminished in any foreseeable manner.
 - As the Report says: "This is often the most difficult standard ... to address". It involves input (which has been near-unanimously negative) from neighborhood residents or property owners; it also acknowledges that "The proposed building has elicited a significant amount of correspondence from nearby residents ..." There is no known, first-hand knowledge of (monetary) value ever coming up in local discussions. However, "enjoyment" is subjective (for either residents or the City). To residents: noise, lighting, added traffic, on-street parking, and the profound loss of green space will all impair and diminish enjoyment ... no question. So, did the PC approve this based on "substantially", or not? How does one satisfy the "substantial evidence" Criterion given the criterion for "substantial evidence"? The "substantial evidence" favors residents in this case.

4. The establishment of the conditional use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.

The Report does say: "... the proposed building represents a significantly different building form compared to what currently exists on the subject site and on surrounding properties ..." Although this thought carries forward, there is no other particular comment.

5. Adequate utilities, access roads, drainage, internal circulation improvements for pedestrians, bicyclists, public transit and vehicles, parking supply (in cases with minimum parking requirements) and other necessary site improvements have been or are being provided.

Regarding drainage (see CU #1 comments, above), this issue is still incomplete and unresolved. As to parking, it is very hard to believe that this development will not result in on street parking. This could easily be by residents not wanting to pay monthly underground parking fees or wanting to beat rush hour egress or ingress (on a single, internal driveway).

6. The conditional use conforms to all applicable regulations of the district in which it is located.

No comment.

- 7. Does not apply.
- 8. When applying the above standards to any new construction of a building or an addition to an existing building the Plan Commission shall find that the project creates an environment of sustained aesthetic desirability compatible with the existing or intended character of the area and the statement of purpose for the zoning district. In order to find that this standard is met, the Plan Commission may require the applicant to submit plans to the Urban Design Commission for comment and recommendation.

This is far harder than CU #3 to be justified in compliance by the PC; notably, they made no mention of this in their Minutes. Here, Judgement Criteria #2 & #3 pertain, which use phrasings "creates an environment of sustained aesthetic desirability compatible with the existing or intended character of the area ...", and "seamlessly integrated with surrounding development".

First, note the Reports' comment on "significantly different building form" under CU #4, above. Then, referring to the Table at the end of Ref. 2, there is a comparison with what should be the developer's best case comparable, viz. the adjacent Settlers Woods apartments. That Table summarizes the comparison of properly-scaled vertical height (top panel) and horizontal length (bottom panel); also included is curb setback from Old Sauk Road based on data from the developer and Google Earth. This is the comparison:

- Settlers Woods is somewhat shorter in height 2 floors with a standard, pitched roof falling away from the viewer.
- Curb setback
 - Settlers Wood 84 feet
 - New Development 35 feet

- Apparent (angular, above horizon) Height Owing to shorter height and greater setback, Settlers Woods is 2-3 times shorter than the proposed development
- Length
 - Settlers Woods 100 feet
 - New Development 400 feet
- Dwelling Units per Acre
 - Settlers Woods 14.4 (derived from City records)
 - New Development 36.6 (from developer)

Bearing in mind that the surrounding neighborhood is R1, the comparisons (see figures in Ref 7) weigh even more unfavorably against the new development. Again, does the PC approve this based on "aesthetic desirability" and "seamlessly integrated with surrounding development", or not? How does one satisfy the "substantial evidence" Criterion? Put this to a neighborhood poll (as opposed to the top-down model of governance): Would any reasonable, unbiased poll sustain the City's viewpoint or that of the neighboring community? Objectively, numerical comparisons are quite against this development; and, subjectively, a neighborhood poll would come to the same conclusion.

9. - 16. Do not apply.

• Conclusion – Conditional Uses #1, #2, and #5 are incomplete and unresolved with a default of not satisfied. CU #3 is not satisfied. CUs #4 & #6 have no comment. CUs #7 and #9-16 do not apply. Conditional Use #8 is demonstrably non-compliant with any reasonable interpretation of the Judgement Criteria.

These insufficiencies are the basis for our Appeal. It should be emphasized that fundamental problems in the Staff Report were summarily, and unanimously rubber-stamped by the Plan Commission, oblivious to residential input, thus approving Conditional Uses which do not then come before the Common Council per se; that is, everything downstream from the Staff Report is similarly flawed. Adding grievous insult-to-injury the Common Council then proactively, and excessively upzoned the properties, a practice which the City should know is not well received by neighborhoods as it leverages its position into the future.

References:

- 1) Planning Division Staff Report; see Appendix 4
- 2) Personal critique of the 10 June 2024 PC meeting; see Appendix 1
- 3) Personal CC presentation outline form; see Appendix 2
- 4) Agenda Items 13 (Legistar 83477, Rezoning) and 49 (Legistar 82979, CSM)
- 5) Under <u>Legistar 82972</u> See discussions in "<u>Public Comments June 2024.pdf</u>", pgs 36-37, 144-147, and 200-203
- 6) See, for example, greyed, Major/Non-Standard Conditions on pgs 15 & 16 of the Planning Division Staff Report; Appendix 4
- 7) Opposition Stone House Dev 6610-6706 Old Sauk Rd (Mike & Lynn Green).pdf; Appendix 3

Appendix 1

the-greens31@charter.net

From: the-greens31@charter.net <the-greens31@charter.net>

Sent: Thursday, June 13, 2024 1:05 PM

 $'npollack@madison.com' < \underline{npollack@madison.com} >; 'pfanlund@captimes.com' < \underline{pfanlund@captimes.com} >; 'mtreinen@captimes.com' < \underline{npollack@madison.com} >; 'mtreinen@captimes.com' < \underline{np$

<<u>mtreinen@captimes.com</u>>; 'faye.parks@wortfm.org' <<u>faye.parks@wortfm.org</u>>

Cc: 'pccomments@cityofmadison.com' < pccomments@cityofmadison.com > Subject: Comments on 10 June Plan Comm Mtg OR Medican's Future

Subject: Comments on 10 June Plan Comm Mtg OR Madison's Future

Good afternoon

Attached is a review of the process at the Plan Commission meeting at 5:30 pm on Monday 10 June that considered the proposed development at 6610-6706 Old Sauk Rd.

This is a synthesis of how our family members perceived that meeting. Nevertheless, it likely approximates what others in our opposition would say as well. It is meant to give feedback that will hopefully improve the process.

I wish the circumstances were otherwise and this review were very different; at the heart of this matter is politics and top-down governance versus bottom-up policy that begins with neighborhood communities.

In the future, I look forward to seeing this reversal, possibly with different leadership. It's one thing to chronicle talking to residents, to show "citizen involvement", but it is entirely different if that box is checked and the input ignored.

There also needs to be a truly long-term discussion about what makes Madison what it is, what it takes to preserve that "charm", and set realistic limitations on what we can and want to achieve without eventually diminishing what we love; this needs to be written into the Area Plans and the Comprehensive Plan. Then, the current, short-term impetus to densify needs be consistent with some notion of boundaries written into those Plans.

There is a larger picture here.

Thank you, Michael A. Green 6709 Old Sauk Rd.

Madison

1

Re: Virtual Public Hearing, Plan Commission Meeting, 5:30 pm on 10 June 2024
Agenda Items #23, 24, 25, 26
Concerning the Proposed Development at 6610-6706 Old Sauk Rd

A Citizen's Experience

Disclaimer: Our position was in opposition to the proposed development. On all points, we lost.

Purpose, to Describe: First, how the process was conducted, or "how the game was played".

Second, there are arbitrary, imprecise, qualitative judgement "features" in the process that can, depending on how they are selected, or spun, toggle the outcome to the one desired.

Impression: It was a thoroughly miserable experience in local civics and citizen involvement in the discussion of this proposal and of Madison's housing crisis. In a top-down policy environment the neighborhood is marginalized, rather than coming first.

To Begin:

- 1. Accessibility
 - a. There were Zoom login problems because of incorrect instructions to viewers or IT setup. I had to intervene, when another speaker was called, to bring this up as the problem became known from neighbors; this got a nod of recognition, but no redress or apology from the Plan Commission (PC). Had the public's input been valued, this could have been remedied *before* continuation; that was not the case.
 - b. The answer to any of the following complaints is/was/or_will_be that this is not how PC meetings are run; this is not helpful if this is your first, virtual PC meeting.
 - i. Never have I seen a Zoom setup like this ... and I've been to too many virtual meetings:
 - (1) No chat function (useful to ask moderator a question offline)
 - (2) No video function (my presentation relied on being able to hold up an exhibit and I was unable to do so); a possible counter argument was that my graphic could have been sent to the PC ahead of meeting. I didn't do this since I was unfamiliar with their procedures and setup and I wanted to be able to rehearse and time optimize my presentation
 - (3) No participants icon that would/could have indicated login problems
 - (4) Screen so sparse of detail one couldn't even tell if logged in (to speak) or not
 - ii. With no "time expires in xxx seconds" messaging to speakers, speakers were just cutoff even in mid-sentence.

2. Communication

- a. There was an unmistakable, deferential camaraderie between the PC and the developer. The public are definitely on the "outside looking in" of that relationship.
- b. Public input was metered, but not uniformly. Presentation cutoff times varied from 3-3.5 minutes.
- c. By contrast, the developer could be, and was, granted more, unlimited time by virtue of

- simply being asked a leading, soft-ball question by a PC member. There were no instances of the public or its experts being asked any questions or in any way being engaged in dialog. The tone of the meeting was clearly in favor of the developer.
- d. The PC only had discussion about approval, justification, and praise for developer.
- e. The PC's appreciation to public attendees that "your voice was heard" and "we know how difficult this process can be", etc. came across as hollow and disingenuous on the verge of becoming insulting.
- f. Finally, all four Items were summarily passed in oblivious disregard for the public's input.

3. PC Decision Making

- a. The outcome was clearly pre-ordained and never in doubt; the default on every motion was always "unanimous consent assumed unless a hand is raised" (by a PC member) ... there was never any discussion, or raised hands: every motion was systematically unanimous. The cruxes of public feedback summarily vanished.
- b. In this case, at least, the developer worked with the PC for months to reach a mutually desirable outcome; a Staff Report from the Plan Division had gone to the PC a few days before the Meeting where it is given a "public hearing". Since the public's voice is not listened to, absorbed, thought about, questioned, and/or assimilated, "public hearing" essentially means the public gets to hear, but not interfere with, the agreed-to plan.
- 4. The Staff Report shown in blue are examples of critical measures spun for a desired result
 - a. Pg 12 is a mess Under Recommendations the 1st & 3rd bullets are from another development. Was ignoring these obvious, major written gaffes called for, or was there important text that should have been there and made available to the public?
 - b. There are qualitative, subjective, and tentative wordings such as "could find", "believes", "feels" in critical instances in the absence of more quantitative, objective, and certain measures. This was invariably replaced by either language that discounted negative assertions or resulted in recommended "fact".
 - c. The development's frontal view is nowhere close to the Comprehensive Plan's wording "... newly developing LMR areas should be seamlessly integrated with surrounding development". This has previously been included in written, public comments including graphics; this would have been shown except that the PC does not allow video. But no matter: the developer justified (to itself and the PC) the proposed building's height and massing by comparison to another, higher complex over a mile away that happens to be in a more appropriate zoning setting and has BRT. The best comparable is the apartments immediately to the east of the proposed development; had they been used for comparison it is immediately apparent that it is vastly exceeded by the proposed building. See attachment.
 - d. Rebuttal of storm water issues was incomplete at best; possibly incorrect at worst. New concepts became apparent in the Staff Report that suggest a storm water easement, and which the City will acquire if the developer can't. This is a new chapter in the discussion.
 - e. Select conditions These were spun to justify additional upzoning (in this case du/ac) but also setting future precedent for much greater scale and density (a process the City terms proactive rezoning) elsewhere. Staff and PC stated that arterial status and bus availability were "... most significant factors as to why the proposed development may be approved." despite the complex not meeting at least 3 other, more significant factors. This is arbitrary cherry-picking favorable to a desired outcome.

Here is a table of those factors, with various points of view; red is negative, green is positive, and brown is in between.

		PC	
Factor	Opposition's Position	Position	Resolve
Relationships between proposed buildings and their surroundings	Totally Negative Simply not consistent with Comprehensive Plan wording	Negative: Staff acknowledges that the scale and mass of the proposed building will be unlike any other residential building in the surrounding area. But then side with the developer's efforts	Pass
Amenities	Negative: only has meaning if within walking distance	Other than onsite – Unsubstantiated	Pass
Urban Service	None (other than bus which is double counting)	Other than bus (already included) – unsubstantiated	Pass
Arterial Street	Negative: 2-lane, at capacity; don't use to leverage more usage; overflow parking problem especially in winter; OSR is a minor arterial road Say fulfilled		Pass
Transit	Bus line; leveraged by "arterial" street designation; little usage at present	Overplay	Pass
Natural features	Arguable since not defined. There are trees, wildlife, and good soil. There is a historic barn, likely the last in Madison and one of the few in Dane County	Say fulfilled	Pass
Park	Say fulfilled	Say fulfilled	Pass

In conclusion: This process cannot be distinguished from being political with enough arbitrariness to produce a desired outcome. Is there any monitoring, oversight, check or balance of the Plan Commission procedures? How is meaningful, bottom-up neighborhood feedback and dialog restored to this process?



Side-by-Side Comparison: Top – illustrates height; Bottom – best illustrates frontal length and overall comparison.

BEST, IMMEDIATELY NEIGHBORING, COMPARABLE COMPARISON

Parameter	Proposed Building	Settlers Woods
Frontal Length	400 ft	100 ft
Setback from Curb	35	84
Height	More	Less
Ratio, Apparent (Angular) Height from Curb	2-3 to 1	
Dwelling Units / Acre	36.6	14.4

Appendix 2 Public Hearing, CC 18 June

These remarks oppose the proposed development of the Pierstorff century farm.

Last week the Plan Commission acted on a Staff Report, and proposed demolition, conditional use, rezoning, and CSM. The last two appear as items #13 & #49 on tonight's Agenda.

Last Friday I submitted a critique of that meeting's process, parts of which are addressed here.

- We oppose 3 aspects, in particular
 - 1st Stormwater concerns from vastly increased impervious land coverage and likely climate change ... this is addressed separately;
 - 2nd Overbearing massing
 - 3rd Proliferation of rental-only apartments that rule out owner-occupied missing middle housing.
- Some Specifics of this Process
 - Presentation of storm water issues was incomplete at best; possibly incorrect at worst.
 - As to Massing
 - Judgement Criteria
 - "... findings must be based on substantial evidence (Staff Report)
 - Applicable Conditional Use Standard #8 reads "... Plan Commission shall find that the project creates an environment of sustained aesthetic desirability compatible with the existing or intended character of the area ..."

- "... conditional use shall ... consider ... the recommendations in the ... Comprehensive Plan ... which reads "... newly developing LMR areas should be seamlessly integrated with surrounding development".
- The Plan Commission sustained development
 - Citing "significant setbacks" (from the Plan Commission Minutes)
 - "Despite the proposed building being notably larger than those in the surrounding area, staff *feels* that the building ... etc. can create an environment of sustained aesthetic desirability." (from the Staff Report)
 - Notably, no mention made of Standard #8
- Developer's comparable was over a mile away instead of the adjacent Settlers Woods apartments.
- Picture + other comparisons
- The development's street view is nowhere close to words like aesthetics, seamless, or integrated.
- As to zoning & land use there are the Select Conditions Despite all conditions not being met, including 3 of greater significance, the findings were
 - 1st ... arterial status and bus availability " are the most significant factors as to why the proposed development may be approved."
 - 2nd Spun to justify and leverage additional upzoning and setting precedent for much greater scale and density elsewhere in the future.

In conclusion: This process cannot be distinguished from being political with enough arbitrariness to produce a desired outcome. Please consider how meaningful, bottom-up neighborhood feedback and dialog can be restored to this process.

- There are qualitative, subjective, and tentative wordings such as "could find", "believes", "feels" in critical instances in the absence of more quantitative, objective, and certain measures. Such passages invariably ended in discounting opposing assertions or resulted in recommended "fact".
- Staff Report, Pg 12 "a mess" if it had mattered, it would never have been released as written
- My professional career involved definitive measures, numerical and graphic analysis, and weighted factors in multi-factor requirements. Not nearly so in the case of the Staff Report and its judgement which involved subjective findings and cherry-picked selection criteria. This was a pre-ordained, political exercise.
- There was an unmistakable, deferential camaraderie between the PC and the developer. The public are definitely on the "outside looking in" of that relationship.
- In this case, at least, the developer worked with the PC for months to reach a mutually desirable outcome; a Staff Report from the Plan Division had gone to the PC a few days before the Meeting where it is given a "public hearing". Since the public's voice is not listened to, absorbed, thought about, questioned, and/or assimilated, "public hearing" essentially means the public gets to hear, but not interfere with, the agreed-to plan.
- With 12 members on that Commission, what's striking is that without meaningful discussion all votes are presumed unanimous (by default) ... unless someone raises a hand. Incomplete idea of members present. Unanimity strongly suggests prior discussion and approval; i.e. the outcome is pre-approved outside of and before the "Public Hearing".
- The outcome was clearly pre-ordained and never in doubt; the default on every motion was always "unanimous consent assumed unless a hand is raised" (by a PC member) ... there was never any discussion, or raised hands: every motion was systematically unanimous. The cruxes of public feedback summarily vanished.
- All four Items were summarily passed in oblivious disregard for the public's input.
- This political process features strictly top-down determination with window-dressing community input on consequential issues.
- Public input metered to 3 minutes. By contrast, the developer could be, and was, granted more, unlimited time by virtue of simply being asked a leading, soft-ball question by a PC member. There were no instances of the public or its experts being asked any questions or in any way being engaged in dialog. The tone of the meeting was clearly in favor of the developer. The PC only had discussion about approval, justification, and praise for developer.

Zoom

- Login accessibility: links caused problems.
- No participants icon that would have indicated problems
- No chat; no video (couldn't show graphic)
- Screen so sparse with information couldn't readily tell if logged in
- Last minute release of the Staff Report

Appendix 3

Position Against
Proposed Stone House Development of the
Pierstorff Farm, 6610-6706 Old Sauk Road
Mike and Lynn Green

44 Year Residents at 6709 Old Sauk Rd, Opposite the Proposed Development
5 June 2024

We are firmly against this Proposal as it stands. We are not against change, development, some increase in density, residents of any ethnicity/race or economic status, or proper use. This Proposal has major deficiencies that are technical, that include overbearing size, and that are inappropriate in use as described below.

Originally, Stone House Development (SHD) showed an interest in community/neighborhood feedback. That feedback has consistently been negative. As planning and development progressed, mutual interaction with SHD faded and that with City Planning was most disheartening both for this project and, so far, for the evolving West Side Plan. The developer is out to make money while following the City's lead. As to the latter, there is a stark difference between present City policies and those of past administrations regarding the evolution of Madison. Previously, Madison housing had bottom-up, neighborhood/community driven policies; now that is reversed with top-down policy that marginalizes local involvement. Rationale for current policy is overly weighted, to dominated, by a projected massive influx of new residents over the next few decades; that will come at the expense of current residents with differing values, vision, and preferred use. But, this is a topic in its own right that is being developed elsewhere [Ref 1]. The fundamental point is that there should be a mutual discussion of these values, and not a monolog on our part that is unheard by the City, before a massive, and yet another, rental-only apartment complex is built.

Specifics of Opposition – There are many issues of which these are the most significant.

- STORMWATER MITIGATION Homes immediately to the north, and downhill from the proposed development suffered damage from the "1000 year" rainfall in August 2018; and that was from farmland that could absorb water. This situation will likely/possibly get worse either from climate change or that the real Recurrence Interval for similar storms is actually much less than 1000 years. The problem gets even worse when the site becomes 60% impervious because of construction. These north-border residents have vivid recollections of flooding damage, the heightened likelihood of worsened conditions, and thus major concern for the proposed development.
- MASSING LMR land use permits 3 stories and 30 dwelling units per acre (du/ac). This development is 3 stories and 36 du/ac which would require escalation for "special conditions". First, the escalation increases capacity/density by roughly 20%, which is to say, areal coverage by the same amount. But, not allowing that escalation reduces the building footprint which has two beneficial effects. The first effect is to reduce the storm water problem (above) and the second enables further increasing setback(s) for an already offensive structure
 - The developer shows what are taken to be "comparables" in the area [Ref 2] but does not

- show them juxtaposed with the proposed development. Some of these (not cherry-picked) comparables are shown side-by-side in [Ref 3] with comparison to neighborhood housing and a nearby apartment complex.
- Starting with the comparison most favorable to the developer, the nearby Settlers Woods apartments, one observes a much shorter extent along Old Sauk Road (roughly 100 ft vs 400 ft) and shorter height. But, the most noticeable difference is the setback from the curb: roughly 87 ft vs 37 ft which is to say the "apparent" height of the new development is more than twice that of its nearest "comparable" besides being 4 times longer.
- O Comparison (height and frontal length) of the new development to its surrounding [houses in Ref 3] highlights how <u>incongruous this structure actually is</u>; and in the length comparison bear in mind that the apartment is an unbroken, continuous "wall".
- O The Comprehensive Plan states "... newly developing LMR areas should be seamlessly integrated with surrounding development" with which the Plan Commission is supposed to be consistent. A reasonable comparison of this development to its surroundings shows it is neither seamless or integrated, either in height or frontal extent. This development is literally and figuratively "in your face". On this single, basis alone this proposal should be rejected. Subjectively, it is appalling.
- USE Whereas much is made of the "housing crisis", there is an acknowledged crisis-within-a-crisis in terms of housing alternative to rental, apartment-only construction. This alternative, "Missing Middle" housing offers occupant ownership with several benefits. Renting means landlord control. Rental rate increases are the highest in the country [Ref 4]. Skyrocketing rental rates increase owner profits ... indefinitely. Rentals are already 60% of Madison housing; substantially increasing to more and more apartments from influx exacerbates all of these negatives. It does not appear to be providing, nor is it likely to provide "affordable housing". Non-rental, Missing Middle housing is the needed alternative which must be enabled. Further, and more importantly for the community, ownership provides investment not just financially but also in the neighborhood. Owners are likely to be longer-term residents with families who participate in local, civic activities, send their kids to local schools, and become active and vibrant neighbors that thrive and grow in this housing type. Present understanding is that the Stone House apartment proposal is neither family-oriented nor affordable (especially to families).

City Leveraging – There is another problem at play as well, and that is the City leveraging its position on Old Sauk Road (OSR). This is a two lane road with few crosswalks (three now, it used to be only one at Crestwood School) in the 1.2 mile stretch between Old Middleton Road and Gammon Road. It is a very busy road, with often speeding traffic (passing over the center line or in the parking lane) and scant speed enforcement that, to a resident on OSR, is already at capacity. The SHD proposal will double to triple the number of dwelling units in that stretch of road. Further, the City with its Proactive Zoning philosophy has aspirations to build more higher density units just east of here. All of this is just "piling-on" (leveraging), by the City, to a saturated corridor.

Timing – These comments come ahead of the Plan Commission's Public Review of the SHD Proposal on 10 June. That Review will cover Re-zoning and Conditional Uses but the Staff

Report covering the "specific standards" against which the Proposal will be judged are not available until noon on Friday, 7 June. As a result, comments, above are necessarily incomplete as not only the "specific standards" but the parameters to be judged are not yet spelled out or available. Further, and worst of all, is that there are only a few days over the weekend for citizens to read over the objective details of the Proposal before the Public Review. This simply is grossly unfair to the public reviewers.

Finally, review, and possible passage of the SHD come at a time when other, relevant and possibly consequential meetings are occurring. One such is the series of the Housing Strategy Subcommittee which, in part, is looking into timely solutions for Missing Middle housing; it is believed that results from that study should be released this summer. Additionally, there is the ongoing and maturing West Area Plan meetings and drafts. The property addressed in the Proposal is in the West Area and would, or should, be subject to its recommendations. Both of these series concern getting-it-right where new development is concerned. The City's development polices should reflect, and give substantial weight to, these ongoing studies in lieu of maximizing apartment construction (present form of densification).

References

- [1a] March 8, 2024 [Fanlund, Cap Times] "City hall is taking aim at Madison homeowners' neighborhoods"
- [1b] March 16, 2024 [Soglin, Cap Times] "Madison zoning plan stinks, and so does its implementation"
- [1c] March 25, 2024 [Fanlund, Cap Times] "Does zoning furor suggest Madison is becoming two cities?"
- [1d] March 29, 2024 [Soglin, Cap Times] "Zoning proposals would erode Madison's sense of place"
- [1e] April 1, 2024 [Fanlund, Cap Times] "Historian Mollenhoff laments power shift to Madison planners"
- [1f] May 24, 2024 [Fanlund, Cap Times] "The common narrative around Madison rezoning is misleading"
- [2] Pg 18, Project Plans, #3, Legistar 82972 Version 1
- [3] See side-by-side comparisons (attached)
- [4] March 28, 2023 [Channel 3] "Madison year-over-year rent increases are the highest in the country, study finds"

















0' 8' 16' 32'















0' 8' 16' 32'









2 SOUTH ELEVATION 1/16" = 1'.0"

0' 8' 16' 32'

Appendix 4

PLANNING DIVISION STAFF REPORT

June 10, 2024

Project Addresses: 6610-6706 Old Sauk Road

Application Type: Demolition Permit, Zoning Map Amendment, Conditional Uses, and

Certified Survey Map Referral

Legistar File ID # 82950, 83477, 82972, and 82979

Prepared By: Timothy M. Parks, Planning Division

Report includes comments from other City agencies, as noted

Reviewed By: Kevin Firchow, Planning Division

Bill Fruhling, Interim Planning Division Director

Summary

Applicant: Helen H. Bradbury, Stone House Development; 1010 E Washington Avenue, Suite 101; Madison.

Property Owner: Robert Pierstorff; 6610 Old Sauk Road; Madison.

Surveyor: Zach Reynolds, Wyser Engineering, LLC; 300 E Front Street; Mount Horeb.

Requested Actions:

ID <u>82950</u> – Consideration of a demolition permit for 6610-6706 Old Sauk Road to demolish two single-family residences and a two-family residence;

- ID <u>83477</u> Consideration of a request to rezone 6610-6706 Old Sauk Road from SR-C1 (Suburban Residential–Consistent 1 District) and SR-C3 (Suburban Residential–Consistent 3 District) to TR-U2 (Traditional Residential–Urban 2 District);
- ID <u>82972</u> Consideration of a conditional use in the [Proposed] TR-U2 (Traditional Residential-Urban 2 District) for a multi-family dwelling with greater than 60 units and consideration of a conditional use in the TR-U2 District for outdoor recreation, all to allow construction of a three-story, 138-unit apartment building with an accessory outdoor pool; and
- ID <u>82979</u> Approval of a Certified Survey Map (CSM) to create one lot for the proposed residential development.

Proposal Summary: The applicant is seeking approvals to redevelop two parcels located at 6610 and 6706 Old Sauk Road with a three-story, 138-unit apartment building with an outdoor pool following the demolition of a single-family residence at 6610 Old Sauk Road, a two-family residence at 6612-6614 Old Sauk, and a single-family residence at 6706 Old Sauk. The proposed apartment building will include parking for 143 automobiles underground and in 25 outdoor stalls, and a total of 154 bike parking stalls. The two parcels will be combined into one lot by CSM. The letter of intent indicates that construction will commence as soon as all regulatory approvals, with completion anticipated in September 2025.

Applicable Regulations & Standards: Section 28.182 of the Zoning Code provides the process for zoning map amendments. Table C-1 in Section 28.032(1) identifies a multi-family dwelling with greater than 60 units and outdoor recreation as conditional uses in the proposed TR-U2 (Traditional Residential—Urban 2) zoning district. Section 28.183 provides the process and standards for the approval of conditional use permits. Section 28.185 provides the process and standards for the approval of demolition and removal permits. The subdivision process is outlined in Section 16.23(4)(f) of the Subdivision Regulations.



Review Required By: Plan Commission and Common Council.

Summary Recommendation: if the Plan Commission can find the applicable standards are met, the Planning Division recommends the following actions to the Plan Commission:

- That the Plan Commission find that the standards for demolition permits are met to **approve** demolition of the three residences located at 6610-6706 Old Sauk Road;
- That the Plan Commission forward Zoning Map Amendment ID 28.022–00672, rezoning 6610-6706 Old Sauk Road from SR-C1 and SR-C3 to TR-U2, to the Common Council with a recommendation of **approval**;
- That the Plan Commission find the standards for conditional uses are met to approve a three-story, 138unit apartment building and pool, subject to input at the public hearing and the conditions from reviewing agencies beginning on page 12; and
- That the Plan Commission forward the Certified Survey Map to combine 6610-6706 Old Sauk Road into
 one lot to the Common Council with a recommendation of approval subject to the conditions from
 reviewing agencies beginning on page 20.

Background Information

Parcel Location: Two parcels totaling 3.77 acres located on the north side of Old Sauk Road opposite San Juan Trail; Alder District 19 (Guequierre); Madison Metropolitan School District.

Existing Conditions and Land Use:

- 6610 Old Sauk Road is developed with a single-family residence (6610) and two-family residence (6612-6614), zoned SR-C3 (Suburban Residential–Consistent 3 District);
- 6706 Old Sauk Road is developed with a single-family residence and accessory barn, zoned SR-C1 (Suburban Residential–Consistent 1 District).

Surrounding Land Uses and Zoning: The subject site is bordered on the north, south, and west by single-family residences in SR-C1 (Suburban Residential—Consistent 1 District) zoning. On the east, the site adjoins Saukborough Square, a four-building complex of eight-unit multi-family buildings, and Settlers Woods Condominiums, a complex of single-family residences and a two-family residence; both complexes are zoned PD (Planned Development District).

Adopted Land Use Plan: The 2023 <u>Comprehensive Plan</u> identifies the subject site and parcels to the east for Low-Medium Residential (LMR). The single-family residences otherwise surrounding the site are recommended for Low Residential (LR).

Zoning Summary: The subject site will be zoned TR-U2 (Traditional Residential–Urban 2 District), which will be reviewed in the following sections.

Requirements	Required	Proposed	
Lot Area	350 sq. ft. (48,300 sq. ft.)	161,024 sq. ft.	
Lot Width	50′	553′	
Minimum Front Yard Setback	15′	15/ (Ones nevel es 11/)	
Maximum Front Yard Setback	30′	15' (Open porches: 11')	

Requirements		Required	Proposed				
Side Yard Setback		10'	66' (East) 54' (West)				
Rear Yard		20'	62'				
Maximum Lot Coverage		80%	56%				
Usable Open Space		40 sq. ft./unit (5,520 sq. ft.)	11,000 sq. ft				
Maximum Building Height		6 stories/ 78'	3 stories/ 36.2'				
Auto Parking		1 per dwelling unit (138 total)	143 enclosed/ garage; 25 surface (168 total)				
Electric Vehicle (EV) Stalls		EV Ready: 10%; 14 stalls	19 EV Ready				
	Accessible Stalls	7	7				
Bike Parking		1 per unit up to 2-bedrooms, half- space per add. bedroom (140); 1 guest space per 10 units (14) (154 total)	154				
Loading		None	0				
	Building Forms	Large Multi-Family Building	Will comply (See Zoning Conditions)				
Other	Critical Zoning Items						
Yes:	Utility Easements						
No:	Barrier Free, Urban Design, Transit-Oriented Development Overlay, Wellhead Protection, Waterfront Development, Wetlands, Floodplain, Adjacent to Park, Landmarks						
Prepared by: Jacob Moskowitz, Assistant Zoning Administrator							

Environmental Corridor Status: The subject site is not located in a mapped environmental corridor.

Public Utilities and Services: The site is currently served by a full range of urban services, including Metro Transit, which operates seven-day service with trips at least every 30 minutes along Old Sauk Road (Route R). Metro Transit would initially estimate the following counts of potentially eligible trips towards US Green Building Council/LEED Quality Access to Transit points: 37 weekday and 33 weekend. Please contact Metro Transit if additional analysis would be of interest.

Project Description

The applicant is requesting approval of a demolition permit to demolish three residences located on two parcels at 6610 and 6706 Old Sauk Road and to rezone the parcels from SR-C3 and SR-C1, respectively, to TR-U2 to facilitate redevelopment of the property with a three-story, 138-unit apartment building with outdoor pool. Additionally, a Certified Survey Map (CSM) is proposed to combine the underlying parcels into one lot.

From east to west, the buildings to be demolished are:

• A one-story single-family residence addressed as 6610 Old Sauk Road on the eastern half of the 37,948.2 square-foot (0.87-acre) (per City records) parcel of the same address. The ranch-style single-family residence was constructed in 1956 per City records and contains three bedrooms, one bathroom, and a two-stall attached garage, with a carport adjacent to the eastern wall. The residence is set back approximately 115 feet from the southern property line at Old Sauk Road.

- A split level two-family residence addressed as 6612-6614 Old Sauk, which occupies the western half of the 6610 Old Sauk parcel. The ranch duplex was constructed in 1970 and contains four bedrooms, two bathrooms, and dual one-car basement garages, and is set back 55 feet from the southern property line.
- A one-story single-family residence located in the northwestern corner of the 2.9-acre parcel at 6706 Old Sauk Road. According to City records, the ranch-style residence was built in 1970 and contains three bedrooms, two bathrooms, and a two-car attached garage. The demolition plan also notes a 220 square-foot detached garage located next to the house along the northern property line, and a two-story stone and masonry barn and two silos located southeast of the residence. The residence is located approximately 275 feet from the southern property line and 15 feet at its closest point from the northern property line, while the barn is set back 215 feet and 50.7 feet from those respective property lines.

Photos of the interior and exteriors of the three principal buildings and the accessory barn are included in the materials submitted for the demolition permit. Additionally, a demolition plan is included in the application materials, which highlights the salient features of the 3.77-acre site. Generally, the two single-family residences are located on the high points of the site, with the grade of the property falling towards the property lines and a low-laying area in the center of the site. Additionally, the subject site features considerable tree cover across most of the property, as noted on the demolition plans, including a line of canopy trees located adjacent to curb along the north side of Old Sauk Road. The project team has submitted a report prepared by an ecological consultant and arborist on the condition of the trees located on the perimeter of the site, including in the right of way of Old Sauk Road. The tree report is attached to the conditional use file for the project, ID 82972.

The proposed apartment building will feature three north-south wings of varying depth organized along a single east-west central spine, which will create two north-facing courtyards and two south-facing courtyards. The main entrance to the building will be located along the northern wall of the central wing and be accessed from a surface parking lot that will extend along the northern and eastern walls of the building. A lobby, community room, and tenant amenities will be located on the first floor adjacent to the northern entrance, while a coworking space for tenants will be located along the southern wall of the central wing adjacent to a secondary entrance that will provide direct pedestrian access to Old Sauk Road. The center wing of the building will be roughly centered on San Juan Trail, a local street that intersects Old Sauk Road opposite the subject site. A pool, hot tub, and sauna are proposed in the northeast courtyard of the building, while a fire pit and bocce court are proposed in the northwest courtyard. The south-facing courtyards will be open grass and landscaping. The 138 dwelling units proposed will include 25 studio units, 66 one-bedroom units, 43 two-bedroom units, and four (4) three-bedroom units. Parking for 143 automobiles and 140 bicycles will be provided in an under-building garage, with 25 parking spaces for autos and 14 bike stalls located around the perimeter of the building.

The proposed building will stand approximately 36 feet in height and be topped by a flat roof. The building will be clad with a combination of light brown-colored brick and gray fiber cement siding. Patios and balconies are proposed for all of the units, including patios that will open onto the courtyards for the abutting first floor units and patios connected to the Old Sauk Road sidewalk for the five first floor units to be located along the southern ends of the three wings.

In addition to combining the underlying parcels into one lot for the proposed multi-family development, the proposed CSM will dedicate a total of 40 feet of right of way as measured from the centerline of Old Sauk Road.

Supplemental Regulations

The following supplemental regulations in Section 28.151 of the Zoning Code apply to Outdoor Recreation:

- (a) A minimum 25-foot setback area maintained as open space shall be provided along the perimeter of the site wherever it abuts a residential district.
- (b) If the use will be available to the general public, an arterial or collector street of sufficient capacity to accommodate the traffic that the use will generate shall serve the site. Ease of access to the site by automobiles, transit, bicycles, and pedestrians shall be considered as a factor in the review of any application.
- (c) The site shall be designed in such a way as to minimize the effects of lighting and noise on surrounding properties. Hours of operation may be restricted and noise and lighting limits imposed as part of the conditional use approval.
- (d) An appropriate transition area between the use and adjacent property may be required, using landscaping, screening, and other site improvements consistent with the character of the neighborhood.

Analysis

The applicant is requesting approval of a zoning map amendment to rezone two parcels totaling 3.77 acres from SR-C1 and SR-C3 to TR-U2 to facilitate redevelopment of the site with a three-story, 138-unit apartment building following demolition of three existing residences and a variety of accessory buildings. Additionally, the applicant is seeking approval of a one-lot CSM to combine the parcels for the proposed multi-family redevelopment.

Consistency with Adopted Plans

The subject site is not currently located within the boundaries of an adopted neighborhood, sub-area, or area plan. The 2023 <u>Comprehensive Plan</u> generalized future land use plan recommends the subject site and parcels to the east for Low-Medium Residential (LMR), while the single-family residences otherwise surrounding the site are recommended for Low Residential (LR).

According to the <u>Comprehensive Plan</u>, LMR areas are made up of any or all of the following types of housing: small-lot single-family development, two-unit buildings, three-unit buildings, rowhouses, and small multi-family buildings. LMR areas are largely characterized by what is sometimes referred to as the "Missing Middle" of housing development: the range of multi-unit or clustered housing types that fall between the extremes of detached single-family homes and large apartment buildings. Building forms present in LMR are generally compatible in scale with single-family homes, and may therefore be intermixed with small-lot single-family development or used as a transition from more intense development to lower intensity areas comprised primarily of single-family development. LMR areas should be characterized by a walkable, connected street network to meet the growing demand for walkable urban living. Existing, isolated LMR areas should be better connected with their surroundings when opportunities arise, and newly developing LMR areas should be seamlessly integrated with surrounding development. Development in the LMR category should range in density from 7-30 units per acre and buildings should be up to three stories tall.

However, the 'Residential Future Land Use Categories' table on page 20 of the Growth Framework in the Plan includes a provision that allows large and courtyard multi-family buildings to be considered appropriate on properties recommended for LMR in "select conditions" at up to 70 dwelling units an acre and four stories of

height. The factors to be considered include relationships between proposed buildings and their surroundings, natural features, lot and block characteristics, and access to urban services, transit, arterial streets, parks, and amenities. These factors were expanded with the updates to the <u>Comprehensive Plan</u> adopted by the Common Council on December 5, 2023; previously, the only consideration was whether the LMR site was located along an arterial roadway. The effect of the wording change with the 2023 amendment allows more factors to be considered when determining whether the building forms more commonly associated with the more intensive Medium Residential (MR) land use category are appropriate in LMR and to make those forms possible at more locations compared to the prior language, which effectively limited them to sites on arterial roadways.

Consideration of Zoning Map Amendment Standards

The standards for zoning map amendments found in Section 28.182(6) of the Zoning Code state that such amendments are legislative decisions of the Common Council that shall be based on public health, safety, and welfare, shall be consistent with the <u>Comprehensive Plan</u>, and shall comply with Wisconsin and federal law. Wis. Stats. Section 66.1001(3) requires that zoning map amendments approved after January 1, 2010 be consistent with the City's <u>Comprehensive Plan</u>. 2010 Wisconsin Act 372 clarified "consistent with" as "furthers or does not contradict the objectives, goals, and policies contained in the comprehensive plan."

The statement of purpose for the TR-U zoning districts (TR-U1 and TR-U2) are established to "stabilize and protect and encourage the essential characteristics of high-density residential areas and to accommodate a full range of life-cycle housing." Other stated purposes of the TR-U districts include insuring that new buildings and additions to existing buildings are designed with sensitivity to their context in terms of building placement, facade width, height and proportions, garage and driveway placement, landscaping, and similar design features; maintaining and improving the viability of existing housing of all types, while providing for updating of older housing in a context-sensitive manner; maintaining or increasing compatibility between residential and other allowed uses, and between different housing types, where permitted, by maintaining consistent building orientation and parking placement and screening; and facilitating the preservation, development or redevelopment goals of the Comprehensive Plan and of adopted neighborhood, corridor or special area plans.

The TR-U2 zoning district requested allows for multi-family dwellings to be developed at a density of 124 units an acre (350 square feet of lot area per unit) and 40 square feet of usable open space per unit, with an allowed height of six stories and 78 feet. [Note: The TR-U1 zoning district allows less than half the density of TR-U2 (58 units per acre) and has similar setback requirements. However, TR-U1 requires four times the usable open space (160 square feet per multi-family unit) as TR-U2 (40 square feet), hence the district request.]

The proposed three-story apartment building meets the criteria for both a 'large multi-family building' and a 'courtyard multi-family building' in the Residential Building Form Standards in Section 28.172 of the Zoning Code. The development proposes a net density of 36.6 units per acre based on 138 units on the 3.77-acre site (lest Old Sauk Road right of way.)

In order to find the proposed rezoning consistent with the <u>Comprehensive Plan</u>, the above mentioned select conditions should be considered in turn:

• Relationships between proposed buildings and their surroundings and lot and block characteristics: The proposed apartment building will occupy approximately 56% of the 3.77-acre site, which has 553 feet of frontage along Old Sauk Road. Properties on three sides of the subject site are single-family residences on lots generally created between 1979 and 1988 north of Old Sauk Road and 1965-1978 south of Old Sauk. The subject site is identified as 'Lands' on the plats of Woodland Hills and First Addition to Woodland Hills, which

form the western and northern edges of the site, respectively, and the plat of Saukborough, which forms the eastern line of the site. The Woodland Hills plats feature lots on cul-de-sacs, which back up to the site and afford no opportunity for connectivity. Development in Saukborough includes a variety of single- and two-family residences located on private courts off of Sauk Ridge Trail, and Saukborough Square, a four-building complex of eight-unit multi-family buildings. Like to the north and west, there is no opportunity for development of the subject site to connect to the development to the east. The development pattern surrounding the site effectively limits its development to one that would be exclusively accessed from and primarily oriented to Old Sauk Road.

Staff acknowledges that the scale and mass of the proposed building will be unlike any other residential building in the surrounding area. However, despite the scale of the project, staff feels that efforts have been made to limit the differences in scale between the building and lower-scale and density surrounding uses, most particularly the use of significant building setbacks where the building abuts those uses. The proposed building will be set back over 50 feet at its nearest points from the side and rear property lines, which is well in excess of the minimum setbacks required by zoning. The actual distance between the proposed building and the surrounding buildings will be greater once the setbacks of the existing buildings are considered. The 15-foot front setback along Old Sauk Road will be less than the setbacks of some, but not all of the buildings on the north side of the street, but again, the use of the north-south wings of the building and the courtyards in between should result in scale and massing along Old Sauk Road that is more in keeping with the pattern of buildings along the road.

Staff also believes that the combination of the mass being centered in the site, the relatively short lengths of wall sections, the use of six-foot privacy fencing along the side and rear lot lines, and the use of a lower-profile design for the three-story building that features a flat roof rather than a pitched roof and modest floor-to-floor heights should all help to reduce the appearance of the scale of the building.

- Natural features: Staff does not believe that there are any natural features on the site or on the surrounding
 properties that would suggest that the building should not be built as proposed. While not defined in the
 Comprehensive Plan, staff believes that it would be reasonable for the Plan Commission and Common Council
 to consider "natural features" as those topographic features commonly identified in plans and environmental
 corridor mapping where urban development may not be appropriate, including wetlands, floodplains,
 waterways, and areas of steep slopes. None of those features are present on the site or on surrounding
 parcels.
- Access to urban services, transit, arterial streets, parks, and amenities: Old Sauk Road is classified as a minor arterial roadway according to the Madison Metropolitan Planning Organization and includes marked on-street bike lanes. Metro Transit provides daily service at least every 30 minutes on route R along Old Sauk, with stops west and east of the subject site. The sidewalk network along the north side of Old Sauk Road, however, is incomplete, which will require pedestrians to cross to the south side of the street to where the sidewalk network is fully developed between Old Middleton Road and N Gammon Road. To aid that crossing, the Traffic Engineering Division is requiring a rapid flashing beacon (RRFB), refuge island, and continental crosswalk to be installed by the developer as a condition of approval for the development.

The site is less than a quarter-mile from Everglade Park on Everglade Drive south of Old Sauk Road, while the larger Woodland Hills Park located northwest of the site is closer to a half mile walking distance. Crestwood Elementary School is located three-quarters of a mile east of the site. The site and surrounding neighborhoods are located in an area of the City that does not currently have neighborhood-serving commercial businesses within a reasonable walking distance, thereby requiring that autos, bikes or transit be used to access retail and service businesses located elsewhere. However, two of the four quadrants of the Old Sauk Road-N Gammon Road intersection are recommended for Neighborhood Mixed-Use (NMU) in the Comprehensive

<u>Plan</u>. Those NMU parcels may transition over time into higher density mixed-use developments that may include some amount of neighborhood serving commercial uses. The subject site is just over a quarter-mile east of the Old Sauk-Gammon intersection and accessible by sidewalks on both sides of Old Sauk.

On balance, the Planning Division believes it is possible that the Plan Commission and Common Council could find that the development of a three-story, 138-unit multi-family dwelling on the subject site is consistent with the factors listed in the Comprehensive Plan for large multi-family buildings and courtyard multi-family buildings in the LMR land use category. Staff does not believe that all of the select conditions enumerated in the plan have to be present in order for the larger and denser building form to be allowed. While the proposed building is both a larger building form and denser than what is located in the surrounding area, the height and density (three stories and 36.6 dwelling units per acre) is within the range discussed in the plan should it be found the project meets the select conditions described above. The site's location along a minor arterial and the availability of daily and relatively frequent bus service are the most significant factors as to why the proposed development may be approved. The proposed development is also consistent with other goals and objectives in the Comprehensive Plan that encourage development of a wider mix of housing types, sizes, and costs throughout the City, and to increase the amount of housing available by allowing more housing in more places. Staff will note that the TR-U2 district is the least intensive conventional residential district that could implement this proposal when the amount of lot area and the amount of usable open space required are considered. However, the TR-U2 district does allow for greater intensities than those currently proposed and those supported in the LMR category for large and courtyard multi-family buildings. As a reference, any development exceeding 36 units would require a conditional use consideration from the Plan Commission, though heights up to six stories are permitted.

Consideration of Demolition Permit Standards

In order to approve a demolition request, the Plan Commission shall consider the factors and information specified in Section 28.185(9)(c) and find that the proposed demolition or removal is consistent with the statement of purpose of the demolition permits section and with the health, prosperity, safety, and welfare of the City of Madison. The standards for demolition approval state that the Plan Commission shall consider the report of the City's historic preservation planner regarding the historic value of the property as well as any report that may be submitted by the Landmarks Commission. On April 15, 2024, the Landmarks Commission recommended to the Plan Commission that the residences at 6610, 6612(-6614), and 6706 Old Sauk Road had no known historic value. A member of the Landmarks Commission expressed regret about the demolition of the barn on 6706 Old Sauk Road; however, as an accessory building, it is not subject to the Landmarks Commission's or Plan Commission's purview.

In approving a demolition permit, the Plan Commission may stipulate conditions and restrictions on the proposed building demolition as deemed necessary to promote the public health, safety and general welfare of the community, and to secure compliance with the standards of approval. The proposed conditions for this demolition may be found in the 'Recommendation' section of the report, which follows.

Consideration of the Conditional Use Standards

A conditional use is defined in the Zoning Code as "a use which, because of its unique or varying characteristics, cannot be properly classified as a permitted use in a particular district." The Plan Commission shall not approve an application for a conditional use unless it can find that all of the standards found in Section 28.183(6)(a), Approval Standards for Conditional Uses, are met. That section states: "The City Plan Commission shall not approve a conditional use without due consideration of the recommendations in the City of Madison Comprehensive Plan and any applicable, neighborhood, neighborhood development, or special area plan,

including design guidelines adopted as supplements to these plans. No application for a conditional use shall be granted by the Plan Commission unless it finds that all of the [standards for approval in Section 28.183(6) are met]." Before granting a conditional use, the Plan Commission may stipulate conditions and restrictions on the establishment, location, construction, maintenance and operation of the conditional use. Additionally, state law requires that conditional use findings must be based on "substantial evidence" that directly pertains to each standard and not based on personal preference or speculation.

A review of the standards that apply to the proposed multi-family dwelling and outdoor recreation conditional uses follows.

1. The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, or general welfare.

Unlike some other conditional uses allowed in various zoning districts throughout the Zoning Code, the construction of new residential on lands recommended in adopted plans for residential development, whether in newly developing areas or as infill in established areas, would typically be assumed to meet this standard for approval. This request has been reviewed by various City reviewing agencies who have provided comments and recommended conditions. Planning staff does not believe that the information provided in these comments suggest that this standard cannot be met.

Several public comments have been received for the project expressing various concerns, including comments related to the proposed development worsening existing drainage issues in the neighborhoods surrounding the subject site, to which staff feels this standard is most relevant. Among the comments received are two reports prepared by an independent civil engineer (Nahn) and comments submitted by a soils scientist (Norman) responding to the preliminary stormwater management plans that have been submitted by the development team (dated April 8 and May 24, 2024). It is rare in staff's experience for a stormwater management plan to be submitted prior to the Plan Commission's consideration of a project like the one proposed, let alone for the applicant's civil engineer to provide an amended plan in response to comments received. Additionally, the Plan Commission should note the four-page memo submitted by Assistant City Engineer Greg Fries dated May 31, 2024, which provides an initial staff review of the plans submitted by the development team for conformance with MGO Chapter 37, The Public Stormwater System Including Erosion Control.

It is the opinion of City staff that a residential development like the one proposed, which complies with the requirements in MGO Chapter 37, can meet conditional use standard 1 as it pertains to impacts from storm drainage and erosion. As noted in the Fries memo and in the conditions recommended in the City Engineering Division section of the 'Recommendations' section of this report, the applicants will be required to submit a final stormwater management plan and erosion control plan for approval by the City Engineer before the conditional use plans could be signed-off and permits issued for the project.

2. The City is able to provide municipal services to the property where the conditional use is proposed, given due consideration of the cost of providing those services.

The comments and recommended conditions of approval received from reviewing departments and included in the last section of this report, including conditions from the City Engineering Division, Traffic Engineering Division, and Madison Fire Department, suggest nothing out of the ordinary in providing municipal services to this property because of the proposed development.

3. The uses, values and enjoyment of other property in the neighborhood for purposes already established will not be substantially impaired or diminished in any foreseeable manner.

This standard is often the most difficult standard for the Plan Commission to address in the process of reviewing conditional use applications. Information provided by residents or property owners in the neighborhood at the Plan Commission hearing usually provides additional information for the Commission to use to determine whether this standard has been met or not. The proposed building has elicited a significant amount of correspondence from nearby residents, which has been provided for consideration as part of the approval of the project. The Plan Commission will need to weigh the application materials, the comments and conditions submitted by reviewing agencies, the recommendations of the Comprehensive Plan, and the public input it receives in order to determine whether this standard is met.

Planning staff believes that the supplemental regulations for outdoor recreation can be met for the tenant amenities located in the northern courtyards of the proposed building. The pool, hot tub, and other amenities will exceed the 25-foot setback for outdoor recreation, and the amenities should be adequately screened from nearby properties. However, in order to limit impacts on adjacent properties from the outdoor recreation, staff believes that it would appropriate for the Plan Commission to require the applicant to submit proposed hours for the outdoor recreation uses for approval by the Planning Division in consultation with the district alder prior to final sign-off of the conditional uses for the project.

- 4. The establishment of the conditional use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.
 - Staff does not believe that construction of the new building will impede the normal and orderly development or improvement of surrounding properties or preclude the development of other nearby properties in a manner consistent with the LR recommendations in the <u>Comprehensive Plan</u> and zoning of those properties. While the proposed building represents a significantly different building form compared to what currently exists on the subject site and on surrounding properties, staff does not foresee how construction of the apartment building will cause the surrounding residential neighborhoods to not continue in much the same fashion as the area has functioned historically.
- 5. Adequate utilities, access roads, drainage, internal circulation improvements for pedestrians, bicyclists, public transit and vehicles, parking supply (in cases with minimum parking requirements) and other necessary site improvements have been or are being provided.
 - Staff believes that the Plan Commission can find that this standard is met overall. As noted above in the discussion of standard 1, City staff feels that the drainage component of standard 5 can be met subject to its compliance with MGO Chapter 37 as administered by the City Engineer.

Regarding the adequacy of the utilities to serve the project, the developer will be required to provide wastewater flow calculations for the development that demonstrate that adequate sanitary sewer capacity exists to serve the project, as is typical for projects that propose a significant increase in dwelling units compared to the existing conditions. If additional capacity is needed, it may be the development team's responsibility to construct off-site sanitary sewer improvements to provide the needed capacity. No comments have been received from the Madison Water Utility that would suggest that there is not adequate water capacity to serve the development.

Finally, staff is aware of concerns by some residents of the surrounding area about the potential for the proposed development to create a significant amount of additional traffic along Old Sauk Road. However, the Traffic Engineering Division has not expressed significant concerns with the proposal and has accepted the traffic impact analysis submitted by the applicants, which is attached to the conditional use legislative file, ID 82972 for reference. As noted elsewhere in the Analysis section of this report, the project will be required to construct improvements to Old Sauk Road to improve pedestrian safety related to the project,

including constructing public sidewalk along the frontage and a pedestrian refuge island, and installing a rapid flashing beacon.

- 6. The conditional use conforms to all applicable regulations of the district in which it is located.
 - The Zoning Administrator has reviewed the project and determined that it will comply with the requirements of the proposed TR-U2 district.
- 8. When applying the above standards to any new construction of a building or an addition to an existing building the Plan Commission shall find that the project creates an environment of sustained aesthetic desirability compatible with the existing or intended character of the area and the statement of purpose for the zoning district. In order to find that this standard is met, the Plan Commission may require the applicant to submit plans to the Urban Design Commission for comment and recommendation.

Staff believes that the Plan Commission can find this standard is met. Despite the proposed building being notably larger than those in the surrounding area, staff feels that the building can create an environment of sustained aesthetic desirability. The impacts on surrounding properties will be moderated by the proposed mass being centered on the site and through the use of narrow north-south wings to form courtyards in an effort to limit the presence of the building when viewed from the north and along Old Sauk Road. Staff believes that the project is well designed and that review by the Urban Design Commission is unnecessary. However, following the public hearing, the Plan Commission has the option to refer the project to the Urban Design Commission should it feel its input is needed in order to find standard 8 met.

[Note: Standards 7 and 9-16 do not apply to the conditional use requests for 6610-6706 Old Sauk Road.]

As with any conditional use, the Plan Commission retains continuing jurisdiction in the event that complaints are received about the multi-family dwelling and accessory outdoor recreation, which could result in more restrictive conditions being applied if deemed necessary following an investigation and public hearing.

Criteria for Certified Survey Map

Finally, if the Plan Commission finds that the related land use approvals meet the standards for approval, it may also find that the proposed one-lot Certified Survey Map meets the standards and criteria for approval subject to the conditions in the Recommendations section of the report.

Conclusion

The applicant proposes to demolish the former two single-family residences and a two-family residence to allow redevelopment of the 3.77-acre site with a three-story, 138-unit apartment building in TR-U2 zoning. The requests are subject to the standards for approval for demolition permits, zoning map amendments, conditional uses, and land divisions.

The subject site is recommended for Low-Medium Residential (LMR) by the <u>Comprehensive Plan</u>, which is a land use category intended to primarily encourage development of small-lot single-family, two-unit buildings, three-unit buildings, rowhouses, and small multi-family buildings at densities between 7-30 units an acre. However, the applicants are requesting approval using a provision in the <u>Comprehensive Plan</u> that allows large multi-family buildings and courtyard multi-family buildings up to four stories in height and at densities up to 70 units an acre

to be considered appropriate in LMR under select conditions. On balance, the Planning Division believes that the Plan Commission and Common Council could find that proposed development is consistent with the select conditions to allow large multi-family and courtyard multi-family building forms in LMR. Specifically, the site's location along a minor arterial roadway that has relatively frequent daily bus service as the most significant factors as to why the proposed development may be approved. Additionally, staff believes that the conditional uses required for the development can meet the applicable standards for approval subject to the conditions in the following section.

In reviewing the project, the Plan Commission should carefully consider the dozens of public comments received since the development was first made public last fall. Those comments are attached to the legislative files associated with the project.

Recommendation

Planning Division Recommendation (Contact Timothy M. Parks, (608) 261-9632)

If the Plan Commission can find the applicable standards are met, the Planning Division recommends the following to the Plan Commission:

- That the Plan Commission that the standards for demolition permits are met to **approve** demolition of the two-story office building located at 1617 Sherman Avenue;
- That the Plan Commission forward Zoning Map Amendment ID 28.022–00621, rezoning 6610-6706 Old Sauk Road from SE to TR-U2, to the Common Council with a recommendation of approval;
- That the Plan Commission find the standards for conditional uses are met to approve a residential building
 complex containing approximately 310 apartments in two five-story buildings and a residential building
 complex containing 20 townhouse units in three two-story building, subject to input at the public hearing,
 final approval by the Urban Design Commission, and the conditions from reviewing agencies that follow;
 and
- That the Plan Commission forward the Certified Survey Map to divide 6610-6706 Old Sauk Road into three
 lots and one outlot for stormwater management to the Common Council with a recommendation of
 approval subject to the conditions from reviewing agencies beginning on page 23.

Recommended Conditions of Approval

Major/Non-Standard Conditions are Shaded

Planning Division

- Provide hours of operation for the proposed outdoor recreation for approval by the district alder and Planning
 Division director prior to issuance of building permits for the apartment building. Any revision to the approved
 hours of operation shall require approval of an alteration to the conditional use to be approved by the district
 alder and Director of the Planning Division or the Plan Commission.
- 2. Revise Sheet C100 to show the side and rear yard setback dimensions.

<u>City Engineering Division</u> (Contact Tim Troester, (608) 2671-995)

- 3. The applicant shall provide projected wastewater flow calculations to Mark Moder (mmoder@cityofmadison.com). The proposed development may result in off-site sanitary sewer improvements being required of the developer as a condition of development.
- 4. The area adjacent to this proposed development has been determined by the City Engineering Division to have a known flooding risk. City Engineering has set the minimum protective lowest entrance elevation opening at an elevation of 821.30. This standard is not intended to be protective in all cases. The developer is strongly encouraged to complete their own engineering analysis to determine and meet a protective elevation which they are comfortable with. In no case shall the protective elevation be set below the minimum threshold determined by the City Engineering Division.
- 5. Enter into a City / Developer agreement for the required infrastructure improvements. The agreement shall be executed prior to sign off. Allow 4-6 weeks to obtain agreement. Contact the City Engineering Division to schedule the development and approval of the plans and the agreement.
- 6. Construct sidewalk, terrace, curb and gutter, and pavement along the Old Sauk Road frontage to a plan approved by the City Engineer. Note: In order to save trees, a public limited easement may be required.
- 7. Madison Metropolitan Sewerage District (MMSD) charges are due and payable prior to City Engineering Division sign-off, unless otherwise collected with a Developer's / Subdivision Contract. Contact Mark Moder ((608) 261-9250) to obtain the final MMSD billing a minimum of two (2) working days prior to requesting City Engineering signoff.
- 8. Obtain a permanent sewer plug permit for each existing sanitary sewer lateral serving a property that is not to be reused and a temporary sewer plug permit for each sewer lateral that is to be reused by the development.
- 9. An Erosion Control Permit is required for this project.
- 10. A Storm Water Management Report and Storm Water Management Permit is required for this project.
- 11. A Storm Water Maintenance Agreement (SWMA) is required for this project.
- 12. This site appears to disturb over one (1) acre of land and requires a permit from the Wisconsin Department of Natural Resources (WDNR) for stormwater management and erosion control. The City of Madison has been required by the WDNR to review projects for compliance with NR-216 and NR-151; however, a separate permit submittal is still required to the WDNR for this work. The City of Madison cannot issue its permit until concurrence is obtained from the WDNR via their NOI or WRAPP permit process. Contact Eric Rortvedt at (608) 273-5612 of the WDNR to discuss this requirement. The applicant is notified that the City of Madison is an approved agent of the Department of Safety and Professional Services (DSPS) and no separate submittal to this agency or the Capital Area Regional Planning Commission (CARPC) is required for this project to proceed.
- 13. Revise the plans to show a proposed private internal drainage system on the site. Include the depths and locations of structures and the type of pipe to be used.

- 14. The proposed development proposes to construct underground parking. The proposed entrance to the underground parking is adjacent to a street low point. The applicant shall provide at a minimum of one (1) foot of rise from the adjacent back of walk in the driveway before breaking grade to the down ramp to the underground parking to protect the underground parking from inundation. The stated elevation is intended to be protective but does not guarantee a flood proof structure. The developer/owner are strongly encouraged to complete their own calculations and determine an elevation that protects their property to a level of service that they are comfortable with.
- 15. Provide additional detail how the enclosed depression(s) created by the parking entrance(s) to the below building parking area(s) is/are served for drainage purposes. The building must be protected from receiving runoff up through the 100-year design storm that is current in MGO Chapter 37. If the enclosed depression(s) is/are to be served by a gravity system provide calculations stamped by a Wisconsin P.E. that show inlet and pipe capacities meet this requirement. If the enclosed depression(s) is/are to be served by a pump system provide pump sizing calculations stamped by a Wisconsin P.E. or licensed Plumber that show this requirement has been met.
- 16. This project falls in the area subject to increased erosion control enforcement as authorized by the fact that it is in a TMDL ZONE and therefore will be regulated to meet a higher standard.
- 17. This project will disturb 20,000 square feet or more of land area and require an Erosion Control Plan. Please submit an 11" x 17" copy of an erosion control plan (pdf electronic copy preferred) to Megan Eberhardt (west) at meberhardt@cityofmadison.com, or Daniel Olivares (east) at daolivares@cityofmadison.com, for approval.
- 18. Demonstrate compliance with MGO Sections 37.07 and 37.08 regarding permissible soil loss rates. Include Universal Soil Loss Equation (USLE) computations for the construction period with the erosion control plan. Measures shall be implemented in order to maintain a soil loss rate below 5.0 tons per acre per year.
- 19. Complete weekly self-inspection of the erosion control practices and post these inspections to the City of Madison website as required by MGO Chapter 37.
- 20. Prior to approval, this project shall comply with Chapter 37 of Madison General Ordinances regarding stormwater management. Specifically, this development is required to submit a Storm Water Management Permit application, associated permit fee, Stormwater Management Plan, and Storm Water Management Report to City Engineering. The Storm Water Management Plan & Report shall include compliance with the following:

Submit prior to plan sign-off, a stormwater management report stamped by a P.E. registered in the State of Wisconsin.

Provide electronic copies of any stormwater management modeling or data files including SLAMM, RECARGA, TR-55, HYDROCAD, Sediment loading calculations, or any other electronic modeling or data files. If calculations are done by hand or are not available electronically, the hand copies or printed output shall be scanned to a PDF file and provided to City Engineering.

Detain the 2-, 5-, 10-, 100-, and 200-year storm events, matching post development rates to predevelopment rates and using the design storms identified in MGO Chapter 37.

If the development has an enclosed area that provides existing storage, the existing storage will need to be accounted for in addition to meeting the requirements for detention.

Provide infiltration of 90% of the pre-development infiltration volume.

Reduce TSS by 80% (control the 5-micron particle) off of newly developed areas compared to no controls.

Treat the first half-inch of runoff over the proposed parking facility.

Provide onsite volumetric control limiting the post construction volumetric discharge to the predevelopment discharge volume as calculated using the 10-year storm event.

The applicant shall demonstrate that water can leave the site and reach the public right of way without impacting structures during a 100-year event storm. This analysis shall include reviewing overflow elevations and unintended storage occurring on site when the storm system has reached capacity.

Submit a draft Stormwater Management Maintenance Agreement (SWMA) for review and approval that covers inspection and maintenance requirements for any best management practices (BMP) used to meet stormwater management requirements on this project.

21. Submit, prior to plan sign-off but after all revisions have been completed, digital PDF files to the Engineering Division. Email PDF file transmissions are preferred to: bstanley@cityofmadison.com (East) or ttroester@cityofmadison.com (West).

<u>City Engineering Division – Mapping Section</u> (Contact Julius Smith, (608) 264-9276)

- 22. Pending the final design for the public sidewalk improvements, a public sidewalk easement may be required for this project to protect existing trees. If so required this may be done with so on the face of the concurrent CSM. Contact Jule Smith (jsmith4@cityofmadison.com) for the required language to be included on the face of the CSM.
- 23. It is anticipated that the public improvements required to serve this proposed development will require additional right of way and/or easements located beyond the project boundary. The developer shall acquire the right of way and/or easements as required by the City at the developer's expense. In the event that the developer is unable to acquire the right of way and/or easements required, the City shall proceed to acquire the easements. The developer shall reimburse the City for all costs associated with the acquisition, including attorney's fees and any and all costs associated with court ordered awards. The developer shall provide a deposit at the time of contract execution to cover the estimated City staff expenses and easement cost for the acquisition. The developer shall note that separate, additional surety in an amount estimated to cover any potential court ordered awards shall be retained by the City until such time as appeal rights have expired. The additional surety shall be provided prior to the City making an offer for the easement.

The purpose of Outlot 1 of Woodland Hills, recorded as Document No. 1623944, was never stated on the plat. This outlot appears to function a public utility routing for sanitary sewer and drainage way for the overflow route from Old Sauk Road to East Spyglass Court at the time it was platted. It is not fully known why this outlot was not dedicated, or an easement was given for these purposes when it was platted, as it seems to be the intention of the outlot. Currently the outlot is privately owned by the owner of Lot 13 subject to the public facilities conditions without recorded rights.

The subject site accepts the drainage overflow of Old Sauk Road and transfers it to the outlot. Therefore, a public easement for stormwater drainage should be established over the path of the drainage on Outlot 1. The proposed development may add to the discharge to the outlot.

Additionally, while this easement is being acquired for stormwater drainage for the project, additional easement rights for the sanitary sewer shall be acquired across the outlot.

- 24. Any portion(s) of a public easement that is intended to be released shall be released by separate document prepared by City Office of Real Estate Services. Contact Jule Smith of Engineering Mapping (jsmith4@cityofmadison.com, ((608) 264-9276) to coordinate the Real Estate project, and associated information and fees required. If any release is required prior to recording of the plat, acknowledgement of the release and document number shall be noted on the face of the plat. Provide Fee Legal description and Exhibit for the Portions Sanitary Sewer Easements Document Nos. 1275466 and 1275467 that are being requested to be released with the development. The final area to be released are to be approved by the City Engineering Division.
- 25. Coordinate and request from the utility companies serving this area the easements required to serve this development. Those easements shall be properly shown, dimensioned and labeled on the CSM.
- 26. The address of the proposed apartment building is 6624 Old Sauk Road. The site plan shall reflect a proper street address of the property as reflected by official City of Madison Assessor's and Engineering Division records.
- 27. The pending Certified Survey Map application for this property shall be completed and recorded with the Dane County Register of Deeds, the new parcel data created by the Assessor's Office, and the parcel data available to Zoning and Building Inspection staff prior to issuance of building permits and an early start permits for new construction.
- 28. Submit a site plan and a complete building Floor Plan in PDF format to Lori Zenchenko (Izenchenko@cityofmadison.com) that includes a floor plan of each floor level on a separate sheet/page for the development of a complete interior addressing plan. Also, include a unit matrix for apartment buildings that shows the number of apartments on each floor. The Addressing Plan for the entire project shall be finalized and approved by Engineering (with consultation and consent from the Fire Marshal if needed) PRIOR to the verification submittal stage of this LNDUSE with Zoning. The final approved Addressing Plan shall be included in said Site Plan Verification application materials. Per 34.505 MGO, a full copy of the approved addressing plan shall be kept at the building site at all times during construction until final inspection by the Madison Fire Department. For any changes pertaining to the location, deletion or addition of a unit, or to the location of a unit entrance, (before, during, or after construction), a revised Address Plan shall be resubmitted to Lori Zenchenko to review addresses that may need to be changed and/or reapproved.

Traffic Engineering Division (Contact Sean Malloy, (608) 266-5987)

- 29. The applicant shall be responsible for the construction of a traffic island, marked continental crosswalk, pedestrian ramps, and a Rectangular Rapid Flashing Beacons (RRFB) to facilitate pedestrian crossings of Old Sauk Road.
- 30. Items in the right of way are not approvable through the site plan approval process. The right of way is the sole jurisdiction of the City of Madison and is subject to change at any time per the recommendation/plan of Traffic Engineering and City Engineering Divisions.
- 31. The applicant shall dedicate right of way or grant a public sidewalk easement for and be responsible for the construction of a minimum five (5)-foot wide sidewalk along their site's frontage of Old Sauk Road.

- 32. Note: The applicant has submitted the Traffic Impact Analysis study requested by the Traffic Engineering Division; the study has been reviewed and accepted by Traffic Engineering.
- 33. The applicant shall submit a waste removal plan for review by the City Traffic Engineer, which shall include vehicular turning movements.
- 34. The applicant shall submit one contiguous plan showing proposed conditions and one contiguous plan showing existing conditions for approval. The plan drawing shall be scaled to 1" = 20' and include the following, when applicable: existing and proposed property lines; parcel addresses; all easements; pavement markings; signing; building placement; items in the terrace such as signs, street light poles, hydrants; surface types such as asphalt, concrete, grass, sidewalk; driveway approaches, including those adjacent to and across street from the project lot location; parking stall dimensions, including two (2) feet of vehicle overhang; drive aisle dimensions; semitrailer movement and vehicle routes; dimensions of radii; and percent of slope.
- 35. The developer shall post a security deposit prior to the start of development. In the event that modifications need to be made to any City owned and/or maintained traffic signals, street lighting, signing, pavement marking and conduit/handholes, the Developer shall reimburse the City for all associated costs including engineering, labor and materials for both temporary and permanent installations.
- 36. The City Traffic Engineer may require public signing and marking related to the development; the developer shall be financially responsible for such signing and marking.
- 37. All parking facility design shall conform to the standards in MGO Section 10.08(6).
- 38. All bicycle parking adjacent pedestrian walkways shall have a two (2)-foot buffer zone to accommodate irregularly parked bicycles and/or bicycletrailers.
- 39. Per MGO Section 12.138 (14), this project is not eligible for residential parking permits. It is recommended that this prohibition be noted in the leases for the residential units.
- 40. The applicant shall adhere to all vision triangle requirements as set in MGO Section 27.05 (no visual obstructions between the heights of 30 inches and 10 feet at a distance of 25 feet behind the property line at streets and 10 feet at driveways.). Alteration necessary to achieve compliance may include but are not limited to; substitution to transparent materials, removing sections of the structure and modifying or removing landscaping elements. If applicant believes public safety can be maintained they shall apply for a reduction of MGO Section 27.05(2)(bb), Vision Clearance Triangles at Intersections Corners. Approval or denial of the reduction shall be the determination of the City Traffic Engineer.
- 41. The applicant shall provide a clearly defined five-foot walkway from the front door to the public right of way clear of all obstructions to assist citizens with disabilities, especially those who use a wheelchair or are visually impaired. Obstructions include but are not limited to tree grates, planters, benches, parked vehicle overhang, signage and doors that swing outward into walkway.
- 42. The applicant shall show the dimensions for the proposed Class III driveway including the width of the drive entrance, width of the flares, and width of the curb cut.

43. All existing driveway approaches on which are to be abandoned shall be removed and replaced with curb and gutter and noted on the plan.

Parking Division (Contact Trent W. Schultz, (608) 246-5806)

44. The applicant shall submit a revised Transportation Demand Management (TDM) Plan to tdm@cityofmadison.com. The revised TDM Plan shall include: a) Site TDM Coordinator contact information; b) Selected TDM measures, totaling the required TDM point value (15). Applicable fees will be assessed after the revised TDM Plan is reviewed by staff.

Zoning Administrator (Contact Jacob Moskowitz, (608) 266-4560)

- 45. Section 28.185(9) requires that every applicant for a demolition or removal approval that requires approval by the Plan Commission is required to get a Reuse and Recycling Plan approved by the City Recycling Coordinator, Bryan Johnson at streets@cityofmadison.com prior to receiving a raze permit. Every person who is required to submit a reuse and recycling plan pursuant to Section 28.185(9) shall submit documents showing compliance with the plan within 60 days of completion of demolition. A demolition or removal permit is valid for two (2) years from the date of the Plan Commission approval.
- 46. Provide adequate development frontage landscaping per Section 28.142(5) Development Frontage Landscaping. Landscaping and/or ornamental fencing shall be provided between buildings or parking areas and the adjacent street(s), except where buildings are placed at the sidewalk. One overstory deciduous tree and five shrubs shall be planted for each 30 lineal feet of lot frontage. Two ornamental trees or two evergreen trees may be used in place of one overstory deciduous tree. In cases where development frontage landscaping cannot be provided due to site constraints, the zoning administrator may waive the requirement or substitute alternative screening methods for the required landscaping. Note that landscaping must be installed on the private property.
- 47. Provide details demonstrating compliance with bird-safe glass requirements Section 28.129. For building façades where the first 60 feet from grade are comprised of less than 50% glass, at least 85% of the glass on glass areas 50 square feet or over must be treated. Of all glass areas over 50 square feet, any glass within 15 feet of a building corner must be treated. Identify which glass areas are 50 square feet or greater and which glass areas will be treated. Provide a detail of the specific treatment product that will be used.

Fire Department (Contact Matt Hamilton, (608) 266-4457)

- 48. Provide fire apparatus access as required by IFC 503 2021 edition, MGO Section 34.503. Provide plan documenting fire access. A Fire Apparatus Access and Fire Hydrant Worksheet is available on the MFD website to assist in development.
- 49. Per MGO Section 34.503/IFC 503 Appendix D105, Provide an aerial apparatus access fire lane that is at least 26 feet wide, if any part of the building is over 30 feet in height. The near edge of the aerial fire lane shall be within 30 xxfeet and not closer than 15 feet from the structure, and parallel to one entire side. The aerial fire lane shall cover not less than 25% of the building perimeter.
- 50. A dead-end fire lane that is longer than 150 feet shall terminate in a turnaround. Provide an approved turnaround (cul-de-sac, 45-degree wye, 90-degree tee) at the end of a fire lane that is more than 150 feet in length.

51. Fire access lanes shall be designed to support 85,000 lbs.

Parks Division (Contact Ann Freiwald, (608) 243-2848)

52. Park Impact Fees (comprised of the Park Infrastructure Impact Fee, per MGO Sec. 20.08(2)), and Park-Land Impact Fees, per MGO Sec. 16.23(8)(f) and 20.08(2) will be required for all new residential development associated with this project. This development is within the West Park-Infrastructure Impact Fee district. Please reference ID# 24025 when contacting Parks Division staff about this project.

Water Utility (Contact Jeff Belshaw, (608) 261-9835)

53. A Water Service Application Form and fees must be submitted before connecting to the existing water system. Provide at least two working days' notice between the application submittal and the requested installation or inspection appointment. Application materials are available on the Water Utility's Plumbers & Contractors website (http://www.cityofmadison.com/water/plumbers-contractors), otherwise they may be obtained from the Water Utility Main Office at 119 E Olin Avenue. A licensed plumber signature is required on all water service applications. For new or replacement services, the property owner or authorized agent is also required to sign the application. A Water Meter Application Form will subsequently be required to size & obtain a water meter establish a Water Utility customer account and/or establish a Water Utility fire service account. If you have questions regarding water service applications, please contact Madison Water Utility at (608) 266-4646.

Forestry Section (Contact Brandon Sly, (608) 266-4816)

- 54. An existing inventory of street trees located within the right of way shall be included on the site, demo, utility, landscape, grading, fire aerial apparatus and street tree plan sets. The inventory shall include the following: location, size (diameter at 4 1/2 feet), and species of existing street trees. The inventory should also note if a street tree is proposed to be removed and the reason for removal.
- 55. All proposed street tree removals within the right of way shall be reviewed by City Forestry before the Plan Commission meeting. Street tree removals require approval and a tree removal permit issued by City Forestry. Any street tree removals requested after the development plan is approved by the Plan Commission or the Board of Public Works and City Forestry will require a minimum of a 72-hour review period which shall include the notification of the Alderperson within who's district is affected by the street tree removal(s) prior to a tree removal permit being issued. Add as a note on the street tree plan set.
- 56. Contractor shall take precautions during construction to not disfigure, scar, or impair the health of any street tree. Contractor shall operate equipment in a manner as to not damage the branches of the street tree(s). This may require using smaller equipment and loading and unloading materials in a designated space away from trees on the construction site. Any damage or injury to existing street trees (either above or below ground) shall be reported immediately to City Forestry at (608) 266-4816. Penalties and remediation shall be required. Add as a note on the site, grading, utility, demolition, and street tree plan set.
- 57. As defined by the Section 107.13 of City of Madison Standard Specifications for Public Works Construction: No excavation is permitted within 5 feet of the trunk of the street tree or when cutting roots over 3 inches in diameter. If excavation is necessary, the Contractor shall contact Madison City Forestry at (608) 266-4816 prior to excavation. City of Madison Forestry personnel shall assess the impact to the tree and to its root system prior to work commencing. Add as a note on the site, grading, utility, demolition and street tree plan sets.

- 58. Section 107.13(g) of *City of Madison Standard Specifications for Public Works Construction* addresses soil compaction near street trees and shall be followed by the contractor. The storage of parked vehicles, construction equipment, building materials, refuse, excavated spoils or dumping of poisonous materials on or around trees and roots within five (5) feet of the tree or within the protection zone is prohibited. Add as a note on both the site and street tree plan sets.
- 59. On this project, street tree protection zone fencing is required. The fencing shall be erected before the demolition, grading or construction begins. The fence shall include the entire width of terrace and extend at least 10 feet on both sides of the outside edge of the tree trunk. Do not remove the fencing to allow for deliveries or equipment access through the tree protection zone. Add as a note on both the site and street tree plan sets.
- 60. Street tree pruning shall be coordinated with City Forestry at a minimum of two weeks prior to the start of construction for this project. Contact City Forestry at (608) 266-4816. All pruning shall follow the American National Standards Institute (ANSI) A300 Part 1 Standards for pruning. Add as a note on both the site and street plan sets.
- 61. The developer shall submit a Street Tree Report performed by International Society of Arboriculture Certified Arborist prior to the Plan Commission meeting for City Forestry's review of project. This report shall identify all street trees on proposed project site, species type, canopy spread, tree condition, proposed tree removals, the impacts of proposed construction, and any requested pruning.
- 62. The developer shall post a security deposit prior to the start of the development to be collected by City Engineering as part of the developer's agreement. In the event that street trees are damaged during the construction process, City Forestry will draw from this deposit for damages incurred.
- 63. Additional street trees are needed for this project. Tree planting specifications can be found in Section 209 of *City of Madison Standard Specifications for Public Works Construction*. All street tree planting locations and tree species within the right of way shall be determined by City Forestry. A landscape plan and street tree planting plan shall be submitted in PDF format to City Forestry for approval of planting locations within the right of way and tree species. All available street tree planting locations shall be planted within the project boundaries. Add following note on both the landscape and street tree plan sets: At least one week prior to street tree planting, Contractor shall contact City Forestry at (608) 266-4816 to schedule inspection and approval of nursery tree stock and review planting specifications with the landscaper.

Metro Transit (Contact Tim Sobota, (608) 261-4289)

This agency has reviewed the request and recommended no conditions of approval.

Certified Survey Map – Recommended Conditions of Approval

Major/Non-Standard Conditions are Shaded

<u>City Engineering Division</u> (Contact Tim Troester, (608) 267-1995)

1. A Phase 1 environmental site assessment (per ASTM E1527-21), is required for lands dedicated to the City. Provide one (1) digital copy and staff review will determine if a Phase 2 ESA is also required. Submit report(s) to Brynn Bemis (608-267-1986, bbemis@cityofmadison.com).

- 2. Enter into a City / Developer agreement for required infrastructure improvements. The agreement shall be executed prior to sign off. Allow 4-6 weeks to obtain agreement. Contact the City Engineering Division to schedule the development and approval of the plans and the agreement.
- 3. Construct sidewalk, terrace, curb and gutter, and pavement along the Old Sauk Road frontage to a plan approved by the City Engineer. Note: In order to save trees, a public limited easement may be required.
- 4. Madison Metropolitan Sewerage District (MMSD) charges are due and payable prior to City Engineering Division sign-off, unless otherwise collected with a Developer's / Subdivision Contract. Contact Mark Moder (261-9250) to obtain the final MMSD billing a minimum of two (2) working days prior to requesting City Engineering signoff.
- 5. A minimum of two (2) working days prior to requesting City Engineering signoff on the CSM, contact either Tim Troester (West) at (608) 261-1995 (ttroester@cityofmadison.com) or Brenda Stanley (East) at (608) 261-9127 (bstanley@cityofmadison.com) to obtain the final stormwater utility charges that are due and payable prior to sub-division of the properties. The stormwater utility charges (as all utility charges) are due for the previous months of service and must be cleared prior to the land division (and subsequent obsolesces of the existing parcel).

<u>City Engineering Division – Mapping Section</u> (Contact Julius Smith, (608) 264-9276)

- 6. Pending the final design for the public sidewalk improvements, a public sidewalk easement may be required for this project to protect existing trees. If required, this may be done with so on the face of the CSM. Contact Jule Smith (jsmith4@cityofmadison.com) for the required language to be included on the face of the CSM.
- 7. Any portion(s) of a public easement that is intended to be released shall be released by separate document prepared by City Office of Real Estate Services. Contact Jule Smith of Engineering-Mapping (jsmith4@cityofmadison.com, ((608) 264-9276) to coordinate the Real Estate project, and associated information and fees required. If any release is required prior to recording of the plat, acknowledgement of the release and document number shall be noted on the face of the plat. Provide fee legal description and exhibit for the portions sanitary sewer easements Document Nos. 1275466 and 1275467 that are being requested to be released with the development and shown on the CSM. The final area to be released are to be approved by the City Engineering Division.
- 8. The applicant shall dedicate the existing 33 feet of right of way and an additional 7 feet for Old Sauk Road as shown on the CSM.
- 9. Coordinate and request from the utility companies serving this area the easements required to serve this development. Those easements shall be properly shown, dimensioned and labeled on the final CSM.
- 10. Update the Madison Common Council certificate to include the current standard language as required by the Office of Real Estate Services. This will include the required acceptance language for the dedications included in the CSM.
- 11. Wisconsin Administrative Code A-E 7.08 identifies when Public Land System (PLS) tie sheets must be filed with the Dane County Surveyor's office. The Developer's Surveyor and/or Applicant must submit copies of required

tie sheets or monument condition reports (with current tie sheet attached) for all monuments, including center of sections of record, used in this survey, to Julius Smith, City Engineering (jsmith4@cityofmadison.com).

- 12. In accordance with Section s. 236.18(8), Wisconsin Statutes, the applicant shall reference City of Madison WCCS Dane Zone, 1997 Coordinates on all PLS corners on the Certified Survey Map in areas where this control exists. The surveyor shall identify any deviation from City Master Control with recorded and measured designations.
- 13. Prior to Engineering final sign-off by main office for Certified Survey Maps, the final CSM shall be submitted in PDF format by email transmittal to Engineering Land Records Coordinator Jule Smith (jsmith4@cityofmadison.com) for final technical review and approval. This submittal must occur a minimum of two working days prior to final Engineering Division sign-off.
- 14. Old Sauk Road does not vary. Remove 'Varies' from the description of the existing right of way.
- 15. Make the building labels and dimensions legible on sheet 3 of 5.
- 16. Correct the area for Lot 1. The overall lands appear to be listed for Lot 1 with the dedications included. Lot 1 should be about 161,000 square feet, +/- 3.70 acres.
- 17. Confirm the proper wording and page numbers on Note 6 on Sheet 1, Note 3 on Sheet 3, and Note 2 on Sheet 3.
- 18. Revise Sheet 3 to be Sheet 3 of 6.
- 19. The pending Certified Survey Map application for this property shall be completed and recorded with the Dane County Register of Deeds, the new parcel data created by the Assessor's Office, and the parcel data available to Zoning and Building Inspection staff prior to issuance of building permits for new construction or an early start permits for new construction.
- 20. The applicant shall submit to Jeff Quamme, prior to final Engineering sign-off of the subject CSM, one (1) digital CADD drawing in a format compatible with AutoCAD. The digital CADD file(s) shall be referenced to the Dane County Coordinate System and shall contain, at minimum, the list of items stated below, each on a separate layer/level name. The line work shall be void of gaps and overlaps and match the final recorded CSM: right of way lines (public and private); lot lines; lot numbers; lot/plat dimensions; street names, and; easement lines (including wetland and floodplain boundaries).
 - *This transmittal is a separate requirement than the required submittals to Engineering Streets Section for design purposes. The Developer/Surveyor shall submit new updated final plat, electronic data and a written notification to Engineering Mapping for any changes to the plat which occur subsequent to any submittal.

<u>Traffic Engineering Division</u> (Contact Sean Malloy, (608) 266-5987)

21. The applicant shall dedicate seven (7) feet of right of way along their frontage of Old Sauk Road for a total of 40 feet from the centerline.

Zoning Administrator (Contact Jenny Kirchgatter, (608) 266-4429)

This agency has reviewed the request and recommended no conditions of approval.

Fire Department (Contact Bill Sullivan, (608) 261-9658)

This agency has reviewed the request and recommended no conditions of approval.

Water Utility (Contact Jeff Belshaw, (608) 261-9835)

This agency has reviewed the request and recommended no conditions of approval.

Metro Transit (Contact Tim Sobota, (608) 261-4289)

This agency has reviewed the request and recommended no conditions of approval.

Parks Division (Contact Ann Freiwald, (608) 243-2848)

This agency has reviewed the request and recommended no conditions of approval.

Office of Real Estate Services (Andy Miller, (608) 261-9983)

- 22. Prior to approval sign-off by the Office of Real Estate Services ("ORES"), the Owner's Certificate(s) on the CSM shall be executed by all parties of interest having the legal authority to do so, pursuant to Wis. Stats. 236.21(2)(a). Said parties shall provide documentation of legal signing authority to the notary or authentication attorney at the time of execution. The title of each certificate shall be consistent with the ownership interest(s) reported in the most recent title report. When possible, the executed original hard stock recordable CSM shall be presented at the time of ORES approval sign-off. If not, the City and the Register of Deeds are now accepting electronic signatures. A PDF of the CSM containing electronic signatures shall be provided to ORES to obtain approval sign-off.
- 23. Prior to CSM approval sign-off, an executed and notarized or authenticated certificate of consent for all mortgagees/vendors shall be included following the Owner's Certificate(s). If a mortgage or other financial instrument is reported in record title, but has been satisfied or no longer encumbers the lands or ownership within the CSM boundary, a copy of a recorded satisfaction or release document for said instrument shall be provided prior to CSM approval sign-off.
- 24. As of May 31, 2024, real estate taxes are paid for the subject property. Per 236.21(3) Wis. Stats. and MGO Section 16.23(4)(f), the property owner shall pay all real estate taxes that are accrued or delinquent for the subject property prior to CSM recording. Receipts from the City of Madison Treasurer are to be provided before or at the time of sign-off.
- 25. As of May 31, 2024, there are no special assessments reported. All known special assessments are due and payable prior to CSM approval sign-off. If special assessments are levied against the property during the review period and prior to CSM approval sign-off, they shall be paid in full pursuant to MGO Section 16.23(4)(f).
- 26. A standard 60-year report of title was not received by Office of Real Estate Services with the CSM application materials. Pursuant to MGO Section 16.23(4)(f), the owner shall furnish a 60-year title report via email to Andy Miller in the ORES (acmiller@cityofmadison.com), as well as the survey firm preparing the proposed CSM. The

report shall include copies of all associated documents of record. A title commitment may be provided, but will be considered only as supplementary information to the title report update. The surveyor shall update the CSM with the most recent information reported in the title update. ORES reserves the right to impose additional conditions of approval in the event the title report contains changes that warrant revisions to the CSM.

- 27. Depict, dimension, name, note and/or identify by document number all relevant easements, declarations, plans, conditions, agreements, and other documents cited in record title and the updated title report, and include relevant notes from plats or CSMs of record. If documents included in the December 23, 2019 title report do not apply to the area within the proposed CSM, have them removed from the updated title report.
- 28. Depict and dimension public easements for utilities and storm water drainage rights of way to be dedicated on the proposed CSM where necessary.

From: Chuck Nahn

To: All Alders; Figueroa Cole, Yannette; Guequierre, John; Mayor; Plan Commission Comments

Cc: Mary Umbeck; jeff western; Fries, Gregory; Schmidt, Janet; Tim Burns

Subject: Please Post as Public Comments for 82950, 82972, 83477, 82979 and 84123, 6610-6706 Old Sauk Rd

Date: Friday, July 5, 2024 1:34:13 PM

Some people who received this message don't often get email from chucknahn@gmail.com. <u>Learn why this is important</u>

Caution: This email was sent from an external source. Avoid unknown links and attachments.

Dear Mayor Rhodes-Conway, President Cole and All Alders,

My name is Chuck Nahn and I reside at 5623 Sandhill Drive in Middleton. I am a registered Civil Engineer, with over 40 years of experience, retained by the adjacent neighboring property owners to review and comment on the Stormwater Management and Erosion Control Plan for the Old Sauk Road Apartments.

My overall stormwater review of this development is that a high-density multi-family residential development with corresponding greatly increased paved surfaces is being proposed into a small undeveloped area with existing flooding problems caused primarily by inadequate storm sewer infrastructure along Old Sauk Road. To meet City ordinances and achieve the high-density development, the developer is implementing novel, untested underground practices to meet the runoff rate, water quality, infiltration and oil and grease requirements of the City ordinance. I have a number of concerns as detailed in my review comments based on two revisions of the stormwater plan dated April 8, April 22 and May 24 including but not limited to:

- Underground Tank Infiltration Rate
 - o The infiltration rates used in the report are too high and do not have a correction factor applied to account for soil compaction during construction. Please note the design infiltration rate is integral toward meeting City ordinance for runoff rate control, water quality and infiltration requirements.
 - o Soil compaction during construction is inevitable based on the weight of rock and concrete vault structure on top of native soil interface for underground tanks.
 - o Mixing the soils 5 feet below the native soil interface will not increase infiltration based on Dr. John Norman's (professor emeritus of soil science) comments.
 - Sodium Chloride used for winter deicing of street, driveway and parking lot may cause soil clogging and immediate infiltration failure based on Dr. Norman's comments.
- Pre-existing Detention not applied to on-site discharge- City ordinance requires pre-existing detention applied to on-site discharge. Stormwater plan applies pre-existing detention to off-site discharge from Old Sauk Road flooding and not on-site discharge from paved area increase associated with proposed development.
- Potential Increased Flooding to Lower basements for North Property Owners-Underground Tank infiltration can potentially cause groundwater mounding and increased groundwater flow to the north inundating northern property owner's household lower level and basement. Please note these basements are 7 feet below the native soil interface of Underground Tank #1 which is located 40 feet from the native soil interface.
- Proposed Underground Tank Outflow pipes elevations- If underground infiltration tanks should not infiltration as designed, the outflow pipe elevation will negate 3/4 of the existing storage of the underground tanks.

I have numerous additional stormwater management plan comments that I submitted to City Engineering on June 4, 2024 with no response received. I request an in-person meeting with City Engineering and the developer's engineer to review these additional issues. Given the uncertainties

that exist at this time, we ask that you defer a decision on the zoning change until further detail becomes available regarding the proposed stormwater practices for this development. The risk of increasing flooding in an already flooded area if these practices do not perform as designed definitely should be considered in more detail before a decision to change the zoning and demolish existing structures is made. For example, if the underground tanks remain filled with water, flood protection volume is lost which is needed to protect downstream property owners

Thank you for your consideration of these issues.

Chuck Charles E. Nahn III, P.E. Nahn and Associates 5623 Sandhill Drive Middleton WI 53562 (608) 712-9199



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83714

File ID: 83714 File Type: License Status: Public Hearing

Version: 1 Reference: Controlling Body: ALCOHOL

LICENSE REVIEW

COMMITTEE

File Created Date: 05/29/2024

File Name: Final Action:

Title: Public Hearing - New License

Handyspot 108 LLC • dba Handyspot 108 LLC 5551 Eastpark Blvd • Agent: Harvinder Singh

Class A Beer

Police Sector 520 (District 17)

passed by voice vote/other.

Notes:

Sponsors: Effective Date:

Attachments: LICLIA-2024-00290 App.pdf, LICLIA-2024-00290 Enactment Number:

Supplemental.pdf, 5551 Eastpark Blvd map.pdf

Author: Hearing Date:

Entered by: jverbick@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:		
1	Clerk's Office	05/29/2024	Referred for						
	Action Text: Notes:	This License was Referr Alcohol License Review Co	ed for Introduction mmittee- Public Hearing (6/26	5/24), Common Council (7/2/	24)				
1	COMMON COUN	NCIL 06/04/2024	Refer For Public Hearing	ALCOHOL LICENSE REVIEW COMMITTEE			Pass		
	Action Text:	•	igueroa Cole, seconded b VIEW COMMITTEE. The	•	•	the			
1	ALCOHOL LICEN REVIEW COMMI		RECOMMEND TO COUNCIL TO RE-REFER - PUBLIC HEARING	ALCOHOL LICENSE REVIEW COMMITTEE			Pass		
	Action Text: A motion was made by Donnelly, seconded by Barushok, to RECOMMEND TO COUNCIL TO								

RE-REFER - PUBLIC HEARING to the ALCOHOL LICENSE REVIEW COMMITTEE. The motion

1 COMMON COUNCIL 07/02/2024 Re-refer for ALCOHOL Pass

Recessed Public LICENSE
Hearing REVIEW
COMMITTEE

Action Text: A motion was made by Figueroa Cole, seconded by Myadze, to Re-refer for Recessed Public Hearing

to the ALCOHOL LICENSE REVIEW COMMITTEE special meeting of 7/11/24. The motion passed by

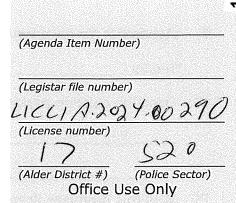
voice vote/other.

Text of Legislative File 83714

Title

Public Hearing - New License Handyspot 108 LLC • dba Handyspot 108 LLC 5551 Eastpark Blvd • Agent: Harvinder Singh Class A Beer Police Sector 520 (District 17)





Class A: Beer, Liquor, Cider Cider Class B: Reer, Liquor

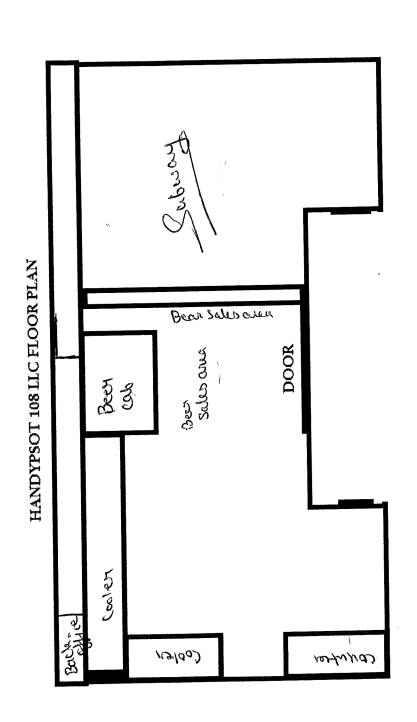
Clas	ss B: ☐ Beer, ☐ Liquor, <u>licensing@cityofmadison.com</u> ☐ Class C Wine 608-266-4601
Sec 1.	List the name of your Sole Proprietor, Partnership, Corporation/Nonprofit Organization or Limited Liability Company exactly as it appears on your State Seller's Permit.
	HANDYSTOP 108 LLC (HANDYSPOT, 108 LLC)
2.	Trade Name (doing business as) HANDY SPOT 108 LLC
3.	Address to be licensed 5551, East Park Blvd, Machinon, WT 53718
4.	Mailing address 5551, East Park Blvd, Machinon, WI 53718
5.	Anticipated opening date $06/23/2023$
6.	Is the applicant an employee or agent of, or acting of behalf of anyone except the applicant named in question 1? No Yes (explain)
7.	Does another alcohol beverage licensee or wholesale permitee have interest in this business? \square No \square Yes (explain)
Sec	tion B—Premises
8.	Describe in words the building or buildings where alcohol beverages are to be sold and stored. Include all rooms including living quarters, if used, and any outdoor seating used for the sales, service, and/or storage of alcohol beverages and receipts. Alcohol beverages may be sold and stored only on the premises as approved by Common Council and described on license.
3 ca	MY WALKTNG COOLER, SALES AREA AT STORE AND
tat	194 WALKTNIG COOLER, SALEC AREA AT STORE AND BE BACKROON AT SSSI, EAST PIARK Blvd, MADISON.
	•

9.	Applicants for on-	premises const	иприон оту	y. Estimated capacity	y (patrons and employees).			
	Indoor:		Outdoor: _		N/A			
10.				ot is to be monitored				
	FRONT	OF STORE	AND	CIBROTINOM	BY SURVILLANCE			
	CAME	RA.						
11.	Was this premises	licensed for th	ne sale of liq	luor or beer during t	he past license year?			
	□ No □ Yes,	license issued t	ONA 1-1 0:	YS107 108 LL	(name of licensee)			
This	ction C—Corpora section applies to c. Sole proprietorsh	corporations, n	nonprofit org		ited Liability Companies			
12.	Name of liquor lic	ense agent	HARVIL	HONES A3DE				
				ICFARLAND				
					consin? 10 months			
	Has the liquor license agent completed the responsible beverage server training course?							
	\square No, but will complete prior to ALRC meeting \square Yes, date completed \square \square Yes, date \square Yes, date completed \square Yes, date \square Yes,							
16.				, nonprofit organizat	1 /			
	wIS CON	STN C	06/23/201	<u> </u>				
17.	In the table below	list the directo	ors of your c	corporation or the modified or director/member.	embers of your LLC.			
	Title	Name		City and State of Re				
	OWNER	HARVINI	DER SINGH	MCFARLAND FORT ATKEN	IW, U			
	OWNER	KULTITPP	WL SINGH	FORT AT KIN	STO N, WI			
18.	notice or demand necessarily the sa	required or pe	rmitted by la uor agent.	aw to be served on t	for service of process, the corporation. This is not			
	Is applicant a sub ☑ No ☐ Yes (•	•					
20.					liquor agent, LLC, any beverage license or permit			
	No D Yes (explain)						

	tion D—Bus What type of □ Tavern	establishmer	nt is contemp	olated? urant 🏻 Liqu	uor Store 🛭] Grocery St	core
	☐ Convenie	nce Store wit	hout gas pur	nps 🛭 Conv	enience Store	e with gas pu	ımps
	□ Other						
22.	. Private organizations (clubs): Do your membership policies contain any requirement of "invidious" (likely to give offense) discrimination in regard to race, creed, color, or national origin? No Yes						
23.	Hours of ope	ration: please	e enter openi	ng and closing	times in the	table below.	
	Sunday			Wednesday			
	5:0000x 10:01pm	recent lolm	San - 10/24	Sam-lopui	sam 10pm	Sam - 10pm	Sam - 10pm
	(Class B on	ly) Enter belo	ow any hours	when food ser	vice will not b	pe available,	if applicable
	-	-	-	-	-		-
This (cor	and alcohol beverage sales broken down by percentage. (Note: Non-alcoholic drinks are classified as "Food.") New establishments estimate percentages: % Alcohol % Food % Other						
	If applicable, describe "Other":						
26.	Do you plan	to have live ϵ	entertainmen	t? ☑ No □	Yes—what k	ind?	
	dance floor,	please also co	omplete an E	music (except ntertainment L), a DJ, or a (designated
Sec 27.	tion F—Req I understand regardless of	that liquor/b	eer license r	i lings enewal applicat granted. 🏻 N	cions are due lo ⁄ Yes	April 15 of e	very year,
28.		that I am red g. □ No ↓E		t an informatio	n session at l	least one wee	ek before the
29.				this location to sion. 🗖 No 🞣		pplication ar	d to invite

30.	I agree to contact the Police Department District Captain for this location prior to the ALRC meeting. \Box No \Box Yes
31.	I agree to contact the Deputy Clerk prior to the ALRC meeting. No Yes
32.	I agree to contact the neighborhood association representative prior to the ALRC meeting. \Box No $\sqrt{\Box}$ Yes
33.	I intend to operate under the alcohol license within 180 days of the Common Council granting this license. The license shall be considered surrendered if not issued within 180 days of being granted. \Box No \Box Yes
34.	I understand we must file a Special Occupational Tax return (TTB form 5630.5) before beginning business. [phone 1-800-937-8864] \square No \square Yes
35.	I understand a Wisconsin Seller's Permit must be applied for and issued in the same name as that shown in question 1, above. [phone 608-266-2776] \square No \square Yes
36.	Is the applicant indebted to any wholesaler beyond 15 days for beer or 30 days for liquor? No \square Yes
Sec	tion G—Information for Clerk's Office
37.	This application is for the license period ending June 30, 20 14.
38.	State Seller's Permit 4 5 6 - 1 0 3 1 4 4 1 8 7 5 0 4
	Federal Employer Identification Number 93-1509389
40.	Who may we contact between 8 a.m. and 4:30 p.m. regarding this license?
	Contact person HAR VINDER STNGH
	Business phone 269-605-8005 Business e-mail address HANDYSPUT 108@GMHAIL.(0)
	Preferred language \{NGLISH
•	If needed, a qualified interpreter can be provided at no charge to you. Would you like an interpreter? Yes (language:) No (If you answer no and you do require an interpreter, the ALRC will refer your application to a subsequent meeting and this may delay your application process)
	Si usted requiere o necesita un/a intérprete, nosotros podemos proveer un/a intérprete sin costo alguno. ¿Le gustaría tener un/a intérprete? Sí, lenguaje: No. Si ustad cosago "no" on la policitud (antiención cosago") cosago "no" on la policitud (antiención cosago "no" on la policitud
	☐ No. Si usted escoge "no" en la solicitud/aplicación, y usted sí requiere un/a intérprete, el comité remitirá su solicitud para una nueva junta y esto puede atrasar el proceso de su solicitud.
41.	Corporate attorney, if applicable: Name
	Phone E-mail

NOTICE: Completed application are due by noon of the following to get on the agenda for the proceeding months must be accompanied by the following items:	third Monday (fourth, if the Clerk's office is s Alcohol License Review Committee. A com	closed on the third npleted application			
Copy of State Seller's Permit (Not Business Tax Regis	atration Cartificate) Wannaintment of Ag	ent (if Corn/LLC)			
Member background investigation forms. Articles					
		alis,			
Copy of Lease, Wasiness Plan, and Ample Me	enu (ir applying for Class B license)				
If required items are missing, the application will not be Office until all requirements are submitted. No exception		ed by the Clerk's			
Read carefully before signing: Under penalty provided been truthfully completed to the best of the knowledge of to law, and that the rights and responsibilities conferred Lack of access to any portion of licensed premises during Such refusal is a misdemeanor and grounds for revocation	of the signer. Signer agrees to operate the l by the license(s), if granted, will not be ass n inspection will be deemed a refusal to per	business according signed to another.			
Penalty for materially false application information: Any on this application may be required to forfeit not more the		false information			
(Officer of Corporation/Member of LLC/Partner/Sole Proprietor)	05 02 2024 (Date)				
Clerk's Office checklist for complete applications					
☑ WI Seller's Permit Certificate ☑ Backgr	round investigation form(s)	Floor Plans			
	or surrender of previous license	Lease			
(incorporation) / / *Articl		Business Plan			
	intment of Agent	3 **Sample Menu			
✓ Written description of premises* Corporation	ation/LLC only **	* Class B only			
Upon Application Submission, the Clerk's Off	ice issued to the application:				
☐ Orange sign ☐ Orange business card					
☐ "Applying for a Liquor/Beer License in the City of Madison" brochure with contact information					
111, 2					
Date complete application filed with Clerk's Office	of Madison" brochure with contact info	rmation			
Date complete application filed with Clerk's Office Date of ALRC meeting Date license gr		rmation 			



HANDYSPOT 108 LLC BUSINESS PLAN

Prepared by:

5551 EAST PARK BLVD

MADISON, Wisconsin 53718

608-284-9579

HANDYSPOT108@GMAIL.COM

L EXECUTIVE SUMMARY

HANDYSPOT 108 LLC (referred to from hereon in as the "Company") was established as a Limited Liability Company at 5551 EAST PARK BLVD, MADISON, Wisconsin 53718 with the expectation of rapid expansion in the gas station & convience store industry.

Business Description

The Company was formed on 06/01/2023 as Limited Liability Company under Wisconsin state laws and headed by

The Company currently employs 2 full-time employees and part-time employees.

New Service

The Company is prepared to introduce the following service to the market:

CONVINENCE STORE: GAS, PACKAGE FOOD, BEER, POP







II. BUSINESS SUMMARY

Industry Overview

In the United States, the gas station & convience store industry presently makes 1,580,000.00 dollars in sales.

Research shows that consumers in this industry primarily focus on the following factors when making purchasing decisions:

Legal Issues

The Company affirms that its promoters have acquired all legally required trademarks and patents.

III. MARKETING SUMMARY

Target Markets

The Company's major target markets are as follows:

MADISON AREA

MADISON AREA

The estimated number of potential clients within the Company's geographic scope is 120.00.

Competition

In	the	gas	station	&	convience	store	industry,	customers	make	choices
ba	sed t	upon	STATE OF THE PARTY				•			

The level of competition is HIGHLY COMPETITIVE

The primary competitors for the business are the following: KWIK TRIP.

Services

First-rate service is intended to be the focus of the Company and a cornerstone of the brand's success. All clients will receive conscientious, one-on-one, timely service in all capacities, be they transactions, conflicts or complaints. This is expected to create a loyal brand following and return business.

June 2024 ALRC New License

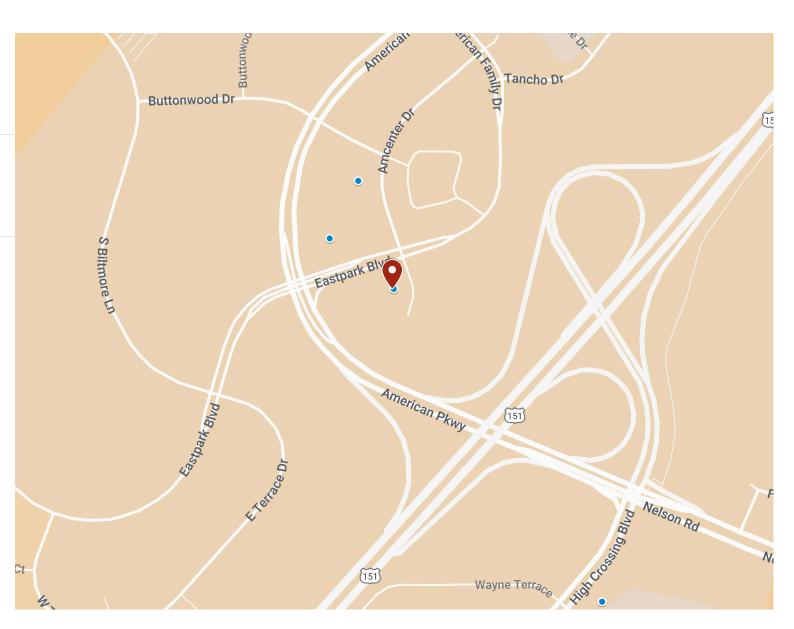
Existing Licenses.xlsx

All items

New Licenses.xlsx

All items

New licenses are drops, existing licenses are dots.





City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84234

File ID:84234File Type:LicenseStatus:Public Hearing

Version: 1 Reference: Controlling Body: ALCOHOL

LICENSE REVIEW

COMMITTEE

File Created Date: 07/01/2024

File Name: Final Action:

Title: Public Hearing - New License

Sriram Retails 3 LLC • dba Regent Street Mini Mart

1401 Regent St • Agent: Meenu Kaushal Estimated Capacity (in/out): 25 total

Class B Combination Liquor & Beer • 30% alcohol, 60% food, 10% other

Police Sector 205 (District 13)

Notes:

Sponsors: Effective Date:

Attachments: LICLIB-2024-00277 Updated App.pdf, Enactment Number:

LICLIB-2024-00277 App.pdf, LICLIB-2024-00277 Supplemental pdf, 1401 Regent St map pdf, Alder

Supplemental.pdf, 1401 Regent St map.pdf, Alder Evers Comments.pdf, 83198_VNA

Comments.pdf, Outdoor capacity change.pdf,
Outdoor seating area.pdf, VNA Comments.pdf,

Carlson Comments.pdf

Author: Hearing Date:

Entered by: jverbick@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Clerk's Office Action Text: Notes:	This License was Referre	Referred for Introduction ed for Introduction mmittee- Public Hearing (7/1	1/24). Common Council (7/16	5/24)		
	Notes.	Alcohol Licelise Neview Col	minitee-1 ablic Healing (771	1/24), Common Council (1/10	<i>1124)</i>		
1	COMMON COUN	ICIL 07/02/2024	Referred for Public Hearing	ALCOHOL LICENSE REVIEW COMMITTEE			Pass
	Action Text:	•	igueroa Cole, seconded l VIEW COMMITTEE. The	•	•	o the	

Text of Legislative File 84234

Master Continued (84234)

Title

Public Hearing - New License
Sriram Retails 3 LLC • dba Regent Street Mini Mart
1401 Regent St • Agent: Meenu Kaushal
Estimated Capacity (in/out): 25 total
Class B Combination Liquor & Beer • 30% alcohol, 60% food, 10% other
Police Sector 205 (District 13)



MILE Class B: & Beer, Liquor,

Class A: ☐ Beer, ☐ Llquor, ☐ Cider

Liquor/Beer License Application

City of Madison Clerk 210 MLK Jr Blvd, Room 103 Madison, WI 53703

licensing@cltvofmadlson.com

(Agenda Item	Numbe	()	SEE SEE	200
	Parket			
(Legistar file n	umber)	No. Service	SHAP TO	300
LICL	13-	202	4-0	727
(Ucense numb	er)	DAME.	E490	375
13		20	5	
	100000	/		- 1

	Class C Wine 608-266-4601
Se 1.	List the name of your Sole Proprietor, Partnership, Corporation/Nonprofit Organization or Limited Liability Company exactly as it appears on your State Seller's Permit.
2. 3. 4.	SRIRAM Retails 3 LLC Trade Name (doing business as) Referred Street Mini Mart, Regent Now & Cofe Address to be licensed 1401 Regent Street Modes on WI 5371] Mailing address 1077 Gas Light Dr Sun prawle WI 5359
5.	Is the applicant an employee or agent of, or acting of behalf of anyone except the applicant named in question 1? Yes (explain)
	Does another alcohol beverage licensee or wholesale permitee have interest in this business? No

license. building. Outdoor 8 they requested Cafe & hat food for customers. Glocery

stored. Include all rooms including living quarters, if used, and any outdoor seating used for the sales, service, and/or storage of alcohol beverages and receipts. Alcohol beverages may be sold and stored only on the premises as approved by Common Council and described on



Liquor/Beer License **Application**

(Agenda Item Number) (Legistar file number) (License number) (Alder District #) office Use Only

City of Madison Clerk
210 MILE 1 DI 1 D

Cla

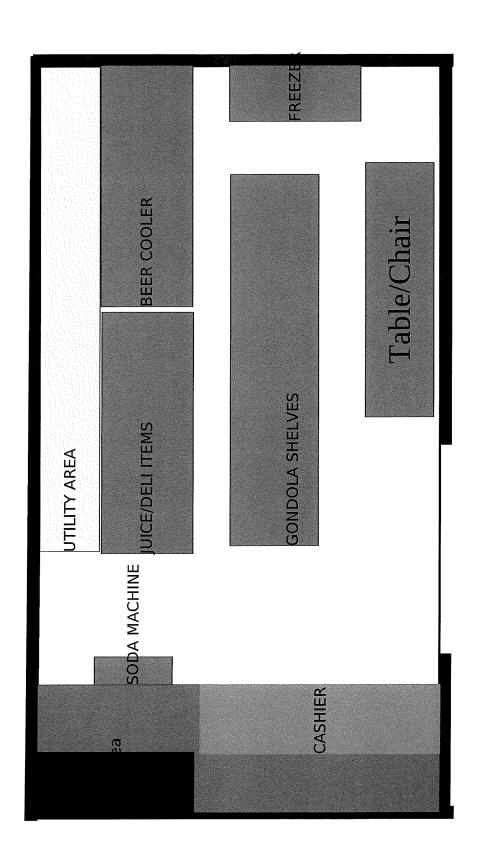
☐ Class C Wine licensing@citvofmadIson.com 608-266-4601	
ection A – Applicant List the name of your Sole Proprietor, Partnership, Corporation/Nonprofit Organization or Limited Liability Company exactly as it appears on your State Seller's Permit. SRIRAM Retails 3 LLC	
Mailing address 1077 Gas Lyft Dr Sun Drawle W1 5371]	_
Is the applicant an employee or agent of, or acting of behalf of anyone except the applicant named in question 1? Does another alcohol beverage licensee or wholesale permitee have interest in this business? No	E
the sales, service, only or storage or account neverages and receipts. Alack of the	
	Ection A - Applicant List the name of your Sole Proprietor, Partnership, Corporation/Nonprofit Organization or Limited Liability Company exactly as it appears on your State Seller's Permit. SRRAM Retails 3 L.C Trade Name (doing business as) Refeat Street Modes in No. 1 & 371] Address to be licensed 401 Refeat Street Modes in No. 1 & 371] Mailing address 1071 Gas Lust Dr Sun Dyambo in 5 & 371] Mailing address 1071 Gas Lust Dr Sun Dyambo in 5 & 371] Is the applicant an employee or agent of, or acting of behalf of anyone except the applicant named in question 1? Does another alcohol beverage licensee or wholesale permitee have interest in this business? D No Yes (explain) Tion B—Premises Describe in words the building or buildings where alcohol beverages are to be sold and stored. Include all rooms including living quarters, if used, and any outdoor seating used for the sales, service, and/or storage of alcohol beverages and receipts. Alcohol beverages may be sold and stored only on the premises as approved by Common Council and described on license. 3200 Square for bouldary Outdoor Setting Agrumed Indigor Williams Indian Agrum Agruma Agru

Applicants for on-premises consumption only. Estimated capacity (patrons and employees):
Indoor: 0 0 Outdoor: 0 0
10. Describe existing parking and how parking lot is to be monitored.
Security Monetary Cameras are installed - To Yarley Staces
11. Was this premises licensed for the sale of liquor or beer during the past license year?
No Ves, license issued to Shiking letails 3 LLC (name of licensee)
Section C—Corporate Information This section applies to corporations, nonprofit organizations, and Limited Liability Companies only. Sole proprietorships and partnerships, skip to Section D.
12. Name of liquor license agent Menukaunal
13. City, state in which agent resides Sun rame WI
14. How long has the agent continuously resided in the State of Wisconsin? 20 1000
15. Has the liquor license agent completed the responsible beverage server training course?
□ No, but will complete prior to ALRC meeting □ Yes, date completed 1+2-2022
16. State and date of registration of corporation, nonprofit organization, or LLC. W - 2-2022
17. In the table below list the directors of your corporation or the members of your LLC. Attach background check forms for each director/member. Title Name City and State of Residence Owner Member Mcente Koundel Sunframe W 1 Owner Member Neeray Kauskal Sangawe W 1
18. Registered agent for your corporation or LLC. This is your agent for service of process, notice or demand required or permitted by law to be served on the corporation. This is not necessarily the same as your liquor agent. Menu Kauku
19. Is applicant a subsidiary of any other corporation or LLC? □ No □ Yes (explain)
20. Does the corporation, any officer, any director, any stockholder, liquor agent, LLC, any member, or any manager hold any interest in any other alcohol beverage license or permit in Wisconsin?
No DY Yes (explain) SRIRAM Retail LLC

	tion D—Busi What type of Tavern	establishmer		ilated? urant 🛭 Liqu	Jor Store	Grocery SI	tore		
	☐ Convenience Store without gas pumps ☐ Convenience Store with gas pumps								
	□ Other								
22.	"invidious", (l	Private organizations (clubs): Do your membership policies contain any requirement of "Invidious" (likely to give offense) discrimination in regard to race, creed, color, or national origin? No Yes							
23.	Hours of operation: please enter opening and closing times in the table below.								
	Sunday	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday		
	8am-11Pm	80m - 11Pm	8am - 11 Pm	89m-11PA	80m - 11 Pm	8am-11Pm	Ram-IHM		
				when food ser					
	8pm - 8 (Pm	8Pm - 11Pm	8/m - 11 Pa	8 Pm - 11 Pm	8Pm -11 Am	8Pm - 11Pm	-		
Thi (co	Section E—Consumption on Premises This section applies to Class B and Class C applicants only. Class A license applicants (consumption off premises) may skip to Section F. 24. Indicate any other product/service offered. Cale Cale Not food but the Smarthum.								
24.	Indicate any	other produc	t/service offe	ered	Coffee Ho	14 9000 / PI	hm.		
25.	25. All restaurants and taverns serving alcohol must substantiate their gross receipts for food and alcohol beverage sales broken down by percentage. (Note: Non-alcoholic drinks are classified as "Food.") New establishments estimate percentages:						drinks are		
	Do you have written records to document the percentages shown? \(\simega\) No \(\simega\) Yes You may be required to submit documentation verifying the percentages indicated.								
26.	26. Do you plan to have live entertainment? No 🗆 Yes—what kind?								
If planned entertainment includes live music (except solo acoustic), a DJ, or a designated dance floor, please also complete an Entertainment License. Section F—Required Contacts and Filings 27. I understand that liquor/beer license renewal applications are due April 15 of every year, regardless of when license was initially granted. No Yes 28. I understand that I am required to host an information session at least one week before the									
29.	I agree to co	gree to contact the Alderperson for this location to discuss my application and to invite Alderperson to my information session. No Yes							
			F	Page 3 of 5					

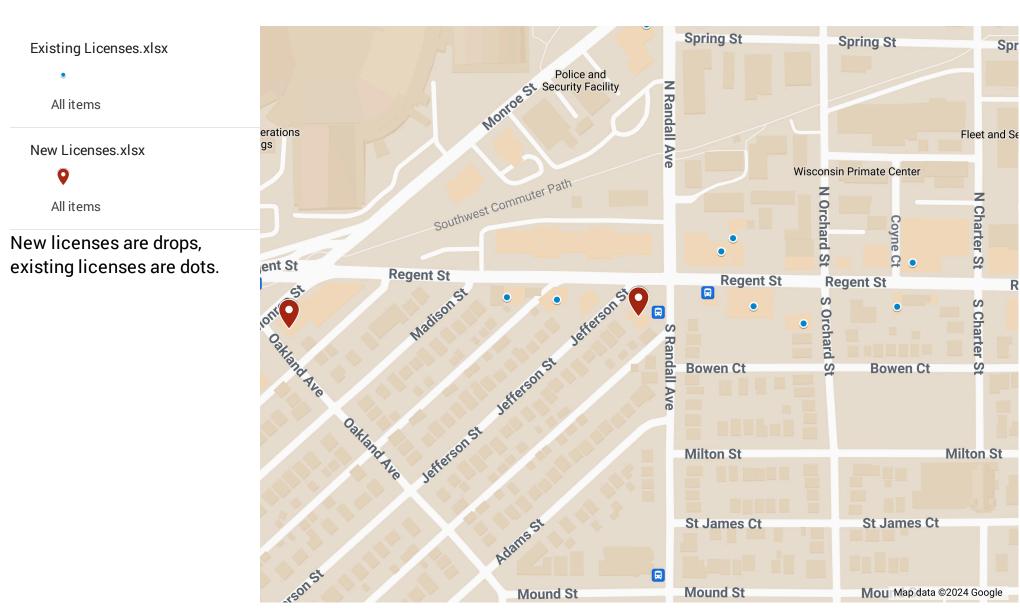
	I agree to contact the Police Department District Captain for this location prior to the ALRC meeting. No. Yes						
0.	Tagree to contact the Police Department District Coperation of th						
1.	I agree to contact the Deputy Clerk prior to the ALRC meeting. ☐ No ☑ Yes						
32.	I agree to contact the neighborhood association representation re						
	□ No ☑ Yes I intend to operate under the alcohol license within 180 days of the Common Council granting this license. The license shall be considered surrendered if not issued within 180 days of being granted. □ No ☑ Yes						
34.	I understand we must file a Special Occupational Tax return (118 lotti 300 l						
35.	I understand a Wisconsin Seller's Permit must be applied for and issued in the selection of						
36.	as that shown in question 1, above: [phone of some larger of some						
Sec	ction G—Information for Clerk's Office						
	the linear period ending June 30, 20, 23						
38	State Seller's Permit 4 5 6 - 1 0 3 0 0 1 0 0 0 1 0 0						
30.	Federal Employer Identification Number 874 403 235						
40	. Who may we contact between 8 a.m. and 4:30 p.m. regarding this license?						
40.	Contact person Meeny Kaushal						
	Business phone 6 08 36/ 8529 Business e-mail address Mceny ksh/ 2 mail. Gm						
	Preferred language						
	If needed, a qualified interpreter can be provided at no charge to you. Would you like an interpreter?						
	Yes (language:) No (If you answer no and you do require an interpreter, the ALRC will refer your application to a subsequent meeting and this may delay your application process)						
	Si usted requiere o necesita un/a intérprete, nosotros podemos proveer un/a interprete sin costo alguno. ¿Le gustaría tener un/a intérprete?						
	 Sí, lenguaje:						
41.	Corporate attorney, if applicable: Name						
	Phone E-mail						
	Page 4 of 5						

NOTICE: Completed application are due by Monday) to get on the agenda for the proc must be accompanied by the following item	y noon of the third Monday (fourth, if the Cierk's offi eeding months Alcohol License Review Committee. A ns:	ice is closed on the third A completed application
Member background investigation form Copy of Lease, D Business Plan, and If required items are missing, the application	ness Tax Registration Certificate), Appointment on	or Plans,
been truthfully completed to the best of the	enalty provided by law, the applicant states that the e knowledge of the signer. Signer agrees to operate ties conferred by the license(s), if granted, will not b remises during inspection will be deemed a refusal to	e assigned to another.
on this application may be required to forf	1 Kaushel 3-20-24	
(Officer of Corporation/Member of LLC/Partner/ Clerk's Office checklist for complete		WG-00-0
 □ WI Seller's Permit Certificate (matching articles of incorporation) □ FEIN □ Written description of premises 	 □ Background investigation form(s) □ Form for surrender of previous license □ *Articles of Incorporation □ *Appointment of Agent * Corporation/LLC only 	☐ Floor Plans ☐ Lease ☐ Business Plan ☐ **Sample Menu ** Class B only
☐ Orange sign ☐ Orange busines	Clerk's Office issued to the application: is card e in the City of Madison" brochure with contact	information
Date complete application filed with Clerk	's Office	
Date provisional issued	Pate license granted by Common Council Pate license Issued	mide.
A STATE OF THE STA		





May 2024 ALRC New License



licensing

From: Evers, Tag

Sent: Tuesday, May 14, 2024 7:08 PM

To: Verbick, Jim; Esser, Gregory; Freedman, Jason
Cc: licensing; Doug Carlson; meenukaushal@gmail.com

Subject: Re: New Alcohol License for May ALRC - Sriram Retails 3 LLC

Addendum:

I am aware the Turkish Kitchen application last month also did not contact me, but they did meet with Dudgeon-Monroe Neighborhood Association.

And that application was not asking for outdoor seating, while this one does.

That's an important difference and is the main reason I am requesting referral.

Thanks.

TE

From: Evers, Tag <district13@cityofmadison.com>

Sent: Tuesday, May 14, 2024 6:12 PM

To: Verbick, Jim <JVerbick@cityofmadison.com>; Esser, Gregory <GEsser@cityofmadison.com>; Freedman, Jason <JFreedman@cityofmadison.com>

Cc: licensing < licensing@cityofmadison.com>; Doug Carlson < dcarlson5dc@aim.com>; meenukaushal@gmail.com

<meenukaushal@gmail.com>

Subject: Re: New Alcohol License for May ALRC - Sriram Retails 3 LLC

Hi all,

Please note the applicant, Meenu Kaushal, has not contacted me regarding this application.

Moreover, there has not been an information meeting held to date. I have spoken with Doug Carlson, president of the Vilas Neighborhood Association, and prior to today, he was not aware of this application.,

The application includes a commitment by the applicant to contact the Alder and to hold an information meeting one week prior to appearing before the ALRC.

Neither has happened.

I called and left a mssage with the number listed in the application.

I cannot read the email listed on the application as the handwriting is not clear.

I am copying meenukauahal@gmail.com, hoping that is the correct email.

Given the application is for outdoor seating, I am asking that this be referred to the next ALRC meeting so that the neighborhood has an opportunity to meet with the applicant. I, too, would appreciate an opportunity to discuss this application with Meenu. I am also asking that Captain Freedman be given an opportunity to weigh in.

I will be unable to attend Wednesday's meeting as I will be out of town on business.

I do ask that you grant my request that this be referred so that the applicant can complete the required steps in the process for approval.

The neighborhood has supported the opening of this business, but this next step calls for neighborhood input before you grant approval.

Thanks.

Tag Evers District 13 Alder

From: Verbick, Jim <JVerbick@cityofmadison.com>

Sent: Thursday, May 2, 2024 10:08 AM

To: Evers, Tag <district13@cityofmadison.com>; Esser, Gregory <GEsser@cityofmadison.com>; Freedman, Jason

<JFreedman@cityofmadison.com>

Subject: New Alcohol License for May ALRC - Sriram Retails 3 LLC

Good morning,

The following application is for consideration at the May ALRC meeting.

Public Hearing - New License
Sriram Retails 3 LLC • dba Regent Street Mini Mart
1401 Regent St • Agent: Meenu Kaushal
Estimated Capacity (in/out): 100/100
Class B Beer • 30% alcohol, 60% food, 10% other
Police Sector 205 (District 13)

Sincerely,

Jim Verbick, Deputy Clerk
City of Madison City Clerk's Office
City-County Building-Room 105,
210 Martin Luther King Jr. Blvd. 53703

TEL: (608) 266-4601 FAX: (608) 266-4666

jverbick@cityofmadison.com

Follow us on Twitter @MadisonWIClerk

"We exist to assist"

 From:
 VNA President

 To:
 licensing

 Cc:
 Evers, Tag

Subject: VNA Comment on 5/15/24 Agenda Item #50; Sriram Retails 3

Date: Wednesday, May 15, 2024 1:13:09 PM

Some people who received this message don't often get email from vnapresident@gmail.com. <u>Learn why this is important</u>

Caution: This email was sent from an external source. Avoid unknown links and attachments.

Dear ALRC (copy Alder Evers, D13):

With regards to Agenda Item #50 for 5/15/24, Sriram Retails 3 LLC, Legistar #83198, the Vilas Neighborhood Association (VNA) requests that this discussion be referred to a future meeting to allow time for a public meeting and some Q&A.

I was first made aware of this agenda item yesterday morning by a member of the Vilas Zoning Committee and discussed briefly yesterday afternoon with Alder Evers, who was also unaware of it until yesterday. There is a notification sign at the bottom of a front window at the business, but it is not secured and is falling down and curled on one side with low visibility (picture available upon request). A referral would allow time to:

- Notify the neighborhood through our listsery and at our VNA Council meeting on 5/22.
- Hold a public meeting with the owner to discuss the plan.
- Allow the applicant to provide a drawing of where the seating for up to 100 patrons inside and 100 patrons outside would be placed.
- Discuss how any indoor and outdoor seating and alcohol sales would be treated on football game days.

The Regent Mart has been a welcomed addition to the Vilas neighborhood and the ownership has proven to be responsible and responsive. While we don't envision major issues with the proposed application, it needs time for a proper process and vetting of key questions.

Thanks for your consideration,

-Doug Carlson, VNA President

 From:
 wendy.fearnside@att.net

 To:
 Verbick, Jim; licensing

 Cc:
 "VNA President"

Subject: ALRC 6-26-24 Agenda Item #57, File #83198 **Date:** Wednesday, June 26, 2024 2:58:02 PM

You don't often get email from wendy.fearnside@att.net. Learn why this is important

Caution: This email was sent from an external source. Avoid unknown links and attachments.

Because the previously missing information and amendments to the Regent Mini Mart's application were not provided timely on Legistar, I respectfully request that the application be referred to the next meeting of the ALRC in order to allow time for public review and input. Thank you.

Wendy Fearnside 912 Van Buren St. Madison, WI 53711 From: meenu kaushal
To: Verbick, Jim

Subject: Fwd: Next Steps Toward Patio License **Date:** Monday, June 3, 2024 12:30:28 PM

Caution: This email was sent from an external source. Avoid unknown links and attachments.

Can u pls update seating / indoor outdoor seating to 25 max and 6 table chairs in front on application.

Thanks

From: meenu kaushal
To: Verbick, Jim

Subject: Fwd: Front Sitting - please review **Date:** Wednesday, June 19, 2024 10:08:20 AM

Attachments: image.png

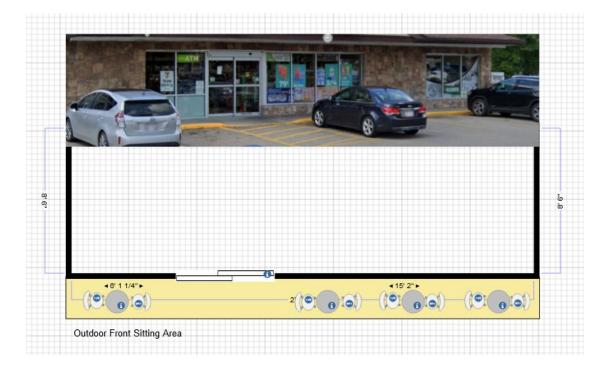
Caution: This email was sent from an external source. Avoid unknown links and attachments.

based on our discussion, you reuqested sitting plan that is in front and involves front tables and chairs, four to six table chairs

----- Forwarded message -----

From: **Roger K** < <u>roger.n1977@gmail.com</u>>
Date: Wed, Jun 19, 2024 at 10:06 AM
Subject: Front Sitting - please review

To: meenu kaushal < meenukshl@gmail.com >



 From:
 VNA President

 To:
 licensing

 Cc:
 Evers, Tag

Subject: Request for Referral of Agenda #57 **Date:** Tuesday, June 25, 2024 2:24:15 PM

Caution: This email was sent from an external source. Avoid unknown links and attachments.

Dear ALRC (copy Alder Evers, D13):

The Vilas Neighborhood Association (VNA) is requesting a referral on 6/26/24 Agenda Item #57, legislar #83198 until the application can be updated with revised information about capacity, a seating site plan, a serving/monitoring plan, and serving times. This agenda item, which was referred from the 5/15 ALRC meeting, is not ready for consideration and we have been unable to contact the owner who may not be aware that this is on the 6/26 agenda.

Background: A neighborhood meeting was held on May 20 with about a dozen attendees. The Regent Mart's owners' original plan to serve alcohol at the side of the building along Jefferson Street (not specified in the application) drew concerns from neighbors about spillover noise and monitoring since it is not visible from the store's windows. The owner verbally agreed to move the seating to the front of the store, reduce seating to 5-6 small tables, set a closing time of 9:00 pm, and not serve alcohol for on-site consumption on football Saturdays. We would like to see these items explicitly specified in an updated application before consideration by the ALRC, and we believe that the owner has the intention to do so. As an example of the status, the current application requests a seating capacity of 100 inside and 100 outside, which is unrealistic. This should be 10-15 inside and 10-15 outside. Other items such as an outdoor seating site plan, closing times, and a serving/monitoring plan are absent.

The Regent Mart has been a welcomed addition to the Vilas neighborhood and the owners have made improvements to the store, expanded the selection of ethnic foods, and are conscientious of their customers' needs. We believe that an amicable resolution with appropriate conditions can be reached prior to discussion at the ALRC.

Thanks for your consideration,
-Doug Carlson, VNA President

From: Doug Carlson
To: licensing

Subject: Comments on ALRC 6/26/26 Agenda Item #57

Date: Wednesday, June 26, 2024 1:37:59 PM

You don't often get email from dcarlson5dc@aim.com. Learn why this is important

Caution: This email was sent from an external source. Avoid unknown links and attachments.

Dear ALRC-

With regards to Agenda Item #57, the application of Regent Mart (1401 Regent St.) to serve alcohol inside and outside, I submit the following comments:

The owners held a neighborhood meeting on May 20 and concerns were expressed about the original patio location, the capacity limits on the application (200), noise spillover, the inability to adequately monitor outdoor serving areas, and overall alcohol density in this area. The original application has not been re-filed, but some changes have been provided separately by the owner and have recently been posted on legistar. At a minimum, I request that the ALRC put the following conditions on the requested Class B license that are consistent with the May 20 discussions:

- Maximum seating capacity of 25 (indoor + outdoor total).
- Outdoor seating only in front of the building, not on the side.
- Areas with open alcohol be visible from the front counter or another acceptable monitoring plan.
- Serving time ends no later than 9:00pm.
- No on-site alcohol consumption on Badger home football game days.

FYI- These are my personal comments. Although I am the President of VNA, the VNA Council has not taken a formal position on this license since we just became aware of the amendments to the application this morning and our VNA meeting overlaps tonight's ALRC meeting. The VNA request for referral has been filed separately.

Regards,

-Doug Carlson, 1018 Oakland Ave.



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84253

File ID: 84253 File Type: Resolution Status: Public Hearing

Version: 1 Reference: Controlling Body: BOARD OF

PUBLIC WORKS

File Created Date: 07/01/2024

Final Action:

File Name: Approving Plans, Specifications, And Schedule Of

Assessments For South Pinckney Street Resurfacing

Assessment - 2024

Title: Approving Plans, Specifications, And Schedule Of Assessments For South

Pinckney Street Resurfacing Assessment - 2024 (District 4).

Notes: Aaron Canton

Sponsors: Michael E. Verveer Effective Date:

Attachments: 11543EN-PinckneyResurfacing_BPWExhibit.pdf, Enactment Number:

84253 BPW Mailing.pdf,

11543_ExampleCCLtr_Pinckney.pdf

Author: Hearing Date:

Entered by: jjohnson@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Division Action Text: This Resolu		Refer	BOARD OF PUBLIC WORKS PUBLIC WORKS due ba	07/10/2024	07/10/2024	
	Action Text. This Reson	ulion was Rei	er to the BOARD OF I	PUBLIC WORKS due ba	ICK ON 7/10/2024		
1	BOARD OF PUBLIC WORKS	07/10/2024	RECOMMEND TO COUNCIL TO ADOPT - PUBLIC HEARING				Pass
	Action Text: A motion was made by Guequierre, seconded by Conklin, to RECOMMEND TO COUNCIL TO ADOPT - PUBLIC HEARING. The motion passed by voice vote/other.						

Text of Legislative File 84253

Fiscal Note

The proposed resolution approves plans, specifications, and schedule of assessments and authorizes the Board of Public Works to advertise and receive bids for the South Pinckney Street Resurfacing Assessment - 2024. The total estimated cost of the project is \$30,000, comprised of \$24,943.84 in City Costs and \$5,056.16 in assessments. Funding for the City portion of the costs is available in Munis #11543. No additional appropriation is required.

Title

Approving Plans, Specifications, And Schedule Of Assessments For South Pinckney Street

Resurfacing Assessment - 2024 (District 4) .

Body

The Board of Public Works and the City Engineer having made reports of all proceedings in relation to the improvement of South Pinckney Street Resurfacing Assessment - 2024 pursuant to a resolution of the Common Council, Resolution No. 84253, ID No. , adopted ______, which resolution was adopted thereto, and the provisions of the Madison General Ordinances and the Wisconsin Statutes in such case made and provided, and the Common Council being fully advised.

BE IT RESOLVED:

- 1. That the City at large is justly chargeable with and shall pay the sum of \$24,943.84 of the entire cost of said improvement.
- That for those eligible property owners requesting construction of a rain garden in the public right-of-way adjacent to their property shall execute the necessary waiver of special assessments on forms provided by the City Engineer;
- 3. That the sum assigned to each separate parcel, as indicated on the attached schedule of assessment, is hereby specially assessed upon each such parcel.
- 4. That the Common Council determines such special assessments to be reasonable.
- 5. That the work or improvement be carried out in accordance with the reports as finally approved.
- That such work or improvement represents an exercise of the police power of the City of Madison.
- That the plans & specifications and schedule of assessments in the Report of the Board of Public Works and the Report of the City Engineer for the above named improvement be and are hereby approved.
- 8. That the Board of Public Works be and is hereby authorized to advertise for and receive bids for the said improvements.
- 9. That the due date by which all such special assessments shall be paid in full is October 31st of the year in which it is billed, or,
- 10. That such special assessments shall be collected in eight (8) equal installments, with interest thereon at 5.0 percent per annum, except those special assessments paid in full on or before October 31st of that year.
- 11. That the Mayor and City Clerk are hereby authorized to accept dedication of lands and/or easements from the Developer/Owner for public improvements located outside of existing public fee title or easement right-of-ways.

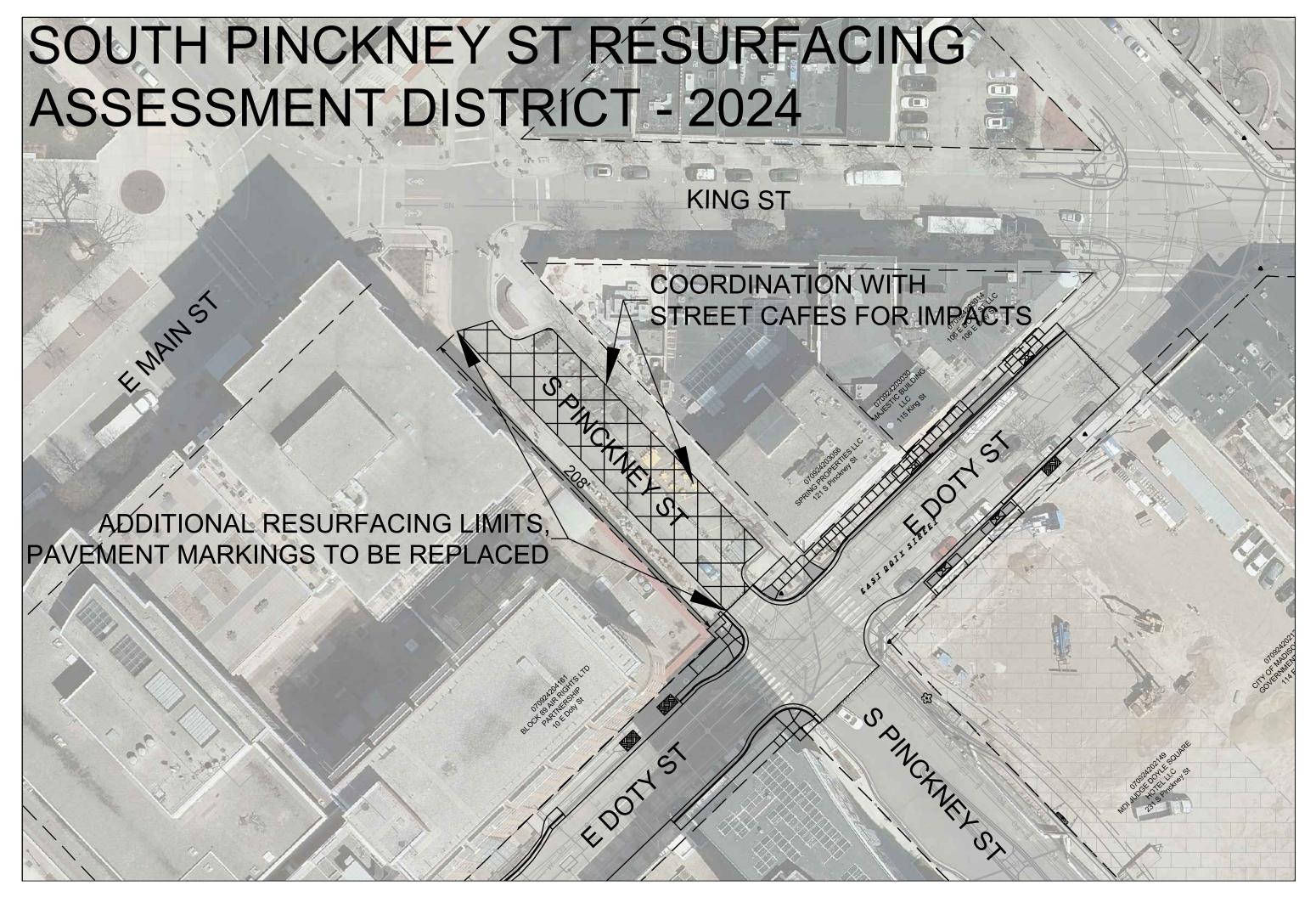
INSTALLMENT ASSESSMENT NOTICE

Notice is hereby given that a contract has been (or is about to be) let for South Pinckney Street Resurfacing Assessment - 2024 (District 4) and that the amount of the special assessment therefore has been determined as to each parcel of real estate affected thereby and a statement of the same is on file with the City Clerk; it is proposed to collect the same in eight (8) installments, as provided for by Section 66.0715 of the Wisconsin Statutes, with interest thereon at 5.0 percent per annum; that all assessments will be collected in installments as above provided except such assessments on property where the owner of the same has paid the assessment to the City Treasurer on or before the next succeeding November 1st.

NOTICE OF APPEAL RIGHTS

"Pursuant to Sec.4.09(14), Madison General Ordinances, as authorized by Sec. 66.0701(2), Wisconsin Statutes, any person against whose land a special assessment has been levied by

this resolution has the right to appeal therefrom in the manner prescribed in Sec. 66.0703(12), Wisconsin Statutes, within forty (40) days of the day of the final determination of the Common Council, said date being the date of adoption of this resolution."





Department of Public Works

Engineering Division

James M. Wolfe, P.E., City Engineer

City-County Building, Room 115 210 Martin Luther King, Jr. Boulevard Madison, Wisconsin 53703 Phone: (608) 266-4751

Fax: (608) 264-9275 engineering@cityofmadison.com www.cityofmadison.com/engineering

Parcel Number: «Parcel No» Situs Address: «Situs Address»

«OwnerLine1» «OwnerLine2» «OwnerLine3» «OwnerLine4»

June 28, 2024

To: **Property Owners along S Pinckney St**

Re: **Proposed Street Resurfacing Project & Public Hearing**

In coordination with the on-going E Wilson Street and E Doty Street Reconstruction project, the City is proposing to resurface asphalt pavement on S Pinckney Street from King Street to E Doty Street. The resurfacing will occur this Summer likely in mid-August 2024. The project manager will be coordinating with existing street cafes on S Pinckney Street that will be impacted by this resurfacing work. Enclosed with this letter is a notification for the Public Hearing before the Board of Public Works. The date and time of the public hearing can be found on the notification.

This project will involve special assessments to the owners of property adjacent to the project, per the City's adopted Street Assessment Policy for resurfacing projects. A table detailing the City's standard assessment policy for the items of work is included on the enclosed fact sheet.

Also enclosed is a preliminary Schedule of Assessments, which shows the estimated costs for your property adjacent to the project. The Schedule of Assessments for all properties adjacent to the project is available on the project webpage at: www.cityofmadison.com/WilsonEastDotyEast. A hard copy can be mailed to you upon request. The frontages of each property to be assessed are listed on the full schedule. After the work is complete, a final assessment will be calculated based on bid prices and work actually performed. The final assessment, which will be billed after the completion of the project, is payable in one lump sum or over a period of 8 years, with 5% interest charged on the unpaid balance.

To request accommodations for special needs or disabilities or if you have any other questions regarding this project, please contact Aaron Canton, Project Engineer, (608) 242-4763, acanton@cityofmadison.com. This includes requests relating to the Public Hearing and also requests relating to the actual operations of construction. A list of contacts for this project can be found on the enclosed fact sheet.

Sincerely

City Engineer

Assistant City Engineer

Bryan Cooper, AIA Gregory T. Fries, P.E.

Chris Petykowski, P.E.

Deputy Division Manager Kathleen M. Cryan

Principal Engineer 2

John S. Fahrney, P.E. Janet Schmidt, P.E.

Principal Engineer 1

Mark D. Moder, P.E. Fadi El Musa Gonzalez, P.E.

Andrew J. Zwieg, P.E.

Financial Manager

Steven B. Danner-Rivers

June 28, 2024

Page 2

Project Name: South Pinckney Street Resurfacing Assessment District - 2024

Project Limits: King St to E Doty St

Project ID: 11543

Owner:

«OwnerLine1» «OwnerLine2»

Parcel(s) being assessed:

Parcel Number: «Parcel_No» Parcel Location: «Situs_Address»

10' Pav	ement Resurfacing Assessment
	Item 1
	«Cost1»

Total	
«Total»	

The Schedule of Assessments for all properties adjacent to the project is available on the project webpage at: www.cityofmadison.com/WilsonEastDotyEast; a hard copy can be mailed to you upon request. The full Schedule includes greater details for the cost determination.

JMW: fbeg cc by email:

Michael Verveer, District 4 Alder Mark Moder, City Engineering Janet Schmidt, City Engineering Yang Tao, Traffic Engineering Gretchen Aviles, Traffic Engineering Tim Pearson, Water Utility Lorissa Banuelos, Common Council Brad Hofmann, City Forestry Tim Sobota, Madison Metro Transit

Dane County 911

Cedric Hodo cdhodo@madison.k12.wi.us Vanessa Cruz vacruz@madison.k12.wi.us Amanda Nagel ajnagel@madison.k12.wi.us Svetlin Borisov sbborisov@madison.k12.wi.us Andrew Zwieg, City Engineering Chris Petykowski, City Engineering Kyle Frank, City Engineering Thomas Mohr, Traffic Engineering Lukas Collins, Traffic Engineering Adam Wiederhoeft, Water Utility Charles Romines, Streets Division Aaron Leair, City Forestry Bill Sullivan, Fire Department Jennifer Hannah, Police Department

Kenneth Thomas krthomas1@madison.k12.wi.us
Cara Hanson cmhanson@madison.k12.wi.us
Mike MacDonald mmacdonald@madison.k12.wi.us

NOTICE OF PUBLIC HEARING BEFORE THE BOARD OF PUBLIC WORKS FOR

PLANS, SPECIFICATIONS, AND SPECIAL ASSESSMENTS FOR

PUBLIC WORKS IMPROVEMENTS MADISON, WISCONSIN

PART I

The Common Council of the City of Madison, Wisconsin, having heretofore decided that it is expedient and necessary that the improvements as listed in Part II hereof be improved at the expense of the property thereby on which would be conferred some special benefit.

NOW, THEREFORE, NOTICE IS HEREBY GIVEN:

That the City Engineer has prepared plans, specifications, a proposed schedule of assessments, and an estimate of the entire costs of the improvement for each of the districts listed in Part II hereof, and;

That the plans, specifications, a proposed schedule of assessments, and an estimate of the entire cost of the improvements listed hereto are available and open to inspection by all interested persons in the office of the City Engineer, Room 115, City-County Building, 210 Martin Luther King Jr. Blvd. and will so continue to be for ten (10) working days from the first published date of this notice, (please email Board of Public Works, boardofpublicworks@cityofmadison.com) and;

That on WEDNESDAY, JULY 10, 2024 AT 5:30 P.M., the Board of Public Works will be held remotely and the public can attend using a laptop or call in by phone. If you would like instruction on how to participate, please email boardofpublicworks@cityofmadison.com The Board will consider any objections that may be filed in writing or in person and hear all persons desiring to be heard, and;

That special assessments may be paid over an eight (8) year period, with the owner paying 1/8 of the principal each year plus five (5%) percent interest on the unpaid balance, as determined by the Board of Public Works.

That if the total assessment is paid in full before October 31st in the year that the billing is made, irrespective of project completion, then no interest shall be charged.

If you require an interpreter, materials in alternate formats, or other accommodations to access this public hearing, please contact the Engineering Division at phone (608) 266-4751 or email us at engineering@cityofmadison.com. Please make contact at least 72 hours prior to the date of this public hearing so that we can make proper accommodations.

PART II

S. PATERSON STREET SANITARY SEWER ASSESSMENT DISTRICT – 2024

S. PINCKNEY STREET RESURFACING ASSESSMENT DISTRICT - 2024

By Order of the Board of Public Works Madison, Wisconsin

PUB: WSJ June 28, 2024

AFFIDAVIT OF MAILING

STATE OF WISCONSIN)
) ss
COUNTY OF DANE)

ISAAC GABRIEL, being first duly sworn on oath, deposes and says that:

- He is an Program Assistant 1 with the Office of the City Engineer, City of Madison, Dane 1. County, Wisconsin, and did on the 28th day of June, 2024 placed in envelopes addressed to each interested owner of respective addresses as indicated by attached assessment list, a true and correct copy of the notice of assessments for their property for the project titled S. PINCKNEY STREET RESURFACING ASSESSMENT DISTRICT - 2024 attached hereto.
- 2. He delivered the envelopes to the custody of the Mail Room of the Dane County Printing and Services Division, 210 Martin Luther King, Jr. Blvd., in the city of Madison, Dane County Wisconsin, for postage and depositing in the United States Mail.

Subscribed and sworn to before months 28rd day of June, 2024

Heidi Fleegel

Notary Public, State of Wisconsin, FOF W My Commission expires: April 28, 2026

City of Madison Engineering Division - Preliminary Schedule of Assessments

Date: 6/27/2024

Project ID: 11543

Project Name: South Pinckney Street Resurfacing Assessment District - 2024

Project Description: Resurfacing S Pinckney St (King St to E Doty St), Included with the E Wilson St & E Doty St Reconstruction Project

Parcel Information				Frontage		10' Pavement Resurfacing Assessment @		Total
Parcel No./	Owner Name/	Situs Address/			Multiple	\$11.85 per LF		Assessment
Zoning	Mailing Address	Parcel Location	LF	Frontage Streets	Frontage	LF	Cost	1
070924203056	SPRING PROPERTIES LLC	121 S Pinckney St	60	S Pinckney St	Yes			
DC			75.25	E Doty St		60.00	¢711 00	¢711.00
	121 S PINCKNEY ST STE 400					60.00	\$711.00	\$711.00
	MADISON, WI 53703							
070924203064	107 KING ST LLC	107 King St	82.59	King St	Yes			
HIS-L			45.15	S Pinckney St		45.15	\$535.03	¢525.02
	502 OWEN RD					45.15	\$555.05	\$535.03
	MONONA, WI 53716							
070924203072	101 KING LLC	101 King St	81.71	King St	Yes			
PD			112.00	S Pinckney St		104.33	\$1,236.31	\$1,236.31
	10 E DOTY ST STE 300					104.55	\$1,230.31	\$1,230.31
	MADISON, WI 53703							
070924204012	BLOCK 89 AIR RIGHTS LTD	33 E Main St	99	E Main St	Yes			
PD	PARTNERSHIP		150.59	S Pinckney St		103.79	\$1,229.91	\$1,229.91
	10 E DOTY ST # 300					103.79	\$1,229.91	\$1,229.91
	MADISON, WI 53703-5120							
070924204161	BLOCK 89 AIR RIGHTS LTD	10 E Doty St	198.76	E Doty St	Yes			
PD	PARTNERSHIP		113.41	S Pinckney St		113.41	\$1,343.91	\$1,343.91
	10 E DOTY ST STE 300					115.41	\$1,545.51	\$1,545.51
	MADISON, WI 53703-5120							

TOTALS 426.68 \$5,056.16 \$5,056.16



Department of Public Works

Engineering Division

James M. Wolfe, P.E., City Engineer

City-County Building, Room 115 210 Martin Luther King, Jr. Boulevard Madison, Wisconsin 53703 Phone: (608) 266-4751

Fax: (608) 264-9275 engineering@cityofmadison.com www.cityofmadison.com/engineering Assistant City Engineer
Bryan Cooper, AIA

Bryan Cooper, AIA Gregory T. Fries, P.E. Chris Petykowski, P.E.

Deputy Division Manager Kathleen M. Cryan

Principal Engineer 2

John S. Fahrney, P.E. Janet Schmidt, P.E.

Principal Engineer 1 Mark D. Moder, P.E.

Fadi El Musa Gonzalez, P.E. Andrew J. Zwieg, P.E.

> Financial Manager Steven B. Danner-Rivers

Parcel Number: «Parcel_No»
Situs Address: «Situs Address»

«OwnerLine1» «OwnerLine2»

«OwnerLine3» «OwnerLine4»

July 2, 2024

NOTICE OF PUBLIC HEARING BEFORE THE COMMON COUNCIL FOR PLANS, SPECIFICATIONS, AND SPECIAL ASSESSMENTS FOR PUBLIC WORKS IMPROVEMENTS

MADISON, WISCONSIN

PARTI

Notice is hereby given that the reports of the Board of Public Works and the City Engineer of the City of Madison, Wisconsin, on the plans, specifications, and special assessment of benefits accruing to the various lots and parcels of land affected by the proposed improvements, as listed in Part II hereof, in the City of Madison, have been filed in the office of the City Clerk, in the City-County Building, Madison, Wisconsin, and that said reports will be presented to the Common Council at a regular meeting-to be held **TUESDAY**, **July 16**, **2024**, **at 6:30 P.M.**, **VIA A HYBRID MEETING** and that the Common Council will then and there consider said reports and will hold a public hearing to hear all persons interested, or their agents or attorneys, concerning matters contained in said reports, and will determine what portion, if any, of the cost of said improvement shall be paid by the City at large, and other action as the Common Council may deem proper.

<u>IF YOU WISH TO SPEAK VIRTUALLY AT THE MEETING, YOU MUST REGISTER.</u> You can register at https://www.cityofmadison.com/MeetingRegistration. When you register to speak, you will be sent an email with the information you need to join the virtual meeting.

<u>IF YOU WISH TO SPEAK IN-PERSON AT THE MEETING, YOU MUST REGISTER WITH THE COUNCIL'S SECRETARY PRIOR TO 6:30 P.M.</u>

The Common Council meeting is held in Room 201 of the City-County Building, 210 Martin Luther King Jr., Blvd., Madison, WI.

Notice is hereby given that a contract has been (or is about to be) let for below listed project(s) and that the amount of the special assessment therefore has been determined as to each parcel of real estate affected thereby and a statement of the same is on file with the City Clerk; it is proposed to collect the same in eight (8) installments, as provided for by Section 66.0715 of the Wisconsin Statutes, with interest thereon at 5.0% percent per annum; that all assessments will be collected in installments as above provided except such assessments on property where the owner of the same has paid the assessment to the City Treasurer on or before the next succeeding November 1st. The Board of Public Works may recommend increasing the number of proposed installments. Changes recommended by the Board of Public Works will be indicated in the resolution considered by the Common Council.

If you require an interpreter, materials in alternate formats, or other accommodations to access

July 2, 2024 Page 2

this public hearing, please contact the Engineering Division at phone (608) 266-4751 or email us at engineering@cityofmadison.com. Please make contact at least 72 hours prior to the date of this public hearing so that we can make proper accommodations.

PART II

S. PINCKNEY STREET RESURFACING ASSESSMENT DISTRICT - 2024

BY: Maribeth Witzel-Behl MariBeth Witzel-Behl, City Clerk

PUB: WSJ July 5, 2024

Project Name: South Pinckney Street Resurfacing Assessment District – 2024

Project Limits: King St to E Doty St

Project ID: 11543

The Schedule of Assessments for all properties adjacent to the project is available on the project webpage at: www.cityofmadison.com/WilsonEastDotyEast; a hard copy can be mailed to you upon request. The full Schedule includes greater details for the cost determination.

AFFIDAVIT OF MAILING

STATE OF WISCONSIN)
) ss
COUNTY OF DANE)

ISAAC GABRIEL, being first duly sworn on oath, deposes and says that:

- 1. He is an Program Assistant 1 with the Office of the City Engineer, City of Madison, Dane County, Wisconsin, and did on the 2nd day of July, 2024 placed in envelopes addressed to each interested owner of respective addresses as indicated by attached assessment list, a true and correct copy of the notice of assessments for their property for the project titled <u>S. PINCKNEY STREET RESURFACING ASSESSMENT DISTRICT 2024</u> attached hereto.
- 2. He delivered the envelopes to the custody of the Mail Room of the Dane County Printing and Services Division, 210 Martin Luther King, Jr. Blvd., in the city of Madison, Dane County Wisconsin, for postage and depositing in the United States Mail.

Isaac Gabriel

Subscribed and sworn to before me this 2nd day of July, 2024

Johanna L. Johnson

Notary Public, State of Wisconsin

My Commission expires: January 8, 2026



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83907

File ID:83907File Type:OrdinanceStatus:Council Public

Hearing

Version: 1 Reference: Controlling Body: PLAN

COMMISSION

File Created Date: 06/11/2024

File Name: 306 North Brooks Rezone Final Action:

Title: Creating Section 28.022-00676 of the Madison General Ordinances to change the zoning of property located at 306 North Brooks Street from TR-U2 (Traditional Residential-Urban 2) District to RMX (Regional Mixed-Use) District. (District 8)

Notes: 6876NBrooksRZ

Sponsors: Planning Division Effective Date:

Attachments: 83907-306 N Brooks Street.pdf, Staff Comments.pdf, Enactment Number:

Link to Demo File 83527, Link to Cond Use File 83529, Link to CSM File 83531, Staff Comments

Addendum

Author: Kate Smith Hearing Date:

Entered by: mglaeser@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Attorney's Office	06/11/2024	Referred for Introduction				
	Action Text:	This Ordinance was Refe	erred for Introduction				
	Notes:	Plan Commission (Public He	earing - 7/8/24), Common Cou	ıncil (7/16/24)			
1	COMMON COUN	ICIL 06/18/2024	Referred for Public Hearing	PLAN COMMISSION		07/08/2024	
	Action Text:	This Ordinance was Refe	erred for Public Hearing to	the PLAN COMMISSION	N		
1	PLAN COMMISS	ION 07/08/2024	RECOMMEND TO COUNCIL TO ADOPT - PUBLIC HEARING				Pass
	Action Text:		Guequierre, seconded by E e motion passed by voice		TO COUNC	L TO ADOPT	
	Notes:		erre, seconded by Ald. Dunca ne zoning map amendment to ote/ other.				

Text of Legislative File 83907

Fiscal Note

No City appropriation required.

Title

Creating Section 28.022-00676 of the Madison General Ordinances to change the zoning of property located at 306 North Brooks Street from TR-U2 (Traditional Residential-Urban 2) District to RMX (Regional Mixed-Use) District. (District 8)

Body

DRAFTER'S ANALYSIS: This ordinance amendment rezones property located at 306 North Brooks Street from TR-U2 (Traditional Residential-Urban 2) District to RMX (Regional Mixed-Use) District to facilitate redevelopment with a 15-story, 189-apartment building following demolition of existing lodging house.

The Common Council of the City of Madison do hereby ordain as follows:

1. Map Amendment 00676 of Section 28.022 of the Madison General Ordinances is hereby created to read as follows:

"28.022-00676. The following described property is hereby rezoned to RMX (Regional Mixed-Use) District.

Lots 1-6, Conklin Replat of the South 1/2 of Block 1, University Addition to the City of Madison, City of Madison, Dane County, Wisconsin. Said described area contains 34,083.75 square feet (0.78 acres)."

REQUEST FOR AMENDMENT TO THE MADISON GENERAL ORDINANCES

TO: Michael R. Haas, City Attorney	Proposed/Current Section No
FROM: Tim Parks, Planning Division	Amendment: _X Repeal:
Please draft the following ordinance:	Creation:
Note: Is this ordinance exempt from the pr	rovisions of Section 2.05(4)?
If so, circle the appropriate pa is claimed. [1, 2, 3, 4, 5, 6, 7, 8	ragraph number under which exemption , 9]
If not, the signature of the Mayo this ordinance is required below	or or the Alderperson who will sponsor w.
See Attachment(s):	
Date to be Presented: 18 June 202	4
Referral(s): Plan Commission: 8 Ju	ly 2024; Common Council: 16 July 2024
Fiscal Note: No Fiscal Impact	
Sponsor(s): Planning Division	
When completed:	
Send DRAFT to: Tim Parks	(original will be held until otherwise notified
Send copy to:	
Note: Unless otherwise indicated, this ordinate	ance will be submitted directly to Common Council.
If request is to rezone property, the following the ordinance can be drafted:	wing additional information must be furnished before
Rezone following property:	
Address 306 N Brooks Street	Alder District 8
From TR-U2	District To RMX District
Proposed Use: Rezone to facilitate redefollowing demolition of	evelopment with a 15-story, 189-unit apartment building existing lodging house
By Direction Of:	Date: 11 June 2024

226

Project Addresses: 306 N Brooks Street

Application Type: Demolition Permit, Zoning Map Amendment, Conditional Uses, and

Certified Survey Map Referral

Legistar File ID # 83527, 83907, 83529, and 83531

Prepared By: Chris Wells, Planning Division

Report includes comments from other City agencies, as noted

Reviewed By: Kevin Firchow, Planning Division

Summary

Applicant & John Leja; LZ Ventures; 8301 Machine Drive, Suite 102; Madison, WI 53717

Property Owner:

Contact: Duane Johnson; Knothe & Bruce Architects; 8401 Greenway Blvd., Suite 900; Middleton,

WI 53717

Surveyor: Kevin Pape, Vierbicher & Associates; 999 Fourier Drive, Suite 201; Madison, WI 53717

Requested Actions:

ID 83527 – Consideration of a demolition permit to demolish a lodging house at 306 N Brooks Street;

- ID <u>83907</u> Consideration of a request to rezone 306 N Brooks Street from the TR-U2 (Traditional Residential-Urban 2) District to the RMX (Regional Mixed-Use) District;
- ID <u>83529</u> Consideration of a conditional use in the [Proposed] Regional Mixed-Use (RMX) District for a
 building exceeding five stories and 78 feet in height; and Consideration of a conditional use for a vehicle
 parking reduction of more than 20 spaces and 25 percent or more of the required parking, to allow
 construction of a 15-story, 189-unit apartment building.
- ID <u>83531</u> Approval of a Certified Survey Map (CSM) to create one lot for the proposed residential development.

Proposal Summary: The applicant is seeking approvals to construct 15-story, 189-unit apartment building at the northwest corner of W Johnson Street and N Brooks Street following demolition of a four-story lodging house. The underlying lots comprising the subject site will be combined into one lot by CSM. The letter of intent indicates that construction will commence in December of 2025, with completion anticipated in June of 2027.

Applicable Regulations & Standards: Section 28.182 of the Zoning Code provides the process for zoning map amendments. MGO Section 28.069(3)(c) notes that building height exceeding the maximum (i.e. 5 stories or 78 feet) may be allowed in the RMX District with conditional use approval. Additionally, Table 28I-4 in MGO Section 28.141(5) notes that a vehicle parking reduction of more than 20 spaces and 25 percent or more of the required parking requires conditional use approval. Section 28.183 provides the process and standards for the approval of conditional use permits. MGO Section 28.185 provides the process and standards for the approval of demolition and removal permits. The subdivision process is outlined in MGO Section 16.23(4)(f) of the Subdivision Regulations.

Review Required By: Plan Commission and Common Council.



Summary Recommendation: The Planning Division recommends the following to the Plan Commission:

- That the Plan Commission that the standards for demolition permits are met to approve demolition of a lodging house located at 306 N Brooks Street;
- That the Plan Commission forward Zoning Map Amendment ID 28.022-00676, rezoning 306 N Brooks Street from TR-U2 to RMX, to the Common Council with a recommendation of **approval**;
- That the Plan Commission find the standards for conditional uses are met to **approve** a 15-story, 189-unit apartment building, subject to input at the public hearing and the conditions from reviewing agencies beginning on **page 10**; and
- That the Plan Commission forward the Certified Survey Map to remove underlying lot lines at 306 N Brooks
 Street to the Common Council with a recommendation of approval subject to the conditions from
 reviewing agencies beginning on page 19.

Background Information

Parcel Location: The 34,251-square-foot (0.79-acre) parcel is located at the northwest corner of W Johnson Street and N Brooks Street; Alder District 8 (Govindarajan); Madison Metropolitan School District.

Existing Conditions and Land Use: A four-story, roughly 50,000-square-foot lodging house, currently occupied by the community organization Porchlight, containing 102 units (87 single room occupancy rooms (SROs) and 15 efficiency units) with surface parking. The parcel is zoned PD (Planned Development District).

Surrounding Land Uses and Zoning:

North: Across Conklin Place is a nine-story, 79-unit apartment building zoned PD (Planned Development) District and Luther Memorial Church, zoned TR-U2 (Traditional Residential-Urban 2) District;

<u>South</u>: Across W Johnson Street is the University of Wisconsin's roughly 11-story Educational Sciences building in the CI (Campus Institutional) District;

West: The 12-story, 179-unit Grand Central apartment building, zoned PD District; and

<u>East</u>: Across N Brooks Street is University of Wisconsin's five-story Grainger Hall, zoned CI District.

Adopted Land Use Plans: The 2023 Comprehensive Plan recommends Special Institutional (SI) Uses for the subject parcel but provides a Map Note (No. 2) which states, in part: "There are some privately owned properties within the SI-designated areas. If such privately owned parcels redevelop, their use and design should be consistent with adopted sub-area plans, the most relevant of which, as of the adoption of this Plan, is the Regent Street-South Campus Neighborhood Plan. In the rare case where private redevelopment is proposed for an area that is not covered by a sub-area plan, multifamily residential and mixed-use development shall be considered appropriate, so long as the scale, massing, and design of the building fits in with the surrounding context, as determined by the Plan Commission and City Council." Staff note that the Regent Street-South Campus Neighborhood Plan does not extend to the north side of W Johnson Street and no other neighborhood plan provides recommendations for the subject parcel.

Zoning Summary: The subject site will be zoned RMX (Regional Mixed-Use District), which will be reviewed in the following sections.

Requirements	Required	Proposed	
Lot Area (sq. ft.): For exclusive	None	34,083.75 sq. ft.	
residential use			
Front Yard Setback	None	11.9'	
Max. Front Yard Setback	25'	11.9'	
Side Yard Setback: Other cases	None unless needed for access	11.6' north side	
		12.0' south side	
Rear Yard Setback: For corner lots,	The required rear yard setback shall be	36.4'	
where all abutting property is in a	the same as the required side yard		
nonresidential zoning district	setback: None unless needed for access		
Usable Open Space	None	As shown on proposed plan	
Maximum Lot Coverage	90%	84% (See Comment #47)	
Minimum Building Height	2 stories	15 stories/< Capitol View	
		Preservation limit	
Maximum Building Height	5 stories/78'	15 stories/< Capitol View	
		Preservation limit	

Site Design	Required	Proposed		
Number Parking Stalls	Multi-family dwelling: 1 per dwelling	117 garage		
	(189)	11 surface		
		(128 total)		
Electric Vehicle Stalls	13 EV ready	13 EV ready		
		3 EV installed		
Accessible Stalls	Yes	Yes		
Loading	None	Loading zone		
Number Bike Parking Stalls	Multi-family dwelling: 1 per unit up to 2-	222 garage		
	bedrooms, ½ space per add'l bedroom	64 surface		
	(267)	(286 total)		
	1 guest space per 10 units (19)	(See Comment #48)		
	(286 total)			
Landscaping and Screening	Yes	Yes (See Comment #49)		
Lighting	Yes	Yes		
Building Form and Design	Yes	Large multi-family building		

Other Critical Zoning Items	
Yes:	Utility Easements; Barrier Free (ILHR 69)
No:	Urban Design, Historic District; Floodplain, Wetlands, Wellhead Protection, Adjacent to Park, TOD Overlay

Tables Prepared by Jenny Kirchgatter, Assistant Zoning Administrator

Environmental Corridor Status: The subject site is not located in a mapped environmental corridor.

Public Utilities and Services: The site is currently served by a full range of urban services, including Metro Transit which operates daily all-day transit service along N Brooks Street opposite this property – with trips at least every 60 minutes (every 30 minutes or less during the day on weekdays). Metro Transit operates additional daily all-day transit service along University Avenue, W Johnson Street and S Park Street near this property - with trips on most

routes at least every 30 minutes. Metro Transit would initially estimate the following counts of potentially eligible trips towards US Green Building Council/LEED Quality Access to Transit points: 444 Weekday & 201 Weekend (average). Please contact Metro Transit if additional analysis would be of interest.

Project Description

The applicant is requesting approval of a demolition permit to demolish a four-story, roughly 50,000-square-foot lodging house located on a 0.79-acre parcel at 306 N Brooks Street and to rezone the parcel from TR-U2 to RMX to facilitate redevelopment of the property with a 15-story, 189-unit apartment building. Additionally, a Certified Survey Map (CSM) is proposed to combine the underlying lots comprising the subject parcel into one lot.

The existing building, built in 1952 as the University YMCA, has served for the past 21 years as the Porchlight nonprofit organization's administrative and long-term affordable housing facility. The building is four-stories in height and contains 102 units (87 single room occupancy rooms (SROs) and 15 efficiency units). Seventeen surface automobile stalls are located on the west side of the site and are accessed from W Johnson Street. Photos of the interior and exterior of the building are included in the materials submitted for the demolition permit. Planning Division staff is not aware of structural issues with the building.

After demolishing the subject building, the applicant proposes to construct a 15-story, 189-unit apartment building. Like its predecessor, surface automobile stalls (now 11) are located on the west side of the site and are accessed from W Johnson Street. However, unlike its predecessor, the new building has under building parking which is accessed via a ramp located off of this west drive aisle. The structured parking, which starts on the ground level, spirals down for roughly two levels and contains a total of 117 stalls. It also contains 223 long-term bicycle stalls located in various locations over the two levels.

Staff note that the provision of 128 automobile stalls (117 structured and 11 surface) is 61 stalls (or 32.2-percent) short of the 189 stalls required by the Zoning Code. Table 28I-4 in MGO Section 28.141(5) states that a vehicle parking reduction of more than 20 spaces and 25 percent or more of the required parking requires conditional use approval.

Justifying their provision of parking, the applicant notes in submitted materials, "The project is at the heart of campus and the vast majority of the tenants will be UW students who would be walking or biking to nearby classes. The site is within close proximity to multiple bus stops including BRT route options. In addition, there are many options for bike and pedestrian travel, including established bike paths and routes. Furthermore, the project will be owned and managed by LZ Management, who owns and manages two student housing projects on the same block; Grand Central and X-01 have a combined 211 parking stalls, which have proven over the years to be more than adequate, and many of these are leased to non-students. If viewed cumulatively between all three buildings, the parking ratio would be .78 stalls per unit. Shared parking between all these buildings would be available."

The new building has two entrances, one located along each street-facing façade. Community rooms occupy the southwest and southeast corners of the ground floor while a fitness room occupies the northeast corner. The upper 14 floors contain a total of 189 units – eight studios, 14 one-bed, 56 two-bed, 74 three-bed, 29 four-bed, and 8 five-bedroom units. A small, 724-square-foot lounge and adjacent 271-square-foot terrace, both common amenities, are located on the 15th floor.

The new building is roughly 161 feet in height. (While the submitted drawings show the top of the building mechanicals is roughly seven inches below the Capital View Preservation height limit, Zoning will need to confirm this compliance before final sign off.) As for massing stepbacks, the building mass steps back roughly nine feet on all four sides at the sixth-floor level.

Regarding exterior materials, beige-colored brick is the primary material on the lower 12 floors while a light grey composite metal paneling clads the upper three. Dark grey composite metal paneling is used to cover the floor slabs.

Analysis

Section 28.182 of the Zoning Code provides the process for zoning map amendments. MGO Section 28.069(3)(c) notes that building height exceeding the maximum (i.e. 5 stories or 78 feet) may be allowed in the RMX District with conditional use approval. Additionally, Table 28I-4 in MGO Section 28.141(5) notes that a vehicle parking reduction of more than 20 spaces and 25 percent or more of the required parking requires conditional use approval. Section 28.183 provides the process and standards for the approval of conditional use permits. MGO Section 28.185 provides the process and standards for the approval of demolition and removal permits. The subdivision process is outlined in MGO Section 16.23(4)(f) of the Subdivision Regulations.

Conformance with Adopted Plans

The 2023 <u>Comprehensive Plan</u> recommends Special Institutional (SI) Uses for the subject parcel but provides a Map Note (No. 2) which states, in part: "There are some privately owned properties within the SI-designated areas. If such privately owned parcels redevelop, their use and design should be consistent with adopted sub-area plans, the most relevant of which, as of the adoption of this Plan, is the Regent Street-South Campus Neighborhood Plan. In the rare case where private redevelopment is proposed for an area that is not covered by a sub-area plan, multifamily residential and mixed-use development shall be considered appropriate, so long as the scale, massing, and design of the building fits in with the surrounding context, as determined by the Plan Commission and City Council." Staff note that the <u>Regent Street-South Campus Neighborhood Plan</u> does not extend to the north side of W Johnson Street and no other neighborhood plan provides recommendations for the subject parcel.

As a measure of building scale, staff have used building height as a proxy. The proposed building height is roughly 161 feet, which equates to roughly 1,032 feet above sea level. (The Capital View Preservation height limit is 187.2 feet above City datum or 1,032.8 ft above sea level). For comparison's sake, the applicant team prepared a building height survey of the surrounding parcels (see Image 1 below).

North: Across Conklin Place is X01 Apartments, a nine-story, 79-unit apartment building, whose height reaches up to roughly 1,013 feet and Luther Memorial Church, which rises up to the roughly 972-foot mark;

South: Across W Johnson
Street is the University of
Wisconsin's roughly 11story Educational
Sciences building, which
reaches up to the roughly
1,029-foot mark;

West: The 12-story, 179unit, Grand Central

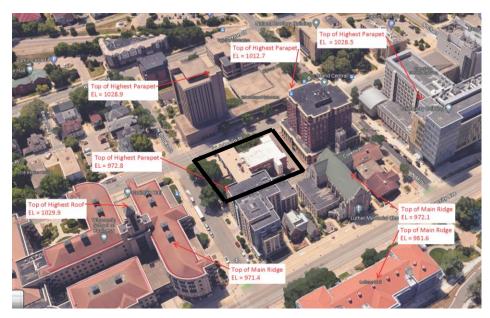


Image 1: Building Heights Surrounding the Subject Site. (Source: Vierbicher Associates)

apartment building, which reaches up to the 1,013-foot mark, beyond which is the University of Wisconsin's Chemistry Building, which reaches up to the roughly 1,028-foot mark; and

<u>East</u>: Across N Brooks Street is University of Wisconsin's five-story Grainger Hall whose roof ridge reaches up to the 971-foot mark but whose tower spire rises to the 1,029-foot height mark.

This means that there are three buildings located in the surrounding block (four if you count the Grainger Hall spire) that rise up to at least the 1,012-foot mark (or within roughly 20 feet of the proposed building), two of which rise up to less than five feet of the proposed building. Furthermore, the proposed building's mass and stepbacks closely resembles that of the adjacent Grand Central apartment building to the west which also has a roughly five-story base before stepping back on all sides.

It is for these reasons that Staff believe the proposed building could be found 'appropriate' and that its scale, massing, and design fit in with the surrounding context. Therefore, Staff believe it is consistent with adopted plan recommendations.

Demolition Permit Standards

At its October 5, 2021 meeting, the Common Council adopted Ordinance 21-00065 (ID 67074) to revise the standards and process for demolition and removal permits in Section 28.185 of the Zoning Code to remove consideration of proposed future use as a factor in approving demolition requests. The Plan Commission shall consider the seven approval standards in §28.185(9)(c) M.G.O. when making a determination regarding demolition requests and find them met in order to approve. The factors include a finding that the proposed demolition or removal is consistent with the statement of purpose of the demolition permits section and with the health, prosperity, safety, and welfare of the City of Madison. The statement of purpose in Section 28.185 states, in part:

"The purpose of this section is therefore to ensure the preservation of historic buildings, encourage applicants to strongly consider relocating rather than demolishing existing buildings, aid in the implementation of adopted City plans, maximize the reuse or recycling of materials resulting from a demolition, protect the public from potentially unsafe structures and public nuisances..."

On balance, staff believes that standards can be found met and provides the following additional information.

In regards to Standard 1 related to the possible building relocation efforts, the Plan Commission is required to consider any information provided related to possible relocation activities. The applicant noted in submitted materials that relocation was considered but was deemed not a "good prospect for relocation."

In regards to Standard 6 related to the condition of the buildings proposed for demolition, Planning Division staff have not received any comments from City Building Inspection, Fire, or Police. In the submitted information, the applicant provides photos of the existing structure and notes that the building has become "outdated and obsolete and has become a financial and maintenance burden to its users. The facility has not seen significant updates for many years: it has its original windows, insulation, and mechanical systems and is very inefficient. In addition, the building does not have a fire sprinkler system, which is unsafe and no longer allowed for a building of this type." However, from the submitted information, staff is not aware of significant structural issues. While the code notes condition must be considered, it does not specify that a certain structural condition or level of disrepair is necessary in order to approve a demolition.

Related to the historic value of the structures, staff notes Standard 4 states:

"The Plan Commission has received and considered the report of the City's historic preservation planner regarding the historic value of the property as well as any report that may be submitted by the Landmarks Commission."

The Landmarks Commission reviewed the demolition of the building at its May 6, 2024 meeting, and at that meeting found that the building at 306 N Brooks Street "has historic and cultural value related to its historic association with the Women's Rights Movement and Gay Liberation Movement."

Regarding the historic and cultural significance the existing structure has had, the applicant notes the following in submitted materials,

"The building is not on the National or State Register of Historic Places; however, there was a survey conducted in 2019 called the "Wisconsin Underrepresented Communities Historic Resource Survey Report" which highlighted 306 N. Brooks Street as the Women's Center of the University YMCA from the 1970's-1990's and housing the Lesbian Switchboard beginning in 1974. Other possibly significant events in this building include the Teachers Assistance Association office. We would like to honor the history of this building by highlighting the cultural significance this property has with the Women's Movement, Gay Liberation Movement, Teacher's Assistance Association, and more recently, Porchlight. This would be done by creating a legacy history wall or exhibit that visually shows this history in a creative and engaging way." The applicant further clarified, "The legacy wall will be on the interior [of the building]. We've hired Legacy Architects to do additional research to holistically understand the history of the site and be able to tell that story in an engaging way within the public space of the building."

Standard 7, which includes references the standards 1-6 states that "The Plan Commission shall consider the factors and information specified in items 1-6 and find that the proposed demolition or removal is consistent with the statement of purpose of this section and with the health, prosperity, safety, and welfare of the City of Madison."

Given the condition of the structure, the lack of a fire sprinkler system, the legacy history wall that would be created within the new building, and the fact the Porchlight itself of supportive of the demolition (due to the fact that it would potentially relocate to a new 70-units mixed-use building at 521 E Washington Avenue), Staff believe the Demolition Permit standards can be found met subject to input at the public hearing and comments from reviewing agencies.

Zoning Map Amendment Standards

Staff believes that the Zoning Map Amendment standards can be found met. These standards state that such amendments are legislative decisions of the Common Council that shall be based on public health, safety and welfare, shall be consistent with the Comprehensive Plan, and shall comply with Wisconsin and federal law. Chapter 66.1001(3) of Wisconsin Statutes requires that zoning ordinances (of which the zoning map is part) enacted or amended after January 1, 2010 be consistent with the City's Comprehensive Plan. 2010 Wisconsin Act 372 clarified "consistent with" as "furthers or does not contradict the objectives, goals and policies contained in the comprehensive plan." As noted above, Staff believes that the project is consistent with the Comprehensive Plan.

Conditional Use Standards

The Conditional Use approval standards state that the Plan Commission shall not approve a conditional use without due consideration of the City's adopted plan recommendations, design guidelines, supplemental regulations, and finding that all of the conditional use standards of MGO §28.183(6) are met. Please note, recent changes to state law requires that conditional use findings must be based on "substantial evidence" that directly pertains to each standard and not based on personal preference or speculation. Staff provides additional discussion on the following standards.

Standard 5 states, "Adequate utilities, access roads, drainage, internal circulation improvements for pedestrians, bicyclists, public transit and vehicles, parking supply (in cases with minimum parking requirements) and other necessary site improvements have been or are being provided." As noted previously in this report, the applicant is providing just 128 of the required 189 automobile parking stalls; a reduction which requires a conditional use approval. The applicant has provided justification for why they believe this is warranted including the vast majority of tenants being students, the proximity to campus, and the wealth of nearby transportation options such as bicycle, car share, and Metro's BRT.

Standard 11 states, "When applying the above standards to an application for height in excess of that allowed in the district, the Plan Commission shall consider recommendations in adopted plans; the impact on surrounding properties, including height, mass, orientation, shadows and view; architectural quality and amenities; the relationship of the proposed building(s) with adjoining streets, alleys, and public rights of ways; and the public interest in exceeding the district height limits."

As described above, Staff believe the proposed building's scale, massing, and design fit in with the surrounding context.

On balance, staff believes that the Conditional Use Standards can be found met subject to input at the public hearing and comments from reviewing agencies.

Land Division

The applicant has submitted a one-lot Certified Survey Map to combine the underlying lots that comprise the subject parcel. M.G.O. Section 16.23(5)(g) provides the process for certified survey maps. Staff believe that all applicable standards for land divisions can be found met, subject to the comments recommended by reviewing agencies.

Public Input

At time of writing, Staff have not received any public comments.

Conclusion

The applicant is requesting approval of a zoning map amendment to rezone the 0.79-acre parcel from TR-U2 to RMX and conditional use approvals to facilitate redevelopment of the site with a 15-story, 189-unit apartment building following demolition of a lodging house.

The Planning Division believes that the Plan Commission may find that the proposed development meets the various standards for approval. Regarding the height and mass, Staff note that there are two other buildings in the immediate area that are within five feet of the proposed building's absolute height of 1,032 feet above sea level. Furthermore, Staff note the proposed building's mass and stepbacks closely resembles that of the adjacent Grand Central apartment building to the west which also has a roughly five-story base before stepping back on all sides. For these reasons, Staff believe the proposed building's scale, massing, and design fit in with the surrounding context and therefore believe it is consistent with adopted plan recommendations.

Recommendation

<u>Planning Division Recommendation</u> (Contact Chris Wells, (608) 261-9135)

The Planning Division recommends the following to the Plan Commission:

- That the Plan Commission that the standards for demolition permits are met to approve demolition of a lodging house located at 306 N Brooks Street;
- That the Plan Commission forward Zoning Map Amendment ID 28.022-00676, rezoning 306 N Brooks Street from TR-U2 to RMX, to the Common Council with a recommendation of **approval**;
- That the Plan Commission find the standards for conditional uses are met to approve a 15-story, 189-unit
 apartment building, subject to input at the public hearing and the conditions from reviewing agencies that
 follow; and
- That the Plan Commission forward the Certified Survey Map to remove underlying lot lines at 306 N Brooks
 Street to the Common Council with a recommendation of approval subject to the conditions from
 reviewing agencies beginning on page 19.

Recommended Conditions of Approval Major/Non-Standard Conditions are Shaded

Planning Division (Contact Chris Wells, (608) 261-9135)

Prior to final sign off and the issuance of permits, the applicant shall submit a management plan on the form provided by the Zoning Administrator. The management plan shall be approved by the Planning Division Director and Zoning Administrator (or their designees) prior to final approval.

City Engineering Division (Contact Tim Troester, (608) 267-1995)

- 2. The City has limited sanitary sewer capacity in this area. Applicant shall submit projected wastewater flow calculations for the proposed development. Applicant may be required to build offsite sanitary sewer improvements as a condition of plan approval with a developer's agreement with the City. Applicant shall submit projected wastewater calculations to Mark Moder, mmoder@cityofmadison.com.
- 3. Enter into a City / Developer agreement for the required infrastructure improvements. Agreement to be executed prior to sign off. Allow 4-6 weeks to obtain agreement. Contact City Engineering to schedule the development and approval of the plans and the agreement. (MGO 16.23(9)c)
- 4. Construct sidewalk, terrace, curb and gutter and street and alley pavement to a plan as approved by City Engineer
- 5. Madison Metropolitan Sewerage District (MMSD) charges are due and payable prior to Engineering sign-off, unless otherwise collected with a Developer's / Subdivision Contract. Contact Mark Moder (608-261-9250) to obtain the final MMSD billing a minimum of two (2) working days prior to requesting City Engineering signoff. (MGO 16.23(9)(d)(4)
- 6. Obtain a permanent sewer plug permit for each existing sanitary sewer lateral serving a property that is not to be reused and a temporary sewer plug permit for each sewer lateral that is to be reused by the development. procedures available online The and fee schedule is http://www.cityofmadison.com/engineering/permits.cfm. (MGO CH 35.02(14))
- 7. An Erosion Control Permit is required for this project. See Storm comments for permit specific details and requirements.
- 8. A Storm Water Management Report and Storm Water Management Permit is required for this project. See Storm comments for report and permit specific details and requirements.
- 9. This site appears to disturb less than one (1) acre of land. No submittal to the WDNR, CARPC or Department of Safety and Professional Services (DSPS) is required as the City of Madison Building Inspection Department is an approved agent for DSPS.
- 10. Revise the plans to show a proposed private internal drainage system on the site. Include the depths and locations of structures and the type of pipe to be used. (POLICY AND MGO 10.29)

- 11. Revise the plans to identify the location of the public storm sewer (proposed or existing) that will serve the development show the connection of the private internal drainage system to the public storm sewer. (POLICY AND MGO OVER 10,000 SF OF IMPERVIOUS AREA 10.29 and 37.05(7)(b))
- 12. The proposed development proposes to construct underground parking. The proposed entrance to the underground parking is adjacent to a street low point. The applicant shall provide at a minimum of one (1) foot of rise from the adjacent back of walk in the driveway before breaking grade to the down ramp to the underground parking to protect the underground parking from inundation. The stated elevation is intended to be protective but does not guarantee a flood proof structure. The Developer/Owner are strongly encouraged to complete their own calculations and determine an elevation that protects their property to a level of service that they are comfortable with.
- 13. Provide additional detail how the enclosed depression(s) created by the parking entrance(s) to the below building parking area(s) is/are served for drainage purposes. The building must be protected from receiving runoff up through the 100-yr design storm that is current in Madison General Ordinance Chapter 37. If the enclosed depression(s) is/are to be served by a gravity system provide calculations stamped by a Wisconsin P.E. that show inlet and pipe capacities meet this requirement. If the enclosed depression(s) is/are to be served by a pump system provide pump sizing calculations stamped by a Wisconsin P.E. or licensed Plumber that show this requirement has been met.
- 14. This project will disturb 20,000 sf or more of land area and require an Erosion Control Plan. Please submit an 11" x 17" copy of an erosion control plan (pdf electronic copy preferred) to Megan Eberhardt (west) at meberhardt@cityofmadison.com, or Daniel Olivares (east) at daolivares@cityofmadison.com, for approval. Demonstrate compliance with Section 37.07 and 37.08 of the Madison General Ordinances regarding permissible soil loss rates. Include Universal Soil Loss Equation (USLE) computations for the construction period with the erosion control plan. Measures shall be implemented in order to maintain a soil loss rate below 5.0 tons per acre per year. The WDNR provided workbook to compute USLE rates can be found online at https://dnr.wi.gov/topic/stormwater/publications.html

This project will require a concrete management plan and a construction dewatering plan as part of the erosion control plan to be reviewed and approved by the City Engineer's Office. If contaminated soil or groundwater conditions exist on or adjacent to this project additional WDNR, Public Health, and/or City Engineering approvals may be required prior to the issuance of the required Erosion Control Permit. (POLICY)

This project appears to require fire system testing that can result in significant amounts of water to be discharged to the project grade. The Contractor shall coordinate this testing with the erosion control measures and notify City Engineering 608-266-4751 prior to completing the test to document that appropriate measures have been taken to prevent erosion as a result of this testing.

Complete weekly self-inspection of the erosion control practices and post these inspections to the City of Madison website - as required by Chapter 37 of the Madison General Ordinances.

15. Prior to approval, this project shall comply with Chapter 37 of the Madison General Ordinances regarding stormwater management. Specifically, this development is required to submit a Storm Water Management Permit application, associated permit fee, Stormwater Management Plan, and Storm Water Management Report to City Engineering. The Stormwater Management Permit application can be found on City Engineering's website at http://www.cityofmadison.com/engineering/Permits.cfm

The Storm Water Management Plan & Report shall include compliance with the following:

Report: Submit prior to plan sign-off, a stormwater management report stamped by a P.E. registered in the State of Wisconsin.

Electronic Data Files: Provide electronic copies of any stormwater management modeling or data files including SLAMM, RECARGA, TR-55, HYDROCAD, Sediment loading calculations, or any other electronic modeling or data files. If calculations are done by hand or are not available electronically, the hand copies or printed output shall be scanned to a PDF file and provided to City Engineering. (POLICY and MGO 37.09(2))

Rate Control Redevelopment: By design detain the 10-year post construction design storm such that the peak discharge during this event is reduced 15% compared to the peak discharge from the 10-year design storm in the existing condition of the site. Further, the volumetric discharge leaving the post development site in the 10-year storm event shall be reduced by 5% compared to the volumetric discharge from the site in an existing condition during the 10-year storm event. These required rate and volume reductions shall be completed, using green infrastructure that captures at least the first 1/2 inch of rainfall over the total site impervious area. If additional stormwater controls are necessary beyond the first 1/2 inch of rainfall, either green or non-green infrastructure may be used.

TSS Redevelopment with TMDL: Reduce TSS by 80% off of the proposed development when compared with the existing site.

Submit a draft Stormwater Management Maintenance Agreement (SWMA) for review and approval that covers inspection and maintenance requirements for any BMP used to meet stormwater management requirements on this project.

16. Submit, prior to plan sign-off but after all revisions have been completed, digital PDF files to the Engineering Division. Email PDF file transmissions are preferred to: bstanley@cityofmadison.com (East) or ttroester@cityofmadison.com (West).

City Engineering – Mapping (Contact Julius Smith, (608) 264-9276)

- 17. Applicant shall dedicate Right-of-Way or Grant a Public Sidewalk Easement to the City along N Brooks St. and E. Johnson St. on the face of the concurrent Certified Survey Map. The final width and location of the easement to be approved by Engineering and Traffic Engineering
- 18. Coordinate and request from the utility companies serving this area the easements required to serve this development. Those easements shall be properly shown, dimensioned and labeled on the final plat.
- 19. The Owner on the application is not the current owner of record for the property. Provide the recorded conveyance of the parcel to the Owner noted on the application prior to final site plan sign off.
- 20. Proposed development conflicts with existing underground electric easement. Release MGE Easement per Document no. 3605277 to make way for proposed improvements. provide the recorded release prior to site plan approval.
- 21. Show the 12' of the existing driveway easement per document nos. 4403378 and 4409641 and 15' storm easement per doc 2815108 on sheets C 300 and C400.

- 22. Note drive area appears smaller than 12 feet and parking area as shown in document no. 44009641 while be altered with this document. Amend Document nos. 4403378 and 44009641 as necessary and provide to after recording and prior to site plan approval.
- 23. The address of 306 N Brooks St will be inactivated and archived with the demolition of the building. The address of the proposed apartment building is 1008 W Johnson St. The site plan shall reflect a proper street address of the property as reflected by official City of Madison Assessor's and Engineering Division records.
- 24. The pending Certified Survey Map application for this property shall be completed and recorded with the Dane County Register of Deeds (ROD), the new parcel data created by the Assessor's Office and the parcel data available to zoning and building inspection staff prior to issuance of building permits for new construction.
- 25. Submit a site plan and a complete building Floor Plan in PDF format to Lori Zenchenko (<u>Izenchenko@cityofmadison.com</u>) that includes a floor plan of each floor level on a separate sheet/page for the development of a complete interior addressing plan. Also, include a unit matrix for apartment buildings.

The Addressing Plan for the entire project shall be finalized and approved by Engineering (with consultation and consent from the Fire Marshal if needed) PRIOR to the verification submittal stage of this LNDUSE with Zoning. The final approved Addressing Plan shall be included in said Site Plan Verification application materials or a revised plan shall provided for additional review and approval by Engineering.

Per 34.505 MGO, a full copy of the approved addressing plan shall be kept at the building site at all times during construction until final inspection by the Madison Fire Department.

For any changes pertaining to the location, deletion or addition of a unit, or to the location of a unit entrance, (before, during, or after construction), a revised Address Plan shall be resubmitted to Lori Zenchenko to review addresses that may need to be changed and/or reapproved.

Traffic Engineering Division (Contact Luke Peters, (608) 266-6543)

- 26. The applicant shall dedicate Right of Way or grant a Public Sidewalk Easement for and be responsible for the construction of a minimum eight (8)-foot wide sidewalk, six (6)-foot terrace, and additional one (1) foot for maintenance, where applicable, along their site's frontage of East Johnson Street.
- 27. The applicant shall dedicate Right of Way or grant a Public Sidewalk Easement for and be responsible for the construction of a minimum five (5)-foot wide sidewalk, eight (8)-foot terrace, and additional one (1) foot for maintenance, where applicable, along their site's frontage of North Brooks Street.
- 28. The applicant shall submit one contiguous plan showing proposed conditions and one contiguous plan showing existing conditions for approval. The plan drawing shall be scaled to 1" = 20' and include the following, when applicable: existing and proposed property lines; parcel addresses; all easements; pavement markings; signing; building placement; items in the terrace such as signs, street light poles, hydrants; surface types such as asphalt, concrete, grass, sidewalk; driveway approaches, including those adjacent to and across street from the project lot location; parking stall dimensions, including two (2) feet of vehicle overhang; drive aisle dimensions; semitrailer movement and vehicle routes; dimensions of radii; and percent of slope.

- 29. The developer shall post a security deposit prior to the start of development. In the event that modifications need to be made to any City owned and/or maintained traffic signals, street lighting, signing, pavement marking and conduit/handholes, the Developer shall reimburse the City for all associated costs including engineering, labor and materials for both temporary and permanent installations.
- 30. The City Traffic Engineer may require public signing and marking related to the development; the developer shall be financially responsible for such signing and marking.
- 31. All parking facility design shall conform to the standards in MGO Section 10.08(6).
- 32. All bicycle parking adjacent pedestrian walkways shall have a two (2)-foot buffer zone to accommodate irregularly parked bicycles and/or bicycletrailers.
- 33. All pedestrian walkways adjacent parking stalls shall be 7 feet wide to accommodate vehicle overhang, signage and impediments to walkway movements. Any request for variance shall be submitted to and reviewed by City Traffic Engineering.
- 34. Per MGO Section 12.138 (14), this project is not eligible for residential parking permits. It is recommended that this prohibition be noted in the leases for the residential units.
- 35. The applicant shall adhere to all vision triangle requirements as set in MGO Section 27.05 (no visual obstructions between the heights of 30 inches and 10 feet at a distance of 25 feet behind the property line at streets and 10 feet at driveways.). Alteration necessary to achieve compliance may include but are not limited to; substitution to transparent materials, removing sections of the structure and modifying or removing landscaping elements. If applicant believes public safety can be maintained they shall apply for a reduction of MGO Section 27.05(2)(bb), Vision Clearance Triangles at Intersections Corners. Approval or denial of the reduction shall be the determination of the City Traffic Engineer.
- 36. All parking ramps as the approach the public Right-of-Way shall not have a slope to exceed 5% for 20 feet; this is to ensure drivers have adequate vision of the Right-of-Way. If applicant believes public safety can be maintained they shall apply for a waiver, approval or denial of the waiver shall be the determination of the City Traffic Engineer.
- 37. City of Madison radio systems are microwave directional line of sight to remote towers citywide. The building elevation will need to be reviewed by Traffic Engineering to accommodate the microwave sight and building. The applicant shall submit grade and elevations plans if the building exceeds three stories prior to sign-off to be reviewed and approved by Andrew Oliver, ((608) 267-1979, aoliver@cityofmadison.com) Traffic Engineering Shop, 4151 Nakoosa Trail. The applicant shall return one signed approved building elevation copy to the City of Madison Traffic Engineering office with final plans for sign-off.
- 38. The driveway slope to the underground parking is not identified in the plan set, Traffic Engineering recommends driveway slope under 10%; if the slope is to exceed 10%, the applicant shall demonstrate inclement weather mitigation techniques to provide safe ingress/egress to be approved by the City Traffic Engineer.

- 39. The applicant shall provide a clearly defined 5' walkway clear of all obstructions to assist citizens with disabilities, especially those who use a wheel chair or are visually impaired. Obstructions include but are not limited to tree grates, planters, benches, parked vehicle overhang, signage and doors that swing outward into walkway.
- 40. "Stop" signs shall be installed at a height of seven (7) feet from the bottom of the sign at all class III driveway approaches, including existing driveways, behind the property line and noted on the plan. All directional/regulatory signage and pavement markings on the site shall be shown and noted on the plan.
- 41. The applicant shall show the dimensions for the proposed class III driveway including the width of the drive entrance, width of the flares, and width of the curb cut.
- 42. All existing driveway approaches on which are to be abandoned shall be removed and replaced with curb and gutter and noted on the plan.
- 43. Applicant shall submit for review a waste removal plan. This shall include vehicular turning movements.
- 44. Note: This site presents difficult constructability issues; access to neighboring sites must be maintained at all times, protected walkways will be constructed and maintained as soon as possible and little to no access to the Public Right-of-Way on East Johnson Street will be granted for construction purposes. Provide a detailed construction plan to Traffic Engineering for review prior to final signoff.

Parking Division (Contact Trent W. Schultz, (608) 246-5806)

45. The applicant shall submit a Transportation Demand Management (TDM) Plan to tdm@cityofmadison.com. The TDM Plan is required per MGO Section 16.03. Applicable fees will be assessed after the TDM Plan is reviewed by staff.

Zoning Administrator (Contact Jenny Kirchgatter, (608) 266-4429)

- 46. Section 28.185(9)(b) requires that every applicant for a demolition or removal approval that requires approval by the Plan Commission is required to get a Reuse and Recycling Plan approved by the City Recycling Coordinator, Bryan Johnson at streets@cityofmadison.com prior to receiving a raze permit. Every person who is required to submit a reuse and recycling plan pursuant to Section 28.185(9)(b) shall submit documents showing compliance with the plan within sixty (60) days of completion of demolition. A demolition or removal permit is valid for two (2) years from the date of the Plan Commission approval.
- 47. Provide a calculation and plan detail for lot coverage with the final submittal. The lot coverage maximum is 90%. Lot coverage is defined as the total area of all buildings, measured at grade, all accessory structures including pools, patios, etc., and all paved areas as a percentage of the total area of the lot, with the following exceptions: sidewalks or paved paths no wider than five (5) feet, pervious pavement, green roofs and decks. It appears that some 7' wide sidewalk areas may have been excluded from the lot coverage calculation. Provide details of any pervious pavement areas that are used.

- 48. Bicycle parking for the residential dwelling units shall comply with the requirements of MGO Sections 28.141(4)(g) and 28.141(11) and shall be designated as short-term or long-term bicycle parking. A minimum of 267 resident bicycle stalls are required plus a minimum of 19 short-term guest stalls. A minimum of 90% of the resident stalls (240 stalls) shall be designed as long-term parking. Up to twenty-five percent (25%) of bicycle parking may be structured parking, vertical parking or wall mount parking, provided there is a five (5) foot access aisle for wall mount parking. NOTE: A bicycle stall is a minimum of two (2) feet by six (6) feet with a five (5) foot wide access area. Submit a detail showing the model of bike rack to be installed.
- 49. Submit the landscape plan and landscape worksheet stamped by the registered landscape architect. Per Section 28.142(3) Landscape Plan and Design Standards, landscape plans for zoning lots greater than ten thousand (10,000) square feet in size must be prepared by a registered landscape architect.
- 50. Per Section 28.186(4)(b), the property owner or operator is required to bring the property into compliance with all elements of the approved site plans by the date established by the Zoning Administrator as part of the site and building plan approval. Work with Zoning staff to establish a final site compliance date.
- 51. Signage approvals are not granted by the Plan Commission. Signage must be reviewed for compliance with Chapter 31 Sign Codes of the Madison General Ordinances. Signage permits are issued by the Zoning Section of the Department of Planning and Community and Economic Development.

Fire Department (Contact Matt Hamilton, (608) 266-4457)

- 52. Fire command center to be visible from vestibule entry. Relocate door to be visible from entry.
- 53. Provide fire service access elevators
- 54. NFPA 14 requires multiple FDCs remotely located from each other. MFD anticipates that FDCs will be located along the Brooks St and Johnson St elevations.
- 55. Please consider allowing Madison Fire Dept. to conduct training sequences prior to demolition. Contact MFD Training Division to discuss possibilities: Division Chief Ron Blumer at rblumer@cityofmadison.com or (608) 266-5959.

Parks Division (Contact Ann Freiwald, (608) 243-2848)

56. Park Impact Fees (comprised of the Park Infrastructure Impact Fee, per MGO Sec. 20.08(2)), and Park-Land Impact Fees, per MGO Sec. 16.23(8)(f) and 20.08(2) will be required for all new residential development associated with this project. This development is within the West Park-Infrastructure Impact Fee district. Please reference ID# 24032 when contacting Parks Division staff about this project.

Water Utility (Contact Jeff Belshaw, (608) 261-9835)

57. A Water Service Application Form and fees must be submitted before connecting to the existing water system. Provide at least two working days' notice between the application submittal and the requested installation or inspection appointment. Application materials are available on the Water Utility's Plumbers & Contractors website (http://www.cityofmadison.com/water/plumbers-contractors); otherwise, they may be obtained

from the Water Utility Main Office at 119 E Olin Avenue. A licensed plumber signature is required on all water service applications. For new or replacement services, the property owner or authorized agent is also required to sign the application. A Water Meter Application Form will subsequently be required to size & obtain a water meter establish a Water Utility customer account and/or establish a Water Utility fire service account. If you have questions regarding water service applications, please contact Madison Water Utility at (608) 266-4646.

58. The Madison Water Utility shall be notified to remove the water meter at least two working days prior to demolition. Contact the Water Utility Meter Department at (608) 266-4765 to schedule the meter removal appointment.

Forestry Section (Contact Bradley Hofmann, (608) 267-4908)

- 59. An existing inventory of street trees located within the right of way shall be included on the site, demo, utility, landscape, grading, fire aerial apparatus and street tree plan sets. The inventory shall include the following: location, size (diameter at 4 1/2 feet), and species of existing street trees. The inventory should also note if a street tree is proposed to be removed and the reason for removal.
- 60. All proposed street tree removals within the right of way shall be reviewed by City Forestry before the Plan Commission meeting. Street tree removals require approval and a tree removal permit issued by City Forestry. Any street tree removals requested after the development plan is approved by the Plan Commission or the Board of Public Works and City Forestry will require a minimum of a 72-hour review period which shall include the notification of the alderperson within who's district is affected by the street tree removal(s) prior to a tree removal permit being issued. Add as a note on the street tree plan set.
- 61. Contractor shall take precautions during construction to not disfigure, scar, or impair the health of any street tree. Contractor shall operate equipment in a manner as to not damage the branches of the street tree(s). This may require using smaller equipment and loading and unloading materials in a designated space away from trees on the construction site. Any damage or injury to existing street trees (either above or below ground) shall be reported immediately to City Forestry at (608) 266-4816. Penalties and remediation shall be required. Add as a note on the site, grading, utility, demolition, and street tree plan set.
- 62. As defined by the Section 107.13 of City of Madison Standard Specifications for Public Works Construction: No excavation is permitted within 5 feet of the trunk of the street tree or when cutting roots over 3 inches in diameter. If excavation is necessary, the Contractor shall contact Madison City Forestry at (608) 266-4816 prior to excavation. City of Madison Forestry personnel shall assess the impact to the tree and to its root system prior to work commencing. Tree protection specifications can be found on the following website: https://www.cityofmadison.com/business/pw/specs.cfm Add as a note on the site, grading, utility, demolition and street tree plan sets.
- 63. Section 107.13(g) of City of Madison Standard Specifications for Public Works Construction (website: https://www.cityofmadison.com/business/pw/specs.cfm) addresses soil compaction near street trees and shall be followed by Contractor. The storage of parked vehicles, construction equipment, building materials, refuse, excavated spoils or dumping of poisonous materials on or around trees and roots within five (5) feet of the tree or within the protection zone is prohibited. Add as a note on both the site and street tree plan sets.

- 64. On this project, street tree protection zone fencing is required. The fencing shall be erected before the demolition, grading or construction begins. The fence shall include the entire width of terrace and, extend at least 5 feet on both sides of the outside edge of the tree trunk. Do not remove the fencing to allow for deliveries or equipment access through the tree protection zone. Add as a note on both the site and street tree plan sets.
- 65. Street tree pruning shall be coordinated with City Forestry at a minimum of two weeks prior to the start of construction for this project. Contact City Forestry at (608)266-4816. All pruning shall follow the American National Standards Institute (ANSI) A300 Part 1 Standards for pruning. Add as a note on both the site and street plan sets.
- 66. The Developer shall submit a Street Tree Report performed by International Society of Arboriculture Certified Arborist for City Forestry's review of project. This report shall identify all street trees on proposed project site, species type, canopy spread, tree condition, proposed tree removals, the impacts of proposed construction, and any requested pruning.
- 67. The Developer shall post a security deposit prior to the start of the development to be collected by City Engineering as part of the Developers Agreement. In the event that street trees are damaged during the construction process, City Forestry will draw from this deposit for damages incurred.
- 68. Additional street trees are needed for this project. Tree planting specifications can be found in section 209 of Specifications City of Madison Standard for Public Works Construction (website: https://www.cityofmadison.com/business/pw/specs.cfm) - All street tree planting locations and tree species within the right of way shall be determined by City Forestry. A landscape plan and street tree planting plan shall be submitted in PDF format to City Forestry for approval of planting locations within the right of way and tree species. All available street tree planting locations shall be planted within the project boundaries. Add following note on both the landscape and street tree plan sets: At least one week prior to street tree planting, Contractor shall contact City Forestry at (608) 266-4816 to schedule inspection and approval of nursery tree stock and review planting specifications with the landscaper.
- 69. On this project, the installation of a pavement support system (Silva Cell®, GreenBlue® or equivalent as approved by city) surrounding tree grate locations is required where the terrace is concrete. The Contractor shall contact City Forestry at (608) 266-4816 to identify and locate underground conflicts (utilities, vaults, conduit) or other underground obstructions and determine grate locations before support system installation. Add as a note on both the site and street tree plan set.
- 70. No later than five business days prior to forming concrete and constructing tree grate sites, the Contractor shall contact City Forestry at (608) 266-4816 to identify and locate underground conflicts (utilities, vaults, conduit) or other underground obstructions and determine grate locations. The Contractor will be required to obtain the grate, matching frame and/or tree guard. Tree grate type and matching frame: Neenah 4x8 (R-8815-A). Tree guard Neenah (R-8501-4818). Add as a note on both the landscape and street tree plan set.

Metro Transit (Contact Tim Sobota, (608) 261-4289)

- 71. The existing curbside bus stop zone and accessible pedestrian sidewalk and concrete boarding pad on the east side of North Brooks Street, north of West Johnson Street, provides critical access to the City's transit operations, and any planned or permitted obstruction of the existing bus stop zones in excess of thirty (30) days may require additional right-of-way improvements and/or other mitigation measures in coordination with Traffic Engineering and Metro Transit staff in order to create an alternate bus stop zone that would serve the North Brooks Street at West Johnson Street intersection area in a comparable operational and accessible manner.
- 72. Metro Transit operates daily all-day transit service along North Brooks Street opposite this property with trips at least every 60 minutes (every 30 minutes or less during the day on weekdays). Metro Transit operates additional daily all-day transit service along University Avenue, West Johnson Street and Park Street near this property with trips on most routes at least every 30 minutes.
- 73. Metro Transit would initially estimate the following counts of potentially eligible trips towards US Green Building Council/LEED Quality Access to Transit points: 444 Weekday & 201 Weekend (average). Please contact Metro Transit if additional analysis would be of interest.

Certified Survey Map

Recommended Conditions of Approval

Major/Non-Standard Conditions are Shaded

City Engineering Division (Contact Tim Troester, (608) 267-1995)

- 74. Enter into a City / Developer agreement for the required infrastructure improvements. Agreement to be executed prior to sign off. Allow 4-6 weeks to obtain agreement. Contact City Engineering to schedule the development and approval of the plans and the agreement. (MGO 16.23(9)c)
- 75. Construct sidewalk, terrace, curb and gutter and street and alley pavement to a plan as approved by City Engineer.
- 76. Madison Metropolitan Sewerage District (MMSD) charges are due and payable prior to City Engineering Division sign-off, unless otherwise collected with a Developer's / Subdivision Contract. Contact Mark Moder (261-9250) to obtain the final MMSD billing a minimum of two (2) working days prior to requesting City Engineering signoff.
- 77. A minimum of two (2) working days prior to requesting City Engineering signoff on the CSM, contact either Tim Troester (West) at (608) 261-1995 (ttroester@cityofmadison.com) or Brenda Stanley (East) at (608) 261-9127 (bstanley@cityofmadison.com) to obtain the final stormwater utility charges that are due and payable prior to sub-division of the properties. The stormwater utility charges (as all utility charges) are due for the previous months of service and must be cleared prior to the land division (and subsequent obsolesces of the existing parcel).

<u>City Engineering – Mapping</u> (Contact Julius Smith, (608) 264-9276)

- 78. Grant a Public Sidewalk Easement to the City on the face of this Certified Survey Map along W. Johnson St. and N. Brooks St. the final location and width to be approved by City Engineering and Traffic Engineering. Contact Jule Smith (jsmith4@cityofmadison.com) with Engineering Mapping for the final language to be included on the face of the map. Note the Easement along N. Brooks St. shall be at a minimum 1' behind the existing back of sidewalk, but is up to the final approval and requirements of City Engineering and Traffic Engineering
- 79. Coordinate and request from the utility companies serving this area the easements required to serve this development. Those easements shall be properly shown, dimensioned and labeled on the final plat.
- 80. Note the original document that set forth the rights for the joint driveway easement per Document No. 4403778 and document No.4409641. Additionally note somewhere that the agreement subjected the parcel to a parking agreement.
- 81. The concurrent proposed developments location appears to conflict the 6' wide underground electrical easement per Document No. 3605277 this document should be released, and removed from the CSM after the release has been recorded. If not a note should be made that is location has been show by location of the actual facilities and the recorded as 30' distances shall be shown for L1 and L3
- 82. Note Parcel is Subject to Land Use Restriction Agreement Recorded as Document No. 2937168. It appears this agreement will require it to be released to allow for the development to proceed.
- 83. Add the City of Madison as a Cooperate Mortgagee for this CSM
- 84. Wisconsin Administrative Code A-E 7.08 identifies when Public Land System (PLS) tie sheets must be filed with the Dane County Surveyor's office. The Developer's Surveyor and/or Applicant must submit copies of any required U.S. public land survey monument record provided to the County Surveyor's Office, or, in instances where a public the monuments and witness ties area recovered under A-E 7.08(1g), the Surveyor shall provide to the City of Madison monument condition reports (with current tie sheet attached) for all Public Land Survey monuments, including center of sections of record, used in this survey, to Julius Smith (JSmith4@cityofmadison.com) of City Engineering, Land Information.
- 85. In accordance with Section s. 236.18(8), Wisconsin Statutes, the Applicant shall reference City of Madison WCCS Dane Zone, 1997 Coordinates on all PLS corners on the Plat or Certified Survey Map in areas where this control exists. The Surveyor shall identify any deviation from City Master Control with recorded and measured designations. Visit the Dane County Surveyor's Office (web address https://www.countyofdane.com/PLANDEV/records/surveyor.aspx) for current tie sheets and control data that has been provided by the City of Madison.
- 86. Prior to Engineering final sign-off by main office for Plats or Certified Survey Maps (CSM), the final Plat or CSM in pdf format must be submitted by email transmittal to Engineering Land Records Coordinator Jule Smith (Jsmith4@cityofmadison.com) for final technical review and approval. This submittal must occur a minimum of two working days prior to final Engineering Division sign-off.
- 87. Per A-E 7.05(11) list the Datum and Adjustment of the coordinate system used. eg. NAD83(97) or (2011)

- 88. Along the West line of the parcel show the recorded as bearing and distance from Conklin Replat of N1°18'E 166.35' and the recorded as distance of 105.0' along the South lines of lots 4-6.
- 89. The pending Certified Survey Map application for this property shall be completed and recorded with the Dane County Register of Deeds (ROD), the new parcel data created by the Assessor's Office and the parcel data available to zoning and building inspection staff prior to issuance of building permits and early start permits for new construction.
- 90. Submit to Jule Smith, prior to Engineering sign-off of the subject plat, one (1) digital CADD drawing in a format compatible with AutoCAD. The digital CADD file(s) shall be referenced to the Dane County Coordinate System and shall contain, at minimum, the list of items stated below, each on a separate layer/level name. The line work, preferably closed polylines for lot lines, shall be void of gaps and overlaps and match the final recorded plat:
 - a) Right-of-Way lines (public and private)
 - b) Lot lines
 - c) Lot numbers
 - d) Lot/Plat dimensions
 - e) Street names
 - f) Easement lines (i.e. all shown on the plat including wetland & floodplain boundaries.)

NOTE: This Transmittal is a separate requirement from the required Engineering Streets Section for design purposes. The Developer/Surveyor shall submit new updated final plat, electronic data for any changes subsequent to any submittal.

<u>Traffic Engineering Division</u> (Contact Luke Peters, (608) 266-6543)

- 91. The applicant shall dedicate Right of Way or grant a Public Sidewalk Easement for and be responsible for the construction of a minimum eight (8)-foot wide sidewalk, six (6)-foot terrace, and additional one (1) foot for maintenance, where applicable, along their site's frontage of East Johnson Street.
- 92. The applicant shall dedicate Right of Way or grant a Public Sidewalk Easement for and be responsible for the construction of a minimum five (5)-foot wide sidewalk, eight (8)-foot terrace, and additional one (1) foot for maintenance, where applicable, along their site's frontage of North Brooks Street.

Parking Review (Contact Trent W Schultz, (608) 246-5806)

The agency reviewed this project and determined a Transportation Demand Management (TDM) Plan is not required as part of certified survey map review. A TDM Plan will be reviewed as part of conditional use approval.

Parks Division (Contact Ann Freiwald, (608) 243-2848)

93. Park Impact Fees (comprised of the Park Infrastructure Impact Fee, per MGO Sec. 20.08(2)), and Park-Land Impact Fees, per MGO Sec. 16.23(8)(f) and 20.08(2) will be required for all new residential development associated with this project. This development is within the Central Park-Infrastructure Impact Fee district. Please reference ID# 24032 when contacting Parks about this project.

- 94. The following note should be included on the CSM: "LOTS WITHIN THIS SUBDIVISION ARE SUBJECT TO IMPACT FEES THAT ARE DUE AND PAYABLE AT THE TIME BUILDING PERMIT(S) ARE ISSUED."
- 95. The Parks Division shall be required to sign off on this CSM.

Office of Real Estate Services (Contact Heidi Radlinger, (608) 266-6558)

- 96. Prior to approval sign-off by the Office of Real Estate Services, the Owner's Certificate on the CSM shall be executed by all parties of interest having the legal authority to do so, pursuant to Wis. Stats. 236.21(2)(a). The title of each certificate shall be consistent with the ownership interest(s) reported in the most recent title report.
 - A PDF of the CSM containing electronic signatures shall be provided to ORES to obtain approval sign-off.
- 97. If any portion of the lands within the CSM boundary are subject to an Option to Purchase or other Option interest, include a Certificate of Consent for the option holder and have it executed prior to CSM sign-off, if said ownership interest meets the criteria set forth by Wis. Stat. Sec. 236.34 and Sec. 236.21(2)(a).
 - A Consent of Lessee certificate shall be included on the CSM for all tenant interests in excess of one year, recorded or unrecorded, and executed prior to CSM sign-off.
- 98. If the lands within the CSM boundary are partially located within a Tax Incremental Financing District, discussions with Joe Gromacki, the City of Madison's tax increment financing coordinator, may be necessary before recording the CSM if a TIF application is required. Mr. Gromacki can be reached at 608-267-8724 or igromacki@cityofmadison.com.
- 99. There are no special assessments reported on the subject parcel. If special assessments are levied against the property during the review period prior to CSM approval sign-off they shall be paid in full pursuant to Madison General Ordinance Section 16.23(5)(g)1.
- 100. Pursuant to Madison City Ordinance Section 16.23(5)(g)(4), the owner shall furnish an updated title report to Heidi Radlinger as well as the survey firm preparing the proposed CSM. The report shall search the period subsequent to the date of the initial title report (4/15/24) submitted with the CSM application and include all associated documents that have been recorded since the initial title report. A title commitment may be provided, but will be considered only as supplementary information to the title report update. Surveyor shall update the CSM with the most recent information reported in the title update. ORES reserves the right to impose additional conditions of approval in the event the title update contains changes that warrant revisions to the CSM.
- 101. The owner shall email the document number of the recorded CSM to Heidi Radlinger as soon as the recording information is available.

The following agencies have reviewed this request and has recommended no conditions of approval:

Planning, Zoning, Assessor's Office, Fire, Water Utility, Forestry, Metro

PLANNING DIVISION STAFF REPORT - ADDENDUM

July 8, 2024



PREPARED FOR THE COMMON COUNCIL

Project Addresses: 306 N Brooks Street

Application Type: Demolition Permit, Zoning Map Amendment, Conditional Uses, and Certified Survey

Map Referral

Legistar File ID # 83527, 83907, 83529, and 83531

Prepared By: Chris Wells, Planning Division

Addendum

The applicant has not provided information in their submitted materials about their move in/move out approach of building residents, accommodations for food and Amazon/UPS/Fed Ex deliveries, etc. Therefore, the Planning Division recommends the following condition of approval to address the need for a management plan:

"A management plan addressing items such as move-in/move-out, security, food and other deliveries, and other items shall be provided on the form from the Zoning Administrator with details to be approved by the Zoning Administrator and Planning Division Director (or designees) prior to final sign-off and permit issuance."

No other changes to conditions/comments in the July 8, 2024 Planning Division Staff Report are recommended at this time.



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83908

File ID: 83908 File Type: Ordinance Status: Council Public

Hearing

Version: 1 Reference: Controlling Body: PLAN

COMMISSION

File Created Date: 06/11/2024

Enactment Number:

File Name: University Row & Silvertree Run Rezone Final Action:

Title: Creating Section 28.022-00677 of the Madison General Ordinances to approve an Amended Specific Implementation Plan of a Planned Development District for

property located at 702 University Row and 5119 Silvertree Run. (District 19)

Notes: 6877UniversityRowSilvertreeRZ

Sponsors: Planning Division Effective Date:

Attachments: Locator Maps.pdf, Application.pdf, Letter of

approval passed by voice vote/ other

Intent.pdf, Existing Site Plan.pdf, Staff Comments.pdf, 702 University Row.pdf

Author: Kate Smith Hearing Date:

Entered by: mglaeser@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Attorney's Office	06/11/2024	Referred for Introduction				
	Action Text: Notes:	This Ordinance was Refe Plan Commission (Public He	erred for Introduction earing - 7/8/24), Common Co	uncil (7/16/24)			
1	COMMON COUN	NCIL 06/18/2024	Referred for Public Hearing	PLAN COMMISSION		07/08/2024	
	Action Text:	This Ordinance was Refe	erred for Public Hearing to	the PLAN COMMISSIC	N		
1	PLAN COMMISS	ION 07/08/2024	RECOMMEND TO COUNCIL TO ADOPT - PUBLIC HEARING				Pass
	Action Text:	A motion was made by D	Duncan, seconded by Men	dez, to RECOMMEND T	O COUNCIL T	O ADOPT -	
		PUBLIC HEARING. The	motion passed by voice v	ote/other.			
	Notes:	•	n, seconded by Mendez, the F he zoning map amendment to				

Text of Legislative File 83908

Fiscal Note

No City appropriation required.

Title

Creating Section 28.022-00677 of the Madison General Ordinances to approve an Amended Specific Implementation Plan of a Planned Development District for property located at 702 University Row and 5119 Silvertree Run. (District 19)

Body

DRAFTER'S ANALYSIS: This ordinance amends the University Row Specific Implementation Plan (ORD-23-00073) for property located at 702 University Row and 5119 Silvertree Run to re-approve the use of existing 300-stall temporary parking lot until October 31, 2024.

The Common Council of the City of Madison do hereby ordain as follows:

1. WHEREAS, an Amended Planned Development District Specific Implementation Plan has been duly filed for approval of the Madison Common Council and is hereby made an integral component of the zoning district regulations.

NOW, THEREFORE, the Common Council of the City of Madison do ordain as follows:

"28.022-00677. An Amended Planned Development District Specific Implementation Plan is hereby approved for the following described property:

Lots 2 and 3, University Crossing, City of Madison, Dane County, Wisconsin. Said parcel contains 4.76 acres."



Amended PD(SIP)

Location

702 University Row & 5119 Silvertree Run

Applicant

Mike McKay, Melissa Flint UW Health - UW Hospitals & Clinics Authority

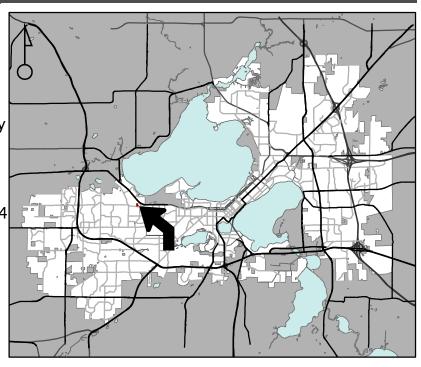
Request

Re-approve use of existing 300-stall temporary parking lot until October 31, 2024

Public Hearing Dates
Plan Commission
July 8, 2024

Common Council

July 16, 2024



RISSER RD SR-C2 22 BRODY TOMAHAWK TRL S/R-C SR-V2 FLAMBEAU RD TR√P PD NMX PD PD PD SE WE GETT NMX SR-V NMX NMX PD MERRILL CREST D FROSTY LN TR-C2 SR-C2 HAMPTON CT PΦ

For Questions Contact: Tim Parks at: 261-9632 or tparks@cityofmadison.com or City Planning at 266-4635

Scale: 1" = 400' City of Madison, Planning Division: JC: Date: 6-4-252









LAND USE APPLICATION - INSTRUCTIONS & FORM



City of Madison Planning Division Madison Municipal Building, Suite 017 215 Martin Luther King, Jr. Blvd. P.O. Box 2985 Madison, WI 53701-2985 (608) 266-4635



FUR OFFICE USE	CONLY:	
Date Received	5/28/24 11:58 a.m.	Initial Submittal
	Paid	Revised Submittal
	<u></u>	

All Land Use Applications must be filed with the Zoning Office. Please see the revised submittal instructions on Page 1 of this document.

This completed form is required for all applications for Plan Commission review except subdivisions or land divisions, which should be filed using the <u>Subdivision Application</u>. If your project requires both Land Use <u>and</u> Urban Design Commission (UDC) submittals, a completed <u>UDC Application</u> and accompanying submittal materials are also required to be submitted.

If you need an interpreter, translator, materials in alternate formats or other accommodations to access these forms, please call the Planning Division at (608) 266-4635.

Si necesita interprete, traductor, materiales en diferentes formatos, u otro tipo de ayuda para acceder a estos formularios, por favor llame al (608) 266-4635.

Yog tias koj xav tau ib tug neeg txhais lus, tus neeg txhais ntawv, los sis xav tau cov ntaub ntawv ua lwm hom ntawv los sis lwm cov kev pab kom paub txog cov lus qhia no, thov hu rau Koog Npaj (Planning Division) (608) 266-4635.

APPLICATION FORM	
1. Project Information	
Address (list all addresses of	on the project site):
Title:	
2. This is an application fo	r (check all that apply)
Zoning Map Amendm	ent (Rezoning) from to
Major Amendment to	an Approved Planned Development - General Development Plan (PD-GDP)
x Major Amendment to	an Approved Planned Development - Specific Implementation Plan (PD-SIP)
Review of Alteration	to Planned Development (PD) (by Plan Commission)
Conditional Use or M	ajor Alteration to an Approved Conditional Use
Demolition Permit	Other requests
3 Annlicant Agent and P	roperty Owner Information
	Company
	City/State/Zip
	Email
Project contact person	Company
Street address	City/State/Zip
Telephone	Email
Property owner (if not ap	plicant)
Street addresk	City/State/Zip
Telephone	Email

LAND USE APPLICATION - INSTRUCTIONS & FORM



APPLICATION FORM (CONTINUED)			
5. Project Description				
Provide a brief description of the	project and all propose	ed uses of the site:		
Proposed Square-Footages by Ty	 /pe:			
	Commercial (net)	:	Office (net):	
Overall (gross):			Institutional (net	
Proposed Dwelling Units by Type				
Efficiency: 1-Bedroom	: 2-Bedroom:	3-Bedroom:	4 Bedroom:	5-Bedroom:
Density (dwelling units per acr	^e):	Lot Area (in square	e feet & acres):	
Proposed On-Site Automobile Pa	arking Stalls by Type (if	applicable):		
Surface Stalls: Under-Bu	uilding/Structured:			
Proposed On-Site Bicycle Parking	g Stalls by Type (if appli	cable): ¹ See <u>Se</u>	ction 28.141(8)(e), MG	of for more information
Indoor (long-term): O	outdoor (short-term):			
Scheduled Start Date:		Planned Comp	oletion Date:	
6. Applicant Declarations				
Pre-application meeting with the proposed development a		• • •		•
Planning staff			Date	
Zoning staff			Date	
Posted notice of the proposed of	demolition on the <u>City's De</u>	molition Listserv (if a	pplicable). Date Posted	
Public subsidy is being requ	ested (indicate in letter	of intent)		
Pre-application notification neighborhood and business of the pre-application notifineighborhood association(s	associations <u>in writing</u> fication or any correspo	no later than 30 ondence granting	days prior to FILING a waiver is required	this request. Evidence
District Alder			Date	
Neighborhood Association(s	5)		Date	
Business Association(s)			Date	
The applicant attests that this form	n is accurately complete	ed and all required	d materials are subm	itted:
Name of applicant		Relations	hip to property	
Authorizing signature of property ow	vner		Date	

To: City of Madison Planning Department

From: Juli Aulik, Director, Community Relations, UW Health

RE: Letter of Intent regarding conditional use of parking lots at 702-750 University Row

Date: May 28, 2024

UW Health requests in the attached application the following: Conditional use of now-vacant parking lots at 702-750 University Row, Lots 2 and 3 of University Crossing for short-term parking by new trainees hired by the organization to work at its new medical center in The American Center on Madison's far-east side. The trainees need to receive training at University Hospital while construction of the new medical center is completed. In addition to the conditional use, the application is for: Major Amendment to an Approved Planned Development - Specific Implementation Plan (PD-SIP)

Existing site conditions: Vacant parking lots

Project schedule: Allowing parking upon approval through October 31, 2024

Phasing plan: N/A

Proposed uses: Parking

Hours of operation: 6 am - 6 pm

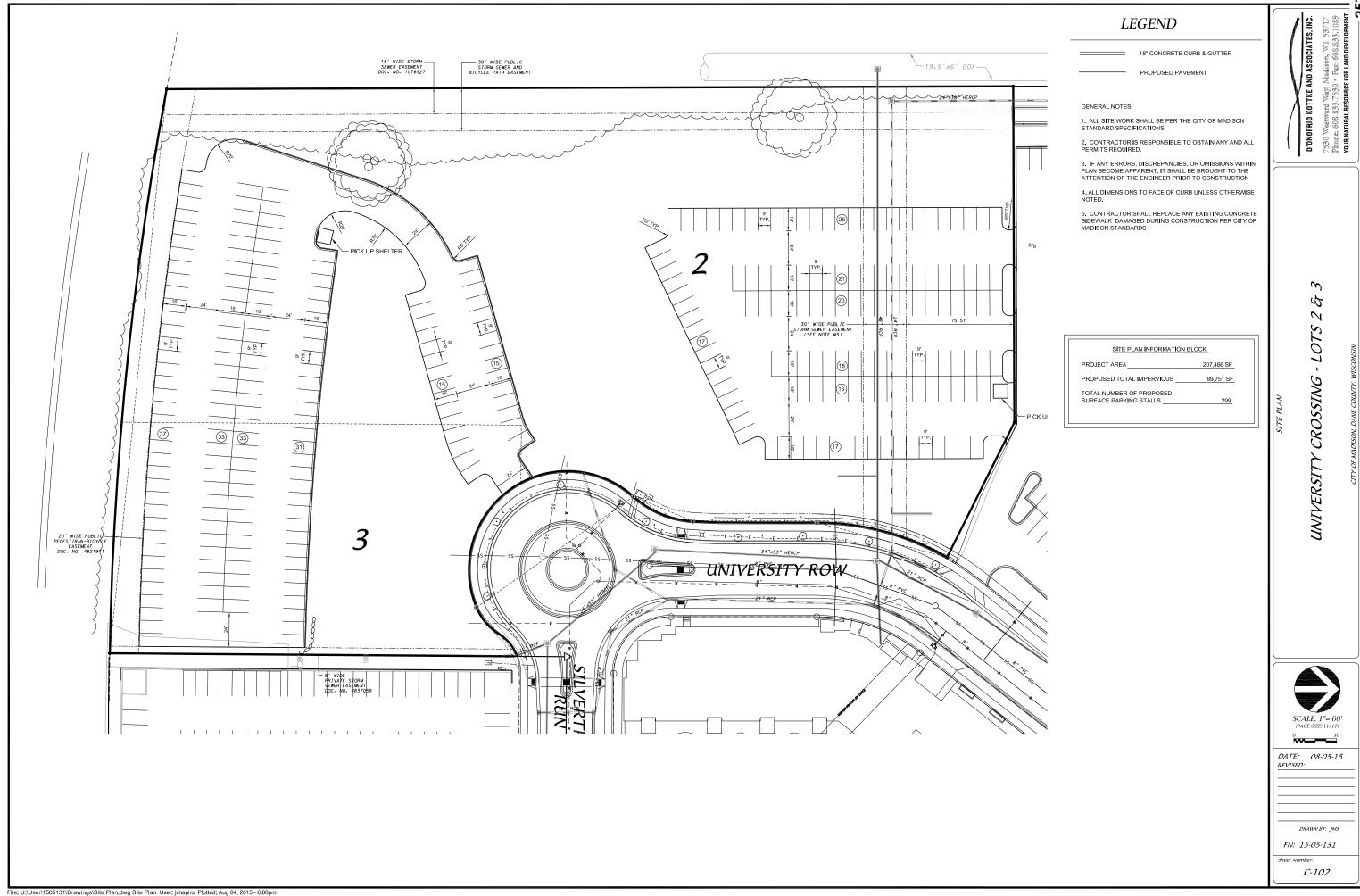
Number of employees: 300 approximately based on 300 spaces

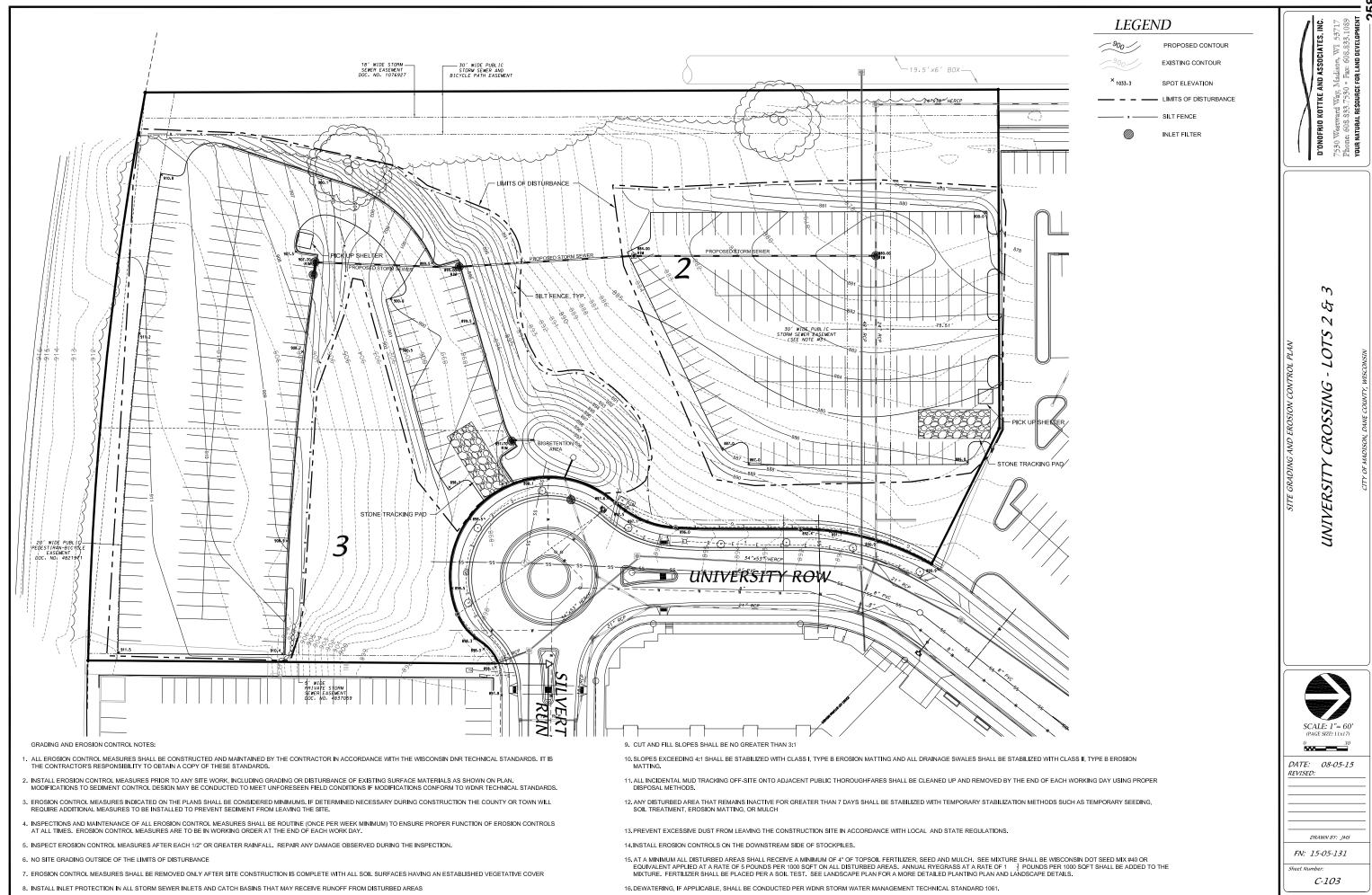
Gross square footage: 207,463

Number of units and bedrooms: N/A

Public subsidy requested: None

Project team: N/A





PREPARED FOR THE PLAN COMMISSION

Project Address: 702 University Row and 5119 Silvertree Run

Application Type: Amended Planned Development – Specific Implementation Plan

Legistar File ID # 83908

Prepared By: Timothy M. Parks, Planning Division

Report includes comments from other City agencies, as noted.

Summary

Applicant & Property Owner: Mike McKay and Melissa Flint, University Hospital and Clinics Authority; 7974 UW Health Court; Middleton.

Requested Actions: Approval of an amended Planned Development–Specific Implementation Plan for 702 University Row and 5119 Silvertree Run (Lots 2 and 3 of University Crossing) to allow a 299-stall temporary parking facility to be used until October 31, 2024 and beyond a previously approved expiration date.

Proposal Summary: The University Hospital and Clinics Authority and UW Health are requesting approval of an amended Specific Implementation Plan to allow two surface parking lots containing 299 stalls on Lots 2 and 3 of the University Crossing Planned Development to be used until October 31, 2024.

The lots, operated together as University of Wisconsin-Madison Lot 203, were first approved in 2015 as temporary off-site parking for University Hospital until a January 1, 2019 expiration date, at which time the lot was supposed to close and be removed. An amended Specific Implementation Plan was approved in 2018 to allow the temporary lot to remain for four additional years until January 1, 2023, and for the installation of full improvements for the parking lots to be postponed until after that date. UW Health is hoping to use the parking facility for off-site parking for University Hospital as soon as all regulatory approvals have been granted to accommodate employees being trained for a new medical facility currently under construction at the American Center. Use of the lot under the current request would cease on October 31, 2024.

Applicable Regulations & Standards: The approval process and standards for Planned Development districts is outlined in Section 28.098 of the Zoning Code. Section 28.182 of the Zoning Code provides the process for zoning map amendments.

The subject site is located in Urban Design District 6, the rules for which are outlined in Section 33.24(13) of the Urban Design Commission ordinance. However, no changes to the existing parking lots are proposed; therefore, staff does not feel that review by the Urban Design Commission is required to allow the extension requested to proceed.

Review Required By: Plan Commission and Common Council.

Summary Recommendation: The Planning Division recommends that the Plan Commission forward Zoning Map Amendment ID 28.022–00677, amending the Specific Implementation Plan for 702 University Row and 5119 Silvertree Run to extend the approval to use two existing temporary parking lots, to the Common Council with a recommendation of **approval** subject to the input at the public hearing and the conditions from reviewing agencies beginning on page 5 of this report.



Background Information

Parcel Location: The subject site is a 4.76-acre parcel generally located on the west side of University Row between University Avenue and Silvertree Run; Alder District 19 (Guequierre); Urban Design District 6; Madison Metropolitan School District.

Existing Conditions and Land Use: The subject site is developed with two surface parking lots totaling 299 stalls, zoned PD.

Surrounding Land Uses and Zoning:

North: UW Health Digestive Health Clinic, zoned PD;

South: Wisconsin Southern Railroad right of way and Old Middleton Road;

<u>West:</u> Spring Harbor Animal Hospital, Harbor View Apartments, Moka, Motorcycle Performance, zoned SE (Suburban Employment District); Trillium Homes, zoned TR-P (Traditional Residential—Planned District);

<u>East:</u> Wisconsin Public Service Commission Building, zoned SE; "University Row" mixed-use building with 115 apartments and 5,800 square feet of first floor commercial and 340-stall parking garage, zoned PD.

Adopted Land Use Plans: The 2023 <u>Comprehensive Plan</u> identifies the site and University Crossing development for Community Mixed-Use (CMU) development. The Growth Priority Areas map on page 16 identifies the site as part of a transitioning community activity center and University Avenue as a regional growth corridor.

The subject site is also located within the boundaries of the 2006 <u>Spring Harbor Neighborhood Plan</u>, which recommends the western two-thirds of the property for employment uses and for community mixed-use development along N Whitney Way. The <u>Spring Harbor Neighborhood Plan</u> also includes a variety of design recommendations for new development and the University Avenue corridor.

Zoning Summary: The site is zoned PD. The project will be reviewed in the following sections.

Other	Other Critical Zoning Items					
Yes:	Wellhead Protection (Zone A, WP 14), Urban Design (PD zoning, Urban Design Dist. 6), Utility Easements;					
Transit-Oriented Development (TOD) Overlay District; Barrier Free						
No:	Floodplain, Landmarks, Waterfront Development, Adjacent to Parkland					
	Prepared by: Jenny Kirchgatter, Assistant Zoning Administrator					

Environmental Corridor Status: This property is not located within a mapped environmental corridor

Public Utilities and Services: This property is served by a full range of urban services. Metro Transit operates daily transit service along University Avenue and through University Crossing on University Row and Silvertree Run.

Previous Approvals and Project History

On October 4, 2011, the Common Council approved a request to rezone 5063-5119 University Avenue and 702 N Whitney Way from C3 (Highway Commercial District) to Planned Unit Development-General Development Plan (PUD-GDP) [1966 Zoning Code] to guide redevelopment of the site as a mixed-use/employment development, and

Planned Unit Development-General Development Plan-Specific Implementation Plan (PUD-GDP-SIP) to allow construction of a 60,000 square-foot medical office building on a 3.75-acre parcel in the first phase, and approved the preliminary plat and final plat of University Crossing, creating seven lots for the future development and one outlot for public stormwater management. The Plan Commission approved a demolition permit on September 19, 2011 to allow eleven commercial buildings to be demolished as part of the proposed redevelopment, and recommended approval of the rezoning and subdivision plat.

The 2011 General Development Plan for University Crossing calls for two medical office/ clinic buildings to be built in phases on Lot 2, including a four-story, 70,000 square-foot building to be located along the west side of University Row, and a three-story, 80,000 square-foot building to be located on the northwestern corner of the University Row-Silvertree Run roundabout. Construction of a three-story, 40,000 square-foot medical-related hospitality use was planned on Lot 3 on the south side of Silvertree Run adjacent to the parking structure for the Public Service Commission Building. This cluster of buildings was to be served by surface parking, with two three-story parking structures with approximately 800 spaces proposed to eventually serve this portion of the development. Overall, approximately 488,200 gross square feet of space is planned in seven buildings across the larger 14.3-acre University Crossing site, with the 1,360 to 1,460 parking spaces to be located in a variety of surface stalls and in three parking structures. Four of the approved buildings and one parking structure have been built so far.

On October 6, 2015, the Common Council approved a Specific Implementation Plan for 702 University Row (University Crossing, Lots 2 and 3) to allow construction of a *temporary* parking lot with 299 stalls subject to conditions, including a condition requiring that the approval for the temporary parking lots shall expire on January 1, 2019, at which time the lots shall be closed. Within 90 days of that date, the condition required that the parking areas be demolished, the curb cuts removed, and the terrace and parking areas seeded with grass unless the applicant has received approval from the Common Council of an amended Specific Implementation Plan to allow the parking areas to remain.

On October 16, 2018, the Common Council approved an amended Planned Development–Specific Implementation Plan for 702 University Row (Lots 2 and 3 of University Crossing) to allow the 299-stall temporary parking facility to remain for four additional years beyond a previously approved expiration date, until January 1, 2023. As a condition of approval of the amended SIP to allow the lots to remain, the applicant would be required to submit plans for the parking areas that include landscaping in excess of the minimums required in Urban Design District 6, sidewalks to connect the parking lots to University Row and Silvertree Run, and permanent curb, gutter, walkways and lighting.

On July 25, 2023, the Common Council approved an Amended Planned Development—General Development Plan for University Crossing and a Specific Implementation Plan to allow construction of a four-story, 180,000 square-foot addition to the existing UW Digestive Health Clinic and a 830-stall parking garage. The amended General Development Plan also includes a 110-unit multi-family dwelling to be located at the southern edge of the site. The Council also approved a two-lot CSM to re-divide Lots 1-3 of the University Crossing plat to accommodate the proposed addition and future multi-family dwelling. Construction of the proposed addition and parking structure were schedule to commence later in 2023, with completion scheduled in early 2026; no timeline was given for construction of the multi-family building.

Project Description, Analysis and Conclusion

The University Hospital and Clinics Authority and UW Health are requesting approval of an amended Specific Implementation Plan to allow two temporary parking lots containing a total of 299 stalls to be used beyond a previously approved January 1, 2023 expiration date. The temporary lots were constructed on Lots 2 and 3 of the University Crossing subdivision and Planned Development to provide replacement satellite parking for UW Health employees that previously parked at the Hill Farms State Office Building on Sheboygan Avenue prior to its redevelopment.

The temporary lots consist of a 135-stall extension of the surface parking lot located south of the Digestive Health Clinic at 750 University Row, and a separate 164-stall lot primarily located along the southernmost property line of the development. Together, the two lots are signed as "Lot 203" and have been operated by University of Wisconsin-Madison Transportation Services according to signage located at the entrance of each lot. Access to the southern parking area is provided from a driveway extending into the site from the University Row–Silvertree Run roundabout, while the northern lot is accessed from two driveways from University Row that serve the Digestive Health Clinic. Bus stop-style shelters are located in each lot, and previously, private shuttle buses were used to transport users of the lot to University Hospital. The parking lots were installed with minimal site lighting, and landscaping is primarily limited to prairie plantings.

As noted above, the 2015 approval was conditioned upon the closure of the lots on January 1, 2019 and removal of the lots by April 1, 2019 unless the applicant received approval from the Common Council of an amended Specific Implementation Plan to allow the parking areas to remain. Such approval was granted in 2018 to allow the lots to remain until the beginning of 2023. If the lots were to remain past the expiration dates, the prior approvals required the lots to be landscaped in accordance with Urban Design District 6 requirements and for the lots to have curbs/wheel stops, sidewalks, lighting, etc. similar to those found in other permanent parking lots or for the lots to be demolished. The parking lots were neither improved to a more permanent condition or demolished as stipulated in the earlier approvals, though regular use of the parking lots appears to have ceased consistent with the 2018 approval.

The applicant is seeking approval of another amended Specific Implementation Plan to allow the parking lots to be reopened for use until October 31, 2024. As noted in the letter of intent, UW Health is requesting to use the stalls as off-site parking for employees of a new medical center located at the American Center. The employees will be trained at University Hospital while construction of the American Center facility is completed. Hours of operation for the re-opened off-site parking facility will be 6:00 AM to 6:00 PM.

The Planning Division believes that the Plan Commission and Common Council can find the standards met to allow the parking lots on Lots 2 and 3 of University Crossing to be reopened. As noted above, the applicant has received approvals to construct a 180,000 square-foot addition to the existing Digestive Health Center medical clinic and integrated 830-stall parking garage. However, the applicant is contemplating a revision to those plans to separate the parking and clinical buildings, which would require approval of another Amended PD(GDP-SIP) by the Urban Design Commission, Plan Commission and Common Council. Submittal of the subsequent amended development plan is forthcoming for consideration later this year. In the meantime, staff sees no issue with the existing parking lots being used on an interim basis through the remainder of 2024 while either the approved clinic expansion project or an alternate version of those plans moves forward. Conditions from 2018 related to the demolition or improvement of the parking lots and limitations on use of the lots are carried forward for continuity. While the applicant is requesting to use the lots until October 31, 2024, staff is recommending a January 1, 2025 expiration

to provide a small buffer in the event that delays occur in the completion and opening of the American Center medical facility.

Recommendation

Planning Division Recommendation (Contact Timothy M. Parks, (608) 261-9632)

The Planning Division recommends that the Plan Commission forward Zoning Map Amendment ID 28.022–00677, amending the Specific Implementation Plan for 702 University Row and 5119 Silvertree Run to extend the approval to use two existing temporary parking lots, to the Common Council with a recommendation of **approval** subject to the input at the public hearing and the following conditions:

Recommended Conditions of Approval

Major/Non-Standard Conditions are Shaded

- 1. The approval to use the temporary parking lots shall expire on January 1, 2025, at which time the lots shall be closed. Within 90 days of that date, the parking areas shall be demolished, the curb cuts removed, and the terrace and parking areas seeded with grass unless the applicant has received approval from the Common Council of an amended Specific Implementation Plan to allow the parking areas to remain. As a condition of approval of an amended Specific Implementation Plan to allow the lots to remain, the applicant shall submit plans for the parking areas that include landscaping in excess of the minimums required in Urban Design District 6 and the Zoning Code, sidewalks to connect the parking lots to University Row and Silvertree Run, and permanent curb, gutter, walkways and lighting.
- 2. Use of the temporary parking lots and any shuttle transportation from the site shall continue to only be for use by off-site University Hospital and Clinics Authority and UW Health employees and permitted contractors. Use of the parking and shuttle shall be by permit only. No public parking shall be permitted. The parking lots shall not be used for special events.

The following conditions have been submitted by reviewing agencies:

<u>City Engineering Division</u> (Contact Tim Troester, (608) 267-1995)

The agency has reviewed this request and recommended no conditions of approval.

<u>City Engineering Division–Mapping Section</u> (Contact Julius Smith, (608) 264-9276)

The agency has reviewed this request and recommended no conditions of approval.

<u>Traffic Engineering Division</u> (Contact Luke Peters, (608) 266-6543)

The agency has reviewed this request and recommended no conditions of approval.

Zoning Administrator (Contact Jenny Kirchgatter, (608) 266-4429)

3. Show the existing accessible stalls on the site plan.

Fire Department (Contact Matt Hamilton, (608) 266-4457)

The agency has reviewed this request and recommended no conditions of approval.

Water Utility (Contact Jeff Belshaw, (608) 261-9835)

The agency has reviewed this request and recommended no conditions of approval.

Parks Division (Contact Ann Freiwald, (608) 243-2848)

The agency has reviewed this request and recommended no conditions of approval.

Metro Transit (Contact Tim Sobota, (608) 261-4289)

This agency has reviewed this request and recommended no conditions or approval.

Parking Division (Contact Trent Schultz, (608) 246-5806)

The agency has reviewed this request and recommended no conditions of approval. A Transportation Demand Management (TDM) Plan may be required as part of future development.

REQUEST FOR AMENDMENT TO THE MADISON GENERAL ORDINANCES

TO: Michael R. Haas, City At	torney	Proposed/Curr	ent Section No	
FROM: Tim Parks, Planning [Division	Repeal: _	ent: _X	
Please draft the following or	dinance:	Creation	:	
Note: Is this ordinance exe	empt from the provisions of S	Section 2.05(4)?		
	e appropriate paragraph num 2, 3, 4, 5, 6, 7, 8, 9]	ber under which exer	nption	
•	ture of the Mayor or the Ald is required below.	erperson who will spo	onsor	
See Attachment(s):				
Date to be Presented:	18 June 2024			
Referral(s): Plan Co	mmission: 8 July 2024; Co	mmon Council: 16 .	July 2024	
Fiscal Note: No F	iscal Impact	<u></u>		
Sponsor(s): Planr	ning Division			
When completed:				
Send DRAFT to:T	im Parks	(original wil	l be held until othe	erwise notified)
Send copy to:				
Note: Unless otherwise indi	cated, this ordinance will be s	submitted directly to	Common Council.	
If request is to rezone pro the ordinance can be drafted	. ,	nal information must b	e furnished before	
Rezone following property	у:			
Address 702 Univer	rsity Row and 5119 Silvert	ree Run	Alder District	19
From PD(SIP)	District	To Amended	PD(SIP)	District
	Iniversity Row Specific Imp		re-approve use	of existing
By Direction Of	Vata St	Dat	o: 11 luna 2024	



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84176

File ID: 84176 File Type: Appointment Status: Unfinished

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 06/26/2024

File Name: 7-16-2024 Resident committee appointments Final Action:

Title: Report of the Mayor submitting resident committee appointments (introduction

7-2-2024; action 7-16-2024).

Notes:

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Satya Rhodes-Conway Hearing Date:

Entered by: Idcosta@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Mayor's Office	06/26/2024	Referred for Introduction				
	Action Text:	This Appointment was R	Appointment was Referred for Introduction				
	Notes:	Confirm 7/16/24					
1	COMMON COUN	NCIL 07/02/2024	Refer to a future Meeting to Confirm	COMMON COUNCIL			Pass
	Action Text:	•	Figueroa Cole, seconded by Duncan, to Refer to a future Meeting to Confirm NCIL meeting of 7/16/24. The motion passed by voice vote/other.				
	Notes:	Confirm 7/16/24	-				

Text of Legislative File 84176

Title

Report of the Mayor submitting resident committee appointments (introduction 7-2-2024; action 7-16-2024).

Body

I hereby submit, for your consideration and approval, the following resident committee appointments.

BOARD OF PUBLIC WORKS

JOSEPH R. CLAUSIUS (17th A.D.) - reappoint to a two-year term to the position of Adult City Resident. First appointed 3-7-2017.

TERM EXPIRES: 4-30-2026

MICHAEL CULLEN (5th A.D.) - reappoint to a two-year term to the position of Adult City Resident - Engineering Background. First appointed 1-18-2019.

TERM EXPIRES: 4-30-2026

FOR INFORMATION ONLY

LISA STERN (11th A.D.) - reappoint to a two-year term to the position of Mayor or Designee. First appointed 6-7-2022.

TERM EXPIRES: 4-30-2026

CITY-COUNTY HOMELESS ISSUES COMMITTEE

KATHERINE S. KAMP (10th A.D.) - reappoint to a three-year term to the position of Homeless Services Consortium or Person Experienced in Homeless Services or Advocacy. First appointed 10-16-2018.

TERM EXPIRES: 6-30-2027

PUBLIC SAFETY REVIEW COMMITTEE

JOHNNIE C. MILTON, JR. (9th A.D.) - reappoint to a three-year term to the position of Resident Member. First appointed 8-3-2021.

TERM EXPIRES: 4-30-2027

Respectfully submitted,

Satya Rhodes-Conway Mayor



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83971

File ID:83971File Type:ReportStatus:Presidential

Business

Version: 1 Reference: Controlling Body: Council Office

File Created Date: 06/17/2024

File Name: Confirming the Madison Common Council meeting Final Action:

formats through December 10, 2024

Title: Confirming the Madison Common Council meeting formats through December 10,

2024:

8/6/24 - Hybrid (Virtual & CCB 201)

8/20/24 - Virtual

9/10/24 - Hybrid (Virtual & CCB 201) 9/24/24 - Hybrid (Virtual & CCB 201) 10/8/24 - Hybrid (Virtual & CCB 201) 10/29/24 - Hybrid (Virtual & CCB 201)

11/12/24 (Budget) - Hybrid (Virtual & CCB 201)

11/13/24 (Budget, if needed) - Hybrid (Virtual & CCB 201)

11/14/24 (Budget, if needed) - Hybrid (Virtual & CCB 201)

11/26/24 - Virtual

12/10/24 - Hybrid (Virtual & CCB 201)

Notes:

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: lwindsor-engnell@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Council Office	06/17/2024	RECOMMEND TO COUNCIL TO ACCEPT - REPORT OF OFFICER				
	Action Text:	This Report was RECOM	MEND TO COUNCIL	TO ACCEPT - REP	ORT OF OFFICER		

Text of Legislative File 83971

Title

Confirming the Madison Common Council meeting formats through December 10, 2024: 8/6/24 - Hybrid (Virtual & CCB 201)

- 8/20/24 Virtual
 9/10/24 Hybrid (Virtual & CCB 201)
 9/24/24 Hybrid (Virtual & CCB 201)
 10/8/24 Hybrid (Virtual & CCB 201)
 10/29/24 Hybrid (Virtual & CCB 201)
 11/12/24 (Budget) Hybrid (Virtual & CCB 201)
 11/13/24 (Budget, if needed) Hybrid (Virtual & CCB 201)
 11/14/24 (Budget, if needed) Hybrid (Virtual & CCB 201)
- 11/26/24 Virtual
- 12/10/24 Hybrid (Virtual & CCB 201)



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83905

File ID: 83905 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: BOARD OF

HEALTH FOR MADISON AND DANE COUNTY

File Created Date: 06/11/2024

Final Action:

File Name: Authorizing a non-competitive service contract with

UW-Madison's Population Health Institute

(contracted under Board of Regents of the University of Wisconsin System on behalf of the University of Wisconsin-Madison) for continued data services to

support

Title: Authorizing a non-competitive service contract with UW-Madison's Population

Health Institute (contracted under Board of Regents of the University of Wisconsin System on behalf of the University of Wisconsin-Madison) for continued data services to support updates to the Dane County Respiratory

Pathogens dashboard

Notes:

Sponsors: Yannette Figueroa Cole Effective Date:

Attachments: Non-Competitive Selection Request Enactment Number:

Author: Hearing Date:

Entered by: KMayoh@publichealthmdc.com Published Date:

History of Legislative File

Ver-	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return	Result:
sion:						Date:	

1 Health Department 06/11/20

06/11/2024 Referred for Introduction

This Resolution was Referred for Introduction

btes: Board of Health for Madison and Dane County (7/15/24), Finance Committee (6/24/24), Common Council (7/16/24)

COMMON COUNCIL 06/18/2024 Referred BOARD OF

HEALTH FOR MADISON AND DANE COUNTY

COMMITTEE

Action Text: This Resolution was Referred to the BOARD OF HEALTH FOR MADISON AND DANE COUNTY

Notes: Additional referral to Finance Committee

1 BOARD OF HEALTH 06/18/2024 Referred FINANCE 06/24/2024

FOR MADISON AND

DANE COUNTY

Action Text: This Resolution was Referred to the FINANCE COMMITTEE

FINANCE COMMITTEE 06/24/2024

06/24/2024 Return to Lead with BOARD OF

HEALTH FOR

Pass

Recommendation for Approval

the

MADISON AND DANE COUNTY

Action Text:

A motion was made by Figueroa Cole, seconded by Rummel, to Return to Lead with the

Recommendation for Approval to the BOARD OF HEALTH FOR MADISON AND DANE COUNTY. The

motion passed by voice vote/other.

Text of Legislative File 83905

Fiscal Note

The proposed resolution authorizes a non-competitive service contract with UW-Madison's Population Health Institute for continued data services to support updates to the Dane County Respiratory Pathogens dashboard. The cost of services is estimated to be a maximum cost of \$64,013 between September 2024 and February 2025 and will be funded by the COVID Immunization grant issued by the Wisconsin Department of Health Services. No appropriation is required.

Title

Authorizing a non-competitive service contract with UW-Madison's Population Health Institute (contracted under Board of Regents of the University of Wisconsin System on behalf of the University of Wisconsin-Madison) for continued data services to support updates to the Dane County Respiratory Pathogens dashboard Body

WHEREAS, Public Health Madison & Dane County (Public Health) has worked with UW-Madison's Population Health Institute (contracted under Board of Regents of the University of Wisconsin System on behalf of the University of Wisconsin-Madison) to host a COVID Response Corps Fellow since March 2021 and has the opportunity to continue funding the Fellow with grant funding from September 2024 through February of 2025; and

WHEREAS, the Fellow has supported the maintenance, data quality, and regular updating of the Dane County COVID Data Dashboard since 2021 and the Fellow has been funded through state Epidemiology & Laboratory Capacity grant funds through June 30, 2023. At that time, the Fellow transitioned to a Public Health Outreach Specialist role, funded by a UW grant focused on respiratory pathogen monitoring; and

WHEREAS, in 2023, Public Health transitioned its COVID dashboard to a more comprehensive respiratory pathogen monitoring dashboard and the Outreach Specialist was uniquely poised to assist with this transition given their experience and expertise with Public Health's dashboard infrastructure. The Outreach Specialist was part of the team selected for the 2023 cohort of the Council of State and Territorial Epidemiologists' Data Science Team Training program, where the team project focused on developing a respiratory surveillance data system and dashboard; and

WHEREAS, Public Health was awarded an addition and extension of COVID Immunization grant funding issued by the Wisconsin Department of Health Services, to provide an expanded vaccination outreach model, which includes a deeper data dive in order to assess, plan and implement robust vaccination strategies for improved systems and health outcomes; and

WHEREAS, the Outreach Specialist's partnership with health systems and knowledge of the Wisconsin Immunization Registry system will provide rapid access to data on which to make decisions regarding where to invest resources in vaccinator outreach; and

WHEREAS, the cost of services is estimated to be a maximum cost of \$64,013 between September 2024 and February 2025, to be covered by the COVID Immunization grant issued by the Wisconsin Department of Health Services; and

NOW, THEREFORE, BE IT RESOLVED, that Board of Health for Madison and Dane County on behalf of Public Health Madison and Dane County is authorized to execute a non-competitive service contract with the Board of Regents of the University of Wisconsin System on behalf of the University of Wisconsin-Madison for the purpose and at the price described above; and

BE IT FINALLY RESOLVED, that the Public Health Director, or their designee, is authorized to sign the agreement.



CITY OF MADISON • FINANCE DEPARTMENT • PURCHASING SERVICES

Non-Competitive Selection Request

Date:	05/07/2024					
Requisition Number:		(8 characters)				
Requestor Name:	Katarina Grande					
Requestor Phone Number:						
Requestor Email:	kgrande@publich	kgrande@publichealthmdc.com				
Fund:	6100 PUBLIC HEA	LTH MADISON DANE				
Agency:	32 PUBLIC HEALTH MADISON DANE					
Major:	543** Softwa	es ng/Facility Maintenance/Repair are/Equipment Maintenance/Repair				
	_	Works Maintenance/Repair				
		ng/HR-Related Services				
		Iting/Professional Services				
	548° Grants	:/Loans/Insurance/Other Services				
Total Purchase Amount:	\$64,013.00					
Vendor Name:	Board of Regents	of the University of Wisconsin System on behalf of the U				
Product/Service Description:	Data support for I	PHMDC Respiratory Illness Data Dashboard: PHMDC has				
	\$50,000 and UND This form will be s	ER ent to the Purchasing Supervisor for review.				
	provided by the Ci	n and draft a resolution using the sample resolutions ty Attorney to your Budget Analyst. Your resolution will the Finance Committee agenda without this form.				
Check the box(es) for the except	ion criteria you fee	l are applicable:				
 Public exigency (emer processes. 	gency) will not per	mit the delay incident to advertising or other competitive				
2. The services or goods	required are availa	ble from only one person or firm (i.e., true sole source).				
3. The services are for p	rofessional services	to be provided by attorneys.				
4. The services are to be	rendered by a univ	versity, college, or other educational institution.				
5. No acceptable bids ha	ave been received a	fter formal advertising.				
6. Service fees are estab	lished by law or pro	ofessional code.				

~	7.	A particular consultant has provided services to the City on a similar or continuing project in the recent past, and it would be economical to the City on the basis of time and money to retain the same consultant.
	8.	Otherwise authorized by law, rule, resolution, or regulation. Explain:
		ocurement is being paid with Federal or State grant funds, the vendor was identified by name in the roved Grant Application. (OPTIONAL)
REAS	SON	FOR REQUEST
Provid servid uniqu detail	de de e can e per the u	MPETITIVE SELECTION PROCESS CANNOT BE USED: tailed explanation below. For a true sole source, provide all information to explain why this product or only be purchased from this vendor. For one-of-a-kind items not sold through distributors, explain the formance features of the product requested that are not available from any other product. For services, inique qualifications this vendor possesses, or other reason(s) that meet the criteria selected above. ecific, measurable factors and qualifications.
The P	ublic	Health Outreach Specialist (Manjari Ohja) has provided services to PHMDC on a similar or continuing
same the D & Lab Outre in Au COVII uniqu infras Epide respii to Fel	cons ane Coorato each S gust/S O das lely p tructor miolo ratory oruar	the recent past, and it would be economical to PHMDC on the basis of time and money to retain the cultant. Specifically, the Fellow has supported the maintenance, data quality, and regular updating of county COVID Data Dashboard since 2021 and the Fellow has been funded through state Epidemiology bry Capacity grant funds through June 30, 2023. At that time, Manjari transitioned to a Public Health specialist role, funded by a UW grant focused on respiratory pathogen monitoring. This funding runs out be petermber, 2024, leaving a gap in our timeline for updating our dashboard. PHMDC transitioned its about to a more comprehensive respiratory pathogen monitoring dashboard and Manjari was poised to assist with this transition given her experience and expertise with PHMDC's dashboard are. Manjari was part of the team selected for the 2023 cohort of the Council of State and Territorial poists' Data Science Team Training program, where the team project focused on developing a surveillance data system and dashboard. The specific scope of work for this service (September 2024 y 2025) would be to support the update of the respiratory dashboard based on lessons learned from its of the 2023/2024 respiratory illness season.
CON	IMEI	NTS REGARDING PURCHASES OVER \$50,000
the o	ver 2, pt fro	is paid the University of Wisconsin over \$8,000,000 since 2015. There isn't a good way to filter 200 invoices down to subtotal by selection type; however, services rendered by a university are m competitive selection requirements, and additionally this particular contract is the result of a nich the UW Population Health Institute is specifically named.
Date:	05/	22/2024
		Submit



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83632

File ID: 83632 File Type: Ordinance Status: Report of Officer

Version: 2 Controlling Body: BOARD OF Reference:

PUBLIC WORKS

File Created Date: 05/24/2024

File Name: Large Item Collection & Electronic Waste Recycling Final Action:

Ordinance

Title: SUBSTITUTE: Amending Section 10.18 and 1.08(3)(a), and creating Section 27.05(2)(dd) of the Madison General Ordinances to update the City's large item collection and electronic waste recycling ordinances, and to update the bond

schedule accordingly.

Notes: 6872LargeItemElectronicsPickup

Sponsors: Satya V. Rhodes-Conway, Yannette Figueroa Cole, **Effective Date:**

Derek Field And Sabrina V. Madison

Attachments: 83632 Body-Version 2, 83634 - Large Item & **Enactment Number:**

Electronics Resolution,

070124_CC_public_comment.pdf,

070824FinanceCmte_WrittenComment2.pdf, 070824FinanceCmte WrittenComment1.pdf, 83632 Body FC Amendment.pdf, 83632-Version 1, 83632

Body-Version 1

Author: Doran Viste **Hearing Date:**

Entered by: mglaeser@cityofmadison.com **Published Date:**

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Attorney's Office	05/24/2024	Referred for Introduction				
	Action Text:	This Ordinance was Refe	erred for Introduction				
	Notes:	,	/24), Building Code, Fire Code ee (6/10/24), Common Counc		icensing Appeals	Board	
1	COMMON COUN	CIL 06/04/2024	Refer	BOARD OF PUBLIC WORKS	0	6/26/2024	Pass
	Action Text:	A motion was made by F WORKS. The motion pas	igueroa Cole, seconded based by voice vote/other.	by Duncan, to Refer to the	BOARD OF PU	JBLIC	
	Notes:	Additional referrals to Buildin Committee	ng Code, Fire Code, Conveya	ance Code and Licensing App	oeals Board, Fina	nce	
1	BOARD OF PUBI WORKS	IC 06/05/2024	Refer	FINANCE COMMITTEE	0	6/10/2024	
	Action Text:	This Ordinance was Refe	er to the FINANCE COMI	MITTEE			

1 BOARD OF PUBLIC 06/05/2024 Refer BUILDING CODE, 06/18/2024

WORKS

FIRE CODE, CONVEYANCE CODE AND LICENSING APPEALS BOARD

Action Text: This Ordinance was Refer to the BUILDING CODE, FIRE CODE, CONVEYANCE CODE AND

LICENSING APPEALS BOARD

1 FINANCE COMMITTEE 06/10/2024 Return to Lead with

Pass

Pass

the Following

Recommendation(s)

Action Text: A motion was made by Verveer, seconded by Rummel, to Return to Lead with the Following

Recommendation(s) to the BOARD OF PUBLIC WORKS: re-refer to Finance Committee on 7/8/24

and Common Council on 7/16/24. The motion passed by the following vote:

Notes: Re-refer to Finance Committee on 7/8/24 and Common Council on 7/16/24.

Ayes: 4 Tag Evers; Marsha A. Rummel; Michael E. Verveerand Satya V.

Rhodes-Conway

Noes: 3 Yannette Figueroa Cole; Sabrina V. Madisonand Jael Currie

1 BUILDING CODE, FIRE 06/18/2024 Return to Lead with BOARD OF 06/26/2024

CODE, CONVEYANCE the PUBLIC WORKS

CODE AND LICENSING Recommendation for

APPEALS BOARD Approval

Action Text: A motion was made by George C. Hank III to recommend approval for adoption to the Common

Council; seconded by Darrel D. Feucht. The motion passed on a unanimous voice vote with Louis J.

Olson abstaining.

1 BOARD OF PUBLIC 06/26/2024 RECOMMEND TO Pass

WORKS COUNCIL WITH

THE FOLLOWING RECOMMENDATIONS - REPORT OF

OFFICER

Action Text: A motion was made by Ald Guequierre, seconded by Kliems, to RECOMMEND TO COUNCIL WITH

THE FOLLOWING RECOMMENDATIONS: Re-refer to Finance Committee (7/8/24) and Board of Public Works (7/10/24) - REPORT OF OFFICER. The motion passed by voice vote/other.

Notes: A motion was made by Ald Guequierre, seconded by Kliems, to RECOMMEND TO COUNCIL WITH THE

FOLLOWING RECOMMENDATIONS: Re-refer to Finance Committee (7/8/24) and Board of Public Works (7/10/24)

- REPORT OF OFFICER. The motion passed by voice vote/other.

1 COMMON COUNCIL 07/02/2024 Re-refer FINANCE 07/08/2024 Pass

COMMITTEE

Action Text: A motion was made by Figueroa Cole, seconded by Duncan, to Re-refer to the FINANCE

COMMITTEE. The motion passed by voice vote/other.

Notes: Additional re-referral to Board of Public Works.

1 FINANCE COMMITTEE 07/02/2024 Refer BOARD OF 07/10/2024

PUBLIC WORKS

Action Text: This Ordinance was Refer to the BOARD OF PUBLIC WORKS

1 FINANCE COMMITTEE 07/08/2024 Amend

Action Text: A motion was made by Verveer, seconded by Madison, to Amend.

- Change the effective date in update #9 to October 1, 2024 (see Leg. File 83632 Body_FC

Amendment in Attachments).

The motion passed by voice vote/other.

Notes: A motion was made by Verveer, seconded by Madison, to Amend to change the effective date in update #9 to

October 1, 2024 (see Leg. File 83632 Body FC Amendment in Attachments). The motion passed by voice

vote/other.

The amended motion to return to lead with recommendation for approval to the Board of Public Works passed by

voice vote/other.

1 FINANCE COMMITTEE 07/08/2024 Return to Lead with BOARD OF 07/10/2024 Pass

the PUBLIC WORKS

Recommendation for Approval

Action Text: A motion was made by Figueroa Cole, seconded by Currie, to Return to Lead with the

Recommendation for Approval as amended to the BOARD OF PUBLIC WORKS. The motion passed

by voice vote/other.

1 BOARD OF PUBLIC 07/10/2024 RECOMMEND TO Pass

WORKS COUNCIL TO ADOPT - REPORT

OF OFFICER

Action Text: A motion was made by Williams, seconded by Guequierre, to RECOMMEND TO COUNCIL TO

ADOPT - REPORT OF OFFICER with Version 1. Version 2 was not available at time of meeting. The

motion passed by voice vote/other.

Text of Legislative File 83632

Fiscal Note

The proposed resolution amends the City's large item collection and electronic waste recycling ordinances with a proposed effective date of November 1, 2024. The proposed changes clarify the City's utilization of a work order system, unauthorized disposal of large items on terraces, and electronic waste recycling policy. Under the proposed change, the City will no longer provide large item pickup for residential buildings with 8 or more units. This proposed change will reallocate one-quarter of a Streets position (\$17,500) and provide an estimated savings in tipping fees (\$2,500). Additionally, the proposed ordinance changes includes establishing penalties for violating the ordinance. At this time, the fiscal impact from these penalties is not possible to calculate.

Title

SUBSTITUTE: Amending Section 10.18 and 1.08(3)(a), and creating Section 27.05(2)(dd) of the Madison General Ordinances to update the City's large item collection and electronic waste recycling ordinances, and to update the bond schedule accordingly.

Body

DRAFTER'S ANALYSIS: This ordinance makes several changes to update the City's ordinances regarding large item collection and the City's electronic waste recycling program. Historically, the City provided broad large item collection services across the City and utilized a sticker system for the collection, disposal and recycling of certain large items and electronic waste. However, the City no longer provides this broad service and now utilizes a work order system to make this program more efficient. In addition, while properties not receiving City refuse collection services are supposed to provide their own large item collection, some properties have not been doing this, leading to the unauthorized disposal of large items on the terraces, creating hazardous and blighting conditions that can be a negative influence on a neighborhood. A companion resolution approving the City's Large Item Collection and Electronic Waste Recycling Policy, File No. 83634, will be considered contemporaneously with this ordinance.

This ordinance repeals existing language in Sec. 10.18(11) regarding appliance disposal, as that is now part of the large item collection provisions. The large item collection language in renumbered 10.18(11) is being updated to reflect current operations, and to expressly prohibit the placement of large items on the terrace contrary to the City's Large Item Collection Policy. The electronic waste recycling language in renumbered 10.18(12) is being updated to better reflect current state law and to make it clear that these items are not for curbside collection, but must be recycled consistent with the City's Electronic Waste Recycling Policy. Section 10.18(13)(e)5 is being created to establish penalties for violating these updated provisions so that additional enforcement options are available to ensure compliance with the Ordinances

and the Policies and hopefully encourage use of the work order system, while eliminating unauthorized large item disposal on the terraces. Additionally, Section 27.05(2)(dd) is being added to the property maintenance code to provide the Building Inspection Division with the ability to directly address the placement of large items on the terrace contrary to the Policy. Sec. 1.08(3)(a) is being amended to add a bond schedule for violations associated with these updated and new ordinances.

The substitute eliminates the delayed effective date.	
***************************************	*****

Please see Legistar File No. 83632 Body-Version 2 in Attachments.

Legistar File No. 83632 Body-Version 2

DRAFTER'S ANALYSIS: This ordinance makes several changes to update the City's ordinances regarding large item collection and the City's electronic waste recycling program. Historically, the City provided broad large item collection services across the City and utilized a sticker system for the collection, disposal and recycling of certain large items and electronic waste. However, the City no longer provides this broad service and now utilizes a work order system to make this program more efficient. In addition, while properties not receiving City refuse collection services are supposed to provide their own large item collection, some properties have not been doing this, leading to the unauthorized disposal of large items on the terraces, creating hazardous and blighting conditions that can be a negative influence on a neighborhood. A companion resolution approving the City's Large Item Collection and Electronic Waste Recycling Policy, File No. 83634, will be considered contemporaneously with this ordinance.

This ordinance repeals existing language in Sec. 10.18(11) regarding appliance disposal, as that is now part of the large item collection provisions. The large item collection language in renumbered 10.18(11) is being updated to reflect current operations, and to expressly prohibit the placement of large items on the terrace contrary to the City's Large Item Collection Policy. The electronic waste recycling language in renumbered 10.18(12) is being updated to better reflect current state law and to make it clear that these items are not for curbside collection, but must be recycled consistent with the City's Electronic Waste Recycling Policy. Section 10.18(13)(e)5 is being created to establish penalties for violating these updated provisions so that additional enforcement options are available to ensure compliance with the Ordinances and the Policies and hopefully encourage use of the work order system, while eliminating unauthorized large item disposal on the terraces. Additionally, Section 27.05(2)(dd) is being added to the property maintenance code to provide the Building Inspection Division with the ability to directly address the placement of large items on the terrace contrary to the Policy. Sec. 1.08(3)(a) is being amended to add a bond schedule for violations associated with these updated and new ordinances.

The substitute eliminates the delayed effective date.	The s	substitute	eliminates	s the delaye	ed effective	date.		
***************************************		****	+++++++	****			···	++++

The Common Council of the City of Madison do hereby ordain as follows:

- 1. Subsection (11) entitled "Appliance Fees" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances is repealed.
- 2. Subsection (12) entitled "Large Item Fees" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances is renumbered to Subsection (11) and amended as follows:

"(1211)Large Item Fees.

- (a) The Streets Division offers curbside large item collection pursuant to the City's

 Large Item Collection Policy. For the purposes of this subsection, large items are
 items that are too large to fit into City provided receptacles but are meant for
 disposal, either as waste or recycling. Large items include such things as
 furniture, mattresses, appliances, certain remodeling or construction debris, and
 tires. Large items do not include electronics, which are subject to Sub. (12).
- (ab) No large item shall be placed on any terrace or curbside for collection contrary to the City's Large Item Collection Policy by the City unless the large item bears a large item sticker issued by the Street Superintendent. Every property owner shall be responsible for maintaining all of their property, along with the terrace area adjacent thereto, in accordance with this subsection. Fees for such large item collection stickers shall be set by the Street Superintendent.
- (bc) The Street Superintendent shall cause all large items which have been placed at curbside or on the terrace of properties contrary to the City's Large Item Collection Policy without a large item sticker to be removed and disposed of properly. The Street Superintendent shall keep an accurate account of the expenses thereof and report the same to the Finance Director, who shall annually prepare a statement of the expenses so incurred in front of or on each lot or parcel of land and report the same to the City Clerk, and the amount therein charged to each lot or parcel of land shall be entered by said Clerk in the tax roll as a special charge against said lot or parcel of land, and the same shall be collected in all respects like other special charges upon real estate under Wis. Stat. § 66.0627."
- 3. Subsection (13) entitled "Electronics" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances is renumbered to Subsection (12) and amended as follows:

"(1312)Electronics.

(a) Wis. Stat. § 287.07(5) and § 287.17, and regulations and guidelines adopted by the Wisconsin Department of Natural Recourses, prohibit the disposal of certain electronic waste, including No-televisions, computer monitors, laptop or desktop computers, or CPUs may printers, monitors, computer accessories, video players, and cell phones. Such items must be recycled and cannot be placed or kept on any terrace or curbside for collection by the City. Every owner shall be responsible for maintaining all their property in accordance with this subsection. Televisions, computer monitors, laptop or desktop computers, or CPUs Such electronic waste must be disposed of at an authorized recycling deposit site or in any other lawful manner, consistent with State law. No person shall deposit such recyclables at any authorized recycling center or deposit site while the site is closed. No televisions, computer monitors, laptop or desktop computers, or

- <u>CPUs electronic waste</u> shall be deposited at an authorized public recycling site <u>contrary to the City's Electronic Recycling Policy.</u> unless the item bears a fee <u>sticker issued by the Street Superintendent.</u> Fees for such stickers <u>Appropriate recycling fees for these items</u> shall be set by the Street Superintendent.
- (b) The Street Superintendent shall cause all televisions, computer monitors, laptop or desktop computers, or CPUs which have been electronic waste placed at curbside or on the terrace contrary to this Subsection to be removed and disposed of properly. The Street Superintendent shall keep an accurate account of the expenses thereof and report the same to the Finance Director, who shall annually prepare a statement of the expenses so incurred in front of or on each lot or parcel of land and report the same to the City Clerk, and the amount therein charged to each lot or parcel of land shall be entered by said Clerk in the tax roll as a special charge against said lot or parcel of land, and the same shall be collected in all respects like other special charges upon real estate under Wis. Stat. § 66.0627."
- 4. Subsection (14) entitled "Penalty" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances is renumbered to Subsection (13).
- 5. Subdivision (e) of Subsection (13) entitled "Penalty" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances is created as follows:
- "(e) Any person who violates Subsections (11) or (12) shall be subject to a forfeiture of not less than fifty dollars (\$50) nor more than two hundred dollars (\$200) for the first offense, not less than two hundred dollars (\$200) nor more than five hundred dollars (\$500) for the second offense, and not less than five hundred dollars (\$500) nor more than one thousand dollars (\$1000) for the third and any subsequent offense."
- 6. Subsection (15) entitled "Severability" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances is repealed.
- 7. Subdivision (dd) entitled "Large Items on Terrace" of Subsection (2) of Section 27.05 entitled Safe and Sanitary Maintenance of Property" of the Madison General Ordinances is created as follows:
- "(dd) <u>Large Items on Terrace</u>. Large items, as that term is defined in Sec. 10.18(11)(a), may not be placed on the terrace, except temporarily, contrary to the City's Large Item Collection Policy. Property owners are responsible for the removal of such items."
- 8. Subdivision (a) of Subsection (3) entitled "Schedule of Deposits" of Section 1.08 entitled "Issuance of Citations for Violations of Certain Ordinances and Providing a Schedule of Cash Deposits" of the Madison General Ordinances is amended by creating and amending therein the following:

"Offense	Ord. No./Adopted	Deposit *
	Statute No.	

Page 4

Depositing large items without approval at curb/terrace	10.18(11)	\$100, 1 st \$300, 2 nd \$500, 3 rd \$1,000, 4 th +
Improper disposal of electronic waste	10.18(12)	\$50, 1 st \$200, 2 nd \$500, 3 rd +
Large items on terrace	27.05(2)(dd)	\$100, 1 st \$200, 2 nd "

EDITOR'S NOTE: New bail deposits must be approved by the Municipal Judge prior to adoption. This deposit has been so approved.

9. This ordinance will go into effect on November 1, 2024.

EDITOR'S NOTES:

Section 10.18(11) entitled "Appliance Fees" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances currently reads as follows:

"(11) Appliance Fees.

- (a) No appliance shall be placed on any terrace or curbside for collection by the City unless the appliance bears an appliance sticker issued by the Street Superintendent. Every owner shall be responsible for maintaining all her/his property in accordance with this subsection. Fees for such appliance stickers shall be set by the Street Superintendent.
- (b) The Street Superintendent shall cause all appliances which have been placed at curbside or on the terrace without an appliance sticker to be removed and disposed of properly. The Street Superintendent shall keep an accurate account of the expenses thereof and report the same to the Finance Director, who shall annually prepare a statement of the expenses so incurred in front of or on each lot or parcel of land and report the same to the City Clerk, and the amount therein charged to each lot or parcel of land shall be entered by said Clerk in the tax roll as a special charge against said lot or parcel of land, and the same shall be collected in all respects like other special charges upon real estate under Wis. Stat. § 66.0627."

Section 10.18(15) entitled "Severability" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances currently reads as follows:

"(15) <u>Severability</u>. Should any portion of this ordinance be declared unconstitutional or invalid by a court of competent jurisdiction, the remainder of this ordinance shall not be affected."

Page 5

DRAFTER'S ANALYSIS: This ordinance makes several changes to update the City's ordinances regarding large item collection and the City's electronic waste recycling program. Historically, the City provided broad large item collection services across the City and utilized a sticker system for the collection, disposal and recycling of certain large items and electronic waste. However, the City no longer provides this broad service and now utilizes a work order system to make this program more efficient. In addition, while properties not receiving City refuse collection services are supposed to provide their own large item collection, some properties have not been doing this, leading to the unauthorized disposal of large items on the terraces, creating hazardous and blighting conditions that can be a negative influence on a neighborhood. A companion resolution approving the City's Large Item Collection and Electronic Waste Recycling Policy, File No. 83634, will be considered contemporaneously with this ordinance.

This ordinance repeals existing language in Sec. 10.18(11) regarding appliance disposal, as that is now part of the large item collection provisions. The large item collection language in renumbered 10.18(11) is being updated to reflect current operations, and to expressly prohibit the placement of large items on the terrace contrary to the City's Large Item Collection Policy. The electronic waste recycling language in renumbered 10.18(12) is being updated to better reflect current state law and to make it clear that these items are not for curbside collection, but must be recycled consistent with the City's Electronic Waste Recycling Policy. Section 10.18(13)(e)5 is being created to establish penalties for violating these updated provisions so that additional enforcement options are available to ensure compliance with the Ordinances and the Policies and hopefully encourage use of the work order system, while eliminating unauthorized large item disposal on the terraces. Additionally, Section 27.05(2)(dd) is being added to the property maintenance code to provide the Building Inspection Division with the ability to directly address the placement of large items on the terrace contrary to the Policy. Sec. 1.08(3)(a) is being amended to add a bond schedule for violations associated with these updated and new ordinances. Finally, this ordinance will go into effect on November 1, 2024 to allow certain multi-family developments sufficient time to arrange for private large item disposal for their tenants.

The substitute eliminates the delayed effective date.

From: noreply
To: All Alders

Subject: [All Alders] 7/2/24 #79 on Agenda - Large item pickup

Date: Monday, July 1, 2024 1:52:07 PM

Recipient: All Alders:

Monday, July 1, 2024 – 1:50pm

Karen Louise Miskimen She her 9 SHERMAN TER UNIT 6

Madison, Wisconsin. 53704 Yes, by email. <u>K09l06m04@gmail.com</u> All Alders 7/2/24 #79 on Agenda - Large item pickup I'm writing in opposition to proposal #83632. I see no justification or reason to discontinue large item pickup service at Sherman Terrace or any other condo or apartment complex. Please register my opposition and know there are others who are doing the same. I'll be registering to speak at tomorrow's meeting, and sending this message to the mayor as well.

From: john zwickey

To: Finance Committee

Subject: Large Item Pick Up Policy Change **Date:** Sunday, July 7, 2024 5:31:41 PM

You don't often get email from rjzwickey@gmail.com. Learn why this is important

Caution: This email was sent from an external source. Avoid unknown links and attachments.

There were pictures posted on the Next Door app of the pile of large items and trash on the terrace by Meadows Apartments. The pile surrounded a no dumping sign that was erected recently to combat on going dumping. The pictures generated over 100 comments about the trash pile. According to the Building Inspectors office a complaint was filed May 31. The trash pile was not picked up until June 26 (give or take). The pile started again the very next day. I've personally seen 3+ people pull up and dump stuff out of their car. I strongly urge to to pass these changes as quickly as possible.

John Zwickey

rom: Lauron Chang
fo: Finance Committee: Board of Public Worl
kt: Field, Denok
ludject: Changes to Large Item Pick Policy
ladde: Sunday, July 7, 2024 3:51:27 PM

[Some people who received this message don't often get email from speckson@charter.net. Learn why this is important at https://aka.ms/LearnAboutSenderIdentification

Caution: This email was sent from an external source. Avoid unknown links and attachment

Dear Board and Committee Members

As a member of our community who runs or drives by the monthly heaps o' garbage, redolent on the soggy summer mornings, I recommend you adopt changes to the policy as proposed by my alder, Derek Fields

The lining of the street for weeks at a time with the sad leftovers from people likely evicted or unable to move even their beds does little to enhance neighborhood pride or appeal - especially for the apartment home it all clearly tumbled out of

While I have no desire to treat multi-family housing owners/operators unfairly, the volume of detritus and the frequency (yup, every month) as well as the cost to our strapped city demands that they adopt - and pay for - a strategy and service that responds to the routine house cleaning.

Many thanks to Alder Fields for responding to the need and best wishes in your decision-making to require new policy elements that put the costs and responsibilities on the large building owners









District 3 Resider Lauren Cnare

Legistar File No. 83632 Body

DRAFTER'S ANALYSIS: This ordinance makes several changes to update the City's ordinances regarding large item collection and the City's electronic waste recycling program. Historically, the City provided broad large item collection services across the City and utilized a sticker system for the collection, disposal and recycling of certain large items and electronic waste. However, the City no longer provides this broad service and now utilizes a work order system to make this program more efficient. In addition, while properties not receiving City refuse collection services are supposed to provide their own large item collection, some properties have not been doing this, leading to the unauthorized disposal of large items on the terraces, creating hazardous and blighting conditions that can be a negative influence on a neighborhood. A companion resolution approving the City's Large Item Collection and Electronic Waste Recycling Policy, File No. 83634, will be considered contemporaneously with this ordinance.

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Page 2

The Common Council of the City of Madison do hereby ordain as follows:

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- 3. Subsection (13) entitled "Electronics" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances is renumbered to Subsection (12) and amended as follows:

"(1312) Electronics.

- (a) Wis. Stat. § 287.07(5) and § 287.17, and regulations and guidelines adopted by the Wisconsin Department of Natural Recourses, prohibit the disposal of certain electronic waste, including No-televisions, computer monitors, laptop or desktop computers, or CPUs may printers, monitors, computer accessories, video players, and cell phones. Such items must be recycled and cannot be placed or kept on any terrace or curbside for collection by the City. Every owner shall be responsible for maintaining all their property in accordance with this subsection. Televisions, computer monitors, laptop or desktop computers, or CPUs—Such electronic waste must be disposed of at an authorized recycling deposit site or in any other lawful manner, consistent with State law. No person shall deposit such recyclables at any authorized recycling center or deposit site while the site is closed. No televisions, computer monitors, laptop or desktop computers, or CPUs electronic waste shall be deposited at an authorized public recycling site contrary to the City's Electronic Recycling Policy, unless the item bears a fee sticker issued by the Street Superintendent. Fees for such stickers Appropriate recycling fees for these items shall be set by the Street Superintendent.
- (b) The Street Superintendent shall cause all televisions, computer monitors, laptop or desktop computers, or CPUs which have been electronic waste placed at curbside or on the terrace contrary to this Subsection to be removed and disposed of properly. The Street Superintendent shall keep an accurate account of the expenses thereof and report

the same to the Finance Director, who shall annually prepare a statement of the expenses so incurred in front of or on each lot or parcel of land and report the same to the City Clerk, and the amount therein charged to each lot or parcel of land shall be entered by said Clerk in the tax roll as a special charge against said lot or parcel of land, and the same shall be collected in all respects like other special charges upon real estate under Wis. Stat. § 66.0627."

- 4. Subsection (14) entitled "Penalty" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances is renumbered to Subsection (13).
- 5. Subdivision (e) of Subsection (13) entitled "Penalty" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances is created as follows:
- "(e) Any person who violates Subsections (11) or (12) shall be subject to a forfeiture of not less than fifty dollars (\$50) nor more than two hundred dollars (\$200) for the first offense, not less than two hundred dollars (\$200) nor more than five hundred dollars (\$500) for the second offense, and not less than five hundred dollars (\$500) nor more than one thousand dollars (\$1000) for the third and any subsequent offense."
- 6. Subsection (15) entitled "Severability" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances is repealed.
- 7. Subdivision (dd) entitled "Large Items on Terrace" of Subsection (2) of Section 27.05 entitled Safe and Sanitary Maintenance of Property" of the Madison General Ordinances is created as follows:
- "(dd) <u>Large Items on Terrace</u>. Large items, as that term is defined in Sec. 10.18(11)(a), may not be placed on the terrace, except temporarily, contrary to the City's Large Item Collection Policy. Property owners are responsible for the removal of such items."
- 8. Subdivision (a) of Subsection (3) entitled "Schedule of Deposits" of Section 1.08 entitled "Issuance of Citations for Violations of Certain Ordinances and Providing a Schedule of Cash Deposits" of the Madison General Ordinances is amended by creating and amending therein the following:

" <u>Offense</u>	Ord. No./Adopted Statute No.	Deposit *
Depositing large items without approval at curb/terrace	10.18(11)	\$100, 1 st \$300, 2 nd \$500, 3 rd \$1,000, 4 th +
Improper disposal of electronic waste	10.18(12)	\$50, 1 st \$200, 2 nd \$500, 3 rd +
Large items on terrace	27.05(2)(dd)	\$100, 1 st \$200, 2 nd "

EDITOR'S NOTE: New bail deposits must be approved by the Municipal Judge prior to adoption. This deposit has been so approved.

9. This ordinance will go into effect on November October 1, 2024.

EDITOR'S NOTES:

Section 10.18(11) entitled "Appliance Fees" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances currently reads as follows:

"(11) Appliance Fees.

- (a) No appliance shall be placed on any terrace or curbside for collection by the City unless the appliance bears an appliance sticker issued by the Street Superintendent. Every owner shall be responsible for maintaining all her/his property in accordance with this subsection. Fees for such appliance stickers shall be set by the Street Superintendent.
- (b) The Street Superintendent shall cause all appliances which have been placed at curbside or on the terrace without an appliance sticker to be removed and disposed of properly. The Street Superintendent shall keep an accurate account of the expenses thereof and report the same to the Finance Director, who shall annually prepare a statement of the expenses so incurred in front of or on each lot or parcel of land and report the same to the City Clerk, and the amount therein charged to each lot or parcel of land shall be entered by said Clerk in the tax roll as a special charge against said lot or parcel of land, and the same shall be collected in all respects like other special charges upon real estate under Wis. Stat. § 66.0627."

Section 10.18(15) entitled "Severability" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances currently reads as follows:

"(15) <u>Severability</u>. Should any portion of this ordinance be declared unconstitutional or invalid by a court of competent jurisdiction, the remainder of this ordinance shall not be affected."



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83632

File ID:83632File Type:OrdinanceStatus:Council New

Business

Version: 1 Reference: Controlling Body: Attorney's Office

File Created Date: 05/24/2024

File Name: Large Item Collection & Electronic Waste Recycling Final Action:

Ordinance

Title: Amending Section 10.18 and 1.08(3)(a), and creating Section 27.05(2)(dd) of the

Madison General Ordinances to update the City's large item collection and electronic waste recycling ordinances, and to update the bond schedule

accordingly.

Notes: 6872LargeItemElectronicsPickup

Sponsors: Satya V. Rhodes-Conway, Derek Field And Sabrina Effective Date:

V. Madison

Attachments: Enactment Number:

Author: Doran Viste Hearing Date:

Entered by: mglaeser@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Attorney's Office	05/24/2024	Referred for Introduction				
	Action Text:	This Ordinance was Ref	erred for Introd	uction			
	Notes:	Board of Public Works (6/26 (6/18/24); Common Council	,	de, Fire Code, Conveyance Code,	, and Licensing Appeals	s Board	

Text of Legislative File 83632

Fiscal Note

[Enter Fiscal Note Here]

Title

Amending Section 10.18 and 1.08(3)(a), and creating Section 27.05(2)(dd) of the Madison General Ordinances to update the City's large item collection and electronic waste recycling ordinances, and to update the bond schedule accordingly.

Body

DRAFTER'S ANALYSIS: This ordinance makes several changes to update the City's ordinances regarding large item collection and the City's electronic waste recycling program. Historically, the City provided broad large item collection services across the City and utilized a sticker system for the collection, disposal and recycling of certain large items and electronic waste. However, the City no longer provides this broad service and now utilizes a work order

system to make this program more efficient. In addition, while properties not receiving City refuse collection services are supposed to provide their own large item collection, some properties have not been doing this, leading to the unauthorized disposal of large items on the terraces, creating hazardous and blighting conditions that can be a negative influence on a neighborhood. A companion resolution approving the City's Large Item Collection and Electronic Waste Recycling Policy, File No. 83634, will be considered contemporaneously with this ordinance.

This ordinance repeals existing language in Sec. 10.18(11) regarding appliance disposal, as that is now part of the large item collection provisions. The large item collection language in renumbered 10.18(11) is being updated to reflect current operations, and to expressly prohibit the placement of large items on the terrace contrary to the City's Large Item Collection Policy. The electronic waste recycling language in renumbered 10.18(12) is being updated to better reflect current state law and to make it clear that these items are not for curbside collection, but must be recycled consistent with the City's Electronic Waste Recycling Policy. Section 10.18(13)(e)5 is being created to establish penalties for violating these updated provisions so that additional enforcement options are available to ensure compliance with the Ordinances and the Policies and hopefully encourage use of the work order system, while eliminating unauthorized large item disposal on the terraces. Additionally, Section 27.05(2)(dd) is being added to the property maintenance code to provide the Building Inspection Division with the ability to directly address the placement of large items on the terrace contrary to the Policy. Sec. 1.08(3)(a) is being amended to add a bond schedule for violations associated with these updated and new ordinances. Finally, this ordinance will go into effect on November 1, 2024 to allow certain multi-family developments sufficient time to arrange for private large item disposal for their tenants.

Please see Legistar File No. 83632 Body in Attachments.

Legistar File No. 83632 Body

DRAFTER'S ANALYSIS: This ordinance makes several changes to update the City's ordinances regarding large item collection and the City's electronic waste recycling program. Historically, the City provided broad large item collection services across the City and utilized a sticker system for the collection, disposal and recycling of certain large items and electronic waste. However, the City no longer provides this broad service and now utilizes a work order system to make this program more efficient. In addition, while properties not receiving City refuse collection services are supposed to provide their own large item collection, some properties have not been doing this, leading to the unauthorized disposal of large items on the terraces, creating hazardous and blighting conditions that can be a negative influence on a neighborhood. A companion resolution approving the City's Large Item Collection and Electronic Waste Recycling Policy, File No. 83634, will be considered contemporaneously with this ordinance.

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Page 2

The Common Council of the City of Madison do hereby ordain as follows:

- 1. Subsection (11) entitled "Appliance Fees" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances is repealed.
- 2. Subsection (12) entitled "Large Item Fees" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances is renumbered to Subsection (11) and amended as follows:

"(1211) Large Item Fees.

- (a) The Streets Division offers curbside large item collection pursuant to the City's Large

 ltem Collection Policy. For the purposes of this subsection, large items are items that are
 too large to fit into City provided receptacles but are meant for disposal, either as waste
 or recycling. Large items include such things as furniture, mattresses, appliances, certain
 remodeling or construction debris, and tires. Large items do not include electronics,
 which are subject to Sub. (12).
- (ab) No large item shall be placed on any terrace or curbside for collection contrary to the City's Large Item Collection Policy by the City unless the large item bears a large item sticker issued by the Street Superintendent. Every property owner shall be responsible for maintaining all of their property, along with the terrace area adjacent thereto, in accordance with this subsection. Fees for such large item collection stickers shall be set by the Street Superintendent.
- (bc) The Street Superintendent shall cause all large items which have been placed at curbside or on the terrace of properties contrary to the City's Large Item Collection Policy without a large item sticker to be removed and disposed of properly. The Street Superintendent shall keep an accurate account of the expenses thereof and report the same to the Finance Director, who shall annually prepare a statement of the expenses so incurred in front of or on each lot or parcel of land and report the same to the City Clerk, and the amount therein charged to each lot or parcel of land shall be entered by said Clerk in the tax roll as a special charge against said lot or parcel of land, and the same shall be collected in all respects like other special charges upon real estate under Wis. Stat. § 66.0627."
- 3. Subsection (13) entitled "Electronics" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances is renumbered to Subsection (12) and amended as follows:

"(1312) Electronics.

- (a) Wis. Stat. § 287.07(5) and § 287.17, and regulations and guidelines adopted by the Wisconsin Department of Natural Recourses, prohibit the disposal of certain electronic waste, including No televisions, computer monitors, laptop or desktop computers, or CPUs may printers, monitors, computer accessories, video players, and cell phones. Such items must be recycled and cannot be placed or kept on any terrace or curbside for collection by the City. Every owner shall be responsible for maintaining all their property in accordance with this subsection. Televisions, computer monitors, laptop or desktop computers, or CPUs Such electronic waste must be disposed of at an authorized recycling deposit site or in any other lawful manner, consistent with State law. No person shall deposit such recyclables at any authorized recycling center or deposit site while the site is closed. No televisions, computer monitors, laptop or desktop computers, or CPUs electronic waste shall be deposited at an authorized public recycling site contrary to the City's Electronic Recycling Policy, unless the item bears a fee sticker issued by the Street Superintendent. Fees for such stickers Appropriate recycling fees for these items shall be set by the Street Superintendent.
- (b) The Street Superintendent shall cause all televisions, computer monitors, laptop or desktop computers, or CPUs which have been electronic waste placed at curbside or on the terrace contrary to this Subsection to be removed and disposed of properly. The Street Superintendent shall keep an accurate account of the expenses thereof and report

the same to the Finance Director, who shall annually prepare a statement of the expenses so incurred in front of or on each lot or parcel of land and report the same to the City Clerk, and the amount therein charged to each lot or parcel of land shall be entered by said Clerk in the tax roll as a special charge against said lot or parcel of land, and the same shall be collected in all respects like other special charges upon real estate under Wis. Stat. § 66.0627."

- 4. Subsection (14) entitled "Penalty" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances is renumbered to Subsection (13).
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- "(e) Any person who violates Subsections (11) or (12) shall be subject to a forfeiture of not less than fifty dollars (\$50) nor more than two hundred dollars (\$200) for the first offense, not less than two hundred dollars (\$200) nor more than five hundred dollars (\$500) for the second offense, and not less than five hundred dollars (\$500) nor more than one thousand dollars (\$1000) for the third and any subsequent offense."
- 6. Subsection (15) entitled "Severability" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances is repealed.
- 7. Subdivision (dd) entitled "Large Items on Terrace" of Subsection (2) of Section 27.05 entitled Safe and Sanitary Maintenance of Property" of the Madison General Ordinances is created as follows:
- "(dd) <u>Large Items on Terrace</u>. Large items, as that term is defined in Sec. 10.18(11)(a), may not be placed on the terrace, except temporarily, contrary to the City's Large Item Collection Policy. Property owners are responsible for the removal of such items."
- 8. Subdivision (a) of Subsection (3) entitled "Schedule of Deposits" of Section 1.08 entitled "Issuance of Citations for Violations of Certain Ordinances and Providing a Schedule of Cash Deposits" of the Madison General Ordinances is amended by creating and amending therein the following:

" <u>Offense</u>	Ord. No./Adopted Statute No.	Deposit *
Depositing large items without approval at curb/terrace	10.18(11)	\$100, 1 st \$300, 2 nd \$500, 3 rd \$1,000, 4 th +
Improper disposal of electronic waste	10.18(12)	\$50, 1 st \$200, 2 nd \$500, 3 rd +
Large items on terrace	27.05(2)(dd)	\$100, 1 st \$200, 2 nd "

EDITOR'S NOTE: New bail deposits must be approved by the Municipal Judge prior to adoption. This deposit has been so approved.

9. This ordinance will go into effect on November 1, 2024.

EDITOR'S NOTES:

Section 10.18(11) entitled "Appliance Fees" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances currently reads as follows:

"(11) Appliance Fees.

- (a) No appliance shall be placed on any terrace or curbside for collection by the City unless the appliance bears an appliance sticker issued by the Street Superintendent. Every owner shall be responsible for maintaining all her/his property in accordance with this subsection. Fees for such appliance stickers shall be set by the Street Superintendent.
- (b) The Street Superintendent shall cause all appliances which have been placed at curbside or on the terrace without an appliance sticker to be removed and disposed of properly. The Street Superintendent shall keep an accurate account of the expenses thereof and report the same to the Finance Director, who shall annually prepare a statement of the expenses so incurred in front of or on each lot or parcel of land and report the same to the City Clerk, and the amount therein charged to each lot or parcel of land shall be entered by said Clerk in the tax roll as a special charge against said lot or parcel of land, and the same shall be collected in all respects like other special charges upon real estate under Wis. Stat. § 66.0627."

Section 10.18(15) entitled "Severability" of Section 10.18 entitled "Collection of Refuse and Recycling of Waste" of the Madison General Ordinances currently reads as follows:

"(15) <u>Severability</u>. Should any portion of this ordinance be declared unconstitutional or invalid by a court of competent jurisdiction, the remainder of this ordinance shall not be affected."



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83634

File ID: 83634 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: BOARD OF

PUBLIC WORKS

File Created Date: 05/24/2024

File Name: Large Item Collection & Electronic Waste Recycling Final Action:

Resolution

Title: Approving the Large Item Collection and Electronic Waste Recycling Policy.

Notes: LargeItemElectronicsPickup

Sponsors: Satya V. Rhodes-Conway, Yannette Figueroa Cole, Effective Date:

Derek Field And Sabrina V. Madison

Attachments: Large Item and Electronic Waste Policy-Ver 2.0.pdf, Enactment Number:

Large Item and Electronic Waste Policy, 83632 -

Large Item & Electronics Ordinance,

070824FinanceCmte_WrittenComment2.pdf, 070824FinanceCmte_WrittenComment1.pdf

Author: Doran Viste Hearing Date:

Entered by: mglaeser@cityofmadison.com Published Date:

History of Legislative File

WORKS

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Attorney's Office	05/24/2024	Referred for Introduction				
	Action Text: Notes:	•	erred for Introduction 6/24), Building Code, Fire Code ee (6/10/24), Common Cound	•	icensing Appea	ls Board	
1	COMMON COUN	ICIL 06/04/2024	Refer	BOARD OF PUBLIC WORKS		06/26/2024	Pass
	Action Text:	•	igueroa Cole, seconded lessed by voice vote/other.	by Duncan, to Refer to the	BOARD OF	PUBLIC	
	Notes:		ng Code, Fire Code, Convey	ance Code, and Licensing Ap	opeals Board, Fi	nance	
1	BOARD OF PUB WORKS	LIC 06/05/2024	Refer	BUILDING CODE, FIRE CODE, CONVEYANCE CODE AND LICENSING APPEALS BOARD		06/18/2024	
Action Text: This Resolution was Refer to the BUILDING CODE, FIRE CODE, CONVEYANG LICENSING APPEALS BOARD					YANCE CODE	AND	
1	BOARD OF PUB	LIC 06/05/2024	Refer	FINANCE		06/10/2024	

COMMITTEE

Action Text: This Resolution was Refer to the FINANCE COMMITTEE

1 FINANCE COMMITTEE 06/10/2024 Return to Lead with BOARD OF 06/26/2024 Pass

the Following PUBLIC WORKS

Recommendation(s)

Action Text: A motion was made by Verveer, seconded by Rummel, to Return to Lead with the Following

Recommendation(s) to the BOARD OF PUBLIC WORKS: re-refer to Finance Committee on 7/8/24

and Common Council on 7/16/24. The motion passed by the following vote:

Notes: Re-refer to Finance Committee on 7/8/24 and Common Council on 7/16/24

Ayes: 4 Tag Evers; Marsha A. Rummel; Michael E. Verveerand Satya V.

Rhodes-Conway

Noes: 3 Sabrina V. Madison; Jael Currieand Yannette Figueroa Cole

1 BUILDING CODE, FIRE 06/18/2024 Return to Lead with BOARD OF 06/26/2024

CODE, CONVEYANCE the PUBLIC WORKS

CODE AND LICENSING Recommendation for

APPEALS BOARD Approval

Action Text: A motion was made by George C. Hank III to recommend approval for adoption to the Common

Council; seconded by Sean B. Size The motion passed on a unanimous voice vote.

Council, Seconded by Sean B. Size The motion passed on a unanimous voice vote

BOARD OF PUBLIC 06/26/202 WORKS

06/26/2024 RECOMMEND TO

Pass

ORKS COUNCIL WITH
THE FOLLOWING
RECOMMENDATIO

NS - REPORT OF OFFICER

Action Text: A motion was made by Ald. Guequierre, seconded by Clausius, to RECOMMEND TO COUNCIL WITH

THE FOLLOWING RECOMMENDATIONS: re-refer to Finance Committee (7/8/24) and Board of Public

Works (7/10/24)- REPORT OF OFFICER. The motion passed by voice vote/other.

Notes: A motion was made by Ald. Guequierre, seconded by Clausius, to RECOMMEND TO COUNCIL WITH THE

FOLLOWING RECOMMENDATIONS: re-refer to Finance Committee (7/8/24) and Board of Public Works (7/10/24)-

REPORT OF OFFICER. The motion passed by voice vote/other.

1 COMMON COUNCIL 07/02/2024 Re-refer FINANCE 07/08/2024 Pass

COMMITTEE

Action Text: A motion was made by Figueroa Cole, seconded by Duncan, to Re-refer to the FINANCE

COMMITTEE. The motion passed by voice vote/other.

Notes: Additional referral to Board of Public Works.

1 FINANCE COMMITTEE 07/02/2024 Refer BOARD OF 07/10/2024

PUBLIC WORKS

Action Text: This Resolution was Refer to the BOARD OF PUBLIC WORKS

1 FINANCE COMMITTEE 07/08/2024 Return to Lead with BOARD OF 07/10/2024 Pass

the PUBLIC WORKS

Recommendation for

Approval

Action Text: A motion was made by Figueroa Cole, seconded by Currie, to Return to Lead with the

Recommendation for Approval to the BOARD OF PUBLIC WORKS,. The motion passed by the

ollowing vote:

Notes: The motion to return to lead with the recommendation for approval to the Board of Public Works passed by the

following vote: 5-1. Aye: Figueroa Cole, Currie, Madison, Evers, Rummel. Nay: Verveer. Non-voting:

Rhodes-Conway. Motion to return to lead with the recommendation for approval passed.

Ayes: 5 Yannette Figueroa Cole; Tag Evers; Sabrina V. Madison; Marsha A.

Rummeland Jael Currie

Noes: 1 Michael E. Verveer

Non Voting: 1 Satya V. Rhodes-Conway

1 BOARD OF PUBLIC 07/10/2024 RECOMMEND TO Pass

WORKS COUNCIL TO

ADOPT - REPORT OF OFFICER

Action Text: A motion was made by Guequierre, seconded by Williams, to RECOMMEND TO COUNCIL TO

ADOPT - REPORT OF OFFICER. The motion passed by voice vote/other.

Text of Legislative File 83634

Fiscal Note

The proposed resolution amends the City's large item collection and electronic waste recycling ordinances with a proposed effective date of November 1, 2024. The proposed changes clarify the City's utilization of a work order system, unauthorized disposal of large items on terraces, and electronic waste recycling policy. Under the proposed change, the City will no longer provide large item pickup for residential buildings with 8 or more units. This proposed change will reallocate one-quarter of a Streets position (\$17,500) and provide an estimated savings in tipping fees (\$2,500). Additionally, the proposed ordinance changes includes establishing penalties for violating the ordinance. At this time, the fiscal impact from these penalties is not possible to calculate.

Title

Approving the Large Item Collection and Electronic Waste Recycling Policy.

Under RES-10-00362, Legistar File No. 17693 and the adopted Resource Recovery Special Charge Policy, RES-22-00399, Legistar File No. 71186, the Streets Division's collection of refuse and recycling is generally restricted to multi-family buildings of 8 units and fewer, although some small commercial properties and larger residential properties are provided refuse and recycling services as well. Historically, the City of Madison's Streets Division provided large item collection services through a combination of dedicated collection routes, a sticker program, and drop-off sites open to City residents. Due to inherent inefficiencies in that arrangement, and the updated service criteria, the Streets Division no longer operates dedicated large item collection routes and instead uses a work order system to manage its large item collection program. The Streets Division is also tasked with providing large item pick up during student moving days and in response to nuisance complaints. In order to continue to effectively provide large item collection services to its core customer base, and to ensure that non-customers are effectively managing their own waste and recycling needs, it is necessary to create and implement a large item collection policy.

In addition, State law prohibits the disposal of certain electronic waste, including televisions, computers, printers, monitors, computer accessories, video players, and cell phones. Such items must be recycled at authorized recycling sites and cannot be placed or kept on any terrace or curbside for collection by the City. The City does offer public recycling sites for this purpose, and has historically charged for this e-recycling service, when necessary, through sticker fees. In order to continue to effectively provide electronic waste recycling services, it is also necessary to create an implement an electronic waste recycling policy.

As a companion to this resolution, an ordinance update (File No. 83632) is also under consideration that would tie large item and electronic waste requirements and enforcement efforts to this policy.

WHEREAS, the Streets Division provides refuse and recycling services to City properties, consistent with enacted and adopted City ordinances, resolutions and policies. Among the services provided include large item collection and electronic waste recycling; and,

WHEREAS, over time, the City's large item collection and electronic waste recycling programs have adapted to better meet the City's needs; and,

WHEREAS, the Streets Division has prepared the attached Large Item Collection and Electronic Waste Recycling Policy for consideration by the Council, which policy reflects the City's current operations.

NOW, THEREFORE, BE IT RESOLVED, that the Common Council approves the Large Item

Collection and Electronic Waste Recycling Policy as prepared by the Streets Division, which policy is attached hereto.

BE IT FURTHER RESOLVED, that minor changes to this Policy may be made periodically by the Streets Superintendent, with the approval of the Board of Public Works.

Large Item Collection and Electronic Waste Recycling Policy of the City of Madison

Αŗ	proved	on	, 2024 ¹

Background

In the City of Madison (the "City"), large item collection has historically been handled by the City's Streets Division through a combination of dedicated collection routes, a sticker program, and drop-off sites open to City residents. Due to inherent inefficiencies in that arrangement, the Streets Division no longer operates dedicated large item collection routes and instead uses a work order system to manage its large item collection program. The Streets Division is also tasked with providing large item pick up during student moving days and in response to nuisance complaints. In order to continue to effectively provide this service to its core customer base, and to ensure that non-customers are effectively managing their own waste and recycling needs, it is necessary to create and implement this large item collection policy.

Wis. Stat. § 287.07(5) and § 287.17, and regulations and guidelines adopted by the Wisconsin Department of Natural Recourses, prohibit the disposal of certain electronic waste, including televisions, computers, printers, monitors, computer accessories, video players, and cell phones. Such items must be recycled at authorized recycling sites and cannot be placed or kept on any terrace or curbside for collection by the City. The City does offer public recycling sites for this purpose, and has historically charged for this erecycling service, when necessary, through sticker fees.

The purpose of this Large Item Collection and Electronic Recycling Policy (the "Policy") is to define the large item collection services of the City of Madison Streets Division (what items are covered, what properties are eligible, who has to pay fees), as well as the general guidelines users of the large item collection service must follow in order to use this service. Under this Policy, Customers will be provided large item collection services through the work order system. Non-Customers will need to arrange for large item collections separately, similarly to how their refuse and recycling is already handled. Non-Customers may not use the work order system. Prior to January 1, 2025, or at such later date as determined by the Streets Superintendent, multi-family buildings with more than 8 units that are not refuse or recycling customers will be considered Customers. This Policy also addresses electronic waste recycling.

Defining Properties to Be Provided Large Item Collection Services

Under City Resolution (<u>RES-10-00362</u>, <u>Legistar File No. 17693</u>) and the adopted Resource Recovery Special Charge Policy (<u>RES-22-00399</u>, <u>Legistar File No. 71186</u>), the Streets Division's collection of refuse and recycling is generally restricted to multi-family buildings of 8 units and fewer, although some small commercial properties and larger residential properties are provided refuse and recycling services as well.

For the purposes of this Policy, properties provided curbside refuse and/or recycling services by the Streets Division, single or two-family residential properties on private drives or carriage lanes, or other properties as determined by the Streets Superintendent, are "Customers", while properties that are not

¹ Policy was approved by the Common Council by RES-24-_____, File No. _____, on _____, 2024.

serviced by the Streets Division with refuse or recycling collection carts are "Non-Customers". However, prior to January 1, 2025, multi-family buildings with more than 8 units that are not refuse or recycling customers will be considered Customers. As of January 1, 2025, or at such later date as determined by the Streets Superintendent, these buildings will be Non-Customers. In addition, properties that otherwise qualify as Customers may be removed from the large item customer base and considered Non-Customers if the Superintendent determines that the property imposes unreasonable or disproportionate demands on the large item collection program.

Defining "Large Item"

A large item is considered to be an item meant for disposal, either as waste or for recycling, that is too large to be placed into a collection cart, or otherwise cannot be placed into one due to certain handling restrictions or the items may require a fee for disposal.

Large items can include furniture, lumber, bicycles, pianos, carpeting, fencing, mattresses, appliances, and many other items.

Large items do not include:

- bundles of excess cardboard;
- brush;
- electronics waste such as televisions, computers, printers, gaming systems, and similar electronics items that are required to be recycled by the Wisconsin Department of Natural Resources;
- overflow bags of trash that do not fit into a full curbside collection cart; and,
- loose debris/trash small enough to be placed into a collection cart for proper disposal.

Large Item Collection Process

Work Orders Required

In order to effectively manage the collection of large items across the City, Customers seeking large item collection services must first fill out a work order that provides the Streets Division with the list of items that need collection and the address and location of the items. The form also asks for contact information regarding who is completing the form.

The form can be completed online at www.cityofmadison.com/LargeItemWorkOrder or by following the link at www.cityofmadison.com/LargeItem.

Customers can also contact the Streets Divisions offices over the phone or in person for assistance in setting up the collection.

Customers choose the items that need collecting from the options in the work order form.

Work orders should reasonably account for the items placed at the curb for Streets Division crews to collect. Items may be left behind at the curb if they do not match the work order.

Some large items, like appliances, may have an additional charge Customers will need to pay. Items requiring a fee from must be on the work order without exception.

More details regarding large item fees can be found later in this policy document. The work order system handles the payment of these fees.

Importance of Work Orders

The Streets Division uses the work orders for two important functions in performing large item collection:

- Items in the work order determine which collection vehicle responds to the property to collect
 the material. Large item collection is actually performed by two separate crews. One to collect
 items destined for landfill disposal and a second to collect items for recycling, such as metal and
 appliances.
- The number of work orders received in a given week also helps determine staffing levels.

Non-Customers

Non-Customers cannot use the work order system for large item collection as curb-side large item collection service will not be provided to these properties. Instead, Non-Customers are required to arrange for large item collections separately, similarly to how their refuse and recycling is handled. A Non-Customer may temporarily use the terrace area adjacent to the property to store large-items for private collection purposes, but such items shall not be placed in the terrace area adjacent to the property for longer than 24 hours for this purpose.

Streets Division Large Item Collection Guidelines

When items subject to a work order are set out for collection, Customers are expected to follow the below guidelines in order to receive service from the Streets Division.

Failure to follow these guidelines may result in the items not being collected.

- Items must be stacked neatly on the right of way space in front of the home in the work order.
 - Exception: Some addresses may place items in locations other than directly in front of their homes, such as a home is on a corner or in those accessed only by private drives that cannot be used by the Streets Division. In these situations, the work order must clearly indicate where the items will be placed so crews will know where to collect them.
- Metals, appliances, and other recyclable large items must be placed out for collection separate from non-recyclable items such as furniture or carpeting.
- Items must be placed at least four feet away from obstructions such as utility poles, electrical boxes, street signs, fire hydrants, and other objects that can interfere with collection.
 - o Items should not be placed underneath low wires or low branches
- Items longer than 8 feet in length will not be collected as they cannot be reasonably expected to fit into the collection trucks.
- All previously defined guidelines must also be followed. These include:
 - Items out for pickup must meet the definition of large item;
 - Work orders must reasonably represent what it placed out for collection; and,
 - The items must be placed out for collection on the set out date selected.

Large Item and Electronics Fees

The Streets Division Superintendent may set fees and their rates for certain large items and electronics in accordance with MGO 10.18.

The Streets Division will keep current a list of all items requiring a fee on the Streets Division's large item website (www.cityofmadison.com/LargeItem).

The items requiring a fee may fall into of the following conditions:

- Items causing the Streets Division to incur additional costs to process items for recycling due to special handling requirements (i.e., air conditioners, dehumidifiers, refrigerators, fluorescent light fixtures, tanning beds, microwaves, computers, televisions, etc.);
- Items requiring special collection consideration (i.e., hot tubs, boats, large medical devices, etc.);
- Items previously charged in the large item collection program (lawnmowers, gas grills, etc.).

Paying for Large Item Collection and Electronics Recycling Fees

Currently, large item collection fees can be paid online via the large item work order system, over the phone by contacting the Streets Division offices, in person at the Streets Division offices, or by mailing in a check made payable to the City Treasurer's office.

As payment technology with the City changes, the ability to pay the necessary fees in different ways may also change.

The Streets Division will keep the payment options current on the Streets Division's large item website.

Drop-off Site Fees vs. Curbside Collection Fees

If a fee is required for a large item, curbside collection fees are paid when a large item work order is scheduled via the work order system.

Before an item that requires a fee is taken to drop-off site, the resident must first pay the necessary fee, receive a fee sticker, and then affix the sticker to the item.

Information regarding the process for paying fees will be kept current on the large item website and the drop-off site pages.

Refund Policy

There are no refunds for curbside collection fees.

Refunds can be granted when a fee sticker is returned unused to a Streets Division office.

Fees for Recycling Large Items and Electronics

Definitions

Customers who receive recycling collection services from the City of Madison Streets Division pay the Resource Recovery Special Charge (RRSC), which charge is imposed to collect the City's costs to provide recycling services to the property. For the purposes of this Policy, Customers who pay the

RRSC are "Ratepayers", and any other property or resident that does not pay the RRSC is a "Non-Ratepayer".

Ratepayers

If the RRSC rate includes sufficient funding to cover costs associated with the recycling of certain large items or electronics, Ratepayers will not be required to pay additional fees to cover said costs for the recycling of these recyclable large items or electronics.

For example, if the RRSC rate includes covering the costs for appliance or electronics recycling, Ratepayers would not be required to pay any additional fee, such as a fee sticker, to recycle a washing machine through curb side collection, or to bring a television, computer, or similar device to a City public recycling site.

Should the RRSC stop providing the funding to cover certain recycling costs of large items or electronics items, then the Ratepayers may need to pay the necessary fees to recycle these items unless these costs are recovered elsewhere in the budgeting process.

Non-Ratepayers

Non-Ratepayers do not provide funding for recycling services through the RRSC. Since they do not contribute to the RRSC, they cannot receive benefits provided by RRSC funding.

Thus, if a Non-Ratepayer is eligible for large item collection under this Policy, or if they bring a large item or electronic waste to a drop-off site, the Non-Ratepayer is required to pay any applicable fee for the item which would otherwise be covered by RRSC funding.

August Student Moving Days

The August Moving Days will not be affected by this Policy.

The annual services provided in response to the significant number of Madison residents moving simultaneously in the beginning weeks of August will continue within the Student Move Out Area as it has in years prior.

During the first weeks of August, the Streets Division will perform extra collection of materials in the defined Student Move Out Area. The Streets Division will empty trash and recycling carts, and they will collect large items, without the need for work orders.

Student Move Out Area

This area has been traditionally defined as ranging from the Yahara River to neighborhoods near the Camp Randall football stadium.

A map defining the boundaries of the Student Move Out Area will be kept current on the Moving Days website, www.cityofmadison.com/MovingDays.

Unscheduled Collections in Areas Served by Neighborhood Resource Teams

The Streets Division has a service level goal of providing weekly unscheduled collections in neighborhoods served by Neighborhood Resource Teams in approximately 75% of the weeks of the year.

Why Provide Elevated Level to NRT-Served Neighborhoods

Neighborhoods served by NRTs tend to see high turnover rates from residents. As a result, large items are frequently at the curb. In an effort to keep these neighborhoods welcoming and uncluttered, regular unscheduled collection is a service level goal.

Weekly Collection Not Guaranteed in the NRT Area

Due to a variety of reasons, from shortened holiday weeks to severe winter weather to equipment availability issues, the Streets Division may not be able to provide weekly collections.

Work Orders Encouraged in NRT Areas

Residents in NRT areas should still create large item work orders for material that needs collection. The unscheduled sweeps are a supplementary service designed to help these traditionally challenged neighborhoods.

Enforcement and Non-Customer Services

Unauthorized placement of large items in the terrace contrary to this Policy is a property maintenance issue under MGO Sec. 27.05(2)(dd). The Streets Division will work with Building Inspection to encourage compliance with this Policy.

Customers

If large items are placed at the terrace adjacent to a Customer's property contrary to this Policy, the Streets Division will work with Building Inspection on trying to get the Customer to properly complete a work order for the items. The Streets Division may collect large items from the terrace adjoining a Customer's property without a work order, although the Customer may be subject to the issuance of a citation for this service.

Non-Customers

Non-Customers are required to provide their own private large-item collection services to meet their property's needs, and the work order process for large item collection is not available to Non-Customers. Building Inspection will work with Non-Customers to ensure compliance with this Policy and effective and timely private collection of large items. Except as noted above for work associated with student moving days and within NRT-served neighborhoods, if the Streets Division collects large-items from the terrace adjoining a Non-Customer's property, the Non-Customer will be subject to the issuance of a citation for this service and a special charge to recover the City's cost to provide this service.

Large Item Collection and Electronic Waste Recycling Policy of the City of Madison

Approved on _______, 2024¹

Background

In the City of Madison (the "City"), large item collection has historically been handled by the City's Streets Division through a combination of dedicated collection routes, a sticker program, and drop-off sites open to City residents. Due to inherent inefficiencies in that arrangement, the Streets Division no longer operates dedicated large item collection routes and instead uses a work order system to manage its large item collection program. The Streets Division is also tasked with providing large item pick up during student moving days and in response to nuisance complaints. In order to continue to effectively provide this service to its core customer base, and to ensure that non-customers are effectively managing their own waste and recycling needs, it is necessary to create and implement this large item collection policy.

Wis. Stat. § 287.07(5) and § 287.17, and regulations and guidelines adopted by the Wisconsin Department of Natural Recourses, prohibit the disposal of certain electronic waste, including televisions, computers, printers, monitors, computer accessories, video players, and cell phones. Such items must be recycled at authorized recycling sites and cannot be placed or kept on any terrace or curbside for collection by the City. The City does offer public recycling sites for this purpose, and has historically charged for this erecycling service, when necessary, through sticker fees.

The purpose of this Large Item Collection and Electronic Recycling Policy (the "Policy") is to define the large item collection services of the City of Madison Streets Division (what items are covered, what properties are eligible, who has to pay fees), as well as the general guidelines users of the large item collection service must follow in order to use this service. Under this Policy, Customers will be provided large item collection services through the work order system. Non-Customers will need to arrange for large item collections separately, similarly to how their refuse and recycling is already handled. Non-Customers may not use the work order system. This Policy also addresses electronic waste recycling.

Defining Properties to Be Provided Large Item Collection Services

Under City Resolution (<u>RES-10-00362</u>, <u>Legistar File No. 17693</u>) and the adopted Resource Recovery Special Charge Policy (<u>RES-22-00399</u>, <u>Legistar File No. 71186</u>), the Streets Division's collection of refuse and recycling is generally restricted to multi-family buildings of 8 units and fewer, although some small commercial properties and larger residential properties are provided refuse and recycling services as well.

For the purposes of this Policy, properties provided curbside refuse and/or recycling services by the Streets Division, single or two-family residential properties on private drives or carriage lanes, or other properties as determined by the Streets Superintendent, are "Customers", while properties that are not serviced by the Streets Division with refuse or recycling collection carts are "Non-Customers".

¹ Policy was approved by the Common Council by RES-24-_____, File No. _____, on _____, 2024.

Defining "Large Item"

A large item is considered to be an item meant for disposal, either as waste or for recycling, that is too large to be placed into a collection cart, or otherwise cannot be placed into one due to certain handling restrictions or the items may require a fee for disposal.

Large items can include furniture, lumber, bicycles, pianos, carpeting, fencing, mattresses, appliances, and many other items.

Large items do not include:

- bundles of excess cardboard;
- brush
- electronics waste such as televisions, computers, printers, gaming systems, and similar electronics items that are required to be recycled by the Wisconsin Department of Natural Resources;
- overflow bags of trash that do not fit into a full curbside collection cart; and,
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Large Item Collection Process

Work Orders Required

In order to effectively manage the collection of large items across the City, Customers seeking large item collection services must first fill out a work order that provides the Streets Division with the list of items that need collection and the address and location of the items. The form also asks for contact information regarding who is completing the form.

The form can be completed online at www.cityofmadison.com/LargeItem. or by following the link at www.cityofmadison.com/LargeItem.

Customers can also contact the Streets Divisions offices over the phone or in person for assistance in setting up the collection.

Customers choose the items that need collecting from the options in the work order form.

Work orders should reasonably account for the items placed at the curb for Streets Division crews to collect. Items may be left behind at the curb if they do not match the work order.

Some large items, like appliances, may have an additional charge Customers will need to pay. Items requiring a fee from must be on the work order without exception.

More details regarding large item fees can be found later in this policy document. The work order system handles the payment of these fees.

Importance of Work Orders

The Streets Division uses the work orders for two important functions in performing large item collection:

Items in the work order determine which collection vehicle responds to the property to collect
the material. Large item collection is actually performed by two separate crews. One to collect

items destined for landfill disposal and a second to collect items for recycling, such as metal and appliances.

• The number of work orders received in a given week also helps determine staffing levels.

Non-Customers

Non-Customers cannot use the work order system for large item collection as curb-side large item collection service will not be provided to these properties. Instead, Non-Customers are required to arrange for large item collections separately, similarly to how their refuse and recycling is handled. A Non-Customer may temporarily use the terrace area adjacent to the property to store large-items for private collection purposes, but such items shall not be placed in the terrace area adjacent to the property for longer than 24 hours for this purpose.

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When items subject to a work order are set out for collection, Customers are expected to follow the below guidelines in order to receive service from the Streets Division.

Failure to follow these guidelines may result in the items not being collected.

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 - Exception: Some addresses may place items in locations other than directly in front of their homes, such as a home is on a corner or in those accessed only by private drives that cannot be used by the Streets Division. In these situations, the work order must clearly indicate where the items will be placed so crews will know where to collect them.
- Metals, appliances, and other recyclable large items must be placed out for collection separate from non-recyclable items such as furniture or carpeting.
- Items must be placed at least four feet away from obstructions such as utility poles, electrical boxes, street signs, fire hydrants, and other objects that can interfere with collection.
 - o Items should not be placed underneath low wires or low branches
- Items longer than 8 feet in length will not be collected as they cannot be reasonably expected to fit into the collection trucks.
- All previously defined guidelines must also be followed. These include:
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Definitions

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For example, if the RRSC rate includes covering the costs for appliance or electronics recycling, Ratepayers would not be required to pay any additional fee, such as a fee sticker, to recycle a washing

machine through curb side collection, or to bring a television, computer, or similar device to a City public recycling site.

Should the RRSC stop providing the funding to cover certain recycling costs of large items or electronics items, then the Ratepayers may need to pay the necessary fees to recycle these items unless these costs are recovered elsewhere in the budgeting process.

Non-Ratepayers

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Page 6 of 6

Work Orders Encouraged in NRT Areas

Residents in NRT areas should still create large item work orders for material that needs collection. The unscheduled sweeps are a supplementary service designed to help these traditionally challenged neighborhoods.

Enforcement and Non-Customer Services

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Customers

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From: john zwickey

To: Finance Committee

Subject: Large Item Pick Up Policy Change **Date:** Sunday, July 7, 2024 5:31:41 PM

You don't often get email from rjzwickey@gmail.com. Learn why this is important

Caution: This email was sent from an external source. Avoid unknown links and attachments.

There were pictures posted on the Next Door app of the pile of large items and trash on the terrace by Meadows Apartments. The pile surrounded a no dumping sign that was erected recently to combat on going dumping. The pictures generated over 100 comments about the trash pile. According to the Building Inspectors office a complaint was filed May 31. The trash pile was not picked up until June 26 (give or take). The pile started again the very next day. I've personally seen 3+ people pull up and dump stuff out of their car. I strongly urge to to pass these changes as quickly as possible.

John Zwickey

rom: Lauren Chang
o: Finance Committee: Board of Public Wor
e: Fidel, Denok
ublject: Changes to Large Item Pick Policy
utee: Sunday, July 7, 2024 3:51:27 PM

[Some people who received this message don't often get email from speckson@charter.net. Learn why this is important at https://aka.ms/LearnAboutSenderIdentification

Caution: This email was sent from an external source. Avoid unknown links and attachmen

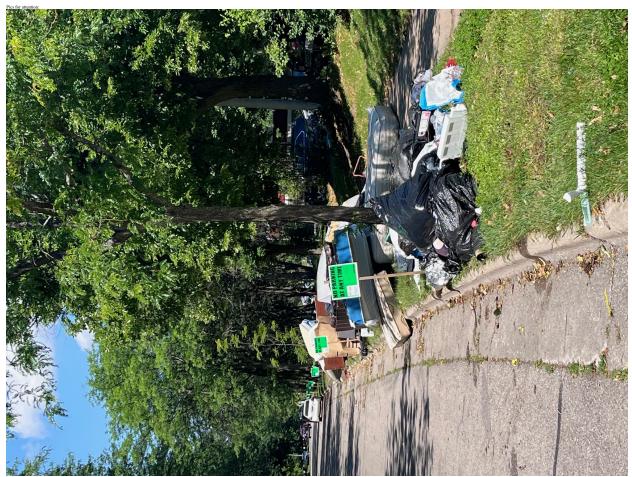
Dear Board and Committee Members

As a member of our community who runs or drives by the monthly heaps o' garbage, redolent on the soggy summer mornings, I recommend you adopt changes to the policy as proposed by my alder, Derek Fields

The lining of the street for weeks at a time with the sad leftovers from people likely evicted or unable to move even their beds does little to enhance neighborhood pride or appeal - especially for the apartment home it all clearly tumbled out of.

While I have no desire to treat multi-family housing owners/operators unfairly, the volume of detritus and the frequency (yup, every month) as well as the cost to our strapped city demands that they adopt - and pay for - a strategy and service that responds to the routine house cleaning

Many thanks to Alder Fields for responding to the need and best wishes in your decision-making to require new policy elements that put the costs and responsibilities on the large building owner









District 3 Reside Lauren Cnare



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83793

File ID:83793File Type:ResolutionStatus:Report of Officer

Version: 1 Reference: Controlling Body: BOARD OF

PUBLIC WORKS

File Created Date: 06/04/2024

File Name: Assessing Benefits Halo Lane and Twilight Trail Final Action:

Resurfacing Assessment District - 2023.

Title: Assessing Benefits Halo Lane and Twilight Trail Resurfacing Assessment District

- 2023. (District 16)

Notes: Steve Sonntag

Sponsors: BOARD OF PUBLIC WORKS Effective Date:

Attachments: 13011_HaloAssessmentsFinal.pdf Enactment Number:

Author: Jim Wolfe, City Engineer Hearing Date:

Entered by: hfleegel@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Division Action Text: This Resc	06/04/2024 olution was Ref		BOARD OF PUBLIC WORKS PUBLIC WORKS due ba	06/26/2024 ack on 6/26/2024	06/26/2024	
1	BOARD OF PUBLIC WORKS	06/26/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass
		•	•	led by Kliems, to RECC passed by voice vote/otl		JNCIL TO	

Text of Legislative File 83793

Fiscal Note

No appropriation required.

Title

Assessing Benefits Halo Lane and Twilight Trail Resurfacing Assessment District - 2023. (District 16)

Body

WHEREAS, the construction of improvement(s) on Halo Lane and Twilight Trail Resurfacing Assessment District - 202 has been completed; and,

WHEREAS, it has been determined that the abutting property is to pay the amount per the

attached schedule of assessments.

NOW, THEREFORE, BE IT RESOLVED, that the sums so charged to the abutting property be and are hereby assessed and levied upon said property payable by the owners of the lots or parcels of land benefited as determined by the Board of Public Works, and in accordance with its recommendations, the amount so charged to each of the lots or parcels of land so benefited being as follows, to wit:

City of Madison Engineering Division - Schedule of Assessments

Date: 12/7/2022

Project ID: 11182

Halo Lane and Twilight Trail Resurfacing Assessment District - 2023 Halo Lane from Angel Crest Way to Twilight Trail Project Name:

Project Description: Twilight Trail from Halo Lane to East Buckeye Road

*A factor has been applied for pavement work adjacent to lots that abut more than one street and are single-family or two-family residential dwellings.

	•	·		Fro				Street Resurfacing Items			<u> </u>
Parcel No./ Zoning	Owner Name Address	Parcel Location/Condo	LF	Frontage St	Multiple Front			rfacing per LF	Total Assessment		
						Factor	\$10.88	Cost	Assessment		
071015201280 SR-C1	Withheld pursuant to sec. 19.35(1)(am) Wis. Stats. 1613 TWILIGHT TRL MADISON, WI 53716	1613 Twilight Trl	115.00 100.00	Twilight Trl Jaarsma Ct	Yes	0.5	70	\$380.80	\$380.80		
071015201412 SR-C1	MEYSEMBOURG, GERALD L & KATHLEEN J MEYSEMBOURG 1 JAARSMA CT MADISON, WI 53716-1886	1 Jaarsma Ct	125.62 100.00	Jaarsma Ct Twilight Trl	Yes	0.5	160.62	\$873.77	\$873.77		
071015201420 SR-C1	KEITH JR, LEE E & LINDA M KEITH PO BOX 14222 MADISON, WI 53708	1709 Twilight Trl	125.67 75.00	Twilight Trl E Buckeye Rd	Yes	0.5	125.61	\$683.32	\$683.32		
071015202204 SR-C1	GRAFF, ROGER & CAROLYN 4817 HALO LN MADISON, WI 53716-1837	4817 Halo Ln	77.80	Halo Ln	No	1	15	\$163.20	\$163.20		
071015206123 SR-C1	MILLER, CHRIS J TARA MILLER 1633 ANGEL CREST WAY MADISON, WI 53716	1633 Angel Crest Way	81.65 78.65	Angel Crest Way Halo Ln	Yes	0.5	15	\$81.60	\$81.60		
071015206131 SR-C1	PEETZ REV TRUST, STEPHEN AND JULIE 1614 TWILIGHT TRL MADISON, WI 53716-1882	1614 Twilight Trl	90.00 111.42	Twilight Trl Halo Ln	Yes	0.5	146.42	\$796.52	\$796.52		
071015210017 SR-C1	KUNTZ, PETER & REBECCA 1710 TWILIGHT TRL MADISON, WI 53716-1884	1710 Twilight Trl	86.28 116.16	Twilight Trl E Buckeye Rd	Yes	0.5	86.28	\$469.36	\$469.36		
071015210025 SR-C1	SEIFERT, MATTHEW W & ASTRI K OLSON 1706 TWILIGHT TRL MADISON, WI 53716-1884	1706 Twilight Trl	80.00	Twilight Trl	No	1	80	\$870.40	\$870.40		
071015210033 SR-C1	KELLEY, DEREK A CASEY L BULTMAN 1702 TWILIGHT TRL MADISON, WI 53716-1884	1702 Twilight Trl	85.00 112.36	Twilight Trl Halo Ln	Yes	0.5	197.36	\$1,073.64	\$1,073.64 \$5,392.62		

\$5,392.62



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83794

File ID: 83794 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: BOARD OF

PUBLIC WORKS

File Created Date: 06/04/2024

Final Action:

File Name: Accepting storm sewer improvements constructed by

Private Contract for 413-429 Yellowstone Drive Storm Sewer Relocation, Private Contract No. 2341

Title: Accepting storm sewer improvements constructed by Private Contract for

413-429 Yellowstone Drive Storm Sewer Relocation, Private Contract No. 2341

(District 19).

Notes: Amy Kemp

Sponsors: BOARD OF PUBLIC WORKS Effective Date:

Attachments: Yellowstone Dr Storm Schedule A Unit Cost.pdf Enactment Number:

 Author:
 Jim Wolfe, City Engineer
 Hearing Date:

 Entered by:
 hfleegel@cityofmadison.com
 Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Divis			BOARD OF PUBLIC WORKS	06/26/2024	06/26/2024	
	Action Text:	This Resolution was Ref	er to the BOARD OF P	JBLIC WORKS due ba	ack on 6/26/2024		
1	BOARD OF PUB WORKS	LIC 06/26/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass
	Action Text:	A motion was made by A ADOPT - REPORT OF (•	•		JNCIL TO	

Text of Legislative File 83794

Fiscal Note

No City Funds required. Private contract.

Title

Accepting storm sewer improvements constructed by Private Contract for 413-429 Yellowstone Drive Storm Sewer Relocation, Private Contract No. 2341 (District 19).

Body

WHEREAS, BKD Wynwood of Madison West Real Estate, LLC, has satisfactorily completed the installation of storm sewer by private contract, in accordance with the City of Madison

Standard Specifications for Public Works Construction, in: 413-429 Yellowstone Drive Storm Sewer Relocation.

NOW, THEREFORE, BE IT RESOLVED

1. That said storm sewer improvements are hereby assigned to the Madison Storm Water Utility for ownership and maintenance and that the attached Schedule "A" representing the actual cost of construction of these improvements shall be reflected in the Madison Storm Water Utility's "Utility Plant In Service".

Increase in Total Plant Value per Schedule "A" \$53,767.71

STORM SEWER PLANT VALUE

(Schedule 'A' / Storm Pipes)

413-429 Yellowstone Dr

53B2341

PIPES

1 11 20	Description	Quantity	Unit	Unit Cost	Total Cost
36" RCP		489	LF	\$ 55.00	\$ 26,895.00
Engineering (@ 12%				\$ 3,227.40
Total Cost				\$ 61.60	\$ 30,122.40
Storm Sewe	r Trench Compaction	489	LF	\$ 1.17	\$ 572.13
Engineering (@ 12%				\$ 68.66
Total Cost				\$ 1.31	\$ 640.79
Engineering (@ 12%				
Total Cost		┤ −−−-			l
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Engineering	@ 12%				
Total Cost		J l			l
GRAND TOT	TAL COST				\$ 30,763.19
<u> </u>					

Prepared By:	EEA
Date:	6/4/2024

STORM SEWER PLANT VALUE

(Schedule 'A' / Storm Structures)

413-429 Yellowstone Dr

53B2341

STRUCTURES

Description	Quantity	Unit	Unit Cost	Total Cost
4X4 SAS	7	EACH		\$ 20,539.75
Engineering @ 12%				\$ 2,464.77
Total Cost		 	\$ 3,286.36	\$ 23,004.52
Engineering @ 12%				
Total Cost		 		
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Engineering @ 12%				
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Engineering @ 12%				
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Engineering @ 12%				
Total Cost		I		
GRAND TOTAL COST			-	\$ 23,004.52

Total Storm Sewer Improvements: \$ 53,767.71 Pate: 6/4/2024



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83795

File ID: 83795 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: BOARD OF

PUBLIC WORKS

File Created Date: 06/04/2024

Final Action:

File Name: Approving plans and specifications and authorizing

the Board of Public Works to advertise and receive bids for CIPP Lining Rehabilitation - Blair Street

Sanitary Sewer Siphon.

Title: Approving plans and specifications and authorizing the Board of Public Works to

advertise and receive bids for CIPP Lining Rehabilitation - Blair Street Sanitary

Sewer Siphon.(District 4 & District 6)

Notes: Eric Cefalu

Sponsors: BOARD OF PUBLIC WORKS Effective Date:

Attachments: 15295 BPW Exhibit_5-30-24.pdf Enactment Number:

 Author:
 Jim Wolfe, City Engineer
 Hearing Date:

 Entered by:
 hfleegel@cityofmadison.com
 Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Division	06/18/2024	Refer	BOARD OF PUBLIC WORKS	06/26/2024	06/26/2024	
	Action Text: This Re	esolution was Ref	er to the BOARD OF	PUBLIC WORKS due ba	ack on 6/26/2024		
1	BOARD OF PUBLIC WORKS	06/26/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass
		•	•	nded by Kliems, to RECC n passed by voice vote/ot		UNCIL TO	

Text of Legislative File 83795

Fiscal Note

The proposed resolution approves plans and specifications and authorizes the Board of Public Works to advertise and receive bids for the CIPP Lining Rehabilitation - Blair Street Sanitary Sewer Siphon. The total estimated cost of the project is \$99,250. Funding is available in Munis #15295. No additional appropriation is required.

Title

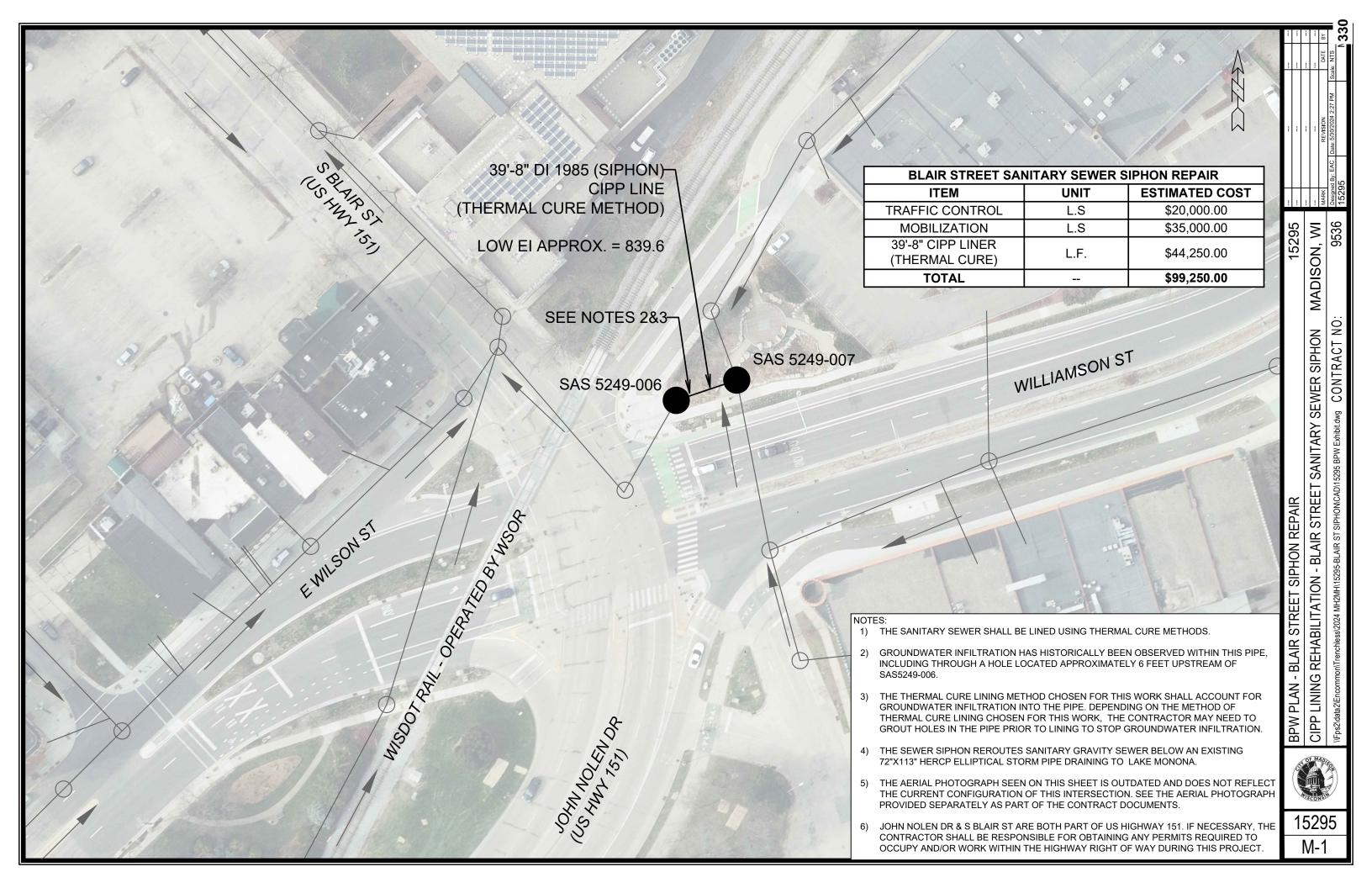
Approving plans and specifications and authorizing the Board of Public Works to advertise and receive bids for CIPP Lining Rehabilitation - Blair Street Sanitary Sewer Siphon.(District 4 &

District 6)

Body

BE IT HEREBY RESOLVED:

- 1. That the plans and specifications for CIPP Lining Rehabilitation Blair Street Sanitary Sewer Siphon, be and are hereby approved.
- 2. That the Board of Public Works be and is hereby authorized to advertise and receive bids for said project.
- 3. That the Mayor and City Clerk are hereby authorized to sign and grant easements or right-of-way release or procurement documents, maintenance agreements or encroachment agreements, as necessary and grant or accept dedication of lands and/or easements from/to the Developer/Owner for public improvements located outside of existing public fee title or easement right-of-ways.





City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83995

File ID: 83995 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: BOARD OF

PUBLIC WORKS

File Created Date: 06/18/2024

Final Action:

File Name: Accepting street improvements (sidewalk)

constructed by Private Contract 502 - 504 W. Main

Street, Private Contract No. 2392

Title: Accepting street improvements (sidewalk) constructed by Private Contract 502 -

504 W. Main Street, Private Contract No. 2392 (District 4).

Notes: Amy Kemp

Sponsors: BOARD OF PUBLIC WORKS Effective Date:

Attachments: 2392 Sidewalk Schedule A.pdf Enactment Number:

Author: Jim Wolfe, City Engineer Hearing Date:

Entered by: hfleegel@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Division Action Text: This Resol	06/18/2024	Refer	BOARD OF PUBLIC WORKS	06/26/2024	06/26/2024	
	Action Text. This Neson	iulion was itei	er to the BOARD OF T	ODEIC WORKS due ba	ICK 011 0/20/2024		
1	BOARD OF PUBLIC WORKS	06/26/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass
	Action Text: A motion w	vas made by A	Ald. Guequierre, second	ed by Kliems, to RECO	MMEND TO COL	JNCIL TO	
		•	DFFICER. The motion p	•			

Text of Legislative File 83995

Fiscal Note

No City Funds required. Private contract.

Title

Accepting street improvements (sidewalk) constructed by Private Contract 502 - 504 W. Main Street, Private Contract No. 2392 (District 4).

Body

WHEREAS, the 502 - 504 W. Main, LLC, has satisfactorily completed the installation of street improvements (sidewalk) by private contract, in accordance with the City of Madison Standard Specifications for Public Works Construction, in: 502 - 504 W. Main Street.

NOW, THEREFORE, BE IT RESOLVED

- 1. That said street improvements (sidewalk) be and are hereby accepted.
- 2. That the street improvements, **sidewalk**, on the following streets be and are hereby accepted:
 - West Main Street Sidewalk from N of S Bedford Street to S Bassett Street.
 - South Bassett Street Sidewalk from W of W Washington Ave to W Main Street.

The attached Schedule "A" representing the actual cost of construction of these improvements, which shall increase the street infrastructure value for Project No. 10229 \$ 705.60 for a useful life of 40 years.

STREET PLANT VALUE

SIDEWALKS

502 - 504 W. Main Street 10229

SIDEWALK

Description	Limits	Limits	Quantity	Unit		Cost	To	tal Cost
West Main Street Sidewalk	N of S Bedford Street	S Bassett Street	100	SF	\$	3.15		315.00
5' concrete sidewalk			Enginee	ring @ 12%			\$	37.80
MS LINK 4438		L	<u> </u>	Total Cost			\$	352.80
South Bassett Street Sidewalk	W of W Washington Ave	W Main Street	100		\$	3.15		315.00
5' concrete sidewalk			Enginee	ring @ 12%			\$	37.80
MS LINK 4425		L	↓	Total Cost	L		\$	352.80
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GRAND TOTAL COST				- — —			- <i></i>	705.60
CIGHD TOTAL COST						-	Ψ	700.00

Prepared By:	aek
Date:	6/10/2024



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83996

File ID: 83996 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: BOARD OF

PUBLIC WORKS

File Created Date: 06/18/2024

Final Action:

Enactment Number:

File Name: Accepting storm sewer and street improvements

(excluding bituminous surface pavement)

constructed by Private Contract In Secret Places at Siggelkow Preserve - Phase 6, Private Contract No.

2251

Title: Accepting storm sewer and street improvements (excluding bituminous surface

pavement) constructed by Private Contract In Secret Places at Siggelkow

Preserve - Phase 6, Private Contract No. 2251 (District 16).

Notes: Amy Kemp

Sponsors: BOARD OF PUBLIC WORKS Effective Date:

Attachments: 2251 Street Schedule A excluding bituminous.pdf,

Secret Places Ph 6 Storm Schedule A Unit Cost.pdf

Author: Jim Wolfe, City Engineer Hearing Date:

Entered by: hfleegel@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Division Action Text: This Resol	06/18/2024 ution was Ref		BOARD OF PUBLIC WORKS PUBLIC WORKS due ba	06/26/2024 ck on 6/26/2024	06/26/2024	
1	BOARD OF PUBLIC WORKS	06/26/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass
		,		led by Kliems, to RECO passed by voice vote/oth		JNCIL TO	

Text of Legislative File 83996

Fiscal Note

No City Funds required. Private contract.

Title

Accepting storm sewer and street improvements (excluding bituminous surface pavement) constructed by Private Contract In Secret Places at Siggelkow Preserve - Phase 6, Private Contract No. 2251 (District 16).

Body

WHEREAS, Great Neighborhoods South, LLC, has satisfactorily completed the installation of storm sewer and street improvements (excluding bituminous surface pavement) by private contract, in accordance with the City of Madison Standard Specifications for Public Works Construction, in: Secret Places at Siggelkow Preserve - Phase 6.

NOW, THEREFORE, BE IT RESOLVED

- 1. That said storm sewer and street improvements (excluding bituminous surface pavement) be and are hereby accepted.
- 2. That said storm sewer improvements are hereby assigned to the Madison Storm Water Utility for ownership and maintenance and that the attached Schedule "A" representing the actual cost of construction of these improvements shall be reflected in the Madison Storm Water Utility's "Utility Plant In Service".

Increase in Total Plant Value per Schedule "A" \$137,942.24.

- 3. That the street improvements, **excluding the bituminous pavement**, on the following streets be and are hereby accepted:
 - · Pacific Crest Road from West Line of Lot 231 to Ice Pond Drive.
 - · Ice Pond Drive from North Line of Lot 214 to South Line of Lot 218.

The attached Schedule "A" representing the actual cost of construction of these improvements, which shall increase the street infrastructure value for Project No. 53B2251 by \$87,554.88 for a useful life of 40 years.

STREET PLANT VALUE

DOES NOT INCLUDE BITUMINOUS SURFACE WITH SIDEWALK

Secret Places at Siggelkow Preserve - Phase 6 53B2251

STREET

Description	From	То	Quantity	Unit	Ur	nit Cost		Total Cost
Pacific Crest Road	West Line of Lot 231	Ice Pond Drive	475	LF	\$	101.00	\$	47,975.00
32' WIDE			Engine	Total Cost		101.00	\$	5,757.00
MS LINK: 20897		J		Total Cost	<u>_</u>		\$	53,732.00
Ice Pond Drive	North Line of Lot 214	South Line of Lot 218	299	LF	\$	101.00	\$	30,199.00
32' WIDE			Engine	ering @ 12%			\$	3,623.88
MS LINK: 20899	 			Total Cost	<u> </u>		\$	33,822.88
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	1		Engine	ering @ 12%				
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TOTAL STREET IMPROVEMENTS \$

87,554.88

Prepared By: aek

Date: 6/12/2024

STORM SEWER PLANT VALUE

(Schedule 'A' / Storm Structures)

Secret Places at Sigglekow Preserve Ph 6

53B2251

STRUCTURES

Description	Quantity	Unit	Unit Cost	Total Cost
H Inlet	8	EACH	\$ 1,995.29	\$ 15,962.32
Engineering @ 12%				\$ 1,915.48
Total Cost			\$ 2,234.72	\$ 17,877.80
4X4 SAS	5	EACH	\$ 2,934.25	\$ 14,671.25
Engineering @ 12%				\$ 1,760.55
Total Cost			\$ 3,286.36	\$ 16,431.80
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Engineering @ 12%				
Total Cost				
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GRAND TOTAL COST				\$ 34,309.60
			i	Ţ 01,000.00

Prepared By: <u>EEA</u>

Date: <u>6/10/2024</u>

\$

STORM SEWER PLANT VALUE

(Schedule 'A' / Storm Pipes)

Secret Places at Sigglekow Preserve Ph 6

53B2251

PIPES

Description	Quantity	Unit	Unit Cost	Total Cost
12" RCP	106	LF	\$ 41.08	\$ 4,354.48
Engineering @ 12%	100	LF	φ 41.00	\$ 4,334.46
Total Cost			\$ 46.01	\$ 4,877.02
18" RCP	148		\$ 49.30	\$ 7,296.40
Engineering @ 12%	140	LF	φ 49.30	\$ 7,290.40
Total Cost	+		\$ 55.22	\$ 8,171.97
27" RCP	444		\$ 60.00	\$ 26,640.00
Engineering @ 12%	444	LF	φ 60.00	\$ 20,040.00
Total Cost			\$ 67.20	\$ 29,836.80
30" RCP	119		\$ 65.00	\$ 7,735.00
Engineering @ 12%	119	LF	\$ 05.00	\$ 7,733.00
Total Cost			\$ 72.80	\$ 8,663.20
14"X23" HERCP	361		\$ 125.00	\$ 45,125.00
Engineering @ 12%	301	LI	Ψ 123.00	\$ 5,415.00
Total Cost			\$ 140.00	\$ 50,540.00
Storm Sewer Trench Compaction	1178		\$ 1.17	\$ 1,378.26
Engineering @ 12%	1170	- 11	Ψ 1.17	\$ 165.39
Total Cost	+		\$ 1.31	\$ 1,543.65
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Engineering @ 12%			+	
Total Cost				
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GRAND TOTAL COST				\$ 103,632.64
S.S.I.B TOTAL GOOT				Ψ 100,002.04

Prepared By: EEA

Date: 6/10/2024



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83997

File ID: 83997 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: BOARD OF

PUBLIC WORKS

File Created Date: 06/18/2024

Final Action:

File Name: Declaring the City of Madison's intention to exercise

its police powers establishing the South Pinckney Street Resurfacing Assessment District - 2024.

Title: Declaring the City of Madison's intention to exercise its police powers establishing

the South Pinckney Street Resurfacing Assessment District - 2024.(District 4)

Notes: Aaron Canton

Sponsors: Michael E. Verveer Effective Date:

Attachments: Enactment Number:

Author: Jim Wolfe, City Engineer Hearing Date:

Entered by: hfleegel@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Division Action Text: This Resolu	06/18/2024 ution was Ref	Refer	BOARD OF PUBLIC WORKS JBLIC WORKS due ba	06/26/2024 ck on 6/26/2024	06/26/2024	
1	BOARD OF PUBLIC WORKS	06/26/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass
		•	Ald. Guequierre, seconde DFFICER. The motion pa			JNCIL TO	

Text of Legislative File 83997

Fiscal Note

No Funds Required.

Title

Declaring the City of Madison's intention to exercise its police powers establishing the South Pinckney Street Resurfacing Assessment District - 2024.(District 4)

Body

BE IT HEREBY RESOLVED:

1. That the Common Council of the City of Madison hereby establishes an assessment district

known as the South Pinckney Street Resurfacing Assessment District - 2024, to serve South Pinckney Street from King Street to East Doty Street, all in accordance with Section 66.0701 (formerly Section 66.62) of the Wisconsin State Statutes and Section 4.09 of the Madison General Ordinances.

- 2. That the contemplated purposes of this assessment district is to mill & overlay the top layer of asphalt pavement.
- 3. That the benefited properties shall have the opportunity to pay the special assessments which may be levied as a result of this assessment district in eight (8) annual installments, subject to the current interest rate in effect at the time of establishment of the special assessment.
- 4. That the City Engineer is hereby directed to prepare a report consisting of preliminary or final plans and specifications; an estimate of the entire cost of the proposed work or improvements within the assessment district; a schedule of proposed assessments, constituting an exercise of police power of the City of Madison; and a statement that the property against which the assessments are proposed is benefited.
- 5. That upon completion of this aforesaid report, the City Engineer shall proceed in accordance with Section 4.09 of the Madison General Ordinances.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83998

File ID: 83998 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: BOARD OF

PUBLIC WORKS

File Created Date: 06/18/2024

Final Action:

File Name: Accepting street improvements (sidewalk)

constructed by Private Contract In 1109-1123 S Park

Street, Private Contract No. 8926

Title: Accepting street improvements (sidewalk) constructed by Private Contract In

1109-1123 S Park Street, Private Contract No. 8926 (District 13).

Notes: Amy Kemp

Sponsors: BOARD OF PUBLIC WORKS Effective Date:

Attachments: 8926 Street (Sidewalk) Schedule A.pdf Enactment Number:

Author: Jim Wolfe, City Engineer Hearing Date:

Entered by: hfleegel@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Division Action Text: This Resolu		Refer	BOARD OF PUBLIC WORKS UBLIC WORKS due ba	06/26/2024 ack on 6/26/2024	06/26/2024	
1	BOARD OF PUBLIC WORKS	06/26/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass
		•	Ald. Guequierre, second DFFICER. The motion p			UNCIL TO	

Text of Legislative File 83998

Fiscal Note

No City Funds required. Private contract.

Title

Accepting street improvements (sidewalk) constructed by Private Contract In 1109-1123 S Park Street, Private Contract No. 8926 (District 13).

Body

WHEREAS, Glen Rock Development, LLC, has satisfactorily completed the installation of street improvements (sidewalk) by private contract, in accordance with the City of Madison Standard Specifications for Public Works Construction, in: 1109-1123 S Park Street.

NOW, THEREFORE, BE IT RESOLVED

- 1. That said street improvements (sidewalk) be and are hereby accepted.
- 2. That the street improvements, **sidewalk**, on the following streets be and are hereby accepted:
 - S Park Street Sidewalk from N of S Park Street to Emerson Street.
 - Emerson Street Sidewalk from W of Lowell ST Alley to S Park Street.

The attached Schedule "A" representing the actual cost of construction of these improvements, which shall increase the street infrastructure value for Project No. 13044 by \$1,591.81 for a useful life of 40 years.

STREET PLANT VALUE

SIDEWALKS

1109-1123 S Park Street 13044

SIDEWALK

State Stat	Description	Limits	Limits	Quantity	Unit	Unit Cost		Total Cost
Emerson Street Sidewalk	S Park Street Sidewalk	N of S Park Street	Emerson Street	252	SF	\$ 3.58		902.16
Emerson Street Sidewalk				Enginee	ring @ 12%		\$	108.26
Engineering @ 12% \$ 62.4			L		Total Cost			1,010.42
## SUNK 6088 Total Cost \$ 581.* Engineering @ 12% Total Cost	merson Street Sidewalk	W of Lowell ST Alley	S Park Street	145	SF	\$ 3.58		519.10
Total Cost \$ 581.	5' concrete sidewalk			Enginee	ring @ 12%			62.29
Total Cost	MS LINK 6088				Total Cost		\$	581.39
Total Cost								
Engineering @ 12% Total Cost				Enginee	ring @ 12%			
Total Cost					Total Cost			
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Engineering @ 12% Total Cost					Total Cost			
Total Cost							T	
Total Cost				Enginee	ring @ 12%			
CRAND TOTAL COST					Total Cost			
	CDAND TOTAL COST							1,591.81

Prepared By:	aek
Date:	6/18/2024



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84000

File ID: 84000 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: BOARD OF

PUBLIC WORKS

File Created Date: 06/18/2024

Final Action:

File Name: Approving plans and specifications and authorizing

the Board of Public Works to advertise and receive bids for McPike Park - Building Demolition and Site

Restoration.

Title: Approving plans and specifications and authorizing the Board of Public Works to

advertise and receive bids for McPike Park - Building Demolition and Site

Restoration. (District 6)

Notes: Mike Sturm

Sponsors: BOARD OF PUBLIC WORKS Effective Date:

Attachments: Contract 9174 McPike Park Building Demo Plans Enactment Number:

06-07-24.pdf

Author: Jim Wolfe, City Engineer Hearing Date:

Entered by: hfleegel@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Division Action Text: This Resol	06/20/2024 lution was Ref	Refer	BOARD OF PUBLIC WORKS UBLIC WORKS due ba	06/26/2024 ack on 6/26/2024	06/26/2024	
1	BOARD OF PUBLIC WORKS	06/26/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass
		•	Ald. Guequierre, second DFFICER. The motion p			JNCIL TO	

Text of Legislative File 84000

Fiscal Note

The proposed resolution approves plans and specifications and authorizes the Board of Public Works to advertise and receive bids for the McPike Park - Building Demolition and Site Restoration. The total estimated cost of the project is \$500,000. Funding is available in Munis #10525. No additional appropriation is required.

Title

Approving plans and specifications and authorizing the Board of Public Works to advertise and

receive bids for McPike Park - Building Demolition and Site Restoration. (District 6) **Body**

BE IT RESOLVED,

- 1) That the plans and specifications for McPike Park Building Demolition and Sit Restoration be and are hereby approved.
- 2) That the Board of Public Works be and is hereby authorized to advertise and receive bids for said project.
- 3) That the Mayor and City Clerk are hereby authorized to sign and grant easements or right-of-way release or procurement documents, maintenance agreements or encroachment agreements, as necessary and grant or accept dedication of lands and/or easements from/to the Developer/Owner for public improvements located outside of existing public fee title or easement right-of-ways.

LEGEND

© EX. ELECTRICAL HAND HOLE

EX. ELECTRICAL PEDESTAL
EX. TELEPHONE PEDESTAL

EX. TREE

EX. WATER HYDRANT

EX. WATER VALVE

--- EX. PROPERTY LINE

EX. EASEMENT

EX. FENCE LINE
EX. TREE LINE

EX. CONTOUR (INDEX)

EX. CONTOUR (INTER)

EX. ABANDONED UTILITY

E EX. ELECTRIC

EX. FIBER OPTIC

G EX. NATURAL GAS

OH EX. OVERHEAD LINE
SN EX. SANITARY SEWER

EX. STORM SEWER

EX. UNDERGROUND TELEPHONE

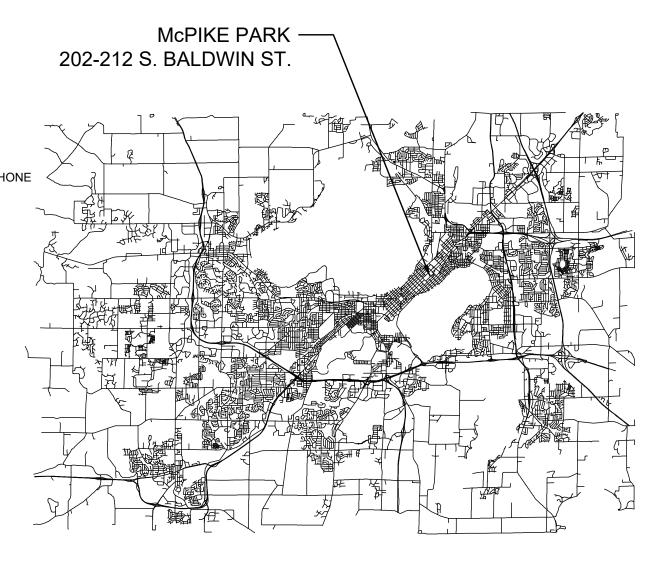
EX. CABLE TV

W EX. WATER LINE

EX. GRAVEL

EX. CONCRETE

McPIKE PARK BUILDING DEMOLITION AND SITE RESTORATION



City of Madison Department of Public Works PARKS DIVISION

City-County Building, Suite 104 210 Martin Luther King, Jr. Blvd. Madison, WI 53703





PROJECT:

MCPIKE PARK
BUILDING
DEMOLITION AND
SITE RESTORATION

202 - 212 S. BALDWIN ST. MADISON, WI 53703

Although every effort has been made in preparing these plans and checking them for accuracy, the contractor and subcontractors must check all details and dimensions of their trade and be responsible for the same.

BID RELEASE DATE 2024-06-18

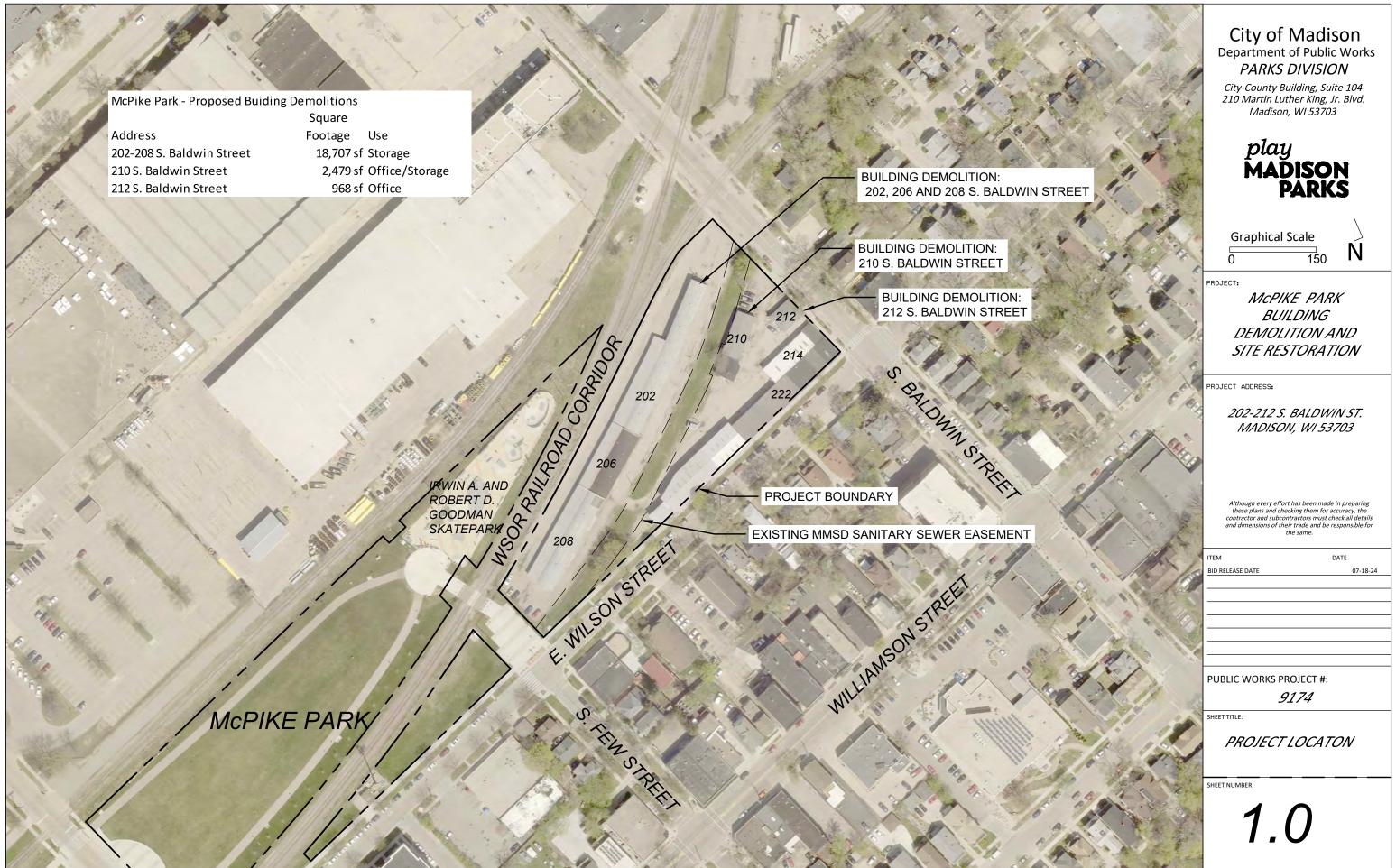
PUBLIC WORKS PROJECT #:

9174

SHEET TITLE:

COVER SHEET

SHEET NUMBER:













202-208 S. BALDWIN ST. EXTERIOR

City of Madison Department of Public Works PARKS DIVISION

City-County Building, Suite 104 210 Martin Luther King, Jr. Blvd. Madison, WI 53703



PROJECT:

MCPIKE PARK
BUILDING
DEMOLITION AND
SITE RESTORATION

PROJECT ADDRESS:

202-212 S. BALDWIN ST. MADISON, WI 53703

Although every effort has been made in preparing these plans and checking them for accuracy, the contractor and subcontractors must check all details and dimensions of their trade and be responsible for the same.

ITEM	DATE
BID RELEASE DATE	07-18-24

PUBLIC WORKS PROJECT #:

9174

SHEET TITLE:

BUILDING PHOTOS

SHEET NUMBER:











202-208 S. BALDWIN ST. INTERIOR

City of Madison Department of Public Works PARKS DIVISION

City-County Building, Suite 104 210 Martin Luther King, Jr. Blvd. Madison, WI 53703



PROJECT:

MCPIKE PARK
BUILDING
DEMOLITION AND
SITE RESTORATION

PROJECT ADDRESS:

202-212 S. BALDWIN ST. MADISON, WI 53703

Although every effort has been made in preparing these plans and checking them for accuracy, the contractor and subcontractors must check all details and dimensions of their trade and be responsible for the same.

ITEM	DATE
BID RELEASE DATE	07-18-24

PUBLIC WORKS PROJECT #:

9174

SHEET TITLE:

BUILDING PHOTOS

SHEET NUMBER:











210 S. BALDWIN ST. EXTERIOR

City of Madison Department of Public Works PARKS DIVISION

City-County Building, Suite 104 210 Martin Luther King, Jr. Blvd. Madison, WI 53703



PROJECT

MCPIKE PARK
BUILDING
DEMOLITION AND
SITE RESTORATION

PROJECT ADDRESS:

202-212 S. BALDWIN ST. MADISON, WI 53703

Although every effort has been made in preparing these plans and checking them for accuracy, the contractor and subcontractors must check all details and dimensions of their trade and be responsible for the same.

ITEM	DATE
BID RELEASE DATE	07-18-24

PUBLIC WORKS PROJECT #:

9174

SHEET TITLE:

BUILDING PHOTOS

SHEET NUMBER:











210 S. BALDWIN ST. INTERIOR

City of Madison Department of Public Works PARKS DIVISION

City-County Building, Suite 104 210 Martin Luther King, Jr. Blvd. Madison, WI 53703



PROJECT:

MCPIKE PARK
BUILDING
DEMOLITION AND
SITE RESTORATION

PROJECT ADDRESS:

202-212 S. BALDWIN ST. MADISON, WI 53703

Although every effort has been made in preparing these plans and checking them for accuracy, the contractor and subcontractors must check all details and dimensions of their trade and be responsible for the same.

07-18	3-24

PUBLIC WORKS PROJECT #:

9174

SHEET TITLE:

BUILDING PHOTOS

SHEET NUMBER:











212 S. BALDWIN ST. EXTERIOR

City of Madison Department of Public Works PARKS DIVISION

City-County Building, Suite 104 210 Martin Luther King, Jr. Blvd. Madison, WI 53703



PROJECT:

MCPIKE PARK
BUILDING
DEMOLITION AND
SITE RESTORATION

PROJECT ADDRESS:

202-212 S. BALDWIN ST. MADISON, WI 53703

Although every effort has been made in preparing these plans and checking them for accuracy, the contractor and subcontractors must check all details and dimensions of their trade and be responsible for the same.

ITEM	DATE
BID RELEASE DATE	07-18-24

PUBLIC WORKS PROJECT #:

9174

SHEET TITLE:

BUILDING PHOTOS

SHEET NUMBER:











212 S. BALDWIN ST. INTERIOR

City of Madison Department of Public Works PARKS DIVISION

City-County Building, Suite 104 210 Martin Luther King, Jr. Blvd. Madison, WI 53703



PROJECT:

MCPIKE PARK
BUILDING
DEMOLITION AND
SITE RESTORATION

PROJECT ADDRESS:

202-212 S. BALDWIN ST. MADISON, WI 53703

Although every effort has been made in preparing these plans and checking them for accuracy, the contractor and subcontractors must check all details and dimensions of their trade and be responsible for the same.

DATE
07-18-24

PUBLIC WORKS PROJECT #:

9174

SHEET TITLE:

BUILDING PHOTOS

SHEET NUMBER:



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84001

File ID: 84001 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: BOARD OF

PUBLIC WORKS

File Created Date: 06/18/2024

File Name: Awarding Public Works Contract No. 8635, Knutson Final Action:

Drive and Green Avenue Assessment District - 2024.

Title: Awarding Public Works Contract No. 8635, Knutson Drive and Green Avenue

Assessment District - 2024. (District 18)

Notes: Andy Zwieg

Sponsors: BOARD OF PUBLIC WORKS Effective Date:

Attachments: 9635 BidOpeningTab.pdf, 8635 KNUTSON AWARD Enactment Number:

revised.pdf

Author: Jim Wolfe, City Engineer Hearing Date:

Entered by: hfleegel@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Division Action Text: This Resolution	06/20/2024	Refer	BOARD OF PUBLIC WORKS	06/26/2024	06/26/2024	
	Action Text. This Resol	iution was Rei	er to the BOARD OF Pt	DELIC WORKS due ba	CK 011 6/26/2024		
1	BOARD OF PUBLIC WORKS	06/26/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass
		•	Ald. Guequierre, seconde DFFICER. The motion pa	•		JNCIL TO	

Text of Legislative File 84001

Fiscal Note

The proposed resolution authorizes awarding the contract for the Knutson Drive and Green Avenue Assessment District - 2024 at a total estimated cost of \$1,812,780.55, including contingency. Funding for the project is available in Munis projects #13812 (\$1,437,497.64), #14813 (\$359,921.29), and #14918 (\$15,361.62). No additional appropriation is required.

Title

Awarding Public Works Contract No. 8635, Knutson Drive and Green Avenue Assessment District - 2024. (District 18)

Body

BE IT RESOLVED, that the following low bids for miscellaneous improvements be accepted

and that the Mayor and City Clerk be and are hereby authorized and directed to enter into a contract with the low bidder contained herein, subject to the Contractor's compliance with Section 39.02 of the Madison General Ordinances concerning compliance with the Affirmative Action provisions and subject to the Contractor's compliance with Section 33.07 of the Madison General Ordinances regarding Best Value Contracting:

BE IT FURTHER RESOLVED, that the funds be encumbered to cover the cost of the projects contained herein.

See attached document (Contract No. 8635) for itemization of bids.

PAGE

1

KNUTSON DRIVE AND GREEN AVENUE ASSESSMENT DISTRICT - 2024

CONTRACT NO. 8635 DATE: 6/13/24

PREQUALIFICATION: 275, 310

PREQUALIFICATION

TOTAL BID	STATUS
\$1,678,500.50	OK
\$1,765,681.59	ОК
\$2,068,000.00	OK
\$2,243,514.50	
	\$1,678,500.50 \$1,765,681.59 \$2,068,000.00

CONTRACT NO. 8635 KNUTSON DRIVE AND GREEN AVENUE ASSESSMENT DISTRICT - 2024 Parisi Construction, LLC

CONTRACT AWARD	\$ 1,678,500.50
MAX CONTINGENCY (8%)	\$ 134,280.05
13812-402-170: 54410 (91350)	967,048.49
ACCT. CONTINGENCY 8%	77,363.88_
ACCT. SUBTOTAL	1,044,412.37
14813-402-200: 54420 (91366)	333,260.45
ACCT. CONTINGENCY 8%	26,660.84
ACCT. SUBTOTAL	359,921.29
13812-402-174: 54445 (91345)	115,926.11
ACCT. CONTINGENCY 8%	9,274.09
ACCT. SUBTOTAL	125,200.20
13812-84-174: 54445 (91345)	248,041.73
ACCT. CONTINGENCY 8%	19,843.34
ACCT. SUBTOTAL	267,885.07
14918-84-174-84500: 54445 (91345)	14,223.72
ACCT. CONTINGENCY 8%	1,137.90
ACCT. SUBTOTAL	15,361.62
GRAND TOTAL INCL. CONTINGENCY	\$ 1,812,780.55



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84002

File ID: 84002 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: BOARD OF

PUBLIC WORKS

File Created Date: 06/18/2024

File Name: Awarding Public Works Contract No. 9522, 20 is Final Action:

Plenty.

Title: Awarding Public Works Contract No. 9522, 20 is Plenty. (Citywide)

Notes: Tom Mohr

Sponsors: BOARD OF PUBLIC WORKS Effective Date:

Attachments: 9522BidOpeningTab.pdf, 9522 20 IS PLENTY Enactment Number:

AWARD.pdf

Author: Jim Wolfe, City Engineer Hearing Date:

Entered by: hfleegel@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Division	06/20/2024		BOARD OF PUBLIC WORKS	06/26/2024	06/26/2024	
	Action Text: This Resol	ution was Ref	er to the BOARD OF PU	JBLIC WORKS due ba	ck on 6/26/2024		
1	BOARD OF PUBLIC WORKS	06/26/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass
	Action Text: A motion was made by Ald. Guequierre, seconded by Kliems, to RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER. The motion passed by voice vote/other.						

Text of Legislative File 84002

Fiscal Note

Funding is available within the Twenty is Plenty capital project approved in Traffic Engineering's 2022 adopted capital budget. No additional appropriation required.

Title

Awarding Public Works Contract No. 9522, 20 is Plenty. (Citywide)

Body

BE IT RESOLVED, that the following low bids for miscellaneous improvements be accepted and that the Mayor and City Clerk be and are hereby authorized and directed to enter into a contract with the low bidder contained herein, subject to the Contractor's compliance with Section 39.02 of the Madison General Ordinances concerning compliance with the Affirmative

Action provisions and subject to the Contractor's compliance with Section 33.07 of the Madison General Ordinances regarding Best Value Contracting:

BE IT FURTHER RESOLVED, that the funds be encumbered to cover the cost of the projects contained herein.

See attached document (Contract No. 9522) for itemization of bids.

PAGE

2

20 IS PLENTY

CONTRACT NO. 9522 DATE: 6/13/24 PREQUALIFICATION: 325

PREQUALIFICATION

TOTAL BID	STATUS
\$474,495.00	ОК
\$478,433.54	
	\$474,495.00

CONTRACT NO. 9522 20 IS PLENTY Mega Rentals, LLC

CONTRACT AWARD MAX CONTINGENCY (8%)	\$ \$	474,495.00 37,959.60
13573-45-200: 54410 (96880) ACCT. CONTINGENCY 8% ACCT. SUBTOTAL		474,495.00 37,959.60 512,454.60
GRAND TOTAL INCL. CONTINGENCY	\$	512,454.60



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84003

File ID: 84003 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: BOARD OF

PUBLIC WORKS

File Created Date: 06/18/2024

Final Action:

File Name: Awarding Public Works Contract No. 9527, Madison

Water Utility Olin Ave. Administrative Building Roof

Replacement.

Title: Awarding Public Works Contract No. 9527, Madison Water Utility Olin Ave.

Administrative Building Roof Replacement. (District 14)

Notes: Jeff Belshaw

Sponsors: BOARD OF PUBLIC WORKS Effective Date:

Attachments: 9527 BidOpeningTab.pdf Enactment Number:

Author: Jim Wolfe, City Engineer Hearing Date:

Entered by: hfleegel@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Division Action Text: This Resol	06/20/2024 ution was Ref	Refer	BOARD OF PUBLIC WORKS JBLIC WORKS due ba	06/26/2024 ck on 6/26/2024	06/26/2024	
1	BOARD OF PUBLIC WORKS	06/26/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass
		•	Ald. Guequierre, seconde DFFICER. The motion p	•		JNCIL TO	

Text of Legislative File 84003

Fiscal Note

Budget authority is available in the Acct. Nos. listed on the attached.

Title

Awarding Public Works Contract No. 9527, Madison Water Utility Olin Ave. Administrative Building Roof Replacement. (District 14)

Body

BE IT RESOLVED, that the following low bids for miscellaneous improvements be accepted and that the Mayor and City Clerk be and are hereby authorized and directed to enter into a contract with the low bidder contained herein, subject to the Contractor's compliance with

Section 39.02 of the Madison General Ordinances concerning compliance with the Affirmative Action provisions and subject to the Contractor's compliance with Section 33.07 of the Madison General Ordinances regarding Best Value Contracting:

BE IT FURTHER RESOLVED, that the funds be encumbered to cover the cost of the projects contained herein.

See attached document (Contract No. 9527) for itemization of bids.

MADISON WATER UTILITY OLIN AVE. ADMINISTRATIVE BUILDING ROOF REPLACEMENT

CONTRACT NO. 9527 DATE: 6/13/24

PREQUALIFICATION: 420, 460

PREQUALIFICATION

CONTRACTORS	TOTAL BID	STATUS
Engineering Estimate	\$440,100.00	
NO BIDDERS		



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84029

File ID: 84029 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: BOARD OF

PUBLIC WORKS

File Created Date: 06/20/2024

File Name: Approving plans and specifications and authorizing Final Action:

the Board of Public Works to advertise and receive

bids for West Towne Path - Phase 2a.

Title: Approving plans and specifications and authorizing the Board of Public Works to

advertise and receive bids for West Towne Path - Phase 2a. (District 9)

Notes: Aaron Canton

Sponsors: BOARD OF PUBLIC WORKS Effective Date:

Attachments: 10169-WestTownePathPh2a BPWExhibit.pdf Enactment Number:

Author: Jim Wolfe, City Engineer Hearing Date:

Entered by: hfleegel@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Division	06/20/2024	Refer	BOARD OF PUBLIC WORKS	06/26/2024	06/26/2024	
	Action Text: This Reso	lution was Ref	er to the BOARD OF	PUBLIC WORKS due ba	ick on 6/26/2024		
1	BOARD OF PUBLIC WORKS	06/26/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass
	Action Text: A motion	vas mada by /		ded by Kliems, to RECC	MMEND TO COL	INCIL TO	
		•	•	passed by voice vote/otl		JINGIL TO	

Text of Legislative File 84029

Fiscal Note

The proposed resolution approves plans and specifications and authorizes the Board of Public Works to advertise and receive bids for the West Towne Path - Phase 2a. The total estimated cost of the project is \$943,000, comprised of \$565,800 in federal funding and \$377,200 in City costs. Funding for the City portion of costs is available in Munis project #10169. No additional appropriation is required.

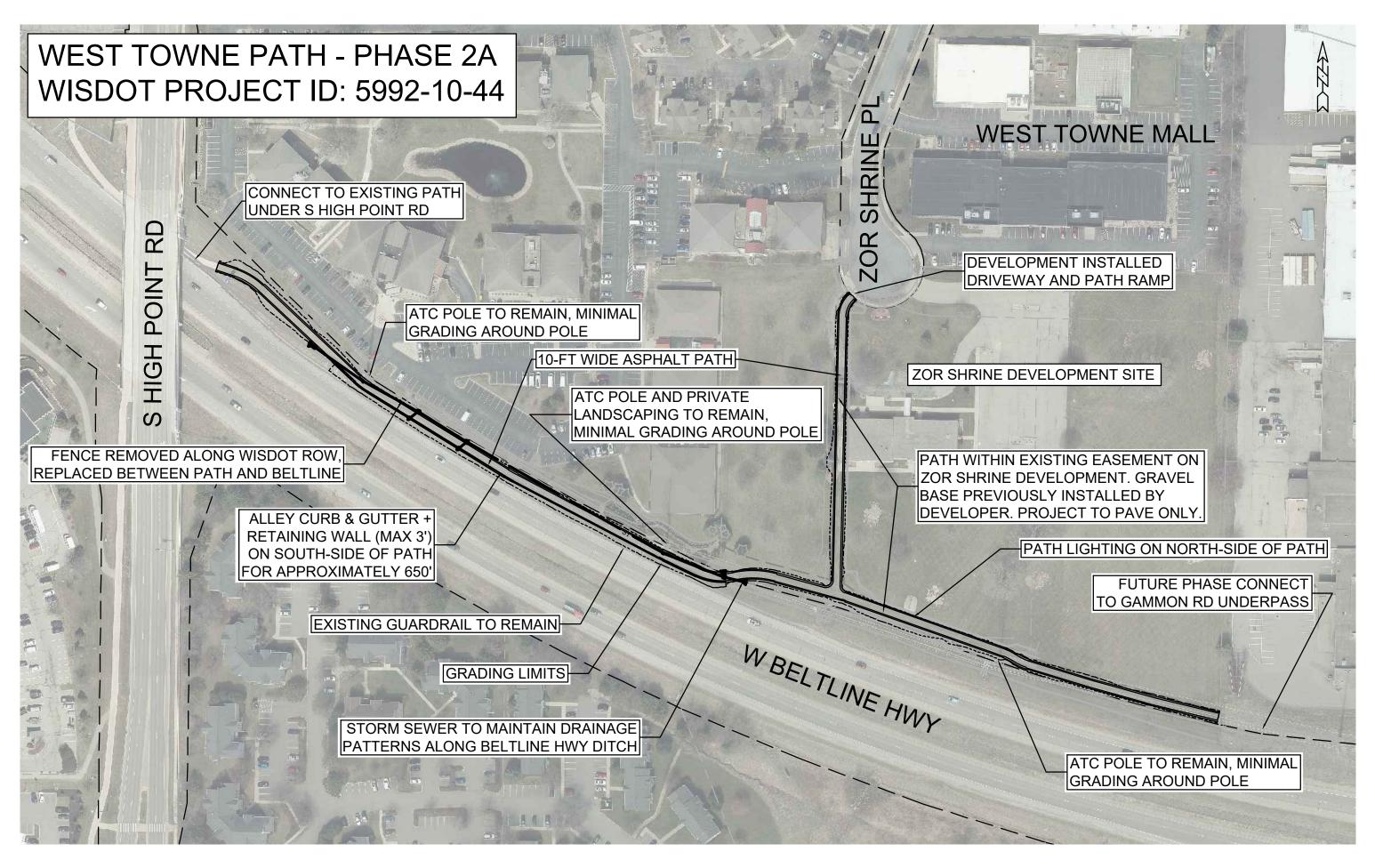
Title

Approving plans and specifications and authorizing the Board of Public Works to advertise and receive bids for West Towne Path - Phase 2a. (District 9)

Body

BE IT RESOLVED,

- 1) That the plans and specifications for West Towne Path Phase 2a, be and are hereby approved.
- 2) That the Board of Public Works be and is hereby authorized to advertise and receive bids for said project.
- 3) That the Mayor and City Clerk are hereby authorized to sign and grant easements or right-of-way release or procurement documents, maintenance agreements or encroachment agreements, as necessary and grant or accept dedication of lands and/or easements from/to the Developer/Owner for public improvements located outside of existing public fee title or easement right-of-ways.





City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84245

File ID: 84245 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: BOARD OF

PUBLIC WORKS

File Created Date: 07/01/2024

File Name: Awarding Public Works Contract No. 8744, Atticus Final Action:

Way Assessment District

Title: Awarding Public Works Contract No. 8744, Atticus Way Assessment District

(District 10)

Notes: Grecia Izquierdo

Sponsors: BOARD OF PUBLIC WORKS Effective Date:

Attachments: 8744 BidOpeningTab Jun272024.pdf, 8744 Enactment Number:

award.pdf

Author: Hearing Date:

Entered by: jjohnson@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Division Action Text: This Reso			BOARD OF PUBLIC WORKS PUBLIC WORKS due ba	07/10/2024 ack on 7/10/2024	07/10/2024	
1	BOARD OF PUBLIC WORKS	07/10/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass
	Action Text: A motion was made by Clausius, seconded by Williams, to RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER. The motion passed by voice vote/other.						

Text of Legislative File 84245

Fiscal Note

Fiscal Note Pending.

Title

Awarding Public Works Contract No. 8744, Atticus Way Assessment District (District 10)

Body

BE IT RESOLVED, that the following low bids for miscellaneous improvements be accepted and that the Mayor and City Clerk be and are hereby authorized and directed to enter into a contract with the low bidder contained herein, subject to the Contractor's compliance with Section 39.02 of the Madison General Ordinances concerning compliance with the Affirmative Action provisions and subject to the Contractor's compliance with Section 33.07 of the

Madison General Ordinances regarding Best Value Contracting:

BE IT FURTHER RESOLVED, that the funds be encumbered to cover the cost of the projects contained herein.

See attached document (Contract No. 8744) for itemization of bids.

CITY OF MADISON BOARD OF PUBLIC WORKS BID OPENING

PAGE

1

ATTICUS WAY ASSESSMENT DISTRICT

CONTRACT NO. 8744

DATE: 6/27/24

PREQUALIFICATION: 275, 310

PREQUALIFICATION

CONTRACTORS	TOTAL BID	STATUS
Speedway Sand & Gravel, Inc.	\$226,597.55	OK
S&L Underground, Inc.	\$240,709.11	ок
Parisi Construction, LLC	\$248,842.00	ОК
Engineering Estimate	\$278,370.00	
R. G. Huston Co., Inc.	\$328,550.00	OK

CONTRACT NO. 8744 Atticus Way Assessment District - 2024 Speedway Sand & Gravel, Inc.

CONTRACT AWARD	\$ 226,597.55
MAX CONTINGENCY (8%)	\$ 18,127.80
44700 400 470 54440 (04050)	100 000 01
14763-402-170: 54410 (91350)	182,902.91
ACCT. CONTINGENCY 8%	 14,632.23
ACCT. SUBTOTAL	197,535.14
44700 400 474 54445 (04045)	04.400.00
14763-402-174: 54445 (91345)	24,186.30
ACCT. CONTINGENCY 8%	 1,934.90
ACCT. SUBTOTAL	26,121.20
44702 400 477, 54420 (04020)	40 540 00
14763-402-177: 54430 (91232)	13,512.00
ACCT. CONTINGENCY 8%	 1,080.96
ACCT. SUBTOTAL	14,592.96
44700 04 474 54445 (04045)	5 000 04
14763-84-174: 54445 (91345)	5,996.34
ACCT. CONTINGENCY 8%	 479.71
ACCT. SUBTOTAL	6,476.05
GRAND TOTAL INCL. CONTINGENCY	\$ 244,725.35



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84248

File ID: 84248 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: BOARD OF

PUBLIC WORKS

File Created Date: 07/01/2024

File Name: Awarding Public Works Contract No. 9521, 2024 Final Action:

Playground Improvements

Title: Awarding Public Works Contract No. 9521, 2024 Playground Improvements

(District 7, District 10, District 11, and District 20).

Notes: Kate Kane

Sponsors: BOARD OF PUBLIC WORKS Effective Date:

Attachments: 9521 BidOpeningTab Jun272024.pdf, 9521 Enactment Number:

BidBreakdown.pdf

Author: Hearing Date:

Entered by: jjohnson@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Division	07/01/2024		BOARD OF PUBLIC WORKS	07/10/2024	07/10/2024	
	Action Text: This Reso	lution was Ref	er to the BOARD OF P	JBLIC WORKS due ba	ck on 7/10/2024		
1	BOARD OF PUBLIC WORKS	07/10/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass
	Action Text: A motion was made by Clausius, seconded by Williams, to RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER. The motion passed by voice vote/other.						

Text of Legislative File 84248

Fiscal Note

Fiscal Note Pending.

Title

Awarding Public Works Contract No. 9521, 2024 Playground Improvements (District 7, District 10, District 11, and District 20).

Body

BE IT RESOLVED, that the following low bids for miscellaneous improvements be accepted and that the Mayor and City Clerk be and are hereby authorized and directed to enter into a contract with the low bidder contained herein, subject to the Contractor's compliance with Section 39.02 of the Madison General Ordinances concerning compliance with the Affirmative

Action provisions and subject to the Contractor's compliance with Section 33.07 of the Madison General Ordinances regarding Best Value Contracting:

BE IT FURTHER RESOLVED, that the funds be encumbered to cover the cost of the projects contained herein.

See attached document (Contract No. 9521) for itemization of bids.

4

2024 PLAYGROUND IMPROVEMENTS

CONTRACT NO. 9521 DATE: 6/27/24

PREQUALIFICATION: 240, 262

		PROPOSAL			_PREQUAL	
CONTRACTORS	TOTAL BID	1	2	3	4	STATUS
Madison Commercial Landscapes Inc.	\$257,694.70	\$91,848.00	\$43,839.50	\$61,847.50	\$60,159.70	ОК
Engineering Estimate	\$280,496.00					
Joe Daniels Construction Co., Inc.	\$287,571.20	\$103,403.00	\$46,730.50	\$69,202.75	\$68,234.95	OK

If GC prequalified in 240, sub must be prequalified in 262 - see contract for specifics/timing

CONTRACT NO. 9521 2024 PLAYGROUND IMPROVEMENTS

MADISON COMMERCIAL LANDSCAPES, INC.

Acct. No. 15149-51-130: 54250(91265) Contingency 8% <u>+</u> Sub-Total	\$ 91,848.00
Acct. No. 15109-51-130: 54250(91265)	\$ 43,839.50
Contingency 8% <u>+</u>	<u>3,510.50</u>
Sub-Total	\$ 47,350.00
Acct. No. 15137-51-130: 54250 (91265)	\$ 61,847.50
Contingency 8% <u>+</u>	4,952.50
Sub-Total	\$ 66,800.00
Acct. No. 15053-51-130: 54250 (91265)	\$ 60,159.70
Contingency 8% <u>+</u>	4,810.30
Sub-Total	\$ 64,970.00



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84258

File ID: 84258 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: BOARD OF

PUBLIC WORKS

File Created Date: 07/01/2024

Final Action:

File Name: Approving plans and specifications and authorizing

Change Order 1 to Contract 8717, Mineral Point Road Widen Sidewalk (for East-West Bus Rapid Transit), to Parisi Construction LLC, for \$86,000.

Title: Approving plans and specifications and authorizing Change Order 1 to Contract

8717, Mineral Point Road Widen Sidewalk (for East-West Bus Rapid Transit), to

Parisi Construction LLC, for \$86,000. (District 19)

Notes: Mike Cechvala

Sponsors: BOARD OF PUBLIC WORKS Effective Date:

Attachments: Nautilus Point Park 8717 Added Specifications and Enactment Number:

Changes.pdf, 8717 Revision Plan Supplement.pdf, CO 1 - 8717 REV20240708.pdf, Nautilus Park CO1

BPW Presentation.pdf

Author: Hearing Date:

Entered by: jjohnson@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Division Action Text: This Reso	07/01/2024 olution was Ref		BOARD OF PUBLIC WORKS PUBLIC WORKS due ba	07/10/2024 ack on 7/10/2024	07/10/2024	
1	BOARD OF PUBLIC WORKS	07/10/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass
	Action Text: A motion was made by Conklin, seconded by Williams, to RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER. The motion passed by voice vote/other.						

Text of Legislative File 84258

Fiscal Note

Fiscal Note Pending.

Title

Approving plans and specifications and authorizing Change Order 1 to Contract 8717, Mineral Point Road Widen Sidewalk (for East-West Bus Rapid Transit), to Parisi Construction LLC, for

\$86,000. (District 19)

Body

WHEREAS, Legistar 70618 approved roadway geometry for the East-West Bus Rapid Transit project, including a widened sidewalk along Mineral Point Road between Tree Lane and Whitney Way to replace the bicycle facility along Mineral Point Road that previously consisted of a shared bus and bike lane; and

WHEREAS, Legistar 80605 approved geometry, plans, and specifications for the Mineral Point Road Widened Sidewalk with a gap along Nautilus Point Park between Nautilus Drive Island Drive with the intent of completing that section while preserving the existing trees; and

WHEREAS, City of Madison Parks Division has recommended the alignment that preserves the Honey Locust trees, Legistar 83231; and

WHEREAS, This work is a minor addition to Contract 8717, awarded to Parisi Construction LLC, Legistar 82098.

NOW, **THEREFORE**, **BE IT RESOLVED**, that supplemental plans and specifications for Mineral Point Road Widen Sidewalk, be and are hereby approved; and

That the Board of Public Works be and is hereby authorized to award the work to Parisi Construction LLC as Change Order 1 to Contract 8717.

BE IT FURTHER RESOLVED, Change Order No. 1 to Contract 8717, East-West Bus Rapid Transit Widened Sidewalk, to Parisi Construction, LLC., for \$86,000 and Approving Plans and Specifications for Nautilus Point Park Widened Sidewalk (District 19)



Department of Public Works

Engineering Division

Andrew Zwieg, Principal Engineer

City-County Building, Room 115 210 Martin Luther King, Jr. Boulevard Madison, Wisconsin 53703 Phone: (608) 266-4751 Fax: (608) 264-9275

engineering@cityofmadison.com www.cityofmadison.com/engineering

June 28, 2024

NOTICE OF PLAN REVISION PLAN REVISION NO. 3 CONTRACT NO. 8717

MINERAL POINT ROAD WIDEN SIDEWALK (FOR EAST-WEST BUS RAPID TRANSIT)

Revise and amend the contract document(s) for the above project as stated in this plan revision, otherwise, the original document shall remain in effect.

SPECIFICATIONS:

ADD: ARTICLE 8-2 Silt sock (8 Inch)-Complete SPV.0090.2008

A Description

This special provision describes the construction of silt sock (8 Inch)-Complete.

B Materials

Materials of silt sock (8 Inch)-Complete shall be per Article 210.2 of the City of Madison Standard Specifications. Per City of Madison Standard Specifications 210.2(a)

C Construction

Construction of silt sock (8 Inch)-Complete shall be per Article 210.1 of the City of Madison Standard Specifications. Per City of Madison Standard Specifications 210.1(e)

D Measurement

The City of Madison will measure Silt sock (8 Inch)-Complete in linear feet, acceptably completed.

E Payment

SPV.0090.2010

The City of Madison will pay for measured quantities at the change order unit price under the following bid items:

ITEM NUMBER **DESCRIPTION UNIT**

Silt sock (8 Inch)-Complete Payment shall be per Article 210.5 of the City of Madison Standard Specifications. Per City of Madison Standard Specifications 210.5(b).

LF

ADD: ARTICLE 15-8 EROSION MATTING, CLASS I, TYPE A - ORGANIC Item SPV.0180.2009

A Description

This section describes providing erosion control mats as shown in the plans.

B Materials

Erosion Matting provided shall be of the Class and Type specified, meeting the requirements and specifications of the Wisconsin Department of Transportation Erosion Control and Storm Water Product Acceptability Lists (PAL). The Class and Type requirements listed below match those of the Wisconsin Department of Transportation nomenclature. Only products listed in the PAL that are constructed with 100 percent biodegradable material will be permitted on City of Madison projects. Class I Type A shall be designated ORGANIC to ensure provision of a product with 100 percent biodegradable matting, netting, and stitching. Photodegradable is NOT equivalent to biodegradable. Products listed in the PAL as Class I Urban Type A. When biodegradable matting is required, anchoring devices also need to be 100 percent biodegradable.

C Construction

Erosion mat shall be installed in compliance with WDNR Conservation Practice Standard 1052 - Non-channel Erosion Mat, WDNR Conservation Practice Standard 1053 - Channel Erosion Mat, the Standard Plate "EROSION MAT" in the City of Madison Standard Specifications for Public Works Construction, and/or in compliance with the manufacture's specifications. Erosion matting shall be installed within three (3) days of seeding.

The Contractor shall submit the name of the specified product proposed for use as well as the manufacturer's recommended installation instructions, including but not limited to: recommended anchoring devices, (i.e. type of stakes or staples); overlap; anchor trench configuration; and anchoring pattern. The Contractor shall provide this submittal at the preconstruction meeting. Where any discrepancy exists between installation methods called out on the Standard Plate and the manufacturer's specifications, the Construction Engineer shall have the final authority to specify the installation method used.

D Measurement

The City of Madison will measure Erosion Matting by the square yard in place not including runout in anchor trenches or overlap acceptably completed.

E Payment

The City of Madison will pay for measured quantities at the change order unit price under the following bid items:

ITEM NUMBER DESCRIPTION UNIT

SPV.0180.2009 EROSION MATTING, CLASS I, TYPE A - ORGANIC SY

Payment shall be per Article 210.5 of the City of Madison Standard Specifications. Per City of Madison Standard Specifications 210.5(d).

ADD: ARTICLE 15-9 Aggressive Native Seed Mix Item SPV.0180.2010

A Description

This special provision describes seeding areas with Aggressive Native Seed Mix.

B Materials

The Contractor shall submit a photo of the aggressive seed mix to the city for approval prior to seeding.

C Construction

Native seeding shall be per Article 207 of the City of Madison Standard Specifications. Per City of Madison Standard Specifications 207.3(b), native seeding may not occur June 16 – October 14th. During this time when native seeding may not occur, a temporary annual cover crop shall be applied in accordance with City of Madison Standard Specifications Temporary Seeding 207.3. Annual rye shall not be allowed as a temporary cover crop and shall be installed under the Class I, Urban Type A Matting. Aggressive Seed Mix shall be installed over Class I, Urban Type A Matting after October 15th. Aggressive Seed Mix may be applied over existing Class I, Urban Type A Matting.

D Measurement

The City of Madison will measure Aggressive Native Seeding Mix by the square yard acceptably completed.

E Payment

The City of Madison will pay for measured quantities at the change order unit price under the following bid items:

ITEM NUMBER DESCRIPTION UNIT

SPV.0180.2010 Aggressive Native Seed Mix SY

Payment shall be per Article 207.6 of the City of Madison Standard Specifications. Per City of Madison Standard Specifications 207.6(a).

PLANS:

INSERT REVISED PLAN SHEETS AS NOTED BELOW.

SEE SUPPLEMENTAL PLANSET: 8717 PLAN REVISION 3

PROPOSAL:

Summary of changes to the bid items and quantities are summarized in the table below.

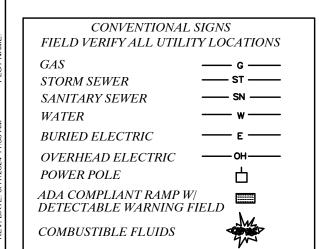
SCC Code	SCC Description	Item Number	Description	Units	Quantity Total
40.01	Demolition, Clearing, Earthwork	204.0155	Removing Concrete Sidewalk	SY	166
40.01	Demolition, Clearing, Earthwork	205.0100	Excavation Common	CY	144
40.01	Demolition, Clearing, Earthwork	208.0100	Borrow	CY	250
40.06	Pedestrian / bike access and accommodation, landscaping	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	177
40.06	Pedestrian / bike access and accommodation, landscaping	460.5223	HMA Pavement 3" 4 LT 58-28 S	TON	64
40.07	Automobile, bus, van accessways including roads, parking lot	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	81
40.06	Pedestrian / bike access and accommodation, landscaping	602.0410	Concrete Sidewalk 5-Inch	SF	130
40.06	Pedestrian / bike access and accommodation, landscaping	602.0420	Concrete Sidewalk 7-Inch	SF	0
40.06	Pedestrian / bike access and accommodation, landscaping	602.0515	Curb Ramp Detectable Warning Field Natural Patina	SF	0
40.08	Temporary Facilities and other indirect costs during construct	619.1000	Mobilization	EACH	1
40.06	Pedestrian / bike access and accommodation, landscaping	625.0500	Salvaged Topsoil	SY	1312
40.08	Temporary Facilities and other indirect costs during construct	628.1905	Mobilizations Erosion Control	EACH	1
40.08	Temporary Facilities and other indirect costs during construct	628.1910	Mobilizations Emergency Erosion Control	EACH	2
40.06	Pedestrian / bike access and accommodation, landscaping	628.6505	Soil Stabilizer Type A	ACRE	0.19
40.02	Site Utilities, Utility Relocation	628.7020	Inlet Protection Type D	EACH	4
40.08	Temporary Facilities and other indirect costs during construct	i 628.756	Tracking Pad	EACH	1
40.06	Pedestrian / bike access and accommodation, landscaping	629.0210	Fertilizer Type B	CWT	0.051
40.06	Pedestrian / bike access and accommodation, landscaping	630.0500	Seed Water	MGAL	10
40.08	Temporary Facilities and other indirect costs during construct	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	81
40.06	Pedestrian / bike access and accommodation, landscaping	650.9000	Construction Staking Curb Ramps	EACH	30
40.06	Pedestrian / bike access and accommodation, landscaping	SPV.0060.501	Tree Protection	EACH	8
40.06	Pedestrian / bike access and accommodation, landscaping	SPV.0085.501	Seeding	LB	3
40.07	Automobile, bus, van accessways including roads, parking lot	SPV.0090.022	Remove, Salvage and Reinstall Fence	LF	17
40.06	Pedestrian / bike access and accommodation, landscaping	SPV.0090.114	Pavement Marking Epoxy Lane Line 4-Inch	LF	371
40.02	Site Utilities, Utility Relocation	SPV.0090.2008	Silt sock (8 Inch) -Complete	LF	747
40.06	Pedestrian / bike access and accommodation, landscaping	SPV.0180.2009	EROSION MATTING, CLASS I, TYPE A - ORGANIC	SY	972
40.06	Pedestrian / bike access and accommodation, landscaping	SPV.0180.2010	Aggressive Native Seed Mix	SY	892

Sincerely,

Andrew Zwieg Principal Engineer

INDEX OF SHEETS

SHEET NO.	D1	NOTES AND DETAILS
SHEET NO.	EC1	EROSION CONTROL
SHEET NO.	R1-R2	REMOVALS
SHEET NO.	P1-P4	PATH PLAN & PROFILES
SHEET NO.	C1	CURB RAMP DETAILS
SHEET NO.	PM1	PAVEMENT MARKINGS
SHEET NO.	<i>X1-X7</i>	CROSS SECTIONS
SHEET NO.	MQ-1	MISCELLANEOUS
		QUANTITIES

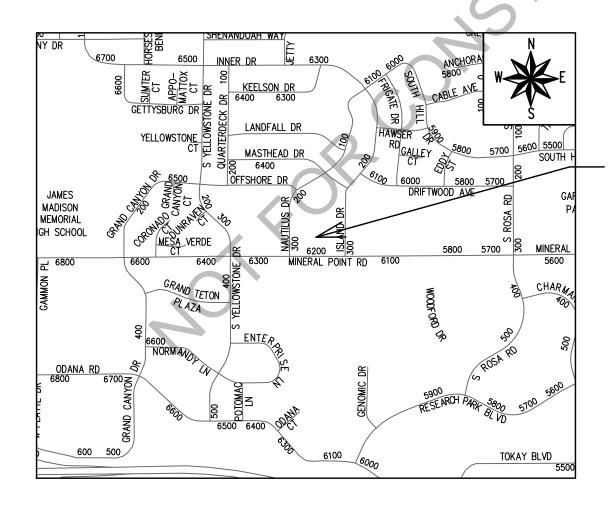


CITY OF MADISON

CITY ENGINEERING DIVISION DEPARTMENT OF PUBLIC WORKS PLAN OF PROPOSED IMPROVEMENT

NAUTILUS POINT PARK WIDENED SIDEWALK CONTRACT 8717 PLAN SUPPLEMENT

> CITY PROJECT NO. 13925 CONTRACT NO. 8717



PROJECT LOCATION PUBLIC IMPROVEMENT PROJECT
APPROVED

APPROVED DATE

BY THE COMMON COUNCIL
OF MADISON, WISCONSIN

PUBLIC IMPROVEMENT DESIGN
APPROVED BY:

City Engineer Date

DESIGNED BY:

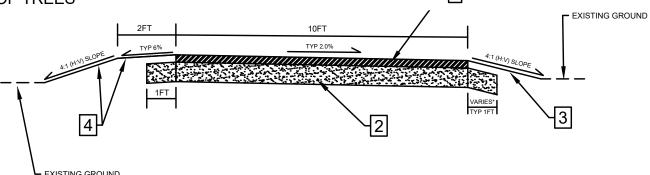
STREET

PAVEMENT MARKINGS DESIGNED BY:

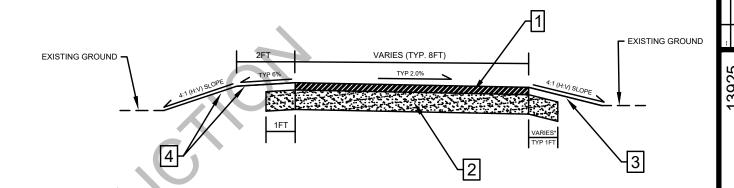
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GENERAL NOTES:

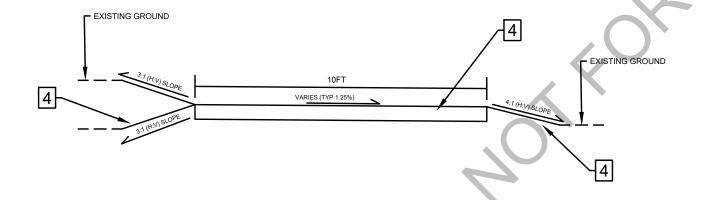
- 1) 3" HMA PAVEMENT TYPE 4 LT 58-28 S
- 2) 8" BASE AGGREGATE DENSE 1 1/4-INCH
- 3) 6" TOPSOIL, TERRACE SEED MIX, CLASS I, URBAN TYPE B ORGANIC MATTING
- 4) 6" TOPSOIL, AGGRESSIVE NATIVE SEED MIX, CLASS I, URBAN TYPE A ORGANIC MATTING
- 5) CONTRACTOR CAN NARROW 1' EXTENSION OF 8" BASE AGGREGATE DENSE 1 1/4-INCH TO 0.5' WHEN WITHIN 5' OF TREES



FINISHED TYPICAL SECTION
WIDENED SIDEWALK - MINERAL PT RD
(STA 100+10 - 103+30)



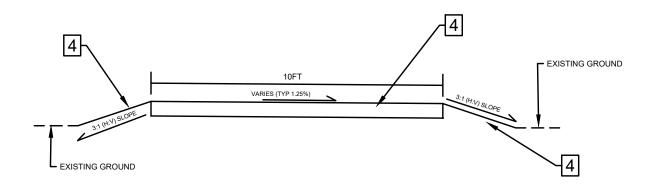
FINISHED TYPICAL SECTION
WIDENED SIDEWALK - MINERAL PT RD
(STA 103+30 - 103+80.88)



FINISHED TYPICAL SECTION

MAINTENANCE PATH WEST - MINERAL PT RD

(STA 300+00 - 301+00)



FINISHED TYPICAL SECTION
MAINTENANCE PATH EAST - MINERAL PT RD
(STA 200+00 - 201+00)

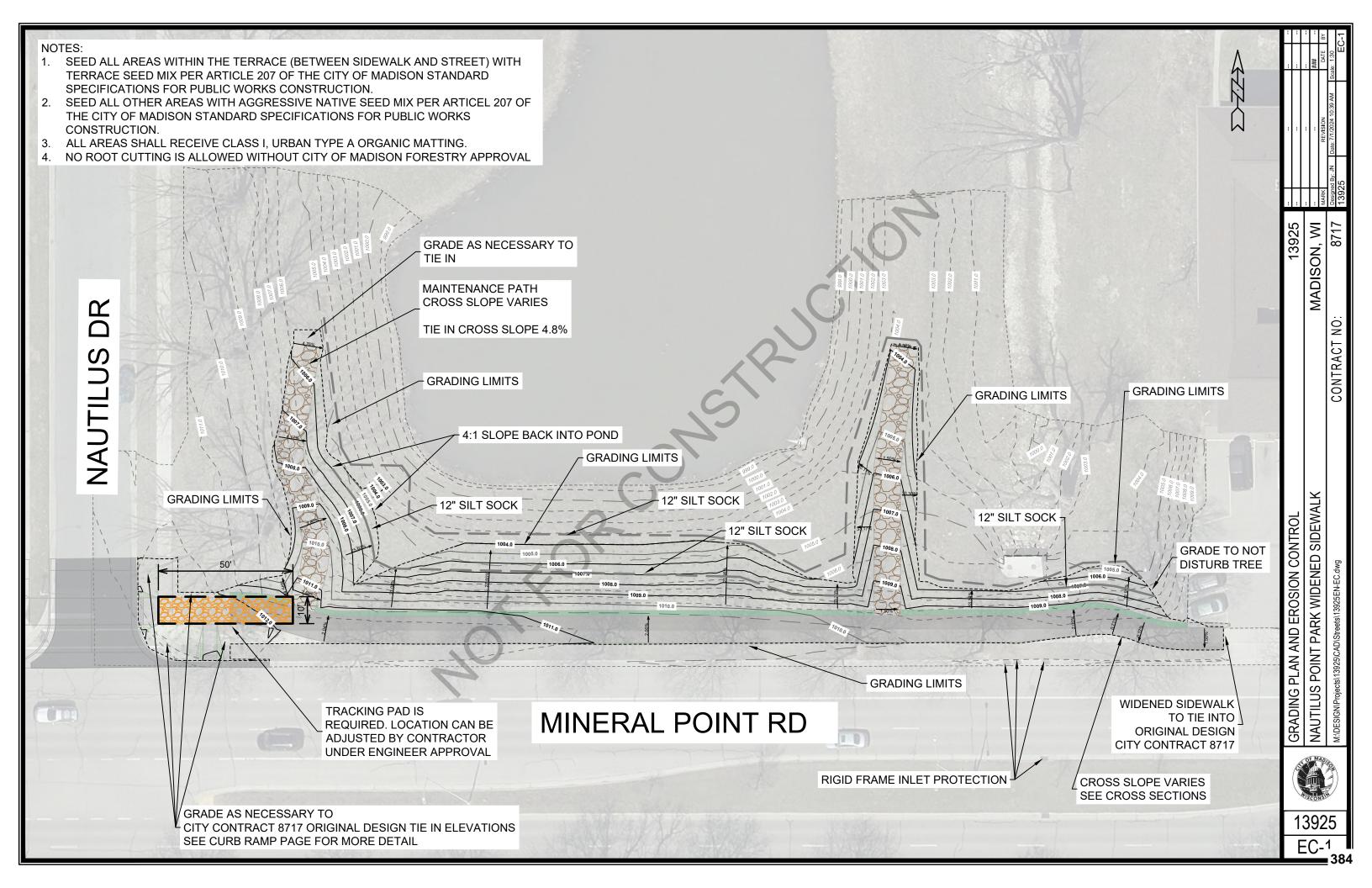


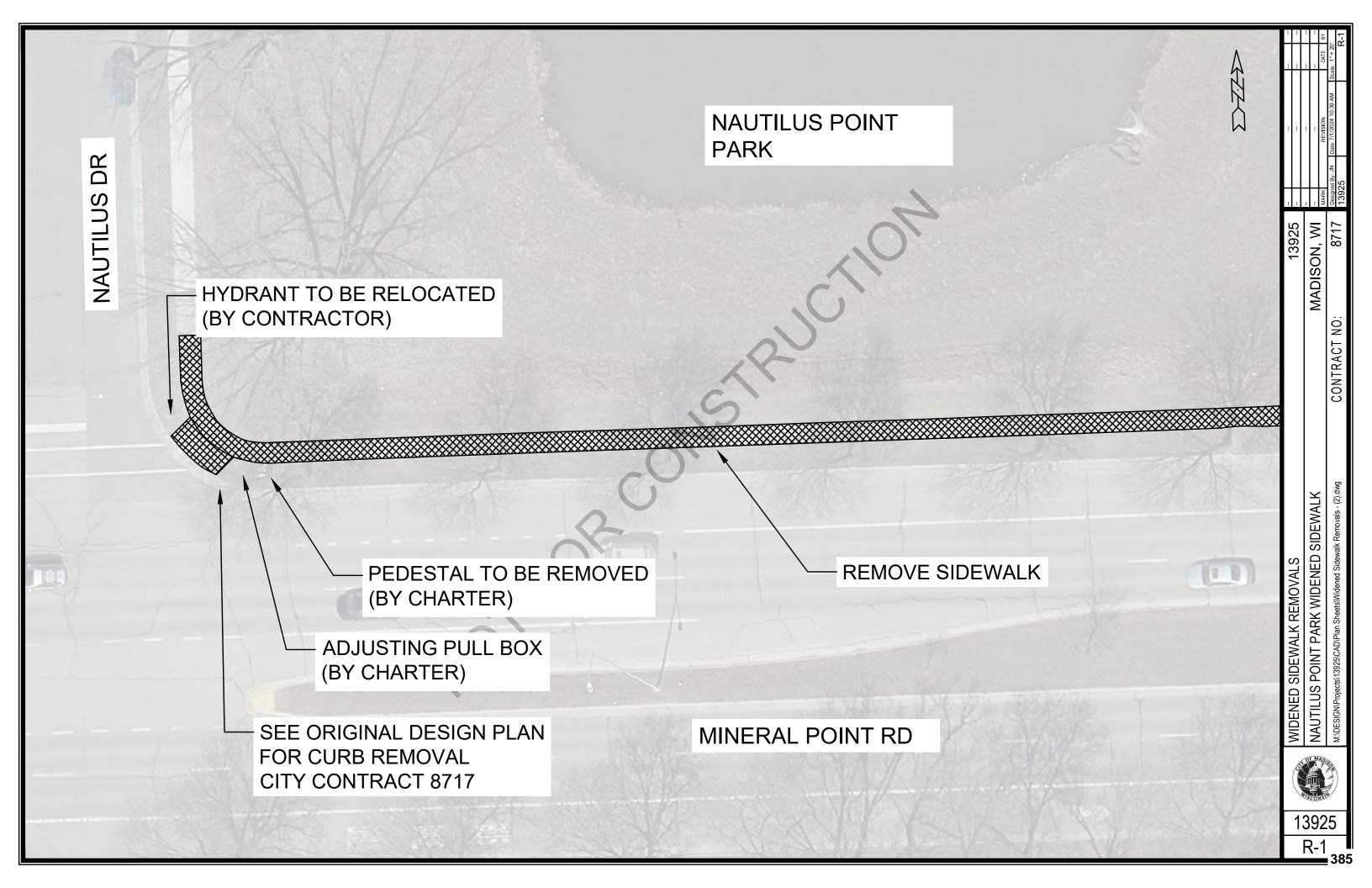
NAUTILUS POINT PARK WIDENED SIDEWALK

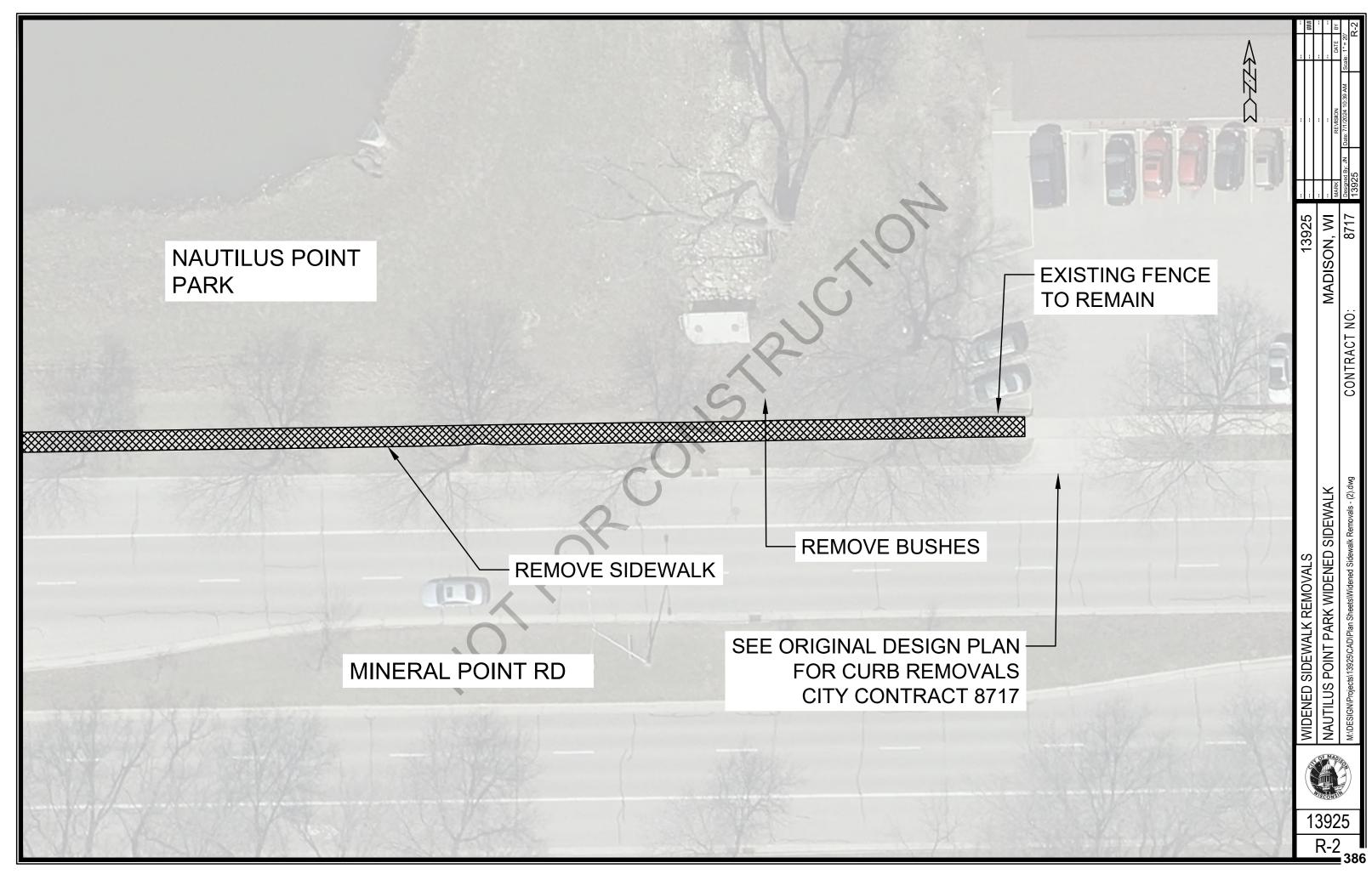
MADISON, WI

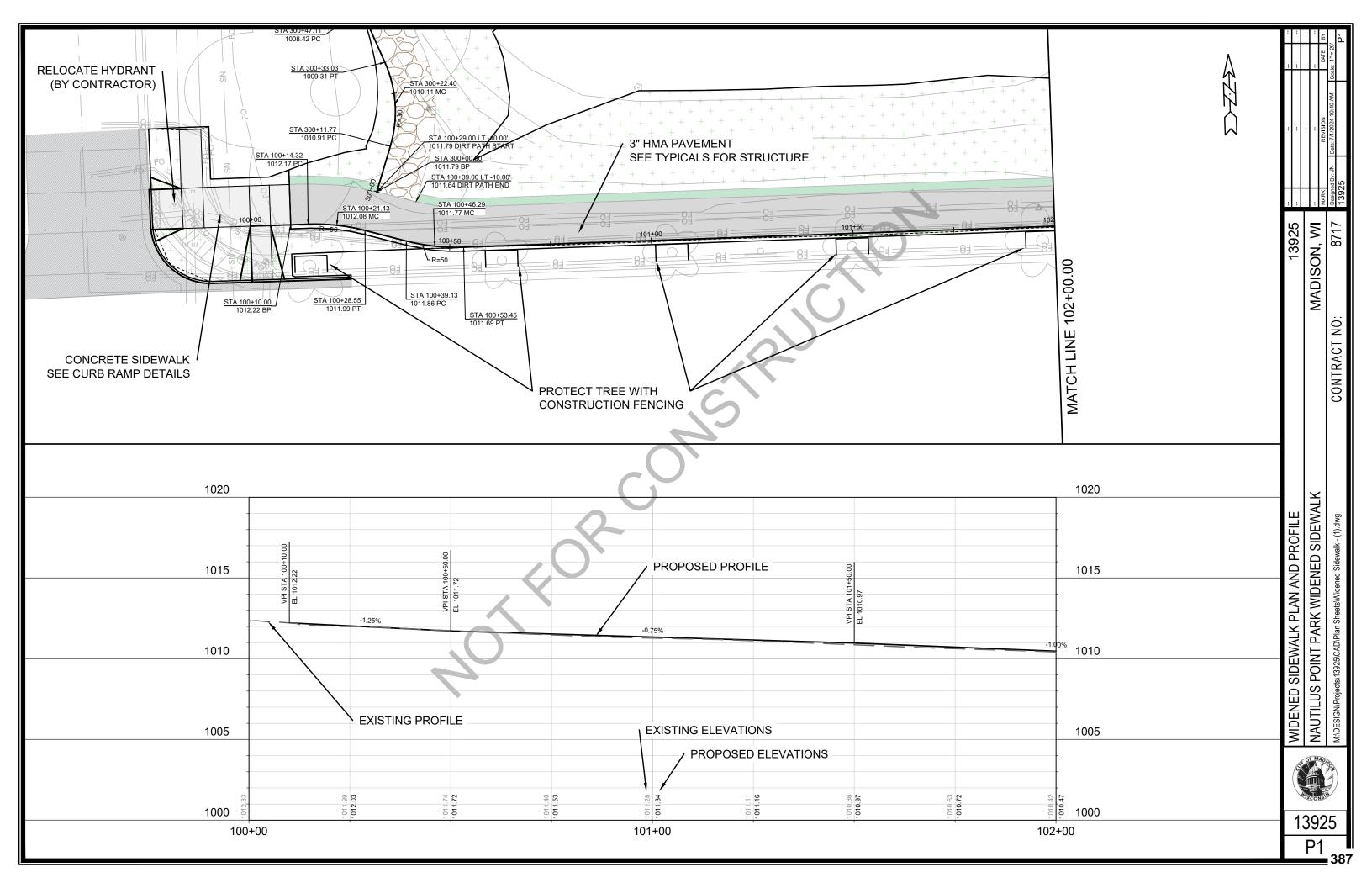
13925

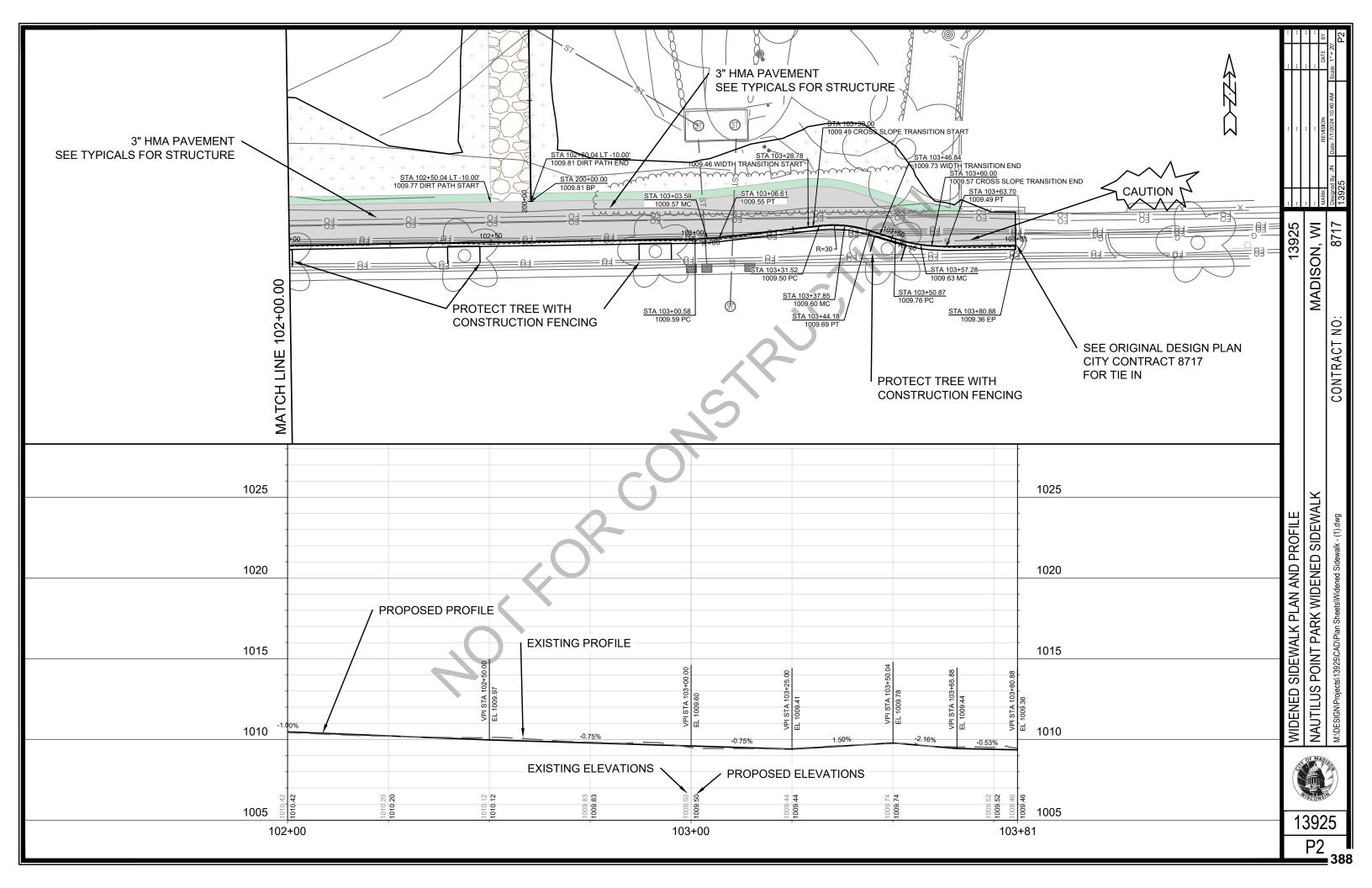
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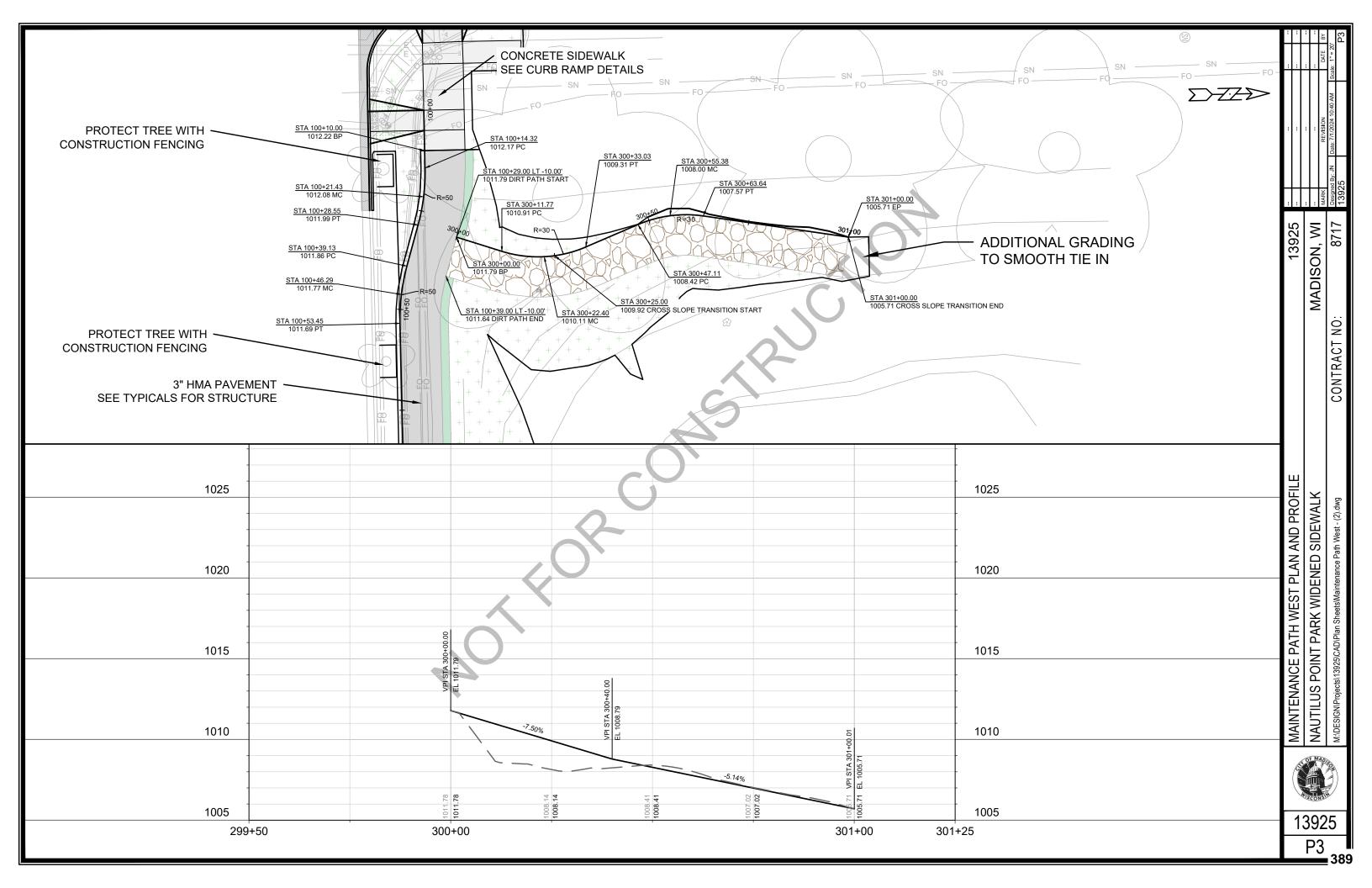


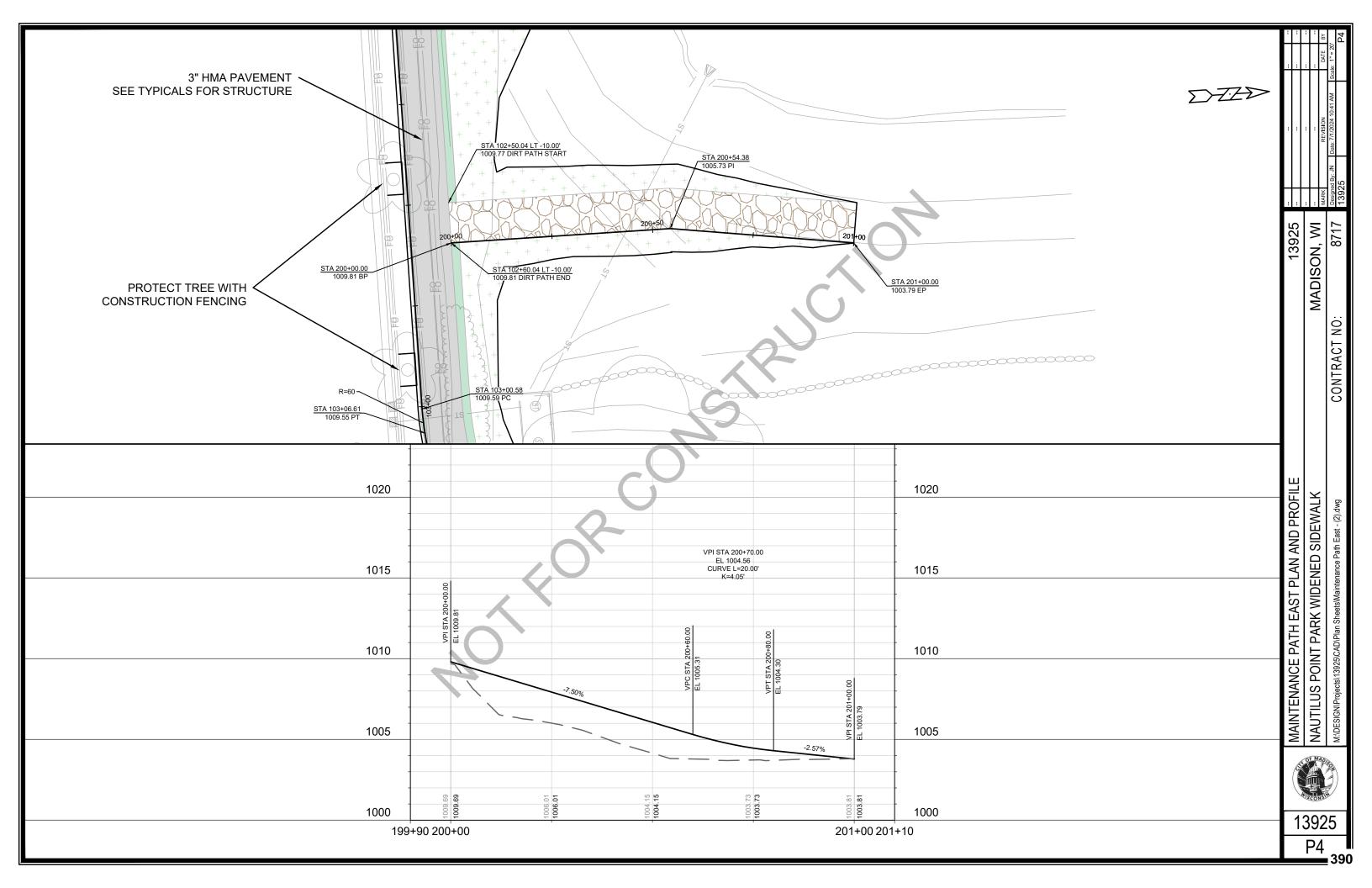


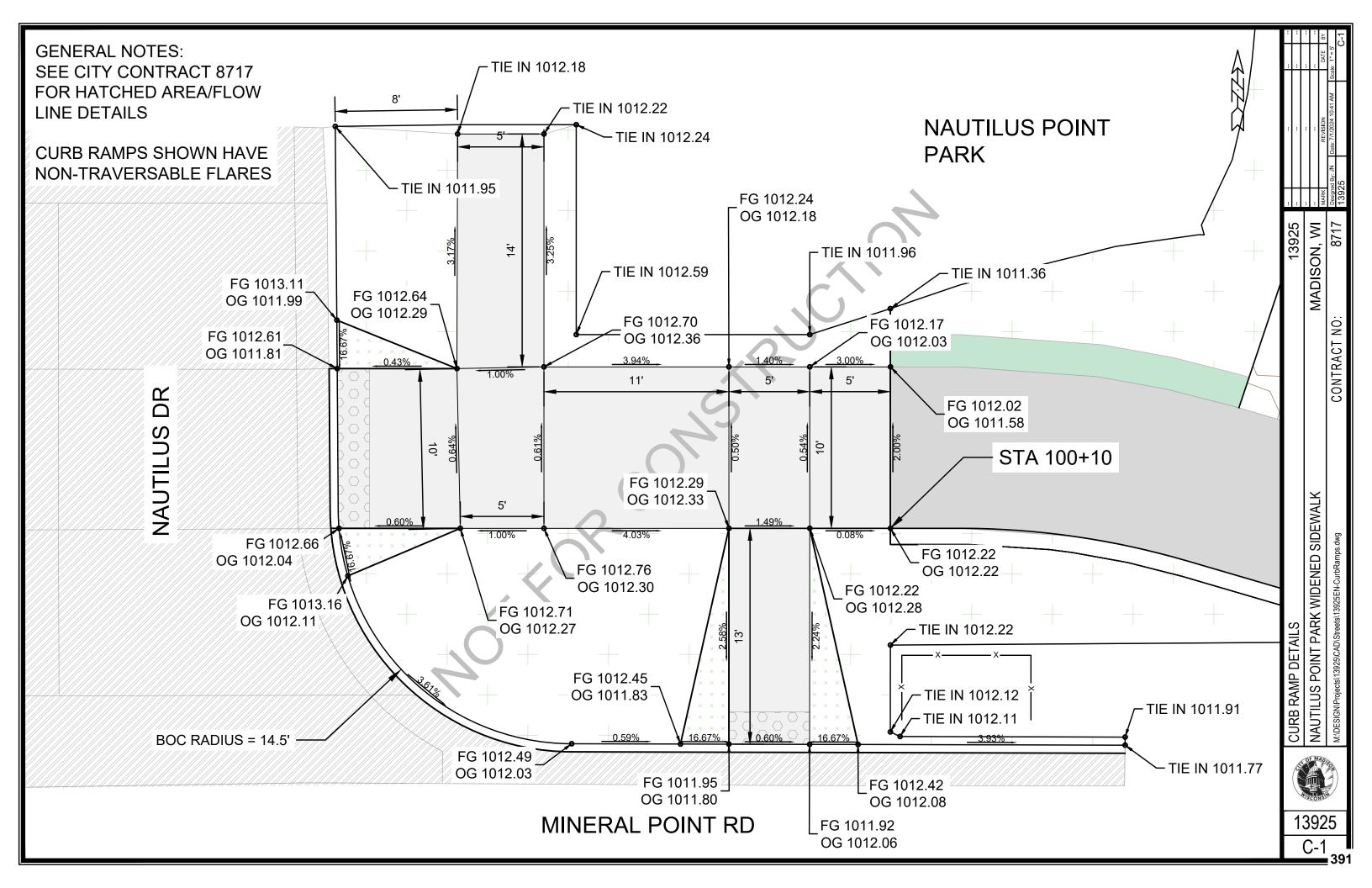


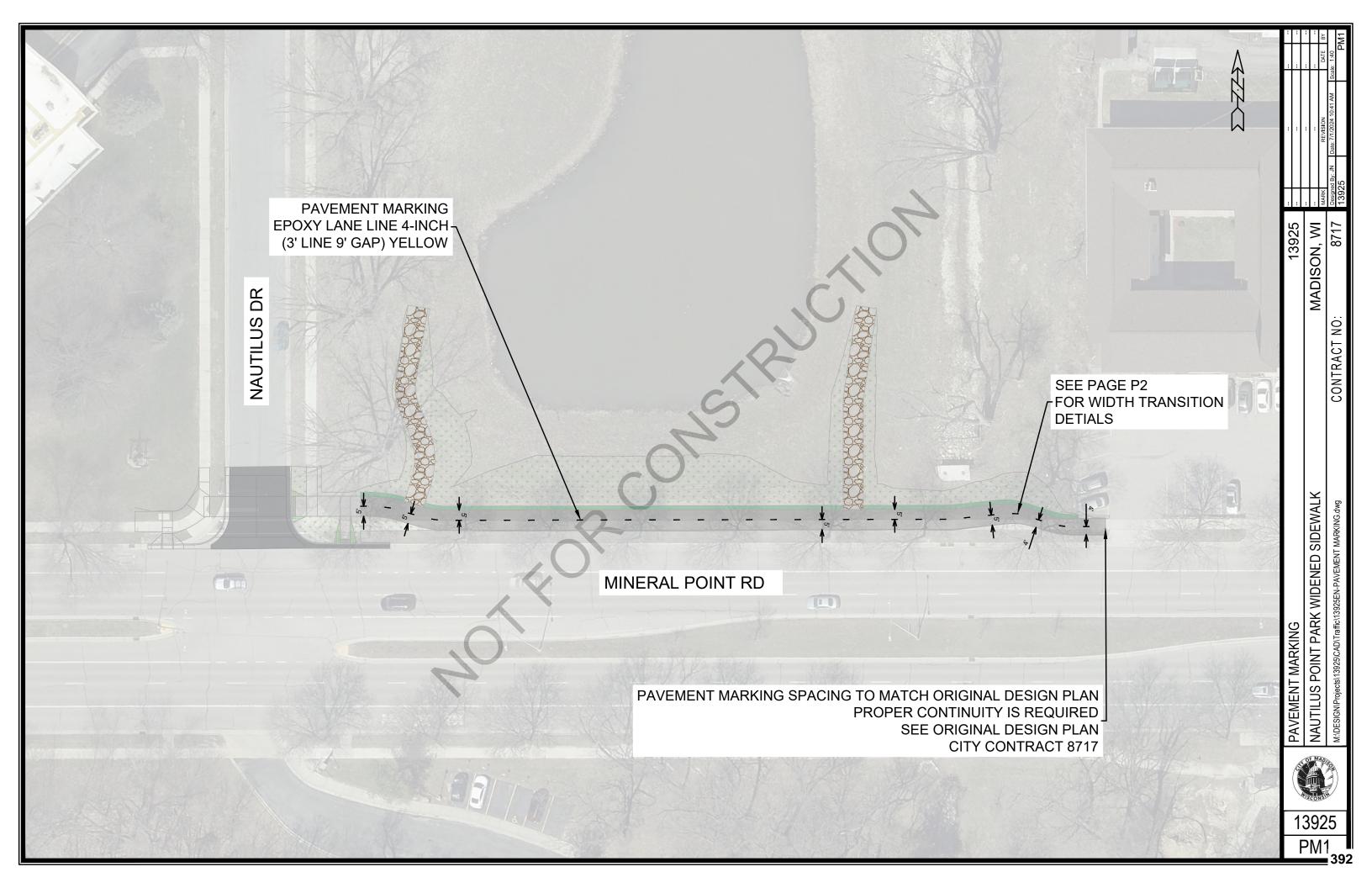


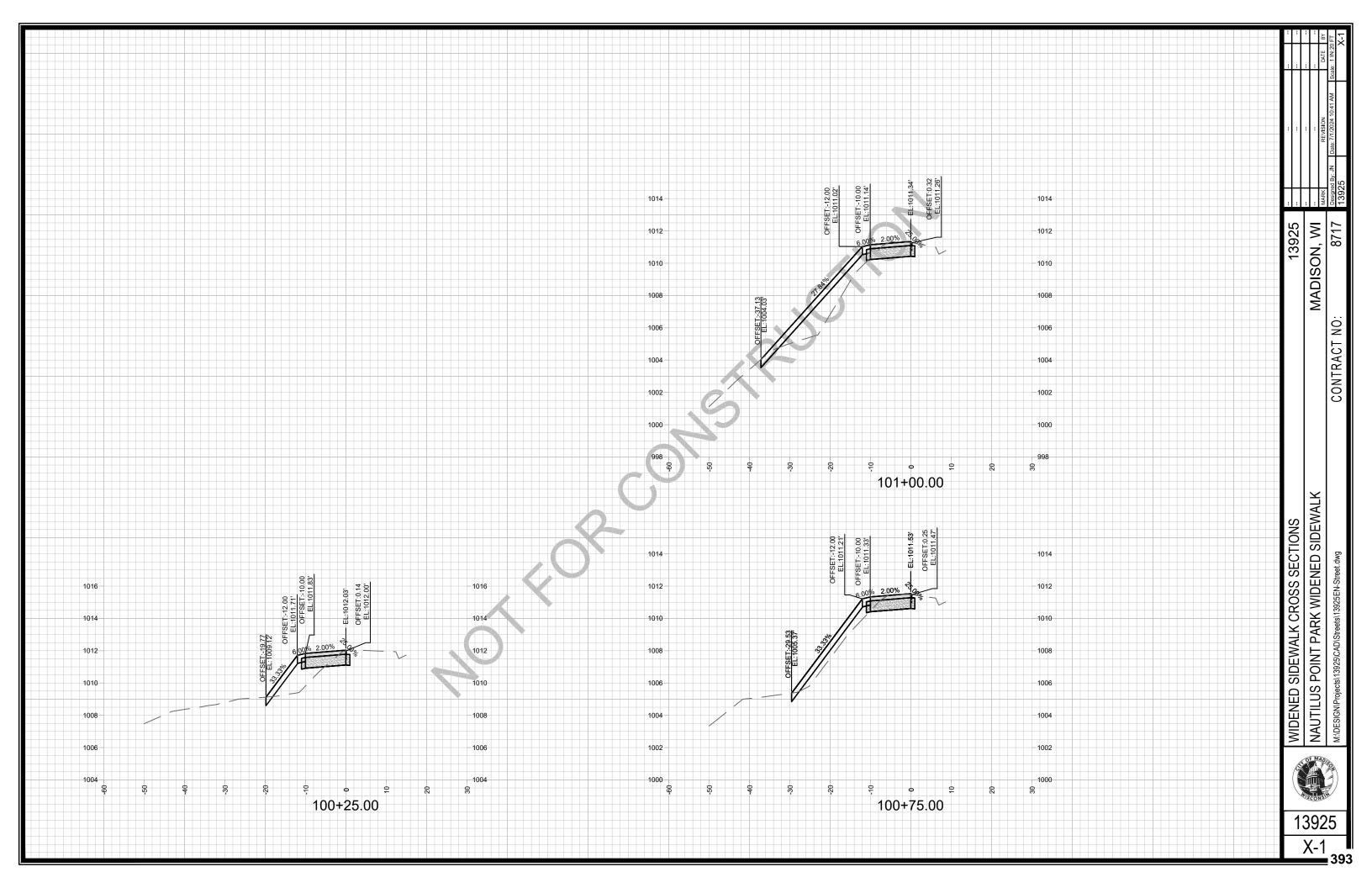


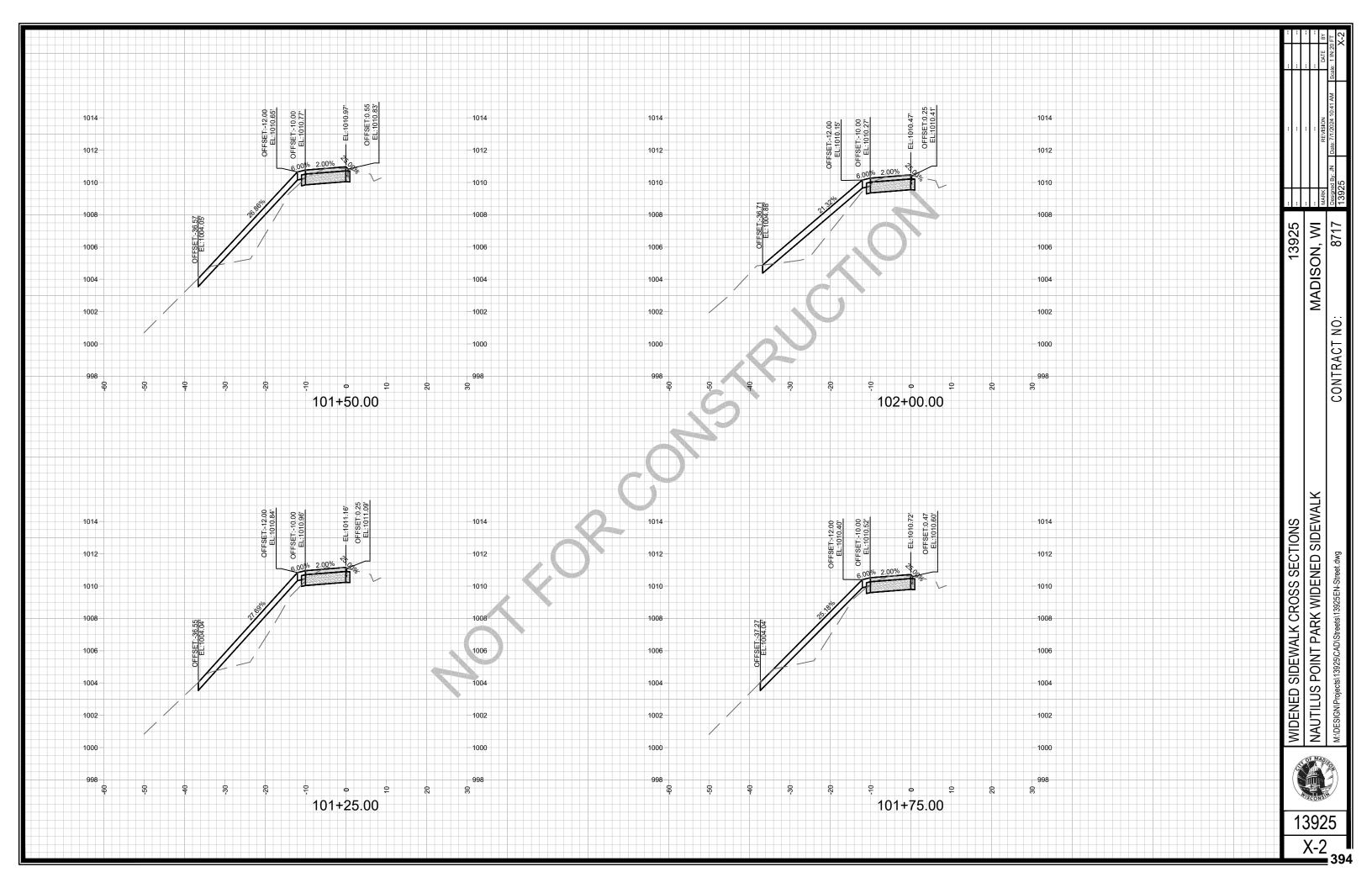


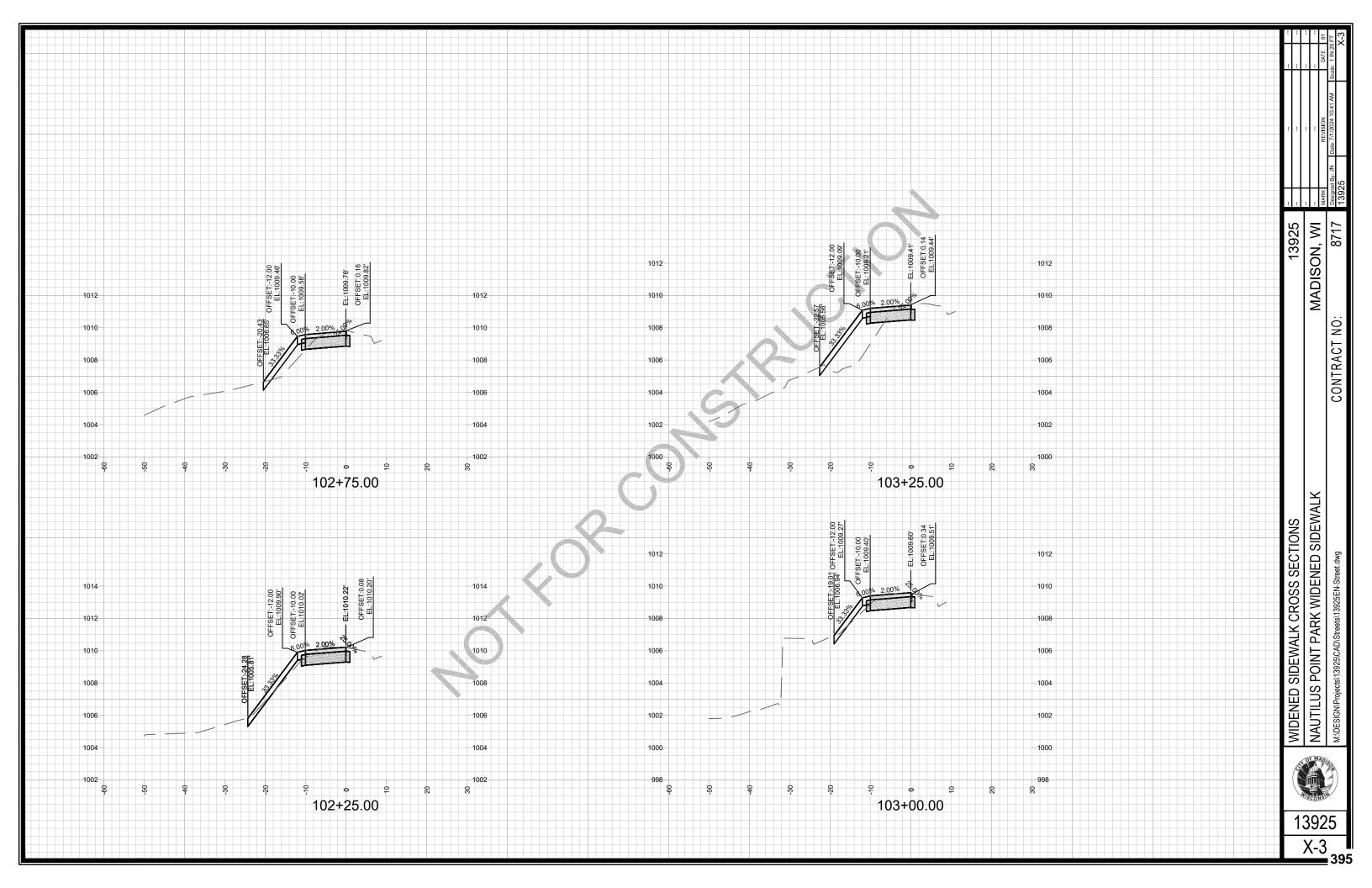


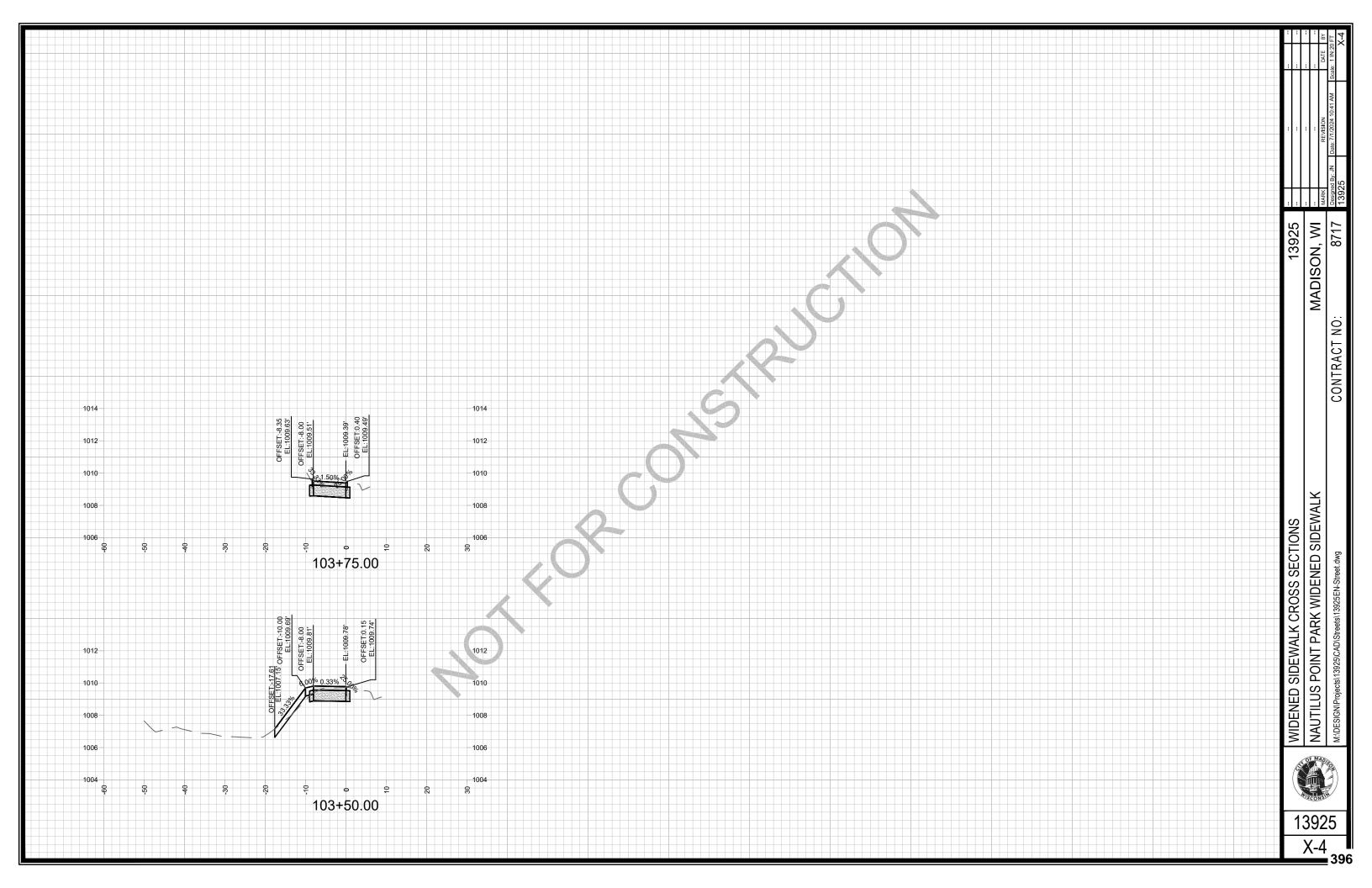


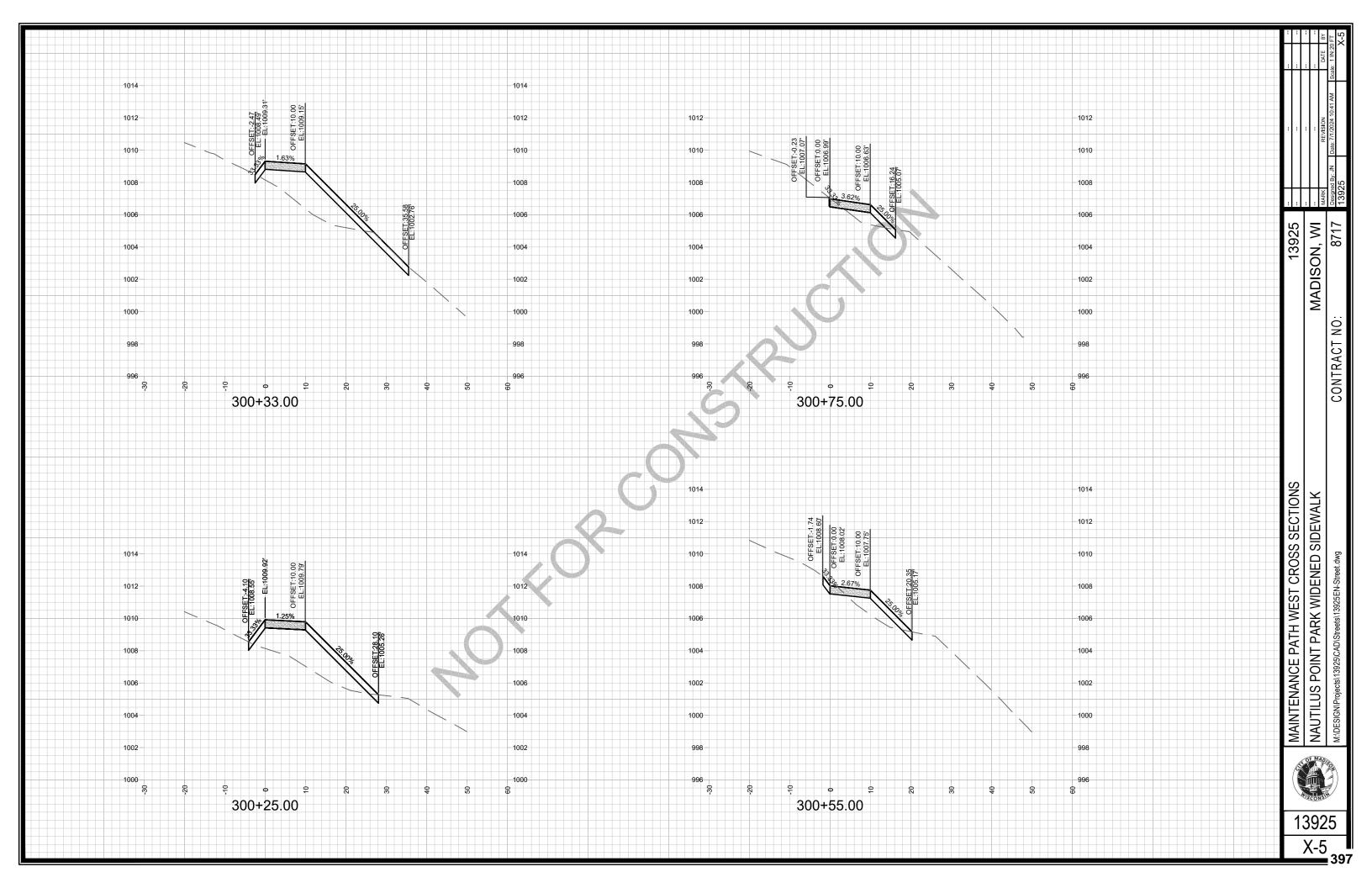


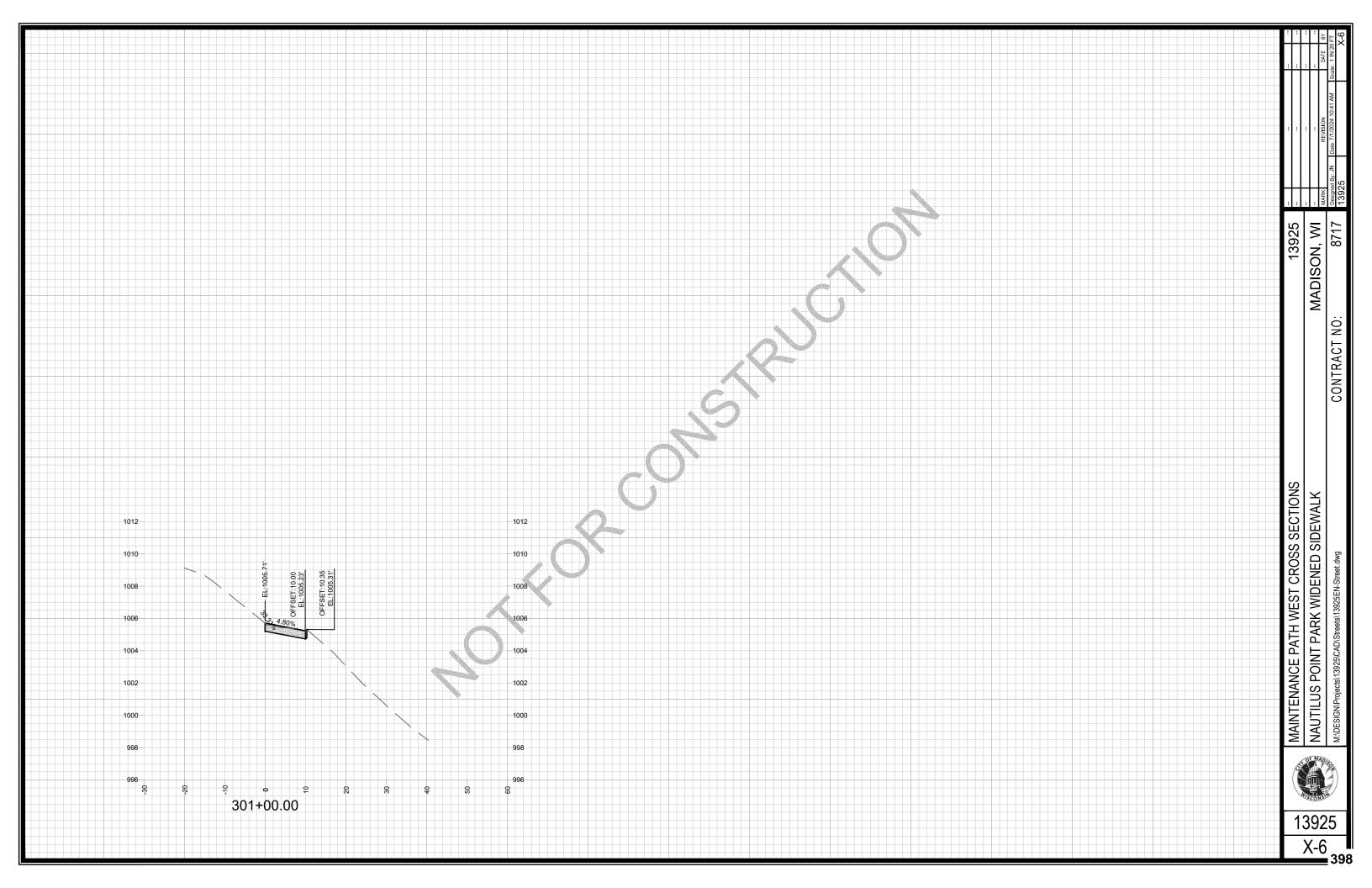


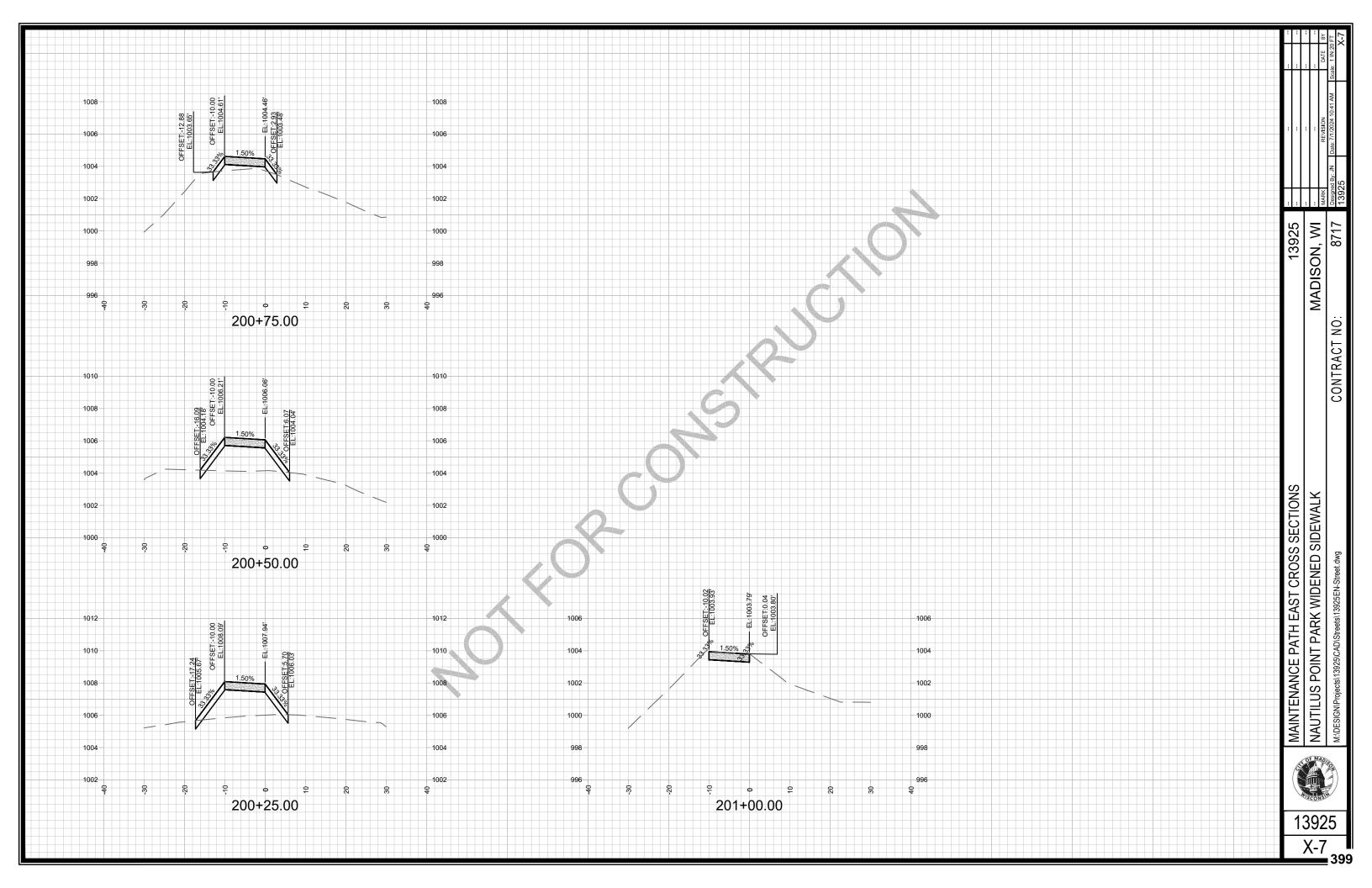












SCC Code SCC Description		Item Number	Description	Units	Quantity Total
40.01 Demolition, Clearing	g, Earthwork	204.0155	Removing Concrete Sidewalk	SY	166
40.01 Demolition, Clearing	g, Earthwork	205.0100	Excavation Common	CY	144
40.01 Demolition, Clearing	g, Earthwork	208.0100	Borrow	CY	250
40.06 Pedestrian / bike ac	cess and accommodation, landscaping	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	177
40.06 Pedestrian / bike ac	cess and accommodation, landscaping	460.5223	HMA Pavement 3" 4 LT 58-28 S	TON	64
40.07 Automobile, bus, va	n accessways including roads, parking lot	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	81
40.06 Pedestrian / bike ac	cess and accommodation, landscaping	602.0410	Concrete Sidewalk 5-Inch	SF	130
40.06 Pedestrian / bike ac	cess and accommodation, landscaping	602.0420	Concrete Sidewalk 7-Inch	SF	0
40.06 Pedestrian / bike ac	cess and accommodation, landscaping	602.0515	Curb Ramp Detectable Warning Field Natural Patina	SF	0
40.08 Temporary Facilities	s and other indirect costs during constructi	619.1000	Mobilization	EACH	1
40.06 Pedestrian / bike ac	cess and accommodation, landscaping	625.0500	Salvaged Topsoil	SY	1312
40.08 Temporary Facilities	and other indirect costs during constructi	628.1905	Mobilizations Erosion Control	EACH	1
40.08 Temporary Facilities	s and other indirect costs during constructi	628.1910	Mobilizations Emergency Erosion Control	EACH	2
40.06 Pedestrian / bike ac	cess and accommodation, landscaping	628.6505	Soil Stabilizer Type A	ACRE	0.19
40.02 Site Utilities, Utility	Relocation	628.7020	Inlet Protection Type D	EACH	4
40.08 Temporary Facilities	s and other indirect costs during constructi	628.756	Tracking Pad	EACH	1
40.06 Pedestrian / bike ac	cess and accommodation, landscaping	629.0210	Fertilizer Type B	CWT	0.060
40.06 Pedestrian / bike ac	cess and accommodation, landscaping	630.0500	Seed Water	MGAL	10
40.08 Temporary Facilities	and other indirect costs during constructi	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	81
40.06 Pedestrian / bike ac	cess and accommodation, landscaping	650.9000	Construction Staking Curb Ramps	EACH	30
40.06 Pedestrian / bike ac	cess and accommodation, landscaping	SPV.0060.501	Tree Protection	EACH	8
40.06 Pedestrian / bike ac	cess and accommodation, landscaping	SPV.0085.501	Seeding	LB	4
40.07 Automobile, bus, va	n accessways including roads, parking lot	SPV.0090.022	Remove, Salvage and Reinstall Fence	LF	17
40.06 Pedestrian / bike ac	cess and accommodation, landscaping	SPV.0090.114	Pavement Marking Epoxy Lane Line 4-Inch	LF	371
40.02 Site Utilities, Utility	Relocation	SPV.0090.2008	Silt sock (8 Inch) -Complete	LF	747
40.06 Pedestrian / bike ac	cess and accommodation, landscaping	SPV.0180.2009	EROSION MATTING, CLASS I, TYPE A - ORGANIC	SY	972
40.06 Pedestrian / bike ac	cess and accommodation, landscaping	SPV.0180.2010	Aggressive Native Seed Mix	SY	892

MISCELLANEOUS QUANTITES						
13925 ###			10001	###	###	#
NAUTILUS POINT PARK WIDENED SIDEWALK MADISON, WI ### MADISON, W	S	MISCELLANEOUS QUAN ES	13925	##	###	#
NAUTILUS POINT PARK WIDENED SIDEWALK MADESIGNIProjects/13925CADIStreets/13925EN-Street.dwg MADISON WIDENICAL MACK WIDENICAL				###	##	##
M:\DESIGN\Projects\13925\CAD\Streets\13925EN-Street.dwg CONTRACT NO: 8717 Designed By: ###	F M	NALITII IIS POINT PARK WIDENED SIDEWALK		###	#	#
M:\DESIGN\Projects\13925\CAD\Streets\13925\CAD\Streets\13925\En-Street.dwg CONTRACT NO: 8717 Designed by: ###	AD		(5)(6)(1)	MARK	REVISION	DATE
M:\DESIGNIFTOJecist/13922bIA-DIStreets/13922bIA-Street.dwg	SOZ		0717	Designed By: ###	Date: 7/1/2024 10:41 AM	Scale: 1 IN:
		M:UESIGNIPTQJects/13923/CAU/STeets/13923/CAU/STeets/13923/CAU/STeets/13923/CAU/STeets/13923/CAU/STEETS/13923/CAU/STEETS/13923/CAU/STEETS/13923/CAU/STEETS/13923/CAU/STEETS/13923/CAU/STEETS/13923/CAU/STEETS/13923/CAU/STEETS/CAU/STEET	/1 /0	13925		2



Contract Name:

East-West Bus Rapid Transit Widened Sidewalk, Madison, Dane County, Wisconsin

Í	
Change Order No.	CO 1
Contract No.	
Project/MUNIS No.	
Change Order Project No.	
Ald District	

Change Order Description:

Increasing quantity for original plan Items, #460.5224 (SCC 40.06), #204.0155 (SCC 40.01), #205.0100 (SCC 40.01), #208.0100 (SCC 40.01), #305.0120 (SCC 40.06), #601.0411 (SCC 40.07), #602.0410 (SCC 40.06), #619.1000 (SCC 40.08), #628.1905 (SCC 40.08), #628.1910 (SCC 40.08), #628.6505 (SCC 40.06), #628.7020 (SCC 40.02), #629.0210 (SCC 40.06), #630.0500 (SCC 40.06), #650.9000 (SCC 40.06), #650.5500 (40.08), #SPV.0060.501 (SCC 40.06), #SPV.0085.501 (SCC 40.06), #SPV.0090.022 (SCC 40.07), #SPV.0090.114 (SCC 40.06), #628.756 (SCC 40.08), #SPV.0090.2008 (SCC 40.02), #SPV.0180.2009 (SCC 40.06), #SPV.0180.2009 (SCC 40.06), #SPV.0180.2010 (SCC 40.06).

Account Numbers for this Change Order:
All items are under City Acct#
13925-43-130-140440; With the exception
of #628.7020 and 628.7560 that are under
13925-43-174-140440

Contractor:

Parisi Construction
508 S Nine Mound Rd.
Verona WI 53593

You are authorized and directed to make the following changes in this contract:

*Mark if negotiated (N) or bid (B) unit price

Item No.	Description	Est. Qty	Unit	Unit Price	N/B*	Total
460.5224	HMA Pavement 3" 4 LT 58-28 S	64.00	TON	93.00	В	5,952.00
204.0155	Removing Concrete Sidewalk	166.00	SY	9.90	В	1,643.40
205.0100	Excavation Common	144.00	CY	38.75	В	5,580.00
208.0100	Borrow	250.00	CY	5.30	В	1,325.00
305.0120	Base Aggregate Dense 1 1/4-Inch	177.00	TON	22.75	В	4,026.75
601.0411	Concrete Curb & Gutter 30-Inch Type D	81.00	LF	39.25	В	3,179.25
602.0410	Concrete Sidewalk 5-Inch	130.00	SF	9.80	В	1,274.00
619.1000	Mobilization	1.00	EA	24,971.23	N	24,971.23
625.0500	Salvaged Topsoil	1,312.00	SY	7.50	N	9,840.00
628.1905	Mobilizations Erosion Control	1.00	EA	1.10	В	1.10
628.1910	Mobilizations Emergency Erosion Control	2.00	EA	1.10	В	2.20
628.6505	Soil Stabilizer Type A	0.19	ACRE	2,300.00	В	437.00
628.7020	Inlet Protection Type D	4.00	EA	130.00	В	520.00
628.7560	Tracking Pad	1.00	EA	1,000.00	N	1,000.00
629.0210	Fertilizer Type B	0.05	CWT	160.00	В	8.16
630.0500	Seed Water	10.00	MGAL	0.01	В	0.10

650.9000	Construction Staking Curb Ramp	30.00	EA	50.00	В	1,500.00
650.5500	Construction Staking Curb Gutter and Curb & Gutter	81.00	LF	0.50	В	40.50
SPV.0060.501	Tree Protection	8.00	EA	740.00	В	5,920.00
SPV.0085.501	Seeding	2.54	LB	10.50	В	26.63
SPV.0090.022	Remove, Salvage and Reinstall Fence	17.00	LF	85.00	В	1,445.00
SPV.0090.114	Pavement Marking Epoxy Lane Line 4-Inch	371.00	LF	4.10	В	1,521.10
SPV.0090.2008	Silt sock (8 Inch) -Complete	747.00	LF	5.25	N	3,921.75
SPV.0180.2009	EROSION MATTING, CLASS I, TYPE A - ORGANIC	972.00	SY	3.00	N	2,916.00
SPV.0180.2010	Aggressive Native Seed Mix	892.00	SY	2.90	N	2,586.80
	Added Contingency	1.00	LS	6,362.03	N	6,362.03

	% of Original	
Net Change Order	1.24%	86,000.00
The Original Contract Total		6,931,035.00
Sum of previous Change Orders	0.00%	0.00
The new Contract Sum including this Change Order will be	101.24%	7,017,035.00

This Contract is a:	Calendar Days
Original Contract Time/Completion Date	8/30/2024
Net Change in Contract Time by previous change order	
Contract Time/Completion Date prior to this change order	
Additional day(s) as a result of this Change Order	
Contract time/completion date as a result of this change order	

Contract Time/Completion Date prior to this change order Additional day(s) as a result of this Change Order Contract time/completion date as a result of this change order	ır	
Contractor's Acceptance By Title Date		
City's Approval (see reverse side for instructions) Construction Manager Construction Supervisor Engineer Board of Public Works		Date 7/1/2024
	Contract pa ROUTING: EN Admin Greg Fries Chase O'B	

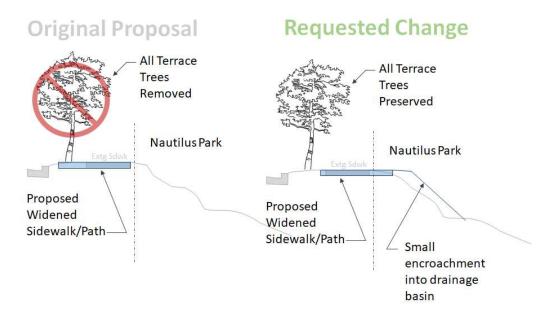
Mineral Point Widened Sidewalk Nautilus Park

Change Order 1



Previous BPW Request 11/20/23 (Leg 80605)

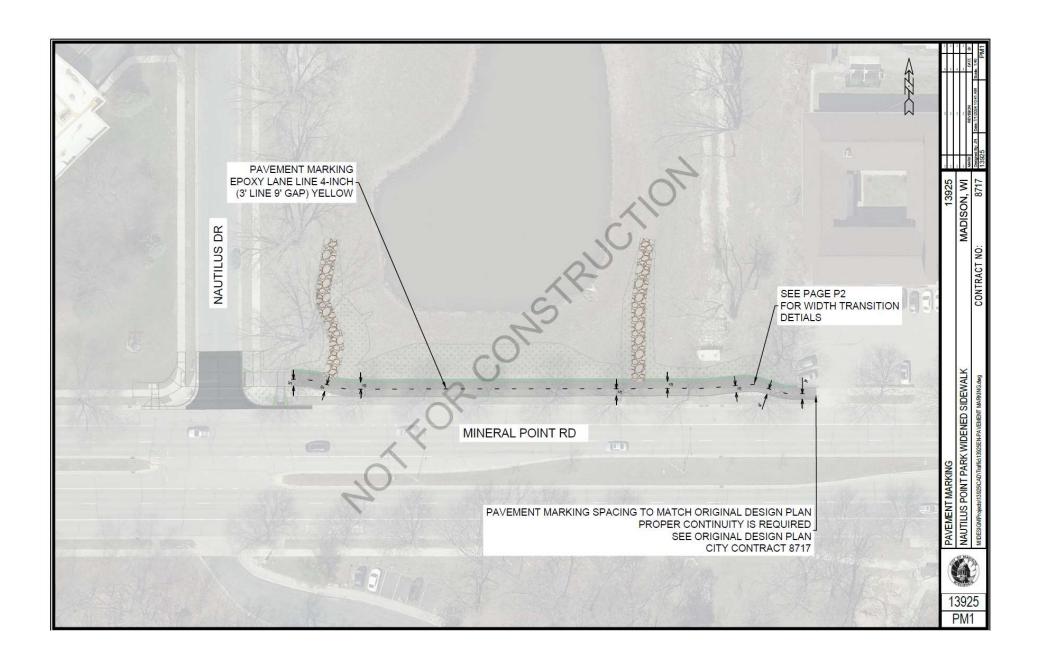
- BPW requested to investigate redesign widened sidewalk adjacent to Nautilus Park to avoid removing street trees.
- Staff agreed to investigate, mentioning potential Section 4f concerns with FTA



Interaction with FTA and Madison Parks

- Parks agreed that the affected area served a stormwater function.
- FTA agreed that the impact amounted to a diminimis impact







	Name

East-West Bus Rapid Transit Widened Sidewalk, Madison, Dane County, Wisconsin

Change Order No.	CO 1
Contract No.	8717
Project/MUNIS No.	13925
Change Order Project No.	
Ald District	

Change Order Description:

Increasing quantity for original plan Items, #460.5224 (SCC 40.06), #204.0155 (SCC 40.01), #205.0100 (SCC 40.01), #208.0100 (SCC 40.01), #305.0120 (SCC 40.06), #601.0411 (SCC 40.07), #602.0410 (SCC 40.06), #619.1000 (SCC 40.08), #628.1905 (SCC 40.08), #628.1910 (SCC 40.08), #628.6505 (SCC 40.06), #628.7020 (SCC 40.02), #629.0210 (SCC 40.06), #630.0500 (SCC 40.06), #650.5500 (40.08), #5PV.0060.501 (SCC 40.06), #SPV.0085.501 (SCC 40.06), #SPV.0090.022 (SCC 40.07), #SPV.0090.114 (SCC 40.06); Adding new items to contract, #625.0100 (SCC 40.06), #628.756 (SCC 40.08), #SPV.0090.2008 (SCC 40.02), #SPV.0180.2009 (SCC 40.06), #SPV.0180.2010 (SCC 40.06).

Account Numbers for this Change Order: All items are under City Acct#
13925-43-130-140440; With the exception
of #628.7020 and 628.7560 that are under 13925-43-174-140440

Contractor:

Parisi Construction 508 S Nine Mound Rd. Verona WI 53593

a authorized and directed to make the following changes in this contract:

	ed and directed to make the following changes in					tiated (N) or bid (B) unit price	
Item No.	Description	Est. Qty	Unit	Unit Price	N/B*	Total	
460.5224	HMA Pavement 3" 4 LT 58-28 S	64.00	TON	93.00	В	5,952.00	
204.0155	Removing Concrete Sidewalk	166.00	SY	9.90	В	1,643.40	
205.0100	Excavation Common	144.00	CY	38.75	В	5,580.00	
208.0100	Borrow	250.00	CY	5.30	В	1,325.00	
305.0120	Base Aggregate Dense 1 1/4-Inch	177.00	TON	22.75	В	4,026.75	
601.04 <mark>11</mark>	Concrete Curb & Gutter 30-Inch Type D	81.00	LF	39.25	В	3,179.25	
602.0410	Concrete Sidewalk 5-Inch	130.00	SF	9.80	В	1,274.00	
619.1000	Mobilization	1.00	EA	24,971.23	N	24,971.23	
625.0500	Salvaged Topsoil	1,312.00	SY	7.50	N	9,840.00	
628.1905	Mobilizations Erosion Control	1.00	EA	1.10	В	1.10	
628.1910	Mobilizations Emergency Erosion Control	2.00	EA	1.10	В	2.20	
628.6505	Soil Stabilizer Type A	0.19	ACRE	2,300.00	В	437.00	
628.7020	Inlet Protection Type D	4.00	EA	130.00	В	520.00	
628.7560	Tracking Pad	1.00	EA	1,000.00	N	1,000.00	
629.0210	Fertilizer Type B	0.05	CWT	160.00	В	8.16	
630.0500	Seed Water	10.00	MGAL	0.01	В	0.10	

650.9000	Construction Staking Curb Ramp	30.00	EA	50.00	В	1,500.00
650.5500	Construction Staking Curb Gutter and Curb & Gutter	81.00	LF	0.50	В	40.50
SPV.0060.501	Tree Protection	8.00	EA	740.00	В	5,920.00
SPV.0085.501	Seeding	2.54	LB	10.50	В	26.63
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SPV.0090.114	Pavement Marking Epoxy Lane Line 4-Inch	371.00	LF	4.10	В	1,521.10
SPV.0090.2008	Silt sock (8 Inch) -Complete	747.00	LF	5.25	N	3,921.75
SPV.0180,2009	EROSION MATTING, CLASS I, TYPE A - ORGANIC	972.00	SY	3.00	N	2,916.00
SPV.0180.2010	Aggressive Native Seed Mix	892.00	SY	2.90	N	2,586.80
	Added Contingency	1.00	LS	6,362.03	N	6,362.03

	% of Original	
Net Change Order	1,24%	86,000.00
The Original Contract Total	V-7675943	6,931,035.00
Sum of previous Change Orders	0.00%	0.00
The new Contract Sum including this Change Order will be	101.24%	7,017,035.00
This Contract is a:	Ca	alendar Days
Original Contract Time/Completion Date		8/30/2024
Net Change in Contract Time by previous change order		
Contract Time/Completion Date prior to this change order		
Additional day(s) as a result of this Change Order		
Contract time/completion date as a result of this change order		
Contractor's Acceptance		
Ву		
Title		
Date		
City's Approval (see reverse side for instructions)	1	Date
Construction Manager		7/1/2024
Construction Supervisor		
Engineer		
Board of Public Works		
3		
	Contract paid to date	\$ 1,438,209.58
	ROUTING:	
	EN Admin	
	Greg Fries	
	Chase O'Brien	

Summary

- Total is \$86,000
 - 1.24% increase to total contract
 - Contract total now 7,017,035.00
- The Nautilus Park widened sidewalk and associated tree removal was removed from the base contract to provide time for Section 4f coordination with FTA.
- Section 4f clearance given
- At Board of Publics works request, a plan was made
- Geometry Approved By TC 10 Foot Widened Sidewalk
- Parks Commission Approved Trees Persevered



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 81488

File ID:81488File Type:LicenseStatus:Report of Officer

Version: 1 Reference: Controlling Body: Clerk's Office

File Created Date: 01/05/2024

File Name: Final Action:

Title: Report of Operator License Applications July 16, 2024. See attached report for list

of operators.

Notes:

Sponsors: Effective Date:

Attachments: Operator Report 7.11.2024.pdf Enactment Number:

Author: Hearing Date:

Entered by: echristianson@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Clerk's Office	04/10/2024	RECOMMEND TO COUNCIL TO GRANT - REPORT OF OFFICER				
	Action Text:	This License was RECO	MMEND TO COUNCI	L TO GRANT - REP	ORT OF OFFICER		

Text of Legislative File 81488

Title

Report of Operator License Applications July 16, 2024. See attached report for list of operators.

Report ID -City of Madison - Clerk's Office Operator (Bartender) Report - Submitted to Common Council Date: 7/11/2024 Time: 1:50:31PM

1 Page:

<u>Holder Name</u> <u>License</u>

License Status - Active

Bowne, Torin F	LICOPR-2024-00047
FLORES, ADRIAN SCOTT	LICOPR-2024-00061
Johnson, Taylor J	LICOPR-2024-00062
Knudson, Riley	LICOPR-2024-00055
McKnight, Linda	LICOPR-2024-00054
MCMURRAY, CYRUS	LICOPR-2024-00049
Menz, Jameson	LICOPR-2024-00052
MUELLER, ALEISHA M	LICOPR-2024-00063



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83528

File ID: 83528 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: Department of

Planning and Community and Economic Development

File Created Date: 05/17/2024

File Name: CSM - 2221 Mustang Way Final Action:

Title: Approving a Certified Survey Map of property owned by H & E Development, LLC

located at 2221 Mustang Way and 5026 Reef Court (District 16).

Notes:

Sponsors: Planning Division Effective Date:

Attachments: Application.pdf, Proposed CSM.pdf, CSM Approval Enactment Number:

Letter.pdf

Author: Hearing Date:

Entered by: tparks@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Department of Planning and Community and Economic Development	07/01/2024	RECOMMEND TO COUNCIL TO ADOPT UNDER SUSPENSION OF MGO 2.055 - REPORT OF OFFICER				
		lution was RE OF OFFICER		IL TO ADOPT UNDER SU	JSPENSION OF N	MGO 2.055	

Text of Legislative File 83528

Fiscal Note

No City appropriation is required with the approval of this certified survey map. City costs associated with urban development in this area will be included in future operating and capital budgets subject to Common Council approval.

Title

Approving a Certified Survey Map of property owned by H & E Development, LLC located at 2221 Mustang Way and 5026 Reef Court (District 16).

Body

WHEREAS a Certified Survey Map of property owned by H & E Development, LLC located at 2221 Mustang Way and 5026 Reef Court, City of Madison, Dane County, Wisconsin has been duly filed for approval by the Plan Commission, its Secretary or their designee, as provided for in Section 16.23(4)(f) of Madison General Ordinances; and

WHEREAS Chapter 236, Wisconsin Statutes requires that the Madison Common Council approve any dedications proposed or required as part of the proposed division of the lands contained on said Certified Survey Map;

NOW THEREFORE BE IT RESOLVED that said Certified Survey Map, bond and subdivision contract, subsequent affidavits of correction, parkland acquisition documents, easement or right-of-way release or procurement documents or any other related document or documents as deemed necessary by the Secretary of the Plan Commission in accordance with the approval of said Certified Survey Map are hereby approved by the Madison Common Council.

BE IT FURTHER RESOLVED that the Mayor and City Clerk of the City of Madison are hereby authorized to sign the above mentioned documents related to this Certified Survey Map.

BE IT FURTHER RESOLVED that all dedications included in this Certified Survey Map or required as a condition of approval of this Certified Survey Map be and are hereby accepted by the City of Madison.

BE IT FURTHER RESOLVED that the Common Council authorizes City staff to request approval from the Capital Area Regional Planning Commission of any minor revisions to adopted environmental corridor boundaries within the Central Urban Service Area relating to this land division, and that the Council recognizes and adopts said revised boundaries.

BE IT FURTHER RESOLVED that the Planning Division is authorized to reflect the recorded Certified Survey Map in the Comprehensive Plan and any applicable neighborhood plans.

SUBDIVISION APPLICATION

** Please read both pages of the application completely and fill in all required fields **

For a digital copy of this form with fillable fields, please visit:

https://www.cityofmadison.com/sites/default/files/city-of-madison/development-services-center/documents/SubdivisionApplication.pdf

If you need an interpreter, translator, materials in alternate formats or other accommodations to access these forms, please call the Planning Division at (608) 266-4635.

Si necesita interprete, traductor, materiales en diferentes formatos, u otro tipo de ayuda para acceder a estos formularios, por favor llame al (608) 266-4635.

Yog tias koj xav tau ib tug neeg txhais lus, tus neeg txhais ntawv, los sis xav tau cov ntaub ntawv ua lwm hom ntawv los sis lwm cov kev pab kom paub txog cov lus qhia no, thov hu rau Koog Npaj (Planning Division) (608) 266-4635.

5/9/24 11:05 a.m. I Building, Suite 017

Madison Municipal Building, Suite 017 215 Martin Luther King, Jr. Blvd. P.O. Box 2985 Madison, WI 53701-2985 (608) 266-4635

City of Madison

Planning Division



NOTICE REGARDING LOBBYING ORDINANCE: If you are seeking approval of a development that has over 40,000 square feet of non-residential space, or a residential development of over 10 dwelling units, or if you are seeking assistance from the City with a value of \$10,000 (including grants, loans, TIF or similar assistance), then you likely are subject to Madison's lobbying ordinance (M.G.O. Sec. 2.40). You are required to register and report your lobbying. Please consult the City Clerk's Office for more information. Failure to comply with the lobbying ordinance may result in fines.

☐ Preliminary Subdivision Plat ☐ Final Subdivision Plat ☐ Land Division/Certified Survey Man (CS)	
I I Land Division/Certified Survey Man (CS)	. 41
If a Plat, Proposed Subdivision Name: Final Subdivision Plat Land Division/Certified Survey Map (CSI	VI)
2. Review Fees	
• For Preliminary and/or Final Plats, an application fee of \$250, plus \$50 per lot or outlot contained on the plat.	
 For Certified Survey Maps, an application fee of \$250 plus \$200 per lot and outlot contained on the CSM. 	
Make checks payable to "City Treasurer" and mail it to the following add to the following add	2001
Madison, WI 53701-2984. Please include a cover page with the check which includes the project address, brief description project, and contact information.	of the
3. Property Owner and Agent Information	
Name of Property Owner: H&E Development, LLC Representative, if any: Ken Ehlen	
Street address: 7597 East US Highway 36 City/State/Zip: Avon, IN	
Telephone: (608) 223-9808 Email: kehlen@oakhousebakery.com	
Firm Preparing Survey: Wyser Engineering Contact: Zach Reynolds	
Street address: 312 E. Main St City/State/Zip: Mount Horeb, WI 53572	
Telephone: (608) 636-9070 Email: zach.reynolds@wyserengineering.com	
Check only ONE – ALL Correspondence on this application should be sent to: ☐ Property Owner, OR ☑ Survey Firm	
4. Property Information for Properties Located within Madison City Limits	
Parcel Addresses: 2221 Mustang Way, Madison, WI & 5026 Reef Court, Madison, WI	
Tax Parcel Number(s): 251/0710-154-0205-2 & 251/0710-154-0204-4	
Zoning District(s) of Proposed Lots: IL School District: MMSD	
 Please include a detailed description of the number and use of all proposed lots and outlots in your letter of intent. 	
4a. Property Information for For Properties Located Outside the Madison City Limits in the City's Extraterritorial Jurisdiction:	
Parcel Addresses (note town if located outside City):	
Date of Approval by Dane County: Date of Approval by Town:	
For an exterritorial request to be scheduled, approval letters from both the Town and Dane County must be submitted.	
5. Subdivision Contents and Description. Complete table as it pertains to your request: do not complete gray areas	

Land Use	Lots	Outlots	Acres
Residential			
Retail/Office			
Industrial	1		3.83

Land Use	Lots	Outlots	Acres
Other (state use):			
Outlots Dedicated to the Public (Parks, Stormwater, etc.)			
Outlots Maintained by a Private Group or Association			
PROJECT TOTALS	1		3.83

6. Required Submittal Materials

<u>Digital (PDF) copies</u> of all items listed below (if applicable) are required. Applicants are to submit each of these documents as <u>individual PDF files</u> in an e-mail sent to <u>PCapplications@cityofmadison.com</u>. The transmittal shall include the name of the project and applicant. Note that <u>an individual email cannot exceed 20MB</u> and it is the <u>responsibility of the applicant</u> to present files in a manner that can be accepted. Electronic submittals via file hosting services (such as Dropbox) are <u>not</u> allowed. Applicants who are unable to provide the materials electronically should contact the Planning Division at <u>Planning@cityofmadison.com</u> or (608) 266-4635 for assistance.

□ A Completed Subdivision	Application Form (i.e	. both sides of this form)
---------------------------	-----------------------	----------------------------

- ☐ Map Copies (prepared by a Registered Land Surveyor):
 - For <u>Preliminary Plats</u>, the drawings must be drawn to scale and are required to provide all information as set forth in <u>M.G.O. Sec. 16.23 (7)(a)</u>.
 - For <u>Final Plats</u>, the drawings must be drawn to scale and drawn to the specifications of <u>§236.20</u>, <u>Wis. Stats</u>.
 - For <u>Certified Survey Maps (CSMs)</u>, the drawings shall include all of the information set forth in <u>M.G.O. Secs. 16.23 (7)(a) and (d)</u>, including existing site conditions, the nature of the proposed division and any other necessary data. Utility data (field located or from utility maps) may be provided on a separate map submitted with application.

For Plat & CSMs, in addition to the PDF copy, a digital CADD file shall also be submitted in a format compatible with AutoCAD. The digital CADD file(s) shall be referenced to the Dane County Coordinate System and shall contain, at minimum, the list of items stated below, each on a separate layer/level name. The line work shall be void of gaps and overlaps and match the plat, preliminary plat or CSM as submitted: a) Right-of-Way lines (public and private); b) Lot lines; c) Lot numbers; d) Lot/Plat dimensions; e) Street names; f) Easement lines (i.e. all in title and shown on the plat or CSM including wetland & floodplain boundaries.)

- ☐ Letter of Intent: One copy of a letter describing the proposed subdivision or land division in detail including, but not limited to:
 - The number and type/use of the lots and outlots proposed with this subdivision or land division, including any outlots to be dedicated to the public;
 - Existing conditions and uses of the property;
 - Phasing schedule for the project, and;
 - The names of persons involved (property owner(s), subdivider, surveyor, civil engineer, etc.).
 - * The letter of intent for a subdivision or land division may be the same as the letter of intent submitted with a concurrent Land Use Application for the same property.
 - ** A letter of intent is not required for Subdivision Applications for lot combinations or split duplexes.
- □ Report of Title and Supporting Documents: One copy of a City of Madison standard 60-year Report of Title obtained from a title insurance company as required in M.G.O. Sec. 16.23 and as satisfactory to the Office of Real Estate Services. Note:
 - The Report of Title must have been completed within three (3) months of the submittal date of this application. Title insurance or a title commitment policy are NOT acceptable (i.e. a Preliminary Title Report or a Record Information Certificate).
 - The electronic PDF submittal shall include images of the vesting deeds and all documents listed in the Report of Title.
 - <u>Do not email these files to the City's Office of Real Estate Services</u>. Send them instead to the email address noted at the top of this page.
- □ For Surveys Outside the Madison City Limits: One copy of the approval letters from the town where the property is located and Dane County shall be submitted with your request. The Plan Commission may not consider an application within its extraterritorial jurisdiction without prior approval from the town and Dane County.

7. Applicant Declarations:

The signer attests that the ap	pplication has been completed	accurately and all requir	ed materials have been su	ubmitted:
Applicant's Printed Name	KENDETH W ENFEL	Signatura. V	A 111 511	

Applicant's Printed Name:	REMOETH W. EHLEKI	Signature: _ Keneth W. Chle.
Date: 5-9-24	Interest In Property On This Da	e: Dwnef

CERTIFIED SURVEY MAP NO. ALL OF LOT 26 AND PART OF LOT 27, FIRST ADDITION TO CAPITAL INDUSTRIAL PARK RECORDED ON JULY 22, 1985 IN VOLUME 55 OF PLATS ON PAGES 33-34, AS DOCUMENT NO. 1890326 AND PART OF VACATED REEF COURT AS DESCRIBED IN RESOLUTION RECORDED DECEMBER 14, 1994 IN VOLUME 29028 OF RECORDS ON PAGES 28-31, AS DOCUMENT NO. 2650467, BEING PART OF THE SOUTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 15, TOWNSHIP 7 NORTH, RANGE 10 EAST, LOCATED IN THE CITY OF MADISON, DANE COUNTY, WISCONSIN CSM NO. 4599 FIRST ADDITION TO CAPITOL INDUSTRIAL PARK MUSTANG WAY LOT 23 PER FIRST ADD. TO CAPITAL EXISTING 60' RIGHT- OF-WAY (N 88° 27' 05" E) PER FIRST ADD. TO CAPITAL INDUSTRIAL PARK INDUSTRIAL PARK (N 80° 35' 45" E 101.90') <u>166</u>.41' N 88° 50',12" E C-2 N 80° 58' 52" E 102.00' 20, TO CAPITOL INDUSTRIAL FIRST ADDITION TO CAPITOL INDUSTRIAL PARK TO CAPITAL INDUSTRIAL PARK EXISTING BUILDING PART OF LOT 27 PARK ADDITION TO CAPITOL INDUSTRIAL PER FIRST ADD. FIRST LOT 1 166,638 SQ. FT. OR 3.83 ACRES 375.32') INDUSTRIAL PARK ≥ ≥ PART OF LOT N 0° 37' 23" 0° 11' INDUSTRIAL PARK 4 PITOL 70. FIRST ADDITION ART OF LOT PART VACATED REEF COURT PER DOCUMENT NO. 2650467 S 88° 50' 12" W 199.49 (\$ 88° 25' 26" W) PER DOC. NO. 2650467 õ REEF COURT(ROAD) P.O.B EXISTING 60' RIGHT- OF-WAY 149.99 S 88° 38' 29" W (\$ 88° 25' 40" W) PER FIRST ADD. TO CAPITAL INDUSTRIAL PARK CUL-DE-SAC LOT 17 (S 88° 25' 26" W) PER FIRST ADD. TO WORLD DAIRY CENTER FIRST ADDITION TO WORLD DAIRY CENTER LOT 17 #1 SOUTH 1/4 CORNER 1775.57 880.82' N 88° 49' 07" E 2656.39' FOUND MONUMENT TO FOUND MONUMENT #2 SE CORNER SEC. 15-T7N-R10E SEE TABLE FOR **LEGEND** NOTES: **NORTH DETAILS** PLSS SECTION CORNER FIELD WORK PERFORMED BY WYSER ENGINEERING, LLC. FOUND AS NOTED ON THE WEEK OF JULY 19TH, 2021 3/4" REBAR FOUND UNLESS NOTED NORTH REFERENCE FOR THIS CERTIFIED SURVEY AND MAP ARE BASED ON THE WISCONSIN COORDINATE 0 1" IRON PIPE FOUND SCONS 2" IRON PIPE FOUND REFERENCE SYSTEM, WISCRS DANE, NAD 83 (2011), GRID **(** 3/4" REBAR SET 1.50 LB/FT NORTH. THE SOUTH LINE OF THE SOUTHEAST QUARTER OF SECTION 15, T7N, R10E, BEARS N 88*49' 07" E **7ACHARY M** CSM BOUNDARY REYNOLDS RIGHT-OF-WAY LINE THIS PARCEL IS SUBJECT TO ALL EASEMENTS AND MOU. WISCONS. WISCONS. ALLANDININ AGREEMENTS, BOTH RECORDED AND UNRECORDED. CENTERLINE SECTION/QUARTER LINE SEE SHEET 2 OF 6 LINE TABLE, CURVE TABLE AND EASEMENT DETAILS PLATTED LINE EXISTING EASEMENT SEE SHEET 3 OF 6 FOR EXISTING SITE FEATURE DETAILS EXISTING BUILDING AND SECTION CORNER MONUMENT COORDINATE TABLE RECORDED INFORMATION PAGE _ VOL. . PREPARED FOR: SURVEYED BY: ZMR/MAL YSER ENGINEERING KSW CONSTRUCTION CORP 1100 JOHN P LIVESEY BLVD VERONA, WI 53593

PROJECT NO:

SHEET NO:

DRAWN BY:

APPROVED BY: ZMR

JWS

17-047

1 of 6

DOC. NO.

C.S.M. NO.

2024 - 10: 29am

May 09, 2

Plotted:

Zach

User:

9 JO

SSM

Layout:

Court CSM.dwg

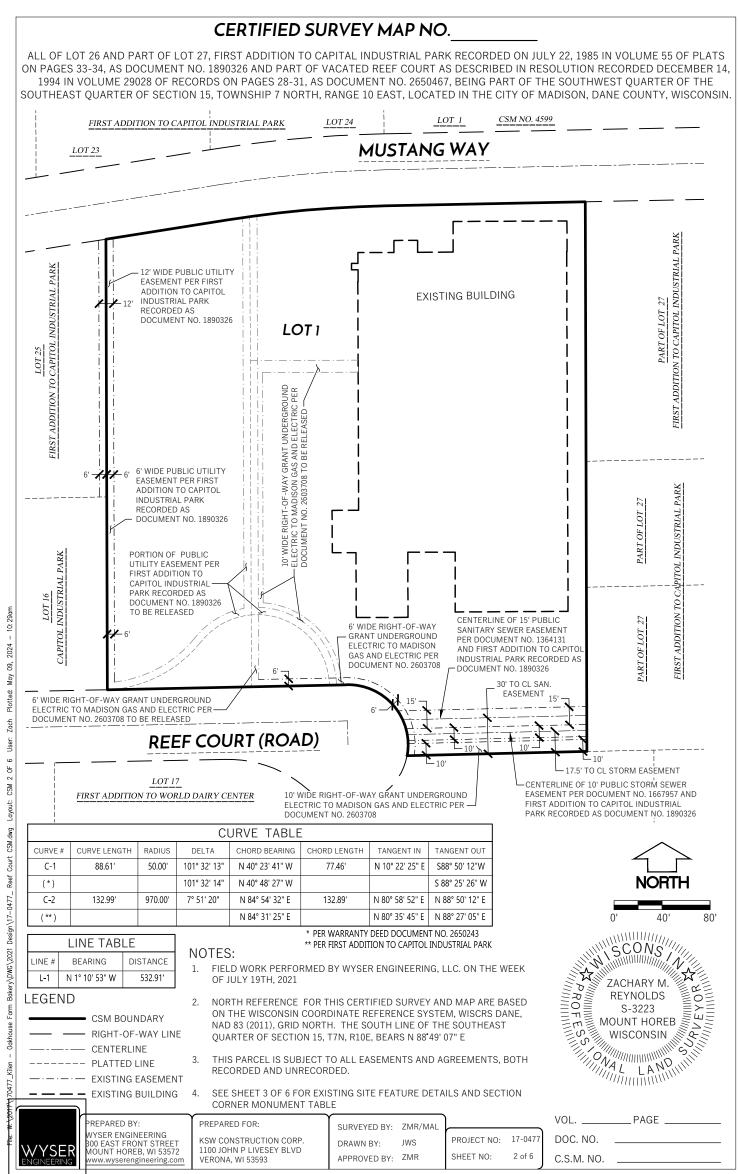
Reef

Design\17-0477_

Bakery\DWG\2021

Oakhouse Farm

EAST FRONT STREET UNT HOREB, WI 53572



REPARED BY:

YSER ENGINEERING

EAST FRONT STREET OUNT HOREB, WI 53572 PREPARED FOR:

KSW CONSTRUCTION CORP 1100 JOHN P LIVESEY BLVD VERONA, WI 53593 PAGE _

VOL.

DOC. NO.

C.S.M. NO.

ZMR/MAL

JWS

PROJECT NO:

SHEET NO:

17-047

3 of 6

SURVEYED BY:

APPROVED BY: ZMR

DRAWN BY:

CERT	IFIFD	SUR	VFV	MAP	NO
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ALL OF LOT 26 AND PART OF LOT 27, FIRST ADDITION TO CAPITAL INDUSTRIAL PARK RECORDED ON JULY 22, 1985 IN VOLUME 55 OF PLATS ON PAGES 33-34, AS DOCUMENT NO. 1890326 AND PART OF VACATED REEF COURT AS DESCRIBED IN RESOLUTION RECORDED DECEMBER 14, 1994 IN VOLUME 29028 OF RECORDS ON PAGES 28-31, AS DOCUMENT NO. 2650467, BEING PART OF THE SOUTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 15, TOWNSHIP 7 NORTH, RANGE 10 EAST, LOCATED IN THE CITY OF MADISON, DANE COUNTY, WISCONSIN.

LEGAL DESCRIPTION

AS PROVIDED

LOT TWENTY-SIX (26), FIRST ADDITION TO CAPITOL INDUSTRIAL PARK, IN THE CITY OF MADISON, DANE COUNTY, WISCONSIN.

AND

THAT PART OF VACATED REEF COURT (ROAD) AS PLATTED IN THE PLAT OF THE FIRST ADDITION TO CAPITOL INDUSTRIAL PARK, IN THE CITY OF MADISON, DANE COUNTY, WISCONSIN, DESCRIBED AS FOLLOWS: BEGINNING AT A POINT OF CURVATURE AT THE SOUTHWEST CORNER OF LOT 26 OF THE PLAT OF FIRST ADDITION TO CAPITOL INDUSTRIAL PARK, ACCORDING TO THE RECORDED PLAT THEREOF; THENCE NORTHEASTERLY 89.57 FEET ALONG THE ARC OF A CURVE TO THE LEFT, HAVING A RADIUS OF 100.00 FEET AND A CENTRAL ANGLE OF 51° 19' 04", SAID ARC HAVING A CHORD BEARING NORTH 62° 46' 08" EAST, 86.60 FEET TO A POINT OF REVERSE CURVATURE; THENCE NORTHEASTERLY 49.83 FEET ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 60.00 FEET AND A CENTRAL ANGLE OF 47° 35' 02", SAID ARC HAVING A CHORD BEARING NORTH 60° 54' 07" EAST, 48.41 FEET; THENCE SOUTH 00° 37' 37" EAST, 59.87 FEET; THENCE SOUTH 88° 25' 26" WEST, 120.00 FEET TO THE POINT OF BEGINNING.

AND

LOT TWENTY-SEVEN (27), FIRST ADDITION TO CAPITOL INDUSTRIAL PARK, IN THE CITY OF MADISON, DANE COUNTY, WISCONSIN, EXCEPT THE EASTERLY 100.47 FEET THEREOF, AND FURTHER EXCEPT THOSE LANDS CONVEYED TO THE CITY OF MADISON, A WISCONSIN MUNICIPAL CORPORATION, IN WARRANTY DEED RECORDED DECEMBER 13, 1994 IN VOLUME 29019 OF RECORDS, PAGE 38 AS DOCUMENT NUMBER 2650243.

AND

A PART OF VACATED REEF COURT (ROAD), VACATED PER RESOLUTION RECORDED DECEMBER 14, 1994 IN VOLUME 29028 OF RECORDS, PAGE 28 AS DOCUMENT NUMBER 2650467, DESCRIBED AS FOLLOWS: PART OF THE SOUTHWEST 1/4 OF THE SOUTHEAST 1/4 OF SECTION 15, TOWNSHIP 7 NORTH, RANGE 10 EAST, IN THE CITY OF MADISON, DANE COUNTY, WISCONSIN, MORE FULLY DESCRIBED AS FOLLOWS: BEGINNING AT A POINT OF CURVATURE AT THE SOUTHEAST CORNER OF LOT 26 OF THE PLAT OF FIRST ADDITION TO CAPITOL

INDUSTRIAL PARK, ACCORDING TO THE RECORDED PLAT THEREOF; THENCE SOUTHEASTERLY 98.14 FEET ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 60.00 FEET AND A CENTRAL ANGLE OF 93° 43′ 50″, SAID ARC HAVING A CHORD BEARING SOUTH 48° 26′ 45″ EAST, 87.56 FEET; THENCE SOUTH 88° 25′ 26″ WEST, 64.90 FEET; THENCE NORTH 00° 37′ 37″ WEST, 59.87 FEET TO THE POINT OF BEGINNING.

LEGAL DESCRIPTION

AS SURVEYED

ALL OF LOT 26 AND PART OF LOT 27, FIRST ADDITION TO CAPITAL INDUSTRIAL PARK RECORDED ON JULY 22, 1985 IN VOLUME 55 OF PLATS ON PAGES 33-34, AS DOCUMENT NO. 1890326 AND PART OF VACATED REEF COURT AS DESCRIBED IN RESOLUTION RECORDED DECEMBER 14, 1994 IN VOLUME 29028 OF RECORDS ON PAGES 28-31, AS DOCUMENT NO. 2650467, BEING PART OF THE SOUTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 15, TOWNSHIP 7 NORTH, RANGE 10 EAST, LOCATED IN THE CITY OF MADISON, DANE COUNTY, WISCONSIN. DESCRIBED MORE PARTICULARLY AS FOLLOWS:

COMMENCING AT THE SOUTH QUARTER CORNER OF AFORESAID SECTION 15, THENCE ALONG THE SOUTH LINE OF THE SOUTHEAST QUARTER OF SAID SECTION 15 NORTH 88 DEGREES 49 MINUTES 07 SECONDS EAST, 880.82 FEET; THENCE NORTH 01 DEGREE 10 MINUTES 53 SECONDS WEST TO A POINT ON THE EASTERLY SIDE OF THE CURVING RIGHT-OF-WAY OF REEF COURT, BEING THE SOUTHEASTERLY CORNER OF THE LANDS DESCRIBED IN WARRANTY DEED RECORDED ON DECEMBER 13, 1994 IN VOLUME 29019 OF DEEDS ON PAGES 38 AND 39 AS DOCUMENT NO. 2650243 AND THE POINT OF BEGINNING;

THENCE NORTHERLY, NORTHWESTERLY AND WESTERLY ALONG THE RIGHT-OF-WAY OF REEF COURT 88.61 FEET ON THE ARC OF A CURVE TO THE LEFT, HAVING A RADIUS OF 50.00 FEET AND A LONG CHORD THAT BEARS NORTH 40 DEGREES 23 MINUTES 07 SECONDS WEST, 77.46 FEET TO A POINT OF TANGENT; THENCE CONTINUING ALONG THE SAID RIGHT-OF-WAY OF REEF COURT SOUTH 88 DEGREES 50 MINUTES 12 SECONDS WEST, 199.49 FEET TO THE WEST LINE OF AFORESAID LOT 26; THENCE ALONG SAID WEST LINE OF LOT 26 NORTH 00 DEGREES 11 MINUTES 25 SECONDS WEST, 375.41 FEET TO SOUTHERLY RIGHT-OF-WAY LINE OF MUSTANG WAY AND NORTHWEST CORNER OF SAID LOT 26; THENCE ALONG SAID RIGHT-OF-WAY OF MUSTANG WAY NORTH 80 DEGREES 58 MINUTES 52 SECONDS BEAST, 102.00 FEET TO A POINT OF TANGENT CURVE; THENCE CONTINUING NORTHEASTERLY ALONG SAID RIGHT-OF-WAY OF MUSTANG WAY 132.99 FEET ON THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 970.00 FEET AND A LONG CHORD THAT BEARS NORTH 84 DEGREES 54 MINUTES 32 SECONDS EAST, 132.89 FEET TO A POINT OF TANGENT; THENCE CONTINUING ALONG SAID RIGHT-OF-WAY OF MUSTANG WAY NORTH 88 DEGREES 50 MINUTES 12 SECONDS EAST, 166.41 FEET; THENCE SOUTH 00 DEGREES 10 MINUTES 09 SECONDS EAST, 457.96 FEET TO THE SOUTH LINE OF AFORESAID LOT 27; THENCE ALONG SAID SOUTH LINE OF LOT 27 SOUTH 88 DEGREES 38 MINUTES 29 SECONDS WEST, 149.99 FEET BACK TO THE POINT OF BEGINNING.

SURVEYOR'S CERTIFICATE

I, ZACHARY M. REYNOLDS, WISCONSIN PROFESSIONAL LAND SURVEYOR S-3223, DO HEREBY CERTIFY THAT BY DIRECTION OF KSW CONSTRUCTION CORPORATION, I HAVE SURVEYED, DIVIDED, AND MAPPED THE LANDS DESCRIBED HEREON AND THAT THE MAP IS A CORRECT REPRESENTATION IN ACCORDANCE WITH THE INFORMATION PROVIDED. I FURTHER CERTIFY THAT THIS CERTIFIED SURVEY MAP IS IN FULL COMPLIANCE WITH CHAPTER 236.34 OF THE WISCONSIN STATUTES AND THE SUBDIVISION REGULATIONS OF THE CITY OF MADISON AND DANE COUNTY, WISCONSIN.

ZACHARY M. REYNOLDS, S-3223 WISCONSIN PROFESSIONAL LAND SURVEYOR DATE





PREPARED BY: WYSER ENGINEERING 300 EAST FRONT STREET MOUNT HOREB, WI 53572 www.wyserengineering.com PREPARED FOR:

KSW CONSTRUCTION CORP. 1100 JOHN P LIVESEY BLVD VERONA, WI 53593 SURVEYED BY: ZMR/MAL DRAWN BY: JWS APPROVED BY: ZMR

PROJECT NO: 17-0477
SHEET NO: 4 of 6

VOL. _____ PAGE _____

DOC. NO. ____

C.S.M. NO. ____

CERTIFIED SURVEY MAP NO
ALL OF LOT 26 AND PART OF LOT 27, FIRST ADDITION TO CAPITAL INDUSTRIAL PARK RECORDED ON JULY 22, 1985 IN VOLUME 55 OF PLATS ON PAGES 33-34, AS DOCUMENT NO. 1890326 AND PART OF VACATED REEF COURT AS DESCRIBED IN RESOLUTION RECORDED DECEMBER 14 1994 IN VOLUME 29028 OF RECORDS ON PAGES 28-31, AS DOCUMENT NO. 2650467, BEING PART OF THE SOUTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 15, TOWNSHIP 7 NORTH, RANGE 10 EAST, LOCATED IN THE CITY OF MADISON, DANE COUNTY, WISCONSING
OWNER'S CERTIFICATE
H&E DEVELOPMENT, LLC, AN INDIANA LIMITED LIABILITY COMPANY, AS OWNER, WE HEREBY CERTIFY THAT WE CAUSED THE LANDS DESCRIBED HEREON TO BE SURVEYED, DIVIDED MAPPED AND DEDICATED AS SHOWN. I ALSO CERTIFY THAT THIS CERTIFIED SURVEY MAP IS REQUIRED BY S. 236.34 OF THE WISCONSIN STATE STATUES TO BE SUBMITTED TO THE CITY OF MADISON FOR APPROVAL.
BY: MANAGING MEMBER H&E DEVELOPMENT, LLC
STATE OF WISCONSIN) SS DANE COUNTY) SS
PERSONALLY CAME BEFORE ME THIS DAY OF, 2024, THE ABOVE NAMED
MANAGING MEMBER FOR H&E DEVELOPMENT, LLC,TO ME KNOWN TO BE THE
PERSON WHO EXECUTED THE FOREGOING INSTRUMENT, AND ACKNOWLEDGED THE SAME.
NOTARY PUBLIC, STATE OF WISCONSIN MY COMMISSION EXPIRES
CONSENT OF MORTGAGEE
ASSOCIATED BANK, NATIONAL ASSOCIATION, MORTAGAGEE OF THE ABOVE DESCRIBED LAND, DOES HEREBY CONSENT TO THE SURVEY, DIVIDING, MAPPING AND DEDICATION OF THE LAND DESCRIBED ON THIS CERTIFIED SURVEY MAP AND DOES HEREBY CONSENT TO THE OWNER'S CERTIFICATE.
BY: AUTHORIZED OFFICER
STATE OF WISCONSIN) SS DANE COUNTY) SS
PERSONALLY CAME BEFORE ME THIS DAY OF, 2024, THE ABOVE
NAMED BANKING ASSOCIATION , ASSOCIATED BANK, NATIONAL ASSOCIATION, AUTHORIZED OFFICER
TO ME KNOWN TO BE THE PERSON WHO EXECUTED THE FOREGOING INSTRUMENT, AND ACKNOWLEDGED THE SAME.
NOTARY PUBLIC, STATE OF WISCONSIN MY COMMISSION EXPIRES REYNOLDS S-3223 WISCONSIN WISCONSIN WISCONSIN WISCONSIN WISCONSIN WISCONSIN WISCONSIN WISCONSIN WISCONSIN
DREPARED BY: DREPARED FOR: VOLPAGE
PREPARED BY: WYSER ENGINEERING 800 EAST FRONT STREET MOUNT HOREB, WI 53572 MWw.wyserengineering.com WYSER ENGINEERING NOUNT HOREB, WI 53572 Www.wyserengineering.com WYSER ENGINEERING NOUNT HOREB, WI 53572 Www.wyserengineering.com VOLPAGE PREPARED BY: KSW CONSTRUCTION CORP. 1100 JOHN P LIVESEY BLVD VERONA, WI 53593 APPROVED BY: ZMR APPROVED BY: ZMR SURVEYED BY: ZMR/MAL PROJECT NO: 17-0477 SHEET NO: 5 of 6 C.S.M. NO

#X20777 Tod77_Kilen - Oakhouse Form Bakery/DWC\2021 Design\17-0477_Reef Court CSM.dwg Layout: CSM 5 OF 6 User: Zach Plotted: May 09, 2024 - 10:27am

ALL OF LOT 28 AND PART OF LOT 27, FIRST ADDITION TO CAPITAL INDUSTRIAL PARK RECORDED ON JULY 22, 1985 IN VOLUME 55 OF PLATS ON PAGES 33-34, AS DOCUMENT NO. 389326 AND PART OF VACATED REFE COURT AS DESCRIBED IN RESOLUTION RECORDED DECEMBER 14, 1994 IN VOLUME 29028 OF RECORDS ON PAGES 28-31, AS DOCUMENT NO. 2659467, BEING PART OF THE SOUTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 15, TOWNSHIP 7 NORTH, RANGE 10 EAST, LOCATED IN THE CITY OF MADISON, DANE COUNTY, WISCONSIN. MADISON COMMON COUNCIL CERTIFICATE RESOLVED THAT THIS CERTIFIED SURVEY MAP LOCATED IN THE CITY OF MADISON WAS HERBY APPROVED BY ENACTMENT NUMBER FILE ID NUMBER ADOPTED ON THIS DAY OF, 2024. DATED THIS DAY OF, 2024. MARIBETH WITZEL-BEHL, CITY CLERK, CITY OF MADISON PLAN COMMISSION CERTIFICATE APPROVED FOR RECORDING PER THE SECRETARY OF THE CITY OF MADISON PLAN COMMISSION. MATTHEW WACHTER, DATE: SECRETARY OF THE PLAN COMMISSION		
ON PAGES 33-3., AS DOCUMENT NO. 18903Z6 AND PART OF VACATED REEF COURT AS DESCRIBED IN RESOLUTION RECORDED DECEMBER 14, 1994 IN VOLUME 2982 OF RECORDS ON PAGES 28-31, AS DOCUMENT NO. 2659467, BURNO PARTO FT SOUTHWEST QUARTER OF THE SOUTHWEST OF PRECIDENCE PROCESSOR. MADISON COMMON COUNCIL CERTIFICATE RESOLVED THAT THIS CERTIFIED SURVEY MAP LOCATED IN THE CITY OF MADISON WAS HERBY APPROVED BY ENACTMENT NUMBER, FILE ID NUMBER, ADOPTED ON THIS DAY OF, 2024. DATED THIS DAY OF, 2024. MARIBETH WITZEL-BEHL, CITY CLERK, CITY OF MADISON CERTIFICATE APPROVED FOR RECORDING PER THE SECRETARY OF THE CITY OF MADISON PLAN COMMISSION. MATTHEW WACHTER, DATE:	CERTIFIED SURVEY MAP NO	
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APPROVED FOR RECORDING PER THE SECRETARY OF THE CITY OF MADISON PLAN COMMISSION. MATTHEW WACHTER, DATE:		
MATTHEW WACHTER, DATE:	CITY OF MADISON PLAN COMMISSION CERTIFICATE	
	APPROVED FOR RECORDING PER THE SECRETARY OF THE CITY OF MADISON PLAN COMMISSION.	
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OFFICE OF THE REGISTER OF DEEDS							
COUNTY, WISCONSIN							
RECEIVED FOR RECORD							
20ATO'CLOCKM AS							
DOCUMENT#							
IN VOLOF CERTIFIED SURVEY							
MAPS ON PAGE(S)							
KRISTI CHLEBOWSKI, REGISTER OF DEEDS							

WYSER

File: W: 1720

PREPARED BY:
WYSER ENGINEERING
300 EAST FRONT STREET
MOUNT HOREB, WI 53572
www.wyserengineering.com

PREPARED FOR:

KSW CONSTRUCTION CORP. 1100 JOHN P LIVESEY BLVD VERONA, WI 53593 SURVEYED BY: ZMR/MAL DRAWN BY: JWS

APPROVED BY: ZMR

PROJECT NO: 17-0477
SHEET NO: 6 of 6

421



Department of Planning & Community & Economic Development

Planning Division

William Fruhling, Interim Director

Madison Municipal Building, Suite 017 215 Martin Luther King Jr. Boulevard Madison, Wisconsin 53703 Phone: (608) 266-4635 www.cityofmadison.com

BY E-MAIL ONLY

July 5, 2024

Zach Reynolds
Wyser Engineering
300 E Front Street
Mount Horeb, Wisconsin 53572

RE: LNDCSM-2024-00024; ID 83528 – Certified Survey Map – 2221 Mustang Way and 5026 Reef Court (H & E Development, LLC)

Dear Zach;

The one-lot Certified Survey Map to combine property located at 2221 Mustang Way and 5026 Reef Court, Section 15, Township 7N, Range 10E, City of Madison, Dane County, Wisconsin, is hereby **conditionally approved**. The property is zoned IL (Industrial–Limited District). The conditions of approval from the reviewing agencies to be satisfied prior to final approval and recording of the CSM are:

Please contact Brenda Stanley of the City Engineering Division at (608) 261-9127 if you have questions regarding the following two (2) items:

- 1. Madison Metropolitan Sewerage District (MMSD) charges are due and payable prior to City Engineering Division sign-off, unless otherwise collected with a Developer's / Subdivision Contract. Contact Mark Moder ((608) 261-9250) toobtain the final MMSD billing a minimum of two (2) working days prior to requesting City Engineering sign-off.
- 2. A minimum of two (2) working days prior to requesting City Engineering Division sign-off on the CSM, contact eitherTim Troester (West) at 608-261-1995 (ttroester@cityofmadison.com) or Brenda Stanley (East) at 608-261-9127 (bstanley@cityofmadison.com) to obtain the final stormwater utility charges that are due and payable prior to sub-division of the properties. The stormwater utility charges (as all utility charges) are due for the previous months of service and must be cleared prior to the land division (and subsequent obsolesces of the existing parcel).

Please contact Julius Smith of the City Engineering Division–Mapping Section at (608) 264-9276 if you have questions regarding the following fourteen (14) items:

3. Any portion(s) of a public easement that is intended to be released shall be released by separate document prepared by City Office of Real Estate Services. Contact Jule Smith of Engineering Mapping (jsmith4@cityofmadison.com) to coordinate the Real Estate project, and associated information and fees required. If any release is required prior to recording of the plat, acknowledgement of the release and document number shall be noted on the face of the plat. The concurrent proposed development

is located over a public utility easement the will require release documents be filed by both the public utilities that serve the area and the City.

- 4. Provide for review, comprehensive reciprocal easements and agreements including, but not limited to, access, ingress egress, and storm management that are necessary to accomplish the land division or site development as proposed prior to final sign off. The document(s) shall be executed and recorded immediately subsequent to the CSM recording and prior to building permit issuance. There is an existing concrete drive apron shown at the Southeast corner of the parcel that appears to connect to the pavement and give large trucks better access to a loading dock on 2326 Vondron Road, which is all of Lot 31 and part of Lot 27 of First Addition to Capital Industrial Park. This appears to be used to allow trucks to back into the dock on the adjacent property. Provide necessary agreements to formalize any rights need for the present uses.
- 5. Coordinate with Madison Gas and Electric for the required releases of the easements per Document Nos. 2603708 and 2681237.
- 6. Show the 12-foot public utility easement per Capital Industrial Park recorded as Document No. 1333368 split along Lot 16 and what was Lot 17 and is now Lot 26, First Addition to Capital Industrial Park. Note the portion that is located in Lot 26 overlaps the public utility easement as set forth in said First Addition.
- 7. Confirm accuracy of title work. There are easements shown on CSM that are not provided or noted in the title work as being recorded against the property and other easements that do not affect, as they are not the same lots. Title work should have had Document No. 1364131 listed, Public Sanitary Sewer Easement it was recorded in the last 60 years. Provide revised title report with a complete and accurate search which would result in Document No. 1364131 being provided and without Document No 1668675 along with and all other recorded documents affecting the property.
- 8. Wisconsin Administrative Code A-E 7.08 identifies when Public Land System (PLS) tie sheets must be filed with the Dane County Surveyor's office. The developer's surveyor and/or applicant shall submit copies of required tie sheets or monument condition reports (with current tie sheet attached) for all monuments, including center of sections of record, used in this survey, to Jule Smith (jsmith4@cityofmadison.com).
- 9. Prior to Engineering final sign-off for Certified Survey Maps (CSM), the final CSM shall be submitted in PDF format by email to City Engineering Division Land Records Coordinator Jule Smith (jsmith4@cityofmadison.com) for final technical review and approval. This submittal must occur a minimum of two working days prior to final Engineering Division sign-off.
- 10. Remove (Road) from the Reef Court street name. It may be referenced with separate less prominent text as 'Platted as Reef Road', as is show in all of the surrounding plats, but it has been officially renamed.
- 11. Note that the 6-foot portion of the MG&E Right-of-Way Grant Underground Electric is per Document No. 2681237 and not 2603708.

- 12. In the second line of the second paragraph of the as Surveyed Legal Description, after NORTH 01 DEGREE 10 MINUTES 53 SECONDS WEST add 532.91 Feet. This should match L-1 in the line table.
- 13. In the second line of the third paragraph of the as Surveyed Legal Description the bearing listed for the chord is NORTH 40 DEGREES 23 MINUTES 07 SECONDS WEST, in the Curve table this is listed as N 40° 23' 41"W. Make a correction of one of the bearings.
- 14. Use the updated Common Council Certificate to be provided by the City's Office of Real Estate Services.
- 15. The pending Certified Survey Map application for this property shall be completed and recorded with the Dane County Register of Deeds, the new parcel data created by the Assessor's Office, and the parcel data available to Zoning and Building Inspection staff prior to issuance of building permits and early start permits for new construction.
- 16. The applicant shall submit to Julius Smith, prior to final Engineering sign-off of the subject CSM, one (1) digital CADD drawing in a format compatible with AutoCAD. The digital CADD file(s) shall be referenced to the Dane County Coordinate System and shall contain, at minimum, the list of items stated below, each on a separate layer/level name. The line work shall be void of gaps and overlaps and match the final recorded CSM: right of way lines (public and private); lot lines; lot numbers; lot/plat dimensions; street names, and; easement lines (including wetland and floodplain boundaries).

*This transmittal is a separate requirement than the required submittals to Engineering Streets Section for design purposes. The Developer/Surveyor shall submit new updated final plat, electronic data and a written notification to Engineering Mapping for any changes to the plat which occur subsequent to any submittal.

Please contact the Melissa Hermann Office of Real Estate Services at mhermann@cityofmadison.com if you have any questions regarding the following eleven (11) items:

- 17. Prior to approval sign-off by the Office of Real Estate Services ("ORES"), the Owner's Certificate(s) on the CSM shall be executed by all parties of interest having the legal authority to do so, pursuant to Wis. Stats. 236.21(2)(a). Said parties shall provide documentation of legal signing authority to the notary or authentication attorney at the time of execution. The title of each certificate shall be consistent with the ownership interest(s) reported in the most recent title report.
- 18. Prior to CSM approval sign-off, an executed and notarized or authenticated certificate of consent for all mortgagees/vendors shall be included following the Owner's Certificate(s).
- 19. If a mortgage or other financial instrument is reported in record title, but has been satisfied or no longer encumbers the lands or ownership within the CSM boundary, a copy of a recorded satisfaction or release document for said instrument shall be provided prior to CSM approval sign-off.
- 20. All ownership consents and certifications for the subject lands shall conform to Wis. Stats. 236.21(2) and 236.29 by including the language "...surveyed, divided, mapped and <u>dedicated</u>..."

- 21. If any portion of the lands within the CSM boundary are subject to an Option to Purchase or other Option interest please include a Certificate of Consent for the option holder and have it executed prior to CSM sign-off, if said ownership interest meets the criteria set forth by Wis. Stat. Sec. 236.34 and Sec. 236.21(2)(a).
- 22. A Consent of Lessee certificate shall be included on the CSM for all tenant interests in excess of one year, recorded or unrecorded, and executed prior to CSM sign-off.
- 23. <u>Madison Common Council Certificate:</u> This certificate is required when dedication of land and the conveyance of rights in land are required. For parcels located within the City of Madison, a Madison Common Council Certificate shall appear as follows:

tesolved that this certified survey map located in the City of Madison was hereby approved by nactment Number, File ID Number, adopted on the day of, 20, and that said enactment further provided for the acceptance of those lands ledicated and rights conveyed by said Certified Survey Map to the City of Madison for public use.
Pated this day of, 2024
Maribeth L. Witzel-Behl, City Clerk City of Madison, Dane County Wisconsin

24. <u>Register of Deeds Certificate</u>: Please include a space for the Register to hand write the recording info on the date of recording, to appear similar to the following:

Off	fice of the Regist	er of D	eeds		
	Dane County, W	/iscons	in		
Received for recording of	on	_, 20	at	o'clock_	_M, and
recorded in Volumeof (CSMs on page(s)		_, Docur	ment No	
				_	
Kristi Chlebowski, Register of Deeds					

- 25. Per 236.21(3) Wis. Stats. and MGO Section 16.23(4)(f), the property owner shall pay all real estate taxes that are accrued or delinquent for the subject property prior to CSM recording. As of <u>June 14</u>, <u>2024</u>, there are real estate taxes due on 2221 Mustang Way of \$12,318.69, and \$822.55 on 5026 Reef Court.
- 26. Pursuant to MGO Section 16.23(4)(f), the owner shall furnish an updated title report to the ORES as well as the survey firm preparing the proposed CSM. The report shall search the period subsequent to the date of the initial title report submitted with the CSM application and include all associated documents that have been recorded since the initial title report. A title commitment may be provided, but will be considered only as supplementary information to the title report update. The surveyor shall update the CSM with the most recent information reported in the title update. ORES reserves

the right to impose additional conditions of approval in the event the title update contains changes that warrant revisions to the CSM.

- 27. Revise the CSM prior to final approval as follows:
 - a) Depict, name, and identify by document number all existing easements cited in record title and the updated title report.
 - b) Depict and dimension public easements for utilities and storm water drainage rights-of-way to be dedicated on the proposed CSM where necessary.
 - c) If all parties of interest agree that certain easements from prior plats or CSM's of record are no longer necessary, the release documents for said easements shall be recorded prior to CSM approval sign-off, with the recording information for the release included as a Note on the proposed CSM.
 - d) Liens or judgments levied against the lands within the CSM boundary shall be satisfied, with proof of satisfaction provided prior to CSM approval sign-off.
 - e) Create notes that define the purpose of and the ownership of (whether public or private) all outlots. The note for an outlot dedicated to the public shall say: "Dedicated to the public for ______ purposes."

Please contact Trent W. Schultz of the Parking Division at (608) 246-5806 if you have any questions regarding the following item:

28. The agency reviewed this project and determined a Transportation Demand Management (TDM) Plan is not required as part of certified survey map review. A TDM Plan may be required as part of future development, if additional parking stalls are proposed.

Specific questions regarding the comments or conditions contained in this letter should be directed to the commenting agency.

A resolution approving the Certified Survey Map and authorizing the City to sign it and any other documents related to the CSM will be reviewed by the Common Council at its <u>July 16, 2024</u> meeting.

In order to commence the process for obtaining the necessary City signatures on the Certified Survey Map, the applicant shall e-mail the revised CSM, updated title report, and any other materials required by reviewing agencies to the reviewing planner. The reviewing planner will share the updated materials with the relevant commenting City agencies for them to verify that their conditions have been satisfied and that the secretary or designee may sign the Plan Commission approval certificate. Once the Plan Commission certificate is executed, the Planning Division will make the City Clerk's Office aware that the Common Council certificate may be executed.

Once all of the necessary City signatures have been affixed to the Certified Survey Map, the instrument may be recording at the Dane County Register of Deeds Office. For information on recording procedures and fees, please contact the Register of Deeds at (608) 266-4141.

Any appeal from this action, including the conditions of approval, must be filed with the Circuit Court within 30 days from the date of this letter. The approval of this CSM shall be null and void if not recorded in twelve (12) months from the date of the approving resolution or this letter, whichever is later.

If you have any questions or if may be of any further assistance, please do not hesitate to contact my office at (608) 261-9632 or tparks@cityofmadison.com.

Sincerely,

Timothy M. Parks

TimethyMParks

Planner

cc: Brenda Stanley, City Engineering Division

Julius Smith, City Engineering Division—Mapping Section

Melissa Hermann, Office of Real Estate Services

Trent Schultz, Parking Division



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84372

File ID: 84372 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: Department of

Planning and Community and Economic Development

File Created Date: 07/09/2024

Enactment Number:

File Name: CSM Re-Approval - 4002 Lien Road Final Action:

Title: Re-Approving a Certified Survey Map of property owned by the Islamic Center

of East Madison Corporation located at 4002 Lien Road (District 17).

Notes:

Sponsors: Planning Division Effective Date:

Attachments: Originally Approved CSM 09-15-22.pdf, Conditional

Approval Letter_10-18-22.pdf, Updated CSM 07-03-24.pdf, Link to Resolution ID 73824

Author: Hearing Date: 07/16/2024

Entered by: cpunt@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Department of Pl and Community a Economic Develo	and	RECOMMEND TO COUNCIL TO ADOPT UNDER SUSPENSION OF MGO 2.055 - REPORT OF OFFICER				Pass
	Action Text: This Resolution was RECOMMEND TO COUNCIL TO ADOPT UNDER SUSPENSION OF MGO 2.055 - REPORT OF OFFICER Notes: The proposed Certified Survey Map has been administratively re-approved as allowed by MGO Section 16.23(4)(f) subject to the conditions included in the attached letter. Staff recommends adoption of the resolution.						;

Text of Legislative File 84372

Fiscal Note

No City appropriation is required with the re-approval of this certified survey map. City costs associated with urban development in this area will be included in future operating and capital budgets subject to Common Council approval.

Title

Re-Approving a Certified Survey Map of property owned by the Islamic Center of East Madison Corporation located at 4002 Lien Road (District 17).

Body

WHEREAS a Certified Survey Map of property owned by the Islamic Center of East Madison Corporation located at 4002 Lien Road, City of Madison, Dane County, Wisconsin has been duly filed for approval by the Plan Commission, its Secretary or their designee, as provided for in Section 16.23(4)(f) of Madison General Ordinances; and

WHEREAS the Common Council previously approved this Certified Survey Map by Resolution 22-00751 (ID 73824) on November 1, 2022; and

WHEREAS Chapter 236, Wisconsin Statutes requires that the Madison Common Council approve any dedications proposed or required as part of the proposed division of the lands contained on said Certified Survey Map;

WHEREAS Wis. Stat. 236.34(2)(b) requires that a Certified Survey Map be offered for recording within twelve (12) months of the approval of same;

NOW THEREFORE BE IT RESOLVED that said Certified Survey Map, bond and subdivision contract, subsequent affidavits of correction, parkland acquisition documents, easement or right-of-way release or procurement documents or any other related document or documents as deemed necessary by the Secretary of the Plan Commission in accordance with the approval of said Certified Survey Map are hereby re-approved by the Madison Common Council subject to the prior conditions of approval.

BE IT FURTHER RESOLVED that the Mayor and City Clerk of the City of Madison are hereby authorized to sign the above mentioned documents related to this Certified Survey Map.

BE IT FURTHER RESOLVED that all dedications included in this Certified Survey Map or required as a condition of approval of this Certified Survey Map be and are hereby accepted by the City of Madison.

BE IT FURTHER RESOLVED that the Planning Division is authorized to reflect the recorded Certified Survey Map in the Comprehensive Plan and any applicable neighborhood plans.

DRAFT

DANE COUNTY **CERTIFIED SURVEY MAP #**

Record Data per

Boundary Line

Section Line

- Street Centerline

Existing R/W Line Platted Lot Line

Melvin A. Randall Plat

– Existing Easement Limits

Record Data per CSM 1510

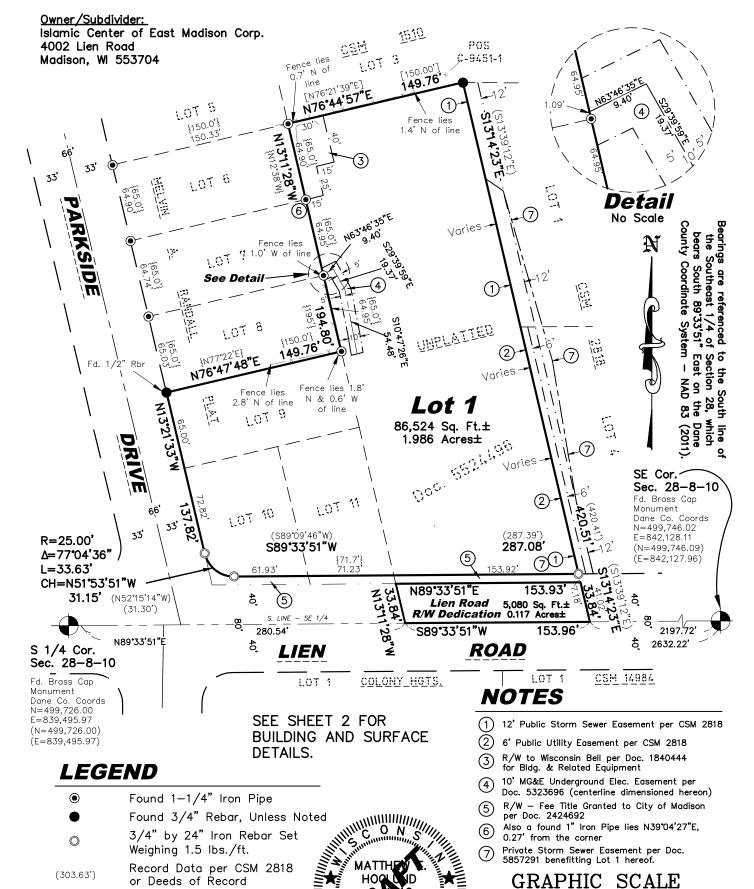
{303.63'}

[303.63']

Lot 9 and a part of Lots 10 and 11 of the Melvin A. Randall Plat and a part of the SW 1/4 of the SE 1/4, all of Section 28, T8N., R10E., City of Madison, Dane County, Wisconsin.

C.S.M. No. Doc. No.

Vol. _____ Page _



(IN FEET)

40

80

QUAM ENGINEERING, LLC

4604 SIGGLEKOW ROAD - SUITE A McFARLAND, WI 53558

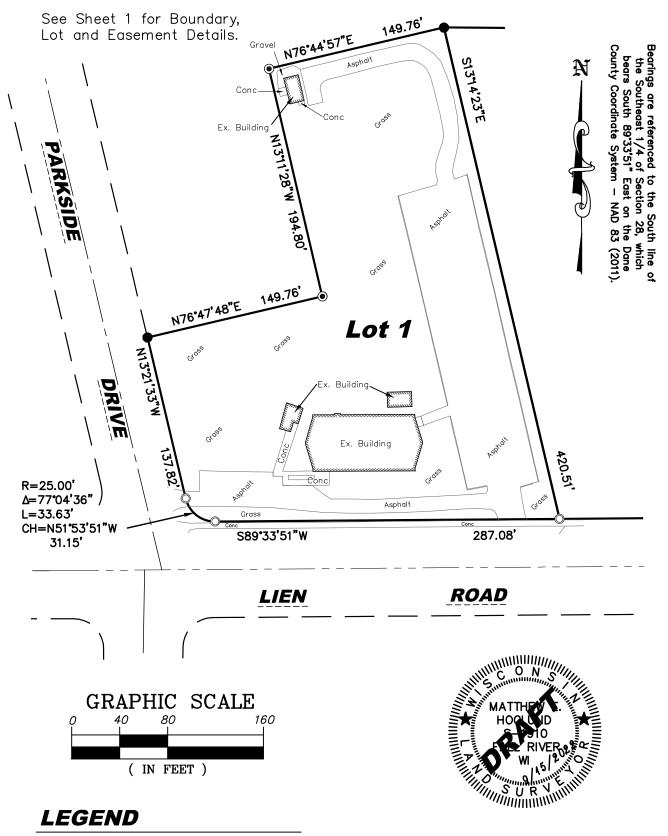
608-838-7750 www.quamengineering.com Project # IC-06-19 SHEET 1 OF 5

160

DANE COUNTY CERTIFIED SURVEY MAP

Lot 9 and a part of Lots 10 and 11 of the Melvin A. Randall Plat and a part of the SW 1/4 of the SE 1/4, all of Section 28, T8N., R10E., City of Madison, Dane County, Wisconsin.

BUILDING & SURFACE DETAILS



- Found 1-1/4" Iron Pipe
- Found 3/4" Rebar, Unless Noted
- 3/4" by 24" Iron Rebar Set 0 Weighing 1.5 lbs./ft.

Boundary Line

Section Line - Street Centerline

Existing R/W Line

QUAM ENGINEERING, LLC

Owner/Subdivider:

4002 Lien Road

Madison, WI 553704

4604 SIGGLEKOW ROAD - SUITE A McFARLAND, WI 53558
608-838-7750 www.quamengineering.com
Project # IC-06-19

Islamic Center of East Madison Corp.

SHEET 2 OF 5



Drawn By: MEH Project # IC-06-19

DANE COUNTY CERTIFIED SURVEY MAP

Lot 9 and a part of Lots 10 and 11 of the Melvin A. Randall Plat and a part of the SW 1/4 of the SE 1/4, all of Section 28, T8N., R10E., City of Madison, Dane County, Wisconsin.

SURVEYOR'S CERTIFICATE:

I, Matthew E. Hoglund, Registered Land Surveyor, do hereby certify to the best of my knowledge and belief that I have surveyed, divided, and mapped the following Certified Survey, bLot 9 and a part of Lots 10 and 11 of the Melvin A. Randall Plat, recorded in Volume 13 of Plats on Page 4 as Document No. 763928 of Dane County Records and part of the Southwest one-quarter of the Southeast one-quarter of Section 28, Township 8 North, Range 10 East, City of Madison, Dane County, Wisconsin, being more particularly described as follows:

COMMENCING the South one-quarter corner of said Section 28; thence, along the South line of said Southeast one-quarter, North 89°33'51" East, 434.51 feet to its intersection with the Southeasterly prolongation of the Westerly line of Certified Survey Map No. 2818; thence, along said Southeasterly prolongation, North 13°14'23" West, 41.02 feet to the **POINT OF BEGINNING** lying on the Northerly right-of-way of Lien Road per Document No. 2424692;

thence, along said Northerly right-of-way line, South 89°35'51" West, 287.08 feet to the beginning of a tangent curve, being concave Northeasterly, having a radius of 25.00 feet and a chord which bears North 51°53'51" West, 31.15 feet;

thence Northwesterly, 33.63 feet along the arc of said curve through a central angle of 77°04'36" to the Point of Tangency thereof, lying on the Easterly right-of-way line of Parkside Drive;

thence, along said Easterly right-of-way line, North 13°21'33" West, 137.82 feet to the Northwest corner of aforesaid Lot 9;

thence, along the Northerly line of said Lot 9, North 76°47'48" East, 149.76 feet to the Northeast corner of said Lot 9;

thence, along the Easterly line of said Melvin A. Randall Plat, North 13°11'28" West, 194.80 feet to the Northeast corner of Lot 6 of said Melvin A. Randall Plat, said corner also being the Southwest corner of Lot 3 of Certified Survey Map No. 1510;

thence, along the Southerly line of said Lot 3, North 76°44'57" East, 149.76 feet to the Northwest corner of Lot 1 of aforesaid Certified Survey Map No. 2818;

thence, along aforesaid Westerly line of Certified Survey Map No. 2818, South 13°14'23" East, 420.51 feet to the **POINT OF BEGINNING**;

TOGETHER WITH that part of the Southwest one-quarter of the Southeast one-quarter of Section 28, Township 8 North, Range 10 East, City of Madison, Dane County, Wisconsin, being more particularly described as follows:

COMMENCING at the South one-quarter corner of said Section 28; thence, along the South line of said Southeast one-quarter, North 89°33'51" East, 280.54 feet to the Southeast corner of aforesaid Melvin A. Randall Plat and the **POINT OF BEGINNING**;

thence, along the Easterly line of said Plat, North 13°11'28" West, 33.84 feet to the South line of Document No. 2424692;

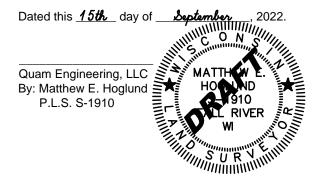
thence, along last said South line, North 89°33'51" East, 153.93 feet to its intersection with aforesaid Southeasterly prolongation of the Westerly line of Certified Survey Map No. 2818;

thence, along said Southeasterly prolongation, South 13°14'23" East, 33.84 feet to said South line of the Southeast one-quarter;

thence, along last said South line, South 89°33'51" West, 153.96 feet to the POINT OF BEGINNING.

Said parcel contains 91,604 square feet or 2.103 acres, more or less, and is subject to all easements and agreements, if any, of record and/or fact.

I further certify, to the best of my knowledge and belief, that this map is a correct representation of all exterior boundaries of the land surveyed and the subdivision thereof made; that I have made such survey, land division, and map by the direction of Oumar Keita of the Islamic Center of East Madison Corporation, the owner of said land; that I have fully complied with the provisions of Chapter 236.34 of the Wisconsin State Statutes and the City of Madison Subdivision Regulations in surveying, dividing, and mapping the same.



C.S.M. No. _____

DANE COUNTY CERTIFIED SURVEY MAP

Lot 9 and a part of Lots 10 and 11 of the Melvin A. Randall Plat and a part of the SW 1/4 of the SE 1/4, all of Section 28, T8N., R10E., City of Madison, Dane County, Wisconsin.

CORPORATE OWNERS CERTIFICATE:

Islamic Center of East Madison Corporation, a Wisconsin Non-Stock Corporation, duly organized and existing under and by virtue of the laws of the State of Wisconsin, as owner, does hereby certify that said company caused the land described on this Certified Survey Map to be surveyed, divided, mapped, and dedicated as represented on this map. Islamic Center of East Madison Corporation, does further certify that this map is required by s.236.10 or s.236.12 to be submitted to the following for approval or objection:

City of Madison

signed by		, its		and
·	, its	at		
on this,	day of	, 20		
In the presence of	: Print Name:		Title:	-
	Print Name:		Title:	
STATE OF WISCOCOUNTY DANE) Personally came to	SS	day of	. 20	_,
and who executed the and	of foregoing instru	the above named Corp ment, and to me knowi	oration, to ments or to be the nd acknowled	e known to be the person(s) Iged that they executed the
(Notary Seal)		Notary Pub	lic,	, Wisconsin
My commission ex	xpires			
REGISTER OF	DEEDS CERTI	FICATE:		
Received for recordi	ing this day of	f, 20	, at	o'clockM. and
recorded in Volume	of Certified	Survey Maps on Pages		
as Document No	·			MATTHEW LAND HOOLOND HOOLOND
Kristi Chlebowski, D	ane County Regis	ster of Deeds		RIVER W

DANE COUNTY CERTIFIED SURVEY MAP

Lot 9 and a part of Lots 10 and 11 of the Melvin A. Randall Plat and a part of the SW 1/4 of the SE 1/4, all of Section 28, T8N., R10E., City of Madison, Dane County, Wisconsin.

Approved for recording per the Plan Commission.	e Secretary of the City of Madison	
By: Secretary of the Plan Commis	Date sion	
Resolved, that this Certified Solved, that this Certified Solved	DMMON COUNCIL CERTIFICA urvey Map located in the City of Madi, File ID Number, 20, and said Enactment furth dicated and rights conveyed by said C	son was hereby approved, adopted on the her provided for the
Dated this day of		
Maribeth Witzel-Behl, City Cle City of Madison, Dane County		
	MATTHEW HOOL VID RIVER OF TO W 15 2 1 1 1 1 1 1 1 1 1	
		C.S.M. No

Vol. ____ Page __



Department of Planning & Community & Economic Development

Planning Division

Heather Stouder, Director

Madison Municipal Building, Suite 017 215 Martin Luther King, Jr. Blvd P.O. Box 2985 Madison, Wisconsin 53701-2985 Phone: (608) 266-4635 www.cityofmadison.com

October 18, 2022

Matthew Hoglund Quam Engineering, LLC 4604 Siggelkow Rd, Ste A McFarland, WI 53558

RE: LNDCSM-2022-00044; Legistar ID 73824 – Certified Survey Map – 4002 Lien Road

Dear Matthew Hoglund;

Your one-lot certified survey of property located at 4002 Lien Road, Section 28, Township 08N, Range 10E, City of Madison, Dane County, Wisconsin, is hereby **conditionally approved**. The property is zoned CC-T (Commercial Corridor-Transitional district). The conditions of approval from the reviewing agencies to be satisfied prior to final approval and recording of the CSM are:

Please contact Tim Troester of the City Engineering Division at 267-1995 if you have questions regarding the following four (4) items:

- 1. Enter into a City / Developer agreement for the required infrastructure improvements. Agreement to be executed prior to sign off. Allow 4-6 weeks to obtain agreement. Contact City Engineering to schedule the development and approval of the plans and the agreement. (MGO 16.23(9)c)
- 2. Construct sidewalk, terrace, curb & gutter and pavement along Parkside Dr to a plan as approved by City Engineer
- Madison Metropolitan Sewerage District (MMSD) charges are due and payable prior to Engineering sign-off, unless otherwise collected with a Developer's / Subdivision Contract. Contact Mark Moder (608-261-9250) to obtain the final MMSD billing a minimum of two (2) working days prior to requesting City Engineering signoff. (MGO 16.23(9)(d)(4)
- 4. A minimum of two (2) working days prior to requesting City Engineering signoff on the plat/csm contact either

Tim Troester (West) at 261-1995 (ttroester@cityofmadison.com) or Brenda Stanley (East) at 608-261-9127 (bstanley@cityofmadison.com) to obtain the final stormwater utility charges that are due and payable prior to sub-division of the properties. The stormwater utility charges (as all utility charges) are due for the previous months of service and must be cleared prior to the land division (and subsequent obsolesces of the existing parcel). (POLICY)

Please contact Julius Smith of the City Engineering Division—Mapping Section at 264-9276 if you have questions regarding the following fifteen (15) items:

- 5. Grant a Public Sidewalk Easement(s) to the City on the face of this Certified Survey Map along Parkside Drive to provide for expanded terraces. the location of this easement will be approved by City Engineering and Traffic Engineering. Contact Julius Smith (jsmith4@cityofmadison.com) with Engineering Mapping for the language to be included for this easement.
- 6. Provide missing support documents listed in title report.
- 7. A note shall be added under all of the street names labeled and to be dedicated on the CSM/plat, "Dedicated to the Public" as required by 236.20(4)(b). label the portion of Lein Road " Dedicated to the Public" for Street Purposes. Rather than R/W Dedication.
- 8. Wisconsin Administrative Code A-E 7.08 identifies when Public Land System (PLS) tie sheets must be filed with the Dane County Surveyor's office. The Developer's Surveyor and/or Applicant must submit copies of required tie sheets or monument condition reports (with current tie sheet attached) for all monuments, including center of sections of record, used in this survey, to Julius Smith, City Engineering (jsmith4@cityofmadison.com)
- 9. In accordance with Section s. 236.18(8), Wisconsin Statutes, the Applicant shall reference City of Madison WCCS Dane Zone, 1997 Coordinates on all PLS corners on the Plat or Certified Survey Map in areas where this control exists. The Surveyor shall identify any deviation from City Master Control with recorded and measured designations. Visit the Dane County Surveyor's Office (web address https://www.countyofdane.com/PLANDEV/records/surveyor.aspx) for current tie sheets and control data that has been provided by the City of Madison.
- 10. Prior to Engineering final sign-off by main office for Plats or Certified Survey Maps (CSM), the final Plat or CSM in pdf format must be submitted by email transmittal to Engineering Land Records Coordinator Julius Smith (jsmith4@cityofmadison.com) for final technical review and approval. This submittal must occur a minimum of two working days prior to final Engineering Division sign-off.
- 11. Add the (recorded as) bearing along the Parkside Drive Right-of-Way on the outer parcel boundary as shown in the overall block bearing on the Melvin A Randall Plat (N 12deg 38' W)
- 12. Tie all the existing buildings to the parcel lines if they are to remain. If they are to be removed label them as such.
- 13. The Sum of the two distances shown along the section line to the lead in line (280.54 + 153.96 = 434.50) and the distance listed in 7th line of the Surveyors Certificate 434.51 are not the same please revise a sub-distance or overall.
- 14. In the 2nd line of the Surveyors Certificate there is a "b" in front of Lot 9. It currently reads bLot 9. Remove the b.
- 15. The blanks shown on sheet 1 and on sheet 5 for CSM no., Doc no., Vol, and Page, are a preference of the Dane County Register of Deeds. It is believed this is desired on all sheets near the bottom

except for the sheet that will contain the recording information and ROD Signature. Please verify with the Dane County ROD.

- 16. Consider moving the ROD certificate to the last Sheet to allow for room for Notary Stamps. But leave the City of Madison signatures separated from other Wet signatures you may need to obtain prior to sign off.
- 17. Submit to Julius Smith, prior to Engineering sign-off of the subject plat, one (1) digital CADD drawing in a format compatible with AutoCAD. The digital CADD file(s) shall be referenced to the Dane County Coordinate System and shall contain, at minimum, the list of items stated below, each on a separate layer/level name. The line work, preferably closed polylines for lot lines, shall be void of gaps and overlaps and match the final recorded plat:
 - a. Right-of-Way lines (public and private)
 - b. Lot lines
 - c. Lot numbers
 - d. Lot/Plat dimensions
 - e. Street names
 - f. Easement lines (i.e. all shown on the plat including wetland & floodplain boundaries.)

NOTE: This Transmittal is a separate requirement from the required Engineering Streets Section for design purposes. The Developer/Surveyor shall submit new updated final plat, electronic data for any changes subsequent to any submittal.

Please contact Sean Malloy of the Traffic Engineering Division at 266-5987 if you have any questions regarding the following one (1) item:

18. The applicant shall dedicate Right of Way or grant a Public Sidewalk Easement for and be responsible for the construction of a five (5)-foot wide sidewalk, eight (8)-foot terrace, and additional one (1) foot for maintenance along Parkside Drive..

Please contact Jeff Belshaw of the Water Utility at 261-9835 if you have any questions regarding the following one (1) item:

19. The following information shall be noted on the CSM prior to final approval: The properties are located within Wellhead Protection District—Zone (WP-15). Uses of the properties are required to comply with the City of Madison Wellhead Protection requirements as provided under MGO Sections 13.22 and 28.102.

Please contact Heidi Radlinger of the Office of Real Estate Services at 266-6558 if you have any questions regarding the following ten (10) items:

- 20. Prior to approval sign-off by the Office of Real Estate Services, the Owner's Certificate on the CSM shall be executed by all parties of interest having the legal authority to do so, pursuant to Wis. Stats. 236.21(2)(a).
- 21. If any portion of the lands within the CSM boundary are subject to an Option to Purchase or other Option interest, include a Certificate of Consent for the option holder and have it executed prior to CSM sign-off, if said ownership interest meets the criteria set forth by Wis. Stat. Sec. 236.34 and Sec. 236.21(2)(a).

- 22. A Consent of Lessee certificate shall be included on the CSM for all tenant interests in excess of one year, recorded or unrecorded, and executed prior to CSM sign-off.
- 23. Matthew Wachter is Secretary of the Plan Commission
- 24. 2021 real estate taxes are paid for the subject property. Per 236.21(3) Wis. Stats. and 16.23(5)(g)(1) Madison General Ordinances, the property owner shall pay all real estate taxes that are accrued or delinquent for the subject property prior to CSM recording. This includes property tax bills for the prior year that are distributed at the beginning of the year. Receipts from the City of Madison Treasurer are to be provided before or at the time of sign-off.
- 25. There are no special assessments reported on the subject parcel. If special assessments are levied against the property during the review period prior to CSM approval sign-off they shall be paid in full pursuant to Madison General Ordinance Section 16.23(5)(g)1.
- 26. Pursuant to Madison City Ordinance Section 16.23(5)(g)(4), the owner shall furnish an updated title report to Heidi Radlinger in City's Office of Real Estate Services as well as the survey firm preparing the proposed CSM. The report shall search the period subsequent to the date of the initial title report (7/14/2022) submitted with the CSM application and include all associated documents that have been recorded since the initial title report. A title commitment may be provided, but will be considered only as supplementary information to the title report update. Surveyor shall update the CSM with the most recent information reported in the title update. ORES reserves the right to impose additional conditions of approval in the event the title update contains changes that warrant revisions to the CSM.
- 27. The owner shall email the document number of the recorded CSM to Heidi Radlinger as soon as the recording information is available.

Specific questions regarding the comments or conditions contained in this letter should be directed to the commenting agency.

A resolution approving the CSM and authorizing the City to sign it and any other documents related to the proposed land division will be reviewed by the Common Council at its November 1, 2022 meeting.

Prior to City Engineering final sign-off by the main office of the CSM, the final CSM shall be submitted to the Engineering Division Surveyor/ Land Records Coordinator for final technical review and approval. This submittal must occur a minimum of two working days prior to final Engineering Division signoff. E-mail submittal of the final CSM in PDF form is preferred. Transmit to jrquamme@cityofmadison.com.

As soon as the comments and conditions have been satisfied, the original along with the revised Certified Survey Map, with all signatures and approvals from the reviewing agencies, shall be brought to this office for final signoff. You or your client may then record the certified survey at the Dane County Register of Deeds. For information on recording procedures and fees, contact the Register of Deeds at 266-4141.

Any appeal from this action, including the conditions of approval, must be filed with the Circuit Court within 30 days from the date of this letter. The approval of this CSM shall be null and void if not recorded in twelve (12) months from the date of the approving resolution or this letter, whichever is later.

If you have any questions or if may be of any further assistance, please do not hesitate to contact my office at 243-0455.

Sincerely,

Colin Punt Planner

cc: Tim Troester, City Engineering Division

Julius Smith, City Engineering Division-Mapping Section

Sean Malloy, Traffic Engineering Division

Jeff Belshaw, Water Utility

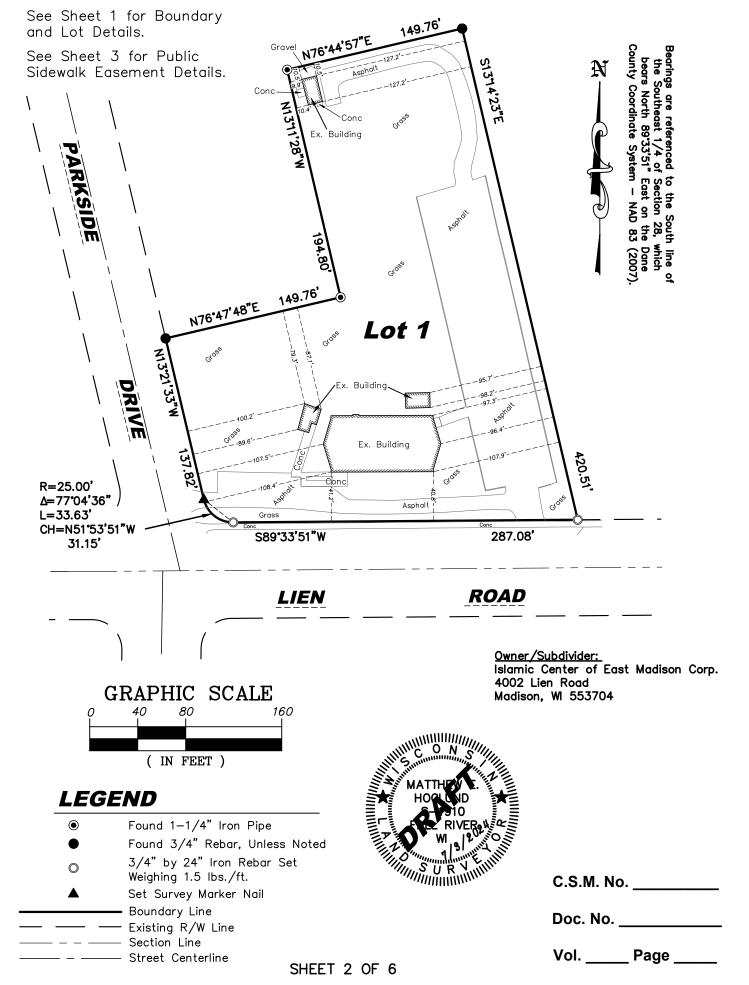
Heidi Radlinger, Office of Real Estate Services

DANE COUNTY C.S.M. No. **CERTIFIED SURVEY MAP #** Doc. No. Lot 9 and a part of Lots 10 and 11 of the Melvin A. Randall Plat and a part of the SW 1/4 of the SE 1/4, all of Section Vol. ____ Page _ 28, T8N., R10E., City of Madison, Dane County, Wisconsin. Lot 1 is located within Wellhead Protection District-Zone(WP-15). Use of Lot 1 is required to comply with the City of Madison 1510 Wellhead Protection requirements as provided under MGO POS OSM Sections 13 22 and 28 102 Fence lies C-9451-1 LOT 3 Owner/Subdivider: Islamic Center of East Madison Corp. [150.00'] **149.76** [N76°21'39"E] N76.44.57 12 (S13.39'). 4002 Lien Road 1.09 (1) Madison, WI 553704 LOT 5 _{{150.0}'} Fence lies 137423"E-[N76°21'39"E] 1.4' N of line N76.44'57"E LOT 6 Detail (7) No Scale Varies County Bearings are refe the Southeast bears North 89 7 1.0 LOT Coordinate See Detail (1)89)*33′51″ E e System LOT 8 UNCLATIED {195.0'} _{{150.0}'} N76.47.48.E 149.76 2 Sec ı Varies Z D Fence lies N & 0.6' 9 Fd. South 28, in the AD 83 2.8 N of line Lot 1 of line LOT 9 13.21 which Dane 86,524 Sq. Ft.± 1.986 Acres± (2007) 5574496 **Varies** SE Cor. Sec. 28-8-10 Fd. Brass Cap Monument & ties intact per 3/20/17 Tie Sheet. 2 LOT 11 LOT 10 (\$89°09'46"W) **\$89°33'51"W** (287.39') **287.08**' Dane Co. Coords N=499,746.02 E=842,128.11 (N=499,746.09) R=25.00' Δ=77°04'36" (5) (7)L=33.63'(E=842,127.96) CH=N51°53'51"W N89°33'51"E 153.93 31.15 (N52°15'14"W) (5) Dedicated to the Public 5,080 Sq. Ft.± for Street Purposes 0.117 Acres± (31.30')S. LINE 2197.72 S89°33'51"W 280.54 153.96 1/4 Cor. N89°33'51"E 5 2632.22' ROAD LIEN Sec. 28-8-10 Fd. Brass Cap Monument – Monument & ties intact per 5/4/21 Tie Sheet. LOT 1 CSM 14984 COLONY_HGTS. LOT 1 NOTES SEE SHEET 2 FOR BUILDING AND SURFACE DETAILS. Dane Co. Coords: N=499,725.99 E=839,495.97 12' Public Storm Sewer Easement per CSM 2818 (2) 6' Public Utility Easement per CSM 2818 R/W to Wisconsin Bell per Doc. 1840444 for Bldg. & Related Equipment LEGEND 10' MG&E Underground Elec. Easement per Doc. 5323696 (centerline dimensioned hereon) • Found 1-1/4" Iron Pipe 4 Found 3/4" Rebar, Unless Noted R/W — Fee Title Granted to City of Madison per Doc. 2424692 THIRININI 3/4" by 24" Iron Rebar Set Also a found 1" Iron Pipe lies N39'04'27"E, 0.27' from the corner 0 Weighing 1.5 lbs./ft. Private Storm Sewer Easement per Doc. 5857291 benefitting Lot 1 hereof. Set Survey Marker Nail Record Data per CSM 2818 (303.63') or Deeds of Record GRAPHIC SCALE Record Data per A CONTROL OF THE PARTY OF THE P 160 {303.63'} Melvin A. Randall Plat [303.63'] Record Data per CSM 1510 **Boundary Line** (IN FEET) Existing R/W Line QUAM ENGINEERING, LLC Platted Lot Line Section Line 4604 SIGGLEKOW ROAD - SUITE A McFARLAND, WI 53558 608-838-7750 www.quamengineering.com Project # IC-06-19 – Existing Easement Limits SHEET 1 OF 6 - Street Centerline

DANE COUNTY CERTIFIED SURVEY MAP

Lot 9 and a part of Lots 10 and 11 of the Melvin A. Randall Plat and a part of the SW 1/4 of the SE 1/4, all of Section 28, T8N., R10E., City of Madison, Dane County, Wisconsin.

BUILDING & SURFACE DETAILS



DANE COUNTY CERTIFIED SURVEY MAP

Lot 9 and a part of Lots 10 and 11 of the Melvin A. Randall Plat and a part of the SW 1/4 of the SE 1/4, all of Section 28, T8N., R10E., City of Madison, Dane County, Wisconsin.

SURVEYOR'S CERTIFICATE:

I, Matthew E. Hoglund, Registered Land Surveyor, do hereby certify to the best of my knowledge and belief that I have surveyed, divided, and mapped the following Certified Survey, Lot 9 and a part of Lots 10 and 11 of the Melvin A. Randall Plat, recorded in Volume 13 of Plats on Page 4 as Document No. 763928 of Dane County Records and part of the Southwest one-quarter of the Southeast one-quarter of Section 28, Township 8 North, Range 10 East, City of Madison, Dane County, Wisconsin, being more particularly described as follows:

COMMENCING the South one-quarter corner of said Section 28; thence, along the South line of said Southeast one-quarter, North 89°33'51" East, 434.50 feet to its intersection with the Southeasterly prolongation of the Westerly line of Certified Survey Map No. 2818; thence, along said Southeasterly prolongation, North 13°14'23" West, 41.02 feet to the **POINT OF BEGINNING** lying on the Northerly right-of-way of Lien Road per Document No. 2424692;

thence, along said Northerly right-of-way line, South 89°35'51" West, 287.08 feet to the beginning of a tangent curve, being concave Northeasterly, having a radius of 25.00 feet and a chord which bears North 51°53'51" West, 31.15 feet;

thence Northwesterly, 33.63 feet along the arc of said curve through a central angle of 77°04'36" to the Point of Tangency thereof, lying on the Easterly right-of-way line of Parkside Drive;

thence, along said Easterly right-of-way line, North 13°21'33" West, 137.82 feet to the Northwest corner of aforesaid Lot 9;

thence, along the Northerly line of said Lot 9, North 76°47'48" East, 149.76 feet to the Northeast corner of said Lot 9;

thence, along the Easterly line of said Melvin A. Randall Plat, North 13°11'28" West, 194.80 feet to the Northeast corner of Lot 6 of said Melvin A. Randall Plat, said corner also being the Southwest corner of Lot 3 of Certified Survey Map No. 1510;

thence, along the Southerly line of said Lot 3, North 76°44'57" East, 149.76 feet to the Northwest corner of Lot 1 of aforesaid Certified Survey Map No. 2818;

thence, along aforesaid Westerly line of Certified Survey Map No. 2818, South 13°14'23" East, 420.51 feet to the **POINT OF BEGINNING**;

TOGETHER WITH that part of the Southwest one-quarter of the Southeast one-quarter of Section 28, Township 8 North, Range 10 East, City of Madison, Dane County, Wisconsin, being more particularly described as follows:

COMMENCING at the South one-quarter corner of said Section 28; thence, along the South line of said Southeast one-quarter, North 89°33'51" East, 280.54 feet to the Southeast corner of aforesaid Melvin A. Randall Plat and the **POINT OF BEGINNING**;

thence, along the Easterly line of said Plat, North 13°11'28" West, 33.84 feet to the South line of Document No. 2424692;

thence, along last said South line, North 89°33'51" East, 153.93 feet to its intersection with aforesaid Southeasterly prolongation of the Westerly line of Certified Survey Map No. 2818;

thence, along said Southeasterly prolongation, South 13°14'23" East, 33.84 feet to said South line of the Southeast one-quarter;

thence, along last said South line, South 89°33'51" West, 153.96 feet to the **POINT OF BEGINNING**.

Said parcel contains 91,604 square feet or 2.103 acres, more or less, and is subject to all easements and agreements, if any, of record and/or fact.

I further certify, to the best of my knowledge and belief, that this map is a correct representation of all exterior boundaries of the land surveyed and the subdivision thereof made; that I have made such survey, land division, and map by the direction of Oumar Keita of the Islamic Center of East Madison Corporation, the owner of said land; that I have fully complied with the provisions of Chapter 236.34 of the Wisconsin State Statutes and the City of Madison Subdivision Regulations in surveying, dividing, and mapping the same.

Dated this day of	July	, 2024.
PTS S-1910	MATTHE HOOL	N S IVER WO IVER WO IVER

C.S.M. No.	
Doc. No	

Vol. ____ Page ____

DANE COUNTY CERTIFIED SURVEY MAP

Lot 9 and a part of Lots 10 and 11 of the Melvin A. Randall Plat and a part of the SW 1/4 of the SE 1/4, all of Section 28, T8N., R10E., City of Madison, Dane County, Wisconsin.

CORPORATE OWNERS CERTIFICATE:

Islamic Center of East Madison Corporation, a Wisconsin Non-Stock Corporation, duly organized and existing under and by virtue of the laws of the State of Wisconsin, as owner, does hereby certify that said company caused the land described on this Certified Survey Map to be surveyed, divided, mapped, and dedicated as represented on this map. Islamic Center of East Madison Corporation, does further certify that this map is required by s.236.10 or s.236.12 to be submitted to the following for approval or objection:

City of Madison

		**		
signed by		, its		_ and
	, its	at		,,
on this,	day of	, 20		
In the presence of:	Print Name:		Title:	
	Print Name:		Title:	
STATE OF WISCO	,			
Personally came b	efore me this	day of	, 20,	,
who executed the and	foregoing instrum	ent, and to me knowr	n to be the nd acknowledged	that they executed the its authority.
(Notary Seal)		Notary Pub	lic,	, Wisconsin
My commission ex	pires	·		
HOOLOND RIVER WIND STIRM S				C.S.M. No Doc. No

Vol. ____ Page _

DANE COUNTY CERTIFIED SURVEY MAP

Lot 9 and a part of Lots 10 and 11 of the Melvin A. Randall Plat and a part of the SW 1/4 of the SE 1/4, all of Section 28, T8N., R10E., City of Madison, Dane County, Wisconsin.

CITY PLAN COMMISSION CERTIFICATE:
Approved for recording per the Secretary of the City of Madison Plan Commission.
By: Matthew Wachter, Date Secretary of the Plan Commission
CITY OF MADISON COMMON COUNCIL CERTIFICATE: Resolved, that this Certified Survey Map located in the City of Madison was hereby approved by Enactment Number, File ID Number, adopted on the day of, 20, and said Enactment further provided for the acceptance of those lands dedicated and rights conveyed by said Certified Survey Map to the City of Madison for public use.
Dated this day of, 20
Maribeth Witzel-Behl, City Clerk City of Madison, Dane County, Wisconsin MATTHE RIVER WISCONSIN
REGISTER OF DEEDS CERTIFICATE: Received for recording this day of, 20, ato'clockM. and recorded in Volume of Certified Survey Maps on Pages as Document No
Kristi Chlebowski, Dane County Register of Deeds



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83723

File ID: 83723 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: FINANCE

COMMITTEE

File Created Date: 05/29/2024

File Name: MFD Paramedic Training MATC 2024-2025 Final Action:

Title: Authorizing a non-competitive service contract between the Madison Area

Technical College and the City of Madison Fire Department for the provision of

paramedic training, 2024-2025 cohort. (Citywide)

Notes:

Sponsors: Derek Field, John W. Duncan And Amani Latimer Effective Date:

Burris

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: afreedman@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Fire Department	05/29/2024	Referred for Introduction				
	Action Text: Notes:	This Resolution was Ref Finance Committee (7/8/24	Ferred for Introduction), Common Council (7/16/24)				
1	COMMON COUN	NCIL 07/02/2024	Refer	FINANCE COMMITTEE		07/08/2024	Pass
	Action Text:	A motion was made by Figure 1. The motion passed by v	Figueroa Cole, seconded boice vote/other.	by Duncan, to Refer to th	ne FINANCE CO	OMMITTEE.	
1	FINANCE COMM	NITTEE 07/08/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass
	Action Text:	•	Figueroa Cole, seconded b DFFICER. The motion pas	•		NCIL TO	

Text of Legislative File 83723

Fiscal Note

The proposed resolution authorizes a one-time, non-competitive service contract for paramedic training services from the Madison Area Technical College. The \$85,080 estimated cost is included in the Madison Fire Department's 2024 operating budget and will be included in the Department's 2025 operating budget request. No additional city appropriation is required at this

time.

Title

Authorizing a non-competitive service contract between the Madison Area Technical College and the City of Madison Fire Department for the provision of paramedic training, 2024-2025 cohort. (Citywide)

Body

WHEREAS, the Madison Fire Department (MFD) has sent MFD firefighter-EMTs to Madison Area Technical College (MATC) for paramedic training for the past seven years;

WHEREAS, the 2024-2025 cohort includes ten students with tuition and fees approximating \$8,508 per student, totaling an estimated \$85,080;

WHEREAS, MGO 4.26 requires Common Council approval of service contracts of \$50,000 or more if the contractor was not selected through a competitive selection process;

NOW, THEREFORE, BE IT RESOLVED, the Mayor and City Clerk are authorized to sign a non-competitive service contract with the Madison Area Technical College for paramedic training services for the 2024-2025 cohort, up to \$85,080.



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83518

File ID: 83518 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: FINANCE

COMMITTEE

File Created Date: 05/17/2024

Final Action:

Enactment Number:

File Name: Requesting information, studies, and plans regarding

the authorization of a local sales tax.

Title: Requesting information, studies, and plans regarding the authorization of a local

sales tax.

Notes: TO BE INTRODUCED FROM THE FLOOR

Sponsors: Amani Latimer Burris Effective Date:

Attachments: Transportation Committee Memo for Resolution

83518.pdf, Part4Powerpoint.pdf, 2025 Operating Budget Update to FC -- 6-10-2024_SB slide

added.pdf

Author: Ald. Amani Latimer Burris Hearing Date:

Entered by: imatthias@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Council Office	05/17/2024	Referred for Introduction				
	Action Text:	This Resolution was Ref	erred for Introduction				
	Notes:	,	**	ecutive Committee (6/4/24), Cual Opportunities Commissio	•	•	
1	COMMON COUN	ICIL 05/21/2024	Refer	FINANCE COMMITTEE		06/24/2024	Pass
	Action Text:	A motion was made by F meeting of 6/24/24. The	•	ded by Duncan, to Refer to ice vote/other.	the FINANCE C	OMMITTEE	
	Notes:			Committee (6/4/24), Committe d Equal Opportunities Commi		ations	
1	FINANCE COMM	IITTEE 05/21/2024	Referred	EQUAL OPPORTUNITIES COMMISSION			
	Action Text:	This Resolution was Ref	erred to the EQUAL	OPPORTUNITIES COMM	MISSION		
1	FINANCE COMM	IITTEE 05/21/2024	Referred	COMMON COUNCIL EXECUTIVE COMMITTEE		06/04/2024	
	Action Text:	This Resolution was Ref	erred to the COMMC	ON COUNCIL EXECUTIVE	COMMITTEE		

1 FINANCE COMMITTEE 05/21/2024 Referred TRANSPORTATI 06/12/2024

ON COMMISSION

Action Text: This Resolution was Referred to the TRANSPORTATION COMMISSION

1 FINANCE COMMITTEE 05/21/2024 Referred COMMITTEE ON 06/12/2024

EMPLOYEE RELATIONS

COMMITTEE

Action Text: This Resolution was Referred to the COMMITTEE ON EMPLOYEE RELATIONS

COMMON COUNCIL 06/04/2024 Return to Lead with FINANCE 06/24/2024 Pass

EXECUTIVE the

COMMITTEE Recommendation to

Place on File without

Prejudice

Action Text: A motion was made by Duncan, seconded by Govindarajan, to Return to Lead with the

Recommendation to Adopt. The motion was withdrawn by Duncan and withdrawal was accepted by

Govindarajan.

A motion was made by Duncan, seconded by Harrington-McKinney, to Return to Lead with the Recommendation to Place on File without Prejudice to the FINANCE COMMITTEE. The motion

passed by the following vote:

Notes: Return to Lead with the Recommendation to Place on File without Prejudice to the FINANCE COMMITTEE. The

motion passed 6-0 by the following roll call vote:

Duncan, Conklin, Martinez-Rutherford, Govindarajan, Guequierre, Harrington-McKinney: Aye

Figueroa Cole, Currie: non-voting

Ayes: 6 John W. Duncan; Nikki Conklin; Dina Nina Martinez-Rutherford; MGR

Govindarajan; John P. Guequierreand Barbara Harrington-McKinney

Non Voting: 2 Yannette Figueroa Coleand Jael Currie

1 COMMITTEE ON 06/12/2024 Return to Lead with COMMON Pass

EMPLOYEE RELATIONS the Following COUNCIL

Recommendation(s)

Action Text: A motion was made by Madison, seconded by Kief, to make no recommendation. The motion passed

by unanimous voice vote/other.

Notes: Unanimous motion by CER to make no recommendation.

1 TRANSPORTATION 06/12/2024 Return to Lead with FINANCE 06/24/2024 Pass

COMMISSION the Following COMMITTEE

Recommendation(s)

Action Text: Assistant City Attorney Eric Finch, Finance Director David Schmiedicke and Director of Transportation

Tom Lynch provided verbal reports and were available for questions. Alder Burris made comments.

Webber moved to Return to Lead with the Following Recommendations: Accept the report that was sent to the Transportation Commission, seconded by Bennett. The motion passed by voice vote/other.

Notes: Accept the report that was sent to the Transportation Commission

FINANCE COMMITTEE 06/24/2024 RECOMMEND TO

COUNCIL TO

ADOPT - REPORT

OF OFFICER

Action Text: A motion was made by Figueroa Cole, seconded by Rummel, to RECOMMEND TO COUNCIL TO

ADOPT - REPORT OF OFFICER. The motion failed by the following vote:

Notes: A motion was made by Figueroa Cole, seconded by Rummel, to RECOMMEND TO COUNCIL TO ADOPT -

REPORT OF OFFICER. The motion failed by the following vote:

Aye: Rummel

Noes: Figueroa Cole, Evers, Madison

Excused: Currie, Verveer Non Voting: Rhodes Conway

Due to failure of the motion to adopt, the resultant recommendation is to PLACE ON FILE WITHOUT PREJUDICE.

Ayes: 1 Marsha A. Rummel

Noes: 3 Yannette Figueroa Cole; Tag Eversand Sabrina V. Madison

Excused: 2 Jael Currieand Michael E. Verveer

Fail

Non Voting: 1 Satya V. Rhodes-Conway

1 COMMON COUNCIL 07/02/2024 Refer EQUAL Pass

OPPORTUNITIES COMMISSION

Action Text: A motion was made by Figueroa Cole, seconded by Duncan, to Refer to the EQUAL

OPPORTUNITIES COMMISSION meeting of 7/11/24 to return to council 7/16/24. The motion passed

by voice vote/other.

Text of Legislative File 83518

Fiscal Note

This resolution requires a number of actions by the Finance Department and various City committees:

The <u>Finance Committee</u>, with assistance from the Finance Department, is directed to confirm a series of statements regarding the possible impacts and implications of a possible sales tax for the City of Madison and an undefined Metro Transit District. The resolution also calls for a study on the incorporation of revenues from a possible sales tax into projections for future City budgets, including sufficient reserves to address future economic downturns, and possible impacts on the need for future property tax revenues. Regarding development of the 2025 budget, the resolution calls for the use of one-time funds to address the deficit for one year, with the assumption that a future State Legislature will pass a law authorizing the City of Madison to implement a 0.5% sales tax in time to use those revenues in the 2026 budget.

The <u>Transportation Committee</u> is directed to work with staff to review the financing of regional transit entities around the country, particularly those funded from a sales tax, work with communities adjacent to Madison to gauge support for a regional sales tax to help fund transit costs, engage with Transit labor organizations regarding their perspectives on shifting employees from the City to a regional transit entity.

The <u>Common Council Executive Committee</u> is directed to gather information from the City of Milwaukee regarding the use of proceeds from its recently authorized 2% sales tax, as well as its advice for working with the State Legislature to secure approval for a sales tax for the City of Madison and a Metro Transit District. This committee is also directed to develop a comprehensive City plan to lobby the State Legislature to approve a sales tax for the City of Madison.

The provisions of this resolution are similar to those adopted by the Council in Legistar 82079 (RES-24-00260, adopted April 16, 2024), regarding lobbying the State Legislature to approve a local sales tax for the City of Madison, and Legistar 82456 (RES-24-00266, adopted April 16, 2024), regarding guiding principles for developing the 2025 City budget. In particular, Legistar 82456 -- Guiding Principles, directs that the following occur:

- 1. Evaluate service levels, staffing, and fiscal impact when developing their budget requests and to seek ways to increase efficiencies in their operations.
- 2. Explore a reduction in expenditures from cost-to-continue levels that does not compromise services to residents.
- 3. Develop language for a referendum to increase the allowable levy limit for a portion of, or the full amount of, the projected budget deficit, as well as options to replace existing non-property tax revenues, for consideration by the voters at the November 5, 2024, general election.
- Prepare contingency plans for the 2025 budget in the event that the voters do not approve the referendum, including options for special charges, which will be presented

- to the Council with the executive budget.
- 5. Develop a multi-year plan to address the on-going structural deficit due to limits on revenues under state law, including options for developing new, or increasing existing, fees and charges, to be included with the executive budget.
- Consider the overall level of general obligation borrowing in developing the City's 2025 capital budget and capital improvement plan in order to reduce the rate of growth in property taxes.
- 7. Develop a community engagement plan to educate the public on the City's financial condition and options for addressing the structural deficit and may engage a consultant to support this work, if needed.

The proposed resolution would require the allocation of additional staff time to accomplish the required tasks at the same time that the 2025 budget is being developed using the guiding principles already adopted by the Council in Legistar 82456.

Title

Requesting information, studies, and plans regarding the authorization of a local sales tax. **Body**

WHEREAS, the Madison Common Council recently passed Resolution 82079: Directing the Finance Department to issue a report analyzing the City operating budgets from 2019-2024 and calling on the State Legislature to authorize a local sales tax options; and,

WHEREAS, the city of Madison, is facing a budget challenge in 2025 potentially amounting to \$27 million dollars or more; and,

WHEREAS, the budget crisis, unfettered, will only get worse in subsequent years; and,

WHEREAS, it's important that the City examine all alternative revenue sources to solve this long-term problem (especially as the City continues to grow); and,

WHEREAS, we realize this issue would benefit from a reasonable remedy beyond what currently exists; and,

WHEREAS, it's in the best interest of the City and its citizens, even if difficult, to take action now; and,

WHEREAS, in action, we can explore and pursue short-term, long-term, and potential solutions alike; and,

WHEREAS, in this way, a significant portion of a sales tax would be paid by visitors; and,

WHEREAS, nationally, many metropolitan areas use a sales tax to pay for public transit operations and operating expenses; and,

WHEREAS, given the current state exemptions for sales tax on things such as:

- Housing
- Health care
- Most non-processed foods

It is suggested that a sales tax would balance out the burden and, when applied, reasonably

suggest that those with more means or higher income (through consumer spending) would balance and pace out the burden, making a sales tax less regressive for those with modest means.

NOW, THEREFORE, BE IT RESOLVED that the Common Council request the following:

That the Finance Committee working in conjunction and in collaboration with the Finance Department:

- 1. Confirm that a half-cent sales tax would provide the City of Madison about \$35 million annually.
- 2. Confirm that a half cent sales tax could provide a Metro Transit District about \$50 million annually,
- 3. Confirm that the use of a sales tax (all things considered) is less regressive and cumbersome to all populations, suggesting that:
 - a. Many items affecting those with less income would be tax exempt in Wisconsin,
 - b. A significant portion of the sales tax will be paid by non-residents,
 - c. A sales tax of such would alleviate the City having to request multiple and multiyear referendums; a pattern we would like to avoid,
 - d. A sales tax of such would help to balance the budget and reduce the threat of the reduction of services, employee layoffs or the holding off on filling needed positions,
 - e. That the city is already collecting fees on automobiles and sanitation expenses which for some may be more regressive than a sales tax,

Then We'd ask that we:

- 4. Explore how much, if any, property tax relief could be provided to Madison property owners (and other owners who would logically pass the expenses onto renters) if:
 - a. The sales tax was to be adopted,
 - b. Funds have been set aside to:
 - i. Cover the structural deficit,
 - ii. Cover the cost of the ongoing operation of a Madison Area Transit District and,
 - iii. Maintain our reserve fund at appropriate levels
 - c. Forecast and raise any other issues that would foil or have impact on this solution.
- 5. Determine what kind of reserve the city should maintain for bad economic times when sales tax revues would drop,

Recognizing that it's unlikely that there will be a special session of the Legislature to take up a Madison sales tax this summer but there is certainly the ability to take this matter up in 2025, we ask that this Committee work with the Finance Department to:

- 6. Devise a temporary solution for 2025 that as necessary:
 - a. Provides money for the 2024 budget,
 - b. Uses one-time sources, revenues and/or rainy-day funds, given the significant returns on city investments in 2023 and anticipated for 2024,
 - c. Considers closing TIFs and,
 - d. Avoids going to a property tax referendum or massive layoffs (that could happen if such a measure fails).

Then, in consideration of finding an equitable, long-term solution that may offset expenses, that it's acknowledged, are generally a revenue loss and requires supplementation, it's asked that the Transportation Commission and Department Staff:

- 7. Contact national organizations that work with metropolitan transit districts to see how they are financed, through the lens of using a regional sales tax, and how collectively, communities could be appointed to serve on and control the board,
- 8. Engage other cities that are part of Madison Metro to gauge their support of, if possible, using a sales tax solution over a property tax,
- 9. Work with the labor organizations, who rightfully represent the interest of the employees and long-term, want a square solution, to determine the best model or an alternative model of employment in a new Madison Area Transit District, including exploring the possibility, impact, and viability of:
 - a. Certain employees being employees of the district,
 - b. Others being city employees with an ongoing city contract to provide staff for the new transit district.

BE IT FURTHER RESOLVED that the Madison Common Council ask the Common Council Executive Committee to gather, study, explore, and interview representatives of the City of Milwaukee to report back on:

- 1. How sales tax was used in Milwaukee
 - a. For the City itself,
 - b. For their transit district;
- 2. Any advice they may have in regard to getting a sales tax approved by the legislature,
- 3. A plan for the City of Madison and the Council, as a collective, and the Mayor or their representatives to lobby the legislature for sales tax authority.

BE IT FINALLY RESOLVED that the Madison Common Council asks that the Administration work in conjunction with us to accomplish these goals.

CITY OF MADISON OFFICE OF THE CITY ATTORNEY ROOM 401 - CITY-COUNTY BUILDING 266-4511

To: Transportation Commission

From: Assistant City Attorney Eric Finch

RE: Resolution #83518 – Requesting information, studies, and plans regarding the authorization

of a local sales tax Date: June 10, 2024

PURPOSE

The purpose of this memo is to provide brief background information requested in the above resolution, including:

- A review financing of Regional Transit Entities around the country
 - o particularly those funded with a sales tax
- Comment on support in communities adjacent to Madison for a regional sales tax
- Comment on support for a City-to-RTA shift among local Transit Labor Organizations

At the bottom is an index of the RTA authority that was removed from state law in 2011.

BACKGROUND

The City of Madison is facing a facing significant shortfall. With regard to transportation and transit specifically, where the City has received millions in federal grants, it is important to understand the difference between the operating budget and capital expenditures. Federal money for capital projects will not fix shortfalls in the operating budget.

Federal, State, and local dollars are all at play in the City of Madison's transportation spending. Annual funding amounts per system are equalized via formula statewide, combining grants from the State Urban Mass Transit Operating Assistance Program as well as Federal Formula programs for Rural and Urban Areas. Approximately one-half of total annual operating expenses for Wisconsin transit systems are reimbursed through state and federal grants. However, Madison has received a lower percentage of State aid than other significant transit systems in Wisconsin. Madison is currently an outlier in how low our state aid share is, and how high our local share is.

The City has explored, through our finance department and attorneys office, routes for increasing transportation funding. In short, there is no single viable option that has been

identified for increasing revenues without requiring a change in state law or a referendum. For example, arrangements to shift transit operations to the county would not result in new taxing authority for property or sales taxes.

Understanding the situation with regard to transportation and with an eye toward revenue generation, the following two City resources provide a primer on the issues:

The first is Part 4 of the Budget outlook series, focused on revenue strategies:

https://www.cityofmadison.com/finance/documents/budget/2025/Part4Powerpoint.pdf

The second is the most recent update given to Finance Committee on June 10, 2024:

 $\frac{\text{https://madison.legistar.com/View.ashx?M=F\&ID=13017733\&GUID=2C7DB2FF-2271-43A8-B1D6-3528D201964D}{\text{https://madison.legistar.com/View.ashx?M=F\&ID=13017733\&GUID=2C7DB2FF-2271-43A8-B1D6-3528D201964D}{\text{https://madison.legistar.com/View.ashx?M=F\&ID=13017733\&GUID=2C7DB2FF-2271-43A8-B1D6-3528D201964D}{\text{https://madison.legistar.com/View.ashx?M=F\&ID=13017733\&GUID=2C7DB2FF-2271-43A8-B1D6-3528D201964D}{\text{https://madison.legistar.com/View.ashx?M=F&ID=13017733\&GUID=2C7DB2FF-2271-43A8-B1D6-3528D201964D}{\text{https://madison.legistar.com/View.ashx?M=F&ID=13017733\&GUID=2C7DB2FF-2271-43A8-B1D6-3528D201964D}{\text{https://madison.legistar.com/View.ashx}}{\text{https://madison.legis$

Finally, the City's website contains additional information, with further 2025 Budget outlook materials are available here for those interested:

https://www.cityofmadison.com/finance/budget/2025/outlook

OVERVIEW OF RTA FUNDING NATIONALLY

RTA Funding Varies greatly, with systems across the country funding by the following different revenues: State Taxes (Sales, Property, and Income); Vehicle Registration Fees (State or Local); Employer/Payroll Taxes (ex. Portland, OR and Transit Authority of River City, KY); Concessions (See State of Pennsylvania program); Local Sales Taxes; Lottery/Casino Revenues; Vehicle Sales, Lease, and Rental Fees; Tollway fees; Cigarette Taxes; Parking Fees and Fines; Property Taxes; Fares; Revenue-Generating Operations (Providing charter services, leasing city/RTA property); Advertisement; Realty Transfer/mortgage recording fees; Hotel/Motel Taxes; Utility Fees; Public/Private Partnerships; Tax-increment Financing Districts (TIF); and, Transportation Development Districts (TDDs).

In Wisconsin, RTAs have generally been stripped of taxing authority by 2011 Act 32. Currently, the only alternatives to the City of Madison running transit operations would be a switch to County administration or the creation of an intergovernmental body of local governments. Neither change would result in additional availability of revenue under current state law.

OVERVIEW OF COMMUNITY AND LABOR ORGANIZATION SUPPORT

At this stage, without details about what sort of boundaries and type of revenues would be involved, it is not possible to accurately gauge public support for an RTA or sales taxes in surrounding communities. Additional sales tax or the creation of an RTA would first require a change in state law.

As of May 24, 2024, Local 695 has merged into Local 120. City staff reached out to Local 120 through longtime Local 695 personnel that are now continuing their work with Local 120 for a comment on switching to an RTA. They are currently discussing internally.

INDEX OF RTA AUTHORITY REMOVED FROM STATE LAW IN 2011 (ACT 32)

ACT 32 - RTAs eliminated; dissolution and winding down of existing RTAs [Sec. 751g, h, 772g, r, 922e-v, 1139m, 1679d-t, 1720b, 1727m, 1729g, r, 1745m, 1747r, 1754r, 1894r, 1895r, 2014r, 2177m, 2180m, 2183d-r, 2187d-n, 2237e-o, 3182g, r, 3471m, 3567m, 9148 (3u), 9448 (6u)]

SECTION 751G. 20.566 (1) (gc) of the statutes is repealed.

SECTION 751H. 20.566 (1) (gh) of the statutes is repealed.

SECTION 772G. 20.835 (4) (gc) of the statutes is repealed.

SECTION 772R. 20.835 (4) (gh) of the statutes is repealed.

SECTION 922E. 32.02 (11) of the statutes is amended to read:

32.02 (11) Any housing authority created under ss. 66.1201 to 66.1211; redevelopment authority created under s. 66.1333; community development authority created under s. 66.1335; local cultural arts district created under subch. V of ch. 229, subject to s. 229.844 (4) (c); or local exposition district created under subch. II of ch. 229; or transit authority created under s. 66.1039.

SECTION 922M. 32.05 (1) (a) of the statutes is amended to read:

32.05 (1) (a) Except as provided under par. (b), a county board of supervisors or a county highway committee when so authorized by the county board of supervisors, a city council, a village board, a town board, a sewerage commission governing a metropolitan sewerage district created by ss. 200.05 or 200.21 to 200.65, the secretary of transportation, a commission created by contract under s. 66.0301, a joint local water authority created by contract under s. 66.0823, a transit authority created under s. 66.1039, a housing authority under ss. 66.1201 to 66.1211, a local exposition district created under subch. II of ch. 229, a local cultural arts district created under subch. V of ch. 229, a redevelopment authority under s. 66.1333 or a community development authority under s. 66.1335 shall make an order providing for the laying out, relocation and improvement of the public highway, street, alley, storm and sanitary sewers, watercourses, water transmission and distribution facilities, mass transit facilities, airport, or other transportation facilities, gas or leachate extraction systems to remedy environmental pollution from a solid waste disposal facility, housing project, redevelopment project, cultural arts facilities, exposition center or exposition center facilities which shall be known as the relocation order. This order shall include a map or plat showing the old and new locations and the lands and interests required. A copy of the order shall, within 20 days after its issue, be filed with the county clerk of the county wherein the lands are located or, in lieu of filing a copy of the order, a plat may be filed or recorded in accordance with s. 84.095.

SECTION 922V. 32.07 (2) of the statutes is amended to read:

32.07 (2) The petitioner shall determine necessity if application is by the state or any commission, department, board or other branch of state government or by a city, village, town, county, school district, board, commission, public officer, commission created by contract under s. 66.0301, joint local water

authority under s. 66.0823, transit authority created under s. 66.1039, redevelopment authority created under s. 66.1333, local exposition district created under subch. II of ch. 229, local cultural arts district created under subch. V of ch. 229, housing authority created under ss. 66.1201 to 66.1211 or for the right-of-way of a railroad up to 100 feet in width, for a telegraph, telephone or other electric line, for the right-of-way for a gas pipeline, main or service or for easements for the construction of any elevated structure or subway for railroad purposes.

SECTION 1139M. 40.02 (28) of the statutes is amended to read:

40.02 **(28)** "Employer" means the state, including each state agency, any county, city, village, town, school district, other governmental unit or instrumentality of 2 or more units of government now existing or hereafter created within the state, any federated public library system established under s. 43.19 whose territory lies within a single county with a population of 500,000 or more, a local exposition district created under subch. II of ch. 229, a transit authority created under s. 66.1039, and a long-term care district created under s. 46.2895, except as provided under ss. 40.51 (7) and 40.61 (3). "Employer" does not include a local cultural arts district created under subch. V of ch. 229. Each employer shall be a separate legal jurisdiction for OASDHI purposes.

SECTION 1679D. 59.58 (6) of the statutes is repealed.

SECTION 1679H. 59.58 (7) of the statutes, as affected by 2011 Wisconsin Act ... (this act), is repealed.

SECTION 1679P. 59.58 (7) (e) (intro.) of the statutes is amended to read:

59.58 (7) (e) (intro.) The Subject to s. 77.9973 (2), the authority may impose the fees under subch. XIII of ch. 77. From the fees, the authority may do all of the following:

SECTION 1679T. 59.58 (7) (i) and (j) of the statutes are repealed.

SECTION 1720B. 66.0301 (1) (a) of the statutes is amended to read:

66.0301 (1) (a) Except as provided in pars. (b) and (c), in this section "municipality" means the state or any department or agency thereof, or any city, village, town, county, school district, public library system, public inland lake protection and rehabilitation district, sanitary district, farm drainage district, metropolitan sewerage district, sewer utility district, solid waste management system created under s. 59.70 (2), local exposition district created under subch. II of ch. 229, local professional baseball park district created under subch. IV of ch. 229, local cultural arts district created under subch. V of ch. 229, transit authority created under s. 66.1039, long-term care district under s. 46.2895, water utility district, mosquito control district, municipal electric company, county or city transit commission, commission created by contract under this section, taxation district, regional planning commission, housing authority created under s. 66.1201, redevelopment authority created under s. 66.1333, community development authority created under s. 66.1355, or city-county health department.

SECTION 1727M. 66.0903 (1) (d) of the statutes is amended to read:

66.0903 (1) (d) "Local governmental unit" means a political subdivision of this state, a special purpose district in this state, an instrumentality or corporation of such a political subdivision or special purpose district, a combination or subunit of any of the foregoing or an instrumentality of the state and any of the foregoing. "Local governmental unit" includes a regional transit authority created under s. 66.1039 and the southeastern regional transit authority created under s. 59.58 (7).

SECTION 1729G. 66.1039 of the statutes, as affected by 2011 Wisconsin Act (this act), is repealed.

SECTION 1729R. 66.1039 (4) (s) 1. of the statutes is amended to read:

66.1039 (4) (s) 1. Impose, by the adoption of a resolution by the board of directors, the taxes under subch. V of ch. 77 in the authority's jurisdictional area, except that no taxes may be imposed under this paragraph unless the resolution of the board of directors is adopted prior to the effective date of this subdivision [LRB inserts date]. If an authority adopts a resolution to impose the taxes, it shall deliver a certified copy of the resolution to the department of revenue at least 120 days before its effective date. The authority may, by adoption of a resolution by the board of directors, repeal the imposition of taxes under subch. V of ch. 77 and shall deliver a certified copy of the repeal resolution to the department of revenue at least 120 days before its effective date.

SECTION 1745M. 67.01 (5) of the statutes is amended to read:

67.01 **(5)** "Municipality" means any of the following which is authorized to levy a tax: a county, city, village, town, school district, board of park commissioners, technical college district, metropolitan sewerage district created under ss. 200.01 to 200.15 or 200.21 to 200.65, town sanitary district under subch. IX of ch. 60, transit authority created under s. 66.1039, public inland lake protection and rehabilitation district established under s. 33.23, 33.235, or 33.24, and any other public body empowered to borrow money and issue obligations to repay the money out of public funds or revenues. "Municipality" does not include the state.

SECTION 1747R. 70.11 (2) of the statutes is amended to read:

70.11 (2) MUNICIPAL PROPERTY AND PROPERTY OF CERTAIN DISTRICTS, EXCEPTION. Property owned by any county, city, village, town, school district, technical college district, public inland lake protection and rehabilitation district, metropolitan sewerage district, municipal water district created under s. 198.22, joint local water authority created under s. 66.0823, transit authority created under s. 59.58 (7) or 66.1039, long-term care district under s. 46.2895 or town sanitary district; lands belonging to cities of any other state used for public parks; land tax-deeded to any county or city before January 2; but any residence located upon property owned by the county for park purposes that is rented out by the county for a nonpark purpose shall not be exempt from taxation. Except as to land acquired under s. 59.84 (2) (d), this exemption shall not apply to land conveyed after August 17, 1961, to any such governmental unit or for its benefit while the grantor or others for his or her benefit are permitted to occupy the land or part thereof in consideration for the conveyance. Leasing the property exempt under this subsection, regardless of the lessee and the use of the leasehold income, does not render that property taxable.

SECTION 1754R. 71.05 (1) (c) 9. of the statutes is repealed.

SECTION 1894R. 71.26 (1) (b) of the statutes is amended to read:

71.26 (1) (b) *Political units*. Income received by the United States, the state and all counties, cities, villages, towns, school districts, technical college districts, joint local water authorities created under s. 66.0823, transit authorities created under s. 59.58 (7) or 66.1039, long-term care districts under s. 46.2895 or other political units of this state.

SECTION 1895R. 71.26 (1m) (j) of the statutes is repealed.

SECTION 2014R. 71.45 (1t) (j) of the statutes is repealed.

SECTION 2177M. Chapter 77 (title) of the statutes, as affected by 2011 Wisconsin Act (this act), is amended to read:

CHAPTER 77
TAXATION OF FOREST CROPLANDS;
REAL ESTATE TRANSFER FEES;

SALES AND USE TAXES;
COUNTY, TRANSIT AUTHORITY, AND
SPECIAL DISTRICT SALES AND USE
TAXES; MANAGED FOREST LAND;
ECONOMIC DEVELOPMENT SURCHARGE;
LOCAL FOOD AND BEVERAGE TAX;
LOCAL RENTAL CAR TAX; PREMIER
RESORT AREA TAXES;
STATE RENTAL VEHICLE FEE;
DRY CLEANING FEES;
SOUTHEASTERN REGIONAL
TRANSIT AUTHORITY FEE

SECTION 2180M. 77.54 (9a) (er) of the statutes is repealed.

SECTION 2183D. Subchapter V (title) of chapter 77 [precedes 77.70] of the statutes is amended to read:

CHAPTER 77 SUBCHAPTER V COUNTY, TRANSIT AUTHORITY, AND SPECIAL DISTRICT SALES AND USE TAXES

SECTION 2183E. 77.708 of the statutes, as affected by 2011 Wisconsin Act (this act), is repealed. **SECTION 2183F.** 77.708 (3) of the statutes is created to read:

77.708 (3) Retailers and the department of revenue may not collect a tax under sub. (1) for any transit authority created under s. 66.1039 after the effective date of this subsection [LRB inserts date], except that the department of revenue may collect from retailers taxes that accrued before the effective date of this subsection [LRB inserts date], and fees, interest, and penalties that relate to those taxes.

SECTION 2183G. 77.71 of the statutes is amended to read:

- 77.71 Imposition of county, transit authority, and special district sales and use taxes. Whenever a county sales and use tax ordinance is adopted under s. 77.70, a transit authority resolution is adopted under s. 77.708, or a special district resolution is adopted under s. 77.705 or 77.706, the following taxes are imposed:
- (1) For the privilege of selling, licensing, leasing, or renting tangible personal property and the items, property, and goods specified under s. 77.52 (1) (b), (c), and (d), and for the privilege of selling, licensing, performing, or furnishing services a sales tax is imposed upon retailers at the rates under s. 77.70 in the case of a county tax, at the rate under s. 77.708 in the case of a transit authority tax, or at the rate under s. 77.705 or 77.706 in the case of a special district tax of the sales price from the sale, license, lease, or rental of tangible personal property and the items, property, and goods specified under s. 77.52 (1) (b), (c), and (d), except property taxed under sub. (4), sold, licensed, leased, or rented at retail in the county, or special district, or transit authority's jurisdictional area, or from selling, licensing, performing, or furnishing services described under s. 77.52 (2) in the county, or special district, or transit authority's jurisdictional area.
- (2) An excise tax is imposed at the rates under s. 77.70 in the case of a county tax, at the rate under s. 77.708 in the case of a transit authority tax, or at the rate under s. 77.705 or 77.706 in the case of a special district tax of the purchase price upon every person storing, using, or otherwise consuming in the county, or special district, or transit authority's jurisdictional area tangible personal property, or items, property, or goods specified under s. 77.52 (1) (b), (c), or (d), or services if the tangible personal property, item, property, good, or service is subject to the state use tax under s. 77.53, except that a receipt indicating that the tax under sub. (1), (3), or (4) has been paid relieves the buyer of liability for the tax under this subsection and except that if the buyer has paid a similar local tax in another state on a

purchase of the same tangible personal property, item, property, good, or service that tax shall be credited against the tax under this subsection and except that for motor vehicles that are used for a purpose in addition to retention, demonstration, or display while held for sale in the regular course of business by a dealer the tax under this subsection is imposed not on the purchase price but on the amount under s. 77.53 (1m).

- (3) An excise tax is imposed upon a contractor engaged in construction activities within the county, or special district, or transit authority's jurisdictional area, at the rates under s. 77.70 in the case of a county tax, at the rate under s. 77.708 in the case of a transit authority tax, or at the rate under s. 77.705 or 77.706 in the case of a special district tax of the purchase price of tangible personal property or items, property, or goods under s. 77.52 (1) (b), (c), or (d) that are used in constructing, altering, repairing, or improving real property and that became a component part of real property in that county or special district or in the transit authority's jurisdictional area, except that if the contractor has paid the sales tax of a county, transit authority, or special district in this state on that tangible personal property, item, property, or good, or has paid a similar local sales tax in another state on a purchase of the same tangible personal property, item, property, or good, that tax shall be credited against the tax under this subsection.
- (4) An excise tax is imposed at the rates under s. 77.70 in the case of a county tax, at the rate under s. 77.708 in the case of a transit authority tax, or at the rate under s. 77.705 or 77.706 in the case of a special district tax of the purchase price upon every person storing, using, or otherwise consuming a motor vehicle, boat, recreational vehicle, as defined in s. 340.01 (48r), or aircraft, if that property must be registered or titled with this state and if that property is to be customarily kept in a county that has in effect an ordinance under s. 77.70, the jurisdictional area of a transit authority that has in effect a resolution under s. 77.705 or 77.706, except that if the buyer has paid a similar local sales tax in another state on a purchase of the same property that tax shall be credited against the tax under this subsection.

SECTION 2183H. 77.73 (2) of the statutes is amended to read:

77.73 (2) Counties, and special districts, and transit authorities do not have jurisdiction to impose the tax under s. 77.71 (2) in regard to items, property, and goods under s. 77.52 (1) (b), (c), and (d), and tangible personal property, except snowmobiles, trailers, semitrailers, and all-terrain vehicles, purchased in a sale that is consummated in another county or special district in this state, or in another transit authority's jurisdictional area, that does not have in effect an ordinance or resolution imposing the taxes under this subchapter and later brought by the buyer into the county, or special district, or jurisdictional area of the transit authority that has imposed a tax under s. 77.71 (2).

SECTION 21831. 77.73 (3) of the statutes is amended to read:

77.73 (3) Counties, and special districts, and transit authorities have jurisdiction to impose the taxes under this subchapter on retailers who file, or who are required to file, an application under s. 77.52 (7) or who register, or who are required to register, under s. 77.53 (9) or (9m), regardless of whether such retailers are engaged in business in the county, or special district, or transit authority's jurisdictional area, as provided in s. 77.51 (13g). A retailer who files, or is required to file, an application under s. 77.52 (7) or who registers, or is required to register, under s. 77.53 (9) or (9m) shall collect, report, and remit to the department the taxes imposed under this subchapter for all counties, or special districts, and transit authorities that have an ordinance or resolution imposing the taxes under this subchapter.

SECTION 2183J. 77.75 of the statutes is amended to read:

77.75 Reports. Every person subject to county, transit authority, or special district sales and use taxes shall, for each reporting period, record that person's sales made in the county, or special district, or jurisdictional area of a transit authority that has imposed those taxes separately from sales made elsewhere in this state and file a report as prescribed by the department of revenue.

SECTION 2183K. 77.76 (1) of the statutes is amended to read:

77.76 (1) The department of revenue shall have full power to levy, enforce, and collect county, transit authority, and special district sales and use taxes and may take any action, conduct any proceeding, impose interest and penalties, and in all respects proceed as it is authorized to proceed for the taxes imposed by subch. III. The department of transportation and the department of natural resources may

administer the county, transit authority, and special district sales and use taxes in regard to items under s. 77.61 (1).

SECTION 2183L. 77.76 (2) of the statutes is amended to read:

77.76 (2) Judicial and administrative review of departmental determinations shall be as provided in subch. III for state sales and use taxes, and no county, transit authority, or special district may intervene in any matter related to the levy, enforcement, and collection of the taxes under this subchapter.

SECTION 2183M. 77.76 (3r) of the statutes is repealed.

SECTION 2183N. 77.76 (4) of the statutes is amended to read:

77.76 (4) There shall be retained by the state 1.5% of the taxes collected for taxes imposed by special districts under ss. 77.705 and 77.706 and transit authorities under s. 77.708 and 1.75% of the taxes collected for taxes imposed by counties under s. 77.70 to cover costs incurred by the state in administering, enforcing, and collecting the tax. All interest and penalties collected shall be deposited and retained by this state in the general fund.

SECTION 21830. 77.76 (5) of the statutes is repealed.

SECTION 2183P. 77.77 (1) of the statutes is amended to read:

77.77 (1) (a) The sales price from services subject to the tax under s. 77.52 (2) or the lease, rental, or license of tangible personal property and property, items, and goods specified under s. 77.52 (1) (b), (c), and (d), is subject to the taxes under this subchapter, and the incremental amount of tax caused by a rate increase applicable to those services, leases, rentals, or licenses is due, beginning with the first billing period starting on or after the effective date of the county ordinance, special district resolution, transit authority resolution, or rate increase, regardless of whether the service is furnished or the property, item, or good is leased, rented, or licensed to the customer before or after that date.

(b) The sales price from services subject to the tax under s. 77.52 (2) or the lease, rental, or license of tangible personal property and property, items, and goods specified under s. 77.52 (1) (b), (c), and (d), is not subject to the taxes under this subchapter, and a decrease in the tax rate imposed under this subchapter on those services first applies, beginning with bills rendered on or after the effective date of the repeal or sunset of a county ordinance, or special district resolution, or transit authority resolution imposing the tax or other rate decrease, regardless of whether the service is furnished or the property, item, or good is leased, rented, or licensed to the customer before or after that date.

SECTION 21830. 77.77 (3) of the statutes is amended to read:

77.77 (3) The sale of building materials to contractors engaged in the business of constructing, altering, repairing or improving real estate for others is not subject to the taxes under this subchapter, and the incremental amount of tax caused by the rate increase applicable to those materials is not due, if the materials are affixed and made a structural part of real estate, and the amount payable to the contractor is fixed without regard to the costs incurred in performing a written contract that was irrevocably entered into prior to the effective date of the county ordinance, special district resolution, transit authority resolution, or rate increase or that resulted from the acceptance of a formal written bid accompanied by a bond or other performance guaranty that was irrevocably submitted before that date.

SECTION 2183R. 77.78 of the statutes is amended to read:

77.78 Registration. No motor vehicle, boat, snowmobile, recreational vehicle, as defined in s. 340.01 (48r), trailer, semitrailer, all-terrain vehicle or aircraft that is required to be registered by this state may be registered or titled by this state unless the registrant files a sales and use tax report and pays the county tax, transit authority tax, and special district tax at the time of registering or titling to the state agency that registers or titles the property. That state agency shall transmit those tax revenues to the department of revenue.

SECTION 2187D. Subchapter XIII (title) of chapter 77 [precedes 77.997] of the statutes is repealed.

SECTION 2187F. 77.9971 of the statutes is repealed.

SECTION 2187H. 77.9972 of the statutes is repealed.

SECTION 2187J. 77.9973 of the statutes is renumbered 77.9973 (1).

SECTION 2187L. 77.9973 of the statutes, as affected by 2011 Wisconsin Act ... (this act), is repealed. **SECTION 2187N.** 77.9973 (2) of the statutes is created to read:

77.9973 (2) Retailers and the department of revenue may not collect fees under this subchapter for the southeastern regional transit authority after the effective date of this subsection [LRB inserts date], except that the department of revenue may collect from retailers fees that accrued before the effective date of this subsection [LRB inserts date], and fees, interest, and penalties that relate to those taxes.

SECTION 2237E. 85.062 (3) (c) of the statutes is repealed.

SECTION 2237M. 85.063 (3) (b) 1. of the statutes is amended to read:

85.063 (3) (b) 1. Upon completion of a planning study under sub. (2), or, to the satisfaction of the department, of a study under s. 85.022, a political subdivision in a county, or a transit authority created under s. 66.1039, that includes the urban area may apply to the department for a grant for property acquisition for an urban rail transit system.

SECTION 22370. 85.064 (1) (b) of the statutes is amended to read:

85.064 (1) (b) "Political subdivision" means any city, village, town, county, <u>or</u> transit commission organized under s. 59.58 (2) or 66.1021 or recognized under s. 66.0301, <u>or transit authority created under s. 66.1039</u> within this state <u>or the southeastern regional transit authority under s. 59.58 (7)</u>.

SECTION 3182G. 345.05 (1) (ag) of the statutes is repealed.

SECTION 3182R. 345.05 (2) of the statutes is amended to read:

345.05 (2) A person suffering any damage proximately resulting from the negligent operation of a motor vehicle owned and operated by a municipality or authority, which damage was occasioned by the operation of the motor vehicle in the course of its business, may file a claim for damages against the municipality or authority concerned and the governing body of the municipality, or the board of directors of the authority, may allow, compromise, settle and pay the claim. In this subsection, a motor vehicle is deemed owned and operated by a municipality or authority if the vehicle is either being rented or leased, or is being purchased under a contract whereby the municipality or authority will acquire title.

SECTION 3471M. 611.11 (4) (a) of the statutes is amended to read:

611.11 (4) (a) In this subsection, "municipality" has the meaning given in s. 345.05 (1) (c), but also includes any transit authority created under s. 66.1039.

SECTION 3567M. 2009 Wisconsin Act 28, section 9150 (1) is repealed. (Section in guestion in italics here – EAF)

2009 WISCONSIN ACT 28 SECTION 9150. Nonstatutory provisions; Transportation.

- (1) Transit authorities.
- (b) Initial terms of Dane County regional transit authority. Notwithstanding the length of terms specified for members of the board of directors of the Dane County transit authority under section 66.1039 (2) (b) and (3) (a) of the statutes, as created by this act, the initial terms for the members appointed under section 66.1039 (3) (c) 1. and 4. of the statutes, as created by this act, shall be two years.
- (c) Initial terms of Chippewa Valley regional transit authority. Notwithstanding the length of terms specified for members of the board of directors of the Chippewa Valley regional transit authority under section 66.1039 (2) (c) and (3) (a) of the statutes, as created by this act, the initial terms shall be 2 years for each of the following:
- 1. One of the members appointed under section 66.1039 (3) (d) 1. b. of the statutes, as created by this act.
- 2. One of the members appointed under section 66.1039 (3) (d) 1. c. of the statutes, as created by this act, if applicable.
 - 3. Each member appointed under section 66.1039 (3) (d) 1. d. of the statutes, as created by this act.

- (d) Initial terms of Chequamegon Bay regional transit authority. Notwithstanding the length of terms specified for members of the board of directors of the Chequamegon Bay regional transit authority under section 66.1039 (2) (e) and (3) (a) of the statutes, as created by this act, the initial terms shall be 2 years for each of the following:
- 1. One member from each county appointed under section 66.1039 (3) (f) 1. b. of the statutes, as created by this act.
- 2. Each of the members appointed under section 66.1039 (3) (f) 1. c. of the statutes, as created by this act.

SECTION 9148. Nonstatutory provisions; Transportation.

- (1) CERTIFICATES OF TITLE. Notwithstanding chapter 342 of the statutes, as affected by this act, beginning on the effective date of this subsection, the department of transportation may, for 6 months after the effective date of this subsection, issue and deliver certificates of title under applicable provisions of chapter 342 of the statutes that are in effect on the day before the effective date of this subsection.
 - (3u) DISSOLUTION AND WINDING DOWN OF TRANSIT AUTHORITIES.
- (a) Any authority created under section 66.1039, 2009 stats., is dissolved on the effective date of this paragraph.
- (b) The authority under section 59.58 (7), 2009 stats., is dissolved on the effective date of this paragraph.
- (c) After the effective date of this paragraph, the counties of Kenosha, Racine, and Milwaukee, and all members of the governing body of the authority under section 59.58 (7) of the statutes, shall begin the process of winding down the authority and shall complete the process by the time the authority is dissolved as provided in paragraph (b). All assets and liabilities of the authority under section 59.58 (7), 2009 stats., including any accumulated moneys received from the fees imposed under subchapter XIII of chapter 77 of the statutes, shall become the assets and liabilities of the counties of Kenosha, Racine, and Milwaukee and shall be divided and distributed as follows:
 - 1. Fifty percent to Milwaukee County.
 - 2. Twenty-five percent to Kenosha County.
 - 3. Twenty-five percent to Racine County.

SECTION 9448. Effective dates; Transportation.

- (1) CERTIFICATES OF TITLE. The treatment of sections 218.0171 (2) (c) and (cm) 2., 218.23 (1), 342.13 (1), 342.15 (1) (a) and (c) and (5), 342.20 (1), 342.22 (1) (intro.) and (2), and 342.23 (2) (a) and (b) and (4) of the statutes, the renumbering and amendment of sections 342.09 (1) of the statutes, and the creation of section 342.09 (1) (b) of the statutes and SECTIONS 9148 (1) and 9348 (2) of this act take effect on January 1, 2012.
- (2) IDENTIFICATION CARD RENEWALS. The repeal and recreation of sections 343.20 (2) (a) and 343.50 (4) and (6) of the statutes takes effect on July 1, 2011, on the day after publication, or on the date on which the creation of section 343.165 of the statutes by 2007 Wisconsin Act 20 takes effect, whichever is latest.
- (4) REAL ID NONCOMPLIANT OPERATOR'S LICENSES AND IDENTIFICATION CARDS. The treatment of sections 343.03 (3r), 343.06 (1) (L), 343.10 (7) (d), 343.11 (3) (by SECTION 3150), 343.14 (3) and (3m), 343.165 (1) (intro.), (2), (3) (a), (4) (a), (c), and (d), (5), and (7), and 343.17 (3) (a) 2. and 14. of the statutes and the repeal and recreation of sections 343.17 (5) and 343.50 (1), (3), and (4g) of the statutes take effect on July 1, 2011, on the day after publication, or on the date on which the creation of section 343.165 of the statutes by 2007 Wisconsin Act 20 takes effect, whichever is latest.
- (6g) SOUTHEAST WISCONSIN FREEWAY FUNDING. Section 9148 (7f) of this act takes effect on the day after publication, or retroactively to June 30, 2011, whichever is earlier.
 - (6u) TRANSIT AUTHORITIES.

- (a) The treatment of sections 59.58 (7) (e) (intro.), (i), and (j) and 66.1039 (4) (s) 1. of the statutes, the renumbering of section 77.9973 of the statutes, and the creation of sections 77.708 (3) and 77.9973 (2) of the statutes and SECTION 9148 (3u) (c) of this act take effect on the 10th day after the day of publication.
- (b) The treatment of sections 20.566 (1) (gc) and (gh), 20.835 (4) (gc) and (gh), 32.02 (11), 32.05 (1) (a), 32.07 (2), 40.02 (28), 59.58 (6), 66.0301 (1) (a) (by Section 1720b), 66.0903 (1) (d), 67.01 (5), 70.11 (2), 71.05 (1) (c) 9., 71.26 (1) (b), 71.26 (1m) (j), 71.45 (1t) (j), chapter 77 (title) (by Section 2177m), 77.54 (9a) (er), subchapter V (title) of chapter 77, 77.71, 77.73 (2) and (3), 77.75, 77.76 (1), (2), (3r), (4), and (5), 77.77 (1) and (3), 77.78, subchapter XIII (title) of chapter 77, 77.9971, 77.9972, 85.062 (3) (c), 85.063 (3) (b) 1., 85.064 (1) (b), 111.70 (1) (j), 345.05 (1) (ag), 345.05 (2), and 611.11 (4) (a) of the statutes, the repeal of sections 59.58 (7), 66.1039, 77.708, and 77.9973 of the statutes, and SECTIONS 3567m and 9148 (3u) (a) and (b) take effect on the 90th day after the day of publication.



Outlook for 2025 City Budget

Information Series on the General & Library Fund Budget Part 4: Revenue Strategies

Series Overview

Part 1: Budget Foundations

Understanding the City's Fund structure & main components of the Operating Budget

Part 2: The Structural Deficit

Internal and external factors driving the deficit

Part 3: Budget Balancing Strategies – Expenditures

Impact of Debt Service and Personnel Costs

Part 4: Budget Balancing Strategies – Revenues

Local Revenues, Special Charges, Property Tax

Additional topics to be determined

Part 4: Budget Balancing Strategies — Revenues

Takeaways:

- Madison was shortchanged by 2023 State Aid Legislation
- There are a limited number of non-property tax revenues
- There are tradeoffs and equity considerations to increasing user fees and/or pursuing a property tax referendum

Madison's Revenues in Context

Comparison with 35 largest Wisconsin Cities (over 20,000 in population)

					% of
	Per Capita	Rank	Average	Median	Average
Federal Aid	\$249	2	\$78	\$45	320%
General Property Taxes	\$928	4	\$694	\$663	134%
Fines, Forfeitures, Penalties	\$19	6	\$13	\$10	146%
Total Taxes	\$1,133	6	\$906	\$855	125%
All Other State Aids	\$59	8	\$51	\$33	117%
Total Intergovernmental Revenue	\$432	9	\$348	\$306	124%
Charges for Service	\$124	10	\$119	\$98	104%
Licenses and Permits	\$41	11	\$45	\$36	91%
Tax Increments (TIF)	\$131	14	\$159	\$121	83%
Total State Aid	\$176	23	\$256	\$240	69%
Shared Revenue (includes municipal, utility, exempt property and expenditure restraint aid)	\$71	25	\$151	\$143	47%
State Highway Aid	\$46	26	\$55	\$53	84%
General Revenues	\$1,815	5	\$1,540	\$1,507	118%
Total Revenues and Other Financing	\$2,529	1	\$1,900	\$1,853	133%

General State Aid

Total State Aid = \$46 million

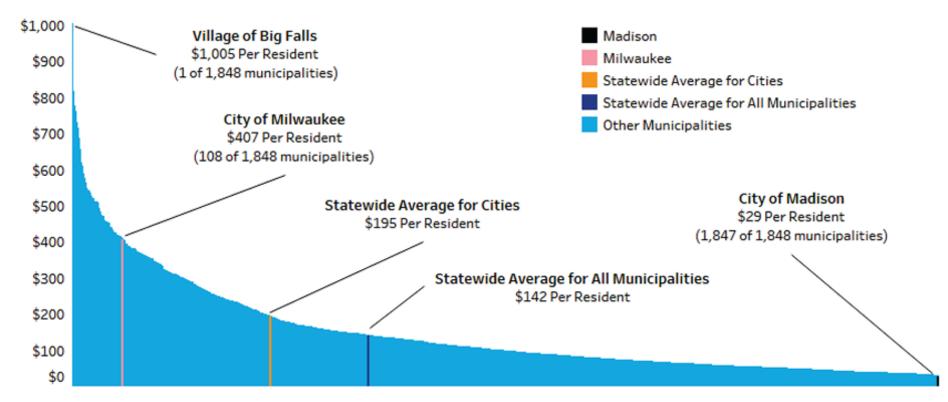
Revenue	2024 Budget
General Transportation Aid	\$13.3 million
Municipal Aid (Shared Revenue)	\$8.1 million
Municipal Services Program	\$8.0 million
Expenditure Restraint Incentive Program	\$6.9 million
Exempt Property Aid (computer, personal, utility)	\$6.3 million
Fire Insurance Dues	\$1.7 million
Recycling	\$0.8 million
Video Provider Aid (offset capped cable franchise fees)	\$0.5 million
Total	\$45.6 million

Excludes state transit aid (\$17 million) which is deposited in the Metro Transit enterprise fund.

Madison received lowest per capita increase from 2023 State Aid Legislation

Figure 11: Madison Now Second Lowest Among 1,848 Wisconsin Municipalities in Key Form of State Aid

Total state per capita county and municipal aid following the legislation known as Act 12, by municipality



Sources: Wisconsin Department of Administration, Legislative Fiscal Bureau, and WPF calculations

City has limited non-property tax revenues

- Local Revenues
 - Charges for services (e.g., ambulance fees)
 - Licenses and Permits (e.g., building permits)
 - Fines and Forfeitures (e.g., parking violations)
 - Other (e.g., room taxes, investment earnings)
- Total local revenues = \$55 million
 - \$27 million would require about a 50% increase in <u>all</u> local revenues

Top 10 Local Revenues

Total Local Revenue = \$55 million (excludes \$8.7 million of one-time ARPA and TID revenue)

Revenue	2024 Budget	Comments
Ambulance Fee	\$11.4 million	
Room Tax (30% general fund share)	\$6.4 million	70% must be spent on tourism marketing
Water Utility Payment-In-Lieu-of-Taxes (PILOT)	\$6.4 million	Formula set by state
Investment Earnings	\$6.0 million	Tied to interest rates and city cash balances
Building Permits	\$5.7 million	Can't exceed cost of issuance
Parking Violations	\$4.0 million	
Cable Franchise Fees	\$1.8 million	At maximum allowed by state
Engineering Service Charges	\$1.5 million	Tied to development activity
Parking Utility PILOT	\$1.5 million	Based on assessed value and tax rate
Clerk's Licenses (e.g., alcohol establishment)	\$1.1 million	Some license types are capped under state law
All Others	\$9.5 million	Approximately 35 different revenue types
Total	\$55.3 million	

Room Tax

Room Tax is the largest single local revenue source (\$20 million/year). State law requires that at least 70% be used for tourism-related activities; 30% for general fund. Collected from hotels, vacation rentals, etc. for transient occupancy stays of 30 days or less. Authorized and defined by state law, which sets a maximum rate of 8%, unless there is outstanding debt for construction or renovation of a convention center. Paid primarily by non-residents.

- Current rate = 10% of occupancy charge (last increased in 2018)
- 2023 Collections = \$20.4 million
- Yield: Each 1 percentage point (10% of total) = \$2 million
 - \$1.4 million for tourism marketing
 - \$0.6 million for general fund

Comparables:

- Brookfield (city) 10.5%
- Milwaukee 10%
- Green Bay 10%
- Middleton 8%
- Monona 8%
- Sun Prairie 7%
- Fitchburg 7%
- Verona 7%
- Waunakee 5%

Ambulance Fees

Paid for conveyance from scene to medical facility; two-thirds of trips paid by Medicare with fixed amount – fee increases do not generate revenue on those trips. Most non-Medicare trips paid by private insurance. City has waiver program based on income and household size.

- Current fee level = \$1,410 per conveyance (last increased in 2022)
- 2023 Collections = \$11.4 million
- Yield: \$100 fee increase = \$700,000
- New state Ground Emergency Medical Transport program may increase Medicaid funding to Madison by between \$1 million and \$3 million annually in 2025.

Surrounding communities:

- Middleton \$1,575
- Deerfield/Cottage Grove \$1,800 (ALS 2)
- Waunakee \$1,500
- Fitchburg/Verona \$1,300
- Monona \$1,200 (ALS 2)

Parking Violations

Paid violations of city ordinances related to parking restrictions. Approximately 80 different violations. Paid tickets are 25% lower than 2019; revenues are about 16% below 2019.

2023 Collections = \$4.2 million (2019 collections = \$5.0 million)

Violation Type	Fee Amount	Number Paid	Change since 2019	Last Increase
Street Sweeping Parking Restrictions	\$35	24,604	Up 16%	2010
Expired Parking Meter on Street	\$25	18,397	Down 4%	2014
Overtime in Two Hour Zone (8AM to 6PM)	\$40	10,231	Down 60%	2018
All Others	\$15 to \$150	24,438	Down 35% (half of the decrease due to large drop in violations for alternate side and snow emergency zone)	~2010

Local Vehicle Registration Fee ("Wheel Tax")

Authorized under state law to raise revenues for transportation purposes. Requires adoption of an ordinance implementing the fee. Revenues collected by the Wisconsin Division of Motor Vehicles with the state annual vehicle registration original or renewal fee. Revenues deposited in Metro Transit Fund. Adopted in 2020.

- Current rate = \$40 annually for each vehicle kept in city
- 2023 Collections = \$6.8 million
- Yield: Each \$10 = \$1.7 million total

Comparables:

- Evansville -- \$40
- Janesville -- \$40
- Oregon -- \$40
- Milwaukee (city) \$30
- Milwaukee (county) -- \$30
- Dane County -- \$28
- Green Bay \$20

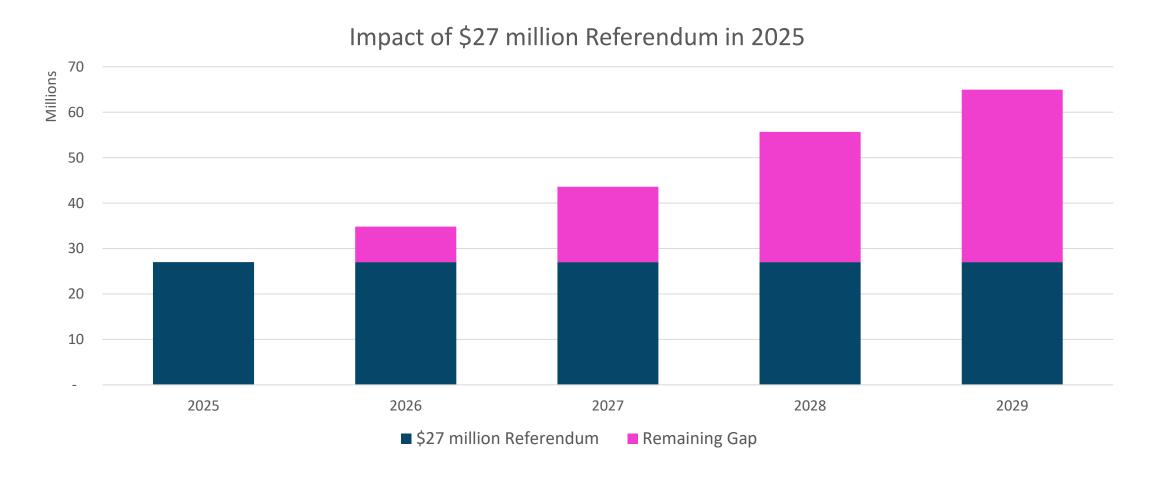
Special Charges

- State law allows the implementation of special charges to pay for the cost of specific services for which a "broad-based" public benefit can be identified.
- New fees for garbage collection, snow removal, fire protection, street sweeping, or storm water management do not result in net new revenue if those services were paid by the levy in 2013. If previously paid by levy, state law requires reduction in allowable levy equal to the new revenue.
- City currently has two special charges: urban forestry, resource recovery (recycling)
- Other Possible Options:
 - Transportation: Traffic Engineering, Streets Division street maintenance, Engineering, Metro subsidy (up to \$30 million)
 - Library: up to \$20 million
 - Parks: up to \$15 million
- Each \$1 per month per resident raises \$1 million in revenue
- \$27 million = \$27 per month or \$324 per year per resident

Property taxes

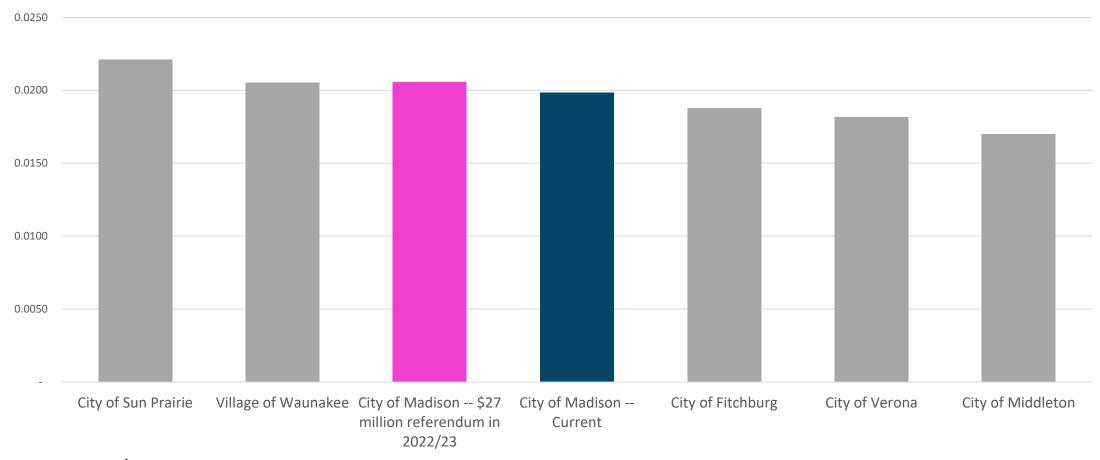
- Increase above levy limit requires voter approval
- Referendum ballot requires Council approval
- In even numbered years, referendum must be held at either Spring or Fall primary or general election
- Referendum allows one-time and on-going levy increases
- Increase is limited to a fixed dollar amount, not a percentage of levy each year in other words, its share of the budget decreases each year
- \$27 million = additional \$284 on average value home or about 9% additional increase above levy limit; equal to 3.7% additional on total tax bill for average value home

State Law Requires that Referendum be Flat Amount Does not Grow to Offset Continuing Growth in Costs



Comparable Property Tax Rates

If \$27 million referendum had occurred in 2022 tax year/2023 budget year



Reflects only a \$27 million increase in property taxes; other levels of government in the city (e.g., Madison Metropolitan School District) can also increase property taxes through referendum that would also increase the tax rate.

479

Property Taxes Compared to User Fees and Charges

	Property Taxes	User Fees and Charges
Calculations for increases	Annual increase limited to change in value of new construction and increase in general obligation debt service; limit can be exceeded through voter referendum 2023/2024 property taxes on an average value home (\$424,400) total \$7,341 (all taxing jurisdictions) 2023/2024 property tax rate (net of state credits) = 1.83%	Cannot exceed cost to provide specific service State law presumption that municipality has to prove fees do not exceed cost if challenged in court
Tax deductions for property owners	Property taxes (and state income taxes) are deductible on federal income taxes up to a combined total of \$10,000	Not deductible on federal income taxes
Who pays	Taxable properties	All properties, both taxable and tax- exempt

Equity Considerations: Property Taxes Compared to User Fees and Charges

	Property Taxes	User Fees and Charges
Equity considerations	Taxes calculated as a flat percentage of value; more tax paid by higher value properties	Fees assessed per property at a fixed amount
	More regressive than progressive income tax (i.e., higher rates as income increases), but more progressive than flat fees	Lower income property owners pay larger share of income for the fee than higher income property owners – most regressive revenue type

Equity Considerations: Property Taxes Compared to User Fees and Charges

Relief for payers State constitution uniformity clause prevents differential property tax rates or targeted property tax relief programs on the tax bill Ambulance fee waiver provides relief based on income and household size the tax bill through school levy tax credit, lottery credit, first dollar credit and, to a		Property Taxes	User Fees and Charges
lesser extent, through state aid to municipalities; state also provides targeted tax relief through income taxes (e.g., Homestead tax credit) City offers property tax assistance program for eligible seniors (reverse mortgage)	Relief for payers	State constitution uniformity clause prevents differential property tax rates or targeted property tax relief programs on the tax bill State provides broad-based tax relief on the tax bill through school levy tax credit, lottery credit, first dollar credit and, to a lesser extent, through state aid to municipalities; state also provides targeted tax relief through income taxes (e.g., Homestead tax credit) City offers property tax assistance program for eligible seniors (reverse	charges for low income households Ambulance fee waiver provides relief

Takeaways

- There are a limited number of non-property tax revenues
 - The City receives \$46 million from State Aid. On a per capita basis, the City receives less than most other large cities, and received the lowest per capita increase from the 2023 State Aid Legislation (Act 12)
 - The operating budget includes \$55 million in local revenues, which includes charges for services, licenses and permits, fines and forfeitures, and other source (e.g. Room Tax)
 - Increasing existing fees and/ or implementing new special charges are some options for closing the budget gap; some of these changes would require additional studies to determine the cost of services
- There are tradeoffs and equity considerations to increasing user fees and/or pursuing a property tax referendum
 - Another option to increase revenues is to pursue a property tax referendum, which would require approval by voters.
 - There are tradeoffs between pursuing revenue increases through user fees versus charges, including differences in who pays, how amounts are calculated, and potential relief to payers. These tradeoffs should be evaluated to make a policy decision on the budget.

Next Steps

Presentation to the Full City Council on February 13, 2024 @ 6:30pm

- Presentation will summarize the 4-part budget series, provide an opportunity to ask questions, and begin discussion on the approach for 2025
- Members of the public may register to speak on this topic during Public Comment or submit written comment to allalders@cityofmadison.com
- Alders may participate in discussion during the presentation; we recommend alders review the recording prior to the Council Meeting
 - Part 1: Budget Foundations
 - Part 2: Structural Budget Deficit
 - Part 3: Expenditure Strategies
 - Part 4: Revenue Strategies
- If alders or members of the public have specific questions you would like addressed during the presentation, email citybudget@cityofmadison.com

Closing the \$27 million gap will likely require multiple strategies

- As demonstrated in Part 3 of this budget series, closing the budget gap solely through expenditure reductions would require drastic actions that would cut back services to residents and have significant operational impacts
- At the same time, closing the budget gap solely through revenue increases will also have impacts on residents and taxpayers; issues related to equity and affordability must be considered
- Deciding on a path forward will require evaluating tradeoffs between strategies and may require taking multiple approaches

Revenues

- Create new special charges
- Increase existing local revenues
- Increase property tax ("levy") through voter referendum

Expenditures

- Reduce all/most agencies by same percentage
- Roll back new programs
- Cut positions/services
- Reduce employee compensation

Next Steps

- Continued discussions with the Council:
 - February 13 Overview and discussion
 - March 5 Step-by-step discussion of values, priorities and possible paths forward.
- Possible legislation to set direction for 2025 budget development
- Finance Department will develop various scenarios to help policymakers understand tradeoffs and assist with decision-making



2025 Operating Budget Update

Finance Committee June 10, 2024

Agenda

- 2025 Operating Outlook & Long Range Forecast
 - Cost to Continue Revenue Assumptions & Expenditures
 - One-time funding
 - Budget Gap
 - Long Range outlook
 - Council Guiding Principles

Current Estimated Budget Gap

(General and Library Funds)

Estimated Expenditures = \$431.4 million

Total expenses are \$26.0 million (6.4%) higher than 2024 adopted budget. Major changes:

- + \$14.5m: Salary + benefits increases
- + \$2.7m: Metro subsidy increase
- + \$500k: PHMDC subsidy
- + \$5m: Placeholder for GF Debt Service
- + \$1.47m: Citywide Purchased Services increases for technology costs (software maintenance contracts, PCI compliance, and credit card processing fees)
- \$1.22m: Adjusts expenses for odd-year election cycle

Estimated Revenues = \$409.4 million

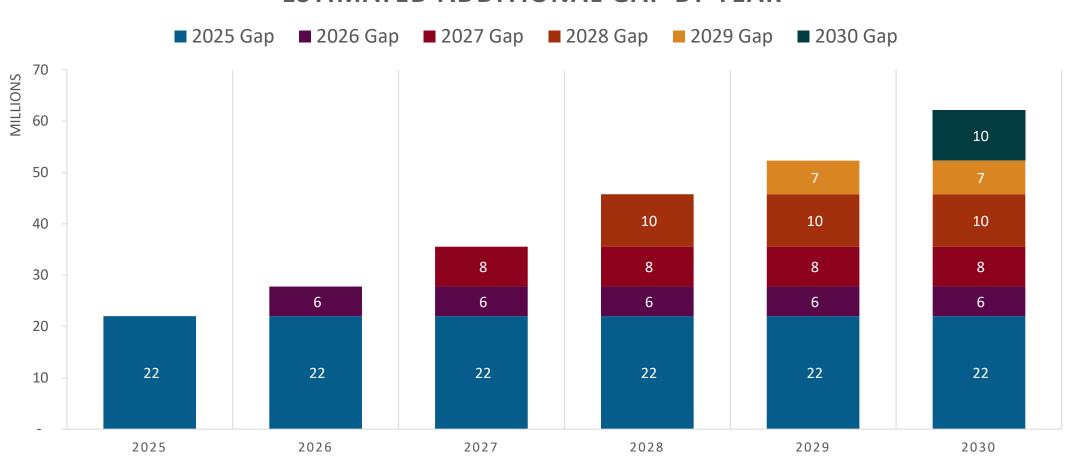
Total revenues are \$4.0 million higher than the 2024 adopted budget. Assumptions:

- + \$12.6m: Levy increase
- + \$6.0m: Increase in interest earnings
- + \$1.0m: Increase in ambulance conveyance fees
- \$17.9m: Removing one-time sources (\$9.2m fund balance, \$5.6m ARPA, \$3.1 TID)

Current Estimate = \$22.0 million

Long-Range Outlook

ESTIMATED ADDITIONAL GAP BY YEAR



Cost to Continue Adjustments: Categorized Adjustments (GF + Library)

Category	Assumptions
Personnel Costs	 \$14.5m increase in salaries and benefits Annualizes 6% GME increase phased in during 2024 (4% catch up effective January 2024; 2% catch up effective July 2024) Adds 3% increase for GMEs and protective service based on negotiated agreements General and Library funds GME 3% - \$3 million includes benefits Includes 1% step and longevity increase Annualizes positions funded for partial year in 2024 Includes funding for potential position reclassifications submitted to Human Resources \$2.97m placeholder for health insurance rate increases; \$500k placeholder for WRS rate increases (held in direct appropriations)
Elections	 Removes \$1.22m for elections (\$990k election officials; \$226k supplies and postage)
Direct Appropriations	Adds \$5.0m placeholder for Debt Service
Facilities & Utilities	 Adds \$489k for higher gas (+15%) and electricity (+5%) costs, Building Use charges (+10%)
Technology Costs	 Adds \$68,625 for PCI Compliance Adds \$1.44m for software maintenance contract increases Adds \$30,000 for credit card processing fees
ARPA	Removes \$1,225,600 transfer in to cover various CDD community agency contracts

Cost to Continue Adjustments: Categorized Adjustments (GF + Library)

Category	Assumptions
Metro Subsidy	• Adds \$2.7m to Metro subsidy (2025 total: \$18,225,000)
PHMDC Subsidy	Adds \$500k to PHMDC subsidy
Fleet Rate	• \$1.4 million increase. Highlights: \$758,000 debt; \$435,000 repairs and supplies; \$211,000 salaries and benefits; \$207,000 decrease fuel. (\$645,000 increase without debt adjustment)
Insurance & Worker's Comp	• Adds \$95,500 (\$485,000 increase in Insurance, \$389,500 decrease in Worker's Comp)
Professional Services	Adds \$125,900 for contractual increases
Park Maintenance	 Adds \$48,175 for operationalizing completed capital projects Adds \$128,900 for Mall maintenance, Door Creek Shelter & Country Grove Shelter operations
Office of Independent Monitor	Reduces budget by \$65,500 to remove one-time start-up funding.
Cost Allocation	 Updated plan increases billings to enterprise agencies by \$469,000.

Cost to Continue Adjustments: Salary & Benefits Increase (GF + Library)

- 2024 Adopted Salaries & Benefits: \$253.9 million
- 2025 Cost to Continue Salaries & Benefits: \$268.2 million
 - Represents a **5.63% increase** over 2024 Adopted Budget

Category	Additional Amount in 2025	
2025 Steps & Longevity (calculated at 1%)	\$2.0 million	
3% COLA for all employees	\$6.1 million	
 Benefits (includes placeholder for 11% increase in health) Health Insurance Commissioned Benefits (Retiree health, accidental death and dismemberment) WRS FICA Wage Insurance & VEBA 	\$5.7 million	
Reclasses; COLA and benefits for Hourly, Overtime, Premium Pay	\$500,000	

Salary Savings and Budget Efficiencies

Salary Savings

- 7.2% vacancy rate in first 5 months of 2024
- Created tiered structure in 2024 so small agencies with less turnover are not adversely impacted
 - Fewer than 20 FTEs = 0%
 - 20 49 FTEs = 0.5%
 - 50 99 FTEs = 1.0%
 - 100 199 FTEs = 3.0%
 - 200+ FTES = 4.0%
- 2025 salary savings = \$5.6m in GF + Library (3.2% of perm wages)

Budget Efficiencies

- Actual underspend varies by agency, but citywide average for last 3 years is 3.8%
- Cost-to-continue includes 2024 1% "savings" -- \$3.2 million

What options does the City have?

The operating budget must be balanced. How much we spend (expenditures) cannot be more than the money we bring (revenues).



Reduce Expenditures

Closing the budget gap through cuts alone will have significant operational impacts and will result in less services to residents.

- Reduce all/most agencies
- Cut specific programs or services
- Cut positions (layoffs) or reduce employee compensation (for example, saving \$22 million would require cutting 220 positions).



Increase Revenues

The City's options for raising revenues are limited by the State. For example, we cannot levy a local sales tax or income tax.

- Create new special charges
- Increase existing local revenues (e.g. fines, fees)
- Ask voters to increase the property tax ("levy") through a voter referendum

Council Guidance on 2025 Budget

Legistar 82456, adopted April 16, 2024

Values and Priorities

- 1. Maintaining services to residents
- 2. Preventing layoffs or furloughs of city staff.
- 3. Maintaining wage parity for general municipal employees
- 4. Meeting the needs of a growing city.
- 5. Choosing the most progressive revenue options that consider housing affordability.

Guidance on Developing 2025 Budget

- 1. Evaluate service levels, staffing and fiscal impact.
- 2. Seek ways to increase efficiencies in operations.
- 3. Explore a reduction in expenditures from cost-tocontinue levels that does not compromise services to residents.
- 4. Develop referendum language to increase property tax to meet some or all of deficit, as well as options to replace existing non-property tax revenues.
- 5. Prepare options other than property tax, including special charges
- 6. Develop a multi-year plan, including options for new, or increasing existing, fees and charges.
- 7. Consider the level of overall general obligation borrowing in 2025 capital budget to reduce rate of growth in property taxes
- 8. Develop a community engagement plan to educate the public.

Key Dates for 2025 Budget Development

(All dates in 2024)

	Capital	Operating	
Kick-Off Meeting	Tuesday, March 19 11:00am	Monday, June 17 1:00pm	
Agency Requests Due	Friday, April 19	Friday, July 19	
Agency Briefings with Mayor's Office	May 8 – 17	August 12 – 14	
Executive Budget introduced to Common Council (CC)	Tuesday, September 10	Tuesday, October 8	
Finance Committee (FC) Briefings	September 16 – 17	October 14 – 15	
FC Amendment Meeting	September 30	October 28	
CC Amendment Week	October 30 – November 8		
CC Budget Adoption Meetings	November 12 – 14		

Detailed Calendar – June and July

Date	Activity
June 10	2025 Operating Budget Update to Finance Committee
June 17	Operating budget instructions to agencies
June 18	2025 Operating Budget Update to Council
June 24	Broad options discussion with Finance Committee
July 2	Briefing and Discussion at Council; Referendum resolution introduced
July 8	Referendum resolution discussed at Finance Committee
July 16	Referendum resolution discussion
August 6	Last scheduled Council meeting before 70-day deadline for referendum ballot resolution



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83872

File ID: 83872 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: FINANCE

COMMITTEE

File Created Date: 06/10/2024

Final Action:

File Name: Authorizing the execution of agreements and

associated easements with Madison Gas and Electric Company relating to the installation,

operation, maintenance and repair of electric vehicle charging stations at the Yahara Hills Golf Course.

(District 16)

Title: Authorizing the execution of agreements and associated easements with Madison

Gas and Electric Company relating to the installation, operation, maintenance and

repair of electric vehicle charging stations at the Yahara Hills Golf Course.

(District 16)

Notes:

Sponsors: Yannette Figueroa Cole Effective Date:

Attachments: Yahara Hills Golf Course Charging MGE redlines 06 Enactment Number:

06 24.pdf, Master

Author: Lisa Laschinger Hearing Date:

Entered by: nmiller@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Parks Division	06/10/2024	Referred for Introduction				
	Action Text:	This Resolution was Re-	ferred for Introduction				
	Notes:	Finance Committee (6/24/2	4), Board of Park Commis	sioners (7/10/24), Common (Council (7/16/24)		
1	COMMON COUN	NCIL 06/18/2024	Referred	FINANCE COMMITTEE		06/24/2024	
	Action Text: Notes:	This Resolution was Re Additional referral to Board		COMMITTEE			
1	FINANCE COMM	MITTEE 06/18/2024	Referred	BOARD OF PARK COMMISSIONER S		07/10/2024	
	Action Text:	This Resolution was Re	ferred to the BOARD C	F PARK COMMISSIONE	ERS		
1	FINANCE COMM	MITTEE 06/24/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass

Action Text: A motion was made by Figueroa Cole, seconded by Rummel, to RECOMMEND TO COUNCIL TO

ADOPT - REPORT OF OFFICER. The motion passed by voice vote/other.

1 BOARD OF PARK 07/10/2024 Return to Lead with Finance

COMMISSIONERS the Department Recommendation for

Approval

Action Text: Assistant Superintendent Lisa Laschinger provided overview of proposed agreement and answered

questions.

Price answered questions regarding EV charging strategies across the City

Motion made by Miller Fienen, seconded by Figueroa Cole, to RETURN TO LEAD WITH THE RECOMMENDATION FOR APPROVAL TO THE FINANCE COMMITTEE. Motion passed by voice

vote/other.

Text of Legislative File 83872

Fiscal Note

The proposed resolution authorizes the Madison Gas and Electric Company to install and operate electric vehicle charging stations at the Yahara Hills Golf Course. The City does not receive revenue from this agreement. No City appropriation is required.

Title

Authorizing the execution of agreements and associated easements with Madison Gas and Electric Company relating to the installation, operation, maintenance and repair of electric vehicle charging stations at the Yahara Hills Golf Course. (District 16)

Body

WHEREAS, Madison Gas and Electric Company desires to install, and after such installation own, operate, maintain, repair, remove, upgrade and reinstall electric vehicle charging stations and related facilities ("Charging Stations") in the City of Madison ("City"), including on City-owned property; and,

WHEREAS, the City is committed to sustainable and renewable energy policies and the installation of Charging Stations in the City, and the use thereof by plug-in hybrid and electric vehicles, is in the public's interest as it will help reducing oil dependence and greenhouse gas emissions; and,

WHEREAS, the Charging Stations will be available for use by residents and visitors of the City; and

WHEREAS, the Yahara Hills Golf Course is readily accessible to the public, can accommodate Charging Stations, and placement of Charging Stations at this location will aid the success of the City's goal of sustainable and renewable energy policies.

NOW, THEREFORE, BE IT RESOLVED that the Common Council finds that it is in the City's and the public's interest to support Madison Gas and Electric Company's request by providing Madison Gas and Electric Company with dedicated locations in the Yahara Hills Golf Course parking lot for the installation, operation, maintenance and repair of electric vehicle charging stations and related facilities; and.

BE IT FURTHER RESOLVED that the Mayor and City Clerk are authorized to enter into agreements and associated easements with Madison Gas and Electric Company relating to the installation, operation, maintenance and repair of Charging Stations at the Yahara Hills Golf

Pass

Course; and,

BE IT FURTHER RESOLVED that the agreements shall be for an initial period of ten (10) years, with one (1) year renewals, and the terms and conditions thereof shall be consistent with the agreement attached hereto and incorporated herein; and,

BE IT FURTHER RESOLVED that the locations of the Charging Stations and any associated easements or distribution facilities shall be as generally depicted on Exhibit A to the charging station agreement, with final approval of actual Charging Stations to be made by the Golf Operations Director.

AGREEMENT RELATING TO CHARGING STATION (Yahara Hills Golf Course)

This Agreement Relating to Charging Station (the "Agreement") is entered into as of the last signature date set forth below ("Effective Date"), by and between Madison Gas and Electric Company, a Wisconsin corporation ("MGE"), and the City of Madison, a Wisconsin municipal corporation ("City") (individually, "Party," and collectively, "Parties").

RECITALS

City is the owner of certain land and improvements located at 7051 Millhond Rd

Λ.	Madison, Wisconsin, which land is known as the Yahara Hills Golf Course (the	
	"Property").	
В.	MGE desires to install, and after such installation own, operate, maintain, repair, ren	m

В.	MGE desires to install, and after such installation own, operate, maintain, repair, remove
	upgrade, and/or reinstall () charging station(s) containing a total of ()
	ports and related facilities (the "Charging Station(s)") on the Property.

C. City desires to accommodate MGE with regard to the Charging Station(s) upon the terms and conditions set forth below.

AGREEMENT

NOW, THEREFORE, for good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged, City and MGE agree as follows:

- 1. **Recitals.** The recitals are incorporated herein and made a part of this Agreement.
- 2. **Right to Install, Operate, Maintain, Repair, Remove, Upgrade, and Reinstall Charging Station(s).** During the term of this Agreement, MGE has the right to, and shall install, operate, maintain, repair, remove, upgrade, and/or reinstall the Charging Station(s), as well as any electric distribution facility equipment needed to serve the Charging Station(s), on the Property (collectively, the "Activities") upon the terms and conditions set forth in this Agreement. The location of the Charging Station(s) and the parking spaces to be served by the Charging Station(s) are depicted in the description and diagram attached hereto as Exhibit A (the "Premises"). The Activities shall be performed by MGE (or on behalf of MGE) at MGE's expense. MGE may install protective bollards to protect the Charging Station(s).
- 3. **Access Over Property.** City grants and conveys to MGE and MGE's employees, agents, contractors, and consultants, reasonable access over the Property and to the electric distribution facility equipment in order for MGE to have access to the Charging Station(s) and to perform the Activities.
- 4. **City Cooperation**. City agrees that it will cooperate in good faith with MGE and its employees, agents, and consultants with regard to the Charging Station(s) and the Activities. City agrees to mark all private underground facilities in the trench route for any electric facility

distribution equipment installed by MGE to serve the Charging Station(s). MGE is not responsible for damage to unmarked private underground facilities in the trench route. City will not charge MGE any fees or payments for the rights granted to MGE in this Agreement.

- 5. **Design of Charging Stations**. MGE will collaborate with City to ensure placement and construction of Charging Stations ensure public safety and do not create undue maintenance and upkeep burdens. If deemed necessary, safety lighting will be installed and maintained at MGE expense.
- 6. **City's Representations and Warranties**. City represents that (a) it is the sole owner of the Property and (b) the terms of this Agreement are not in violation of any mortgage or other agreement to which City is a party.
- 7. Ownership of Charging Station(s). City and MGE agree that at all times during this Agreement, MGE shall be the sole and complete owner of the Charging Station(s) and all components thereof. City and its successors and assigns shall have no right, title, or interest in the Charging Station(s) or any component thereof. The Parties intend that the Charging Station(s) remains at all times personal property and not a fixture under applicable law, even if the Charging Station(s) or any part thereof is affixed or attached to real property or any improvements.
- 8. **City's Premise Responsibilities for area Surrounding Charging Stations**. City will be responsible for parking striping and will perform ongoing maintenance and upkeep of areas of pavement and parking lot surrounding Premises, including mowing, tree pruning, trash collection, etc. Snow removal will be performed consistent with existing service levels during normal working hours and days.
- 9. **MGE Premises Responsibilities for Charging Stations**. MGE is responsible for all maintenance and repair of Charging Stations. Charging Stations must be kept in neat and orderly fashion, including any cables neatly stored when not in use. MGE may contract or arrange for snow removal services of driveway, premise and area surrounding charging stations to achieve higher service standard than can be provided by City.
- 10. **Storage**. No storage of materials or equipment is permitted at or around Charging Stations without express permission from the City's Parks Division.
- 11. **Duty to Notify**. City and MGE shall promptly report to each other any problems or functional issues with the Charging Station(s) or the Premises that come to either's attention. MGE shall have the right, upon reasonable advance notice to City, to perform any of the Activities; provided, however, that in performing the Activities, MGE shall use reasonable care to avoid undue interference or disruption to City, its employees, and invitees to the Property. City shall not remove the Charging Station(s) without MGE's prior written consent. MGE will provide contact information on each Charging Station in the event users encounter issues or have questions.
- 12. **Non-Interference with Charging Station(s)**. City covenants for itself and its employees, agents, invitees, successors, and assigns that it shall neither do nor permit to be done any of the

Activities without MGE's prior written consent or any action that would tend to damage, impair, debilitate, shorten the useful life of, or otherwise work to the detriment of the Charging Station(s). The foregoing covenant shall require City, without limitation, to provide prior notice to MGE of any intended modifications, repairs, or maintenance to the Property that could have the potential to adversely impact the Charging Station(s). City further agrees to coordinate any such modifications, repairs, or maintenance with MGE and to carry them out in a manner that will avoid damage to or undue interference with the Charging Station(s) or the use of the Charging Station(s) by vehicles. MGE recognizes that City may need to temporarily restrict access to the Premises and the parking lot to repave the surface, which temporary closure shall not be contrary to this provision. Such temporary closure shall be only as long as necessary for the repaving work. In the event any damage is caused to the Charging Station(s) during the term of this Agreement for any reason, City agrees to immediately notify MGE of such damage. City shall keep the Premises reasonably free of ice and snow, consistent with Sections 8 and 9 above.

- 13. **Use of the Charging Station(s).** MGE and the general public may use the Charging Station(s) to charge plug-in electric vehicles. Vehicles using the Charging Station(s) may park on the Premises. MGE may charge a fee for use of Charging Stations, pursuant to local, state and federal laws. For Level 2 Charging Stations, additional "After Fees" may not be charged for use less than 5 hours to accommodate a standard 18-hole round of golf.
- 14. **Term/Extension**. The initial term of this Agreement shall be for ten (10) years ("Initial Term"), commencing upon the Effective Date. Upon the expiration of the Initial Term, this Agreement shall automatically renew for successive one- (1) year terms ("Renewal Term") and upon the same terms and conditions, unless either Party provides written notice to the other Party at least ninety (90) days before the expiration of the Initial Term or any Renewal Term of its desire to terminate this Agreement.
- 15. **Removal of Charging Station(s)**. MGE shall remove, or cause the removal of, the Charging Station(s) within ninety (90) days of the termination of the Agreement and repair the portions of the Premises upon which they were located to reasonably match the condition of such portions at the time of installation, unless City has purchased the Charging Station(s) from MGE pursuant to mutually-agreed written terms. MGE may leave in place, and shall not be required to remove, any underground electric cable or conduit used to service the Charging Station(s).
- 16. **Termination of Agreement**. This Agreement shall terminate upon the earliest to occur of the following: (a) the expiration of the Initial Term (or if extended, a Renewal Term) if either Party has provided notice of its desire to terminate the Agreement under Section 14 above, (b) by mutual agreement of MGE and City, (c) upon MGE's election to terminate this Agreement for safety reasons or by order of any regulatory body, (d) upon City's election to terminate this Agreement following an Event of Default (as defined in Section 24) by MGE under this Agreement, or (e) upon MGE's election to terminate this Agreement following an Event of Default (as defined in Section 24) by City under this Agreement or MGE's receipt of notice of a proceeding under Section 24.b.
- 17. **DISCLAIMER.** The Charging Station(s) is provided on an "As Is" basis. MGE expressly disclaims all warranties of any kind, whether express or implied, including but not limited to, the implied warranties of merchantability, fitness for a particular purpose, and non-

infringement. MGE makes no warranty that the Charging Station(s) will operate uninterrupted or error free.

- 18. **Amendment**. This Agreement shall not be amended without the prior written consent of MGE and City.
- 19. **Assignment of Agreement/Sale of Property**. Neither Party hereto shall be entitled to sell or assign all or any part of its interest in this Agreement or in the Charging Station(s) without the prior written consent of the other except as follows:
 - a. If City wishes to sell the Property, City shall be entitled to do so as long as City incorporates into a binding contract to sell the Property a provision that discloses the existence of this Agreement and includes an assignment of this Agreement which obligates the purchaser to assume in writing all obligations of City under this Agreement or pay to have MGE remove the Charging Station(s). City shall provide MGE with thirty (30) days' advance written notice of the sale of the Property and assignment and assumption of this Agreement by the purchaser.
 - b. MGE shall be permitted to assign its interest in the Charging Station(s) and this Agreement, without the written consent of City, to a subsidiary or an affiliate or to an unrelated third party in connection with a merger, consolidation, or restructuring transaction. MGE shall provide City with written notice of the sale or assignment.
- 20. **Compliance with Laws**. MGE shall comply with all applicable federal, state, and local laws, rules, regulations, ordinances, permits, and orders governing the Activities. This shall include, without limitation, any applicable rules, orders, or tariffs of the Public Service Commission of Wisconsin. City shall comply with all applicable federal, state, and local laws, rules, regulations, ordinances, permits, and orders relating to the Property and the Premises.
- 21. **Taxes**. City shall pay the personal property and real property taxes, if any, pertaining to the Property. MGE shall pay any personal property tax pertaining to the Charging Station(s).
- 22. **Responsibility for Acts/Limitation of Liability**. Each party shall be responsible for its own acts, errors, or omissions and for the acts, errors, or omissions of its employees, officers, officials, agents, boards, committees, and commissions, and shall be responsible for any losses, claims, and liabilities that are attributable to such acts, errors, or omissions, including providing its own defense, arising out of this Agreement. In situations involving joint liability, each party shall only be responsible for such losses, claims, and liabilities that are attributable to its own acts, errors, or omissions and the acts, errors, or omissions of its employees, officers, officials, agents, boards, committees, and commissions. In no event will either Party, its affiliates, subsidiaries, or respective directors, officers, employees, or agents be liable for any actions, damages, claims, liabilities, costs, expenses, or losses in any way arising out of or relating to these terms and conditions arising out of the acts, errors, or omissions of the other Party or those of the other Party's employees, agents, boards, commissions, agencies, officers, and representatives. It is not the intent of either party to waive, limit, or otherwise modify the protections and limitations of liability found in Wis. Stat. § 893.80 or any other protections

available to the parties by law. This paragraph shall survive the termination or expiration of this Agreement.

- 23. **Insurance**. MGE shall carry, and shall require its installation contractors to carry, commercially reasonable amounts of liability insurance during the installation or reinstallation of the Charging Station(s). MGE shall be responsible for carrying liability insurance coverage upon the Charging Station(s), and City shall be responsible for carrying commercially reasonable amounts of commercial general liability insurance and property damage insurance for the location of the Property and upon the other improvements upon the Property. MGE shall be responsible for the reasonable costs of any damage to City's real or personal property caused by the Charging Station(s) or the Activities.
- 24. **Default**. Any of the following events shall constitute an "Event of Default" under this Agreement:
 - a. If either Party shall become insolvent or become the subject of any bankruptcy, insolvency, or similar proceeding, which has not been terminated, stayed, or dismissed within sixty (60) business days after it was commenced.
 - b. If City shall become the subject of any proceeding to foreclose any interest in the Property or improvements thereon. City shall provide MGE written notice of any such proceeding as soon as practicable but in no case more than sixty (60) days after it is commenced.
 - c. If any material representation made under this Agreement shall have been materially false or misleading when made.
 - d. If either Party shall default in the performance of any other obligation under this Agreement.

Notwithstanding the forgoing, any default in the performance under Section 24.c. of this Agreement shall not be deemed to constitute an "Event of Default" unless the Party in default shall have failed to cure such default within thirty (30) days after receiving written notice from the other in accordance with the terms of this Agreement.

- 25. **Remedies**. Following an Event of Default (as defined in Section 24), the Party not in default may exercise one or more of the following remedies:
 - a. Terminate this Agreement and cause the removal of the Charging Station(s) at the expense of the Party in default.
 - b. Cure the Event of Default and obtain reimbursement from the other Party.
 - c. Exercise any remedy available at law or in equity.
- 26. **MGE's Right to Show Charging Station(s)**. MGE shall have the right to show the Charging Station(s) to interested persons.

- 27. MGE's Right to Erect Signs, Pavement Markings and Informational Displays.
- MGE, at MGE's expense, shall have the right to erect and maintain informational signage to be located immediately adjacent to the Charging Station(s). The location of the signage is subject to the reasonable approval of City. The contents of the signage may, among other things, identify MGE as the owner of the Charging Station(s) and provide information about the purpose and use of the Charging Station(s) and must be approved by Parks Superintendent or designee. All signs, displays, markings must be in compliance with Madison General Ordinance requirements.
- 28. **City's Right to Erect Signs and Informational Displays**. City, at City's expense, shall have the right to erect and maintain informational signage to be located adjacent to the Charging Station(s) in a way and location that does not block or interfere with MGE's signage pursuant to Section 27 above. The contents of the signage may encourage or limit parking for electronic vehicles only in the spaces designated at the Charging Station(s).
- 29. **Notices**. All notices required under this Agreement shall be in writing and shall be delivered to MGE and City as follows:

To MGE:

Rob Crain
Madison Gas and Electric Company
PO Box 1231
Madison WI 53701-1231
Email: rcrain@mge.com

To City:

Eric Knepp Madison Parks Superintendent 330 E. Lakeside Street Madison, WI 53715 parks@cityofmadison.com

Notices shall be deemed delivered upon receipt if sent by facsimile or email or personally delivered and shall be deemed delivered two (2) business days after delivery into the United States first class mail, postage prepaid, addressed to the other Party.

- 30. **Memorandum of Agreement**. Upon the execution of this Agreement, City and MGE shall execute a recordable form of memorandum of this Agreement, in the form attached hereto as Exhibit C. MGE may record the memorandum with the Dane County Register of Deeds at its sole cost.
- 31. **Terms Run with the Land**. The terms and conditions of this Agreement shall run with the land and be binding on the parties and their heirs, successors, and assigns unless and until terminated as provided herein.
- 32. **No Partnership**. None of the terms and provisions of this Agreement shall be deemed to

create a partnership between MGE and City in their respective businesses or otherwise, nor shall any terms or provisions of this Agreement cause MGE or City to be considered joint venturers or members of any joint enterprise.

- 33. **Non-Discrimination**. In the performance of the services under this Agreement, MGE agrees not to discriminate against any employee or applicant because of race, religion, marital status, age, color, sex, handicap, national origin, or ancestry, income level or source of income, arrest record or conviction record, less than honorable discharge, physical appearance, sexual orientation, gender identity, political beliefs, or student status. MGE further agrees not to discriminate against any subcontractor or person who offers to subcontract on this contract because of race, religion, color, age, disability, sex, sexual orientation, gender identity, or national origin.
- 34. **Entire Agreement**. This Agreement contains the entire agreement between MGE and City with respect to the subject matter of this Agreement and supersedes all previous negotiations, agreements, or understandings whether written or oral. This Agreement is binding on each Party's permitted successors and assigns.
- 35. Counterparts and Electronic Signatures. This Agreement and any document executed in connection herewith may be executed in counterparts, each of which shall be deemed an original, but all of which together shall constitute the same document. Signatures on this Agreement may be exchanged between the Parties by facsimile, electronic scanned copy (.pdf) or similar technology and shall be as valid as original; and this Agreement may be converted into electronic format and signed or given effect with one or more electronic signature(s) if the electronic signature(s) meets all requirements of Wis. Stat. ch. 137 or other applicable Wisconsin or Federal law. Executed copies or counterparts of this Agreement may be delivered by facsimile or email and upon receipt will be deemed original and binding upon the Parties hereto, whether or not a hard copy is also delivered. Copies of this Agreement, fully executed, shall be as valid as an original.
- 36. **Nature of MGE's Business**. City acknowledges that MGE is engaged in the business of generating and distributing electricity and purchasing and distributing natural gas and will warn its employees against the hazards incident in such operations. Neither City nor any of its employees, consultants, agents, subcontractors, or representatives shall touch, move, manipulate, or tamper with any wires, pipes, fixtures, machines, appliances, or equipment of MGE's without express permission from MGE's authorized representative.
- 37. **Non-Waiver**. The failure of MGE to insist upon or enforce in any instance strict performance by City of any of the terms of this Agreement or to exercise any rights herein conferred shall not be construed as a waiver or relinquishment to any extent of its rights to assert or rely upon any such terms or rights on any future occasion.
- 38. **Survival.** The terms and conditions of this Agreement which by their nature require performance by either Party after the termination of the Agreement including, but not limited to, limitations of liability, indemnification provisions, governing law, and any other provisions or partial provisions which by their nature would reasonably extend beyond termination of the Agreement shall be and remain enforceable after such termination or expiration of the

Agreement for any reason whatsoever.

39. **Governing Law and Venue**. Interpretation and enforcement of this Agreement shall be in accordance with the laws of the State of Wisconsin. The Parties agree that the proper and exclusive venue to resolve any dispute arising out of or in any way related to this Agreement shall be the Circuit Court for Dane County, Wisconsin. The Parties agree to submit to the jurisdiction of the courts of the State of Wisconsin with regard to any litigation resulting from such dispute and waive any objection to such jurisdiction.

IN WITNESS WHEREOF, the Parties hereto have executed this Agreement by their duly authorized agents.

	MADISON GAS AND ELECTRIC COMPANY	
	By:	
Date		-
Bate	Name: Rob Crain	
	Director Customer Experience and Energy	
	Title: Services	

CITY OF MADISON

Satya Rhodes-Conway, Mayor	Date
Maribeth Witzel-Behl, City Clerk	Date
Countersigned:	
David P. Schmiedicke, Finance Director	Date
Approved as to form:	
Michael Haas, City Attorney	Date
Execution of this Amendment by the City is author ID No, adopted by the Common Council of the City of Parks Commissioners approval granted on, 20	of Madison on, 202, and the Board
Attachments: Exhibit A - Location of the Charging Station(s)	
Exhibit R - Memorandum of Agreement	

EXHIBIT A LOCATION OF THE CHARGING STATION(S)





EXHIBIT C Memorandum of Agreement

4. Additional terms and conditions of the Agreement are set

	This Memorandum of Agreement ("Memorandum") is	
ent	ered into this day of 20, by	
and	d between Madison Gas and Electric Company ("MGE") and	
	("City").	
Mo	GE and City state as follows:	
1.	MGE and City entered into an Agreement Relating to Charging Stations, of even date herewith ("Agreement").	
2.	The Agreement gives MGE the right to install, own, operate, maintain, repair, remove, upgrade, and reinstall one	THIS SPACE RESERVED FOR RECORDING DATA
	or more charging stations on City's property described in	RETURN TO:
	Exhibit A attached hereto and incorporated herein.	Betsy Winterhack
3.	The Agreement also grants and conveys access rights and	Madison Gas and Electric Company PO Box 1231
	various other rights and protections to MGE.	Madison WI 53701-1231
		P.I.N.

- 5. The entire Agreement runs with the land and is binding on MGE and Owner and their respective heirs, successors, and assigns unless terminated as provided in the Agreement.
- 6. This Memorandum is intended to provide notice of the Agreement. This Memorandum is not intended to and does not amend the Agreement. In the event of a conflict between the terms and conditions of this Memorandum and the terms and conditions of the Agreement, the terms and conditions of the Agreement shall govern and prevail.

This Instrument Drafted By: Elisabeth E. Winterhack Madison Gas and Electric Company

forth in the Agreement itself.

IN WITNESS WHEREOF, the parties hereto have executed this Memorandum of Agreement as of the year and date first set forth above.

Madison Gas and Electric Company

	By: Name:	Deborah L. Branson
	Title:	Manager Electrification
State of Wisconsin County of Dane		
Personally came before me this day of, foregoing instrument and acknowledged the	to me know e same.	20, the above-named own to be the person who executed the
Notary Public, Dane County, Wisconsin		
Name:		(Please print)
My commission expires on		
	CITY (OF MADISON
	By:	
	Name: Title:	
State of		
Personally came before me this day above-named foregoing instrument and acknowledged the	, to me k	
Notary Public, State of		
Name:		(Please print)
My commission expires on		

EXHIBIT A TO MEMORANDUM OF AGREEMENT LEGAL DESCRIPTION AND PARCEL IDENTIFICATION NUMBER

Parcel Identification Number:





City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83872

File ID:83872File Type:ResolutionStatus:Council New

Business

Version: 1 Reference: Controlling Body: BOARD OF PARK

COMMISSIONERS

File Created Date: 06/10/2024

Final Action:

File Name: Authorizing the execution of agreements and

associated easements with Madison Gas and Electric Company relating to the installation, operation, maintenance and repair of electric vehicle charging stations at the Yahara Hills Golf Course.

Title: Authorizing the execution of agreements and associated easements with Madison

Gas and Electric Company relating to the installation, operation, maintenance and

repair of electric vehicle charging stations at the Yahara Hills Golf Course.

Notes:

Sponsors: Yannette Figueroa Cole Effective Date:

Attachments: Yahara Hills Golf Course Charging MGE redlines 06 Enactment Number:

06 24.pdf

Author: Lisa Laschinger Hearing Date:

Entered by: nmiller@cityofmadison.com Published Date:

Approval History

Version Seq # Action Date Approver Action	Due Date	
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History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Parks Division	06/10/2024	Referred for Introduction				
	Action Text: Notes:	This Resolution was Referred for Introduction Finance Committee (6/24/24), Board of Park Commissioners (7/10/24), Common Council (7/16/24)					

Text of Legislative File 83872

Fiscal Note

The Proposed resolution allows Madison Gas and Electric Company to install and operate electric vehicle charging stations at the Yahara Hills Golf Course. The city does not receive

revenue from this arrangement. Additionally, no City appropriation is required.

Title

Authorizing the execution of agreements and associated easements with Madison Gas and Electric Company relating to the installation, operation, maintenance and repair of electric vehicle charging stations at the Yahara Hills Golf Course.

Body

WHEREAS, Madison Gas and Electric Company desires to install, and after such installation own, operate, maintain, repair, remove, upgrade and reinstall electric vehicle charging stations and related facilities ("Charging Stations") in the City of Madison ("City"), including on City-owned property; and,

WHEREAS, the City is committed to sustainable and renewable energy policies and the installation of Charging Stations in the City, and the use thereof by plug-in hybrid and electric vehicles, is in the public's interest as it will help reducing oil dependence and greenhouse gas emissions; and,

WHEREAS, the Charging Stations will be available for use by residents and visitors of the City; and

WHEREAS, the Yahara Hills Golf Course is readily accessible to the public, can accommodate Charging Stations, and placement of Charging Stations at this location will aid the success of the City's goal of sustainable and renewable energy policies.

NOW, THEREFORE, BE IT RESOLVED that the Common Council finds that it is in the City's and the public's interest to support Madison Gas and Electric Company's request by providing Madison Gas and Electric Company with dedicated locations in the Yahara Hills Golf Course parking lot for the installation, operation, maintenance and repair of electric vehicle charging stations and related facilities; and,

BE IT FURTHER RESOLVED that the Mayor and City Clerk are authorized to enter into agreements and associated easements with Madison Gas and Electric Company relating to the installation, operation, maintenance and repair of Charging Stations at the Yahara Hills Golf Course; and,

BE IT FURTHER RESOLVED that the agreements shall be for an initial period of ten (10) years, with one (1) year renewals, and the terms and conditions thereof shall be consistent with the agreement attached hereto and incorporated herein; and,

BE IT FURTHER RESOLVED that the locations of the Charging Stations and any associated easements or distribution facilities shall be as generally depicted on Exhibit A to the charging station agreement, with final approval of actual Charging Stations to be made by the Golf Operations Director.



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83887

File ID: 83887 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: FINANCE

COMMITTEE

File Created Date: 06/11/2024

Final Action:

Enactment Number:

File Name: Authorizing the Mayor and City Clerk to amend the

contract for Purchase of Services (Architect) with Engberg Anderson, Inc. to provide extra professional architectural and engineering consultant design services and construction administrative services for

Title: Authorizing the Mayor and City Clerk to amend the contract for Purchase of Services (Architect) with Engberg Anderson, Inc. to provide extra professional architectural and engineering consultant design services and construction administrative services for the expanded design and construction scope at the existing Warner Park Community Recreation Center located at 1625 Northport

Drive. (District 12)

Notes: Amy Scanlon

Sponsors: Yannette Figueroa Cole, Amani Latimer Burris And Effective Date:

Charles Myadze

Attachments: 9170 Contract Amendment 02 052324.pdf, 9170

Attachment A.pdf

Author: Jim Wolfe, City Engineer Hearing Date:

Entered by: hfleegel@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:		Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Divis	sion 06	6/11/2024	Referred for Introduction				
	Action Text:	This Resolutio	n was Ref	erred for Introduction				
	Notes:	Finance Commit	ttee (7/8/24)	, Board of Public Works (6/20	6/24), Common Council (7/16	6/24)		
1	COMMON COU	NCIL 06	6/18/2024	Referred	BOARD OF PUBLIC WORKS			
	Action Text: Notes:	This Resolutio		erred to the BOARD OF I e Committee	PUBLIC WORKS			
1	BOARD OF PUB WORKS	SLIC 06	6/18/2024	Referred	FINANCE COMMITTEE		07/08/2024	
	Action Text:	This Resolutio	n was Ref	erred to the FINANCE Co	OMMITTEE			
1	BOARD OF PUB WORKS	ELIC 06	5/26/2024	Return to Lead with the Recommendation for Approval	Finance Department			Pass

Action Text: A motion was made by Guequierre, seconded by Clausius, to Return to Lead with the

Recommendation for Approval to the Finance Department. The motion passed by voice vote/other.

FINANCE COMMITTEE 07/08/2024 RECOMMEND TO

COUNCIL TO ADOPT - REPORT OF OFFICER

Action Text: A motion was made by Figueroa Cole, seconded by Rummel, to RECOMMEND TO COUNCIL TO

ADOPT - REPORT OF OFFICER. The motion passed by voice vote/other.

Text of Legislative File 83887

Fiscal Note

The proposed resolution authorizes the Mayor and City Clerk to execute amendment #2 to the contract for Purchase of Services (Architect) with Engberg Anderson, Inc. to provide additional professional architectural and engineering consultant design services and construction administrative services for the expansion at the existing Warner Park Community Recreation Center located at 1625 Northport Drive at an additional cost of \$3,080. The original Purchase of Services Contract with Enberg Anderson, Inc. was approved via RES-22-00527 (Legistar #72262) on August 2, 2022 for an estimated cost of \$353,155. The first amendment to the contract was approved via RES-24-00135 (Legistar #81984) on March 5, 2024 for an additional \$53,800. If approved, the total contract, including the two amendments will total \$410,035. Funding is available in the 2024 Parks Division Adopted Capital Budget (Munis #17196). No additional appropriation is required.

Title

Authorizing the Mayor and City Clerk to amend the contract for Purchase of Services (Architect) with Engberg Anderson, Inc. to provide extra professional architectural and engineering consultant design services and construction administrative services for the expanded design and construction scope at the existing Warner Park Community Recreation Center located at 1625 Northport Drive. (District 12)

Body

WHEREAS the 2024 Capital Budget includes funding for the Warner Park Community Recreation Center Expansion; and,

WHEREAS, the Engineering Division requires consulting services in order to complete the Pre-Design, Schematic Design, Design Development, Construction Documentation, Bidding, Construction Administration, and Warranty phases of the project; and,

WHEREAS, Engberg Anderson, Inc. was selected through a "Request for Proposal" process, and is the final selection candidate in accordance with MGO Section 4.26 of Madison General Ordinances and in accordance with Administrative Procedure Memorandum No. 3-3, "Retaining Architect, Professional Engineers, Designers and Land Surveyors for Consultant Service;" and,

WHEREAS, the original contract for Purchase of Services (Architect) with Engberg Anderson, Inc. was executed August 22, 2022 (Contract #9170); and,

WHEREAS, the previously planned expansion project was being designed and revised to align with the limited project budget; and

WHEREAS, the existing fire alarm system is in good working order and has not been cited for any inadequacies; and

WHEREAS, the change in occupant load and the age of the existing system warrants an upgrade of the existing fire alarm system; and

Pass

WHEREAS, the new fire alarm system requires additional design and coordination by the consultant;

WHEREAS, the existing contract term is unchanged and expires September 30, 2025; and WHEREAS, the total compensation of this contract including amendment #2 shall not exceed \$410,035; and

NOW THEREFORE BE IT RESOLVED, that the Mayor and City Clerk are authorized to sign contract amendment #2 with Engberg Anderson, Inc. to provide extra professional architectural and engineering design services and construction administration services for the expanded design and construction scope at the existing Warner Park Community Recreation Center located at 1625 Northport Drive.



Finance Department

David P. Schmiedicke, Finance Director

www.cityofmadison.com/finance/purchasing

City-County Building, Room 406 210 Martin Luther King, Jr. Boulevard Madison, Wisconsin 53703 Phone: (608) 266-4521 Fax: (608) 266-5948 finance@cityofmadison.com **Purchasing Services**

AMENDMENT # 02

to the Contract For Purchase of Services (#9170) between the City of Madison and Enberg Anderson, Inc.

The City of Madison and Contractor listed above agree to amend the Contract for Purchase of Services executed by the City on 8-22-22 and as amended on 4-15-24 ("Original Contract"), as follows:

- 1. Revise Section 3 of the original contract Scope of Services to include changing the fire alarm system to a voice system per Madison Fire Department direction as described in Attachment A for the Warner Park Community Recreation Center located at 1625 Northport Drive. The original scope for the project did not include these professional services. See Attachment A.
- 2. Revise Section 23 of the original contract COMPENSATION. It is expressly understood and agreed that in no event will the total compensation under this contract exceed \$410,035 (addition of \$3,080).
- 3. All other provisions of the Original Contract shall remain unchanged and in full force and effect.
- 4. <u>Electronic Signature and Delivery</u>. Signatures on this Amendment may be exchanged between the parties by facsimile, electronic scanned copy (.pdf) or similar technology and shall be as valid as original; and this Amendment may be converted into electronic format and signed or given effect with one or more electronic signature(s) if the electronic signature(s) meets all requirements of Wis. Stat. ch. 137 or other applicable Wisconsin or Federal law. Executed copies of this Amendment may be delivered by facsimile or email and upon receipt will be deemed original and binding upon the parties hereto, whether or not a hard copy is also delivered. Copies of this Amendment, fully executed, shall be as valid as an original.
- 5. <u>Authority to Sign.</u> The person signing on behalf of the Contractor represents and warrants that they have been duly authorized to bind the Contractor and sign this amendment on the Contractor's behalf.

IN WITNESS WHEREOF, the parties hereto have set their hands at Madison, Wisconsin.

CONTRACTOR:

			(Type or Print Name of Contracting Entity)
		Dv.	
		Ву:	(Signature)
			(Print Name and Title of Person Signing)
		Date:	
			F MADISON, WISCONSIN cipal corporation:
		Ву:	Satya Rhodes-Conway, Mayor
		5.	
		Date:	
Approv	ed:		
	David P. Schmiedicke, Finance Director	By:	Maribeth Witzel-Behl, City Clerk
Date:		Date:	
		Approv	ved as to Form:
	Eric T. Veum, Risk Manager	<u> </u>	Michael Haas, City Attorney
Date:		Date:	





A/E/CONSULTANT PROPOSAL

May 21, 2024

To:

Amy Scanlon, Project Manager City of Madison, Department of Public Works Engineering Division City County Building, Room 115 210 Martin Luther King, Jr. Blvd. Madison, WI 53703-3342 ascanlon@cityofmadison.com

Project Name: Warner Park Community Recreation Center Expansion

Contract Number: 9170 Engberg Anderson Project Number: 223471

Engberg Anderson, Inc. is submitting this proposal for additional design services related to scope requests and changes to the consultant team.

SCOPE OF BASIC SERVICES

A. PROJECT SCOPE:

During the WPCRC Expansion CD page-turn meeting on April 16th, the Madison Fire Department noted that the fire alarm system for the existing building needs to be upgraded to a voice system due to the increased occupant load of the facility. The fire alarm system for the existing building is in good working order and has not been cited for any inadequacies, nor was it flagged during multiple meetings with the Madison Fire Department during the predesign phase as requiring an upgrade. The core team determined following the CD page-turn meeting that upgrading the fire alarm system for the existing building to a voice system based on the Madison Fire Department's comment would be added to the project scope.

B. PROJECT BUDGET:

The changes noted in the project scope were captured in the project budget completed at the conclusion of the Construction Documents phase.

C. SCHEDULE OF SERVICES:

The design team will provide the following additional services in accordance with the increased project scope.

Change Fire Alarm for Entire Building to Voice System

\$3,080

Site visit to review existing conditions. (4 hrs.)

Update as-built condition on fire alarm floor plans. (4 hrs.)

Revise fire alarm plans to show conversion to voice system in existing building. (8 hrs.)

Revise specifications and details to call for a voice system. (6 hrs.)

305 W Washington Av | Madison, WI 53703 | (608) 250-0100 | www.engberganderson.com

MILWAUKEE MADISON TUCSON CHICAGO 524

D. A/E/CONSULTANT TEAM:

Unchanged from Contract and Amendment No. 1.

E. SERVICES:

Unchanged from Contract.

F. DELIVERABLES:

Unchanged from Contract.

G. FEE:

We propose the following Fee for the Additional Services:

Total Lump Sum Fee of \$3,080.

(Includes overhead costs such as travel, lodging, meals, reproduction of drawings for design meetings, handling and postage, telephone, faxes, etc.)

H. REIMBURSABLE EXPENSES:

Unchanged from Contract.

Sincerely,

William Robison, AIA

President WR/dk-b

Y:\Madison Projects\2022 3429\223471 Warner Park Expansion\1-Project Administration\1-Contracts & Fees\1-Client\Change Order #2\9170 AE Design CO #2.docx



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84167

File ID: 84167 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: FINANCE

COMMITTEE

Fail

File Created Date: 06/25/2024

File Name: 2025 Budget Rules and Procedures Final Action:

Title: Establishing rules and procedures for Common Council deliberations on the City

of Madison's 2025 Capital and Operating Budgets.

Notes:

Sponsors: Yannette Figueroa Cole Effective Date:

Attachments: 2025 Budget Calendar (File 84167).pdf Enactment Number:

Author: Hearing Date:

Entered by: mmcclain@cityofmadison.com Published Date:

History of Legislative File

Ver-	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return	Result:
sion:						Date:	

1 Finance Department 06/25/2024 Referred for

Introduction

Action Text: This Resolution was Referred for Introduction

Notes: Finance Committee (7/8/24), Common Council (7/16/24)

1 COMMON COUNCIL 07/02/2024 Add Referral(s)

Action Text: A motion was made by Latimer Burris, seconded by Harrington-McKinney, to Add a Referral to the

Common Council Executive Committee. The motion failed by the following vote:

Ayes: 5 Nasra Wehelie; William Tishler; Amani Latimer Burris; Charles

Myadzeand Barbara Harrington-McKinney

Noes: 14 Juliana R. Bennett; Derek Field; Michael E. Verveer; Regina M. Vidaver;

Marsha A. Rummel; MGR Govindarajan; Nikki Conklin; Yannette Figueroa Cole; Isadore Knox Jr.; Dina Nina Martinez-Rutherford; Jael Currie;

Sabrina V. Madison; John P. Guequierreand John W. Duncan

Excused: 1 Tag Evers

Non Voting: 1 Satya V. Rhodes-Conway

1 COMMON COUNCIL 07/02/2024 Refer FINANCE 07/08/2024 Pass

COMMITTEE

Action Text: A motion was made by Figueroa Cole, seconded by Duncan, to Refer to the FINANCE COMMITTEE.

The motion passed by voice vote/other.

1 FINANCE COMMITTEE 07/08/2024 RECOMMEND TO Pass

COUNCIL TO ADOPT - REPORT OF OFFICER Action Text: A motion was made by Figueroa Cole, seconded by Currie, to RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER. The motion passed by voice vote/other.

Text of Legislative File 84167

Fiscal Note

No appropriation required.

Title

Establishing rules and procedures for Common Council deliberations on the City of Madison's 2025 Capital and Operating Budgets.

Body

WHEREAS, the attached 2025 Budget Calendar identifies the proposed schedule for consideration of the 2025 Budget, with Budget Adoption by the Common Council scheduled for the week of November 12th, 2024;

NOW, THEREFORE, BE IT RESOLVED, that the Common Council hereby establishes the following rules and procedures for the deliberation of the 2025 Executive Operating and Capital Budgets as amended by the Finance Committee:

- 1) All proposed amendments to be reviewed and distributed in advance of the Common Council meeting must be submitted to the Finance Department by email no later than 12:00pm on Wednesday, November 6th, 2024. The Finance Department staff will prepare fiscal notes and distribute a list of the proposed amendments by Friday, November 8th, 2024, at 1:00pm
- 2) In the case of amendments from the floor of the Council, members may consider an amendment to a proposed amendment as well as other amendments that are written and distributed to Council members.
- 3) Proposed amendments must have two sponsors to be considered by the Council.

BE IT FURTHER RESOLVED, that the above rules shall be in effect for the deliberation of the 2025 Capital and Operating Budgets on Tuesday, November 12th, Wednesday, November 13th, and Thursday, November 14th, if meetings are held.

BE IT FURTHER RESOLVED, that the November 12th and November 13th meetings of the Council shall recess or adjourn not later than 12:00 am (Midnight) unless the Council by a 2/3 vote agrees to extend the time of recess to a different time.

BE IT FINALLY RESOLVED, that any rules or procedures that are inconsistent with the above rules are suspended for these meetings pursuant to the provisions of Section 2.34 of the Madison General Ordinances.

Key Dates for 2025 Budget Development

	Capital	Operating
Kick-Off Meeting	Tuesday, March 19 11:00am	Monday, June 17 1:00pm
Agency Requests Due	Friday, April 19	Friday, July 19
Agency Briefings with Mayor's Office	May 8 – 17	August 12 – 14
Executive Budget introduced to Common Council (CC)	Tuesday, September 10	Tuesday, October 8
Finance Committee (FC) Briefings	September 16 – 17	October 14 – 15
FC Amendment Meeting	Monday, September 30	Monday, October 28
CC Amendment Week	October 29 – November 6	
CC Budget Adoption Meetings	Novembe	er 12 – 14

Detailed Timeline for FC and CC Hearings

		Capital	Operating
1.	Mayor Introduces Executive Budget	September 10	October 8
2.	Finance Committee (FC) Hearings	Agencies present budget; alders have opportuSeptember 16 & 17	nity to ask questions • October 14 & 15
3.	FC Amendment Week	 Alders submit requests to budget analysts; and September 18 - 25 Alders submit by Weds., Sept. 25 at 12pm Analysts publish by Fri., Sept. 27 at 1pm 	• October 16 - 23
4.	FC Vote on Amendments	September 30	October 28
5.	City Council Amendments	 City Council has the opportunity to propose and October 29 - November 6 Alders submit by Weds., Nov. 6 at 12pm Analysts publish by Fri., Nov. 8 at 1pm 	nendments to capital and operating budgets
6.	City Council Adoption	Up to three (3) CC meetings to vote on amend • November 12, 13, 14	lments and adopt budget



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84174

File ID: 84174 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: FINANCE

COMMITTEE

File Created Date: 06/26/2024

Final Action:

File Name: Amending the 2024 Adopted Capital Budget of

Engineering-Other to appropriate \$200,000 of Landfill

Reserves to fund a new project for Landfill Generators at the Greentree and Mineral Point

Landfills.

Title: Amending the 2024 Adopted Capital Budget of Engineering-Other to appropriate

\$200,000 of Landfill Reserves to fund a new project for Landfill Generators at the

Greentree and Mineral Point Landfills. (District 19 & District 20)

Notes: Jim Wolfe

Sponsors: John P. Guequierre And Barbara Effective Date:

Harrington-McKinney

Attachments: Enactment Number:

Author: Jim Wolfe, City Engineer Hearing Date:

Entered by: hfleegel@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Engineering Divis	sion 06/26/2024	Referred for Introduction				
	Action Text: Notes:	This Resolution was Ref Finance Committee (7/8/24)	erred for Introduction), Common Council (7/16/24)				
1	COMMON COUN	ICIL 07/02/2024	Refer	FINANCE COMMITTEE		07/08/2024	Pass
	Action Text:	A motion was made by F The motion passed by vo	rigueroa Cole, seconded boice vote/other.	y Duncan, to Refer to the	FINANCE CC	OMMITTEE.	
1	FINANCE COMM	IITTEE 07/08/2024	RECOMMEND TO COUNCIL TO ADOPT (15 VOTES REQUIRED) - REPORT OF OFFICER				Pass
	Action Text:	•	Figueroa Cole, seconded b EQUIRED) - REPORT OF	•			

Text of Legislative File 84174

Fiscal Note

The proposed resolution seeks to amend the 2024 Adopted Engineering-Other Capital Budget to create a new capital project, 'Landfill Generators at the Greentree and Mineral Point Landfills" at an estimated cost of \$200,000 in 2024 funded by Landfill reserves. The project will fund backup generators for the Landfill during power outages. No additional appropriation is required.

Title

Amending the 2024 Adopted Capital Budget of Engineering-Other to appropriate \$200,000 of Landfill Reserves to fund a new project for Landfill Generators at the Greentree and Mineral Point Landfills. (District 19 & District 20)

Body

WHEREAS, the City of Madison Engineering Division is responsible for operating, monitoring, and maintaining the City's five closed landfill sites with the goal of protecting both public health and the environment.

WHEREAS, the underground landfill gas extraction and leachate collection systems operate at these sites 24 hours per day (require power to operate) and are connected to a SCADA (Supervisory Control and Data Acquisition) system which provides the ability to monitor system equipment operational status remotely.

WHEREAS, the SCADA system notifies Engineering of a system failure, such as a power outage, so that staff can respond to maintain system operations.

WHEREAS, currently, in the event of a power outage, a portable generator is transported to the impacted site to provide temporary power until the power outage is resolved.

WHEREAS, the Demetral, Olin, and Sycamore, sites are able to be without power for up to 24 hours before back up power is required. Greentree and Mineral Point, due to higher methane production, can only be without power for 6 hours.

WHEREAS, a storm on May 21, 2024 resulted in more than 42,000 MG&E customers losing power.

WHEREAS, the City's network went down due to power outages in the Park Street area. WHEREAS, the loss of City network services resulted in the SCADA system being unable to notify staff that the Greentree Landfill had lost power and it was not until the next day that staff arrived to work and found the SCADA system down.

WHEREAS, staff determined that the Greentree landfill was without power and deployed a generator to provide temporary power until power was restored on May 24, 2024.

WHEREAS, The Engineering Division has 2 portable generators used to power landfills and sanitary sewer lift stations during power outages. While both generators are in good operating condition and have low hours of usage, they are more than 25 years old and are scheduled for replacement.

WHEREAS, New police, fire, and public works facilities typically include a permanent on-site generator to provide temporary power, if needed. The Sewer Utility has an annual capital program to install stand-by generators at its lift stations, which started in 2017.

WHEREAS, it is likely that the increased frequency of extreme weather events will lead to more power outages in the years to come.

WHEREAS, given the critical nature of systems serving the City's landfills, and the small window of time during which power must be restored to the Greentree and Mineral Point landfills, it is desirable to have dedicated, stand-by generators at both sites, instead of replacing the existing portable generators.

WHEREAS, the 2 existing portable generators can still be used to provide temporary power at the other 3 landfill sites and sewer lift stations that do not yet have dedicated stand-by generators as well as other smaller City facilities.

NOW, THEREFORE, BE IT RESOLVED:

That the 2024 Adopted Capital Budget of Engineering-Other is hereby amended to appropriate \$200,000 of Landfill Reserves to fund a new project for Landfill Generators at the Greentree and Mineral Point Landfills.



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84175

File ID: 84175 File Type: Resolution Status: Report of Officer

Version: 1 Controlling Body: FINANCE Reference:

COMMITTEE

File Created Date: 06/26/2024

Final Action:

File Name: Authorizing the Transportation Department, to submit

a grant application to the Wisconsin Department of Transportation for up to \$500,000 dollars, accept grant funding if awarded, and authorizing the Mayor and Clerk to sign the resulting grant agreement.

Title: Authorizing the Transportation Department, to submit a grant application to the Wisconsin Department of Transportation for up to \$500,000 dollars, accept grant funding if awarded, and authorizing the Mayor and Clerk to sign the resulting

grant agreement.

Notes:

Sponsors: MGR Govindarajan, Marsha A. Rummel, Barbara

Harrington-McKinney, Michael E. Verveer And Derek

Field

Attachments: 070824FinanceCmte WrittenComment.pdf

Author: Stefanie Cox, Parking Division Manager

Entered by: ALarson2@cityofmadison.com

Effective Date:

Enactment Number:

Hearing Date:

Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Transportation Department Action Text: Notes:	06/26/2024 This Resolution was Ref Finance Committee (7/8/24	Referred for Introduction Ferred for Introduction), Common Council (7/16/24)				
1	COMMON COUN Action Text:		Refer Figueroa Cole, seconded b	FINANCE COMMITTEE by Duncan, to Refer to the	_	7/08/2024 MMITTEE.	Pass
		The motion passed by v	•	•			
1	FINANCE COMM	IITTEE 07/08/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass
	Action Text: A motion was made by Figueroa Cole, seconded by Rummel, to RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER. The motion passed by voice vote/other.						

Text of Legislative File 84175

Fiscal Note

The proposed resolution authorizes the application and acceptance of a WI DOT Carbon Reduction Program grant to support the development of a Curb Management Action Plan. The requested grant amount is up to \$500,000 with a local match up to \$100,000. Funding for the local match will be included in the Parking Division's 2025 operating budget request and supported by Parking revenues.

Title

Authorizing the Transportation Department, to submit a grant application to the Wisconsin Department of Transportation for up to \$500,000 dollars, accept grant funding if awarded, and authorizing the Mayor and Clerk to sign the resulting grant agreement.

Body

WHEREAS, the City of Madison is dedicated to reducing carbon emissions in our community; and

WHEREAS, the Transportation Department and Parking Division is committed to carrying out the City's Complete Green Streets goals; and

WHEREAS, Transportation Department and Parking Division are interested in obtaining a carbon reduction grant from the Wisconsin Department of Transportation to develop a curb management program that reduces transportation emissions by optimizing curb use to decrease idling and traffic congestion, coordinate curbside needs, such as deliveries, passenger pick-ups, micro-mobility, green infrastructure, and parking through an equity and sustainability lens; and enhance multimodal transportation options by prioritizing safe infrastructure for biking and public transit, thus reducing reliance on single-occupancy vehicles; and

WHEREAS, the City of Madison continues to be one of the fastest-growing communities in Wisconsin, and the related development has resulted in increased demand for curbside space in neighborhoods around the city; and

WHEREAS, the grant will provide funding for costs associated with planning, development, data collection, and support of the new program; and

WHEREAS, we will be requesting funding for up to five hundred thousand dollars; and

WHEREAS, the local match requirement for the grant is up to \$100,000 and will be included in Parking's 2025 operating budget request and supported by the Parking Enterprise Fund; and

WHEREAS, the grant application form includes legal terms, obligations, and/or commitments from the City, and/or requires a signature from a person who is authorized to bind the City to such things; and

WHEREAS, the City of Madison Department of Transportation staff can attest to the validity and veracity of the statements and representations contained in the grant application; and

WHEREAS, if awarded, the granting agency may require the City to sign a contract, grant agreement, and/or other documents binding the City to certain obligations,

NOW, THEREFORE, BE IT RESOLVED, the City of Madison Transportation Department is authorized, on behalf of the City, to apply for the Wisconsin DOT Carbon Reduction Program grant and sign all application materials, after any legal terms within the application are reviewed by the City Attorney; and

BE IT FURTHER RESOLVED, that, if awarded, the City is authorized to enter into an agreement with Wisconsin Department of Transportation for the grant, and the Mayor and City Clerk are authorized to sign the agreement and related paperwork, in a format approved by the City Attorney; and

BE IT FINALLY RESOLVED that Department staff are authorized to sign related paperwork or forms that are not part of the grant agreement or that are provided after the Mayor and Clerk sign the initial grant agreement, if required, after any legal terms included in those forms are reviewed by the City Attorney.

From: Guequierre, John
To: Finance Committee

Subject: FW: Finance Committee /2 items (legistrar 84175 and 84178)

Date: Monday, July 8, 2024 3:18:50 PM

From: taylor warsek < taylor.warsek@gmail.com >

Sent: Monday, July 8, 2024 1:46 PM

To: Guequierre, John < district19@cityofmadison.com >

Subject: Finance Committee /2 items

Caution: This email was sent from an external source. Avoid unknown links and attachments.

In light of our current budget shortfall, we are questioning the matching funds required plus any ongoing operating expenses estimates of the legistar item 84178 which is asking for grant application of \$920,000 to the DOT for an "all ages, all ability, bike and pedestrian FACILITY" (facility sounds like a building) We don't care how much the grant is for, we simply can not afford to match funds in any amount and pay for the operation of some kind of new building or bikeway (which is not maintenance free)

We are also wondering about legistar 84175 which asks to apply for a DOT grant of \$500,000 for ?? . We are hoping that the Alders are now going to ask how much matching funds will the city be responsible for and what is the estimated ongoing operating expenses that will be associated with this project? Interesting that there is NO statement as to what the grant money is for???

In our minds, it seems with the shortfall we have, that NO NEW PROGRAMS OR BUILDING have any business being funded for at least the next budget cycle. Rick Taylor and Linda Warsek



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84178

File ID: 84178 File Type: Resolution Status: Report of Officer

Version: 1 Controlling Body: FINANCE Reference:

COMMITTEE

File Created Date: 06/26/2024

Final Action:

File Name: Authorizing the City of Madison Traffic Engineering

Division to submit, and if awarded, to accept a grant of up to \$920,000 from the U.S. Department of Transportation Federal Highway Administration to develop a plan and design for a preferred alternative

Title: Authorizing the City of Madison Traffic Engineering Division to submit, and if

awarded, to accept a grant of up to \$920,000 from the U.S. Department of

Transportation Federal Highway Administration to develop a plan and design for a preferred alternative for a north side, all-ages, all-ability, bicycle and pedestrian facility and authorizing the Mayor and Clerk to sign the resulting grant agreement

(District 12 & District 18).

Notes:

Sponsors: Satya V. Rhodes-Conway, Dina Nina **Effective Date:**

Martinez-Rutherford, Charles Myadze And Amani

Latimer Burris

Attachments: 070824FinanceCmte_WrittenComment.pdf **Enactment Number:**

Author: Renee Callaway, Assistant Director of Traffic Engineering **Hearing Date:**

Published Date:

Entered by: ALarson2@cityofmadison.com

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1		This Resolution was Ref	Introduction				
1	COMMON COUN	CIL 07/02/2024	Refer	FINANCE COMMITTEE	07	7/08/2024	Pass
	Action Text: A motion was made by Figueroa Cole, seconded by Duncan, to Refer to the FINANCE COM The motion passed by voice vote/other.						
1	FINANCE COMMI	TTEE 07/08/2024	RECOMMEND TO COUNCIL TO ADOPT - REPORT OF OFFICER				Pass

A motion was made by Figueroa Cole, seconded by Rummel, to RECOMMEND TO COUNCIL TO Action Text:

ADOPT - REPORT OF OFFICER. The motion passed by voice vote/other.

Text of Legislative File 84178

Fiscal Note

The proposed resolution authorizes the submission, and if awarded, acceptance of a grant to plan and design a preferred alternative for the north side. The grant award is up to \$920,000. The local match of \$184,000 will be requested in Traffic Engineering's 2025 capital budget.

Title

Authorizing the City of Madison Traffic Engineering Division to submit, and if awarded, to accept a grant of up to \$920,000 from the U.S. Department of Transportation Federal Highway Administration to develop a plan and design for a preferred alternative for a north side, all-ages, all-ability, bicycle and pedestrian facility and authorizing the Mayor and Clerk to sign the resulting grant agreement (District 12 & District 18).

Body

WHEREAS, the Active Transportation Infrastructure Investment Program (ATIIP) grant was created to help communities plan, design, and construct safe and connected active transportation networks such as sidewalks, bikeways, and trails that connect destinations such as schools, workplaces, residences, businesses, recreation areas, and medical facilities within a community or metropolitan region; and

WHEREAS, ATIIP projects are intended to improve the safety, efficiency, and reliability of active transportation networks and communities; improve connectivity between active transportation modes and public transportation; enhance the resiliency of on- and off-road active transportation infrastructure and help protect the environment; and improve quality of life in disadvantaged communities through the delivery of connected active transportation networks and expanded mobility opportunities; and

WHEREAS, US DOT has set aside some funds to support grants specifically for planning and design of active transportation networks; and

WHEREAS, the development of a feasible alternatives for an all ages and ability bicycle and pedestrian connection from the north side of Madison that would connect into the rest of the City bicycle and pedestrian network and to the planned North South Bus Rapid Transit route would promote safety and connectivity throughout the region; and

WHEREAS, the grant application would also include funding to hold community engagement to gather input on the alternatives as well as to undertake initial design work on the preferred alternative; and

WHEREAS, applicants may apply for both a Planning and Design grant and a Construction grant; and

WHEREAS, the City of Madison Transportation Commission approved a resolution on June 12, 2024 to indicate support for Traffic Engineering;s efforts to apply for the Active Transportation Infrastructure Investment Program (ATIIP) grant; and

NOW, THEREFORE, BE IT RESOLVED that the City of Madison Traffic Engineering Division is authorized to apply for an Active Transportation Infrastructure Improvement Program (ATIIP) grant to support bicycle and pedestrian route planning and design and to accept the grant if awarded.

BE IT FURTHER RESOLVED that, if awarded the grant, the Mayor and City Clerk are authorized to sign a grant agreement with the US Department of Transportation Federal Highway Administration for the Active Transportation Infrastructure Improvement Program grant, in a form approved by the City Attorney,; and,

BE IT FURTHER RESOLVED that the Mayor and Common Council commit to funding the 20% local match requirements, up to \$184,000 if the City is awarded funding through this grant program; and,

BE IT FINALLY RESOLVED that the Mayor and Clerk are hereby authorized to execute the above grant agreement and other necessary documents related to this transaction in a form to be approved by the City Attorney.

From: Guequierre, John
To: Finance Committee

Subject: FW: Finance Committee /2 items (legistrar 84175 and 84178)

Date: Monday, July 8, 2024 3:18:50 PM

From: taylor warsek < taylor.warsek@gmail.com>

Sent: Monday, July 8, 2024 1:46 PM

To: Guequierre, John < district19@cityofmadison.com >

Subject: Finance Committee /2 items

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City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84277

File ID: 84277 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: FINANCE

COMMITTEE

File Created Date: 07/02/2024

Final Action:

File Name: Amending the 2024 Adopted Capital Budget for

Traffic Engineering - Traffic Safety Infrastructure to accept a \$177,584 grant from the Wisconsin Department of Transportation, and authorizing the Mayor and City Clerk to execute the updated

agreement, Project

Title: Amending the 2024 Adopted Capital Budget for Traffic Engineering - Traffic Safety Infrastructure to accept a \$177,584 grant from the Wisconsin Department of Transportation, and authorizing the Mayor and City Clerk to execute the updated agreement, Project I.D. 5992-22-12 Safe Routes to School Plan.

Notes:

Sponsors: Yannette Figueroa Cole And Satya V. Effective Date:

Rhodes-Conway

Attachments: Enactment Number:

Author: Renee Callaway, Assistant Director of Traffic Engineering

Hearing Date:

Entered by: ALarson2@cityofmadison.com

Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Traffic Engineerin Division Action Text: Notes:	This Resolution was Ref	Referred for Introduction erred for Introduction , Common Council (7/16/24)				
1	COMMON COU	NCIL 07/02/2024	Refer	FINANCE COMMITTEE		07/08/2024	Pass
	Action Text:	A motion was made by F The motion passed by vo	igueroa Cole, seconded boice vote/other.	y Duncan, to Refer to th	e FINANCE C	OMMITTEE.	
1	FINANCE COMM	MITTEE 07/08/2024	RECOMMEND TO COUNCIL TO ADOPT (15 VOTES REQUIRED) - REPORT OF OFFICER				Pass
	Action Text:	•	igueroa Cole, seconded b EQUIRED) - REPORT OF	•			

Text of Legislative File 84277

Fiscal Note

The proposed resolution amends Traffic Engineering's 2024 Adopted Capital Budget to appropriate \$177,584 in federal funds within the Traffic Safety Infrastructure capital program. These funds are additional funding through the Wisconsin Department of Transportation FFY 2024 federal redistribution. The grant requires a 20% local match, which will be funded from existing appropriation from the Traffic Safety Infrastructure capital program. No additional local appropriation is required.

Title

Amending the 2024 Adopted Capital Budget for Traffic Engineering - Traffic Safety Infrastructure to accept a \$177,584 grant from the Wisconsin Department of Transportation, and authorizing the Mayor and City Clerk to execute the updated agreement, Project I.D. 5992-22-12 Safe Routes to School Plan.

Body

Federal redistribution is an annual process that occurs in August that affords a state an opportunity to request additional federal funds above their annual initial obligation limit. FFY 2024 is expected to have \$206 million available for WisDOT to request, a historic amount of redistribution funds. To obligate these funds, states must have projects eligible for the federal programs and WisDOT requested municipal partners to collaborate and develop eligible projects that would meet federal project readiness requirements.

The City of Madison endeavors to provide its citizens with a robust, sustainable, safe, multi-modal transportation system. The City has made major investments in bicycling, walking, and safe routes to school and this priority is reflected in current budgets.

WHEREAS, the City of Madison Safe Routes to School Plan project meets FFY 2024 Redistribution Transportation Alternatives Program (TAP) federal and state requirements, and

WHEREAS, The City needs to enter into a revised project agreement with the Wisconsin Department of Transportation that updates the estimated costs for the total project based on additional work items to include 18 additional school plans, review and recommendations related to crossing guard locations, enhanced equitable engagement with families, data collection, and multi-lingual school safety campaign, and

WHEREAS, approval of the original agreement for Project I.D. 5992-22-12 occurred on September 22, 2022, (Resolution File ID 73498, RES-22-00664-00945), and

WHEREAS, the awarded projects will be reimbursed at 80% of the participating costs,

NOW, THEREFORE BE IT HEREBY RESOLVED, that the 2024 Adopted Capital Budget is amended to appropriate an additional \$177,584 of federal sources within Traffic Engineering's Traffic Safety Infrastructure capital program and the Mayor and City Clerk are authorized to execute an updated agreement with the State of Wisconsin Department of Transportation for WISDOT Project ID 5992-12-12.



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83530

File ID: 83530 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: PLAN

COMMISSION

File Created Date: 05/17/2024

File Name: CSM - 521 E Washington Ave Final Action:

Title: Approving a Certified Survey Map of property owned by LZ, LLC located at 521 E

Washington Avenue (District 6).

Notes:

Sponsors: Planning Division Effective Date:

Attachments: Locator Maps.pdf, Application.pdf, Proposed Enactment Number:

CSM.pdf, Staff Comments.pdf, Link to Demo Permit File 83525, Link to Cond Use File 83526, Public

Comments 07-08-24.pdf

Author: Hearing Date:

Entered by: tparks@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	PLAN COMMISS	SION 07/08/2024	RECOMMEND TO COUNCIL TO ADOPT UNDER SUSPENSION OF MGO 2.055 - REPORT OF OFFICER				Pass
Action Text: A motion was made by Solheim, seconded by Guequierre, to RECOMMEND TO COUNCIL TO ADOPT UNDER SUSPENSION OF MGO 2.055 - REPORT OF OFFICER. The motion passed by voice vote/other. Notes: On a motion by Solheim, seconded by Ald. Guequirre, the Plan Commission found that the standards were met and				Γ			
	1101001	recommended approval of t Commission materials. The	he Certified Survey Map	subject to the comments a			

Text of Legislative File 83530

Title

Approving a Certified Survey Map of property owned by LZ, LLC located at 521 E Washington Avenue (District 6).

Body

WHEREAS a Certified Survey Map of property owned by LZ, LLC located at 521 E Washington

Avenue, City of Madison, Dane County, Wisconsin has been duly filed for approval by the Plan Commission, its Secretary or their designee, as provided for in Section 16.23(4)(f) of Madison General Ordinances; and

WHEREAS Chapter 236, Wisconsin Statutes requires that the Madison Common Council approve any dedications proposed or required as part of the proposed division of the lands contained on said Certified Survey Map;

NOW THEREFORE BE IT RESOLVED that said Certified Survey Map, bond and subdivision contract, subsequent affidavits of correction, parkland acquisition documents, easement or right-of-way release or procurement documents or any other related document or documents as deemed necessary by the Secretary of the Plan Commission in accordance with the approval of said Certified Survey Map are hereby approved by the Madison Common Council.

BE IT FURTHER RESOLVED that the Mayor and City Clerk of the City of Madison are hereby authorized to sign the above mentioned documents related to this Certified Survey Map.

BE IT FURTHER RESOLVED that all dedications included in this Certified Survey Map or required as a condition of approval of this Certified Survey Map be and are hereby accepted by the City of Madison.

BE IT FURTHER RESOLVED that the Planning Division is authorized to reflect the recorded Certified Survey Map in the Comprehensive Plan and any applicable neighborhood plans.



City of Madison

Demolition, Conditional Use & Certified Survey Map Referral

Location

521 E Washington Avenue

Applicant

Duane Johnson, Knothe & Bruce Architects, LLC Zachary Reynolds, Wyser Engineering John Leja, LZ Ventures

Request

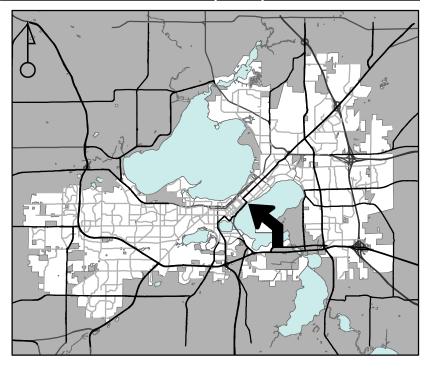
- Demolish two-story commercial building
- Construct an eight-story mixed-use building containing 3,812 sq ft. of office space and 70 apartments
- Create one lot by CSM

Public Hearing Dates Plan Commission

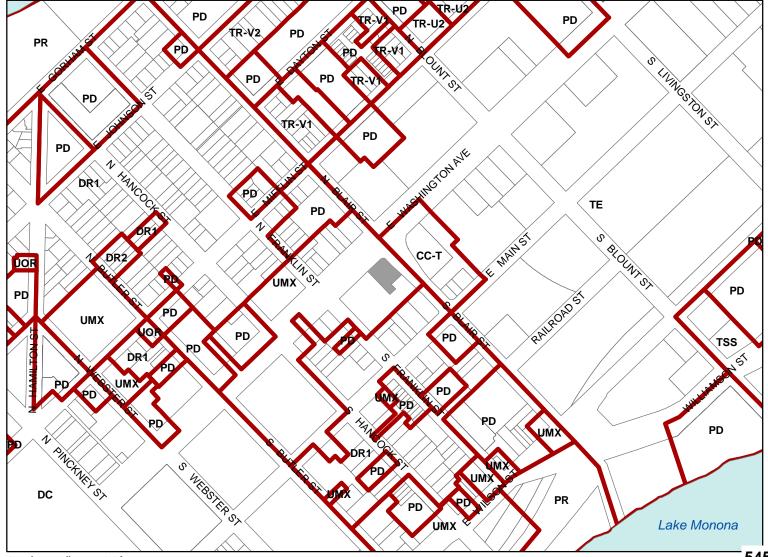
July 8, 2024

Common Council

July 16, 2024 (for CSM only)



For Questions Contact: Chris Wells at: 261-9135 or cwells@cityofmadison.com or City Planning at 266-4635



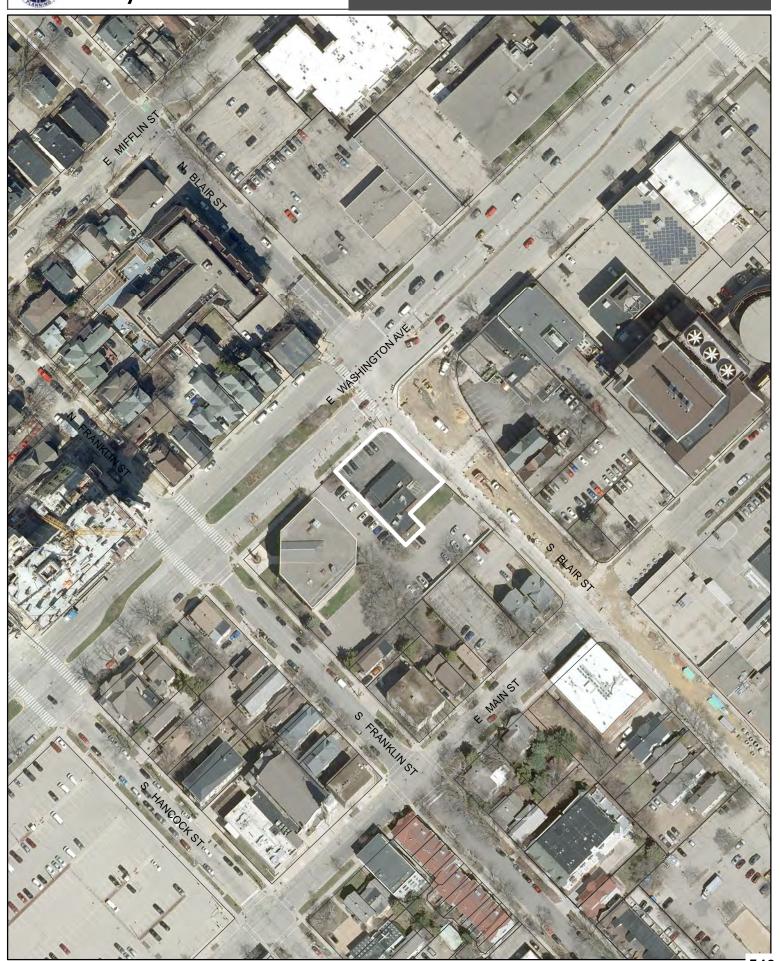
Scale : 1" = 400'

City of Madison, Planning Division : JC : Date : 5-20-545



City of Madison

521 E Washington Avenue



Date of Aerial Photography : Spring 2 546

SUBDIVISION APPLICATION

** Please read both pages of the application completely and fill in all required fields **

For a digital copy of this form with fillable fields, please visit:

https://www.cityofmadison.com/sites/default/files/city-of-madison/development-services-center/documents/SubdivisionApplication.pdf

If you need an interpreter, translator, materials in alternate formats or other accommodations to access these forms, please call the Planning Division at (608) 266-4635.

Si necesita interprete, traductor, materiales en diferentes formatos, u otro tipo de ayuda para acceder a estos formularios, por favor llame al (608) 266-4635.

Yog tias koj xav tau ib tug neeg txhais lus, tus neeg txhais ntawv, los sis xav tau cov ntaub ntawv ua lwm hom ntawv los sis lwm cov kev pab kom paub txog cov lus qhia no, thov hu rau Koog Npaj (Planning Division) (608) 266-4635. City of Madison 5/13/24 8:10 a.m. Planning Division Madison Municipal Building, Suite 017 215 Martin Luther King, Jr. Blvd. P.O. Box 2985 Madison, WI 53701-2985 (608) 266-4635



NOTICE REGARDING LOBBYING ORDINANCE: If you are seeking approval of a development that has over 40,000 square feet of non-residential space, or a residential development of over 10 dwelling units, or if you are seeking assistance from the City with a value of \$10,000 (including grants, loans, TIF or similar assistance), then you likely are subject to Madison's lobbying ordinance (M.G.O. Sec. 2.40). You are required to register and report your lobbying. Please consult the City Clerk's Office for more information. Failure to comply with the lobbying ordinance may result in fines.

1. A	pplication Type							
	Preliminary Sul	odivision Plat		Final Subdivision	Plat 🗸	1	Land Division/Certified Survey Map (CSM)	
If	a Plat, Proposed S	ubdivision Name	:					
2. Re	eview Fees							
•	For Certified Surv Make checks payable	vey Maps, an app to "City Treasure	licatio r" and	on fee of \$250 plus d mail it to the follo	\$200 per lot a	an Cit	er lot or outlot contained on the plat. Ind outlot contained on the CSM. Ity of Madison Building Inspection; P.O. Box 29	84;
	roject, and contact		de a c	over page with the	check which ii	ncı	cludes the project address, brief description of	uie
3. P	roperty Owner and	Agent Information	n					
N S T	Name of Property Co treet address: elephone:	Numer: LZ, LLC 8301 Machine D 608-576-3489	r, Ste		Representative, if any: John Leja City/State/Zip: Madison, WI 53717 Email: jleja@me.com			
	irm Preparing Surv		neerin	g	Contact: Zac			
	treet address: elephone:	300 E. Front St. 608-437-1980					Mount Horeb, WI 53572 ynolds@wyserengineering.com	_
Ch	eck only ONE – ALL (Correspondence o	n this	application should b	e sent to:	P	Property Owner, OR	
4. P	roperty Information	n for Properties Lo	ocated	d within Madison C	ity Limits			
P	arcel Addresses: 5	21 E. Washington	Ave.	(removal of underl	ying lot line)			
Т	- ax Parcel Number(:	s): <u>251/0709-133</u> -	1610-	.9				
	oning District(s) of				Scho	loc	l District: MMSD	
•	Please include a d	etailed description	of th	e number and use o	f all proposed l	lots	ts and outlots in your letter of intent.	
4a. F	Property Informatio	n for For Properti	es Loc	cated <i>Outside</i> the N	ladison City Li	mi	its in the City's Extraterritorial Jurisdiction:	

Date of Approval by Town: ___

5. Subdivision Contents and Description. Complete table as it pertains to your request; do not complete gray areas.

For an exterritorial request to be scheduled, approval letters from both the Town and Dane County must be submitted.

Land Use	Lots	Outlots	Acres
Residential	1	0	0.24
Retail/Office			
Industrial			

Parcel Addresses (note town if located outside City): _____

Date of Approval by Dane County: _____

Land Use	Lots	Outlots	Acres
Other (state use):			
Outlots Dedicated to the Public (Parks, Stormwater, etc.)			
Outlots Maintained by a Private Group or Association			
PROJECT TOTALS	1	0	0.24

6. Required Submittal Materials

<u>Digital (PDF) copies</u> of all items listed below (if applicable) are required. Applicants are to submit each of these documents as <u>individual PDF files</u> in an e-mail sent to <u>PCapplications@cityofmadison.com</u>. The transmittal shall include the name of the project and applicant. Note that <u>an individual email cannot exceed 20MB</u> and it is the <u>responsibility of the applicant</u> to present files in a manner that can be accepted. Electronic submittals via file hosting services (such as Dropbox) are <u>not</u> allowed. Applicants who are unable to provide the materials electronically should contact the Planning Division at <u>Planning@cityofmadison.com</u> or (608) 266-4635 for assistance.

☐ A Completed Subdivision Application Form (i.e. both sides of this form)

☐ Map Copies (prepared by a Registered Land Surveyor):

- For <u>Preliminary Plats</u>, the drawings must be drawn to scale and are required to provide all information as set forth in <u>M.G.O. Sec. 16.23 (7)(a)</u>.
- For Final Plats, the drawings must be drawn to scale and drawn to the specifications of §236.20, Wis. Stats..
- For <u>Certified Survey Maps (CSMs)</u>, the drawings shall include all of the information set forth in <u>M.G.O. Secs. 16.23 (7)(a)</u> and (d), including existing site conditions, the nature of the proposed division and any other necessary data. Utility data (field located or from utility maps) may be provided on a separate map submitted with application.

For Plat & CSMs, in addition to the PDF copy, a digital CADD file shall also be submitted in a format compatible with AutoCAD. The digital CADD file(s) shall be referenced to the Dane County Coordinate System and shall contain, at minimum, the list of items stated below, each on a separate layer/level name. The line work shall be void of gaps and overlaps and match the plat, preliminary plat or CSM as submitted: a) Right-of-Way lines (public and private); b) Lot lines; c) Lot numbers; d) Lot/Plat dimensions; e) Street names; f) Easement lines (i.e. all in title and shown on the plat or CSM including wetland & floodplain boundaries.)

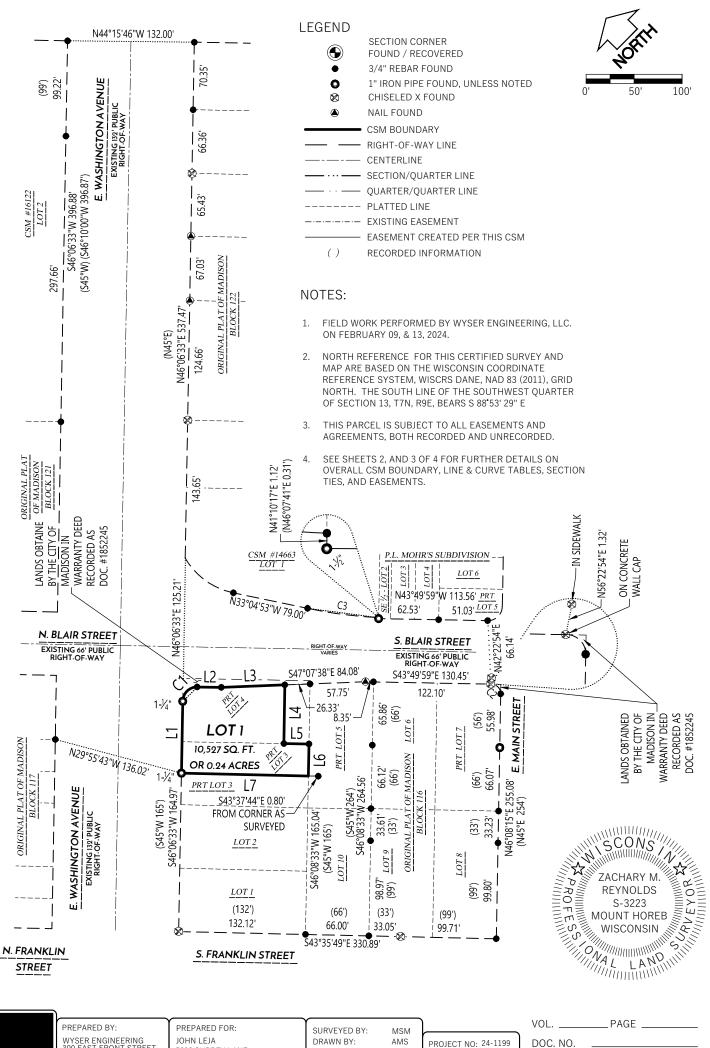
- □ Letter of Intent: One copy of a letter describing the proposed subdivision or land division in detail including, but not limited to:
 - The number and type/use of the lots and outlots proposed with this subdivision or land division, including any outlots to be dedicated to the public;
 - · Existing conditions and uses of the property;
 - Phasing schedule for the project, and;
 - The names of persons involved (property owner(s), subdivider, surveyor, civil engineer, etc.).
 - * The letter of intent for a subdivision or land division may be the same as the letter of intent submitted with a concurrent Land Use Application for the same property.
 - ** A letter of intent is not required for Subdivision Applications for lot combinations or split duplexes.
- Report of Title and Supporting Documents: One copy of a City of Madison standard 60-year Report of Title obtained from a title insurance company as required in M.G.O. Sec. 16.23 and as satisfactory to the Office of Real Estate Services. Note:
 - <u>The Report of Title must have been completed within three (3) months of the submittal date of this application. Title insurance or a title commitment policy are NOT acceptable (i.e. a Preliminary Title Report or a Record Information Certificate).</u>
 - The electronic PDF submittal shall include images of the vesting deeds and all documents listed in the Report of Title.
 - <u>Do not email these files to the City's Office of Real Estate Services</u>. Send them instead to the email address noted at the top of this page.
- □ For Surveys Outside the Madison City Limits: One copy of the approval letters from the town where the property is located and Dane County shall be submitted with your request. The Plan Commission may not consider an application within its extraterritorial jurisdiction without prior approval from the town and Dane County.

7. /	q۶	oli	cant	Dec	ara	tic	ons	

The signer attests that the application has been completed accurately and all required materials have been submitted:				
Applicant's Printed Name: TOAW LESA Signature: 925.				
Date: 5-13-2024 Interest In Property On This Date:				

CERTIFIED SURVEY MAP NO.

THE NE $\frac{1}{2}$ OF LOT 3, AND THE NW 106 FEET OF LOT FOUR, BLOCK 116, ORIGINAL PLAT OF MADISON, AS RECORDED IN VOLUME A OF PLATS, ON PAGE 3, AS DOCUMENT NUMBER 102, DANE COUNTY REGISTER OF DEEDS, EXCEPT THAT PART CONVEYED TO THE CITY OF MADISON IN WARRANTY DEED RECORDED AS DOCUMENT NUMBER 1852305, DANE COUNTY REGISTER OF DEEDS, LOCATED IN THE NE $\frac{1}{4}$ -SW $\frac{1}{4}$ AND THE SE $\frac{1}{4}$ -SW $\frac{1}{4}$ ALL IN FRACTIONAL SECTION 13, TOWNSHIP 07 NORTH, RANGE 09 EAST, IN THE CITY OF MADISON, DANE COUNTY, WISCONSIN



File: W:\2024\241199_John |

5:01pm

May 10, 2024 -

Plotted:

Zach

User:

CSM 1 0F 4

Layout:

Madison/dwg/241199_CSM.dwg

Wash,

- Blair & E.

Leja

WYSER ENGINEERING 300 EAST FRONT STREET MOUNT HOREB, WI 53572

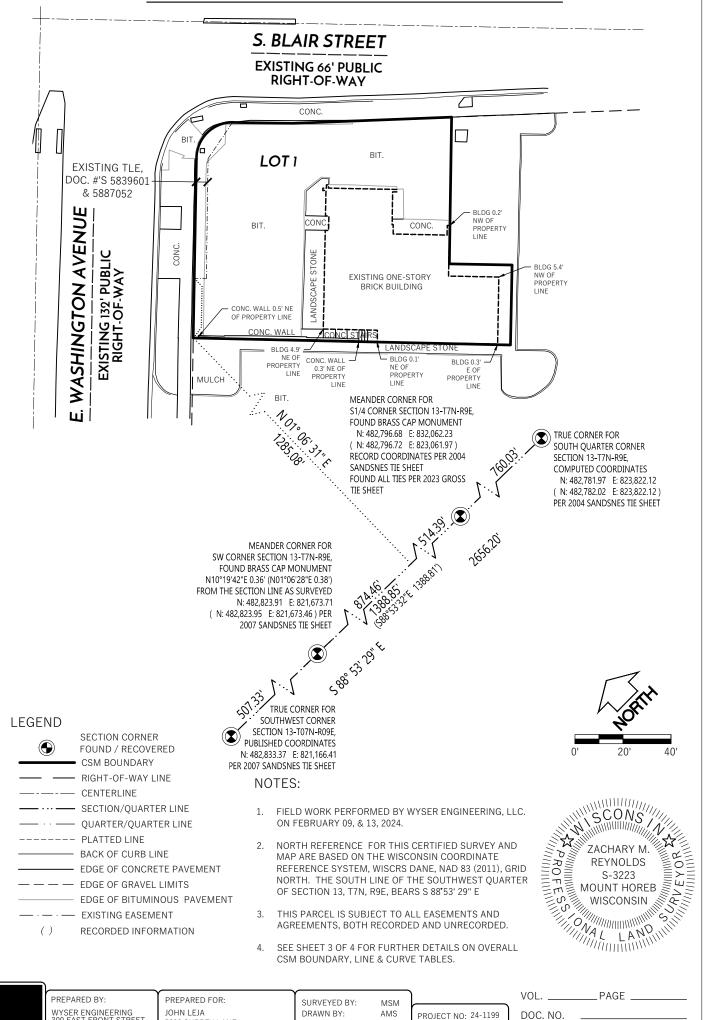
JOHN LEJA 5603 SURREY LANE WAUNAKEE, WI 53597

CHECKED BY: 7MR APPROVED BY

PROJECT NO: 24-1199 SHEET NO: 3 of 4

DOC. NO. C.S.M. NO THE NE $\frac{1}{2}$ OF LOT 3, AND THE NW 106 FEET OF LOT FOUR, BLOCK 116, ORIGINAL PLAT OF MADISON, AS RECORDED IN VOLUME A OF PLATS, ON PAGE 3, AS DOCUMENT NUMBER 102, DANE COUNTY REGISTER OF DEEDS, EXCEPT THAT PART CONVEYED TO THE CITY OF MADISON IN WARRANTY DEED RECORDED AS DOCUMENT NUMBER 1852305, DANE COUNTY REGISTER OF DEEDS, LOCATED IN THE NE $\frac{1}{4}$ -SW $\frac{1}{4}$ AND THE SE 1/4-SW 1/4 ALL IN FRACTIONAL SECTION 13, TOWNSHIP 07 NORTH, RANGE 09 EAST, IN THE CITY OF MADISON, DANE COUNTY, WISCONSIN

EXISTING IMPROVEMENTS, EASEMENTS AND SECTION TIE DETAIL



WYSER ENGINEERING 300 EAST FRONT STREET MOUNT HOREB, WI 53572

JOHN LEJA 5603 SURREY LANE WAUNAKEE, WI 53597

DRAWN BY:

CHECKED BY:

APPROVED BY:

7MR

PROJECT NO: 24-1199

3 of 4

SHEET NO:

DOC. NO.

C.S.M. NO.

2024 - 4:57pm

CERTIFIED SURVEY MAP NO.

THE NE ½ OF LOT 3, AND THE NW 106 FEET OF LOT FOUR, BLOCK 116, ORIGINAL PLAT OF MADISON, AS RECORDED IN VOLUME A OF PLATS, ON PAGE 3, AS DOCUMENT NUMBER 102, DANE COUNTY REGISTER OF DEEDS, EXCEPT THAT PART CONVEYED TO THE CITY OF MADISON IN WARRANTY DEED RECORDED AS DOCUMENT NUMBER 1852305, DANE COUNTY REGISTER OF DEEDS, LOCATED IN THE NE ½-SW ¼ AND THE SE ½-SW ¼ ALL IN FRACTIONAL SECTION 13, TOWNSHIP 07 NORTH, RANGE 09 EAST, IN THE CITY OF MADISON, DANE COUNTY, WISCONSIN

CURVE TABLE					
CURVE NUMBER	ARC LENGTH	RADIUS	CENTRAL ANGLE	CHORD DIRECTION	CHORD LENGTH
C1	23.54'	15.00'	089°56'04"	S89°55'59"E (S89°16'17"E)	21.20' (21.22')
C2	16.40'	10.00'	093°57'29"	S00°32'18"E (S00°44'24"W)	14.62' (14.14')
C3	74.74' (74.64')	491.00'	008°43'17" (008°42'36")	N37°26'32"W (N37°28'27"W)	74.67' (74.57')

LINE TABLE					
LINE NUMBER	DIRECTION	LENGTH			
L1	N46°06'33"E (N45°E)	74.78' (75')			
L2	S44°01'57"E (S44°15'36"E)	25.16' (25.26')			
L3	S47°07'38"E (S47°41'37"E)	66.20' (65.85')			
L4	S45°56'16"W (S45°W)	61.11' (60.94')			
L5	S43°51'27"E (S45°E)	26.07' (26')			
L6	S46°08'33"W (S45°W)	33.33' (33')			
L7	N43°37'44"W (N45°W)	132.21' (132')			

LEGAL DESCRIPTION

THE NE $\frac{1}{2}$ OF LOT 3, AND THE NW 106 FEET OF LOT FOUR, BLOCK 116, ORIGINAL PLAT OF MADISON, AS RECORDED IN VOLUME A OF PLATS, ON PAGE 3, AS DOCUMENT NUMBER 102, DANE COUNTY REGISTER OF DEEDS, EXCEPT THAT PART CONVEYED TO THE CITY OF MADISON IN WARRANTY DEED RECORDED AS DOCUMENT NUMBER 1852305, DANE COUNTY REGISTER OF DEEDS, LOCATED IN THE NE $\frac{1}{4}$ -SW $\frac{1}{4}$ AND THE SE $\frac{1}{4}$ -SW $\frac{1}{4}$ ALL IN FRACTIONAL SECTION 13, TOWNSHIP 07 NORTH, RANGE 09 EAST, IN THE CITY OF MADISON, DANE COUNTY, WISCONSIN, DESCRIBED MORE PARTICULARLY AS FOLLOWS:

COMMENCING AT THE TRUE CORNER LOCATION OF THE SOUTHWEST CORNER OF AFORESAID SECTION 13; THENCE, ALONG THE SOUTH LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 13, SOUTH 88 DEGREES 52 MINUTES 35 SECONDS EAST, 507.14 FEET TO THE MEANDER CORNER FOR AFORESAID SOUTHWEST CORNER OF SAID SECTION 13; THENCE, CONTINUING ALONG SAID SOUTH LINE, SOUTH 88 DEGREES 52 MINUTES 35 SECONDS EAST, 313.29 FEET; THENCE, NORTH 01 DEGREES 07 MINUTES 25 SECONDS EAST, 911.47 FEET TO THE WEST CORNER OF AFORESAID NORTHEAST HALF OF LOT 3, ALSO BEING A POINT ON THE SOUTHEASTERLY RIGHT-OF-WAY OF EAST WASHINGTON AVE, AND THE POINT OF BEGINNING;

THENCE, CONTINUING ALONG SAID RIGHT-OF-WAY, NORTH 46 DEGREES 06 MINUTES 33 SECONDS EAST, 74.78 FEET TO THE START OF A TANGENT CURVE; THENCE, 23.54 FEET ALONG THE ARC OF A TANGENT CURVE TO THE RIGHT, HAVING A RADIUS OF 15.00 FEET AND LONG CHORD THAT BEARS SOUTH 89 DEGREES 55 MINUTES 59 SECONDS EAST, 21.20 FEET TO A POINT ON THE SOUTHWESTERLY RIGHT-OF-WAY OF SOUTH BLAIR STREET; THENCE, ALONG SAID RIGHT OF WAY OF SOUTH BLAIR STREET, SOUTH 44 DEGREES 01 MINUTES 57 SECONDS EAST, 25.16 FEET; THENCE, CONTINUING ALONG SAID RIGHT-OF-WAY, SOUTH 47 DEGREES 07 MINUTES 38 SECONDS EAST 66.20 FEET; THENCE, SOUTH 45 DEGREES 56 MINUTES 16 SECONDS WEST, 61.11 FEET TO A POINT ON THE NORTHEASTERLY LINE OF AFORESAID LOT 3; THENCE, ALONG SAID NORTHEASTERLY LINE, SOUTH 43 DEGREES 51 MINUTES 27 SECONDS EAST, 26.07 FEET TO THE EAST CORNER OF THE AFORESAID LOT 3; THENCE, ALONG THE SOUTH CORNER OF AFORESAID NORTH HALF OF LOT 3; THENCE, ALONG THE SOUTHWESTERLY LINE OF SAID NORTHEAST HALF OF LOT 3, NORTH 43 DEGREES 37 MINUTES 44 SECONDS 132.21 FEET, BACK TO THE POINT OF BEGINNING.

SAID PARCEL CONTAINS 10,527 SQUARE FEET OR 0.24 ACRES.

SURVEYOR'S CERTIFICATE

I, ZACHARY M. REYNOLDS, WISCONSIN PROFESSIONAL LAND SURVEYOR S-3223, DO HEREBY CERTIFY THAT BY DIRECTION OF JOHN LEJA, I HAVE SURVEYED, DIVIDED, AND MAPPED THE LANDS DESCRIBED HEREON AND THAT THE MAP IS A CORRECT REPRESENTATION IN ACCORDANCE WITH THE INFORMATION PROVIDED. I FURTHER CERTIFY THAT THIS CERTIFIED SURVEY MAP IS IN FULL COMPLIANCE WITH CHAPTER 236.34 OF THE WISCONSIN STATUTES AND THE SUBDIVISION REGULATIONS OF THE CITY OF MADISON, AND DANE COUNTY, WISCONSIN.

ZACHARY M. REYNOLDS, S-3223 WISCONSIN PROFESSIONAL LAND SURVEYOR DATE





PREPARED BY:
WYSER ENGINEERING
300 EAST FRONT STREET
MOUNT HOREB, WI 53572

PREPARED FOR: JOHN LEJA 5603 SURREY LANE WAUNAKEE, WI 53597

SURVEYED BY: MSM
DRAWN BY: AMS
CHECKED BY: ZMR
APPROVED BY: ZMR

PROJECT NO: 24-1199 SHEET NO: 3 of 4 VOL. _____PAGE _____

DOC. NO. _____

C.S.M. NO. ____

		CERT	IFIED SURVEY	MAP NO.			
	ON PAGE 3, AS DOCU WARRANTY DEED RE	THE NE ½ OF LOT 3, AND THE NW 106 FEET OF LOT FOUR, BLOCK 116, ORIGINAL PLAT OF MADISON, AS RECORDED IN VOLUME A OF PLATS, ON PAGE 3, AS DOCUMENT NUMBER 102, DANE COUNTY REGISTER OF DEEDS, EXCEPT THAT PART CONVEYED TO THE CITY OF MADISON IN WARRANTY DEED RECORDED AS DOCUMENT NUMBER 1852305, DANE COUNTY REGISTER OF DEEDS, LOCATED IN THE NE ½-SW ¼ AND THE SE ½-SW ¼ ALL IN FRACTIONAL SECTION 13, TOWNSHIP 07 NORTH, RANGE 09 EAST, IN THE CITY OF MADISON, DANE COUNTY, WISCONSIN					
	OWNER'S CERTIF	FICATE					
	SURVEYED, DIVIDED		AS SHOWN. WE ALSO CE	ERTIFY THAT THIS CE	LANDS DESCRIBED HEREON TO BE RTIFIED SURVEY MAP IS REQUIRED BY S. PR APPROVAL.		
	BY: MANAGING ME ORB AND SCEF	MBER PTER HOLDINGS LLC					
	STATE OF WISCONSI COUNTY OF DANE	N)SS					
	PERSONALLY CAME	BEFORE ME THIS DAY	Y OF	, 2024.			
		MANAGING MEMBER FOR (E THE PERSONS WHO EXE(CKNOWLEDGED THE		
	NOTARY PUBLIC, S	TATE OF WISCONSIN		Y COMMISSION EXPIR	RES/IS PERMANENT		
24 – 4:48pm	RESOLVED THAT THI BY ENACTMENT NUM OF DEDICATED AND RIG	IBER	LOCATED IN THE CITY CFILE ID NUMBE I SAID ENACTMENT FUR ERTIFIED SURVEY MAP I	R THER PROVIDED FOR	OUNTY, WISCONSIN, WAS HEREBY APPROVED ADOPTED ON THE DAY THE ACCEPTANCE OF THOSE LANDS SON FOR PUBLIC USE.		
JF 4 User: Zach Plotted: May 10, 2024 - 4:48pm	CITY OF MADIS	ZEL-BEHL, CITY CLERK, SON, DANE COUNTY, WISCO	JNSIN		SCONS SCONS A CHARY M. REYNOLDS S-3223 MOUNT HOREB WISCONSIN WISCONSIN A L L A N MINIMAL L MINIMAL L A N MINIMAL L MINIMAL L A N MINIMAL L MINIMAL L		
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Blair & E. Wash, Madison\dwg\241199_CSM.dwg Layout:	CITY OF MADISON PL	AN COMMISSION.			· mmu.		
g\241199_C					OFFICE OF THE REGISTER OF DEEDS		
Madison∖dw	MATTHEW WACHTER SECRETARY OF THE I		DATE:		COUNTY, WISCONSIN		
E. Wash,					RECEIVED FOR RECORD		
- Blair &					DOCUMENT#		
John Leja					IN VOL. OF CERTIFIED SURVEY		
2024\241199_John Leja	WYSER Engineering				MAPS ON PAGE(S)		
File: W:\202	PREPARED BY: WYSER ENGINEERING 300 EAST FRONT STREET MOUNT HOREB, WI 53572 www.wyserengineering.com	PREPARED FOR: JOHN LEJA 5603 SURREY LANE WAUNAKEE, WI 53597	SURVEYED BY: MSM DRAWN BY: AMS CHECKED BY: ZMR APPROVED BY: ZMR	PROJECT NO: 24-1199 SHEET NO: 4 of 4	KRISTI CHLEBOWSKI, REGISTER OF DEEDS		
L			i				

Project Addresses: 521 E Washington Avenue

Application Type: Demolition Permit, Conditional Uses, and Certified Survey Map

Referral

Legistar File ID # 83525, 83526, and 83530

Prepared By: Chris Wells, Planning Division

Report includes comments from other City agencies, as noted

Reviewed By: Kevin Firchow, Planning Division

Summary

Applicant & John Leja; LZ Ventures; 8301 Machine Drive, Suite 102; Madison, WI 53717

Property Owner:

Contact: Duane Johnson; Knothe & Bruce Architects; 8401 Greenway Blvd., Suite 900; Middleton,

WI 53717

Surveyor: Zachary Reynolds; Wyser Engineering; 300 E Front Street; Mount Horeb, WI 53572

Requested Actions:

ID <u>83525</u> – Consideration of a demolition permit to demolish a two-story commercial building at 521 E
 Washington Avenue;

- ID <u>83526</u> Consideration of a conditional use in the Urban Mixed-Use (UMX) District for a new building
 greater than 20,000 square-feet or more than four stories to allow construction of an eight-story, mixeduse building containing approximately 3,800 square feet of office space and 70 apartments; and
- ID <u>83530</u> Approval of a Certified Survey Map (CSM) to create one lot for the proposed residential development.

Proposal Summary: The applicant is seeking approvals to construct an eight-story, mixed-use building containing approximately 3,800 square-feet of office space and 70 apartments located to the south of the intersection of E Washington Avenue and S Blair Street following demolition of a two-story commercial building. The underlying lots comprising the subject site will be combined into one lot by CSM. The letter of intent indicates that construction will commence in January of 2025, with completion anticipated in November of 2025.

Applicable Regulations & Standards: This proposal is subject to the standards for Demolitions [MGO Section 28.185(7)]. It is also subject to the standards for Conditional Uses [MGO §28.183(6)] as Section 28.076(4)(c) notes, "All new buildings and additions greater than twenty thousand (20,000) square feet or that have more than four (4) stories shall obtain conditional use approval. In addition, the Urban Design Commission shall review such projects for conformity to the design standards in Sec. 28.071(3), if applicable, and the Downtown Urban Design Guidelines and shall report its findings to the Plan Commission." The Urban Design Commission is also an approving body as the subject site is located in Urban Design District No. 4. Lastly, MGO Section 16.23(5)(g) provides the process and standards of approval for certified survey maps.

Review Required By: Urban Design Commission, Plan Commission and Common Council (CSM Only).



Summary Recommendation: The Planning Division recommends the following to the Plan Commission:

- That the Plan Commission that the standards for demolition permits are met to **approve** the demolition of a two-story commercial building located at 521 E Washington Avenue;
- That the Plan Commission find the standards for conditional uses are met to approve an eight-story
 mixed-use building containing approximately 3,800 square feet of office space and 70 apartments, subject
 to input at the public hearing and the conditions from reviewing agencies beginning on page 9; and
- That the Plan Commission forward the Certified Survey Map to combine 521 E Washington Avenue into
 one lot to the Common Council with a recommendation of approval subject to the conditions from
 reviewing agencies beginning on page 17.

Background Information

Parcel Location: The 10,527-square-foot (0.24-acre) parcel located to the south of the intersection of E Washington Avenue and S Blair Street; Alder District 6 (Rummel); Urban Design District No. 4; Madison Metropolitan School District.

Existing Conditions and Land Use: The existing two-story, 5,000-square-foot commercial building was, according to City Assessor's Office Records, originally constructed 1958 (as a gas station), converted into an office building in 1981, added onto in 1989, and remodeled in 2021. The building sits at the south end of the site, nine surface automobile parking stalls are located along the northwest (E Washington Avenue) property line, and the lone assessable automobile stall is located along the southeast property line. The site takes access off S Blair Street and is zoned UMX (Urban Mixed-Use District).

Surrounding Land Uses and Zoning:

<u>Northeast</u>: Across S Blair Street is a parcel with a two-story commercial building with ground floor restaurant and single residential unit above and a three-unit and two-unit apartment buildings, all zoned CC-T (Commercial Corridor – Transition District);

<u>Northwest</u>: Across E Washington Avenue is a three-story mixed-use building with ground floor commercial and seven units above; 3- and 4-unit apartment buildings; and a two-story office building, all in the UMX (Urban Mixed-Use) District; and

<u>Southeast & Southwest</u>: The two-story Wisconsin Manufacturers & Commerce office building, zoned UMX (Urban Mixed-Use District), beyond which are 2- and 5-unit apartment buildings and single-family residences in the DR1 (Downtown Residential 1 District).

Adopted Land Use Plans: The 2023 <u>Comprehensive Plan</u> recommends Downtown Core (DC) development for the subject site and the adjacent parcel to the southwest. The Plan notes that DC "represents the nucleus of downtown and accommodates a wide variety of employment, service, retail, government, residential, and other uses in large scale buildings that comprise the most intensely developed part of the city." For details on heights, mix of uses, ground floor uses, pedestrian friendly design, and other considerations which the Plan notes "must be addressed for development within this category", the Plan defers to the <u>Downtown Plan</u>.

The 2012 <u>Downtown Plan</u> notes that the appropriate height for new buildings is influenced by numerous factors such as topography, important view corridors and viewsheds, the presence of historic buildings, the use and scale recommendations for an area, and the existing scale of buildings in the vicinity. For the subject site, it recommends a maximum of eight stories.

It also does identify the subject site within the "Downtown Core Mixed-Use" neighborhood/district for which it states as an objective, "should generally possess the highest intensity of development. A mix of office, employment, retail, government, residential, cultural entertainment and other uses should be pursued to retain the area's vibrancy, including beyond normal business hours." Lastly, the Plan's Streetscape Design Map on Page 39 identifies the E Washington Avenue / Blair Street intersection as a 'Major Gateway Intersection.' As the accompanying text talks about the higher the level (classification) of street, the higher the level of design and amenities, therefore being a major gateway intersection, the streetscape design at the subject site's main intersection is particularly important.

The <u>First Settlement Neighborhood Master Plan</u> (1995) notes about the subject site, that there is "potential for greater utilization and higher density mixed-use development."

Zoning Summary: The subject site is zoned Urban Mixed-Use (UMX) District

Requirements	Required	Proposed
Lot Area (sq. ft.)	3,000 sq. ft.	10,527 sq. ft.
Lot Width	30'	99'
Front Yard Setback	0' or 5'	6.8'
Max. Front Yard Setback	10'	6.8'
Side Yard Setback: Street side yard setback	0' or 5'	11.9'
Side Yard Setback	0'	Adequate
Rear Yard Setback	10'	22.0'
Usable Open Space	10 sq. ft. per bedroom (700 sq. ft.)	740 sq. ft.
Maximum Lot Coverage	90%	78.7%
Minimum Building Height	2 stories	8 stories/<116'
Maximum Building Height: See Downtown Height Map	8 stories/116'	8 stories/<116'
Stepback: See Downtown Stepback Map	None	None

Site Design	Required	Proposed
Number Parking Stalls	No minimum required: Central Area	8
Electric Vehicle Stalls	1 EV Ready	None (See Comment
		#44)
Accessible Stalls	Yes	1
Loading	Not required	None
Number Bike Parking Stalls	Multi-family dwelling: 1 per unit up to	13 surface
	2-bedrooms, ½ space per add'l	65 garage
	bedroom (70)	(78 total)
	1 guest space per 10 units (7)	(See Comments #45 & #46)
	Office: 1 per 2,000 sq. ft. floor area (2)	
	(79 total)	
Landscaping and Screening	Yes	Yes (See Comment #47)
Lighting	Yes	Yes
Building Form and Design	Yes	Flex building (See Comment #48)

Other Critical Zoning Items	
Yes:	Urban Design (UDD #4), Utility Easements; Barrier Free (ILHR 69), Wellhead Protection (WP-17)
No:	Historic District; Floodplain, Wetlands, Adjacent to Park, TOD Overlay

Tables Prepared by Jenny Kirchgatter, Assistant Zoning Administrator

Environmental Corridor Status: The subject site is not located in a mapped environmental corridor.

Public Utilities and Services: The site is currently served by a full range of urban services, including Metro Transit, which operates daily all-day transit service along East Washington Avenue adjacent this property - with trips at least every 30 minutes (every 15 minutes or less during the day on weekdays and Saturdays). Metro Transit operates additional daily all-day transit service along East Wilson Street and Williamson Street near this property - with trips at least every 30 minutes (every 15 minutes or less during the day on weekdays). Metro Transit would initially estimate the following counts of potentially eligible trips towards US Green Building Council/LEED Quality Access to Transit points: 220 Weekday & 131 Weekend (average). Please contact Metro Transit if additional analysis would be of interest.

Project Description

The applicant is seeking approvals to construct an eight-story, mixed-use building containing approximately 3,800 square-feet of office space and 70 apartments following demolition of a two-story commercial building. The underlying lots comprising the subject site will be combined into one lot by CSM.

The building proposed for demolition is a two-story, 5,000-square-foot commercial building which, according to City Assessor's Office Records, was originally constructed 1958 (as a gas station), converted into an office building in 1981, added onto in 1989, and remodeled in 2021. Photos of the interior and exterior of the building are included in the materials submitted for the demolition permit. Planning Division staff is not aware of structural issues with the building.

After demolishing the subject building, the applicant proposes to construct an eight-story, mixed-use building containing approximately 3,800 square-feet of office space and 70 apartments. The building's main entrance, located at the primary (east) corner, provides access a central lobby, common space, and vertical circulation, which together cut diagonally through the ground floor plan. Flanking these central common spaces are various office spaces which will become the administrative headquarters for the community non-profit organization Porchlight. The basement level contains more space for the organization with various rooms – for donations, various services, and conference/break room, building maintenance, etc. A bike room with space for 65 bikes occupies the balance of the basement level. The upper seven floors are entirely residential with each floor containing ten studio apartments. In communications with Staff, the applicant has noted that each unit contains a kitchenette. Staff note that the Fire Department is requiring the cooking appliances to be induction cooktops to help reduce the number of calls for cooking and false alarms.

Regarding automobile parking, eight surface stalls are located on site and are all accessed from S Blair Street – five are located beneath the building overhang with the other three located at the south corner of the site. As for bicycle parking, 65 long-term stalls are located on the basement level, and a total of 14 short-term stalls are located along the two street-facing facades.

Along the southwest side of the building is a roughly 450-square-foot patio with seating for use by the residents and Porchlight staff. Users will access it via the door located along the southeast façade, near the accessible parking stall.

Regarding exterior materials, the proposed building is clad primarily with slate grey-colored horizontal metal paneling. The four corners will be wrapped with midnight-bronze-colored standing seam metal paneling while the building's primary (east) corner will be accentuated with colonial-red-colored horizontal metal paneling. Wall-pack (i.e. HVAC) units will be located below the large unit windows with the color of the louver covers to match the dark bronze material of the windows above.

Analysis

This proposal is subject to the standards for Demolitions [MGO Section 28.185(7)]. It is also subject to the standards for Conditional Uses [MGO §28.183(6)] as Section 28.076(4)(c) notes, "All new buildings and additions greater than twenty thousand (20,000) square feet or that have more than four (4) stories shall obtain conditional use approval. In addition, the Urban Design Commission shall review such projects for conformity to the design standards in Sec. 28.071(3), if applicable, and the Downtown Urban Design Guidelines and shall report its findings to the Plan Commission." The Urban Design Commission is also an approving body as the subject site is located in Urban Design District No. 4. Lastly, MGO Section 16.23(5)(g) provides the process and standards of approval for certified survey maps. This analysis begins with a summary of the adopted plan recommendations.

Conformance with Adopted Plans

The 2023 <u>Comprehensive Plan</u> recommends Downtown Core (DC) development for the subject site and the adjacent parcel to the southwest. The Plan notes that DC "represents the nucleus of downtown and accommodates a wide variety of employment, service, retail, government, residential, and other uses in large scale buildings that comprise the most intensely developed part of the city." For details on heights, mix of uses, ground floor uses, pedestrian friendly design, and other considerations which the Plan notes "must be addressed for development within this category", the Plan defers to the Downtown Plan.

The 2012 <u>Downtown Plan</u> notes that the appropriate height for new buildings is influenced by numerous factors such as topography, important view corridors and viewsheds, the presence of historic buildings, the use and scale recommendations for an area, and the existing scale of buildings in the vicinity. For the subject site, it recommends a maximum of eight stories. Staff notes that the Downtown Plan also specifically identifies East Washington Avenue as a "Premier Corridor" as one of eight major streets that frame views to and from the Capitol. As such, staff believes it is very important to consider how the building is perceived from various long views, including those of the Capitol and other prominent structures.

It also does identify the subject site within the "Downtown Core Mixed-Use" neighborhood/district for which it states as an objective, "should generally possess the highest intensity of development. A mix of office, employment, retail, government, residential, cultural entertainment and other uses should be pursued to retain the area's vibrancy, including beyond normal business hours." Lastly, the Plan's Streetscape Design Map on Page 39 identifies the E Washington Avenue / Blair Street intersection as a 'Major Gateway Intersection.' As the accompanying text talks about the higher the level (classification) of street, the higher the level of design and amenities, therefore being a major gateway intersection, the streetscape design at the subject site's main intersection is particularly important.

The <u>First Settlement Neighborhood Master Plan</u> (1995) notes about the subject site, that there is "potential for greater utilization and higher density mixed-use development."

Staff believe the proposed eight-story, mixed-use building is consistent with adopted plan recommendations described above.

Demolition Permit Standards

At its October 5, 2021 meeting, the Common Council adopted Ordinance 21-00065 (ID 67074) to revise the standards and process for demolition and removal permits in Section 28.185 of the Zoning Code to remove consideration of proposed future use as a factor in approving demolition requests. The Plan Commission shall consider the seven approval standards in §28.185(9)(c) M.G.O. when making a determination regarding demolition requests and find them met in order to approve. The factors include a finding that the proposed demolition or removal is consistent with the statement of purpose of the demolition permits section and with the health, prosperity, safety, and welfare of the City of Madison. The statement of purpose in Section 28.185 states, in part:

"The purpose of this section is therefore to ensure the preservation of historic buildings, encourage applicants to strongly consider relocating rather than demolishing existing buildings, aid in the implementation of adopted City plans, maximize the reuse or recycling of materials resulting from a demolition, protect the public from potentially unsafe structures and public nuisances..."

On balance, staff believes that standards can be found met and provides the following additional information.

In regards to Standard 1 related to the possible building relocation efforts, the Plan Commission is required to consider any information provided related to possible relocation activities. The applicant noted in submitted materials that relocation was considered but was deemed not a "good prospect for relocation."

In regards to Standard 6 related to the condition of the buildings proposed for demolition, Planning Division staff have not received any comments from City Building Inspection, Fire, or Police. In the submitted information, the applicant provides <u>photos</u> of the existing structure. From the submitted information, staff is not aware of significant structural issues. While the code notes condition must be considered, it does not specify that a certain structural condition or level of disrepair is necessary in order to approve a demolition.

Related to the historic value of the structures, staff notes Standard 4 states:

"The Plan Commission has received and considered the report of the City's historic preservation planner regarding the historic value of the property as well as any report that may be submitted by the Landmarks Commission."

The Landmarks Commission reviewed the demolition of the building at its May 6, 2024 meeting, and at that meeting found that the building at 521 E Washington Avenue "has no known historic value."

Standard 7, which includes references the standards 1-6 states that "The Plan Commission shall consider the factors and information specified in items 1-6 and find that the proposed demolition or removal is consistent with the statement of purpose of this section and with the health, prosperity, safety, and welfare of the City of Madison."

The Planning Division has no information otherwise to suggest that the demolition would not meet the standards of approval in Section 28.185(9)(c) and recommends that the demolition be approved subject to input at the public hearing and comments from reviewing agencies.

Conditional Use Standards

The Conditional Use approval standards state that the Plan Commission shall not approve a conditional use without due consideration of the City's adopted plan recommendations, design guidelines, supplemental regulations, and finding that all of the conditional use standards of MGO §28.183(6) are met. Please note, recent changes to state law requires that conditional use findings must be based on "substantial evidence" that directly pertains to each standard and not based on personal preference or speculation. Staff provides additional discussion on the following standards.

Standard 2 states, "The City is able to provide municipal services to the property where the conditional use is proposed, given due consideration of the cost of providing those services." The Madison Police Department has expressed concern in the additional project number of service calls projected anticipated for the subject development site given the already high level of daily calls they make to the nearby Beacon, Salvation Army, and YMCA. The Madison Police Department provided the following report regarding service numbers:

From **April 1-June 30, 2024**, these are the calls for service at each location:

The Beacon, 615 E. Washington Ave: **152** (this address alone is the often the top address for calls for service in the entire city)

Salvation Army, 630 E. Washington Ave: 61

YWCA, 101 E. Mifflin St: 75

In the last three months, officers have responded to **288** calls for service to these three addresses, <u>all within</u> a 5 block radius [of the subject site]

If we add in the calls that the current Porchilight location at 306 N. Brooks generated for the past three months, that adds another **46** calls. Anecdotally, if it was located in the new proposed site, Central officers would have responded to **334** calls for service within the 5 block area.

At any given time, the Central District has 4-5 officers on duty. Many of the calls generated at the above addresses are at least two officer calls.

To address security concerns at the proposed building, the Madison Police Department has worked with City staff and Porchlight staff to develop the following list of security-related measures:

- Controlled access to the building via key fob, to include controlled access at all times on the back and side
 doors, and on the main entry after business hours when the offices close. Controlled access to the elevator
 via key fob. Lock box on outside of the building for police and fire to access a fob for the building.
- Security cameras on the exterior as well as on all the interior common spaces and hallways on each floor.
- Secured package area for resident package deliveries.
- Security and/or staff to monitor cameras and do routine walk throughs of the building, during nonbusiness hours.
- Building posted for No Trespass.

Staff have recommended a condition requiring the applicant to submit a security plan to address the points listed above. The revised plan shall be approved by the Central Police District Captain, after consultation with the Planning Division Director (or designee) to include the aforementioned list of security-related measures prior to final sign off and the issuance of building permits.

Staff also provide the following note regarding the consideration of Standard 2: the applicant is seeking a conditional use expressly for the building's height above four stories. If it were a four-story building with just as many units, it would be a permitted use (only requiring a demolition permit). This is because the individual uses — a 'counseling, community services organization' (for the Porchlight's office space) and 'dwelling units in a mixed-use building' — are both permitted uses in the UMX District. That said, staff believes that management-plan related conditions may be appropriate considering the additional intensity that will be created as part of the larger building.

With the Police Department's recommended condition, to which the applicant has indicated they can support, Staff believe the Commission can find Approval Standard #2 met.

Standard 8 states, "When applying the above standards to any new construction of a building or an addition to an existing building the Plan Commission shall find that the project creates an environment of sustained aesthetic desirability compatible with the existing or intended character of the area and the statement of purpose for the zoning district. In order to find that this standard is met, the Plan Commission may require the applicant to submit plans to the Urban Design Commission for comment and recommendation."

Staff note that the applicant has made some revisions to the exterior articulation and material composition since their Informational Presentation at the Urban Design Commission back on May 8, 2024. It is important to note that this site is in a very prominent location and also within a mapped viewshed corridor in the Downtown Plan as one of a limited number of viewsheds framing the State Capitol. As such, the design of the structure, especially the visible corner at East Washington and Blair is of significant importance. The UDC's comments focused on providing additional detail regarding the landscape design and on revising the corner element. Staff emphasize that further enhancing the highly visible corner element, both at grade and for the highly visible upper levels, is a very important consideration. Given the UDC's initial approval granted at their meeting on June 26, 2024 (coupled with the two conditions of approval (please see the next section)), Staff believe that this standard can be found met.

On balance, staff believes that the Conditional Use Standards can be found met subject to input at the public hearing, conditions of approval from the Urban Design Commission, and comments from reviewing agencies.

Urban Design Commission Review

As the proposed building is located within the UMX (Urban Mixed-Use) District, the UDC is an **advisory** body on this request. Section 28.076(4)(c) states that: "All new buildings and additions greater than twenty thousand (20,000) square feet or that have more than four (4) stories shall obtain conditional use approval. In addition, the Urban Design Commission shall review such projects for conformity to the design standards in <u>Section 28.071(3)</u>, if applicable, and the Downtown Urban Design Guidelines and shall report its findings to the Plan Commission." Additionally, the UDC is an **approving** body as the subject site is located within Urban Design District No. 4.

At their meeting on June 26, 2024, the Urban Design Commission made an **ADVISORY RECOMMENDATION to the Plan Commission to grant Initial Approval**. (The Legistar File ID for the UDC's review of this project is <u>83068</u>). A copy of the report of the UDC has been included at the end of this staff report). The motion, which passed with a unanimous vote, included the following conditions:

• The applicant shall provide additional information related to the landscape design and details of the "seating area" shown on the site plan, including providing a screen fence and/or landscaping.

> Revise the corner element to include a more defined building corner that is more cohesive with the other building corners, and with a higher level of design at the pedestrian level. Consideration should be given to including a canopy feature, removal of the white frame, or relocating the accent color at the ground floor, etc. for example.

Land Division

The applicant has submitted a one-lot Certified Survey Map to combine the underlying lots that comprise the subject parcel. M.G.O. Section 16.23(5)(g) provides the process for certified survey maps. Staff believe that all applicable standards for land divisions can be found met, subject to the comments recommended by reviewing agencies.

Public Input

At time of writing, Staff have not received any public comments.

Conclusion

The applicant is requesting approval of a conditional use to facilitate redevelopment of the site with an eightstory, 70-unit apartment building following demolition of a two-story commercial building.

The Planning Division believes that the Plan Commission may find that the proposed development meets the various standards for approval. The proposed building is consistent with the recommendations of the <u>Comprehensive Plan</u> and <u>Downtown Plan</u>, including the recommendations for an eight-story, mixed-use building. Considering the high prominence of this building, staff note the design of this structure is especially important. The proposed development also meets the standards for demolition permits having been found to have no historic value by the Landmarks Commission.

Recommendation

Planning Division Recommendation (Contact Chris Wells, (608) 261-9135)

The Planning Division recommends the following to the Plan Commission:

- That the Plan Commission that the standards for demolition permits are met to approve the demolition of a two-story commercial building located at 521 E Washington Avenue;
- That the Plan Commission find the standards for conditional uses are met to approve an eight-story
 mixed-use building containing approximately 3,800 square feet of office space and 70 apartments, subject
 to input at the public hearing and the conditions from reviewing agencies that follow; and
- That the Plan Commission forward the Certified Survey Map to combine 521 E Washington Avenue into
 one lot to the Common Council with a recommendation of approval subject to the conditions from
 reviewing agencies beginning on page 17.

Recommended Conditions of Approval Major/Non-Standard Conditions are Shaded

<u>Planning Division</u> (Contact Chris Wells, (608) 261-9135)

- Prior to final sign off and the issuance of permits, the applicant shall submit a management plan on the form provided by the Zoning Administrator. The management plan shall be approved by the Planning Division Director and Zoning Administrator (or their designees) prior to final approval.
- 2. The applicant shall submit a security plan to address the following points. The revised plan shall be approved by the Central Police District Captain, after consultation with the Planning Division Director (or designee) to include the following list of security-related measures prior to final sign off and the issuance of building permits:
- Controlled access to the building via key fob, to include controlled access at all times on the back and side doors, and on the main entry after business hours when the offices close. Controlled access to the elevator via key fob. Lock box on outside of the building for police and fire to access a fob for the building.
- Security cameras on the exterior as well as on all the interior common spaces and hallways on each floor.
- Secured package area for resident package deliveries.
- Security and/or staff to monitor cameras and do routine walk throughs of the building, during non-business hours.
- Building posted for No Trespass.

Please contact Jessica Vaughn, Urban Design Commission Secretary, at (608) 267-8740 if you have any questions regarding the following two (2) items which were recommended by the Urban Design Commission to the Plan Commission as part of their motion to grant initial approval at their meeting on June 26, 2024:

- 3. The applicant shall provide additional information related to the landscape design and details of the "seating area" shown on the site plan, including providing a screen fence and/or landscaping.
- 4. Revise the corner element to include a more defined building corner that is more cohesive with the other building corners, and with a higher level of design at the pedestrian level. Consideration should be given to including a canopy feature, removal of the white frame, or relocating the accent color at the ground floor, etc. for example.

City Engineering Division (Contact Brenda Stanley, (608) 261-9127)

5. The property is a closed gas station and contains residual contamination per WDNR BRRTS record #09-13-582647 (MADISON CHIROPRACTIC ASSOCIATION). In addition, the City of Madison has soil boring data from WisDOT's recent reconstruction of S. Blair St. Contact Brynn Bemis at bbemis@cityofmadison.com or 608.695.1385 for this information. If contamination is encountered during construction, follow all WDNR and DSPS regulations for proper handling and disposal.

- 6. Update demolition plan and utility plan to show the 2 sanitary sewer laterals provided with the S. Blair St project (City Project 11135). There is an 8" diameter lateral that is located 24 ft west of the southeast property line and a 6" diameter lateral at 126' west of southeast property line.
- 7. Revise the plans to show a proposed private internal drainage system on the site. Include the depths and locations of structures and the type of pipe to be used. (POLICY AND MGO 10.29)
- 8. Revise the plans to identify the location of the public storm sewer (proposed or existing) that will serve the development show the connection of the private internal drainage system to the public storm sewer. (POLICY AND MGO OVER 10,000 SF OF IMPERVIOUS AREA 10.29 and 37.05(7)(b))
- 9. Enter into a City / Developer agreement for the required infrastructure improvements. Agreement to be executed prior to sign off. Allow 4-6 weeks to obtain agreement. Contact City Engineering to schedule the development and approval of the plans and the agreement. (MGO 16.23(9)c)
- 10. Construct sidewalk, terrace, curb and gutter and pavement to a plan as approved by City Engineer
- 11. Madison Metropolitan Sewerage District (MMSD) charges are due and payable prior to Engineering sign-off, unless otherwise collected with a Developer's / Subdivision Contract. Contact Mark Moder (608-261-9250) to obtain the final MMSD billing a minimum of two (2) working days prior to requesting City Engineering signoff. (MGO 16.23(9)(d)(4)
- 12. Obtain a permanent sewer plug permit for each existing sanitary sewer lateral serving a property that is not to be reused and a temporary sewer plug permit for each sewer lateral that is to be reused by the development. The procedures and fee schedule is available online at http://www.cityofmadison.com/engineering/permits.cfm. (MGO CH 35.02(14))
- 13. An Erosion Control Permit is required for this project. See Storm comments for permit specific details and requirements.
- 14. A Storm Water Management Report and Storm Water Management Permit is required for this project. See Storm comments for report and permit specific details and requirements.
- 15. This site appears to disturb less than one (1) acre of land. No submittal to the WDNR, CARPC or Department of Safety and Professional Services (DSPS) is required as the City of Madison Building Inspection Department is an approved agent for DSPS.
- 16. Revise the site plan to show all existing public sanitary sewer facilities in the project area as well as the size, invert elevation, and alignment of the proposed service. (POLICY)
- 17. Provide additional detail how the enclosed depression(s) created by the parking entrance(s) to the below building parking area(s) is/are served for drainage purposes. The building must be protected from receiving runoff up through the 100-yr design storm that is current in Madison General Ordinance Chapter 37. If the enclosed depression(s) is/are to be served by a gravity system provide calculations stamped by a Wisconsin P.E. that show inlet and pipe capacities meet this requirement. If the enclosed depression(s) is/are to be served by a pump system provide pump sizing calculations stamped by a Wisconsin P.E. or licensed Plumber that show this requirement has been met.

- 18. This project will disturb 4,000 sf or more of land area and require an Erosion Control Permit. Submit the Erosion Control Permit Application (with USLE calculations and associated fee) to Megan Eberhardt (west) at meberhardt@cityofmadison.com, or Daniel Olivares (east) at daolivares@cityofmadison.com, for approval. The permit application can be found on City Engineering's website at http://www.cityofmadison.com/engineering/Permits.cfm.
- 19. Prior to approval, this project shall comply with Chapter 37 of the Madison General Ordinances regarding stormwater management. Specifically, this development is required to submit a Storm Water Management Permit application, associated permit fee, Stormwater Management Plan, and Storm Water Management Report to City Engineering. The Stormwater Management Permit application can be found on City Engineering's website at http://www.cityofmadison.com/engineering/Permits.cfm.

The Storm Water Management Plan & Report shall include compliance with the following:

Report: Submit prior to plan sign-off, a stormwater management report stamped by a P.E. registered in the State of Wisconsin.

Electronic Data Files: Provide electronic copies of any stormwater management modeling or data files including SLAMM, RECARGA, TR-55, HYDROCAD, Sediment loading calculations, or any other electronic modeling or data files. If calculations are done by hand or are not available electronically, the hand copies or printed output shall be scanned to a PDF file and provided to City Engineering. (POLICY and MGO 37.09(2))

Rate Control Redevelopment: By design detain the 10-year post construction design storm such that the peak discharge during this event is reduced 15% compared to the peak discharge from the 10-year design storm in the existing condition of the site. Further, the volumetric discharge leaving the post development site in the 10-year storm event shall be reduced by 5% compared to the volumetric discharge from the site in an existing condition during the 10-year storm event. These required rate and volume reductions shall be completed, using green infrastructure that captures at least the first 1/2 inch of rainfall over the total site impervious area. If additional stormwater controls are necessary beyond the first 1/2 inch of rainfall, either green or non-green infrastructure may be used.

TSS Redevelopment with TMDL: Reduce TSS by 80% off of the proposed development when compared with the existing site.

Submit a draft Stormwater Management Maintenance Agreement (SWMA) for review and approval that covers inspection and maintenance requirements for any BMP used to meet stormwater management requirements on this project.

20. Submit, prior to plan sign-off but after all revisions have been completed, digital PDF files to the Engineering Division. Email PDF file transmissions are preferred to: bstanley@cityofmadison.com (East) or ttroester@cityofmadison.com (West).

<u>City Engineering – Mapping</u> (Contact Jeff Quamme, (608) 266-4097)

- 21. Grant a Public Sidewalk Easement to the City on the face of the pending Certified Survey Map along S Blair St to provide the required terrace as required by City of Madison Traffic Engineering.
- 22. An existing retaining wall is being removed and a new wall constructed less than a foot from the property line. If the adjacent lands will be disturbed during the removal, construction or future maintenance, provide the easement / agreement permitting any of the above uses.

- 23. The Owner on the application is not the current owner of record for the property. Provide the recorded conveyance of the parcel to the Owner noted on the application prior to final site plan sign off.
- 24. The address of 521 E Washington Ave will be inactivated and archived with the demolition of the building. The proposed building has an address of 4 S Blair St. The site plan shall reflect a proper street address of the property as reflected by official City of Madison Assessor's and Engineering Division records.
- 25. The pending Certified Survey Map application for this property shall be completed and recorded with the Dane County Register of Deeds (ROD), the new parcel data created by the Assessor's Office and the parcel data available to zoning and building inspection staff prior to issuance of building permits for new construction or early start permit.
- 26. Submit a site plan and a complete building Floor Plan in PDF format to Lori Zenchenko (lizenchenko@cityofmadison.com) that includes a floor plan of each floor level on a separate sheet/page for the development of a complete interior addressing plan. Also, include a unit matrix for apartment buildings.

The Addressing Plan for the entire project shall be finalized and approved by Engineering (with consultation and consent from the Fire Marshal if needed) PRIOR to the verification submittal stage of this LNDUSE with Zoning. The final approved Addressing Plan shall be included in said Site Plan Verification application materials or a revised plan shall provided for additional review and approval by Engineering.

Per 34.505 MGO, a full copy of the approved addressing plan shall be kept at the building site at all times during construction until final inspection by the Madison Fire Department.

For any changes pertaining to the location, deletion or addition of a unit, or to the location of a unit entrance, (before, during, or after construction), a revised Address Plan shall be resubmitted to Lori Zenchenko to review addresses that may need to be changed and/or reapproved.

<u>Traffic Engineering</u> (Contact Sean Malloy, (608) 266-5987)

- 27. Note: This site presents difficult constructability issues; access to neighboring sites must be maintained at all times, protected walkways will be constructed and maintained as soon as possible and little to no access to the Public Right-of-Way on East Washington Avenue and South Blair Street will be granted for construction purposes. Provide a detailed construction plan to Traffic Engineering for review prior to final signoff.
- 28. The applicant shall submit one contiguous plan showing proposed conditions and one contiguous plan showing existing conditions for approval. The plan drawings shall be to engineering scale and include the following, when applicable: existing and proposed property lines; parcel addresses; all easements; vision triangles; pavement markings; signing; building placement; items in the terrace such as signs, street light poles, hydrants; surface types such as asphalt, concrete, grass, sidewalk; driveway approaches, including those adjacent to and across street from the project lot location; parking stall dimensions, including two (2) feet of vehicle overhang; drive aisle dimensions; semitrailer movement and vehicle routes; dimensions of radii; and percent of slope.
- 29. The Developer shall post a security deposit prior to the start of development. In the event that modifications need to be made to any City owned and/or maintained traffic signals, street lighting, signing, pavement marking and conduit/handholes, the Developer shall reimburse the City for all associated costs including engineering, labor and materials for both temporary and permanent installations.

- 30. The City Traffic Engineer may require public signing and marking related to the development; the Developer shall be financially responsible for such signing and marking.
- 31. All parking facility design shall conform to MGO standards, as set in section 10.08(6).
- 32. All bicycle parking adjacent pedestrian walkways shall have a 2 foot buffer zone to accommodate irregularly parked bicycles and/or bicycle trailers.
- 33. Per Section MGO 12.138 (14), this project is not eligible for residential parking permits. It is recommended that this prohibition be noted in the leases for the residential units.
- 34. The applicant shall adhere to all vision triangle requirements as set in MGO 27.05 (No visual obstructions between the heights of 30 inches and 10 feet at a distance of 25 feet behind the property line at streets and 10 feet at driveways.). Alteration necessary to achieve compliance may include but are not limited to; substitution to transparent materials, removing sections of the structure and modifying or removing landscaping elements. If applicant believes public safety can be maintained they shall apply for a reduction of MGO 27.05(2)(bb) Vision Clearance Triangles at Intersections Corners. Approval or denial of the reduction shall be the determination of the City Traffic Engineer.
- 35. City of Madison radio systems are microwave directional line of sight to remote towers citywide. The building elevation will need to be reviewed by Traffic Engineering to accommodate the microwave sight and building. The applicant shall submit grade and elevations plans if the building exceeds three stories prior to sign-off to be reviewed and approved by Andrew Oliver, (267-1979, aoliver@cityofmadison.com) Traffic Engineering Shop, 4151 Nakoosa Trail. The applicant shall return one signed approved building elevation copy to the City of Madison Traffic Engineering office with final plans for sign off.
- 36. The applicant shall provide a clearly defined 5' walkway clear of all obstructions to assist citizens with disabilities, especially those who use a wheel chair or are visually impaired. Obstructions include but are not limited to tree grates, planters, benches, parked vehicle overhang, signage and doors that swing outward into walkway.
- 37. "Stop" signs shall be installed at a height of seven (7) feet from the bottom of the sign at all class III driveway approaches, including existing driveways, behind the property line and noted on the plan. All directional/regulatory signage and pavement markings on the site shall be shown and noted on the plan.
- 38. The applicant shall show the dimensions for the proposed class III driveway including the width of the drive entrance, width of the flares, and width of the curb cut.
- 39. Applicant shall submit for review a waste removal plan. This shall include vehicular turning movements.
- 40. Applicant shall submit for review a Commercial Delivery Plan. This plan will include times, vehicle size, use of loading zones and all related turning movements.
- 41. The applicant shall dedicate Right of Way or grant a Public Sidewalk Easement for and be responsible for the construction of a minimum five (5)-foot wide sidewalk, eight (8)-foot terrace, and additional one (1) foot for maintenance, where applicable, along their site's frontage of Blair Street.

Parking Review (Contact Trent W Schultz, (608) 246-5806)

42. The applicant shall submit a Transportation Demand Management (TDM) Plan to tdm@cityofmadison.com. The TDM Plan is required per MGO 16.03. Applicable fees will be assessed after the TDM Plan is reviewed by staff.

Zoning Administrator (Contact Jenny Kirchgatter, (608) 266-4429)

- 43. Section 28.185(9)(b) requires that every applicant for a demolition or removal approval that requires approval by the Plan Commission is required to get a Reuse and Recycling Plan approved by the City Recycling Coordinator, Bryan Johnson at streets@cityofmadison.com prior to receiving a raze permit. Every person who is required to submit a reuse and recycling plan pursuant to Section 28.185(9)(b) shall submit documents showing compliance with the plan within sixty (60) days of completion of demolition. A demolition or removal permit is valid for two (2) years from the date of the Plan Commission approval.
- 44. Provide electric vehicle ready stalls per Section 28.141(8)(e) Electric Vehicle Charging Station Requirements. A minimum of 10% of the residential parking stalls (1 stall) must be electric vehicle ready. Identify the location of the electric vehicle ready stall on the plans.
- 45. Bicycle parking for the residential dwelling units shall comply with the requirements of MGO Sections 28.141(4)(g) and 28.141(11) and shall be designated as short-term or long-term bicycle parking. A minimum of 70 resident bicycle stalls are required plus a minimum of 7 short-term guest stalls. Up to twenty-five percent (25%) of bicycle parking may be structured parking, vertical parking or wall mount parking, provided there is a five (5) foot access aisle for wall mount parking. NOTE: A bicycle stall is a minimum of two (2) feet by six (6) feet with a five (5) foot wide access area. Submit a detail showing the model of bike rack to be installed.
- 46. Bicycle parking for the office use shall comply with the requirements of Sections 28.141(4)(g) and 28.141(11). Provide a minimum of two (2) short-term bicycle parking stalls located in a convenient and visible area on a paved or pervious surface. Bicycle parking shall be located at least as close as the closest non-accessible automobile parking and within one hundred (100) feet of a principal entrance.
- 47. Submit the landscape plan and landscape worksheet stamped by the registered landscape architect. Per Section 28.142(3) Landscape Plan and Design Standards, landscape plans for zoning lots greater than ten thousand (10,000) square feet in size must be prepared by a registered landscape architect.
- 48. Provide details showing that the building façades meet the door and window opening requirements of Section 28.071(3)(e). For street-facing facades with ground story non-residential uses, the ground story door and window openings shall comprise a minimum of fifty percent (50%) of the facade area. Upper story openings shall comprise a minimum of fifteen percent (15%) of the facade area per story.
- 49. Per Section 28.186(4)(b), the property owner or operator is required to bring the property into compliance with all elements of the approved site plans by the date established by the Zoning Administrator as part of the site and building plan approval. Work with Zoning staff to establish a final site compliance date.
- 50. Signage approvals are not granted by the Plan Commission. Signage must be reviewed for compliance with Chapter 31 Sign Codes of the Madison General Ordinances and Chapter 33 Urban Design District ordinances. Signage permits are issued by the Zoning Section of the Department of Planning and Community and Economic Development.

Madison Fire Department (Contact Matt Hamilton, (608) 266-4457)

- 51. Cooking appliances to be induction cooktops to help reduce the number of calls for cooking and false alarms.
- 52. Based on floor plate layout with the entry vestibule located on Blair Street MFD would anticipate that the address and first point of entrance for the building would be Blair Street
- 53. Stair 01 door swings the wrong way for Egress.
- 54. IFC 1028.1 Egress from Stair 01 to the exit door shall have a clear unobstructed path. Some of the lobby furniture appears to be obstructing this path.

<u>Parks Division</u> (Contact Adam Kaniewski, <u>akaniewski@cityofmadison.com</u>)

The agency reviewed this request and has recommended no conditions of approval.

Forestry Review (Contact Jeffrey Heinecke, (608) 266-4890)

55. Additional street trees are needed for this project. Tree planting specifications can be found in section 209 of City Madison Standard Specifications for Public Works Construction (website: https://www.cityofmadison.com/business/pw/specs.cfm) - All street tree planting locations and tree species within the right of way shall be determined by City Forestry. A landscape plan and street tree planting plan shall be submitted in PDF format to City Forestry for approval of planting locations within the right of way and tree species. All available street tree planting locations shall be planted within the project boundaries. Add following note on both the landscape and street tree plan sets: At least one week prior to street tree planting, Contractor shall contact City Forestry at (608) 266-4816 to schedule inspection and approval of nursery tree stock and review planting specifications with the landscaper.

Water Utility (Contact Jeff Belshaw, (608) 261-9835)

- 56. Utility plan indicates connecting to an existing 6 inch water service. Currently there is only a 1 inch copper service to this lot. Update utility plan to show a new 6 inch water service if required.
- 57. A Water Service Application Form and fees must be submitted before connecting to the existing water system. Provide at least two working days notice between the application submittal and the requested installation or inspection appointment. Application materials are available on the Water Utility's Plumbers & Contractors website (http://www.cityofmadison.com/water/plumbers-contractors), otherwise they may be obtained from the Water Utility Main Office at 119 E Olin Ave. A licensed plumber signature is required on all water service applications. For new or replacement services, the property owner or authorized agent is also required to sign the application. A Water Meter Application Form will subsequently be required to size & obtain a water meter establish a Water Utility customer account and/or establish a Water Utility fire service account. If you have questions regarding water service applications, please contact Madison Water Utility at (608) 266-4646.

Metro Transit (Contact Timothy Sobota, (608) 261-4289)

- 58. In coordination with any public works improvements, the applicant shall maintain or replace the concrete boarding and amenity pad surface at the existing Metro bus stop on the south side of East Washington Avenue, west of Blair Street (#1579).
- 59. The existing curbside bus stop zone, accessible pedestrian sidewalk and curb ramp/crosswalk across the west side of East Washington Avenue, and the concrete boarding pad on the south side of East Washington Avenue, east of Blair Street, provide critical access to the City's transit operations, and any planned or permitted obstruction of the existing bus stop zones (or accessible route to the future median BRT platform) in excess of thirty (30) days may require additional right-of-way improvements and/or other mitigation measures in coordination with Traffic Engineering and Metro Transit staff in order to create an alternate bus stop zone that would serve the East Washington Avenue at Blair Street intersection area (and future BRT median platform) in a comparable operational and accessible manner. The applicant shall include a site logistics plan outlining all phases of planned construction and any planned impacts to the City's public rights-of-way.
- 60. The applicant shall include the location of these transit facilities and a site logistics plan on the final documents filed with their permit application so that Metro Transit may review and effectively plan for City transit access adjacent this property.
- 61. Metro Transit operates daily all-day transit service along East Washington Avenue adjacent this property with trips at least every 30 minutes (every 15 minutes or less during the day on weekdays and Saturdays). Metro Transit operates additional daily all-day transit service along East Wilson Street and Williamson Street near this property with trips at least every 30 minutes (every 15 minutes or less during the day on weekdays).
- 62. Metro Transit would initially estimate the following counts of potentially eligible trips towards US Green Building Council/LEED Quality Access to Transit points: 220 Weekday & 131 Weekend (average). Please contact Metro Transit if additional analysis would be of interest.

Certified Survey Map

Recommended Conditions of Approval

Major/Non-Standard Conditions are Shaded

City Engineering Division (Contact Brenda Stanley, (608) 261-9127)

- 63. The property is a closed gas station and contains residual contamination per WDNR BRRTS record #09-13-582647 (MADISON CHIROPRACTIC ASSOCIATION). In addition, the City of Madison has soil boring data from WisDOT's recent reconstruction of S. Blair St. Contact Brynn Bemis at bbemis@cityofmadison.com or 608.695.1385 for this information. If contamination is encountered during construction, follow all WDNR and DSPS regulations for proper handling and disposal.
- 64. Enter into a City / Developer agreement for the required infrastructure improvements. Agreement to be executed prior to sign off. Allow 4-6 weeks to obtain agreement. Contact City Engineering to schedule the development and approval of the plans and the agreement. (MGO 16.23(9)c)

- 65. Construct sidewalk, terrace, curb and gutter and pavement to a plan as approved by City Engineer
- 66. Madison Metropolitan Sewerage District (MMSD) charges are due and payable prior to Engineering sign-off, unless otherwise collected with a Developer's / Subdivision Contract. Contact Mark Moder (608-261-9250) to obtain the final MMSD billing a minimum of two (2) working days prior to requesting City Engineering signoff. (MGO 16.23(9)(d)(4)
- 67. A minimum of two (2) working days prior to requesting City Engineering signoff on the plat/csm contact either Tim Troester (West) at 261-1995 (ttroester@cityofmadison.com) or Brenda Stanley (East) at 608-261-9127 (bstanley@cityofmadison.com) to obtain the final stormwater utility charges that are due and payable prior to sub-division of the properties. The stormwater utility charges (as all utility charges) are due for the previous months of service and must be cleared prior to the land division (and subsequent obsolesces of the existing parcel). (POLICY)

City Engineering – Mapping (Contact Jeff Quamme, (608) 266-4097)

- 68. Grant a Public Sidewalk Easement to the City on the face of the Certified Survey Map along S Blair St to provide the required terrace as required by City of Madison Traffic Engineering. Contact Jeff Quamme for the required easement text to be placed on the CSM.
- 69. Wisconsin Administrative Code A-E 7.08 identifies when Public Land System (PLS) tie sheets must be filed with the Dane County Surveyor's office. The Developer's Surveyor and/or Applicant must submit copies of required tie sheets or monument condition reports (with current tie sheet attached) for all monuments, including center of sections of record, used in this survey, to Jeff Quamme, City Engineering (jrquamme@cityofmadison.com)
- 70. Prior to Engineering final sign-off by main office for Plats or Certified Survey Maps (CSM), the final Plat or CSM in pdf format must be submitted by email transmittal to Engineering Land Records Coordinator Jeff Quamme (irquamme@cityofmadison.com) for final technical review and approval. This submittal must occur a minimum of two working days prior to final Engineering Division sign-off.
- 71. Provide tangent bearings of curves as required by statute. Also C1 is not tangent as stated in the legal description. Modify the description accordingly.
- 72. The tie description in the legal description is incorrect and does not match the map or true conditions. Modify the description as necessary.
- 73. This pending Certified Survey Map application for this property shall be completed and recorded with the Dane County Register of Deeds (ROD), the new parcel data created by the Assessor's Office and the parcel data available to zoning and building inspection staff prior to issuance of building permits for new construction or early start permit.

- 74. Submit to Jeff Quamme, prior to Engineering sign-off of the subject plat, one (1) digital CADD drawing in a format compatible with AutoCAD. The digital CADD file(s) shall be referenced to the Dane County Coordinate System and shall contain, at minimum, the list of items stated below, each on a separate layer/level name. The line work, preferably closed polylines for lot lines, shall be void of gaps and overlaps and match the final recorded plat:
 - a) Right-of-Way lines (public and private)
 - b) Lot lines
 - c) Lot numbers
 - d) Lot/Plat dimensions
 - e) Street names
 - f) Easement lines (i.e. all shown on the plat including wetland & floodplain boundaries.)

NOTE: This Transmittal is a separate requirement from the required Engineering Streets Section for design purposes. The Developer/Surveyor shall submit new updated final plat, electronic data for any changes subsequent to any submittal.

Traffic Engineering (Contact Sean Malloy, (608) 266-5987)

- 75. Note: This site presents difficult constructability issues; access to neighboring sites must be maintained at all times, protected walkways will be constructed and maintained as soon as possible and little to no access to the Public Right-of-Way on East Washington Avenue and South Blair Street will be granted for construction purposes. Provide a detailed construction plan to Traffic Engineering for review prior to final signoff.
- 76. The applicant shall submit one contiguous plan showing proposed conditions and one contiguous plan showing existing conditions for approval. The plan drawings shall be to engineering scale and include the following, when applicable: existing and proposed property lines; parcel addresses; all easements; vision triangles; pavement markings; signing; building placement; items in the terrace such as signs, street light poles, hydrants; surface types such as asphalt, concrete, grass, sidewalk; driveway approaches, including those adjacent to and across street from the project lot location; parking stall dimensions, including two (2) feet of vehicle overhang; drive aisle dimensions; semitrailer movement and vehicle routes; dimensions of radii; and percent of slope.
- 77. The Developer shall post a security deposit prior to the start of development. In the event that modifications need to be made to any City owned and/or maintained traffic signals, street lighting, signing, pavement marking and conduit/handholes, the Developer shall reimburse the City for all associated costs including engineering, labor and materials for both temporary and permanent installations.
- 78. The City Traffic Engineer may require public signing and marking related to the development; the Developer shall be financially responsible for such signing and marking.
- 79. All parking facility design shall conform to MGO standards, as set in section 10.08(6).
- 80. All bicycle parking adjacent pedestrian walkways shall have a 2 foot buffer zone to accommodate irregularly parked bicycles and/or bicycle trailers.

- 81. Per Section MGO 12.138 (14), this project is not eligible for residential parking permits. It is recommended that this prohibition be noted in the leases for the residential units.
- 82. The applicant shall adhere to all vision triangle requirements as set in MGO 27.05 (No visual obstructions between the heights of 30 inches and 10 feet at a distance of 25 feet behind the property line at streets and 10 feet at driveways.). Alteration necessary to achieve compliance may include but are not limited to; substitution to transparent materials, removing sections of the structure and modifying or removing landscaping elements. If applicant believes public safety can be maintained they shall apply for a reduction of MGO 27.05(2)(bb) Vision Clearance Triangles at Intersections Corners. Approval or denial of the reduction shall be the determination of the City Traffic Engineer.
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- 86. The applicant shall show the dimensions for the proposed class III driveway including the width of the drive entrance, width of the flares, and width of the curb cut.
- 87. Applicant shall submit for review a waste removal plan. This shall include vehicular turning movements.
- 88. Applicant shall submit for review a Commercial Delivery Plan. This plan will include times, vehicle size, use of loading zones and all related turning movements.
- 89. The applicant shall dedicate Right of Way or grant a Public Sidewalk Easement for and be responsible for the construction of a minimum five (5)-foot wide sidewalk, eight (8)-foot terrace, and additional one (1) foot for maintenance, where applicable, along their site's frontage of Blair Street.

Parking Review (Contact Trent W Schultz, (608) 246-5806)

The agency reviewed this project and determined a Transportation Demand Management (TDM) Plan is not required as part of certified survey map review. A TDM Plan will be reviewed as part of conditional use approval.

Parks Division (Contact Adam Kaniewski, akaniewski@cityofmadison.com)

- 90. The following note should be included on the CSM: "LOTS WITHIN THIS CSM ARE SUBJECT TO IMPACT FEES THAT ARE DUE AND PAYABLE AT THE TIME BUILDING PERMIT(S) ARE ISSUED."
- 91. Prior to sign off on the CSM the applicant shall execute a declaration of conditions and covenants for impact fees.
- 92. The Parks Division shall be required to sign off on this CSM.

Water Utility (Contact Jeff Belshaw, (608) 261-9835)

93. The following information shall be noted on the CSM prior to final approval: The properties are located within Wellhead Protection District—Zone (WP-17). Uses of the properties are required to comply with the City of Madison Wellhead Protection requirements as provided under MGO Sections 13.22 and 28.102.

Office of Real Estate Services (Lance Vest, (608) 245-5794)

- 94. Prior to approval sign-off by the Office of Real Estate Services ("ORES"), the Owner's Certificate(s) on the CSM shall be executed by all parties of interest having the legal authority to do so, pursuant to Wis. Stats. 236.21(2)(a). Said parties shall provide documentation of legal signing authority to the notary or authentication attorney at the time of execution. The title of each certificate shall be consistent with the ownership interest(s) reported in the most recent title report.
 - When possible, the executed original hard stock recordable CSM shall be presented at the time of ORES approval sign-off. If not, the City and the Register of Deeds are now accepting electronic signatures. A PDF of the CSM containing electronic signatures shall be provided to ORES to obtain approval sign-off.
- 95. Prior to CSM approval sign-off, an executed and notarized or authenticated certificate of consent for all mortgagees/vendors shall be included following the Owner's Certificate(s).
- 96. As of June 14, 2024, the 2023 real estate taxes are paid for the subject property.

Per 236.21(3) Wis. Stats. and 16.23(5)(g)(1) Madison General Ordinances, the property owner shall pay all real estate taxes that are accrued or delinquent for the subject property prior to CSM recording. This includes property tax bills for the prior year that are distributed at the beginning of the year. Receipts from the City of Madison Treasurer are to be provided before or at the time of sign-off. Payment is made to:

City of Madison Treasurer 210 Martin Luther King, Jr. Blvd. Madison, WI 53701

- 97. As of June 14, 2024, there are special assessments reported. All known special assessments are due and payable prior to CSM approval sign-off. If special assessments are levied against the property during the review period and prior to CSM approval sign-off, they shall be paid in full pursuant to Madison General Ordinance Section 16.23(5)(g)1.
 - In lieu of this requirement, the owner may present written documentation from the City's Board of Public Works that the special assessments may be continued to be paid on the installment basis authorized by the Board of Public Works. However, if lands within the CSM boundary are to be dedicated, the special assessments levied against the dedicated lands are to be paid in full.
- 98. Pursuant to Madison City Ordinance Section 16.23(5)(g)(4), the owner shall furnish a title report to ORES via email to Lance Vest (Ivest@cityofmadison.com), as well as the survey firm preparing the proposed CSM. The report shall include all associated documents that have been recorded.

A title commitment has been provided but is considered only as supplementary information to the title report. Surveyor shall update the CSM with the most recent information reported in the title report. ORES reserves the right to impose additional conditions of approval in the event the title report contains changes that warrant revisions to the CSM.

The following agencies have reviewed this request and has recommended no conditions of approval:

Planning, Zoning, Assessor's Office, Fire, Forestry, Metro

URBAN DESIGN COMMISSION MEETING REPORT

June 26, 2024

TO MADE OF MAD

Agenda Item #: 4

Project Title: 521 E Washington Avenue - New Mixed-Use Building in Urban Design District (UDD) 4. (District 6)

Legistar File ID #: 83068

Members Present: Cliff Goodhart, Chair; Rafeeq Asad, Christian Harper, Marsha Rummel, Shane Bernau, and

Russell Knudson

Prepared By: Jessica Vaughn, AICP, UDC Secretary

Summary

At its meeting of June 26, 2024, the Urban Design Commission **GRANTED INITIAL APPROVAL** of a new mixed-use building in UDD 4 located at 521 E Washington Avenue. Registered and speaking in support were Duane Johnson, and Joseph Rice. Registered in support and available to answer questions was Joe Porter.

Summary of Commission Discussion and Questions:

Overall, the Commission noted that the design has been much improved from the Informational Presentation, noting that the composition is now more unified.

The Commission discussed the use of color and materials to emphasize the building corner. Nothing in the plan suggests that the corner is more prominent, it is mostly emphasized higher up on the building, and the entrance isn't emphasized enough at the street level.

The Commission inquired about the useable open space being limited to the outdoor seating area and whether consideration was given to other opportunities for rooftop spaces.

The applicant noted that the addition of rooftop space would trigger compliance with high-rise building code requirements, triggering the requirements for fire command centers, etc. raising construction costs significantly. In addition, the street tree requirements and sidewalk requirements along Blair Street also create additional constraints with regard to open space.

The horizontal lines versus vertical lines were discussed. The applicant noted that the proposed mixing of the horizontal and vertical lines and reveals will result in noticeable shade/shadow effects.

The Commission expressed concern about the long views and the building corner. A pop of color at street level where the entrance is would help to emphasize it. The white frame element that goes up and around the parapet that abruptly ends could be done away with and would save money. Why not make the east and west ends of the building mimic each other? The ¾ of the building is awesome, if the one corner was just brought into compliance with the rest of the design, wouldn't it still be a good design? Make the corner like the other sides of the building. The special emphasis at the entrance could be at the entrance on the ground floor.

These renderings don't really do justice to how the building will really pop with those actual materials, with reflectivity and orientation change, nice subtle, ins and outs of materials is nice subtle design that doesn't really need the distraction of a red and white band going all the way up to the top.

The applicant noted that their design reflects a unique corner and importance of the corner. It's a prominent corner – the uniqueness in the architecture is to address the importance of the corner and having it be unique.

The Commission noted that if the corner was redesigned to reflect more of the other corners, adding a canopy may be a way to utilize color and emphasize the entrance. Something is off about the proportions; it is a symmetrical building in every other corner except for this one because of the proportions of the red and white elements. Simplification should be considered. It needs some color and excitement. With regard to the long views, it needs something unique that you can see from a distance and not fade into the background.

The Commission discussed reducing the number of parking stalls to provide for more open space. The applicant stated that they are already struggling with parking. The ground floor accessible entrance orientation was explained. The ground floor windows will be clear with views into office spaces.

The Commission discussed the wall pack louvers, which will be located under the windows, as part of the window system, in the darker color, integrated into the window system. The louvers were removed from the units located in the red element.

The back patio area needs something more; some creative energy needs to be put into what that edge could be including a screen fence, vines, etc. to give you a little protection from that parking lot. Something I would like the applicant to look at.

The white band that dies as it turns the corner seems like it could go away with maybe a canopy that wraps the corner at the entrance in its place. At the street level may be as equally an effective place for a pop of color. This is really a nice improvement.

A motion was made by Asad, seconded by Knudson, to grant Initial Approval with conditions.

Discussion on the motion:

Expression at the entrance, the ground floor is where it is not symmetrical and repetition is happening.

Taking the red and moving it to the center bay along E Washington and making the design more cohesive with the rest of the corners. Keeping some element of color, but not making that color fight what is happening. The swath of red should go to the ground in place of the dark gray. Keep color in it somewhere, making the sides symmetrical, and then doing something more dramatic or design intensive at the lower level.

Commissioner Knudson recognized the importance of this project, noting that he has learned from colleagues about design and the importance of beautiful architecture for the City's residents. The white piece makes us look up. Remembering the Informational Presentation discussion, reflecting on who is operating this building, there is something nice about having something to aspire and look up to in this architecture.

Action

On a motion by Asad, seconded by Knudson, the Urban Design Commission **GRANTED INITIAL APPROVAL**. The motion provided for the following conditions:

- The applicant shall provide additional information related to the landscape design and details of the "seating area" shown on the site plan, including providing a screen fence and/or landscaping.
- Revise the corner element to include a more defined building corner that is more cohesive with the other building corners, and with a higher level of design at the pedestrian level. Consideration should be given to

including a canopy feature, removal of the white frame, or relocating the accent color at the corner, etc. for example.

The motion was passed on a unanimous vote of (5-0).

From: <u>Jordan Mader</u>

To: Plan Commission Comments
Subject: Support Agenda Item 15
Date: Monday, July 8, 2024 3:57:26 PM

You don't often get email from jordanmader13@gmail.com. Learn why this is important

Caution: This email was sent from an external source. Avoid unknown links and attachments.

Hi all,

I'm writing in support of item 15. I believe this project is a great opportunity to increase our city's housing stock. As a city we get to decide on the proposals in front of us, not theoretical ones that some think could be better to be built somewhere else. If you decide this project is not allowed, we'll end up with less housing, higher housing prices, and a vacant Monarch health building.

I live near the proposed Porchlight redevelopment. Some of my neighbors who can make the meeting tonight and speak will likely comment on the proximity to other buildings that provide services to those with lower incomes in the area. Organizing resistance to push lower income housing options further out to other parts of the city does not solve any problems. In fact, the desirability of the land and proximity to the Capitol means that there will be plenty of other higher end new projects coming regardless and a rare opportunity to build a less segregated city.

Thank you, Jordan Mader 123 N. Blount St.



SHERI CARTER

PRESIDENT PORCHLIGHT, INC. BOARD

PLAN COMMISSION

July 8, 2024

RE: Legistar # 83525, 83526, and 83530

Dear PLAN COMMISSION,

I support the proposed development project and request the commissioner's approval tonight. I have attended two community meetings, and we were responsive to the attendees' concerns. In addition, porchlight staff have attended additional meetings of the community and businesses. Let me clear this is not a shelter, but permanent housing for our residents that allows them to have their own apartment without having to share common spaces. Our residents and staff deserve this opportunity.

I am available to speak and answer questions.

Sincerely,

Sheri Carter

From: patricia Kelly

To: Plan Commission Comments

Cc: Wells, Chris

Subject: Fw: Porchlight development at 521 E. Washington Ave., legistar 83526

Date: Monday, July 8, 2024 2:52:08 PM

You don't often get email from pjkmifflin@sbcglobal.net. Learn why this is important

Caution: This email was sent from an external source. Avoid unknown links and attachments.

Sent from AT&T Yahoo Mail for iPhone

Begin forwarded message:

On Friday, July 5, 2024, 9:11 AM, patricia Kelly <pjkmifflin@sbcglobal.net> wrote:

To: City of Madison Plan Commission

Re: Porchlight development proposal at 521 E Washington Ave., Legistar 83526

Dear Plan Commission members,

I am writing you to express my opposition to Porchlight's planned apartment development at the corner of Blair St. and E. Washington. My objections are twofold firstly, the whole idea of a fully segregated housing development for people dealing with mental health challenges flies in the face of what we've learned about integrated vs segregated housing. Segregated housing is stigmatizing for the residents of such housing and severely restricts any possibilities of living an integrated life in a typical neighborhood. Instead of being welcomed into a neighborhood, with all the benefits healthy neighborhood relationships provide, people will purposely avoid getting to know their neighbors who live in a large, segregated housing unit. Porchlight knows this and has an effective model of placing people with mental illness in scattered housing throughout Madison. There are at least two such examples of this integrated housing model in the Tenney-Lapham neighborhood where I live. The Tenney-Lapham neighborhood was supportive of The Shield, the Salvation Army development, because it is a model that combines low income and market rate housing thereby avoiding the significant pitfalls of a fully segregated development.

My second objection weaves into my first concern and that is that the corner that Porchlight has chosen already has two major service providers very close by, The Beacon and The Salvation Army. This is simply another form of carving out a small, 2-3 block radius, to provide services for people with significant needs further enhancing the reality of segregation.

As a city, we need to continue to lead the way in supporting people in a way that more fully integrates them into the ordinary fabric of our neighborhoods. Dane County has done an exemplary job of supporting integrated housing and work for people with developmental disabilities, MMSD has done an exemplary job of supporting students with a variety of disabilities into the classroom. There is no good reason that the city of Madison can't also be an exemplary model of integrated housing.

Thank you,

Pat Kelly

1210 E Mifflin St.

Madison

From: <u>Harry Richardson</u>

To: Plan Commission Comments

Subject: porch light project (item 14,15,16)

Date: Monday, July 8, 2024 1:51:38 PM

You don't often get email from richardsonharry348@gmail.com. Learn why this is important

Caution: This email was sent from an external source. Avoid unknown links and attachments.

Greetings,

As a Tenney-Lapham resident and long time eastsider, I have a number of concerns about this proposed development. My primary concern is that Porch Light has not committed to having 24/7 staff at the building to help this vulnerable population in case of individual or group problems. I have read the police proposal but they are mainly focused on making property secure in the building and cutting down on police calls, an understandable concern from their point of view but that doesn't cover all the needs of this group. I have concerns about what other services might be needed for people in this building, will they get some help navigating the new bus system to get to work, get groceries (Festival Foods is overpriced and I would assume limited income people would need a cheaper option) other needed trips? I also have a concern that the rent is five hundred dollars a month and many of these people will only have social security or related programs to help cover their expenses. It seems like a lot of people in the building will be paying half their income on rent. This seems neither sustainable nor fair. Again, I have not heard Porch Light address this concern. Also the new planned development has fewer units for people than their current location. What happens to those displaced by this change who for whatever reason can't move into the new sight? Just to be clear I am neither opposing or supporting this current development but I strongly feel these questions should be adequately addressed before this project is approved. Thank you for taking the time to read this email. I hope the committee can get answers to these and other concerns about this project.

Sincerely

-Harry Richardson

456 N Few St



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83531

File ID: 83531 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: PLAN

COMMISSION

File Created Date: 05/17/2024

File Name: CSM - 306 N Brooks St Final Action:

Title: Approving a Certified Survey Map of property owned by LZ Ventures, LLC located

at 306 N Brooks Street (District 8).

Notes:

Sponsors: Planning Division Effective Date:

Attachments: Locator Maps.pdf, Application.pdf, Proposed Enactment Number:

CSM.pdf, Staff Comments.pdf, Link to Demo Permit File 83527, Link to Rezoning File 83907, Link to

Cond Use File 83529

Author: Hearing Date:

Entered by: tparks@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	PLAN COMMISS	SION 07/08/2024	RECOMMEND TO COUNCIL TO ADOPT UNDER SUSPENSION OF MGO 2.055 - REPORT OF OFFICER				Pass
	Action Text:	A motion was made by 0 UNDER SUSPENSION vote/other.		•			•
	Notes:	On a motion by Ald. Guequ met and recommended app the Plan Commission mater	roval of the Certified Sur	vey Map subject to the co	mments and conditions co		

Text of Legislative File 83531

Title

Approving a Certified Survey Map of property owned by LZ Ventures, LLC located at 306 N Brooks Street (District 8).

Body

WHEREAS a Certified Survey Map of property owned by LZ Ventures, LLC located at 306 N

Brooks Street, City of Madison, Dane County, Wisconsin has been duly filed for approval by the Plan Commission, its Secretary or their designee, as provided for in Section 16.23(4)(f) of Madison General Ordinances; and

WHEREAS Chapter 236, Wisconsin Statutes requires that the Madison Common Council approve any dedications proposed or required as part of the proposed division of the lands contained on said Certified Survey Map;

NOW THEREFORE BE IT RESOLVED that said Certified Survey Map, bond and subdivision contract, subsequent affidavits of correction, parkland acquisition documents, easement or right-of-way release or procurement documents or any other related document or documents as deemed necessary by the Secretary of the Plan Commission in accordance with the approval of said Certified Survey Map are hereby approved by the Madison Common Council.

BE IT FURTHER RESOLVED that the Mayor and City Clerk of the City of Madison are hereby authorized to sign the above mentioned documents related to this Certified Survey Map.

BE IT FURTHER RESOLVED that all dedications included in this Certified Survey Map or required as a condition of approval of this Certified Survey Map be and are hereby accepted by the City of Madison.

BE IT FURTHER RESOLVED that the Planning Division is authorized to reflect the recorded Certified Survey Map in the Comprehensive Plan and any applicable neighborhood plans.



City of Madison

Demolition, Rezoning, Conditional Use & Certified Survey Map Referral

Location

306 N Brooks Street

Applicant

Duane Johnson, Knothe & Bruce Architects, LLC Kevin Pape, Vierbicher & Assoc. John Leja, LZ Ventures

Requests

- Demolish four-story, 102-unit lodging house
- Rezone from TR-U2 to RMX
- Construct a 15-story, 189-unit apartment building
- Create one lot by CSM

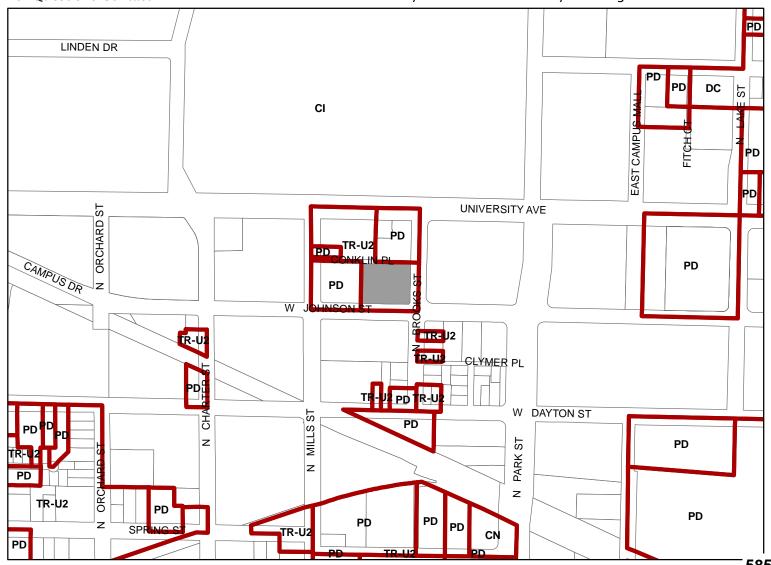
Public Hearing Dates Plan Commission

July 8, 2024

Common Council

July 16, 2024

For Questions Contact: Chris Wells at: 261-9135 or cwells@cityofmadison.com or City Planning at 266-4635



Scale: 1" = 400'

City of Madison, Planning Division : JC : Date : 5-20-585



City of Madison

306 N Brooks Street



Date of Aerial Photography : Spring 2 586

SUBDIVISION APPLICATION

** Please read both pages of the application completely and fill in all required fields **

For a digital copy of this form with fillable fields, please visit:

https://www.cityofmadison.com/sites/default/files/city-of-madison/ development-services-center/documents/SubdivisionApplication.pdf

If you need an interpreter, translator, materials in alternate formats or other accommodations to access these forms, please call the Planning Division at (608) 266-4635.

Si necesita interprete, traductor, materiales en diferentes formatos, u otro tipo de ayuda para acceder a estos formularios, por favor llame al (608) 266-4635.

Yog tias koj xav tau ib tug neeg txhais lus, tus neeg txhais ntawv, los sis xav tau cov ntaub ntawv ua lwm hom ntawv los sis lwm cov kev pab kom paub txog cov lus ghia no, thov hu rau Koog Npaj (Planning Division) (608) 266-4635.

City of Madison

5/13/24 10:00 a.m.

Planning Division

Madison Municipal Building, Suite 017 215 Martin Luther King, Jr. Blvd.

P.O. Box 2985

Madison, WI 53701-2985



(608) 266-4635 NOTICE REGARDING LOBBYING ORDINANCE: If you are seeking approval of a development that has over 40,000 square feet of non-residential space, or a residential development of over 10 dwelling units, or if you are seeking assistance from the City with a value of \$10,000 (including grants, loans, TIF or similar assistance), then you likely are subject to Madison's lobbying ordinance (M.G.O. Sec. 2.40). You are required to register and report your lobbying. Please consult the City Clerk's Office for more information. Failure to comply with the lobbying ordinance may result in fines.

1. A	pplication Type		
	Preliminary Subdivision Plat	Final Subdivision Plat	Land Division/Certified Survey Map (CSM)
I	f a Plat, Proposed Subdivision Name:		

2. Review Fees

- For Preliminary and/or Final Plats, an application fee of \$250, plus \$50 per lot or outlot contained on the plat.
- For Certified Survey Maps, an application fee of \$250 plus \$200 per lot and outlot contained on the CSM.

Make checks payable to "City Treasurer" and mail it to the following address: City of Madison Building Inspection; P.O. Box 2984; Madison, WI 53701-2984. Please include a cover page with the check which includes the project address, brief description of the project, and contact information.

3. Property Owner and Agent Information				
Name of Property Owner:	Representative, if any:			
	City/State/Zip:			
Telephone:	Email:			
Firm Preparing Survey:	Contact:			
Street address: City/State/Zip:				
Telephone:	Email:			
Check only ONE – ALL Correspondence on this application	n should be sent to: Property Owner, OR Survey Firm			
4. Property Information for Properties Located within M	ladison City Limits			
Parcel Addresses:				
Tax Parcel Number(s):				
Zoning District(s) of Proposed Lots: School District:				
Please include a detailed description of the number a	and use of all proposed lots and outlots in your letter of intent.			
4a. Property Information for For Properties Located Outs	side the Madison City Limits in the City's Extraterritorial Jurisdiction:			
Parcel Addresses (note town if located outside City)	:			
Date of Approval by Dane County:	Date of Approval by Town:			
	l letters from both the Town and Dane County must be submitted.			

5. Subdivision Contents and Description. Complete table as it pertains to your request; do not complete gray areas.

Land Use	Lots	Outlots	Acres
Residential			
Retail/Office			
Industrial			

Land Use	Lots	Outlots	Acres
Other (state use):			
Outlots Dedicated to the Public (Parks, Stormwater, etc.)			
Outlots Maintained by a Private Group or Association			
PROJECT TOTALS			

6. Required Submittal Materials

<u>Digital (PDF) copies</u> of all items listed below (if applicable) are required. Applicants are to submit each of these documents as <u>individual PDF files</u> in an e-mail sent to <u>PCapplications@cityofmadison.com</u>. The transmittal shall include the name of the project and applicant. Note that <u>an individual email cannot exceed 20MB</u> and it is the <u>responsibility of the applicant</u> to present files in a manner that can be accepted. Electronic submittals via file hosting services (such as Dropbox) are <u>not</u> allowed. Applicants who are unable to provide the materials electronically should contact the Planning Division at <u>Planning@cityofmadison.com</u> or (608) 266-4635 for assistance.

☑ A Completed Subdivision Application Form (i.e. both sides of this form)

☑ Map Copies (prepared by a Registered Land Surveyor):

- For <u>Preliminary Plats</u>, the drawings must be drawn to scale and are required to provide all information as set forth in <u>M.G.O. Sec. 16.23 (7)(a)</u>.
- For Final Plats, the drawings must be drawn to scale and drawn to the specifications of §236.20, Wis. Stats..
- For <u>Certified Survey Maps (CSMs)</u>, the drawings shall include all of the information set forth in <u>M.G.O. Secs. 16.23 (7)(a)</u> and (d), including existing site conditions, the nature of the proposed division and any other necessary data. Utility data (field located or from utility maps) may be provided on a separate map submitted with application.

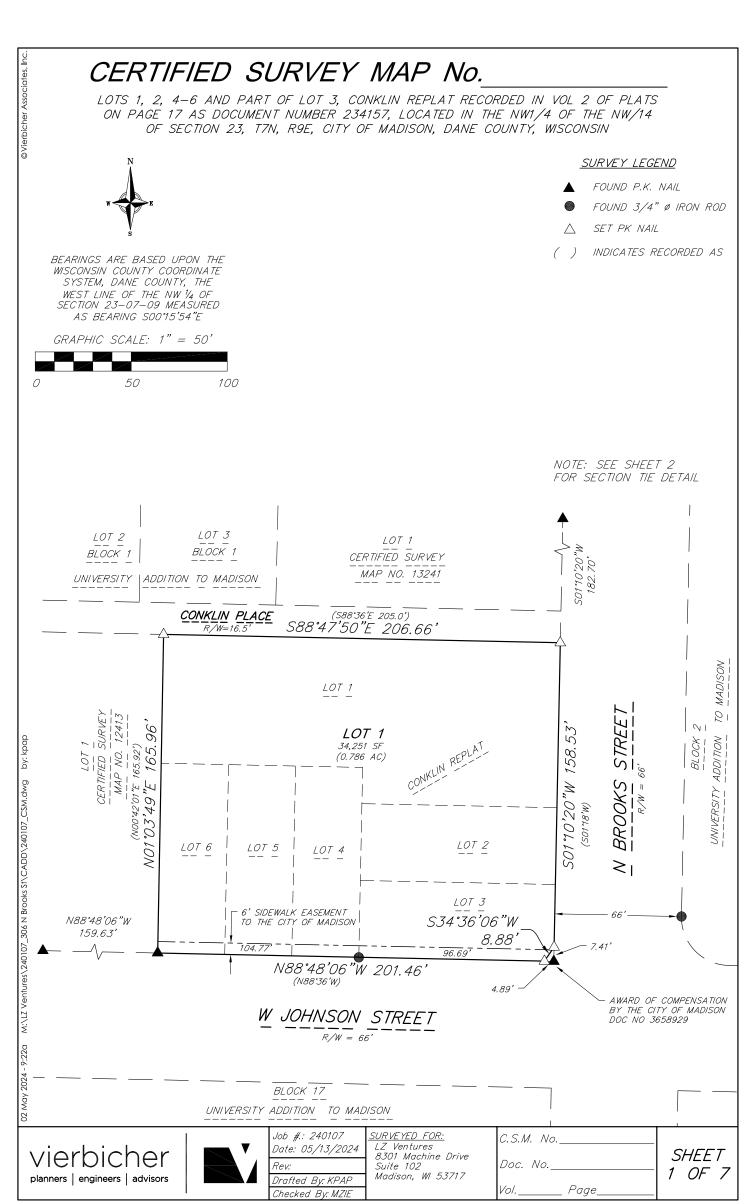
For Plat & CSMs, in addition to the PDF copy, a digital CADD file shall also be submitted in a format compatible with AutoCAD. The digital CADD file(s) shall be referenced to the Dane County Coordinate System and shall contain, at minimum, the list of items stated below, each on a separate layer/level name. The line work shall be void of gaps and overlaps and match the plat, preliminary plat or CSM as submitted: a) Right-of-Way lines (public and private); b) Lot lines; c) Lot numbers; d) Lot/Plat dimensions; e) Street names; f) Easement lines (i.e. all in title and shown on the plat or CSM including wetland & floodplain boundaries.)

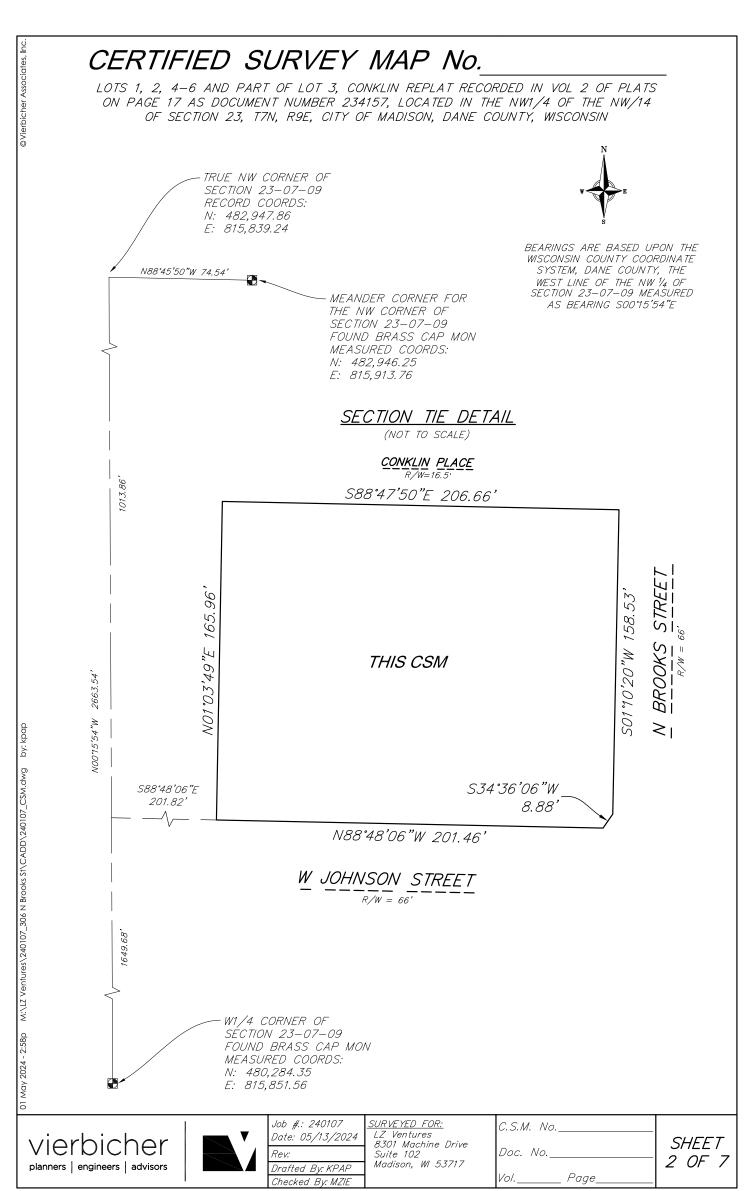
- **Letter of Intent**: One copy of a letter describing the proposed subdivision or land division in detail including, but not limited to:
 - The number and type/use of the lots and outlots proposed with this subdivision or land division, including any outlots to be dedicated to the public;
 - Existing conditions and uses of the property;
 - Phasing schedule for the project, and;
 - The names of persons involved (property owner(s), subdivider, surveyor, civil engineer, etc.).
 - * The letter of intent for a subdivision or land division may be the same as the letter of intent submitted with a concurrent Land Use Application for the same property.
 - ** A letter of intent is not required for Subdivision Applications for lot combinations or split duplexes.
- ☑ **Report of Title and Supporting Documents**: One copy of a City of Madison standard 60-year Report of Title obtained from a title insurance company as required in M.G.O. Sec. 16.23 and as satisfactory to the Office of Real Estate Services. Note:
 - <u>The Report of Title must have been completed within three (3) months of the submittal date of this application. Title insurance or a title commitment policy are NOT acceptable</u> (i.e. a Preliminary Title Report or a Record Information Certificate).
 - The electronic PDF submittal shall include images of the vesting deeds and all documents listed in the Report of Title.
 - <u>Do not email these files to the City's Office of Real Estate Services</u>. Send them instead to the email address noted at the top of this page.

For Surveys Outside the Madison City Limits: One copy of the approval letters from the town where the p	roperty is
located and Dane County shall be submitted with your request. The Plan Commission may not consider an a	pplication
within its extraterritorial jurisdiction without prior approval from the town and Dane County.	

7. Applic	ant Declarations:			
The si	gner attests that the applicat	tion has been completed accurately an	nd all required materials have	e been submitted:
Annlic	ant's Printed Name:	Signat	cure.	170
Дрис			/	19/
Date:	I	nterest In Property On This Date:		

EFFECTIVE: JANUARY 2021 PAGE 2 OF 588





CERTIFIED SURVEY MAP No. LOTS 1, 2, 4-6 AND PART OF LOT 3, CONKLIN REPLAT RECORDED IN VOL 2 OF PLATS ON PAGE 17 AS DOCUMENT NUMBER 234157, LOCATED IN THE NW1/4 OF THE NW/14 OF SECTION 23, T7N, R9E, CITY OF MADISON, DANE COUNTY, WISCONSIN GRAPHIC SCALE: 1" = 50' 50 100 EXISTING CONDITIONS LOT 3 LOT 2 LOT 1 BLOCK 1 BLOCK 1 CERTIFIED SURVEY MAP NO. 13241 UNIVERSITY | ADDITION TO MADISON CONKLIN PLACE BITUMINOUS PAVEMENT MADISON LOT 1 CERTIFIED SURVEY MAP NO. 12 CONCRETE 21 BLOCK ADD/T/ON ру: крар CONCRETE LOT 1 PA VEMENT EXISTING BUILDING TO BE DEMOLISHED M:\LZ Ventures\240107_306 N Brooks St\CADD\240107_CSM.dwg UNIVERSITY BITUMINOUS CONCRETE CONCRETE CONCRETE -L CONCRETE W JOHNSON STREET R/W = 66'May 2024 - 3:14p BLOCK 17 UNIVERSITY ADDITION TO MADISON Job #.: 240107 SURVEYED FOR: C.S.M. No. LZ Ventures 8301 Machine Drive Suite 102 Madison, WI 53717 Date: 05/13/2024 vierbicher SHEET Doc. No. 3 OF 7 planners | engineers | advisors Drafted By: KPAP

Checked By: MZIE

Page

CERTIFIED SURVEY MAP No. LOTS 1, 2, 4-6 AND PART OF LOT 3, CONKLIN REPLAT RECORDED IN VOL 2 OF PLATS ON PAGE 17 AS DOCUMENT NUMBER 234157, LOCATED IN THE NW1/4 OF THE NW/14 OF SECTION 23, T7N, R9E, CITY OF MADISON, DANE COUNTY, WISCONSIN GRAPHIC SCALE: 1" = 50' 50 100 LINE TABLE NUMBER L1 DIRECTION LENGTH NO0°14'48"E 22.75 L2 L3 N89°45'12"W 6.00 500°14'48"W 22.65 <u>L4</u> S88°48'06"E 6.00 EXISTING EASEMENTS LOT 3 LOT 2 LOT 1 BLOCK 1 BLOCK 1 CERTIFIED SURVEY MAP NO. 13241 UNIVERSITY | ADDITION TO MADISON CONKLIN PLACE R/W = 16.5MADISON 1 LOT 1 CERTIFIED SURVEY MAP NO. 12 21 24' WIDE JOINT DRIVEWAY EASEMENT DOC NO 4409641 \prod BLOCK ADD/T/ON M:\LZ Ventures\240107_306 N Brooks St\CADD\240107_CSM.dwg by: kpap LOT 1 - 15' WIDE STORM SEWER EASEMENT DOC NO 2855426 UNIVERSITY 6' WIDE UNDERGROUND ELECTRIC EASEMENT DOC NO 3605277 | |17 97.10' W JOHNSON STREET May 2024 - 3:14p BLOCK 17 UNIVERSITY ADDITION TO MADISON Job #.: 240107 SURVEYED FOR: C.S.M. No. LZ Ventures 8301 Machine Drive Suite 102 Madison, WI 53717 Date: 05/13/2024 vierbicher SHEET

Drafted By: KPAP

Checked By: MZIE

planners | engineers | advisors

4 OF 7

Doc. No.

Page

May 2024 - 3:15p

CERTIFIED SURVEY MAP No.

LOTS 1, 2, 4-6 AND PART OF LOT 3, CONKLIN REPLAT RECORDED IN VOL 2 OF PLATS
ON PAGE 17 AS DOCUMENT NUMBER 234157, LOCATED IN THE NW1/4 OF THE NW/14
OF SECTION 23, T7N, R9E, CITY OF MADISON, DANE COUNTY, WISCONSIN

SURVEYOR'S CERTIFICATE:

I, Kevin J. Pape, Professional Land Surveyor S-2568, do hereby certify that this survey is in full compliance with Chapter 236.34 of the Wisconsin Statutes and the Subdivision Regulations of the City of Madison, Dane County, Wisconsin, and under the direction of the Owners listed hereon, I have surveyed, divided and mapped the land described below and that said map is a correctly dimensioned representation of the exterior boundaries and division of the land surveyed.

Date: May 13, 2024

Signed:

DRAFT

Kevin J. Pape, Professional Land Surveyor S-2568

DESCRIPTION:

Lots 1, 2, 4 thru 6, and part of Lot 3, Conklin Replat, recorded in Volume 2 of Plats on Page 17, as Document Number 234157, located in the NW1/4 of the NW1/4 of Section 23, T7N, R9E, City of Madison, Dane County, Wisconsin, described as follows: Commencing at the west quarter corner of said Section 23; thence N00°15'54"E, 1649.68 feet along the west line of said NW1/4; thence S88°48'06"E, 201.82 feet to the southeast corner of Lot 1, Certified Survey Map Number 12413 and point of beginning; thence N01°03'49"E, 165.96 feet along the east line of said Lot 1 to a point on the south right—of—way line of Conklin Place; thence S88°47'50"E, 206.66 feet along said south right—of—way line to a point on the west right—of—way line of North Brooks Street; thence S01°10'20"W, 158.53 feet along said east right—of—way line; thence S34°36'06"W, 8.88 feet to a point on the north right—of—way line of West Johnson Street; thence N88°48'06"W, 201.46 feet along said north right—of—way line to the point of beginning. Contains 34,251 square feet (0.786 acres).

vierbicher planners | engineers | advisors



Job #.: 240107 Date: 05/13/2024

Rev:
Drafted By: KPAP
Checked By: MZIE

SURVEYED FOR: LZ Ventures 8301 Machine Drive Suite 102 Madison, WI 53717 C.S.M. No._____

Doc. No._____

Vol.____ Page_____

SHEET 5 OF 7

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Job #.: 240107 Date: 05/13/2024 Rev: Drafted By: KPAP <u>SURVEYED FOR:</u> LZ Ventures 8301 Machine Drive Suite 102 Madison, WI 53717 C. S.M. No._____

Doc. No.____

Vol.____ Page_____

SHEET 6 OF 7

CERTIFIED SURVEY MAP No. LOTS 1, 2, 4-6 AND PART OF LOT 3, CONKLIN REPLAT RECORDED IN VOL 2 OF PLATS ON PAGE 17 AS DOCUMENT NUMBER 234157, LOCATED IN THE NW1/4 OF THE NW/14 OF SECTION 23, T7N, R9E, CITY OF MADISON, DANE COUNTY, WISCONSIN MADISON PLAN COMMISSION CERTIFICATE: Approved for recording per the Secretary of the City of Madison Plan Commission. Date: __ Matthew Wachter, Secretary City of Madison Plan Commission CITY OF MADISON COMMON COUNCIL RESOLUTION: Resolved that the Certified Survey Map located in the City of Madison was hereby approved by Enactment Number RES-____, File ID Number ____, adopted on the ___ day of ____, 2024, and that said enactment further provided for the acceptance of those lands dedicated and rights conveyed by said certified survey map to the City of Madison for public use. Dated this _____ day of ___ Maribeth Witzel-Behl, City Clerk, City of Madison REGISTER OF DEEDS CERTIFICATE Received for recording this _____ day of _____, 20__ ____ o'clock ____.m. and recorded in Volume __ M:\LZ Ventures\240107_306 N Brooks St\CADD\240107_CSM.dwg Survey Maps on pages _____, as Doc. No. _ Kristi Chlebowski, Dane County Register of Deeds May 2024 - 3:16p

vierbicher planners | engineers | advisors



Job #.: 240107 Date: 05/13/2024 Rev: Drafted By: KPAP

Checked By: MZIE

<u>SURVEYED FOR:</u> LZ Ventures 8301 Machine Drive Suite 102 Madison, WI 53717 C.S.M. No._____

Doc. No.____

Vol.____ Page_____

SHEET 7 OF 7 Project Addresses: 306 N Brooks Street

Application Type: Demolition Permit, Zoning Map Amendment, Conditional Uses, and

Certified Survey Map Referral

Legistar File ID # 83527, 83907, 83529, and 83531

Prepared By: Chris Wells, Planning Division

Report includes comments from other City agencies, as noted

Reviewed By: Kevin Firchow, Planning Division

Summary

Applicant & John Leja; LZ Ventures; 8301 Machine Drive, Suite 102; Madison, WI 53717

Property Owner:

Contact: Duane Johnson; Knothe & Bruce Architects; 8401 Greenway Blvd., Suite 900; Middleton,

WI 53717

Surveyor: Kevin Pape, Vierbicher & Associates; 999 Fourier Drive, Suite 201; Madison, WI 53717

Requested Actions:

• ID <u>83527</u> – Consideration of a demolition permit to demolish a lodging house at 306 N Brooks Street;

- ID <u>83907</u> Consideration of a request to rezone 306 N Brooks Street from the TR-U2 (Traditional Residential-Urban 2) District to the RMX (Regional Mixed-Use) District;
- ID <u>83529</u> Consideration of a conditional use in the [Proposed] Regional Mixed-Use (RMX) District for a
 building exceeding five stories and 78 feet in height; and Consideration of a conditional use for a vehicle
 parking reduction of more than 20 spaces and 25 percent or more of the required parking, to allow
 construction of a 15-story, 189-unit apartment building.
- ID <u>83531</u> Approval of a Certified Survey Map (CSM) to create one lot for the proposed residential development.

Proposal Summary: The applicant is seeking approvals to construct 15-story, 189-unit apartment building at the northwest corner of W Johnson Street and N Brooks Street following demolition of a four-story lodging house. The underlying lots comprising the subject site will be combined into one lot by CSM. The letter of intent indicates that construction will commence in December of 2025, with completion anticipated in June of 2027.

Applicable Regulations & Standards: Section 28.182 of the Zoning Code provides the process for zoning map amendments. MGO Section 28.069(3)(c) notes that building height exceeding the maximum (i.e. 5 stories or 78 feet) may be allowed in the RMX District with conditional use approval. Additionally, Table 28I-4 in MGO Section 28.141(5) notes that a vehicle parking reduction of more than 20 spaces and 25 percent or more of the required parking requires conditional use approval. Section 28.183 provides the process and standards for the approval of conditional use permits. MGO Section 28.185 provides the process and standards for the approval of demolition and removal permits. The subdivision process is outlined in MGO Section 16.23(4)(f) of the Subdivision Regulations.

Review Required By: Plan Commission and Common Council.



Summary Recommendation: The Planning Division recommends the following to the Plan Commission:

- That the Plan Commission that the standards for demolition permits are met to approve demolition of a lodging house located at 306 N Brooks Street;
- That the Plan Commission forward Zoning Map Amendment ID 28.022-00676, rezoning 306 N Brooks Street from TR-U2 to RMX, to the Common Council with a recommendation of **approval**;
- That the Plan Commission find the standards for conditional uses are met to **approve** a 15-story, 189-unit apartment building, subject to input at the public hearing and the conditions from reviewing agencies beginning on **page 10**; and
- That the Plan Commission forward the Certified Survey Map to remove underlying lot lines at 306 N Brooks
 Street to the Common Council with a recommendation of approval subject to the conditions from
 reviewing agencies beginning on page 19.

Background Information

Parcel Location: The 34,251-square-foot (0.79-acre) parcel is located at the northwest corner of W Johnson Street and N Brooks Street; Alder District 8 (Govindarajan); Madison Metropolitan School District.

Existing Conditions and Land Use: A four-story, roughly 50,000-square-foot lodging house, currently occupied by the community organization Porchlight, containing 102 units (87 single room occupancy rooms (SROs) and 15 efficiency units) with surface parking. The parcel is zoned PD (Planned Development District).

Surrounding Land Uses and Zoning:

North: Across Conklin Place is a nine-story, 79-unit apartment building zoned PD (Planned Development) District and Luther Memorial Church, zoned TR-U2 (Traditional Residential-Urban 2) District;

<u>South</u>: Across W Johnson Street is the University of Wisconsin's roughly 11-story Educational Sciences building in the CI (Campus Institutional) District;

West: The 12-story, 179-unit Grand Central apartment building, zoned PD District; and

<u>East</u>: Across N Brooks Street is University of Wisconsin's five-story Grainger Hall, zoned CI District.

Adopted Land Use Plans: The 2023 Comprehensive Plan recommends Special Institutional (SI) Uses for the subject parcel but provides a Map Note (No. 2) which states, in part: "There are some privately owned properties within the SI-designated areas. If such privately owned parcels redevelop, their use and design should be consistent with adopted sub-area plans, the most relevant of which, as of the adoption of this Plan, is the Regent Street-South Campus Neighborhood Plan. In the rare case where private redevelopment is proposed for an area that is not covered by a sub-area plan, multifamily residential and mixed-use development shall be considered appropriate, so long as the scale, massing, and design of the building fits in with the surrounding context, as determined by the Plan Commission and City Council." Staff note that the Regent Street-South Campus Neighborhood Plan does not extend to the north side of W Johnson Street and no other neighborhood plan provides recommendations for the subject parcel.

Zoning Summary: The subject site will be zoned RMX (Regional Mixed-Use District), which will be reviewed in the following sections.

Requirements	Required	Proposed	
Lot Area (sq. ft.): For exclusive residential use	None	34,083.75 sq. ft.	
Front Yard Setback	None	11.9'	
Max. Front Yard Setback	25'	11.9'	
Side Yard Setback: Other cases	None unless needed for access	11.6' north side	
		12.0' south side	
Rear Yard Setback: For corner lots, where all abutting property is in a nonresidential zoning district	The required rear yard setback shall be the same as the required side yard setback: None unless needed for access	36.4'	
Usable Open Space	None	As shown on proposed plan	
Maximum Lot Coverage	90%	84% (See Comment #47)	
Minimum Building Height	2 stories	15 stories/< Capitol View	
		Preservation limit	
Maximum Building Height	5 stories/78'	15 stories/< Capitol View	
		Preservation limit	

Site Design	Required	Proposed
Number Parking Stalls	Multi-family dwelling: 1 per dwelling	117 garage
	(189)	11 surface
		(128 total)
Electric Vehicle Stalls	13 EV ready	13 EV ready
		3 EV installed
Accessible Stalls	Yes	Yes
Loading	None	Loading zone
Number Bike Parking Stalls	Multi-family dwelling: 1 per unit up to 2-	222 garage
	bedrooms, ½ space per add'l bedroom	64 surface
	(267)	(286 total)
	1 guest space per 10 units (19)	(See Comment #48)
	(286 total)	
Landscaping and Screening	Yes	Yes (See Comment #49)
Lighting	Yes	Yes
Building Form and Design	Yes	Large multi-family building

Other Critical Zoning Items	
Yes:	Utility Easements; Barrier Free (ILHR 69)
No:	Urban Design, Historic District; Floodplain, Wetlands, Wellhead Protection, Adjacent to Park, TOD Overlay

Tables Prepared by Jenny Kirchgatter, Assistant Zoning Administrator

Environmental Corridor Status: The subject site is not located in a mapped environmental corridor.

Public Utilities and Services: The site is currently served by a full range of urban services, including Metro Transit which operates daily all-day transit service along N Brooks Street opposite this property – with trips at least every 60 minutes (every 30 minutes or less during the day on weekdays). Metro Transit operates additional daily all-day transit service along University Avenue, W Johnson Street and S Park Street near this property - with trips on most

routes at least every 30 minutes. Metro Transit would initially estimate the following counts of potentially eligible trips towards US Green Building Council/LEED Quality Access to Transit points: 444 Weekday & 201 Weekend (average). Please contact Metro Transit if additional analysis would be of interest.

Project Description

The applicant is requesting approval of a demolition permit to demolish a four-story, roughly 50,000-square-foot lodging house located on a 0.79-acre parcel at 306 N Brooks Street and to rezone the parcel from TR-U2 to RMX to facilitate redevelopment of the property with a 15-story, 189-unit apartment building. Additionally, a Certified Survey Map (CSM) is proposed to combine the underlying lots comprising the subject parcel into one lot.

The existing building, built in 1952 as the University YMCA, has served for the past 21 years as the Porchlight nonprofit organization's administrative and long-term affordable housing facility. The building is four-stories in height and contains 102 units (87 single room occupancy rooms (SROs) and 15 efficiency units). Seventeen surface automobile stalls are located on the west side of the site and are accessed from W Johnson Street. Photos of the interior and exterior of the building are included in the materials submitted for the demolition permit. Planning Division staff is not aware of structural issues with the building.

After demolishing the subject building, the applicant proposes to construct a 15-story, 189-unit apartment building. Like its predecessor, surface automobile stalls (now 11) are located on the west side of the site and are accessed from W Johnson Street. However, unlike its predecessor, the new building has under building parking which is accessed via a ramp located off of this west drive aisle. The structured parking, which starts on the ground level, spirals down for roughly two levels and contains a total of 117 stalls. It also contains 223 long-term bicycle stalls located in various locations over the two levels.

Staff note that the provision of 128 automobile stalls (117 structured and 11 surface) is 61 stalls (or 32.2-percent) short of the 189 stalls required by the Zoning Code. Table 28I-4 in MGO Section 28.141(5) states that a vehicle parking reduction of more than 20 spaces and 25 percent or more of the required parking requires conditional use approval.

Justifying their provision of parking, the applicant notes in submitted materials, "The project is at the heart of campus and the vast majority of the tenants will be UW students who would be walking or biking to nearby classes. The site is within close proximity to multiple bus stops including BRT route options. In addition, there are many options for bike and pedestrian travel, including established bike paths and routes. Furthermore, the project will be owned and managed by LZ Management, who owns and manages two student housing projects on the same block; Grand Central and X-01 have a combined 211 parking stalls, which have proven over the years to be more than adequate, and many of these are leased to non-students. If viewed cumulatively between all three buildings, the parking ratio would be .78 stalls per unit. Shared parking between all these buildings would be available."

The new building has two entrances, one located along each street-facing façade. Community rooms occupy the southwest and southeast corners of the ground floor while a fitness room occupies the northeast corner. The upper 14 floors contain a total of 189 units – eight studios, 14 one-bed, 56 two-bed, 74 three-bed, 29 four-bed, and 8 five-bedroom units. A small, 724-square-foot lounge and adjacent 271-square-foot terrace, both common amenities, are located on the 15th floor.

The new building is roughly 161 feet in height. (While the submitted drawings show the top of the building mechanicals is roughly seven inches below the Capital View Preservation height limit, Zoning will need to confirm this compliance before final sign off.) As for massing stepbacks, the building mass steps back roughly nine feet on all four sides at the sixth-floor level.

Regarding exterior materials, beige-colored brick is the primary material on the lower 12 floors while a light grey composite metal paneling clads the upper three. Dark grey composite metal paneling is used to cover the floor slabs.

Analysis

Section 28.182 of the Zoning Code provides the process for zoning map amendments. MGO Section 28.069(3)(c) notes that building height exceeding the maximum (i.e. 5 stories or 78 feet) may be allowed in the RMX District with conditional use approval. Additionally, Table 28I-4 in MGO Section 28.141(5) notes that a vehicle parking reduction of more than 20 spaces and 25 percent or more of the required parking requires conditional use approval. Section 28.183 provides the process and standards for the approval of conditional use permits. MGO Section 28.185 provides the process and standards for the approval of demolition and removal permits. The subdivision process is outlined in MGO Section 16.23(4)(f) of the Subdivision Regulations.

Conformance with Adopted Plans

The 2023 <u>Comprehensive Plan</u> recommends Special Institutional (SI) Uses for the subject parcel but provides a Map Note (No. 2) which states, in part: "There are some privately owned properties within the SI-designated areas. If such privately owned parcels redevelop, their use and design should be consistent with adopted sub-area plans, the most relevant of which, as of the adoption of this Plan, is the Regent Street-South Campus Neighborhood Plan. In the rare case where private redevelopment is proposed for an area that is not covered by a sub-area plan, multifamily residential and mixed-use development shall be considered appropriate, so long as the scale, massing, and design of the building fits in with the surrounding context, as determined by the Plan Commission and City Council." Staff note that the <u>Regent Street-South Campus Neighborhood Plan</u> does not extend to the north side of W Johnson Street and no other neighborhood plan provides recommendations for the subject parcel.

As a measure of building scale, staff have used building height as a proxy. The proposed building height is roughly 161 feet, which equates to roughly 1,032 feet above sea level. (The Capital View Preservation height limit is 187.2 feet above City datum or 1,032.8 ft above sea level). For comparison's sake, the applicant team prepared a building height survey of the surrounding parcels (see Image 1 below).

North: Across Conklin Place is X01 Apartments, a nine-story, 79-unit apartment building, whose height reaches up to roughly 1,013 feet and Luther Memorial Church, which rises up to the roughly 972-foot mark;

South: Across W Johnson
Street is the University of
Wisconsin's roughly 11story Educational
Sciences building, which
reaches up to the roughly
1,029-foot mark;

West: The 12-story, 179unit, Grand Central

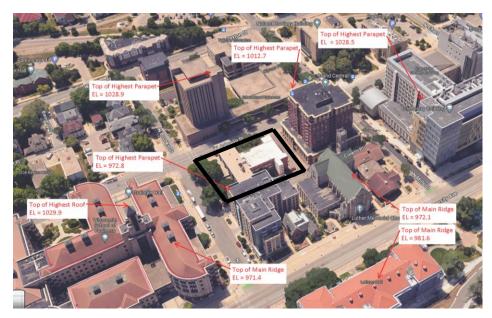


Image 1: Building Heights Surrounding the Subject Site. (Source: Vierbicher Associates)

apartment building, which reaches up to the 1,013-foot mark, beyond which is the University of Wisconsin's Chemistry Building, which reaches up to the roughly 1,028-foot mark; and

<u>East</u>: Across N Brooks Street is University of Wisconsin's five-story Grainger Hall whose roof ridge reaches up to the 971-foot mark but whose tower spire rises to the 1,029-foot height mark.

This means that there are three buildings located in the surrounding block (four if you count the Grainger Hall spire) that rise up to at least the 1,012-foot mark (or within roughly 20 feet of the proposed building), two of which rise up to less than five feet of the proposed building. Furthermore, the proposed building's mass and stepbacks closely resembles that of the adjacent Grand Central apartment building to the west which also has a roughly five-story base before stepping back on all sides.

It is for these reasons that Staff believe the proposed building could be found 'appropriate' and that its scale, massing, and design fit in with the surrounding context. Therefore, Staff believe it is consistent with adopted plan recommendations.

Demolition Permit Standards

At its October 5, 2021 meeting, the Common Council adopted Ordinance 21-00065 (ID 67074) to revise the standards and process for demolition and removal permits in Section 28.185 of the Zoning Code to remove consideration of proposed future use as a factor in approving demolition requests. The Plan Commission shall consider the seven approval standards in §28.185(9)(c) M.G.O. when making a determination regarding demolition requests and find them met in order to approve. The factors include a finding that the proposed demolition or removal is consistent with the statement of purpose of the demolition permits section and with the health, prosperity, safety, and welfare of the City of Madison. The statement of purpose in Section 28.185 states, in part:

"The purpose of this section is therefore to ensure the preservation of historic buildings, encourage applicants to strongly consider relocating rather than demolishing existing buildings, aid in the implementation of adopted City plans, maximize the reuse or recycling of materials resulting from a demolition, protect the public from potentially unsafe structures and public nuisances..."

On balance, staff believes that standards can be found met and provides the following additional information.

In regards to Standard 1 related to the possible building relocation efforts, the Plan Commission is required to consider any information provided related to possible relocation activities. The applicant noted in submitted materials that relocation was considered but was deemed not a "good prospect for relocation."

In regards to Standard 6 related to the condition of the buildings proposed for demolition, Planning Division staff have not received any comments from City Building Inspection, Fire, or Police. In the submitted information, the applicant provides photos of the existing structure and notes that the building has become "outdated and obsolete and has become a financial and maintenance burden to its users. The facility has not seen significant updates for many years: it has its original windows, insulation, and mechanical systems and is very inefficient. In addition, the building does not have a fire sprinkler system, which is unsafe and no longer allowed for a building of this type." However, from the submitted information, staff is not aware of significant structural issues. While the code notes condition must be considered, it does not specify that a certain structural condition or level of disrepair is necessary in order to approve a demolition.

Related to the historic value of the structures, staff notes Standard 4 states:

"The Plan Commission has received and considered the report of the City's historic preservation planner regarding the historic value of the property as well as any report that may be submitted by the Landmarks Commission."

The Landmarks Commission reviewed the demolition of the building at its May 6, 2024 meeting, and at that meeting found that the building at 306 N Brooks Street "has historic and cultural value related to its historic association with the Women's Rights Movement and Gay Liberation Movement."

Regarding the historic and cultural significance the existing structure has had, the applicant notes the following in submitted materials,

"The building is not on the National or State Register of Historic Places; however, there was a survey conducted in 2019 called the "Wisconsin Underrepresented Communities Historic Resource Survey Report" which highlighted 306 N. Brooks Street as the Women's Center of the University YMCA from the 1970's-1990's and housing the Lesbian Switchboard beginning in 1974. Other possibly significant events in this building include the Teachers Assistance Association office. We would like to honor the history of this building by highlighting the cultural significance this property has with the Women's Movement, Gay Liberation Movement, Teacher's Assistance Association, and more recently, Porchlight. This would be done by creating a legacy history wall or exhibit that visually shows this history in a creative and engaging way." The applicant further clarified, "The legacy wall will be on the interior [of the building]. We've hired Legacy Architects to do additional research to holistically understand the history of the site and be able to tell that story in an engaging way within the public space of the building."

Standard 7, which includes references the standards 1-6 states that "The Plan Commission shall consider the factors and information specified in items 1-6 and find that the proposed demolition or removal is consistent with the statement of purpose of this section and with the health, prosperity, safety, and welfare of the City of Madison."

Given the condition of the structure, the lack of a fire sprinkler system, the legacy history wall that would be created within the new building, and the fact the Porchlight itself of supportive of the demolition (due to the fact that it would potentially relocate to a new 70-units mixed-use building at 521 E Washington Avenue), Staff believe the Demolition Permit standards can be found met subject to input at the public hearing and comments from reviewing agencies.

Zoning Map Amendment Standards

Staff believes that the Zoning Map Amendment standards can be found met. These standards state that such amendments are legislative decisions of the Common Council that shall be based on public health, safety and welfare, shall be consistent with the Comprehensive Plan, and shall comply with Wisconsin and federal law. Chapter 66.1001(3) of Wisconsin Statutes requires that zoning ordinances (of which the zoning map is part) enacted or amended after January 1, 2010 be consistent with the City's Comprehensive Plan. 2010 Wisconsin Act 372 clarified "consistent with" as "furthers or does not contradict the objectives, goals and policies contained in the comprehensive plan." As noted above, Staff believes that the project is consistent with the Comprehensive Plan.

Conditional Use Standards

The Conditional Use approval standards state that the Plan Commission shall not approve a conditional use without due consideration of the City's adopted plan recommendations, design guidelines, supplemental regulations, and finding that all of the conditional use standards of MGO §28.183(6) are met. Please note, recent changes to state law requires that conditional use findings must be based on "substantial evidence" that directly pertains to each standard and not based on personal preference or speculation. Staff provides additional discussion on the following standards.

Standard 5 states, "Adequate utilities, access roads, drainage, internal circulation improvements for pedestrians, bicyclists, public transit and vehicles, parking supply (in cases with minimum parking requirements) and other necessary site improvements have been or are being provided." As noted previously in this report, the applicant is providing just 128 of the required 189 automobile parking stalls; a reduction which requires a conditional use approval. The applicant has provided justification for why they believe this is warranted including the vast majority of tenants being students, the proximity to campus, and the wealth of nearby transportation options such as bicycle, car share, and Metro's BRT.

Standard 11 states, "When applying the above standards to an application for height in excess of that allowed in the district, the Plan Commission shall consider recommendations in adopted plans; the impact on surrounding properties, including height, mass, orientation, shadows and view; architectural quality and amenities; the relationship of the proposed building(s) with adjoining streets, alleys, and public rights of ways; and the public interest in exceeding the district height limits."

As described above, Staff believe the proposed building's scale, massing, and design fit in with the surrounding context.

On balance, staff believes that the Conditional Use Standards can be found met subject to input at the public hearing and comments from reviewing agencies.

Land Division

The applicant has submitted a one-lot Certified Survey Map to combine the underlying lots that comprise the subject parcel. M.G.O. Section 16.23(5)(g) provides the process for certified survey maps. Staff believe that all applicable standards for land divisions can be found met, subject to the comments recommended by reviewing agencies.

Public Input

At time of writing, Staff have not received any public comments.

Conclusion

The applicant is requesting approval of a zoning map amendment to rezone the 0.79-acre parcel from TR-U2 to RMX and conditional use approvals to facilitate redevelopment of the site with a 15-story, 189-unit apartment building following demolition of a lodging house.

The Planning Division believes that the Plan Commission may find that the proposed development meets the various standards for approval. Regarding the height and mass, Staff note that there are two other buildings in the immediate area that are within five feet of the proposed building's absolute height of 1,032 feet above sea level. Furthermore, Staff note the proposed building's mass and stepbacks closely resembles that of the adjacent Grand Central apartment building to the west which also has a roughly five-story base before stepping back on all sides. For these reasons, Staff believe the proposed building's scale, massing, and design fit in with the surrounding context and therefore believe it is consistent with adopted plan recommendations.

Recommendation

Planning Division Recommendation (Contact Chris Wells, (608) 261-9135)

The Planning Division recommends the following to the Plan Commission:

- That the Plan Commission that the standards for demolition permits are met to approve demolition of a lodging house located at 306 N Brooks Street;
- That the Plan Commission forward Zoning Map Amendment ID 28.022-00676, rezoning 306 N Brooks Street from TR-U2 to RMX, to the Common Council with a recommendation of approval;
- That the Plan Commission find the standards for conditional uses are met to approve a 15-story, 189-unit
 apartment building, subject to input at the public hearing and the conditions from reviewing agencies that
 follow; and
- That the Plan Commission forward the Certified Survey Map to remove underlying lot lines at 306 N Brooks
 Street to the Common Council with a recommendation of approval subject to the conditions from
 reviewing agencies beginning on page 19.

Recommended Conditions of Approval Major/Non-Standard Conditions are Shaded

Planning Division (Contact Chris Wells, (608) 261-9135)

Prior to final sign off and the issuance of permits, the applicant shall submit a management plan on the form provided by the Zoning Administrator. The management plan shall be approved by the Planning Division Director and Zoning Administrator (or their designees) prior to final approval.

City Engineering Division (Contact Tim Troester, (608) 267-1995)

- 2. The City has limited sanitary sewer capacity in this area. Applicant shall submit projected wastewater flow calculations for the proposed development. Applicant may be required to build offsite sanitary sewer improvements as a condition of plan approval with a developer's agreement with the City. Applicant shall submit projected wastewater calculations to Mark Moder, mmoder@cityofmadison.com.
- 3. Enter into a City / Developer agreement for the required infrastructure improvements. Agreement to be executed prior to sign off. Allow 4-6 weeks to obtain agreement. Contact City Engineering to schedule the development and approval of the plans and the agreement. (MGO 16.23(9)c)
- 4. Construct sidewalk, terrace, curb and gutter and street and alley pavement to a plan as approved by City Engineer
- 5. Madison Metropolitan Sewerage District (MMSD) charges are due and payable prior to Engineering sign-off, unless otherwise collected with a Developer's / Subdivision Contract. Contact Mark Moder (608-261-9250) to obtain the final MMSD billing a minimum of two (2) working days prior to requesting City Engineering signoff. (MGO 16.23(9)(d)(4)
- 6. Obtain a permanent sewer plug permit for each existing sanitary sewer lateral serving a property that is not to be reused and a temporary sewer plug permit for each sewer lateral that is to be reused by the development. available online The procedures and fee schedule is http://www.cityofmadison.com/engineering/permits.cfm. (MGO CH 35.02(14))
- 7. An Erosion Control Permit is required for this project. See Storm comments for permit specific details and requirements.
- 8. A Storm Water Management Report and Storm Water Management Permit is required for this project. See Storm comments for report and permit specific details and requirements.
- 9. This site appears to disturb less than one (1) acre of land. No submittal to the WDNR, CARPC or Department of Safety and Professional Services (DSPS) is required as the City of Madison Building Inspection Department is an approved agent for DSPS.
- 10. Revise the plans to show a proposed private internal drainage system on the site. Include the depths and locations of structures and the type of pipe to be used. (POLICY AND MGO 10.29)

- 11. Revise the plans to identify the location of the public storm sewer (proposed or existing) that will serve the development show the connection of the private internal drainage system to the public storm sewer. (POLICY AND MGO OVER 10,000 SF OF IMPERVIOUS AREA 10.29 and 37.05(7)(b))
- 12. The proposed development proposes to construct underground parking. The proposed entrance to the underground parking is adjacent to a street low point. The applicant shall provide at a minimum of one (1) foot of rise from the adjacent back of walk in the driveway before breaking grade to the down ramp to the underground parking to protect the underground parking from inundation. The stated elevation is intended to be protective but does not guarantee a flood proof structure. The Developer/Owner are strongly encouraged to complete their own calculations and determine an elevation that protects their property to a level of service that they are comfortable with.
- 13. Provide additional detail how the enclosed depression(s) created by the parking entrance(s) to the below building parking area(s) is/are served for drainage purposes. The building must be protected from receiving runoff up through the 100-yr design storm that is current in Madison General Ordinance Chapter 37. If the enclosed depression(s) is/are to be served by a gravity system provide calculations stamped by a Wisconsin P.E. that show inlet and pipe capacities meet this requirement. If the enclosed depression(s) is/are to be served by a pump system provide pump sizing calculations stamped by a Wisconsin P.E. or licensed Plumber that show this requirement has been met.
- 14. This project will disturb 20,000 sf or more of land area and require an Erosion Control Plan. Please submit an 11" x 17" copy of an erosion control plan (pdf electronic copy preferred) to Megan Eberhardt (west) at meberhardt@cityofmadison.com, or Daniel Olivares (east) at daolivares@cityofmadison.com, for approval. Demonstrate compliance with Section 37.07 and 37.08 of the Madison General Ordinances regarding permissible soil loss rates. Include Universal Soil Loss Equation (USLE) computations for the construction period with the erosion control plan. Measures shall be implemented in order to maintain a soil loss rate below 5.0 tons per acre per year. The WDNR provided workbook to compute USLE rates can be found online at https://dnr.wi.gov/topic/stormwater/publications.html

This project will require a concrete management plan and a construction dewatering plan as part of the erosion control plan to be reviewed and approved by the City Engineer's Office. If contaminated soil or groundwater conditions exist on or adjacent to this project additional WDNR, Public Health, and/or City Engineering approvals may be required prior to the issuance of the required Erosion Control Permit. (POLICY)

This project appears to require fire system testing that can result in significant amounts of water to be discharged to the project grade. The Contractor shall coordinate this testing with the erosion control measures and notify City Engineering 608-266-4751 prior to completing the test to document that appropriate measures have been taken to prevent erosion as a result of this testing.

Complete weekly self-inspection of the erosion control practices and post these inspections to the City of Madison website - as required by Chapter 37 of the Madison General Ordinances.

15. Prior to approval, this project shall comply with Chapter 37 of the Madison General Ordinances regarding stormwater management. Specifically, this development is required to submit a Storm Water Management Permit application, associated permit fee, Stormwater Management Plan, and Storm Water Management Report to City Engineering. The Stormwater Management Permit application can be found on City Engineering's website at http://www.cityofmadison.com/engineering/Permits.cfm

The Storm Water Management Plan & Report shall include compliance with the following:

Report: Submit prior to plan sign-off, a stormwater management report stamped by a P.E. registered in the State of Wisconsin.

Electronic Data Files: Provide electronic copies of any stormwater management modeling or data files including SLAMM, RECARGA, TR-55, HYDROCAD, Sediment loading calculations, or any other electronic modeling or data files. If calculations are done by hand or are not available electronically, the hand copies or printed output shall be scanned to a PDF file and provided to City Engineering. (POLICY and MGO 37.09(2))

Rate Control Redevelopment: By design detain the 10-year post construction design storm such that the peak discharge during this event is reduced 15% compared to the peak discharge from the 10-year design storm in the existing condition of the site. Further, the volumetric discharge leaving the post development site in the 10- year storm event shall be reduced by 5% compared to the volumetric discharge from the site in an existing condition during the 10-year storm event. These required rate and volume reductions shall be completed, using green infrastructure that captures at least the first 1/2 inch of rainfall over the total site impervious area. If additional stormwater controls are necessary beyond the first 1/2 inch of rainfall, either green or non-green infrastructure may be used.

TSS Redevelopment with TMDL: Reduce TSS by 80% off of the proposed development when compared with the existing site.

Submit a draft Stormwater Management Maintenance Agreement (SWMA) for review and approval that covers inspection and maintenance requirements for any BMP used to meet stormwater management requirements on this project.

16. Submit, prior to plan sign-off but after all revisions have been completed, digital PDF files to the Engineering Division. Email PDF file transmissions are preferred to: bstanley@cityofmadison.com (East) or ttroester@cityofmadison.com (West).

City Engineering – Mapping (Contact Julius Smith, (608) 264-9276)

- 17. Applicant shall dedicate Right-of-Way or Grant a Public Sidewalk Easement to the City along N Brooks St. and E. Johnson St. on the face of the concurrent Certified Survey Map. The final width and location of the easement to be approved by Engineering and Traffic Engineering
- 18. Coordinate and request from the utility companies serving this area the easements required to serve this development. Those easements shall be properly shown, dimensioned and labeled on the final plat.
- 19. The Owner on the application is not the current owner of record for the property. Provide the recorded conveyance of the parcel to the Owner noted on the application prior to final site plan sign off.
- 20. Proposed development conflicts with existing underground electric easement. Release MGE Easement per Document no. 3605277 to make way for proposed improvements. provide the recorded release prior to site plan approval.
- 21. Show the 12' of the existing driveway easement per document nos. 4403378 and 4409641 and 15' storm easement per doc 2815108 on sheets C 300 and C400.

- 22. Note drive area appears smaller than 12 feet and parking area as shown in document no. 44009641 while be altered with this document. Amend Document nos. 4403378 and 44009641 as necessary and provide to after recording and prior to site plan approval.
- 23. The address of 306 N Brooks St will be inactivated and archived with the demolition of the building. The address of the proposed apartment building is 1008 W Johnson St. The site plan shall reflect a proper street address of the property as reflected by official City of Madison Assessor's and Engineering Division records.
- 24. The pending Certified Survey Map application for this property shall be completed and recorded with the Dane County Register of Deeds (ROD), the new parcel data created by the Assessor's Office and the parcel data available to zoning and building inspection staff prior to issuance of building permits for new construction.
- 25. Submit a site plan and a complete building Floor Plan in PDF format to Lori Zenchenko (<u>Izenchenko@cityofmadison.com</u>) that includes a floor plan of each floor level on a separate sheet/page for the development of a complete interior addressing plan. Also, include a unit matrix for apartment buildings.

The Addressing Plan for the entire project shall be finalized and approved by Engineering (with consultation and consent from the Fire Marshal if needed) PRIOR to the verification submittal stage of this LNDUSE with Zoning. The final approved Addressing Plan shall be included in said Site Plan Verification application materials or a revised plan shall provided for additional review and approval by Engineering.

Per 34.505 MGO, a full copy of the approved addressing plan shall be kept at the building site at all times during construction until final inspection by the Madison Fire Department.

For any changes pertaining to the location, deletion or addition of a unit, or to the location of a unit entrance, (before, during, or after construction), a revised Address Plan shall be resubmitted to Lori Zenchenko to review addresses that may need to be changed and/or reapproved.

Traffic Engineering Division (Contact Luke Peters, (608) 266-6543)

- 26. The applicant shall dedicate Right of Way or grant a Public Sidewalk Easement for and be responsible for the construction of a minimum eight (8)-foot wide sidewalk, six (6)-foot terrace, and additional one (1) foot for maintenance, where applicable, along their site's frontage of East Johnson Street.
- 27. The applicant shall dedicate Right of Way or grant a Public Sidewalk Easement for and be responsible for the construction of a minimum five (5)-foot wide sidewalk, eight (8)-foot terrace, and additional one (1) foot for maintenance, where applicable, along their site's frontage of North Brooks Street.
- 28. The applicant shall submit one contiguous plan showing proposed conditions and one contiguous plan showing existing conditions for approval. The plan drawing shall be scaled to 1" = 20' and include the following, when applicable: existing and proposed property lines; parcel addresses; all easements; pavement markings; signing; building placement; items in the terrace such as signs, street light poles, hydrants; surface types such as asphalt, concrete, grass, sidewalk; driveway approaches, including those adjacent to and across street from the project lot location; parking stall dimensions, including two (2) feet of vehicle overhang; drive aisle dimensions; semitrailer movement and vehicle routes; dimensions of radii; and percent of slope.

- 29. The developer shall post a security deposit prior to the start of development. In the event that modifications need to be made to any City owned and/or maintained traffic signals, street lighting, signing, pavement marking and conduit/handholes, the Developer shall reimburse the City for all associated costs including engineering, labor and materials for both temporary and permanent installations.
- 30. The City Traffic Engineer may require public signing and marking related to the development; the developer shall be financially responsible for such signing and marking.
- 31. All parking facility design shall conform to the standards in MGO Section 10.08(6).
- 32. All bicycle parking adjacent pedestrian walkways shall have a two (2)-foot buffer zone to accommodate irregularly parked bicycles and/or bicycletrailers.
- 33. All pedestrian walkways adjacent parking stalls shall be 7 feet wide to accommodate vehicle overhang, signage and impediments to walkway movements. Any request for variance shall be submitted to and reviewed by City Traffic Engineering.
- 34. Per MGO Section 12.138 (14), this project is not eligible for residential parking permits. It is recommended that this prohibition be noted in the leases for the residential units.
- 35. The applicant shall adhere to all vision triangle requirements as set in MGO Section 27.05 (no visual obstructions between the heights of 30 inches and 10 feet at a distance of 25 feet behind the property line at streets and 10 feet at driveways.). Alteration necessary to achieve compliance may include but are not limited to; substitution to transparent materials, removing sections of the structure and modifying or removing landscaping elements. If applicant believes public safety can be maintained they shall apply for a reduction of MGO Section 27.05(2)(bb), Vision Clearance Triangles at Intersections Corners. Approval or denial of the reduction shall be the determination of the City Traffic Engineer.
- 36. All parking ramps as the approach the public Right-of-Way shall not have a slope to exceed 5% for 20 feet; this is to ensure drivers have adequate vision of the Right-of-Way. If applicant believes public safety can be maintained they shall apply for a waiver, approval or denial of the waiver shall be the determination of the City Traffic Engineer.
- 37. City of Madison radio systems are microwave directional line of sight to remote towers citywide. The building elevation will need to be reviewed by Traffic Engineering to accommodate the microwave sight and building. The applicant shall submit grade and elevations plans if the building exceeds three stories prior to sign-off to be reviewed and approved by Andrew Oliver, ((608) 267-1979, aoliver@cityofmadison.com) Traffic Engineering Shop, 4151 Nakoosa Trail. The applicant shall return one signed approved building elevation copy to the City of Madison Traffic Engineering office with final plans for sign-off.
- 38. The driveway slope to the underground parking is not identified in the plan set, Traffic Engineering recommends driveway slope under 10%; if the slope is to exceed 10%, the applicant shall demonstrate inclement weather mitigation techniques to provide safe ingress/egress to be approved by the City Traffic Engineer.

- 39. The applicant shall provide a clearly defined 5' walkway clear of all obstructions to assist citizens with disabilities, especially those who use a wheel chair or are visually impaired. Obstructions include but are not limited to tree grates, planters, benches, parked vehicle overhang, signage and doors that swing outward into walkway.
- 40. "Stop" signs shall be installed at a height of seven (7) feet from the bottom of the sign at all class III driveway approaches, including existing driveways, behind the property line and noted on the plan. All directional/regulatory signage and pavement markings on the site shall be shown and noted on the plan.
- 41. The applicant shall show the dimensions for the proposed class III driveway including the width of the drive entrance, width of the flares, and width of the curb cut.
- 42. All existing driveway approaches on which are to be abandoned shall be removed and replaced with curb and gutter and noted on the plan.
- 43. Applicant shall submit for review a waste removal plan. This shall include vehicular turning movements.
- 44. Note: This site presents difficult constructability issues; access to neighboring sites must be maintained at all times, protected walkways will be constructed and maintained as soon as possible and little to no access to the Public Right-of-Way on East Johnson Street will be granted for construction purposes. Provide a detailed construction plan to Traffic Engineering for review prior to final signoff.

Parking Division (Contact Trent W. Schultz, (608) 246-5806)

45. The applicant shall submit a Transportation Demand Management (TDM) Plan to tdm@cityofmadison.com. The TDM Plan is required per MGO Section 16.03. Applicable fees will be assessed after the TDM Plan is reviewed by staff.

Zoning Administrator (Contact Jenny Kirchgatter, (608) 266-4429)

- 46. Section 28.185(9)(b) requires that every applicant for a demolition or removal approval that requires approval by the Plan Commission is required to get a Reuse and Recycling Plan approved by the City Recycling Coordinator, Bryan Johnson at streets@cityofmadison.com prior to receiving a raze permit. Every person who is required to submit a reuse and recycling plan pursuant to Section 28.185(9)(b) shall submit documents showing compliance with the plan within sixty (60) days of completion of demolition. A demolition or removal permit is valid for two (2) years from the date of the Plan Commission approval.
- 47. Provide a calculation and plan detail for lot coverage with the final submittal. The lot coverage maximum is 90%. Lot coverage is defined as the total area of all buildings, measured at grade, all accessory structures including pools, patios, etc., and all paved areas as a percentage of the total area of the lot, with the following exceptions: sidewalks or paved paths no wider than five (5) feet, pervious pavement, green roofs and decks. It appears that some 7' wide sidewalk areas may have been excluded from the lot coverage calculation. Provide details of any pervious pavement areas that are used.

- 48. Bicycle parking for the residential dwelling units shall comply with the requirements of MGO Sections 28.141(4)(g) and 28.141(11) and shall be designated as short-term or long-term bicycle parking. A minimum of 267 resident bicycle stalls are required plus a minimum of 19 short-term guest stalls. A minimum of 90% of the resident stalls (240 stalls) shall be designed as long-term parking. Up to twenty-five percent (25%) of bicycle parking may be structured parking, vertical parking or wall mount parking, provided there is a five (5) foot access aisle for wall mount parking. NOTE: A bicycle stall is a minimum of two (2) feet by six (6) feet with a five (5) foot wide access area. Submit a detail showing the model of bike rack to be installed.
- 49. Submit the landscape plan and landscape worksheet stamped by the registered landscape architect. Per Section 28.142(3) Landscape Plan and Design Standards, landscape plans for zoning lots greater than ten thousand (10,000) square feet in size must be prepared by a registered landscape architect.
- 50. Per Section 28.186(4)(b), the property owner or operator is required to bring the property into compliance with all elements of the approved site plans by the date established by the Zoning Administrator as part of the site and building plan approval. Work with Zoning staff to establish a final site compliance date.
- 51. Signage approvals are not granted by the Plan Commission. Signage must be reviewed for compliance with Chapter 31 Sign Codes of the Madison General Ordinances. Signage permits are issued by the Zoning Section of the Department of Planning and Community and Economic Development.

Fire Department (Contact Matt Hamilton, (608) 266-4457)

- 52. Fire command center to be visible from vestibule entry. Relocate door to be visible from entry.
- 53. Provide fire service access elevators
- 54. NFPA 14 requires multiple FDCs remotely located from each other. MFD anticipates that FDCs will be located along the Brooks St and Johnson St elevations.
- 55. Please consider allowing Madison Fire Dept. to conduct training sequences prior to demolition. Contact MFD Training Division to discuss possibilities: Division Chief Ron Blumer at rblumer@cityofmadison.com or (608) 266-5959.

Parks Division (Contact Ann Freiwald, (608) 243-2848)

56. Park Impact Fees (comprised of the Park Infrastructure Impact Fee, per MGO Sec. 20.08(2)), and Park-Land Impact Fees, per MGO Sec. 16.23(8)(f) and 20.08(2) will be required for all new residential development associated with this project. This development is within the West Park-Infrastructure Impact Fee district. Please reference ID# 24032 when contacting Parks Division staff about this project.

Water Utility (Contact Jeff Belshaw, (608) 261-9835)

57. A Water Service Application Form and fees must be submitted before connecting to the existing water system. Provide at least two working days' notice between the application submittal and the requested installation or inspection appointment. Application materials are available on the Water Utility's Plumbers & Contractors website (http://www.cityofmadison.com/water/plumbers-contractors); otherwise, they may be obtained

from the Water Utility Main Office at 119 E Olin Avenue. A licensed plumber signature is required on all water service applications. For new or replacement services, the property owner or authorized agent is also required to sign the application. A Water Meter Application Form will subsequently be required to size & obtain a water meter establish a Water Utility customer account and/or establish a Water Utility fire service account. If you have questions regarding water service applications, please contact Madison Water Utility at (608) 266-4646.

58. The Madison Water Utility shall be notified to remove the water meter at least two working days prior to demolition. Contact the Water Utility Meter Department at (608) 266-4765 to schedule the meter removal appointment.

Forestry Section (Contact Bradley Hofmann, (608) 267-4908)

- 59. An existing inventory of street trees located within the right of way shall be included on the site, demo, utility, landscape, grading, fire aerial apparatus and street tree plan sets. The inventory shall include the following: location, size (diameter at 4 1/2 feet), and species of existing street trees. The inventory should also note if a street tree is proposed to be removed and the reason for removal.
- 60. All proposed street tree removals within the right of way shall be reviewed by City Forestry before the Plan Commission meeting. Street tree removals require approval and a tree removal permit issued by City Forestry. Any street tree removals requested after the development plan is approved by the Plan Commission or the Board of Public Works and City Forestry will require a minimum of a 72-hour review period which shall include the notification of the alderperson within who's district is affected by the street tree removal(s) prior to a tree removal permit being issued. Add as a note on the street tree plan set.
- 61. Contractor shall take precautions during construction to not disfigure, scar, or impair the health of any street tree. Contractor shall operate equipment in a manner as to not damage the branches of the street tree(s). This may require using smaller equipment and loading and unloading materials in a designated space away from trees on the construction site. Any damage or injury to existing street trees (either above or below ground) shall be reported immediately to City Forestry at (608) 266-4816. Penalties and remediation shall be required. Add as a note on the site, grading, utility, demolition, and street tree plan set.
- 62. As defined by the Section 107.13 of City of Madison Standard Specifications for Public Works Construction: No excavation is permitted within 5 feet of the trunk of the street tree or when cutting roots over 3 inches in diameter. If excavation is necessary, the Contractor shall contact Madison City Forestry at (608) 266-4816 prior to excavation. City of Madison Forestry personnel shall assess the impact to the tree and to its root system prior to work commencing. Tree protection specifications can be found on the following website: https://www.cityofmadison.com/business/pw/specs.cfm Add as a note on the site, grading, utility, demolition and street tree plan sets.
- 63. Section 107.13(g) of City of Madison Standard Specifications for Public Works Construction (website: https://www.cityofmadison.com/business/pw/specs.cfm) addresses soil compaction near street trees and shall be followed by Contractor. The storage of parked vehicles, construction equipment, building materials, refuse, excavated spoils or dumping of poisonous materials on or around trees and roots within five (5) feet of the tree or within the protection zone is prohibited. Add as a note on both the site and street tree plan sets.

- 64. On this project, street tree protection zone fencing is required. The fencing shall be erected before the demolition, grading or construction begins. The fence shall include the entire width of terrace and, extend at least 5 feet on both sides of the outside edge of the tree trunk. Do not remove the fencing to allow for deliveries or equipment access through the tree protection zone. Add as a note on both the site and street tree plan sets.
- 65. Street tree pruning shall be coordinated with City Forestry at a minimum of two weeks prior to the start of construction for this project. Contact City Forestry at (608)266-4816. All pruning shall follow the American National Standards Institute (ANSI) A300 Part 1 Standards for pruning. Add as a note on both the site and street plan sets.
- 66. The Developer shall submit a Street Tree Report performed by International Society of Arboriculture Certified Arborist for City Forestry's review of project. This report shall identify all street trees on proposed project site, species type, canopy spread, tree condition, proposed tree removals, the impacts of proposed construction, and any requested pruning.
- 67. The Developer shall post a security deposit prior to the start of the development to be collected by City Engineering as part of the Developers Agreement. In the event that street trees are damaged during the construction process, City Forestry will draw from this deposit for damages incurred.
- 68. Additional street trees are needed for this project. Tree planting specifications can be found in section 209 of **Specifications** City of Madison Standard for Public Works Construction (website: https://www.cityofmadison.com/business/pw/specs.cfm) - All street tree planting locations and tree species within the right of way shall be determined by City Forestry. A landscape plan and street tree planting plan shall be submitted in PDF format to City Forestry for approval of planting locations within the right of way and tree species. All available street tree planting locations shall be planted within the project boundaries. Add following note on both the landscape and street tree plan sets: At least one week prior to street tree planting, Contractor shall contact City Forestry at (608) 266-4816 to schedule inspection and approval of nursery tree stock and review planting specifications with the landscaper.
- 69. On this project, the installation of a pavement support system (Silva Cell®, GreenBlue® or equivalent as approved by city) surrounding tree grate locations is required where the terrace is concrete. The Contractor shall contact City Forestry at (608) 266-4816 to identify and locate underground conflicts (utilities, vaults, conduit) or other underground obstructions and determine grate locations before support system installation. Add as a note on both the site and street tree plan set.
- 70. No later than five business days prior to forming concrete and constructing tree grate sites, the Contractor shall contact City Forestry at (608) 266-4816 to identify and locate underground conflicts (utilities, vaults, conduit) or other underground obstructions and determine grate locations. The Contractor will be required to obtain the grate, matching frame and/or tree guard. Tree grate type and matching frame: Neenah 4x8 (R-8815-A). Tree guard Neenah (R-8501-4818). Add as a note on both the landscape and street tree plan set.

Metro Transit (Contact Tim Sobota, (608) 261-4289)

- 71. The existing curbside bus stop zone and accessible pedestrian sidewalk and concrete boarding pad on the east side of North Brooks Street, north of West Johnson Street, provides critical access to the City's transit operations, and any planned or permitted obstruction of the existing bus stop zones in excess of thirty (30) days may require additional right-of-way improvements and/or other mitigation measures in coordination with Traffic Engineering and Metro Transit staff in order to create an alternate bus stop zone that would serve the North Brooks Street at West Johnson Street intersection area in a comparable operational and accessible manner.
- 72. Metro Transit operates daily all-day transit service along North Brooks Street opposite this property with trips at least every 60 minutes (every 30 minutes or less during the day on weekdays). Metro Transit operates additional daily all-day transit service along University Avenue, West Johnson Street and Park Street near this property with trips on most routes at least every 30 minutes.
- 73. Metro Transit would initially estimate the following counts of potentially eligible trips towards US Green Building Council/LEED Quality Access to Transit points: 444 Weekday & 201 Weekend (average). Please contact Metro Transit if additional analysis would be of interest.

Certified Survey Map

Recommended Conditions of Approval

Major/Non-Standard Conditions are Shaded

City Engineering Division (Contact Tim Troester, (608) 267-1995)

- 74. Enter into a City / Developer agreement for the required infrastructure improvements. Agreement to be executed prior to sign off. Allow 4-6 weeks to obtain agreement. Contact City Engineering to schedule the development and approval of the plans and the agreement. (MGO 16.23(9)c)
- 75. Construct sidewalk, terrace, curb and gutter and street and alley pavement to a plan as approved by City Engineer.
- 76. Madison Metropolitan Sewerage District (MMSD) charges are due and payable prior to City Engineering Division sign-off, unless otherwise collected with a Developer's / Subdivision Contract. Contact Mark Moder (261-9250) to obtain the final MMSD billing a minimum of two (2) working days prior to requesting City Engineering signoff.
- 77. A minimum of two (2) working days prior to requesting City Engineering signoff on the CSM, contact either Tim Troester (West) at (608) 261-1995 (ttroester@cityofmadison.com) or Brenda Stanley (East) at (608) 261-9127 (bstanley@cityofmadison.com) to obtain the final stormwater utility charges that are due and payable prior to sub-division of the properties. The stormwater utility charges (as all utility charges) are due for the previous months of service and must be cleared prior to the land division (and subsequent obsolesces of the existing parcel).

<u>City Engineering – Mapping</u> (Contact Julius Smith, (608) 264-9276)

- 78. Grant a Public Sidewalk Easement to the City on the face of this Certified Survey Map along W. Johnson St. and N. Brooks St. the final location and width to be approved by City Engineering and Traffic Engineering. Contact Jule Smith (jsmith4@cityofmadison.com) with Engineering Mapping for the final language to be included on the face of the map. Note the Easement along N. Brooks St. shall be at a minimum 1' behind the existing back of sidewalk, but is up to the final approval and requirements of City Engineering and Traffic Engineering
- 79. Coordinate and request from the utility companies serving this area the easements required to serve this development. Those easements shall be properly shown, dimensioned and labeled on the final plat.
- 80. Note the original document that set forth the rights for the joint driveway easement per Document No. 4403778 and document No.4409641. Additionally note somewhere that the agreement subjected the parcel to a parking agreement.
- 81. The concurrent proposed developments location appears to conflict the 6' wide underground electrical easement per Document No. 3605277 this document should be released, and removed from the CSM after the release has been recorded. If not a note should be made that is location has been show by location of the actual facilities and the recorded as 30' distances shall be shown for L1 and L3
- 82. Note Parcel is Subject to Land Use Restriction Agreement Recorded as Document No. 2937168. It appears this agreement will require it to be released to allow for the development to proceed.
- 83. Add the City of Madison as a Cooperate Mortgagee for this CSM
- 84. Wisconsin Administrative Code A-E 7.08 identifies when Public Land System (PLS) tie sheets must be filed with the Dane County Surveyor's office. The Developer's Surveyor and/or Applicant must submit copies of any required U.S. public land survey monument record provided to the County Surveyor's Office, or, in instances where a public the monuments and witness ties area recovered under A-E 7.08(1g), the Surveyor shall provide to the City of Madison monument condition reports (with current tie sheet attached) for all Public Land Survey monuments, including center of sections of record, used in this survey, to Julius Smith (JSmith4@cityofmadison.com) of City Engineering, Land Information.
- 85. In accordance with Section s. 236.18(8), Wisconsin Statutes, the Applicant shall reference City of Madison WCCS Dane Zone, 1997 Coordinates on all PLS corners on the Plat or Certified Survey Map in areas where this control exists. The Surveyor shall identify any deviation from City Master Control with recorded and measured designations. Visit the Dane County Surveyor's Office (web address https://www.countyofdane.com/PLANDEV/records/surveyor.aspx) for current tie sheets and control data that has been provided by the City of Madison.
- 86. Prior to Engineering final sign-off by main office for Plats or Certified Survey Maps (CSM), the final Plat or CSM in pdf format must be submitted by email transmittal to Engineering Land Records Coordinator Jule Smith (Jsmith4@cityofmadison.com) for final technical review and approval. This submittal must occur a minimum of two working days prior to final Engineering Division sign-off.
- 87. Per A-E 7.05(11) list the Datum and Adjustment of the coordinate system used. eg. NAD83(97) or (2011)

- 88. Along the West line of the parcel show the recorded as bearing and distance from Conklin Replat of N1°18'E 166.35' and the recorded as distance of 105.0' along the South lines of lots 4-6.
- 89. The pending Certified Survey Map application for this property shall be completed and recorded with the Dane County Register of Deeds (ROD), the new parcel data created by the Assessor's Office and the parcel data available to zoning and building inspection staff prior to issuance of building permits and early start permits for new construction.
- 90. Submit to Jule Smith, prior to Engineering sign-off of the subject plat, one (1) digital CADD drawing in a format compatible with AutoCAD. The digital CADD file(s) shall be referenced to the Dane County Coordinate System and shall contain, at minimum, the list of items stated below, each on a separate layer/level name. The line work, preferably closed polylines for lot lines, shall be void of gaps and overlaps and match the final recorded plat:
 - a) Right-of-Way lines (public and private)
 - b) Lot lines
 - c) Lot numbers
 - d) Lot/Plat dimensions
 - e) Street names
 - f) Easement lines (i.e. all shown on the plat including wetland & floodplain boundaries.)

NOTE: This Transmittal is a separate requirement from the required Engineering Streets Section for design purposes. The Developer/Surveyor shall submit new updated final plat, electronic data for any changes subsequent to any submittal.

<u>Traffic Engineering Division</u> (Contact Luke Peters, (608) 266-6543)

- 91. The applicant shall dedicate Right of Way or grant a Public Sidewalk Easement for and be responsible for the construction of a minimum eight (8)-foot wide sidewalk, six (6)-foot terrace, and additional one (1) foot for maintenance, where applicable, along their site's frontage of East Johnson Street.
- 92. The applicant shall dedicate Right of Way or grant a Public Sidewalk Easement for and be responsible for the construction of a minimum five (5)-foot wide sidewalk, eight (8)-foot terrace, and additional one (1) foot for maintenance, where applicable, along their site's frontage of North Brooks Street.

Parking Review (Contact Trent W Schultz, (608) 246-5806)

The agency reviewed this project and determined a Transportation Demand Management (TDM) Plan is not required as part of certified survey map review. A TDM Plan will be reviewed as part of conditional use approval.

Parks Division (Contact Ann Freiwald, (608) 243-2848)

93. Park Impact Fees (comprised of the Park Infrastructure Impact Fee, per MGO Sec. 20.08(2)), and Park-Land Impact Fees, per MGO Sec. 16.23(8)(f) and 20.08(2) will be required for all new residential development associated with this project. This development is within the Central Park-Infrastructure Impact Fee district. Please reference ID# 24032 when contacting Parks about this project.

- 94. The following note should be included on the CSM: "LOTS WITHIN THIS SUBDIVISION ARE SUBJECT TO IMPACT FEES THAT ARE DUE AND PAYABLE AT THE TIME BUILDING PERMIT(S) ARE ISSUED."
- 95. The Parks Division shall be required to sign off on this CSM.

Office of Real Estate Services (Contact Heidi Radlinger, (608) 266-6558)

- 96. Prior to approval sign-off by the Office of Real Estate Services, the Owner's Certificate on the CSM shall be executed by all parties of interest having the legal authority to do so, pursuant to Wis. Stats. 236.21(2)(a). The title of each certificate shall be consistent with the ownership interest(s) reported in the most recent title report.
 - A PDF of the CSM containing electronic signatures shall be provided to ORES to obtain approval sign-off.
- 97. If any portion of the lands within the CSM boundary are subject to an Option to Purchase or other Option interest, include a Certificate of Consent for the option holder and have it executed prior to CSM sign-off, if said ownership interest meets the criteria set forth by Wis. Stat. Sec. 236.34 and Sec. 236.21(2)(a).
 - A Consent of Lessee certificate shall be included on the CSM for all tenant interests in excess of one year, recorded or unrecorded, and executed prior to CSM sign-off.
- 98. If the lands within the CSM boundary are partially located within a Tax Incremental Financing District, discussions with Joe Gromacki, the City of Madison's tax increment financing coordinator, may be necessary before recording the CSM if a TIF application is required. Mr. Gromacki can be reached at 608-267-8724 or igromacki@cityofmadison.com.
- 99. There are no special assessments reported on the subject parcel. If special assessments are levied against the property during the review period prior to CSM approval sign-off they shall be paid in full pursuant to Madison General Ordinance Section 16.23(5)(g)1.
- 100. Pursuant to Madison City Ordinance Section 16.23(5)(g)(4), the owner shall furnish an updated title report to Heidi Radlinger as well as the survey firm preparing the proposed CSM. The report shall search the period subsequent to the date of the initial title report (4/15/24) submitted with the CSM application and include all associated documents that have been recorded since the initial title report. A title commitment may be provided, but will be considered only as supplementary information to the title report update. Surveyor shall update the CSM with the most recent information reported in the title update. ORES reserves the right to impose additional conditions of approval in the event the title update contains changes that warrant revisions to the CSM.
- 101. The owner shall email the document number of the recorded CSM to Heidi Radlinger as soon as the recording information is available.

The following agencies have reviewed this request and has recommended no conditions of approval:

Planning, Zoning, Assessor's Office, Fire, Water Utility, Forestry, Metro



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83902

File ID: 83902 File Type: Resolution Status: Report of Officer

Version: 1 Reference: Controlling Body: PLAN

COMMISSION

File Created Date: 06/11/2024

Enactment Number:

File Name: Final Plat Re-Approval - American Center Eastpark Final Action:

Fifth Addition

Title: Re-approving the final plat of The American Center Eastpark Fifth Addition

subdivision on land generally addressed as 4846 Eastpark Boulevard (District 17).

Notes:

Sponsors: Planning Division Effective Date:

Attachments: Locator Maps.pdf, Subdivision Application.pdf, Letter

of Intent.pdf, Proposed Final Plat.pdf, 2023 Plat Approval Letter.pdf, Staff Memo.pdf, Link to Plat

Resolution ID 74911

Author: Hearing Date:

Entered by: tparks@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Planning Division	06/11/2024	Referred for Introduction				
	Action Text:	This Resolution was Ref	erred for Introduction				
	Notes:	Plan Commission (7/8/24),	Common Council (7/16/24)				
1	COMMON COUN	ICIL 06/18/2024	Referred	PLAN COMMISSION		07/08/2024	
	Action Text:	This Resolution was Ref	erred to the PLAN COMM	MISSION			
1	PLAN COMMISS	ION 07/08/2024	RECOMMEND TO COUNCIL TO ADOPT WITH CONDITIONS - REPORT OF OFFICER				Pass
	Action Text: A motion was made by Duncan, seconded by Mendez, to RECOMMEND TO COUNCIL TO ADOPT WITH CONDITIONS - REPORT OF OFFICER. The motion passed by voice vote/other. Notes: On a motion by Ald. Duncan, seconded by Mendez, the Plan Commission found the standards met and recommended re-approval of the final plat subject to the comments and conditions contained in the Plan Commission materials. The motion passed by voice vote/other.						

Text of Legislative File 83902

Fiscal Note

No City appropriation is required with the re-approval of this final plat. City costs associated with urban development in this area will be included in future operating and capital budgets subject to Common Council approval.

Title

Re-approving the final plat of *The American Center Eastpark Fifth Addition* subdivision on land generally addressed as 4846 Eastpark Boulevard (District 17).

Body

WHEREAS American Family Insurance Corporate Real Estate and Cascade Development duly filed a preliminary plat and final plat known as *The American Center Eastpark Fifth Addition* on property on land addressed as 4846 Eastpark Boulevard, City of Madison, Dane County, Wisconsin for approval by the Madison Common Council; and

WHEREAS on February 7, 2023, the Common Council conditionally approved the preliminary plat and final plat of *The American Center Eastpark Fifth Addition* by Resolution 23-00129 (ID 74911); and

WHEREAS Wis. Stat. 236.25(2)(b) requires that a final plat be offered for recording within 12 months of the approval of the plat;

NOW THEREFORE BE IT RESOLVED that said final plat is hereby re-approved subject to all conditions as required by the reviewing City departments as required by Section 16.23 of the General Ordinances of the City of Madison, subject to the conditions noted in the Plan Commission files, and that same shall be recorded with the Dane County Register of Deeds.

BE IT FURTHER RESOLVED that the Mayor and City Clerk of the City of Madison are hereby authorized to sign the plat, bond and subdivision contract, subsequent affidavits of corrections, parkland acquisition documents, easement or right-of-way release or procurement documents or any other related document or documents as deemed necessary by the Secretary of the Plan Commission in accordance with the approved plat.

BE IT FURTHER RESOLVED that the said plat is hereby added to the official map and the street grades for said plat on file in the City Engineer's Office are hereby approved.

BE IT FURTHER RESOLVED that the Planning Division is authorized to reflect the recorded subdivision in the Comprehensive Plan and any applicable neighborhood plans.

BE IT FURTHER RESOLVED that all dedications included in this plat or required as a condition of approval of this plat be and are hereby accepted by the City of Madison.

BE IT FURTHER RESOLVED that the Common Council authorizes City staff to request approval from the Capital Area Regional Planning Commission of any minor revisions to adopted environmental corridor boundaries within the Central Urban Service Area relating to this subdivision, and that the Council recognizes and adopts said revised boundaries.



Final Plat Re-Approval

4846 Eastpark Boulevard

Applicant

Luke Stauffacher, Cascade Development Mike Ziehr, Vierbicher Associates

Requests

- The American Center Eastpark Fifth Addition, creating two lots for future mixed-use development, seven lots for future multi-family development, one outlot for private parkland, and one outlot for public stormwater management

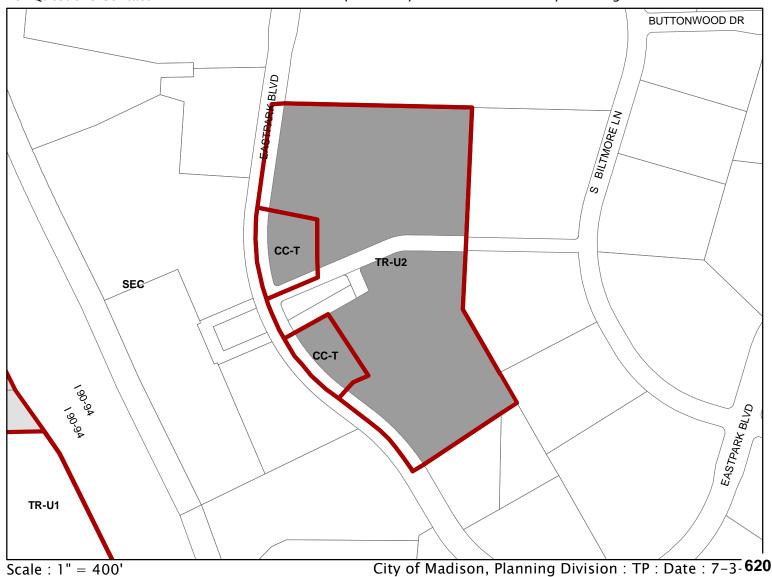
Public Hearing Dates Plan Commission

July 8, 2024

Common Council

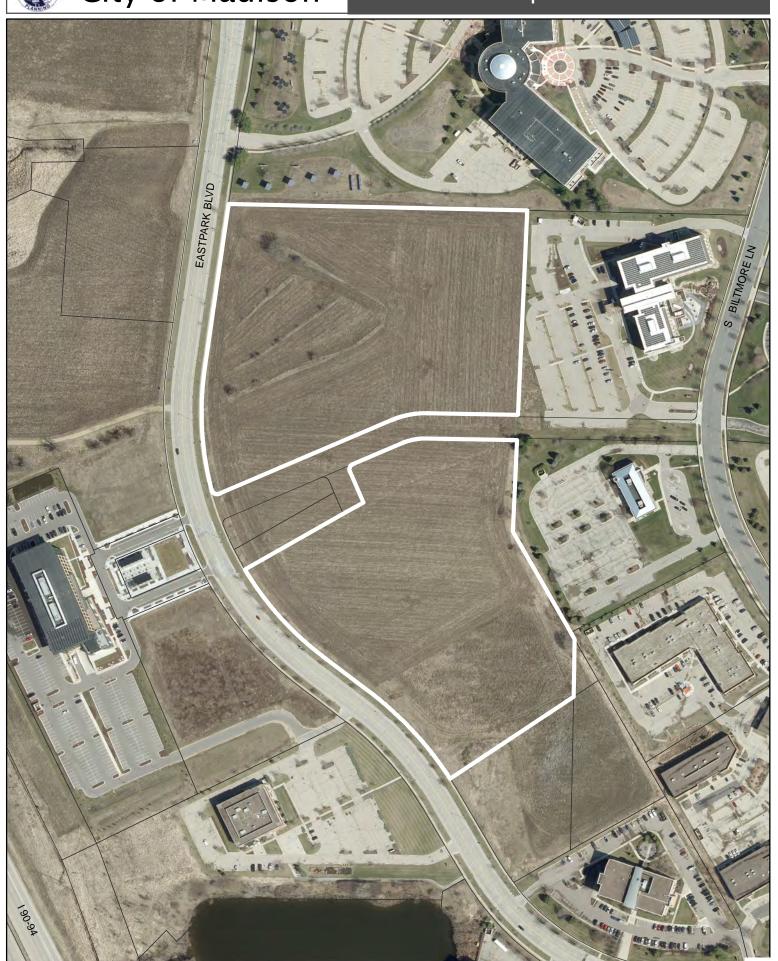
July 16, 2024

For Questions Contact: Tim Parks at: 261-9632 or tparks@cityofmadison.com or City Planning at 266-4635





4846 Eastpark Boulevard



Date of Aerial Photography : Spring 20621

SUBDIVISION APPLICATION

** Please read both pages of the application completely and fill in all required fields **

For a digital copy of this form with fillable fields, please visit:

https://www.cityofmadison.com/sites/default/files/city-of-madison/ development-services-center/documents/SubdivisionApplication.pdf

If you need an interpreter, translator, materials in alternate formats or other accommodations to access these forms, please call the Planning Division at (608) 266-4635.

Si necesita interprete, traductor, materiales en diferentes formatos, u otro tipo de ayuda para acceder a estos formularios, por favor llame al (608) 266-4635.

Yog tias koj xav tau ib tug neeg txhais lus, tus neeg txhais ntawv, los sis xav tau cov ntaub ntawv ua lwm hom ntawv los sis lwm cov kev pab kom paub txog cov lus ghia no, thov hu rau Koog Npaj (Planning Division) (608) 266-4635.

City of Madison Planning Division Madison Municipal Building, Suite 017 215 Martin Luther King, Jr. Blvd. P.O. Box 2985 Madison, WI 53701-2985 (608) 266-4635



NOTICE REGARDING LOBBYING ORDINANCE: If you are seeking approval of a development that has over 40,000 square feet of non-residential space, or a residential development of over 10 dwelling units, or if you are seeking assistance from the City with a value of \$10,000 (including grants, loans, TIF or similar assistance), then you likely are subject to Madison's lobbying ordinance (M.G.O. Sec. 2.40). You are required to register and report your lobbying. Please consult the City Clerk's Office for more information. Failure to comply with the lobbying ordinance may result in fines.

1. Ap	1. Application Type							
V	Preliminary Subdivision Plat	V	Final Subdivision Plat		Land Division/Certified Survey Map (CSM)			
If a Plat, Proposed Subdivision Name:			A merican Center Plat Eastp	oark Fifth	A ddition			
2. Rev	riew Fees							

- - For Certified Survey Maps, an application fee of \$250 plus \$200 per lot and outlot contained on the CSM.

• For Preliminary and/or Final Plats, an application fee of \$250, plus \$50 per lot or outlot contained on the plat.

Make checks payable to "City Treasurer" and mail it to the following address: City of Madison Building Inspection; P.O. Box 2984; Madison, WI 53701-2984. Please include a cover page with the check which includes the project address, brief description of the

project, and contact i	nformation.	
3. Property Owner and	Agent Information	
Name of Property C	wner: American Family Insurance Corp Real Estate	Representative, if any: Luke Stauffacher, Cascade Development
Street address:	6000 American Parkway, Madison, WI 53783	City/State/Zip: 5150 High Crossing Blvd. Madison, WI 53718
Telephone:		Email: luke@ cascadedevelop.com
Firm Preparing Surv	vey: Vierbicher Associates	Contact: Mike Ziehr
Street address:	999 Fourier Drive, Suite 201	City/State/Zip: Madison, WI 53717
Telephone:	608-821-3962	Email: mzie@ vierbicher.com
Check only ONE – ALL C	orrespondence on this application should b	e sent to: 🔲 Property Owner, OR 🗹 Survey Firm
4. Property Information	n for Properties Located within Madison C	ity Limits
Parcel Addresses: 4	846 Eastpark Boulevard	
Tax Parcel Number(s): <u>0810-222-0402-8</u>	
Zoning District(s) of	Proposed Lots: SEC (Site to be Rezoned to CC-T	& TR-U2) School District: Sun Prairie
Please include a de	etailed description of the number and use o	fall proposed lots and outlots in your letter of intent.
4a. Property Information	n for For Properties Located <i>Outside</i> the N	ladison City Limits in the City's Extraterritorial Jurisdiction:
Parcel Addresses (no	ote town if located outside City): N/A	
Date of Approval by	37.4	Date of Approval by Town: N/A

For an exterritorial request to be scheduled, approval letters from both the Town and Dane County must be submitted.

5. Subdivision Contents and Description. Complete table as it pertains to your request; do not complete gray areas.

Land Use	Lots	Outlots	Acres
Residential	7		13.52
Retail/Office	2		3.59
Industrial			

Land Use	Lots	Outlots	Acres
Other (state use):			
Outlots Dedicated to the Public (Parks, Stormwater, etc.)		1	.58
Outlots Maintained by a Private Group or Association		1	2.08
PROJECT TOTALS	9	2	19.77

6. Required Submittal Materials

<u>Digital (PDF) copies</u> of all items listed below (if applicable) are required. Applicants are to submit each of these documents as <u>individual PDF files</u> in an e-mail sent to <u>PCapplications@cityofmadison.com</u>. The transmittal shall include the name of the project and applicant. Note that <u>an individual email cannot exceed 20MB</u> and it is the <u>responsibility of the applicant</u> to present files in a manner that can be accepted. Electronic submittals via file hosting services (such as Dropbox) are <u>not</u> allowed. Applicants who are unable to provide the materials electronically should contact the Planning Division at <u>Planning@cityofmadison.com</u> or (608) 266-4635 for assistance.

☐ A Completed Subdivision Application Form (i.e. both sides of this form)

☐ **Map Copies** (prepared by a Registered Land Surveyor):

- For <u>Preliminary Plats</u>, the drawings must be drawn to scale and are required to provide all information as set forth in M.G.O. Sec. 16.23 (7)(a).
- For Final Plats, the drawings must be drawn to scale and drawn to the specifications of §236.20, Wis. Stats..
- For <u>Certified Survey Maps (CSMs)</u>, the drawings shall include all of the information set forth in <u>M.G.O. Secs. 16.23 (7)(a)</u> and (d), including existing site conditions, the nature of the proposed division and any other necessary data. Utility data (field located or from utility maps) may be provided on a separate map submitted with application.

For Plat & CSMs, in addition to the PDF copy, a digital CADD file shall also be submitted in a format compatible with AutoCAD. The digital CADD file(s) shall be referenced to the Dane County Coordinate System and shall contain, at minimum, the list of items stated below, each on a separate layer/level name. The line work shall be void of gaps and overlaps and match the plat, preliminary plat or CSM as submitted: a) Right-of-Way lines (public and private); b) Lot lines; c) Lot numbers; d) Lot/Plat dimensions; e) Street names; f) Easement lines (i.e. all in title and shown on the plat or CSM including wetland & floodplain boundaries.)

□ **Letter of Intent**: One copy of a letter describing the proposed subdivision or land division in detail including, but not limited to:

- The number and type/use of the lots and outlots proposed with this subdivision or land division, including any outlots to be dedicated to the public;
- Existing conditions and uses of the property;
- Phasing schedule for the project, and;
- The names of persons involved (property owner(s), subdivider, surveyor, civil engineer, etc.).
 - * The letter of intent for a subdivision or land division may be the same as the letter of intent submitted with a concurrent Land Use Application for the same property.
 - ** A letter of intent is not required for Subdivision Applications for lot combinations or split duplexes.

□ **Report of Title and Supporting Documents**: One copy of a City of Madison standard 60-year Report of Title obtained from a title insurance company as required in M.G.O. Sec. 16.23 and as satisfactory to the Office of Real Estate Services. Note:

- <u>The Report of Title must have been completed within three (3) months of the submittal date of this application. Title insurance or a title commitment policy are NOT acceptable</u> (i.e. a Preliminary Title Report or a Record Information Certificate).
- The electronic PDF submittal shall include images of the vesting deeds and all documents listed in the Report of Title.
- <u>Do not email these files to the City's Office of Real Estate Services</u>. Send them instead to the email address noted at the top of this page.

For Surveys Outside the Madison City Limits: One copy of the approval letters from the town where the property is
located and Dane County shall be submitted with your request. The Plan Commission may not consider an application
within its extraterritorial jurisdiction without prior approval from the town and Dane County.

7.	Applicant Declarations:	
	The signer attests that the application has been completed accur	ately and all required materials have been submitted:
	Applicant's Printed Name: Michael J. Ziehr	Signature:
	Date: November 28, 2022 Interest In Property On This Da	te: Land Surveyor

EFFECTIVE: JANUARY 2021 PAGE 2 OF 623



Copy to



		١	999 Fourier Drive, Suite 201 Madison, Wisconsin 53717 608) 826-0532 phone		Project No. 210303			
		(Re: 4846 Eastpark Boulevard Plat Submittal					
Attn: T	im Parks							
2	Dept. of Plannir 215 Martin Luthe Madison, WI 537	er King Jr Blv	unity & Economic Developme d., Suite 17	ent				
					File:			
WE ARE S	ENDING YOU:	x At	tached					
		Ur	nder separate cover via		the following items:			
			Shop Drawings Prints		Plans Samples Specifications			
			Copy of Letter Chang	ge Orc	ler Report			
Copies	Date	No.	T	Ъ	escription			
1	11/28/2022	110.	1 Preliminary Plat		озенрногі			
1	11/28/2022		1 Final Plat					
1	11/28/2022		Subdivision Application Forr	n, Che	eck for Review Fees (\$1600) mailed today			
1	11/28/2022		Letter of Intent					
1	11/28/2022		60 Tear Title Report					
THESE ARE	TRANSMITTED A	78 CHECKEL) REL ∩W:					
	For approval		Approved as submitted	Пг	esubmit copies for approval			
	or your use		Approved as noted		ubmit copies for distribution			
	As requested		Returned for corrections		eturn corrected prints			
	·	 	-	□ [']	eloni conecied pilins			
	For review & co	mment [For your file					
F	FOR BIDS DUE:		(Date)	R	ETURNED AFTER LOAN TO US			
REMARKS:								
An ele	ectronic copy o	of this submi	ttal was sent this morning to <u>p</u>	cappl	ications@cityofmadison.com			
	Please contr	act us if you	need any more information a	or if vo	u have anv auestions. Thank vou			

Letter of Transmittal

November 28, 2022

Date:

If enclosures are not as noted, kindly notify us at once.

Signed

Michael J. Ziehr, P.L.S.

November 28, 2022



Ms. Heather Stouder
Director, Planning Division
City of Madison Department of Planning & Community & Economic Development
215 Martin Luther King Jr. Blvd., Ste 017
Madison, Wisconsin 53703

RE: Letter of Intent

4846 Eastpark Blvd. KBA Project #2155

Ms. Heather Stouder,

The following is submitted together with the plans and application for the staff, Plan Commission and Common Council's consideration of approval.

Organizational Structure:

Owner:

Cascade Development 5150 High Crossing Blvd. Madison, WI 53718 (608) 354-8748 Contact: Luke Stauffacher Luke@cascadedevelop.com

Engineer: Vierbicher 999 Fourier Drive, Suite 201 Madison, WI 53717

(608) 821-3961 Contact: Matthew W. Schreiner

msch@vierbicher.com

Architect:

Knothe & Bruce Architects, LLC 7601 University Avenue, Ste 201 Middleton, WI 53562 (608) 836-3690 Contact: Kevin Burow Kburow@knothebruce.com

Landscape Design:
Olson Toon Landscaping
3570 Pioneer Road
Verona, WI 53593
(608) 827-9401
Contact: Brad Fregien
Brad@olsontoon.com

Introduction

Cascade Development is proposing a mixed-use development for The Commons District at The American Center. This development will occupy the lands east of Eastpark Boulevard, north and south of Dreamer Drive. This project will provide much needed housing and the opportunity for dining, retail and other service uses within The Center. It will also extend the hours of activity within The American Center which will promote a more community-oriented destination. This submittal is made for the plat and rezoning submittals.

Required Approvals and Modifications

PRC and DDC approvals - The American Center
Preliminary and Final plat - City of Madison
Rezone to CC-T (mixed use lots) and TR-U2 (residential lots) - City of Madison
City of Madison Comprehensive Plan - City of Madison
Rattman Neighborhood Plan - City of Madison
The American Center Land Use Plan - The American Center & City of Madison
School District Boundary Change - Department of Public Instruction
Land Use Application and Conditional Use Permit for each lot - City of Madison

The American Center and City of Madison Input

The project team has met with City planning staff and representatives of The American Center on several occasions to discuss the proposed project, including a City of Madison Development Assistance Team (DAT) meeting on August 18, 2022, a meeting with the City of Madison Zoning department on September 22, 2022, a meeting with Alder Paulson on October 13, 2022 and numerous meetings with Jane Grabowski-Miller of The American Center. This input has helped shape this proposed development with adjustments to the general site design, easements, setbacks, stormwater management and zoning.

Project Description

The project will provide a total of up to 950 dwelling units and 14,000 s.f. of commercial space across multiple buildings. The buildings fronting the north and south side of the Commons will be five-story mixed use buildings with commercial space on the first floor. Outlying buildings will be four-story apartments. Buildings will feature underground parking and internal trash/recycling collection. Amenity space for tenants including pools, community and exercise rooms will be interspersed throughout the project. Cascade Development will manage the project with their own on-site staff.

Proposed Zoning

Lot 47 & Lot 55 (Mixed-use lots on The Commons): CC-T All other lots: TR-U2

Site Layout

A plat will be recorded for this development which will include right-of-way for a new public street running north and south from Dreamer Drive. This street will connect Dreamer Drive to Eastpark at the north and south end of the development, improving circulation and connectivity. The street cross section is the minimum width that will permit parking on both sides and fire department aerial access, which Madison Fire has indicated will be required.

Cross access easements will be provided allowing shared access to parking throughout the development, reducing the number of curb-cuts needed. Buildings have been oriented to the street and parking has been positioned to allow screening from the street where possible.

Two acres have been designated for a park on the north phase of the project. This park will be privately owned with a public access easement over it allowing it to be enjoyed by all users of The Center. The design is not finalized, but the intent is to provide both formal and informal recreation areas.

Architecture

We are proposing unique but complementary architecture for the north and south phases of the project. Both designs feature clean, modern lines and high-quality materials that will fit in well with the existing buildings within the park. Featured materials include natural stone and brick veneers, fiber

composite siding and MCM panels. The buildings on The Commons will be five-stories tall, all other buildings are four-stories. Buildings will range from 55-65 feet in height, and all buildings will comply with FAA regulations and the viewshed restriction from the American Family corporate offices. Mechanical equipment will be screened, with equipment located on rooftops screened by building parapets.

Parking

Bus Rapid Transit will serve this site and Planning staff have indicated they would encourage any parking reduction the developer is comfortable with, especially any reduction in surface parking. Structured parking will be provided within all the buildings, with the goal of achieving one structured stall per dwelling unit if possible. Surface and on-street parking will bring the overall ratio to approximately 1.5 stalls per dwelling unit.

Landscaping

Landscape design has not been finalized but will primarily focus on native plants and natural treatments. At the park the natural topography will provide opportunities for grade change and terraces. Landscape plans will be submitted to PRC and Madison's Urban Design Commission for approval.

Exterior Lighting

Exterior lighting will comply with Madison ordinances and will be high efficiency LED and Dark Sky compliant.

Project Schedule

The development team intends to obtain entitlements for the project to allow construction of the streets and infrastructure in the Summer of 2023. Building construction would start immediately following, starting with the mixed-use building on the north side of The Commons and proceed through the site at approximately 150-250 units per year or as market conditions dictate.

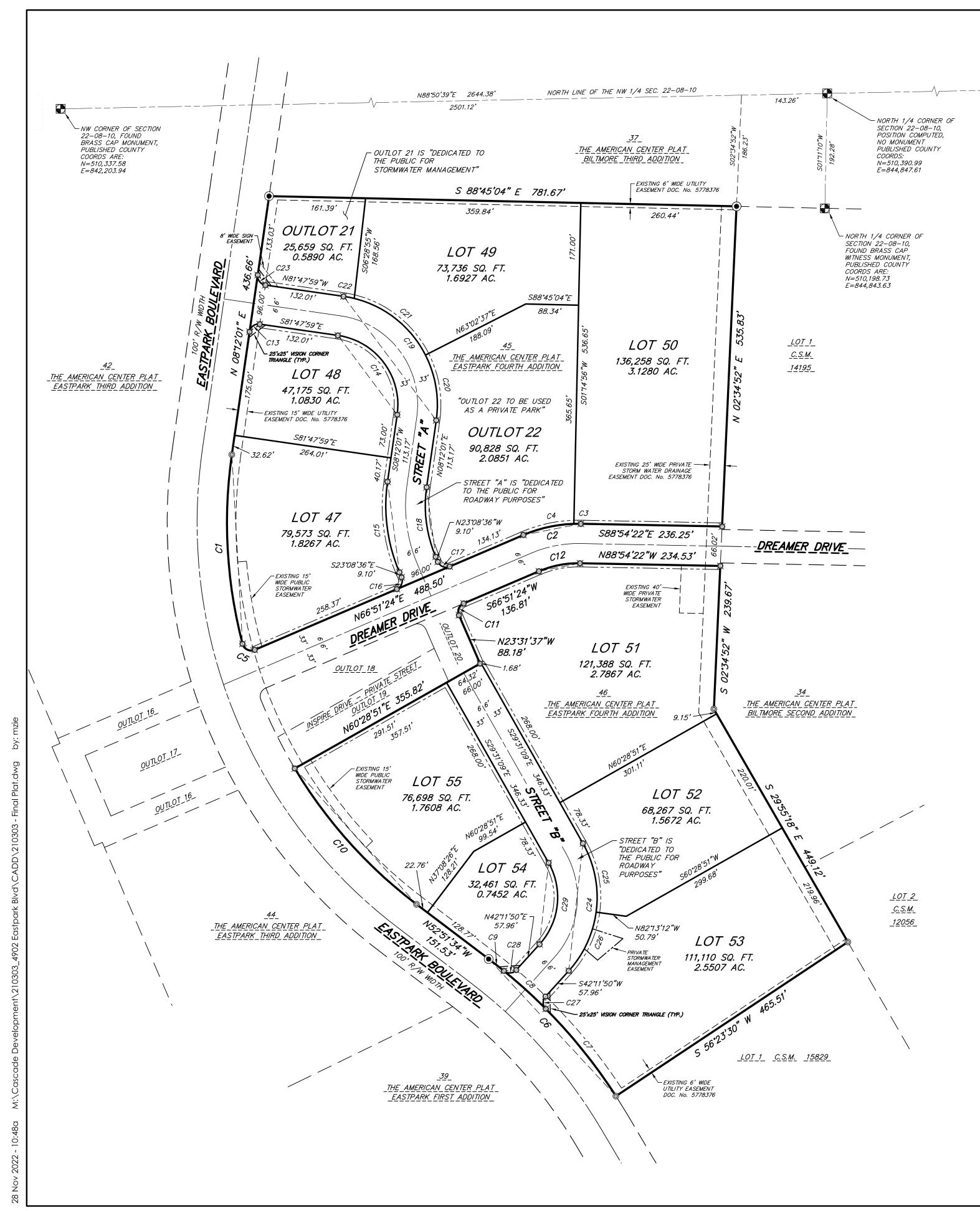
Thank you for your time and consideration of our proposal.

Sincerely

Kevin Burow, AIA, NCARB, LEED AP

Keni Bun

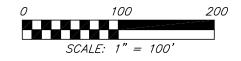
Managing Manager



NE CORNER OF SECTION
22-08-10, FOUND
BRASS CAP MONUMENT,
PUBLISHED COUNTY
COORDS ARE:
N=510,441.89
E=847,462.58

THE AMERICAN CENTER PLAT EASTPARK FIFTH ADDITION

BEING A REDIVISION OF LOT 45 AND LOT 46, THE AMERICAN CENTER PLAT EASTPARK FOURTH ADDITION AS RECORDED IN VOLUME XX—XXXX OF PLATS ON PAGES XXX—XXX, AS DOCUMENT NUMBER XXXXXXXX, DANE COUNTY REGISTRY AND LOCATED IN THE NE 1/4 AND THE SE 1/4 OF THE NW 1/4 AND PART OF THE NW 1/4 AND THE SW 1/4 OF THE NE 1/4 OF SECTION 22, ALL IN TOWNSHIP 08 NORTH, RANGE 10 EAST, CITY OF MADISON, DANE COUNTY, WISCONSIN



TOTAL AREA OF PLAT = 946,767 SQ. FT. (21.7348 ACRES)

CURVE TABLE							
CURVE NUMBER	ARC LENGTH	RADIUS	CENTRAL ANGLE	CHORD LENGTH	CHORD DIRECTION		
C1	318.93'	800.00'	22°50′30″	316.82'	S03° 13' 16"E		
C2	98.56	233.00'	24°14′14″	97.83'	S78° 58′ 31″W		
<i>C3</i>	11.74'	233.00'	02*53'09"	11.73'	S89° 39′ 04″W		
C4	86.83'	233.00'	21°21'05"	86.33'	S77° 31' 57"W		
C5	25.79'	15.00'	98'30'05"	22.73'	N63° 53' 33"W		
C6	314.15	900.00'	19*59'58"	312.56'	N42° 51′ 35″W		
<i>C7</i>	187.49	900.00'	11*56'09"	187.15'	N38° 49′ 40″W		
C8	94.47'	900.00'	06°00'51"	94.43'	N47° 48' 10"W		
<i>C9</i>	32.19	900.00'	02°02'58"	32.19'	N51° 50′ 05″W		
C10	306.84	800.00'	21°58'32"	304.96'	S41° 52' 17"E		
C11	23.66'	15.00'	90°23'02"	21.28'	S21° 39' 53"W		
C12	70.64	167.00'	24°14′14″	70.12'	S78° 58′ 31″W		
C13	23.56'	15.00'	90°00'00"	21.21'	S53° 12' 01"W		
C14	183.78'	117.00'	90°00'00"	165.46	N36° 47′ 59″W		
C15	154.81	283.00'	31°20'37"	152.89'	S07° 28' 17"E		
C16	23.56	15.00'	90'00'00"	21.21'	N21° 51′ 24″E		
C17	23.56'	15.00'	90°00'00"	21.21'	S68° 08' 36"E		
C18	118.71'	217.00'	31°20'37"	117.23'	S07° 28' 17"E		
C19	287.46'	183.00'	90°00'00"	258.80'	N36° 47' 59"W		
C20	112.29'	183.00'	35°09'28"	110.54	N09° 22′ 43″W		
C21	156.88'	183.00'	49°07'02"	152.12'	N51° 30′ 58″W		
C22	18.29	183.00'	05°43′30″	18.28'	N78° 56′ 14″W		
C23	23.56	15.00'	90°00'00"	21.21	S36° 47′ 59″E		
C24	229.06'	183.00'	71°42'58"	214.40'	N06° 20′ 21″E		
C25	119.13'	183.00'	3717'56"	117.04'	N10° 52' 10"W		
C26	109.93'	183.00'	34°25′02″	108.28'	N24° 59' 19"E		
C27	22.77'	15.00'	86*59'35"	20.65	S01° 17' 58"E		
C28	22.77'	15.00'	86*59'35"	20.65	N85° 41′ 37″E		
C29	146.45'	117.00'	71°42′58″	137.07'	N06° 20′ 21″E		

NORTH LINE OF THE NE 1/4 SEC. 22-08-10

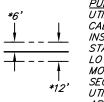
S88°53'06"W 2615.46'

SURVEY LEGEND

- © FOUND 1" Ø IRON PIPE (UNLESS OTHERWISE NOTED)
- FOUND 1-1/4" Ø IRON ROD
- SET 1-1/4" x 18" SOLID IRON
 RE-ROD, MIN. WT. 4.30 lbs./ft. ALL
 OTHER LOT AND OUTLOT CORNERS
 MARKED BY A 3/4" x 18" SOLID
 IRON RE-ROD, WT. 1.50 lbs./ft.
- () INDICATES RECORDED AS

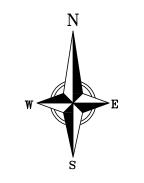
 DISTANCES ARE MEASURED TO THE NEAREST HUNDREDTH OF

*VISION CORNER NOTE: NO STRUCTURE OF ANY KIND SHALL BE PERMITTED WITHIN A VISION CORNER WHICH EXCEEDS A HEIGHT OF 2-1/2 FEET ABOVE THE ELEVATION OF THE INTERSECTION, EXCEPT FOR NECESSARY HIGHWAY AND TRAFFIC SIGNS, APPROVED PUBLIC UTILITY LINES AND OPEN FENCES THROUGH WHICH THERE IS CLEAR VISION, NOR SHALL ANY PLANT MATERIAL, EXCEPT GRASSES AND SIMILAR TURF, BE PERMITTED WHICH OBSCURES SAFE VISION OF THE APPROACHES TO THE INTERSECTION.

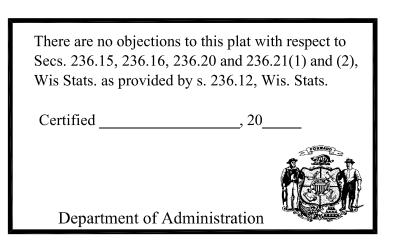


PUBLIC UTILITY EASEMENT NOTE: PUBLIC UTILITY EASEMENTS. NO POLES OR BURIED CABLES ARE TO BE PLACED SUCH THAT THE INSTALLATION WOULD DISTURB ANY SURVEY STAKE, OR OBSTRUCT VISION ALONG ANY LOT LINE. THE DISTURBANCE OF A SURVEY MONUMENT BY ANYONE IS A VIOLATION OF SECTION 236.32 OF WISCONSIN STATUTES. UTILITY EASEMENTS AS HEREIN SET FORTH ARE FOR THE USE OF PUBLIC BODIES AND PRIVATE PUBLIC UTILITIES HAVING THE RIGHT TO SERVE THE AREA.

*UNLESS OTHERWISE NOTED ON THE PLAT.



BEARINGS ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM, DANE COUNTY, WISCONSIN. THE NORTH LINE OF NW 1/4 OF SECTION 22 MEASURED AS BEARING N88'50'39"E



vierbicher
planners | engineers | advisors
Phone: (800) 261-3898

ier | Value |

Drafted by: MZIE Checked by: MMAR

Date: 11/28/2022

FN: 210303

SURVEYED BY:
Vierbicher Associates,
By: Michael J. Ziehr
999 Fourier Drive,
Suite 201
Madison, WI 53717
(608) 821-3962

mzie@vierbicher.com

cc. Cascade Development LLC 5150 High Crossing Blvd. Madison, WI 53718

SHEET 1 OF 2 BEING A REDIVISION OF LOT 45 AND LOT 46, THE AMERICAN CENTER PLAT EASTPARK FOURTH ADDITION AS RECORDED IN VOLUME XX—XXXX OF PLATS ON PAGES XXX—XXX, AS DOCUMENT NUMBER XXXXXXXX, DANE COUNTY REGISTRY AND LOCATED IN THE NE 1/4 AND THE SE 1/4 OF THE NW 1/4 AND PART OF THE NW 1/4 AND THE SW 1/4 OF THE NE 1/4 OF SECTION 22,

ALL IN TOWNSHIP 08 NORTH, RANGE 10 EAST, CITY OF MADISON, DANE COUNTY, WISCONSIN

SURVEYOR'S CERTIFICATE

I, Michael J. Ziehr, Professional Land Surveyor No. 2401 hereby certify that in full compliance with the provisions of Chapter 236 of the Wisconsin Statutes and the subdivision regulations of the City of Madison and under the direction of the owner(s) of said land, I have surveyed, divided, and mapped THE AMERICAN CENTER PLAT EASTPARK FIFTH ADDITION; that such plat correctly represents all exterior boundaries and the subdivision of the lands surveyed; and that this land is a redivision of Lot 45 and Lot 46, The American Center Plat Eastpark Fourth Addition as recorded in Volume xx—xxxx of Plats on pages xxx—xxx, as Document Number xxxxxxxx, Dane County Registry and located in the NE 1/4 and the SE 1/4 of the NW 1/4 and part of the NW 1/4 and the SW 1/4 of the NE 1/4 of Section 22, all in Township 08 North, Range 10 East, City of Madison, Dane County, Wisconsin, more fully described as follows:

All of Lot 45 and Lot 46, The American Center Plat Eastpark Fourth Addition as recorded in Volume xx—xxxx of Plats on pages xxx—xxx, as Document Number xxxxxxxx, Dane County Registry, City of Madison, Dane County, Wisconsin.

Said description contains 946,767 square feet or 21.7348 acres more or less.

Vierbicher Associates Inc.

By: Michael J. Ziehr

Dated this _____ day of _____, 2023.

Signed:

Michael J. Ziehr, P.L.S. No. S-2401

CITY OF MADISON COMMON COUNCIL APPROVAL

Resolved, that the plat of THE AMERICAN CENTER PLAT EASTPARK FIFTH ADDITION, located in the NE 1/4 and the SE 1/4 of the NW 1/4 and part of the NW 1/4 and the SW 1/4 of the NE 1/4 of Section 22, all in Township 08 North, Range 10 East, City of Madison, Dane County, Wisconsin, was hereby approved by Enactment Number _______, File I.D. Number _______,

adopted on this ______ day of ______, 2023, and that said enactment further provided for the acceptance of those lands dedicated and rights conveyed by said plat of THE AMERICAN CENTER PLAT EASTPARK FIFTH ADDITION to the City of Madison for public use.

Dated this _____ day of _____, 2023.

Maribeth Witzel—Behl, City Clerk City of Madison, Dane County, Wisconsin

OWNER'S CERTIFICATE

as owner(s), we hereby certify that they we caused the land described on this plat to be surveyed, divided, mapped, and dedicated as represented on this plat. We further certify that this plat is required by S.236.10 or S.236.12 to be submitted to the following for approval or objection:

Department of Administration Common Council, City of Madison Dane County Zoning and Land Regulation Committee

Ву: _____

State of Wisconsin))ss. County of Dane)

Personally came before me this _____ day of ______, 2023, the above named ______, to me known to be the persons who executed the foregoing instrument and acknowledged the same.

Notary Public, State of Wisconsin

My Commission expires: _____

CONSENT OF MORTGAGEE

IN WITNESS WHEREOF, the said _________, has caused these presents to be signed by ______, and countersigned by ______, its ______, at Madison, Wisconsin, on this ______ day of _____, 2023.

Ву:_____

State of Wisconsin))ss. County of Dane)

Personally came before me this _______ day of ________, 2023, ______ and ________, of the above named banking association, to me known to be the persons who executed the foregoing instrument, and to me known to be such ______ of said banking association, and acknowledged that they executed the foregoing instrument as such officers as the deed of said banking association, by its authority.

My Commission expires:

Notary Public, State of Wisconsin

CITY OF MADISON TREASURER'S CERTIFICATE

As the duly appointed City Treasurer of the City of Madison,
Dane County, Wisconsin, I hereby certify that the records in my
office show no unredeemed tax sales and no unpaid taxes or
unpaid special assessments affecting any of the lands included in
the plat of THE AMERICAN CENTER PLAT EASTPARK FIFTH
ADDITION as of this ______ day of ______,

Craig Franklin, City of Madison Treasurer

CITY OF MADISON PLAN COMMISSION APPROVAL

Approved for recording per the Secretary of the City of Madison Planning Commission.

Dated this _____, 2023.

Matthew Wachter, Secretary of Planning Commission

DANE COUNTY TREASURER'S CERTIFICATE

I, Adam Gallagher, being the duly elected, qualified, and acting Treasurer of the County of Dane, do hereby certify that the records in my office show no unredeemed tax sales and no unpaid taxes or unpaid special assessments affecting any of the lands included in the plat of THE AMERICAN CENTER PLAT EASTPARK FIFTH ADDITION, as of this ______ day of ______, 2023.

Adam Gallagher, Dane County Treasurer

CERTIFICATE OF REGISTER OF DEEDS

RECORDING DATA

There are no objections to this plat with respect to Secs. 236.15, 236.16, 236.20 and 236.21(1) and (2), Wis Stats. as provided by s. 236.12, Wis. Stats.

Department of Administration

Received for recording this ______ day of ______, 20____, at ____ o'clock ____.M. and recorded in Volume _____ of Plats, on pages _____ as Document Number _____.

Kristi Chlebowski Dane County Register of Deeds

vierbicher
planners | engineers | advisors

Phone: (800) 261-3898

Drafted by: MZIE
Checked by: MMAR

FN: 210303
Date: 11/28/2022

Rev:

Rev:

SURVEYED BY:
Vierbicher Associates, In 999 Fourier Drive, Suite 201
Madison, WI 53717
(608) 821-3962
mzie@vierbicher.com

c. Cascade Development LLC 5150 High Crossing Blvd. Madison, WI 53718

SHEET 2 OF 2



Department of Planning & Community & Economic Development

Planning Division

Heather Stouder, Director

Madison Municipal Building, Suite 017 215 Martin Luther King Jr. Boulevard Madison, Wisconsin 53703 Phone: (608) 266-4635 www.cityofmadison.com

February 14, 2023

Mike Ziehr Vierbicher & Associates, Inc. 999 Fourier Drive, Suite 201 Madison, Wisconsin 53717

RE: Approval of a request to rezone 4846 Eastpark Boulevard from SEC (Suburban Employment Center District) to CC-T (Commercial Corridor—Transitional District) and to TR-U2 (Traditional Residential—Urban 2 District); and approving the preliminary plat and final plat of *The American Center Eastpark Fifth Addition*, creating two lots for future mixed-use development, seven lots for future multi-family development, one outlot for private parkland, and one outlot to be dedicated to the public for stormwater management. [ID 75182 and 74911; LNDSPP-2022-00005]

Dear Mike;

At its February 7, 2023 meeting, the Common Council **approved** the rezoning and preliminary and final plat of *The American Center Eastpark Fifth Addition* subject to the conditions of approval in the following sections, which shall be addressed prior to final approval and recording of the final plat.

Please contact Brenda Stanley of the City Engineering Division at (608) 261-9127 if you have questions regarding the following twenty-one (21) items:

- 1. A Phase 1 environmental site assessment (per ASTM E1527-13), is required for lands dedicated to the City. Provide one (1) digital copy to Brynn Bemis ((608) 267-1986, bbemis@cityofmadison.com).
- 2. The City has limited sewer capacity in this area (particularly to the north of the plat). Off-site sanitary sewer improvements may be required by the developer as a condition for development. The applicant shall provide Mark Moder (608) 261-9250, mmoder@cityofmadison.com, with projected wastewater flow data and the direction for which the flow will be directed.
- 3. The streets being constructed as part of the proposed plat are required by NR-151 to be treated for stormwater management. It is unclear how that requirement will be met. The applicant shall provide the City Engineer with a stormwater management plan that documents the intent to meet this requirement.
- 4. Enter into a City / Developer agreement for the required infrastructure improvements. The agreement shall be executed prior to sign-off. Allow 4-6 weeks to obtain agreement. Contact the City Engineering Division to schedule thedevelopment and approval of the plans and the agreement.

- 5. Construct Madison standard street, multi-use path, and sidewalk improvements for all streets within the plat.
- 6. Make improvements to Eastpark Boulevard and S Biltmore Lane in order to facilitate ingress and egress to the development. The improvements shall include medians, street widening, bike lanes and modified driveways as required by City Traffic Engineer.
- 7. Construct Dreamer Drive from the eastern plat terminus to S Biltmore Lane.
- 8. Construct public sanitary sewer, storm sewer, and drainage improvements as necessary to serve the lots within the plat.
- Madison Metropolitan Sewerage District (MMSD) charges are due and payable prior to City Engineering Division sign-off, unless otherwise collected with a Developer's / Subdivision Contract. Contact Mark Moder (261-9250) toobtain the final MMSD billing a minimum of two (2) working days prior to requesting City Engineering Division sign-off.
- 10. A minimum of two (2) working days prior to requesting City Engineering signoff on the plat, the applicant shall contact either Tim Troester at 261-1995 (ttroester@cityofmadison.com) or Brenda Stanley at 261-9127 (bstanley@cityofmadison.com) to obtain the final stormwater utility charges that are due and payable prior to sub-division of the properties. The stormwater utility charges (as all utility charges) are due for the previous months of service. All charges shall be cleared prior to the land division (and subsequent obsolesces of the existing parcel).
- 11. An Erosion Control Permit is required for this project.
- 12. A Storm Water Management Report and Storm Water Management Permit is required for this project.
- 13. A Storm Water Maintenance Agreement (SWMA) is required for this project.
- 14. This site appears to disturb over one (1) acre of land and requires a permit from the Wisconsin Department of Natural Resources (WDNR) for stormwater management and erosion control. The City of Madison has been required by the WDNR to review projects for compliance with NR-216 and NR-151 however a separate permit submittal is still required to the WDNR for this work. The City of Madison cannot issue our permit until concurrence is obtained from the WDNR via their NOI or WRAPP permit process. Contact Eric Rortvedt at 273-5612 of the WDNR to discuss this requirement. The applicant is notified that the City of Madison is an approved agent of the Department of Safety and Professional Services (DSPS) and no separate submittal to this agency or Capital Area Regional Planning Commission (CARPC) is required for this project to proceed.
- 15. Submit a soil boring report that has been prepared by a Professional Engineer two weeks prior to recording the final plat to the City Engineering Division indicating the ground water table and rock conditions in the area. If the report indicates a ground water table or rock condition less than nine (9) feet below proposed street grades, a restriction shall be added to the final plat, as determined necessary by the City Engineer.

- 16. Confirm that adequate sight distance exists where streets intersect per AASHTO design standards for intersection sight distance. If adequate sight distance does not exist, change the location of the street intersection or agree to make improvements to the roadways such that the sight distance is achieved or make mitigating improvements as required by the City. Note that the improvements indicated may require right of way outside of the plat limits.
- 17. This project will disturb 20,000 square feet or more of land area and require an Erosion Control Plan. Please submit an11" x 17" copy of an erosion control plan (pdf electronic copy preferred) to Megan Eberhardt (west) at meberhardt@cityofmadison.com, or Daniel Olivares (east) at daolivares@cityofmadison.com, for approval.
- 18. Demonstrate compliance with MGO Section 37.07 and 37.08 regarding permissible soil loss rates. Include Universal Soil Loss Equation (USLE) computations for the construction periodwith the erosion control plan. Measures shall be implemented in order to maintain a soil loss rate below 5.0 tons per acre per year.
- 19. Complete weekly self-inspection of the erosion control practices and post these inspections to the City of Madison website as required by MGO Chapter 37.
- 20. Prior to approval, this project shall comply with Chapter 37 of the Madison General Ordinances regarding stormwater management. Specifically, this development is required to submit a Storm Water Management Permit application, associated permit fee, Stormwater Management Plan, and Storm Water Management Report to City Engineering. The Stormwater Management Permit application can be found on City Engineering's website. The Storm Water Management Plan & Report shall include compliance with the following:

Submit prior to plan sign-off, a stormwater management report stamped by a P.E. registered in the State of Wisconsin.

Provide electronic copies of any stormwater management modeling or data files including SLAMM, RECARGA, TR-55, HYDROCAD, Sediment loading calculations, or any other electronic modeling or data files. If calculations are done by hand or are not available electronically, the hand copies or printed output shall be scanned to a PDF file and provided to City Engineering. (POLICY and MGO 37.09(2))

Detain the 2-, 5-, 10-, 100-, and 200-year storm events, matching post-development rates to predevelopment rates and using the design storms identified in MGO Chapter 37.

Provide infiltration of 90% of the pre-development infiltration volume.

Reduce TSS by 80% (control the 5-micron particle) off of newly developed areas compared to no controls.

The applicant shall demonstrate that water can leave the site and reach the public ROW without impacting structures during a 100-year event storm. This analysis shall include reviewing overflow elevations and unintended storage occurring on site when the storm system has reached capacity.

Submit a draft Stormwater Management Maintenance Agreement (SWMA) for review and approval that covers inspection and maintenance requirements for any BMP used to meet stormwater

management requirements on this project.

21. Submit, prior to plan sign-off but after all revisions have been completed, digital PDF files to the City Engineering Division. E-mail PDF file transmissions are preferred to: bstanley@cityofmadison.com (East) or ttroester@cityofmadison.com (West).

Please contact Jeff Quamme of the City Engineering Division–Mapping Section at (608) 266-4097 if you have any questions regarding the following twenty-two (22) items:

- 22. The portions of the Public Utility Easements that are within the any area being dedicated to the public for streetpurposes shall be released by the Private Public Utilities. They also shall be released in a separate document prepared by City Office of Real Estate Services. Contact Jeff Quamme of Engineering Mapping (jrquamme@cityofmadison.com, 266-4097) to coordinate the Real Estate project, and associated information and fees required. The releases shall be required prior to recording of the plat, acknowledgement of the release and document number shall be noted on the face of the plat.
- 23. The plat is located in two different school districts. Therefore, Lots 52 and 53 will be split and parceled by the Assessor's Office along the school district line. In order to recombine the parcels into the platted lot, a petition to the School District will be needed in order to place the square footage all in one school district.
- 24. In accordance with Chapter 236, Wisconsin Statutes, the applicant must show the type, location and width of any and all easements on a plat or CSM. Clearly identify the difference between existing easements (cite recording data) and easements which are being conveyed by the land division. Identify the owner and/or benefiting interest of all easements. Include any and all language required to properly and legally create any easement by this subdivision.
- 25. Remove the private sign easement from Outlot 21. Private use of public lands is to be avoided.
- 26. Dimension the vision corners from the property corners and place text in the labels directing to the text having the specifics of the restriction.
- 27. The Stormwater Easement in the southwest corner of Lot 47 will be a Public Storm Sewer Easement granted by the plat of the Fourth Addition. It will also extend to the east right of way of Eastpark Blvd. The note shall be modified accordingly and shall reference the Fourth Addition plat that is granting the easement.
- 28. The Private Storm Water Easements shown in the northeasterly corner of Lot 51 and the northerly side of Lot 53 treat public street stormwater, and therefore will be granted by the Fourth Addition as Public Stormwater Management Easements. Modify the notes accordingly to acknowledge the grant as per the plat of the Fourth Addition.
- 29. Insert standard language pre MGO 16.23(9)(d)2.a. verbatim. When done, all of the previous non-exclusive easements for drainage easements are released and new drainage easements defined by the current proposed plat.

- 30. Note: Comprehensive reciprocal easements and agreements including, but not limited to, access, parking, utilities, common areas, storm management that are necessary in the future to accomplish the future proposed site development within any of the lots within this plat will be required for any site development approval.
- 31. The Public Stormwater Easement at the westerly corner of Lot 55 will be a Private Storm Sewer Easement in favor of Outlot 19 of the Fourth Addition granted on the same plat. The easement shall include terms and conditions of construction, maintenance, repair and replacement.
- 32. Add a label to adjacent Outlot 20 that it is reserved for future public right of way per the Fourth Addition.
- 33. Currently only a portion of the private Drainage Easement per Document No. 5778376 is being released as part of the Fourth Addition. Public drainage rights are also to be granted as a condition of approval for the 4th Addition over the easterly side of Lots 51 53. The release and new easement shall be referenced on this plat. Coordinate with the consultant that is completing the plat of the Fourth Addition.
- 34. Notes containing the title information from the exceptions noted on the Preliminary Plat for Exceptions 10, 12, 16 and 17 shall be added to the final plat. Also on the map and in title correct Document Nos. 2379030 to 2379020.
- 35. The Owner's Certificate and Consent of Mortgagee Certificate shall be correctly updated with the required title update prior to final sign-off.
- 36. The measured coordinates for the Public Land and Meander Corners shall be provided. Currently, only record coordinates are shown and not measured values.
- 37. Wisconsin Administrative Code A-E 7.08 identifies when Public Land System (PLS) tie sheets must be filed with the Dane County Surveyor's office. The developer's surveyor and/or applicant must submit copies of required tie sheets or monument condition reports for all monuments, including center of sections of record, used in this survey, to Jeff Quamme, City Engineering (jrquamme@cityofmadison.com). If a new tie sheet is not required under A-E 7.08, Engineering requests a copy of the latest tie sheet on record with Dane County Surveyor's office and shall be attached to a signed and sealed monument condition report. The applicant shall identify monument types on all PLS corners included on the plat.
- 38. Prior to City Engineering Division final sign-off by main office, the final plat shall be submitted in PDF format by email transmittal to Engineering Land Records Coordinator Jeff Quamme (jrquamme@cityofmadison.com) for final technical review and approval. This submittal must occur a minimum of two working days prior to final City Engineering Division sign-off.
- 39. Conditions of approval noted herein are not intended to be construed as a review determining full compliance with Madison General Ordinances and State of Wisconsin Statutes. The licensed

professional preparing the land division is fully responsible for full compliance with all ordinances and statutes regulating this proposed subdivision.

- 40. The required location map on the plat is missing.
- 41. This pending plat application for this property shall be completed and recorded with the Dane County Register of Deeds (ROD), the new parcel data created by the Assessor's Office and the parcel data available to Zoning and Building Inspection staff prior to issuance of building permits for new construction or early start permit.
- 42. The north road segment will have a name of Luminous Lane. The south road segment will have a name of Jacks Lane.
- 43. Submit to Jeff Quamme, prior to Engineering sign-off of the subject plat, one (1) digital CADD drawing in a format compatible with AutoCAD. The digital CADD file(s) shall be referenced to the Dane County Coordinate System and shall contain, at minimum, the list of items stated below, each on a separate layer/level name. The line work shall be void of gaps and overlaps and match the final recorded plat: right of way lines (public and private); lot lines; lot numbers; lot/plat dimensions; street names, and; easement lines (including wetland and floodplain boundaries).
 - *This transmittal is a separate requirement from the required Engineering Streets Section for design purposes. The Developer/Surveyor shall submit new updated final plat, electronic data for any changes subsequent to any submittal.

Please contact Sean Malloy of the Traffic Engineering Division at (608) 266-5987 if you have any questions regarding the following two (2) items:

- 44. Final public right of way configuration and design along with site plans are not approvable through the plat approval process. The applicant is showing, in their supplemental plans, a proposed 42-foot wide street, which is not supported by Traffic Engineering.
- 45. The applicant shall execute and return a declaration of conditions and covenants for streetlights prior to sign-off.

Please contact Bill Sullivan of the Madison Fire Department at (608) 261-9658 if you have any questions regarding the following item:

46. The Madison Fire Department does not object to the overall design of the project. It is the design team's responsibility to ensure the buildings comply with all applicable building and fire codes as well as all Madison General Ordinances.

Please contact Jeff Belshaw of the Madison Water Utility at (608) 261-9835 if you have any questions regarding the following three (3) items:

- 47. Submit a Site Utility Plan indicating the proposed water main throughout the development.
- 48. The developer shall construct the public water distribution system and services required to serve the proposed subdivision plat per MGO 16.23(9)(d)(3).
- 49. All public water mains and water service laterals shall be installed by a standard City subdivision contract / City- Developer agreement. Applicant shall contact City Engineering Division to schedule the development of plans and the agreement. See Engineering Division comments for additional information.

Please contact Tim Sobota of Metro Transit at (608) 261-4289 if you have any questions regarding the following three (3) items:

- 50. In coordination with public works improvements, the applicant shall install and maintain an accessible concrete boarding pad surface at the planned bus stop on the east side of Eastpark Boulevard, north of the Dreamer Drive intersection.
- 51. In coordination with public works improvements, the applicant shall install and maintain an accessible concrete boarding pad surface at the planned bus stop on the west side of Eastpark Boulevard, south of the Inspire Drive intersection.
- 52. The applicant shall install and maintain a new passenger waiting shelter with seating amenity either as part of the private landscape plan or in the public right-of-way area serving the curbside bus stop zone on the west side of Eastpark Boulevard, south of the Inspire Drive intersection area. If located in the public right-of-way, the applicant shall submit a Privilege in Streets (Bus Shelter) application for review by the City. An approved Encroachment Agreement, for the bus shelter, shall be executed prior to sign off. Contact City Real Estate to start the Privilege in Streets (Bus Shelter) application process. The applicant shall include the location of these transit amenities on the final documents filed with their permit application so that Metro Transit may review the design.

Please contact Kathleen Kane of the Parks Division at (608) 261-9671 if you have any questions regarding the following five (5) items:

- 53. Park Impact Fees (comprised of the Park Infrastructure Impact Fee, per MGO Sec. 20.08(2)), and Park-Land Impact Fees, per MGO Sec. 16.23(8)(f) and 20.08(2) will be required for all new residential development associated with this project. This development is within the North Park-Infrastructure Impact Fee district. Please reference ID# 20055 when contacting Parks Division staff about this project.
- 54. The plat, as currently proposed, includes two lots for future mixed-use development; seven lots for future multi-family development; one outlot for public stormwater management; and one outlot for private open space with public-access easement to occur over several phases of construction. The parkland dedication requirement for a single family / duplex unit is 1,081 square feet and for a multi-family unit is 734 square feet per MGO Sec. 16.23(8)(f) and 20.08(2).

- 55. Outlot 22 is described on the plat as "to be used as a private park" and is +/- 2.0851 acres (90,828 square feet) with a public access easement. Depending on final unit counts and as proposed, Outlot 22 is insufficient in size to fully meet the parkland dedication requirements and park land dedication fees ("fee in lieu") will be due for the proposed development.
- 56. Prior to sign off on the final plat, the applicant shall execute a declaration of conditions and covenants along with an impact fee schedule for the park impact fees for this development. This document will be recorded at the Register of Deeds. The applicant shall be responsible for all recording fees.
- 57. The following note should be included on the final plat: "Lots within this subdivision are subject to impact fees that are due and payable at the time building permit(s) are issued." The Parks Division shall be required to sign-off on this subdivision.

Please contact Jenny Frese of the Office of Real Estate Services at (608) 267-8719 if you have any questions regarding the following five (5) items:

- 58. Prior to approval sign-off, the Owner's Certificate(s) on the final plat shall be executed by all parties having an interest in the property, pursuant to Wis. Stats. 236.21(2)(a). Certificates shall be prepared with the ownership interests consistent with the most recent title report. Signatories shall provide documentation that proves legal authority to sign the Owner's Certificate. The executed original hard stock recordable plat shall be presented at the time of sign-off.
- 59. A certificate of consent for all mortgagees/vendors shall be included following the Owner's Certificate(s) and executed prior to plat approval sign-off. If mortgages of record are paid off prior to plat approval, a copy of the recorded satisfaction for said mortgage shall be provided prior to sign-off.
- 60. Under 236.21(3) Wis. Stats. and MGO Section 16.23(5)(g)(1), the property owner shall pay all real estate taxes that are accrued or delinquent for the subject property prior to plat recording. This includes property tax bills for the prior year that are distributed at the beginning of the year.
- 61. Pursuant to MGO Section 16.23(5)(e)1 and Wis. Stats. 236.21(3), all special assessments levied prior to plat signoff shall be paid in full.
- 62. Pursuant to MGO Section 16.23(5)(g)(4), provide to the Office of Real Estate Services and the surveyor preparing the plat, an updated title report covering the period between the date of the initial title report and the date when sign-off of the final plat is requested. A title commitment may be provided, but will only be considered as supplementary information to the title report update. The surveyor shall update the plat with the most recent information available in the title report update.

Please contact my office at (608) 261-9632 if you have questions about the following three (3) items:

63. The applicant shall work with the property owner to provide an updated/revised preliminary plat for the overall approximately 825-acre *American Center* development generally bounded by Hoepker Road on the north, American Parkway on the east, US Highway 151 on the south, and Interstate 39-

90-94 on the west that reflects the subdivision of the 24-acre subject property into the lots and outlots shown on the concurrent final plat.

- 64. That prior to final approval and recording of the final plat, the applicant shall demonstrate compliance with the requirements in Section 16.23(8)(f)8 of the Subdivision Regulations for privately-owned and maintained open space, including the execution of land use restrictions and open space easements in a form acceptable to the Parks Division, Planning Division, and City Attorney's Office.
- 65. Note: Recording of The American Center Eastpark Fifth Addition cannot occur until The American Center Eastpark Fourth Addition has been granted final City approvals and been recorded at the Dane County Register of Deeds.

Specific questions regarding the comments or conditions contained in this letter should be directed to the commenting agency.

Any appeal regarding the plat, including the conditions of approval related thereto, must be filed with the Circuit Court within thirty (30) days from the date of this letter. If I may be of any further assistance, please do not hesitate to contact me at (608) 261-9632 or tparks@cityofmadison.com.

Sincerely,

Timothy M. Parks

Timothy MParks

Planner

cc: Greg Held, Knothe & Bruce Architects, LLC
Brenda Stanley, City Engineering Division
Fadi El Musa Gonzalez, City Engineering Division
Jeff Quamme, City Engineering Division – Mapping Section
Sean Malloy, Traffic Engineering Division
Jenny Kirchgatter, Assistant Zoning Administrator
Bill Sullivan, Madison Fire Department
Kathleen Kane, Parks Division
Tim Sobota, Metro Transit
Jeff Belshaw, Madison Water Utility
Jenny Frese, Office of Real Estate Services



Department of Planning & Community & Economic Development

Planning Division

William Fruhling, Interim Director

Madison Municipal Building, Suite 017 215 Martin Luther King Jr. Boulevard Madison, Wisconsin 53703 Phone: (608) 266-4635 www.cityofmadison.com

TO: Plan Commission

FROM: Timothy M. Parks, Planner

DATE: July 8, 2024

SUBJECT: ID <u>83902</u> – Re-approving the final plat of *The American Center Eastpark Fifth Addition*

subdivision on land generally addressed as 4846 Eastpark Boulevard (District 17).

On February 7, 2023, the Common Council conditionally approved applications by Luke Stauffacher of Cascade Development for approval of a request to rezone 4846 Eastpark Boulevard from SEC (Suburban Employment Center District) to CC-T (Commercial Corridor—Transitional District) and to TR-U2 (Traditional Residential—Urban 2 District) and for approval of the preliminary plat and final plat of *The American Center Eastpark Fifth Addition* subdivision. The subdivision creates two lots for future mixed-use development, seven lots for future multi-family development, one outlot for private parkland, and one outlot to be dedicated to the public for stormwater management from 24 acres of undeveloped land. The February 14, 2023 approval letter and final plat are attached to file ID 83902.

Wis. Stats. Section 236.25(2)(b) provides twelve (12) months to record a final plat following its last approval. The approved final plat was not recorded before February 14, 2024 (one year from the date of the 2023 approval letter), so the previous final plat approval is expired.

No changes to the subdivision are proposed, and the conditions of approval in 2023 approval would be carried forward with the requested re-approval. The project team anticipates recording the final plat soon after the resolution re-approving the plat is adopted.

The Planning Division and reviewing agencies recommend that the Plan Commission forward Resolution ID 83902, re-approving the final plat of *The American Center Eastpark Fifth Addition*, to the Common Council with a recommendation of **approval**, subject to input at the public hearing and the conditions contained in the attached February 14, 2023 approval letter.

cc: Mike Ziehr, Vierbicher & Associates, Inc. (by e-mail only)



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83934

File ID: 83934 File Type: Claim Status: Report of Officer

Version:1Reference:Controlling Body:Risk Manager

File Created Date: 06/13/2024

File Name: CLAIM: B. Coon - Property Damage - \$104.34 Final Action:

Title: B. Coon - Property Damage - \$104.34

Notes: EVL005634

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	COMMON COUR	NCIL 07/02/2024 This Claim was Referred	Referred I to the Risk Manager	Risk Manager		07/10/2024	
1	Risk Manager	07/10/2024	RECOMMEND TO COUNCIL TO DISALLOW - REPORT OF OFFICER				
	Action Text:	This Claim was RECOM	MEND TO COUNCIL T	O DISALLOW - REPORT	OF OFFICER		

Text of Legislative File 83934

Title

B. Coon - Property Damage - \$104.34

Body

Claim received 6/7/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83937

File ID: 83937 File Type: Claim Status: Report of Officer

Version: 1 Reference: Controlling Body: Risk Manager

File Created Date: 06/13/2024

File Name: CLAIM: J. Vendette - Vehicle Damage - \$227.00 Final Action:

Title: J. Vendette - Vehicle Damage - \$227.00

Notes: EVL005638

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	COMMON COUR	NCIL 07/02/2024 This Claim was Referred	Referred I to the Risk Manager	Risk Manager		07/10/2024	
1	Risk Manager	07/10/2024	RECOMMEND TO COUNCIL TO DISALLOW - REPORT OF OFFICER				
	Action Text:	This Claim was RECOM	MEND TO COUNCIL T	O DISALLOW - REPORT	OF OFFICER		

Text of Legislative File 83937

Title

J. Vendette - Vehicle Damage - \$227.00

Body

Claim received 6/10/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83981

File ID: 83981 File Type: Claim Status: Report of Officer

Version:1Reference:Controlling Body:Risk Manager

File Created Date: 06/18/2024

File Name: CLAIM: H. Frank - Property Damage - \$2,101.99 Final Action:

Title: H. Frank - Property Damage - \$2,101.99

Notes: EVL005647

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	COMMON COUR	NCIL 07/02/2024 This Claim was Referred		Risk Manager		07/10/2024	
1	Risk Manager	07/10/2024	RECOMMEND TO COUNCIL TO DISALLOW - REPORT OF OFFICER				
	Action Text:	This Claim was RECOM	MEND TO COUNCIL T	O DISALLOW - REPORT	OF OFFICER		

Text of Legislative File 83981

Titla

H. Frank - Property Damage - \$2,101.99

Body

Claim received 6/16/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83984

File ID: 83984 File Type: Claim Status: Report of Officer

Version: 1 Reference: Controlling Body: Risk Manager

File Created Date: 06/18/2024

File Name: CLAIM: S. Maughan - Property Damage - \$400.00 Final Action:

Title: S. Maughan - Property Damage - \$400.00

Notes: EVL005650

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	COMMON COUN	NCIL 07/02/2024 This Claim was Referred		Risk Manager		07/10/2024	
1	Risk Manager	07/10/2024	RECOMMEND TO COUNCIL TO DISALLOW - REPORT OF OFFICER				
	Action Text:	This Claim was RECOM	MEND TO COUNCIL T	O DISALLOW - REPORT	OF OFFICER		

Text of Legislative File 83984

Titla

S. Maughan - Property Damage - \$400.00

Body

Claim received 6/14/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83988

File ID: 83988 File Type: Claim Status: Report of Officer

Version: 1 Reference: Controlling Body: Risk Manager

File Created Date: 06/18/2024

File Name: CLAIM: K. Kufahl - Property Damage - \$1,000.00 Final Action:

Title: K. Kufahl - Property Damage - \$1,000.00

Notes: EVL005652

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	COMMON COUN	NCIL 07/02/2024 This Claim was Referred		Risk Manager		07/10/2024	
1	Risk Manager	07/10/2024	RECOMMEND TO COUNCIL TO DISALLOW - REPORT OF OFFICER				
	Action Text:	This Claim was RECOM	MEND TO COUNCIL T	O DISALLOW - REPORT	OF OFFICER		

Text of Legislative File 83988

Title

K. Kufahl - Property Damage - \$1,000.00

Body

Claim received 6/17/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83994

File ID: 83994 File Type: Claim Status: Report of Officer

Version:1Reference:Controlling Body:Risk Manager

File Created Date: 06/18/2024

File Name: CLAIM: Ryan Funeral Home - Property Damage - Final Action:

\$36,038.33

Title: Ryan Funeral Home - Property Damage - \$36,038.33

Notes: EVL00564=54

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	COMMON COUN	NCIL 07/02/2024 This Claim was Referred	Referred to the Risk Manager	Risk Manager		07/10/2024	
1	Risk Manager	07/10/2024	RECOMMEND TO COUNCIL TO DISALLOW - REPORT OF OFFICER				
	Action Text:	This Claim was RECOM	MEND TO COUNCIL	TO DISALLOW - REPOR	RT OF OFFICER		

Text of Legislative File 83994

Title

Ryan Funeral Home - Property Damage - \$36,038.33

Body

Claim received 6/17/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84121

File ID: 84121 File Type: Claim Status: Report of Officer

Version:1Reference:Controlling Body:Risk Manager

File Created Date: 06/21/2024

File Name: CLAIM: T. Goodwin - Property Damage - \$1,107.00 Final Action:

Title: T. Goodwin - Property Damage - \$1,107.00

Notes: EVL005664

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	COMMON COUR	NCIL 07/02/2024 This Claim was Referred	Referred I to the Risk Manager	Risk Manager		07/10/2024	
1	Risk Manager	07/10/2024	RECOMMEND TO COUNCIL TO DISALLOW - REPORT OF OFFICER				
	Action Text:	This Claim was RECOM	MEND TO COUNCIL T	O DISALLOW - REPORT	OF OFFICER		

Text of Legislative File 84121

Title

T. Goodwin - Property Damage - \$1,107.00

Body

Claim received 6/18/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84127

File ID:84127File Type:ClaimStatus:Report of Officer

Version: 1 Reference: Controlling Body: Risk Manager

File Created Date: 06/21/2024

File Name: CLAIM: V. Quach - Vehicle Damage - \$5,801.40 Final Action:

Title: V. Quach - Vehicle Damage - \$5,801.40

Notes: EVL005665

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	COMMON COUN	NCIL 07/02/2024 This Claim was Referred		Risk Manager		07/10/2024	
1	Risk Manager	07/10/2024	RECOMMEND TO COUNCIL TO DISALLOW - REPORT OF OFFICER				
	Action Text:	This Claim was RECOM	MEND TO COUNCIL T	O DISALLOW - REPORT	OF OFFICER		

Text of Legislative File 84127

Title

V. Quach - Vehicle Damage - \$5,801.40

Body

Claim received 6/19/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84131

File ID: 84131 File Type: Claim Status: Report of Officer

Version: 1 Reference: Controlling Body: Risk Manager

File Created Date: 06/21/2024

File Name: CLAIM: E. Bemben of Donegal Group for Ryan Final Action:

Funeral Home - Property Damage - \$25,000.00

Title: E. Bemben of Donegal Group for Ryan Funeral Home - Property Damage -

\$25,000.00

Notes: EVL005654

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	COMMON COUL Action Text:	NCIL 07/02/2024 This Claim was Referred		Risk Manager		07/10/2024	
1	Risk Manager	07/10/2024	RECOMMEND TO COUNCIL TO DISALLOW - REPORT OF OFFICER				
	Action Text:	This Claim was RECOM	MEND TO COUNCIL	TO DISALLOW - REPOR	T OF OFFICER		

Text of Legislative File 84131

Title

E. Bemben of Donegal Group for Ryan Funeral Home - Property Damage - \$25,000.00

Body

Claim received 6/21/2024.



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84220

File ID: 84220 File Type: Claim Status: Report of Officer

Version: 1 Reference: Controlling Body: Risk Manager

File Created Date: 06/28/2024

File Name: CLAIM: S. Nelson w/ Acuity - Property Damage - Final Action:

\$35,000.00

Title: S. Nelson w/ Acuity - Property Damage - \$35,000.00

Notes: EVL005688

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Risk Manager	07/10/2024	RECOMMEND TO COUNCIL TO DISALLOW - REPORT OF OFFICER				
	Action Text:	This Claim was RECOM	MEND TO COUNCIL	TO DISALLOW - RE	PORT OF OFFICER		

Text of Legislative File 84220

Title

S. Nelson w/ Acuity - Property Damage - \$35,000.00

Body

Claim received 6/21/2024.



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 83820

File ID: 83820 File Type: Resolution Status: Unfinished

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 06/05/2024

File Name: Final Action:

Title: Authorizing the Mayor to sign a Memorandum of Understanding with the

President and Fellows of Harvard College acting through the Bloomberg Harvard

City Leadership Initiative at the John F. Kennedy School of Government ("Harvard") for the placement of a Bloomberg Harvard City Hall Fellow in the

Mayor's Office

Notes:

Sponsors: Satya V. Rhodes-Conway, John P. Guequierre, Nikki **Effective Date:**

Conklin And MGR Govindarajan

Attachments: Enactment Number:

Author: Jessica Price Hearing Date:

Entered by: jprice2@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Mayor's Office	06/05/2024	Referred for Introduction				
	Action Text:	This Resolution was Ref	erred for Introduction				
	Notes:	Common Council (7/16/24)					
1	COMMON COUN	ICIL 07/02/2024	Refer to a future	COMMON			Pass
			Meeting to Adopt	COUNCIL			
	Action Text:		Figueroa Cole, seconded by meeting of 7/16/24. The			Adopt to	
	Notes:	Adopt 7/16/24	· ·	,			

Text of Legislative File 83820

Fiscal Note

No appropriation is required.

Title

Authorizing the Mayor to sign a Memorandum of Understanding with the President and Fellows of Harvard College acting through the Bloomberg Harvard City Leadership Initiative at the John F. Kennedy School of Government ("Harvard") for the placement of a Bloomberg Harvard City

Hall Fellow in the Mayor's Office

Body

WHEREAS, the Bloomberg Harvard City Leadership Initiative at the John F. Kennedy School of Government administers the Bloomberg Harvard City Hall Fellow program; and

WHEREAS, the Bloomberg Harvard City Hall Fellowship program enables Harvard University graduates to work with cities nationwide for two (2) years on a specific agreed upon priority area; and

WHEREAS, the Bloomberg Harvard City Hall Fellowship program provides fellows to cities at no cost to the city; and

WHEREAS, the City of Madison ("City"), through an application by the Sustainability and Resilience Program in the Mayor's Office, was selected as a participant city in the program; and

WHEREAS, participating in the fellowship program incurs no cost to the City, and the fellow is not compensated by the City.

NOW, THEREFORE, BE IT RESOLVED, that the Mayor is authorized to sign a Memorandum of Understanding ("MOU") for up to 5 years to authorize the placement of a fellow for the period August 26, 2024 through August 25, 2026 with the President and Fellows of Harvard College acting through the Bloomberg Harvard City Leadership Initiative at the John F. Kennedy School of Government ("Harvard") for the placement of a Bloomberg Harvard City Hall Fellow in the Mayor's Office to work with the Sustainability and Resilience program.



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84168

File ID:84168File Type:ResolutionStatus:Unfinished

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 06/25/2024

Final Action:

Published Date:

File Name: Authorizing the Planning Division to continue the

Artist at Work program, focusing on destination marketing for Madison's music sector, and to select and contract non-profit partners as grantees.

Title: Authorizing the Planning Division to continue the Artist at Work program, focusing

on destination marketing for Madison's music sector, and to select and contract

non-profit partners as grantees.

Notes:

Sponsors: Marsha A. Rummel Effective Date:

Attachments: Enactment Number:

Author: kwolf@cityofmadison.com Hearing Date:

History of Legislative File

Entered by: kwolf@cityofmadison.com

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1		06/25/2024 esolution was Ref n Council (7/16/24)	Referred for Introduction				
1	COMMON COUNCIL	07/02/2024	Refer to a future Meeting to Adopt	COMMON COUNCIL			Pass
		,	Figueroa Cole, seconde L meeting of 7/16/24. T	,	0	Adopt to	

Text of Legislative File 84168

Adopt 7/16/24

Notes:

Fiscal Note

The proposed resolution authorizes the Planning Division Director to execute agreements and expenses for the 2024 Artist at Work program in the total amount of \$75,000. The 2024 Adopted Room Tax Fund Budget included a \$75,000 allocation for the Artist at Work program. No additional City appropriation required.

Title

Authorizing the Planning Division to continue the Artist at Work program, focusing on destination marketing for Madison's music sector, and to select and contract non-profit partners as grantees.

Body

WHEREAS, to support a full creative life for all, the Madison Arts Commission (MAC) commits to championing policies and practices of cultural equity that empower a just, inclusive, equitable City; and

WHEREAS, the COVID19 pandemic caused sudden and overwhelming losses of income for cultural sector workers and a steep decline in revenue opportunities for the sector as a whole; and

WHEREAS, on February 22, 2022, the Common Council approved a resolution RES-22-00138, File ID #69226 to establish the Artist at Work grant program; and

WHEREAS, the Greater Madison Music City (GMMC) team was created as a result of the Task Force on Equity in Music and Entertainment Report, which was accepted by the Common Council on February 26, 2019, under the resolution RES-19-00159, and file #53956; and

WHEREAS, the City of Madison Arts Program is an active member of the Greater Madison Music City team led by the Urban Community Arts Network, LTD (UCAN); and

WHEREAS, the Room Tax Commission allocated \$75,000 of Room Tax funds in the 2024 budget to the City's Arts program to continue support for Artist at Work grant program that specifically focus on promotion and marketing to increase Madison's music sector tourism; and

WHEREAS, the 2024 Room Tax allocation of \$75,000 will help support creative workers, continue the work of the GMMC in implementing their music tourism recovery plan, and market Madison's music sector; and

WHEREAS, Madison's grant program, Artist at Work, has identified four (4) non-profit arts organizations, Destination Madison Foundation, Inc., Make Music Madison, Inc., Overture Center Foundation, Inc, and Urban Community Arts Network, LTD., who have successfully established sustainable positions for creative workers in the music sector and are committed to the goals of the GMMC; and

WHEREAS, \$10,000 of the Room Tax fund allocation will be reserved for the implementation of marketing strategies as GMMC progresses in establishing a brand for Madison's music sector, including, but not limited to, testing the new brand, community engagement, creation of a logo;

NOW, THEREFORE, BE IT RESOLVED, that the Planning Division is authorized to contract with Destination Madison Foundation, Inc. for \$15,000, Make Music Madison, Inc. for \$15,000, Overture Center Foundation, Inc. for \$15,000, and Urban Community Arts Network, LTD for \$20,000 to support positions that promote Madison as a music destination, and use \$10,000 towards testing branding, marketing, and community engagement for Madison's music sector;

BE IT FINALLY RESOLVED, that these Artist at Work grants and marketing expenses will be awarded and administered by the Madison Arts program staff, and authorizes the Planning Division Director to execute the agreements for such projects, upon countersignature of the

Finance Director and approval of the City Attorney.



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84329

File ID: 84329 File Type: Ordinance Status: Council New

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 07/05/2024

File Name: TR-C4Dimensional Minimums Final Action:

Title: Amending Section 28.045 of the Madison General Ordinances to change

minimum dimensional requirements in the TR-C4 District

Notes: 6882TRC4DimensionalMinimums

Sponsors: Tag Evers Effective Date:

Attachments: 84329Body.pdf Enactment Number:

Author: Kate Smith Hearing Date:

Entered by: mglaeser@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Attorney's Office	07/05/2024	Referred for				
	Action Text:	This Ordinance was Ref		ction			
	Notes:	Plan Commission- Public H	earing (8/26/24) ,C	common Council (9/10/24)			

Text of Legislative File 84329

Fiscal Note

Fiscal note pending.

Title

Amending Section 28.045 of the Madison General Ordinances to change minimum dimensional requirements in the TR-C4 District

Body

DRAFTER'S ANALYSIS: This proposed ordinance change amends the minimum setback, lot area and lot width requirements for two-family twin and three-unit buildings in the TR-C4 District. This change is similar to Legistar File 78690 which made similar changes to TR-V1, TR-V2, TR-U1, TR-U2, Downtown Residential 1 and Downtown Residential 2 Districts that was adopted by the Common Council on 8/1/2023. Like 78690, this proposed set of changes would create consistency for permissible residential uses and align bulk standards with the existing built environment.

Please see Legistar File No. 84329 Body in Attachments.

Legistar File No. 84329 Body

DRAFTER'S ANALYSIS: This proposed ordinance change amends the minimum setback, lot area and lot width requirements for two-family twin and three-unit buildings in the TR-C4 District. This change is similar to Legistar File 78690 which made similar changes to TR-V1, TR-V2, TR-U1, TR-U2, Downtown Residential 1 and Downtown Residential 2 Districts that was adopted by the Common Council on 8/1/2023. Like 78690, this proposed set of changes would create consistency for permissible residential uses and align bulk standards with the existing built environment.

The Common Council of the City of Madison do hereby ordain as follows:

1. Subsection (2) entitled "Dimensional Requirements, Permitted and Conditional Uses" of Section 28.045 entitled "TR-C4 District" of the Madison General Ordinances is amended as follows:

"(2) <u>Dimensional Requirements, Permitted and Conditional Uses.</u>

Requirements represent minimums unless otherwise noted. Dimensions are in feet unless otherwise noted.

TR-C4 District							
	Single- family detached	Two- family Two unit	Two- family Twin	Three- unit	Nonresidential		
Lot Area (sq. ft.)	4,000	4,000	3,000 <u>2,000</u> /d.u.	6,000 <u>4,000</u>	4,800		
Lot Width	40	40	25 <u>20</u> /d.u.	40	40		
Front Yard Setback	20	20	20	20	20		
Maximum Front Yard Setback	30 ft. or up to 20% greater than block average	n/a					
Side Yard Setback	One-story: 5 Two-story: 6 Lot width <50: 10% lot width	Two-story: 6 Lot width <50: 10% lot width	One-story: 5 Two-story: 6 Lot width <50: 10% lot width	One-story: 5 Two-story: 6 Lot width <50: 10% lot width	One-story: 5 Two-story or more: 6		
Reversed Corner Side Yard Setback	12	12	12	12	20		
Rear Yard	Lesser of 30% lot depth or 30	Equal to building height but at least 30					
Maximum height	2 stories/35	2 stories/35	2 stories/35	2 stories/35	35		
Maximum lot coverage	65%	65%	65%	65%	65%		
Maximum building coverage	n/a	n/a	n/a	n/a	50%		
Usable open space (sq. ft. per d.u.)	750	750	750	750	n/a"		



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84382

File ID:84382File Type:OrdinanceStatus:Council New

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 07/10/2024

Enactment Number:

File Name: Adopt the Northeast Area Plan Final Action:

Title: Adopting an amendment to the Comprehensive Plan related to the Northeast

Area Plan.

Notes: 6885NortheastAreaPlan

Sponsors: Derek Field And Sabrina V. Madison Effective Date:

Attachments: NEAP Adopting Cover Letter, Northeast Area Plan

Introduction Draft, Resolution 84379-Adopting

Northeast Area Plan

Author: Kate Smith Hearing Date:

Entered by: mglaeser@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Attorney's Office	07/10/2024	Referred for Introduction				
	Action Text:	This Ordinance was Ref		ction			
	Notes: Plan Commission (8/26/24), Economic Development Committee (7/17/24), Transportation Commission (7/31/24), Community Development Block Grant Committee (8/1/24), Landmarks Commission (8/5/24), Board of Parks Commissioners (8/14/24), Urban Design Commission (8/14/24), Common Council (9/10/24)						

Text of Legislative File 84382

Fiscal Note

Fiscal note pending.

Title

Adopting an amendment to the Comprehensive Plan related to the Northeast Area Plan.

Body

DRAFTER'S ANALYSIS: This ordinance adopts the Northeast Area Plan.

The Common Council of the City of Madison do hereby ordain as follows:

WHEREAS, on August 2, 2022 the City of Madison Common Council authorized the Planning

Division's shift to a planning framework with twelve (12) Area Plans that cover the entire city and are updated every ten (10) years (RES-22-00577); and

WHEREAS, the City of Madison Common Council authorized the Planning Division to prepare the Northeast Area Plan on January 17, 2023 (RES-23-00068); and

WHEREAS, the planning framework states that the future land use map developed through each area plan process will directly update the Comprehensive Plan's Generalized Future Land Use Map; and

WHEREAS, the City of Madison Common Council adopted a public participation plan for the Northeast Area Plan on September 19, 2023 (RES-23-00589), as required by Wis. Stat. § 66.1001(4) to amend the Comprehensive Plan, which described how the City intended to make the process accessible to all; and

WHEREAS, the public participation process included more traditional engagement through a project website, interactive mapping, virtual meetings, in-person meetings, emailed project updates to interested parties, distribution of informational materials, focus groups, attendance at neighborhood association events, social media posts, traditional media, and

WHEREAS, the public participation process also included contracting with several Community Navigators and coordinating with the City's Community Connectors to increase engagement with traditionally underrepresented residents; and

WHEREAS, notice was provided pursuant to Wis. Stat. § 66.1001(4)(d) and the Plan Commission held a public hearing on the proposed amendment to the Comprehensive Plan on August 26, 2024; and

WHEREAS, the Plan Commission, by a majority vote of the entire Commission recorded in its official minutes, adopted a resolution recommending the Common Council adopt an amendment to the City of Madison Comprehensive Plan's Generalized Future Land Use Map, Bicycle Facilities Map, and relevant text and maps in the Comprehensive Plan that reference area plans, including underlying plans that are archived; and

NOW, THEREFORE BE IT RESOLVED the Common Council of the City of Madison does, by the enactment of this ordinance, formally amend the City of Madison Comprehensive Plan's Generalized Future Land Use Map, Bicycle Facilities Map, maps and text that reference underlying archived plans, and any amendments to the Plan made from the Common Council floor, pursuant to Wis. Stat. § 66.1001(4)(c).

BE IT FINALLY RESOLVED that staff is directed to implement the actions contained in the Northeast Area Plan.

Department of Planning & Community & Economic Development



Planning Division

Bill Fruhling, Interim Director

215 Martin Luther King Jr. Blvd. Ste 017 P.O. Box 2985 Madison, Wisconsin 53701-2985 Phone: (608) 266-4635

Fax (608) 266-6377 www.cityofmadison.com

To: Mayor, Common Council and (BCCs)

From: Dan McAuliffe and Angela Puerta, Northeast Area Plan project managers

Re: Adopting the Northeast Area Plan

Date: July 8, 2024

Attached is the Northeast Area Plan draft being considered for approval. Along with the West Area Plan, these are the first plans developed under the City's Planning Framework which established 12 plan geographies covering the entire city. While plan development was led by the Planning Division, its creation was an interagency-effort with collaboration and contributions from many City agencies.

Public Engagement

There were many opportunities for the public to participate in the planning process. From initial phases that sought to understand the public's thoughts about issues and opportunities, to the most recent phase which focused on review of draft plan recommendations, multiple engagement strategies were used to reach a broad range of participants. These included traditional public meetings (both in-person and virtual), multi-agency open houses, outdoor plan events, neighborhood walking tours, plan pop-ups at community events, an art charrette and interactive digital engagement. The Planning Division also contracted with Community Navigators to help reach and get feedback from under-represented populations to ensure their voices are reflected in this plan. A business survey was also conducted in collaboration with the City's Office of Business Resources. Summaries of feedback received throughout the process are available at the following links (Feedback through May 2024; Feedback on Public Review Draft).

Public comments and feedback led directly to several Plan recommendations. These include addressing the "fragmented" bike network, new and expanded parks in the Northeast Area, and land use changes to encourage housing development. The planned homeless shelter on Bartillon Drive was often mentioned by participants, and plan recommendations reflects concerns with a lighting audit and the creation of a Neighborhood Resource Team.

In addition to public engagement, the Northeast Area Plan team met with several City Boards, Commissions and Committees (BCCs) which the plan would likely be referred to during the approval process. These mid-process check-ins ensured initial BCC feedback could be considered and incorporated during the planning process instead of the approval process.

Public Review Draft

The Northeast Area Plan Public Review Draft was published on May 20, 2024 with three public engagement events in the following weeks. Those events, as well as an online engagement platform, allowed the public to give feedback and ask questions about the draft document. The Public Review Draft was preceded by the Recommendation Framework, released in December 2023, which outlined an initial draft of the major recommendations of the Plan.

Approval Schedule

The anticipated review and approval schedule is as follows. Any public feedback received during this phase will be forwarded to the BCCs reviewing this plan.

July 16 – Common Council introduction

July 17 – Economic Development Committee

July 31 – Transportation Commission

August 1 - Community Development Block Grant Committee

August 5 – Landmarks Commission

August 14 – Urban Design Commission (4:30 pm); Board of Park Commissioners (6:30 pm)

August 26 – Plan Commission (lead)

September 10 - Common Council

The Northeast Area Plan draft is accompanied by both an adopting resolution and an ordinance amending the Comprehensive Plan. The resolution includes archiving previously adopted underlying plans. The Comprehensive Plan amendment (ordinance) is limited to key maps, such as the Generalized Future Land Use and Bicycle Facilities map, among others.

After plan adoption, staff will incorporate any changes made during the approval process. Minor formatting changes, such as moving/resizing graphics or adding supplemental images, may also be made to improve the usability of the document.

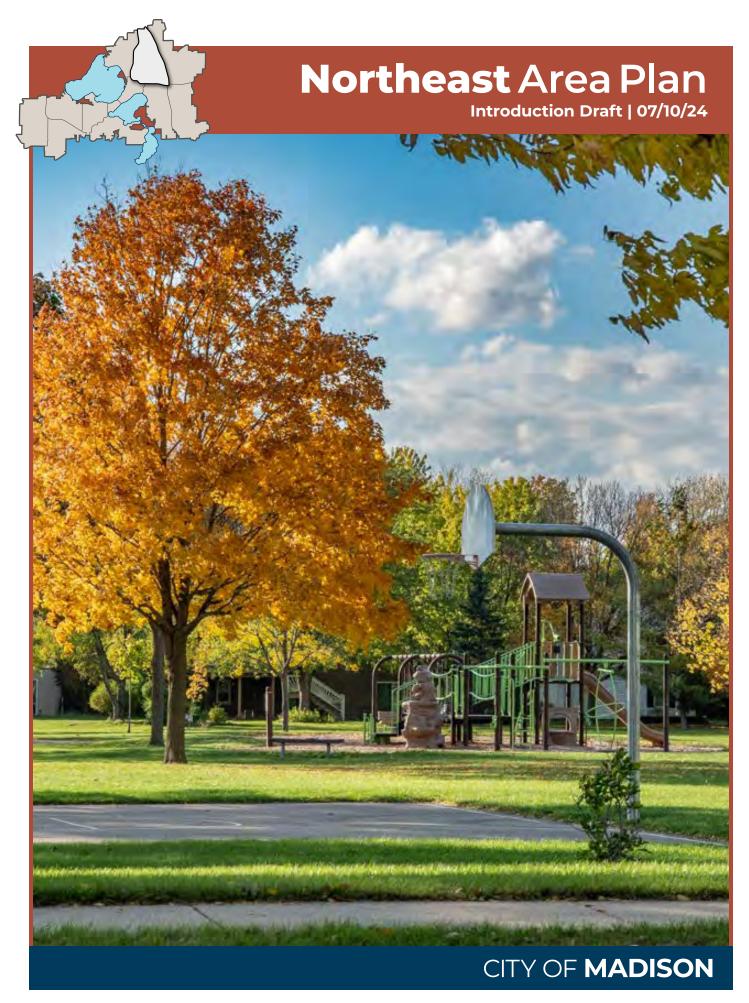


Table of Contents

Acknowledgments
Introduction
Land Use
Transportation
Neighborhoods and Housing41
Economy and Opportunity45
Culture and Character
Green and Resilient58
Effective Government65
Health and Safety
Community Action Strategy74
Annendix 83

Adopted by the Common Council on (Date)

Resolution RES24XXXLegistar file # XXXXX

Acknowledgments

Officials & Administrators

Satya Rhodes-Conway, Mayor Derek Field, District 3 Alder Amani Latimer Burris, District 12 Alder

Sabrina Madison, District 17 Alder Charles Myadze, District 18 Alder Bill Fruhling, Interim Director, Planning Division

Heather Stouder, former Director, Planning Division

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Saran Ouk
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Ruth Rohlich
Julie Spears

Police

Captain Jamar Gary Captain Kelly Donahue

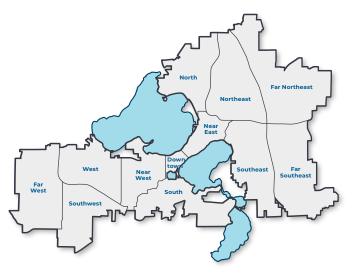
Public Health Madison and Dane County

Justin Svingen Melinda Knuth

Community Navigators

African Center for
Community Development
Mathias Lemos LLC
Harambee Village

Introduction



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Planning Framework

The Northeast Area Plan is one of the first plans following the City's new Planning Framework. The Planning Framework established 12 plan geographies to be the basis for future area plans. These 12 plans will be established and updated on a ten-year cycle, ensuring all areas of Madison have a current plan. This more equitable approach to creating plans avoids past situations where certain areas received greater attention while others were only addressed in the citywide Comprehensive Plan. The Planning Framework creates greater clarity by having a single area plan rather than overlapping plans written at different times with different recommendations.

Previously adopted underlying plans

Previously adopted underlying plans served critical roles in guiding decisions around new development, infrastructure, and budgeting. Adopted by the Common Council, these guided millions of dollars in investment after extensive public outreach efforts. Previously adopted plans significantly overlapping with the Northeast Area will be archived and will no longer guide City decisions. Going forward, the Northeast Area Plan will guide decisions. Archived plans for the Northeast Area:

- 1. Hawthorne-Truax Neighborhood Plan (2023)
- 2. Greater East Towne Area Plan (2022)
- 3. Hanson Road Neighborhood Development Plan (2000; amendments through 2021)
- 4. Ridgewood East Central Development Plan (2002)
- 5. Carpenter-Hawthorne-Ridgeway-Sycamore-Truax Neighborhood Plan (2001)
- 6. East Towne-Burke Heights Neighborhood Development Plan (1987)

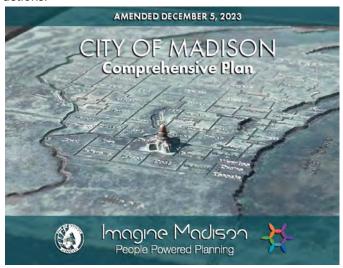
Recommendations in these underlying plans were reviewed and considered as part of this Plan's development. Recommendations were incorporated if they are consistent with the Comprehensive Plan or current City policies, and generally addressed physical or regulatory changes specific to the Northeast Area, including land use, zoning, building height, transportation, parks and open space, public utilities, and stormwater infrastructure. Recommendations already implemented or that repeat existing citywide policy were not incorporated in this Plan.

Relationship between The Northeast Area Plan and the Comprehensive Plan

The Comprehensive Plan includes citywide policies and priorities for investment based on feedback received through an extensive community engagement effort. Recommendations are grouped by the seven elements illustrated below. The Comprehensive Plan also includes policies that guide future growth in a way that supports citywide policies.

The Northeast Area Plan applies policies from the Comprehensive Plan, and other citywide initiatives, into specific recommendations for the Northeast Area geography based on a closer evaluation of the area and additional community engagement.

While Area Plans help guide future growth, they don't automatically create or require development. Instead, these Plans guide the City's review process when development is proposed. Proposals that generally follow plan recommendations are likely to be approved. Those that don't, face greater scrutiny, and approval is less likely. Plans also provide a guide for future City projects and programs, and future City budgets will determine the timing of those actions.













The Northeast Area

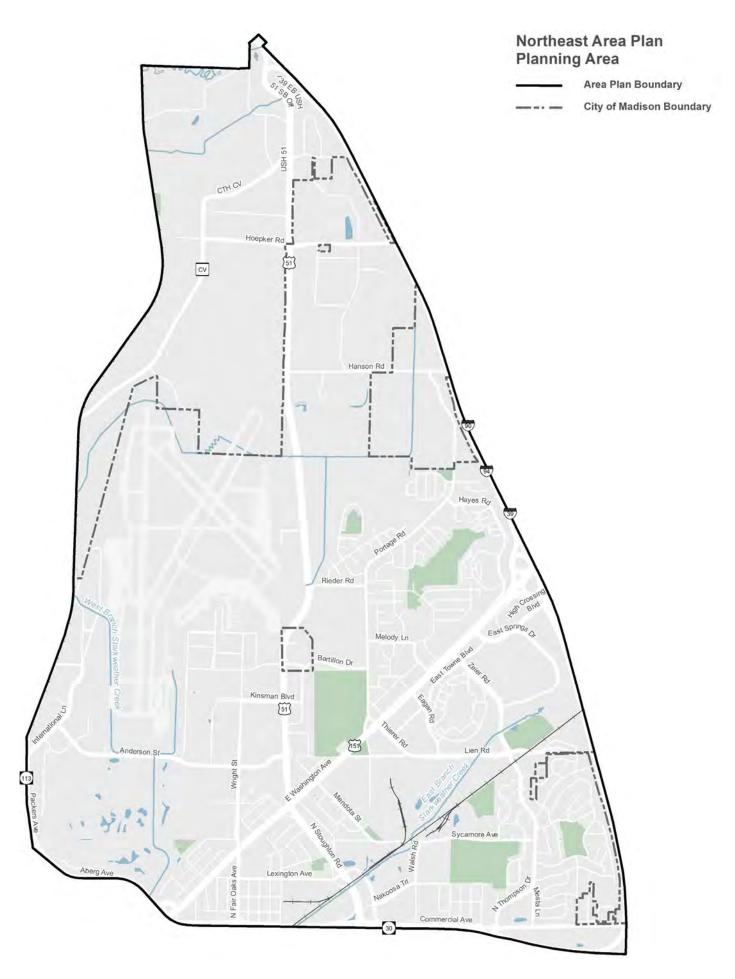
The Northeast Area is generally bound by Interstate 39/90/94 to the east, Highway 30 and Aberg Avenue to the south, and Packers Avenue and the Canadian Pacific rail corridor to the west. The 7,608-acre (12-square mile) area includes major destinations such as the Dane County Regional Airport, Madison College - Truax Campus, and East Towne Mall. It's also defined by natural features including Starkweather Creek, extensive wetlands, and a wooded drumlin at the center of the Sandburg neighborhood.

East Washington Avenue is the primary transportation corridor in the Northeast Area, and a major part of the City's transit network. The east-west Bus Rapid Transit (BRT) line will open in late 2024, providing residents improved travel times and more frequent trips. Highway 51 (Stoughton Road) is also a major route through the Northeast Area, although it has fewer connections to neighborhoods and businesses along its path.

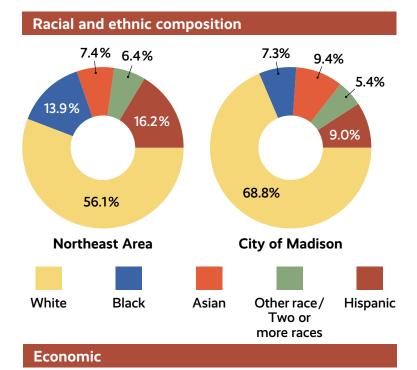
Nearly 15,000 residents call the Northeast Area home. It's one of the most diverse areas of Madison, with more than 40% of residents being persons of color. Resident incomes and college degree rates are lower than citywide averages. The Northeast Area is one of the city's most affordable places to live, with housing costs about 25% lower than the citywide averages. There are two neighborhood elementary schools, Hawthorne and Sandburg, and a future City library: the Imagination Center at Reindahl Park. The area is a major employment hub with 21,000 employees centered around the airport and by East Towne Mall (Source: OnTheMap 2021).

Growth projections in the Comprehensive Plan estimate Madison will add 115,000 residents between 2020 and 2050. As development activity continues to move east along East Washington Avenue from the Downtown area, the Northeast Area could see transformational change on many underutilized sites. This growth will add more than just new residents. It will bring needed new housing that will help slow housing cost increases. It will support existing and future local businesses and create walkable neighborhood destinations. It will add new park and recreational opportunities throughout the Northeast Area.





Demographics



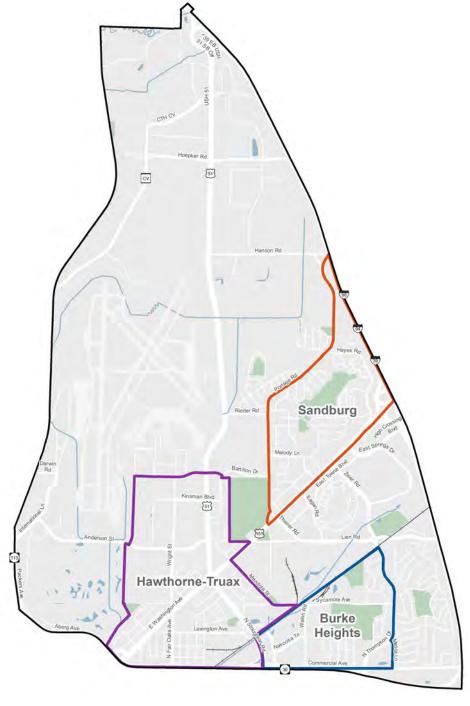
Northeast Area City of Madison

Poverty rate (families)	6.4%	5.9%
Median income	\$59,486	\$70,466
Homeownership (Tenure)	42.4%	44.5%
Cost-burdened households – owners	22.7%	19.8%
Severely cost-burdened households – owners	11.5%	6.8%
Cost-burdened households – renters	36.6%	46.5%
Severely cost-burdened households – renters	14.8%	25.5%

Source: Neighborhood Indicators Project, Census/American Community Survey

Community Action Strategy Areas

The Northeast Area contains three Community Action Strategy areas. Greater numbers of low- to moderate-income households live in these areas, making them eligible for federal Community Development Block Grant funding. The City is directing \$200,000 to Sandburg, \$200,000 to Burke Heights, and \$250,000 to the Hawthorne-Truax area for small scale capital improvements. Additional public engagement and community capacity-building assistance were directed to these areas during the planning process. Given the capacity-building focus, the Community Action Strategy recommendations are not exclusively physical or policy changes, or that are outside City control. These recommendations may rely on community partners for implementation.





Burke Heights



Sandburg



Hawthorne-Truax









Planning Process and Public Engagement

The planning process began in early 2023 and lasted through adoption in summer 2024. It followed a multi-phase approach, with the first meetings focusing on outreach and engagement to ensure community awareness of the process and public feedback. The next series of meetings captured community values and incorporated them into initial recommendations for review, in addition to relevant recommendations from previously adopted underlying plans. The final steps of the process consisted of drafting the plan document, public review of the draft Plan and the formal City approval process. While the public participation strategies evolved over the course of the process, each phase included in-person and virtual events to gather input and facilitate conversations with residents and other stakeholders. Interactive commenting maps and review drafts allowed participants to add their thoughts and see other peoples' comments in a broader community discussion. Neighborhood walks, Park Connects events, and multi-topic open houses were all held to bring more people and viewpoints into the discussion.

The City also hired Community Navigators to help reach a wider range of residents and business owners. Selected from area non-profits with deep connections to northeast Madison, Community Navigators conducted interviews, had informal conversations, promoted the community meetings, and made new connections with people in the Northeast Area.

Primary themes from public engagement is incorporated in each Plan chapter. However, a recurring theme expressed by participants was frustration that the Northeast area was being "dumped on" with negative additions and not seeing positive investments from the public or private sector. Negatives includes noise impacts from the airport, PFAS in Well 15, the homeless encampment at Reindahl Park and the temporary and permanent homeless shelters. While there is excitement for the planned Imagination Center at Reindahl Park, there is frustration that its development is taking more time than originally expected.

Planning Process

Data Gathering & Review

- Interagency staff mtgs
- Public mtgs (in-person/virtual)
- Online interactive commenting map
- Regular email notifications and website updates (applies to all phases)

Phase One

JAN - MAR

Issues & Opportunities

- Community surveys (in-person/virtual)
- 4 public mtgs (in-person/virtual)
- Multiagency Open House
- Review by Board, Commissions & Committees
- Interagency Staff MtgsSandburg Elementary PTA mtg

APR-JUN

Phase Two

Draft Plan Concepts

- Pop-up engagement at community events such as Make Music Madison, Westchester Gardens Block Party & others
- Public outreach by 3
 Community Navigators
 (in-person)
- Connect Events at Sycamore
 & Reindahl Park
- Burke Heights & Sandburg Walk & Talk
- Community Dinner & Alder Forum

JUL - OCT

Phase Three

Draft Plan Recommendations

- Business Postcard Survey
- Art Workshop
- Joint Open House with WisDOT
- Plan Commission
 Board of Parks
 Commissioners
- Transportation Commission

Phase Four

NOV-FEB

Final Draft Plan Review

- Public Meetings (in-person/virtual)
- Landmarks Commission
- Urban Design
- Commission
 CDBG Board
- Public feedback on Draft Plan
- Joint Open House with WisDOT

Phase Five

MAR-JUN

Adoption & Implementation

- Boards,
 Commissions &
 Committees
- Implementation of Plan recommendations

Phase Six

JUL+

2023 2024

Plan Format

The Plan is structured around the Comprehensive Plan's seven elements, with each element generally having four sections:

- 1. An introduction, covering background information and current conditions
- 2. What we heard, including major themes in feedback and comments
- 3. Actions, which focus on City-led items
- 4. Partnerships, which focus items led by entities other than the City but may have some City involvement or contributions

While public feedback themes were generally used to inform actions and partnerships, not all summarized themes have corresponding actions or partnerships. Some feedback themes contradicted each other. Some themes conflicted with adopted City policy, such as the Comprehensive Plan. Lastly, some themes addressed topics the City has little control over. Actions in each chapter are listed by number, but do not have a priority order.



What We Heard

- Have walkable neighborhoods, with a mix of housing types and nearby destinations like restaurants, cafes, and local businesses.
- Development should be focused along transit corridors and activity centers.
- Support new development and more housing options.
- Neighborhoods are currently disconnected by highways, railroads, the airport, and natural features like wetlands.



Commercial Avenue and Pulley Drive



FedEx at Hoepker Drive and HWY 51



East Towne Mall area

Comprehensive Plan Goals:

This Plan aims to advance the following goals:

- Madison will be comprised of compact, interconnected neighborhoods anchored by a network of mixed-use activity centers.
- Madison will have a safe, efficient, and affordable regional transportation system that offers a variety of choices among transportation modes.

Land Use

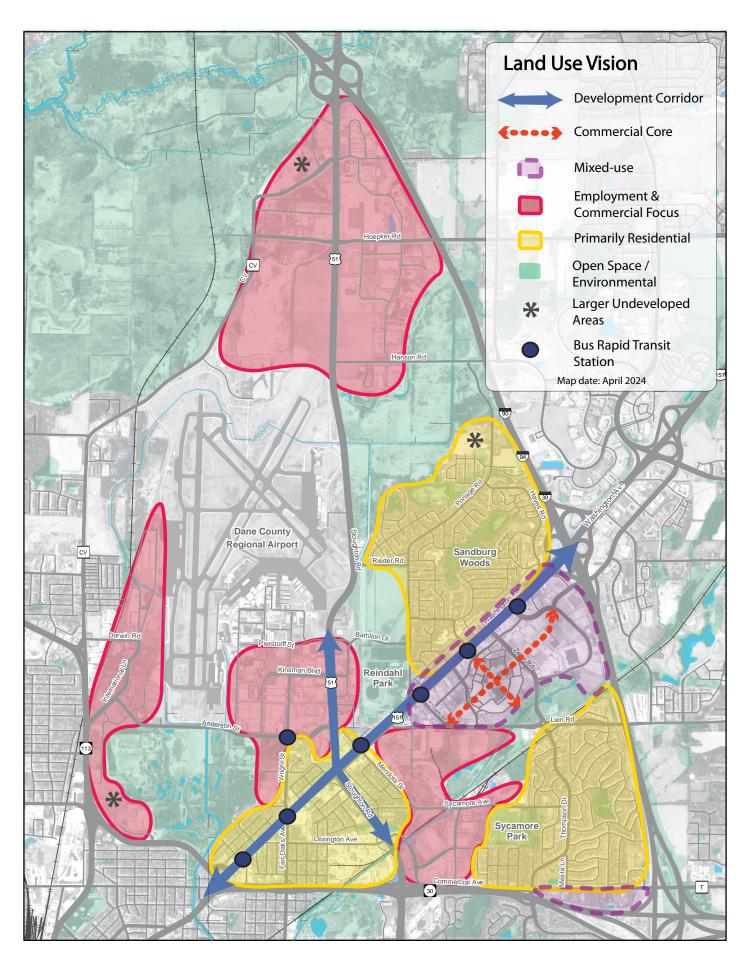
The Northeast Area has a variety of land uses within its nearly 8,000 acres. Parks and open spaces are present throughout the area, and include large wetlands along Starkweather Creek, wooded drumlins, park spaces, and Bridges Golf Course. Approximately 6,900 housing units are predominately located in three separate areas, Carpenter Ridgeway/Hawthorne/Mayfair, Sandburg, and Burke Heights (Thompson Drive). Commercial land, including retail at East Towne and office spaces near the airport, accounts for 11% of the Plan area, with industrial and institutional (educational) land largely centered on Highway 51, occupying 7% of the area.

Land Use	Acres	% Of Total
Residential	946	12%
Commercial	846	11%
Institutional/Governmental	283	4%
Industrial	271	4%
Park and Open Space	1,947	26%
Agriculture/Vacant/Under Construction	644	8%
Mineral Extraction	124	2%
Airport	1,316	17%
Utilities, Transportation	1,231	16%
Total	7,608	100%

Source: Capital Area Regional Planning Commission (CARPC) 2020 Land Use

There is little undeveloped land in the Northeast Area. Most vacant sites are industrial properties near Highway 51 and Hoepker Road. There are few other undeveloped areas, including parcels on Portage Road just north of the Greater Sandburg Neighborhood and on County CV just west of Highway 51.

Much of the recent development within the Northeast area was for industrial uses in the same Highway 51 and Hoepker Road area. In other areas, redevelopment is beginning to occur along the East Washington corridor, including the former Bimbo Bakery site and the Capital Auto Credit property. The East Towne Mall area is also beginning to see investment and redevelopment. Several vacant buildings are being repurposed for new uses, and new hotels, residential, and mixed-use buildings are being built.

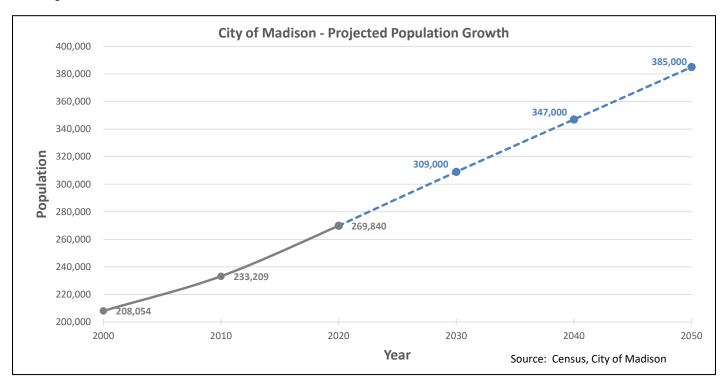




Rise Madison housing development under construction on the former Gardner Bakery site at East Washington and Fair Oaks Avenues

Future Growth

According to the Comprehensive Plan, Madison is projected to add 115,000 new residents and 50,000 jobs between 2020 and 2050¹. This projection is largely based on past growth trends. While this growth will be spread across the entire city, a portion will occur within Northeast Area. The Comprehensive Plan emphasizes accommodating growth through redevelopment of underutilized sites, particularly on transit corridors. Redevelopment generally uses existing infrastructure and can increases housing availability on transit corridors while reducing automobile dependency. Given the lack of undeveloped property in the Northeast Area, redevelopment will likely be the primary way that future growth occurs.



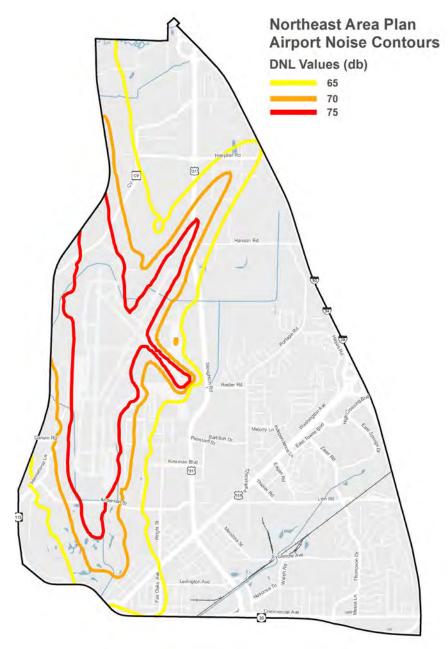


East Washington Avenue - construction of BRT station

14

The Comprehensive Plan identifies <u>Growth Priority Areas</u>, which are locations best suited to accommodate growth consistent with the Comprehensive Plan's goals. Within the Northeast Area, the East Washington Avenue corridor, East Towne area, and the industrial area along Highway 51 are identified as Growth Priority Areas where development and redevelopment should be directed and encouraged. These areas have larger concentrations of underutilized properties and are well-served by transit and utilities, making them attractive for redevelopment and to future residents, visitors, and employers. The area around East Towne Mall is defined by expansive parking lots, nearby big box stores, and a street network designed around cars and not people. On East Washington Avenue, there are several strip commercial buildings likely to redevelop in the coming years. Collectively, these Growth Priority Areas could transition to new, well-connected residential, commercial, and employment areas that better meet the needs of the City and its residents.

¹ Source: City of Madison, Greater Madison MPO



Day Night Average Sound Level (DNL) is a noise metric used to reflect a person's cumulative exposure to sound over a 24-hour period and is the Federal Aviation Administration's required noise metric for the assessment of aircraft noise.

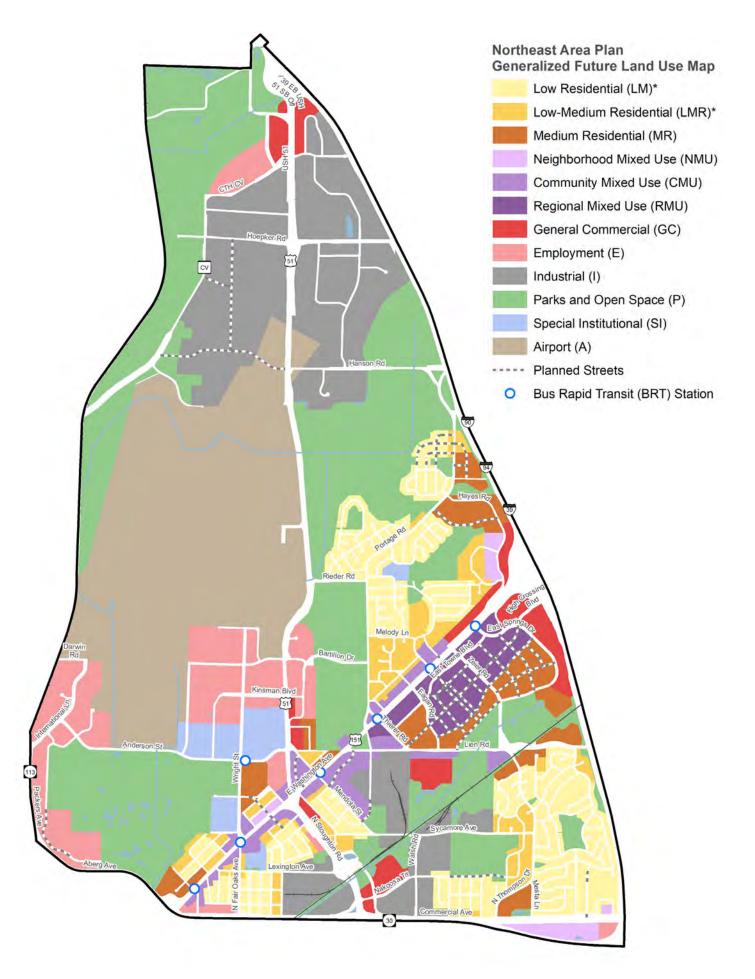


Airport Noise

With F-35 fighter jets replacing F-16s at the Dane County Regional Airport (DCRA), noise exposure in neighborhoods surrounding the airport became a focus of community conversation and concern. Airport sound modeling, which includes both military and commercial flights, indicates the Carpenter Ridgeway area would likely see the greatest impacts, with average sound exposures beyond what is considered compatible for typical residential construction. In 2022, the Common Council established a committee of alders to evaluate alternative approaches to land use and growth in the noise impacted area. Ultimately, those alders found the elevated noise exposure did not merit a prohibition on new residential construction in this area. They did recommend the City encourage sound insulating construction techniques (State law prohibits the City from requiring sound insulating construction).

Dane County Regional Airport (DCRA) is currently developing its noise compatibility program, a federal program that funds strategies to minimize and mitigate noise exposure surrounding airports. Recommended strategies may shift the sound impacts north and away from higher concentrations of residential south of the airport. This may resolve noise compatibility concerns without changes in construction or noise insulation.

As part of the noise compatibility program, DCRA is recommending an expansion of the Airport Affected Area, a legal boundary that allows the DCRA to veto rezonings approved by the City up to three miles away from the airport. While the City can override a veto with a 2/3 majority vote of the Common Council, this creates uncertainty in the development process and could result in fewer housing units getting built.



16

Recommended Future Land Use and Growth Areas

The Generalized Future Land Use (GFLU) Map establishes a framework for how the Northeast Area can grow and evolve over time. The map identifies broad categories of development such as residential, commercial, or industrial uses, as well as recommended building size. The future land use categories guide future growth and establish the basis for how the City reviews development proposals. The land use recommendations in this Plan embody several of the Comprehensive Plan's citywide strategies, including:

- Concentrating the highest intensity development on major transit corridors and at activity centers
- Facilitating compact growth to reduce development of farmland
- Increasing the amount of available housing
- Supporting the development of a wider mix of housing types, sizes, and costs.

While the GFLU Map establishes a future vision, there isn't a defined timeline for change. Many factors impact development, making it hard to predict where and when it might occur. The overall economy, market demand, available properties and financing, and developer priorities all influence the timing and location of development proposals.



Low Residential (LR) *



Neighborhood Mixed-Use (NMU)



Low-Medium Residential (LMR) *





Regional Mixed-Use (RMU)

Land use categories

Images below illustrate types of development commonly found in each category. More information on each of the land use categories can be found in the Comprehensive Plan.

*In select conditions, Low Residential may allow development up to 30 dwelling units/acre (DU/ac) and three stories. In select conditions, Low-Medium Residential may allow development up to 70 DU/ac and four stories.









Medium Residential (MR)





The East Towne area in its current form (above) and a vision for the area at build-out (bottom). Existing building footprints shown in white.



Redevelopment concepts on East Washington Avenue near Highway 51 (above) and Highway 30 (below).



Within the Northeast Area, the most visible change could be at or around East Towne Mall. This Plan incorporates recommendations from the Greater East Towne Area Plan, which envisioned the area transitioning from a regional mall to a vibrant urban neighborhood. A compact, walkable network of streets would support a mixture of uses, including commercial, employment, and residential, with building scales from three to ten stories. Taller, more commercial-oriented buildings are located closer to Bus Rapid Transit (BRT) stations on East Washington Avenue, and the area transitions to a mixture of residential uses towards Starkweather Creek. Areas along the Interstate corridor are recommended to remain as commercial or transition to employment (office, lab, medical, etc.), as road noise makes this location less appropriate for residential. Several new park spaces are recommended to support new residents, including a large green corridor along Starkweather Creek.

Beyond East Towne Mall, the East Washington Avenue corridor has sites that could redevelop in the foreseeable future. Many of these were addressed by the Hawthorne-Truax Neighborhood Plan, which planned for sites to transition from single-story commercial to more intensive mixed-use development.

Along with redevelopment planned for East Washington Avenue and other major sites, several areas are appropriate for "Missing Middle" housing. Missing Middle housing is a range of housing types between single-family detached houses and smaller apartment buildings, such as rowhouses and small multi-family buildings. Low-Medium Residential (LMR) areas on the GFLU Map are intended to accommodate Missing Middle housing. Identifying lower scale residential areas ensures the city will have a mix of housing types, and not just larger residential buildings in Medium Residential (MR) and High Residential (HR) areas.

Implementation and Development Regulation Maximum Building Heights

The Comprehensive Plan provides height guidance for most land uses. A select

few land use categories don't have recommended height ranges or allow Area Plans to recommend heights that may differ from those ranges. Mapping maximum building heights for these categories is intended to increase predictability and simplify the development review process by communicating recommended height limits for these categories. The maximum building height recommendations consider the intensity of development for each land use category, adjacent land uses, desired urban form, and proximity to BRT. The map below illustrates the recommended maximum height in stories. Refer to the Comprehensive Plan for guidance on areas without identified maximum heights on this map. Northeast Area Plan **Building Height Map** Low Residential (1-2) Low-Medium Residential (1-3) Medium Residential (2-5) Neighborhood Mixed Use (2-4) Community Mixed Use (2-6) Regional Mixed Use (2-12)* General Commercial Employment Industrial Parks and Open Space Special Institutional Airport *Land use category where Area Plans may allow height beyond Comprehensive Plan range. White labels are heights limited by Airport Height Limits. Black labels are not limited by Airport Height Limits. 6 4 Low Residential may allow new construction up to 30 DU/ac and three 6 stories in select conditions, generally along arterial streets or where these 4 types of buildings are already present or planned within an adopted subarea plan as part of a pattern of mixed residential development. Low-Medium Residential may allow new construction up to 70 DU/ac and four stories in select conditions. Factors to be considered include relationships between proposed buildings and their surroundings, natural features, lot and block characteristics, and access to urban services, transit, arterial streets, parks, and amenities.

The entire Northeast Area is within <u>Dane County Regional Airport's Height Limitation Zone</u>, which establishes a maximum elevation (feet above sea level) that buildings can't exceed. This, along with ground level elevation, establishes the legal maximum height for every site in the Northeast Area. In several areas, maximum building heights (displayed in stories) are limited by the airport height restrictions and not other considerations.

Commercial Core

The GFLU Map identifies several mixed-use categories, largely focused on the East Washington Avenue corridor and the East Towne Mall area. These areas are planned for a mixture of residential, commercial, and employment uses within the larger area to achieve an active and vibrant place. While a mixture of uses is envisioned, mixed-use buildings with retail or other commercial uses on the ground floor are generally not required. Within these areas, commercial uses are seen as appropriate as part of a broader use mix, but not every location can support ground floor commercial businesses.

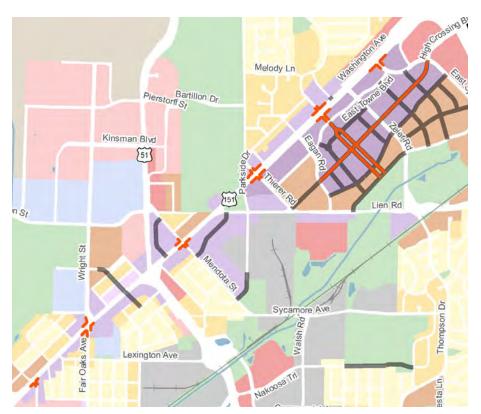
Visibility and enough residents and customers nearby are needed to support the businesses that provide those goods and services.

Future buildings along Commercial Core frontages should have ground floor commercial uses (retail, restaurant, service, or office uses). These areas appear capable of supporting additional commercial spaces and are located at important locations.



Ground floor commercial spaces create active spaces and are required on Commercial Core frontages





Planned StreetsCommercial Core

Aligning Future Land Use and Zoning

While the GFLU Map establishes a vision and future development guidance, zoning regulates what uses are allowed and what can be built now. Because of this difference, sometimes zoning and land use recommendations point in different directions. As part of this Plan, recommended future land uses and current zoning were evaluated to identify potential conflicts.

This Plan recommends several properties be considered for proactive rezoning, a City-initiated process that changes a property's zoning to a district more consistent with the future land use recommended in this Plan. Certain proactive rezonings may simplify approval processes for future development proposals that are consistent with Plan recommendations. Other are intended to avoid potential land use conflicts that could arise from the current zoning districts. Proactive rezoning is a separate legislative process that occurs after the adoption of this Plan and requires further review by the Common Council.

While proactive rezoning addresses a property's zoning, the evaluation also considered overlay districts. In one instance, a recommended land use change altered the context that was the basis of the Transit Oriented Development (TOD) overlay district boundary. The Target site on Lien Road site is nearly a half mile from a BRT station and on the TOD district boundary. It has no street frontage and is bordered by industrial uses and wetlands. As a result of these factors and the changed land use recommendation, this Plan recommends revising the TOD overlay district boundary to remove these parcels.

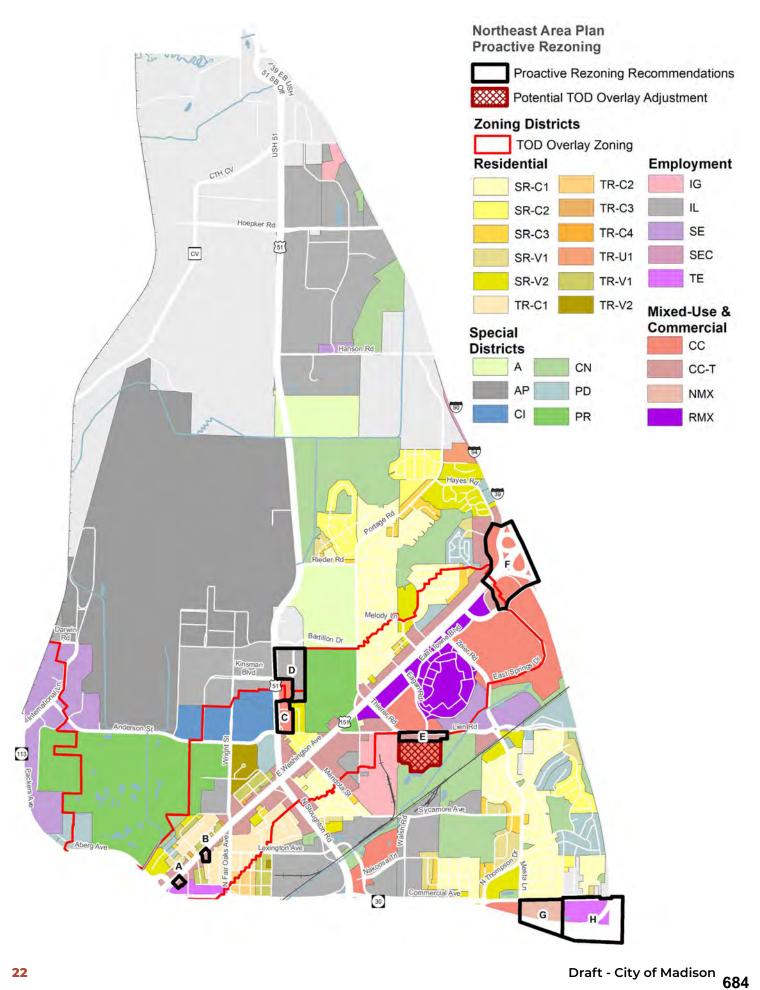
Other potential zoning changes were identified that could improve transparency and usability of the zoning code, but are citywide issues generally beyond the scope of Area Plans. These potential changes center on Planned Development zoning districts and City parks. There are 22 Planned Development zoning districts in the Northeast Area, all adopted under an older version of the City's Zoning Code. These could easily transition to conventional zoning districts that provide greater clarity and transparency while reducing the time it takes for City staff to respond to inquiries and issue routine permits. These transitions would not increase or decrease the allowable use of a property; instead, potential new zoning districts would best match the existing buildings and uses already in place.

The zoning for many parks doesn't reflect their use, potentially creating confusion during park planning processes. This confusion could be avoided by changing the zoning to match the park type established by the Parks Division and the Board of Park Commissioners.

Planned Development Zoning

Many areas in the city have a site-specific zoning called Planned Development (PD). PDs are intended for unique buildings or development projects that can't be approved using conventional zoning districts. These developments don't need to follow other zoning requirements and establish their own unique and detailed zoning code addressing uses, building placement, height, landscaping, parking, etc.

PDs were heavily used prior to the adoption of the current zoning code in 2013. The previous code, adopted in 1966, was so burdensome, complex, and out-of-date that PD zoning was routinely used for relatively typical projects, including single-family homes and small apartment buildings. Over the years, the City approved over 1,500 PDs, and while they simplified the approval process, they create long-term challenges for residents, property owners, and the City. Zoning codes are amended over time to address new issues or priorities that emerge, but PD files are rarely updated and reflect the time when they were adopted. Zoning code updates allowing accessory dwelling units and the keeping of chickens, or those requiring electric vehicle charging or bird-safe glass, do not apply to these PDs. The zoning text for PDs are only available in person (not online, like the rest of the code), and review and approval for minor changes like the addition of a shed can take weeks instead of minutes. The current zoning code is much more adept at handling these now-typical developments that were previously approved as PDs. Transitioning PDs approved under the old code to conventional zoning districts will improve transparency and usability for residents and property owners while improving the ability of the City to respond quickly to inquiries and routine applications like fences, sheds, and additions.



Site	GFLU	Existing Zoning	Recommended Zoning	Notes
Α	CMU	TE	CC-T	Create consistency with GFLU and adjacent zoning.
В	CMU	TR-C1	CC-T	Create consistency with GFLU and adjacent zoning.
С	GC	CC-T	CC	Change to discourage residential in less appropriate area.
D	E	CC	IL	Change to prevent residential in less appropriate area.
Ε	CMU	CC	CC-T	Create consistency with GFLU.
F	GC	CC-T	CC	Change to discourage residential in inappropriate area.
G	NMU	SR-C1, A	NMX	Create consistency with GFLU.
Н	NMU, E	SR-C1	TE	Create consistency with GFLU.

Actions

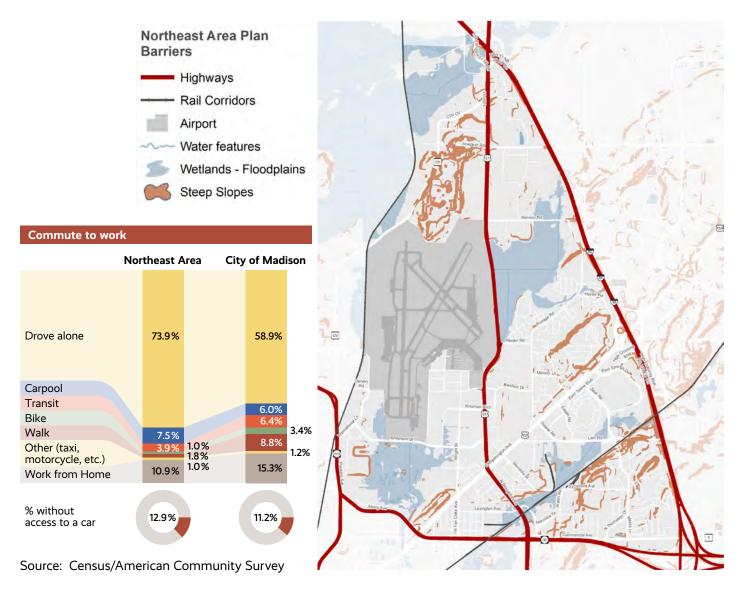
- 1. Update the Comprehensive Plan Generalized Future Land Use Map to be consistent with the Future Land Use map in this Plan.
- 2. Implement maximum building height recommendations shown on the Building Height Map with a mechanism for allowing bonus stories for developments that achieve specific affordability or sustainability parameters, similar to downtown height bonuses in City ordinances (Madison General Ordinances 28.071(2)(a)2).
- 3. Require ground floor commercial space or other active uses in the locations shown on the Commercial Core Areas Map.
- 4. Initiate a follow-up process to consider zoning changes shown on the Proactive Rezoning Map. Ensure all impacted property owners are aware of potential zoning changes.
- 5. Explore transitioning Planned Developments approved prior to 2013 to a matching conventional zoning district to improve transparency, usability, and consistency of regulations while reducing administrative burdens and inquiry response time.

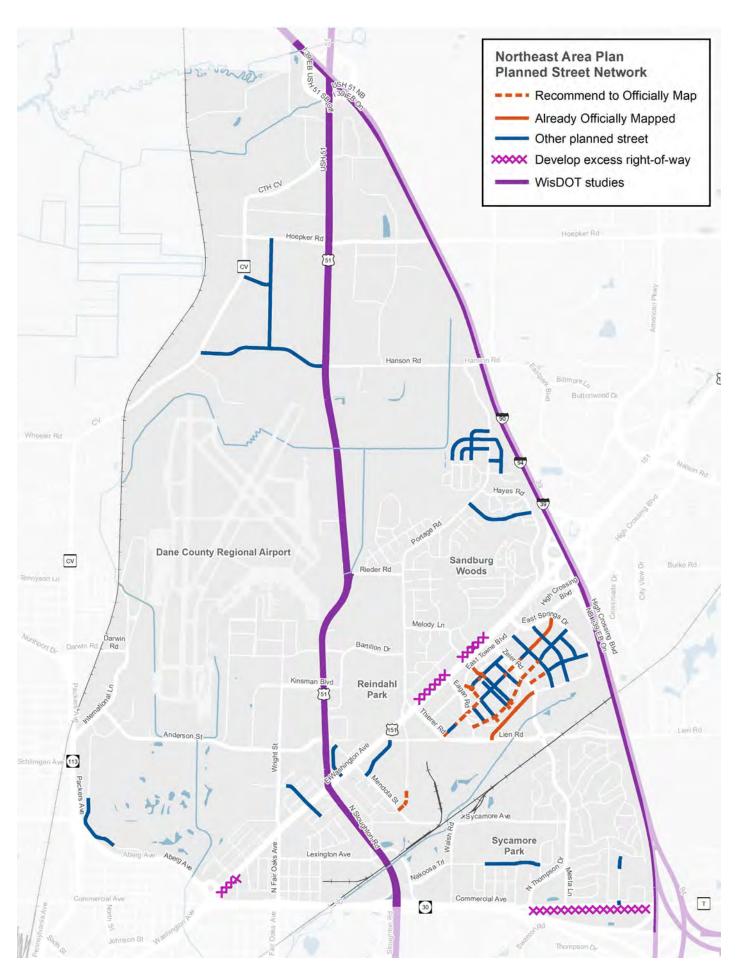
Transportation

What We Heard

- Improve pedestrian safety and comfort along major roads and heavily-traveled neighborhood streets and crossings.
- 2. Speeding in residential areas creates safety concerns.
- Increase bicycle network connectivity to places in the area like schools and to the city as a whole.

The Northeast Area's transportation network includes streets, transit, bicycle routes, and sidewalks. The primary transportation corridors are East Washington Avenue and Highway 51, with Highway 30 and Interstate 39/90/94 on its perimeter. Physical barriers that limit connections and all forms of transportation are prevalent in the Northeast Area. The airport is the largest barrier, spanning three miles between Hoepker Road and Anderson Street, separating Madison's north and northeast sides. Major roadways, such as the Interstate, Highway 30, and Highway 51 have few crossings which limit pedestrian and bicycle options and funnel traffic to those intersections. Two rail lines further limit connections, as it's difficult to obtain approval for new railroad crossings. Large natural barriers also limit connectivity in certain areas, including the Starkweather Creek wetlands south of East Towne Mall. Steep slopes present additional difficulty connecting areas, particularly north of East Washington Avenue.

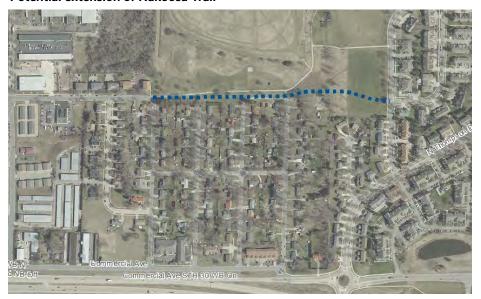




Street Network

The local street networks within individual neighborhoods are generally complete. However, there are limited connections between neighborhoods due to the natural and physical barriers already discussed. Older neighborhoods such as Carpenter Ridgeway and Hawthorne have a simple, grid-like pattern, while newer neighborhoods built between the 1960s and 1990s have a more meandering but relatively connected street network. The East Towne Mall and surrounding big box retail areas were designed for car access and the street network reflects that. For example, one block has a 1.7-mile perimeter and is 26 times the size of blocks in more walkable areas of the city.

Potential extension of Nakoosa Trail



This Plan recommends changes to the street network to create walkable pedestrian-oriented places, better connect neighborhoods, and balance vehicular traffic over multiple routes. The largest concentration of planned streets is within the East Towne Mall area. These streets are needed to support that area's future growth and should be built as redevelopment occurs. Other street connections, such as the proposed extension of Nakoosa Trail, would better link existing neighborhoods while increasing visibility of, access to, and utilization of Sycamore Park.

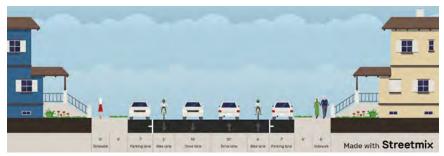
The Street Network Map also identifies the highest priority future streets that could be added to the City's Official Map. The Official Map reserves space for future public use, like streets or parks, by preventing the issuance of building permits in that location. The Official Map is amended through a separate legislative process that would occur after the adoption of this Plan and requires further review by the Common Council.





In addition to new street connections, there are instances where land currently used for roads or right-of-way could serve better purposes, potentially being incorporated into the redevelopment of neighboring properties. Two segments recommended for removal are frontage roads on East Washington Avenue, one at Rethke Terrace and one between Hagan Drive and Zeier Road. Removing these frontage roads would create additional space for new development while bringing new buildings closer to East Washington Avenue, resulting in a better pedestrian environment and improved visual character of the area. The Commercial Avenue right-of-way east of Thompson Drive is the third area that could be partially transitioned over time. The 250-foot right-of-way was the original alignment of Highway 30 before it was shifted south. While Commercial Avenue is an important street and would be maintained, much of its right-of-way could be repurposed for more productive uses.

Illustrations of potential development on East Washington Avenue frontage Roads. Independence Lane existing conditions (top) and with the frontage road removed (bottom).



Fair Oaks Avenue (existing condition)



Potential redesign of Fair Oaks Avenue (long-term)

Street design

Residents often discussed speeding as an issue in the Northeast Area. Older street designs contribute to this issue with wide lane widths and underutilized parking lanes, creating a wide open feel for drivers. Changing the road character through improvements such as bump outs or larger reconstructions can repurpose road space for other uses like street trees, rain gardens or protected bike facilities. Illustration (left) shows a potential transformation of Fair Oaks Avenue. Wide tree terraces and a shared use path could be added in the space that was formerly excess roadway and parking lanes.

Transit

In 2023, Metro Transit started a new route network, which was redesigned to increase access to employment, improve bus frequency, and decrease travel times. The network was designed around the new Bus Rapid Transit (BRT) system. BRT, including Route A on East Washington Avenue, includes dedicated bus lanes, new passenger stations, traffic signal priority, and enhanced fare systems to significantly improve travel times on that route. Planned BRT Route B serves the western edge of the Northeast Area, following the Packers Avenue corridor. Several other local routes serve the Northeast Area: Route P goes through the Sandburg neighborhood; Route G links the Burke Heights neighborhood with East Towne Mall and the South Transfer Point; and Route L follows Highway 51 and Aberg Avenue while connecting to Madison's northern and southeastern neighborhoods.



Northeast Area Plan - Draft 27

Missing sidewalks near Hawthorne Elementary School



Pedestrian crossing of Highway 51 at East Washington Avenue



Rectangular rapid flashing beacon (RRFB) in Madison



Cycle track in Charlotte, North Carolina

A two-way protected bike lane (cycle track) could be added to Portage Road to address the fragmented nature of more comfortable bike facilities

Bike and Pedestrian Network

Sidewalks and paths are essential public infrastructure, providing dedicated space for pedestrians to safely and comfortably move about. Within the Northeast Area, 48% of streets (by length) have sidewalks on both sides, consistent with the City's Complete Green Streets Guide. Most missing sidewalks are located within the Carpenter Ridgeway and Hawthorne neighborhoods, which were originally built when that land was in the Town of Burke and later annexed into the City of Madison. Streets with missing sidewalks surrounding Hawthorne Elementary School are particularly concerning, as children must walk on roads during the morning rush hour. Other notable missing sidewalks link residential areas to commercial areas, including Lexington Avenue and blocks of East Washington Avenue near East Towne Mall. Most streets outside the City but within the Northeast Area don't have sidewalks currently but will generally have pedestrian facilities added when they are reconstructed.

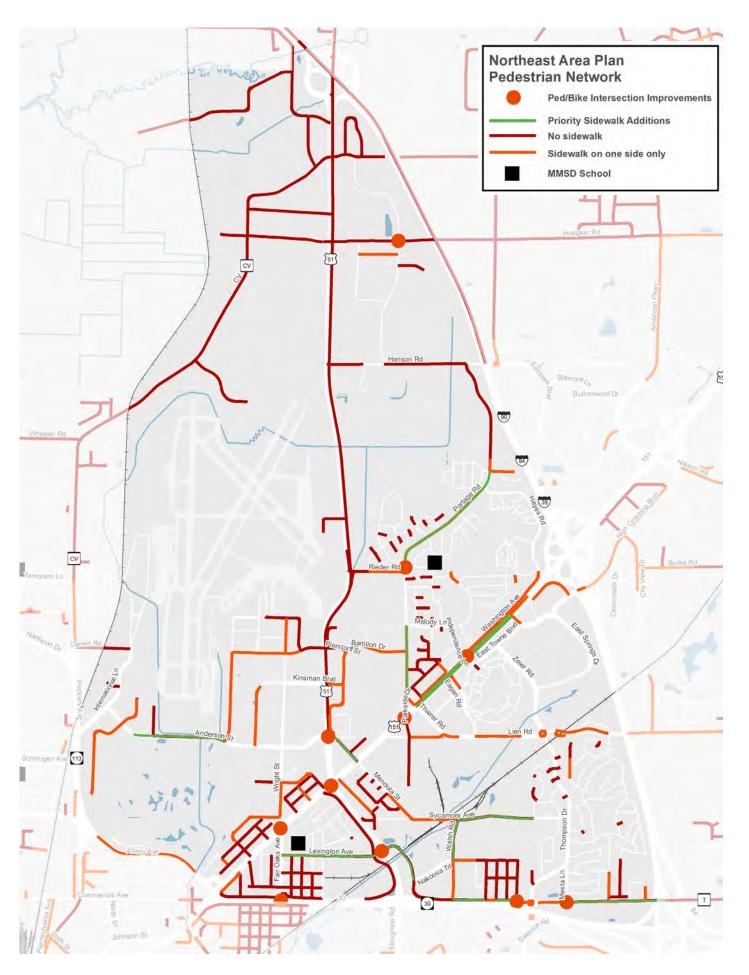
In addition to missing sidewalks, the pedestrian network is impacted by difficult crossings of busier streets. Several intersections on East Washington Avenue as well as Highway 51 were identified by residents as needing improvements to allow safer and more comfortable pedestrian crossings.

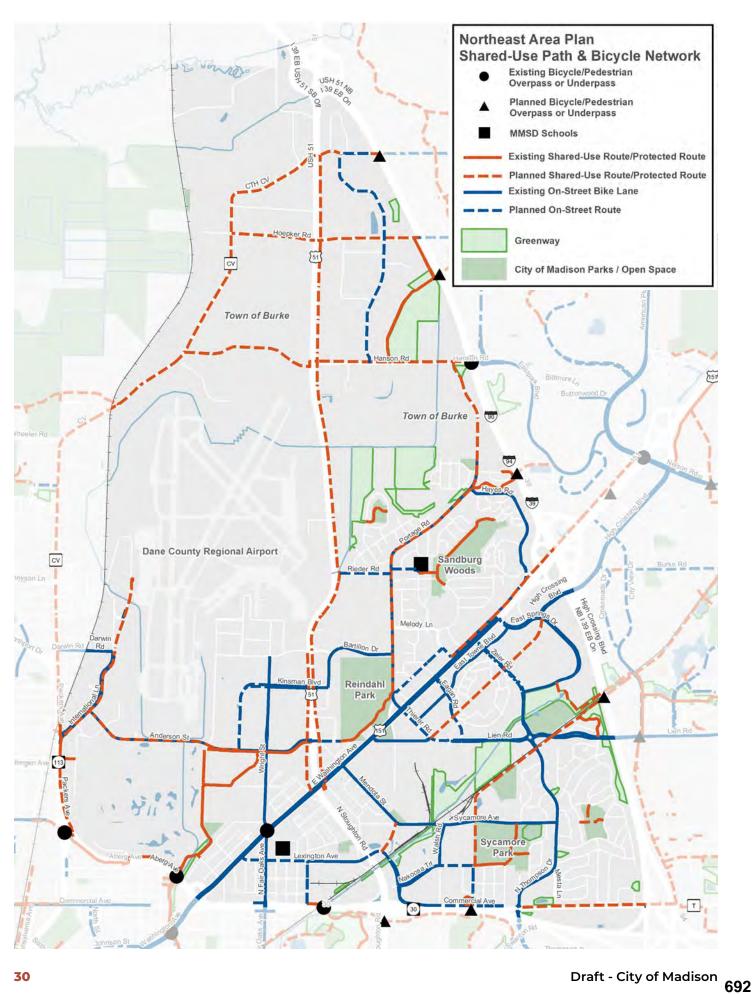
The bicycle network is inconsistent and not well-connected through most of the Northeast Area. Heavily used shared-use paths, like the Starkweather Creek Path, frequently transition to less comfortable on-street routes. Some cyclists don't feel safe using on-street bike lanes, especially on busier streets, and as a result don't use the larger bike network. Residents frequently discussed the challenges of crossing Highway 51, and this barrier separates many Northeast Area neighborhoods from the more robust bike network further to the west.

The Pedestrian Network Map identifies streets that do not have sidewalks on one or both sides. As part of the Hawthorne-Truax Neighborhood Plan, residents expressed broad support for adding sidewalks, particularly near Hawthorne Elementary School. The Pedestrian Network Map also shows Priority Sidewalk Additions, which due to their importance within the pedestrian network and higher traffic volumes, should be considered for construction with or without a street reconstruction project. This Plan identifies specific intersections for safety improvements that may include adding crosswalks, pedestrian islands, tabletop crossings, rapid flashing beacons (RRFBs), and bump outs.

The Bike Network Map shows recommended changes that, once constructed, will provide more consistent and comfortable bike routes. These include enhanced crossings of Highway 51 at Anderson Street and Lexington Avenue and routes that better link eastern neighborhoods. The recommended bicycle network consists of both on-street and shared-use (off street) routes.







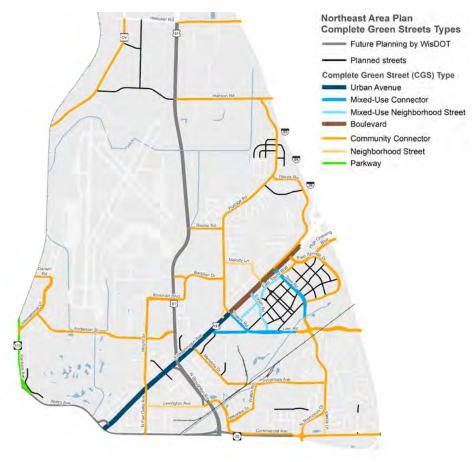
Other Transportation Initiatives

The City has several major transportation initiatives that influenced and are supported by Northeast Area Plan recommendations. The City's Vision Zero Campaign is a data-driven strategy intended to eliminate deaths and injuries on all roadways, bicycle facilities, and sidewalks by 2035. To achieve this goal, physical improvements are made at locations with a high number of traffic-related injuries. The High Injury Network Map uses city-specific crash data to determine where to focus resources to eliminate serious and fatal crashes. Because collisions between motor vehicles and people result in more serious injuries, areas where people tend to walk, bike, and board transit are of particular concern. This includes many of the intersections of East Washington Avenue, Highway 51 and Anderson Street, Portage Road and Rieder Road, and Commercial Avenue and Mesta Lane. Vision Zero is reinforced by the City's Safe Streets Madison Program, which focuses on traffic safety measures (such as speed humps, mini traffic circles, pedestrian islands) and improving connectivity by closing gaps in pedestrian and bicycle networks.

The City established an initial "All Ages and Abilities Bike Network" map in the 2022 Complete Green Streets Guide.

The network identifies streets and paths that are intended to provide a complete network of low-stress bikeways between neighborhoods, key destinations in the city, and to adjacent municipalities. The streets or paths may need to be built or improved to provide a low-stress connection, but the map serves as the long-range plan to reach the goal of a full network. The initial All Ages and Abilities Bike Network map is planned to be updated in the coming years and this Plan's recommended bicycle facilities will inform potential route changes.





Northeast Area Plan - Draft



Stoughton Road and Highway 30 interchange

WisDOT Highway 51 Study

The Wisconsin Department of Transportation (WisDOT) is studying Highway 51 from Interstate 39/90/94 to Highway 30 for potential changes intended to improve safety, mobility, and reduce traffic congestion. While WisDOT's focus is transportation, the City has broader interests regarding potential benefits and impacts from proposed improvements on neighborhoods and businesses, and bicycle and pedestrian connections along its length. The most important intersections within the Northeast Area are East Washington Avenue, Anderson Street, Lexington Avenue, and the Highway 30 interchange. WisDOT has proposed relatively minor changes for the remaining intersections. All intersection designs being evaluated are on WisDOT's project webpage.

East Washington Avenue Intersection:

Two intersection designs are being considered. One is a conventional intersection, similar to the current design but with an additional through lane in each direction and an additional southbound left turn lane. It would require the acquisition of the Schoepp Motors and Subway properties on the northern corners of East Washington and several driveway closures. The second alternative is a jug handle intersection, a design similar to the one at Mineral Point and Junction Roads. This alternative requires significantly more acquisition and demolition on the southwest quadrant of the intersection, needed to accommodate the ramp providing all turns from and to the lanes of southbound Highway 51.

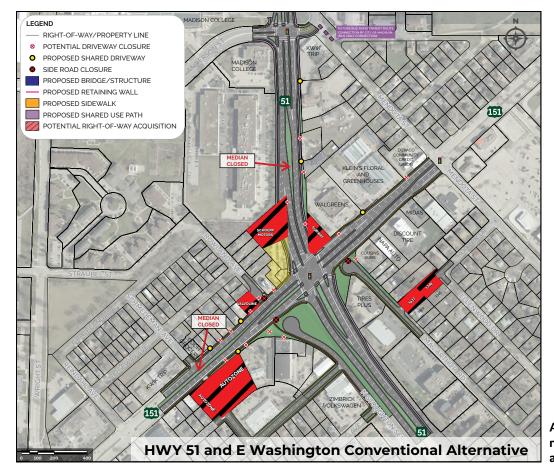
Each design has certain advantages. The conventional intersection requires the least amount of demolition and better supports existing uses and future redevelopment. The jug handle intersection would impact a much larger area, but would result in shorter crossing distances for bikes and pedestrians on East Washington Avenue. In both designs, WisDOT proposes closing the intersections of Rowland Avenue, MacArthur Road, and the frontage road south of East Washington Avenue. Both designs extend Schmedeman Avenue through the Autozone property and Hover Drive to MacArthur Road to provide local access. The City opposes the closures of Rowland Avenue and MacArthur Road.

Lexington Avenue Intersection:

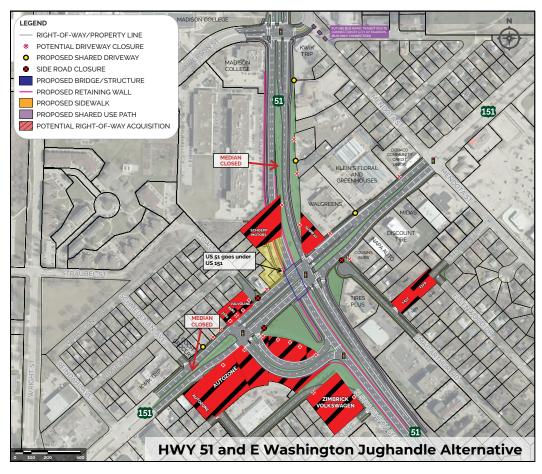
Two intersection designs are being considered. The first is very similar to the current design, but adds pedestrian crossings and shifts the frontage road intersections further west into the County Materials property. The other is a three-legged intersection where Highway 51 passes over Lexington Avenue and turns on and off Highway 51 occur at a new T intersection further north. This design creates a dramatically more comfortable crossing of Highway 51 for bikes and pedestrians, addressing a frequently mentioned issue heard during public engagement events. The grade separation also better supports future Amtrak passenger rail. **The City recommends the three-legged intersection.**

Highway 30

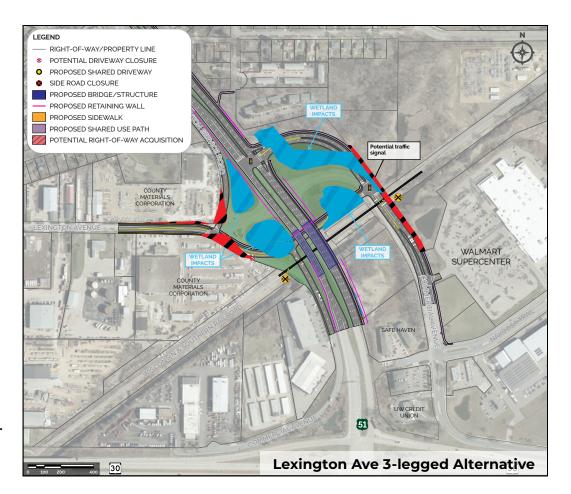
Two options are being considered for the Highway 30 interchange: a <u>conventional interchange</u> similar to the current design and a <u>diverging diamond interchange</u>. The City recommends the conventional interchange as crossings are much simpler for bikes and pedestrians.



At-grade intersection, no bridges required for this alternative



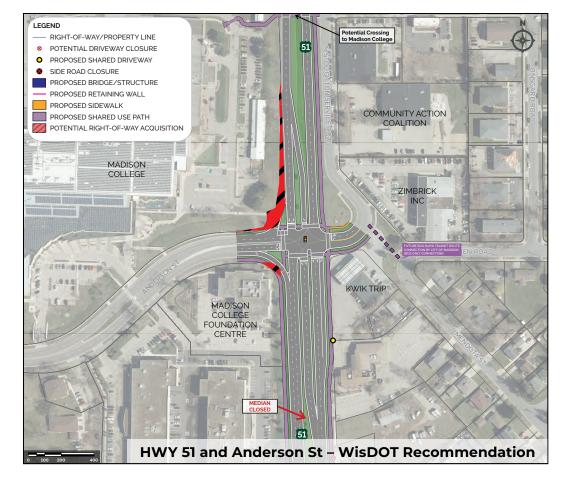
Grade separated intersection, bridges required for southbound U.S. 51 movement in this alternative



Grade separated intersection, bridges required for this alternative. Railroad of intersection is being investigated for potential high-speed rail.



At-grade intersection, no new bridges required for this alternative. Railroad south of intersection if being investigated for potential high-speed rail.



Anderson Street

WisDOT is recommending an intersection very similar to what is there now. Anderson Street is a critical bike and pedestrian crossing, linking the Sandburg neighborhood with Carpenter Ridgeway and shared-use paths connecting to central Madison. Signal timing and phases should ensure comfortable crossing. This intersection will also serve the future BRT crossing and link to Mendota Street.

Bike and Pedestrian Paths:

WisDOT's plans generally reflect the City's planned shared-use path on the east side of Highway 51 south of Kinsman Boulevard. North of Kinsman, there is no sidewalk or shared-use path, preventing safe use of this stretch by those not in motorized vehicles. The City recommends WisDOT include this shared-use path in its plans. If this is not possible, WisDOT should include and fund an alternative path or on-street protected bike lane on a parallel route such as Portage Road.

WisDOT Interstate Study

WisDOT is also studying Interstate 39/90/94 between the Beltline and Wisconsin Dells. WisDOT indicates the study is intended to address existing and future traffic demands, safety issues, and aging and outdated infrastructure.

Interstate

Throughout the Madison area, including the segment that is adjacent to the Northeast Area, WisDOT is proposing to add one lane to the Interstate in each direction. The segment of the Interstate north of Highway 151 would be expanded from 6 lanes to 8 lanes. The portion of the Interstate south of Highway 151 would be expanded from 8 lanes to 10 lanes. To the south of Highway 151 there would also be multi-lane on-off ramps running parallel to the 10 lanes. These "collector-distributor lanes" reduce potential conflict points where drivers can merge between lanes, but they take up a lot of space.

The proposed additional lane in each direction and collector-distributor lanes would result in the Interstate having 17 total lanes in some locations. This increase in the number of lanes would bring traffic closer to noise-sensitive uses along the Interstate such as homes and parks. In some locations, WisDOT would need to acquire private property for this expansion. Additionally, while adding lanes to a highway reduces traffic congestion initially, it incentivizes people to drive more and drive further distances. Over time, as more people drive more and drive further, the highway just becomes congested again.

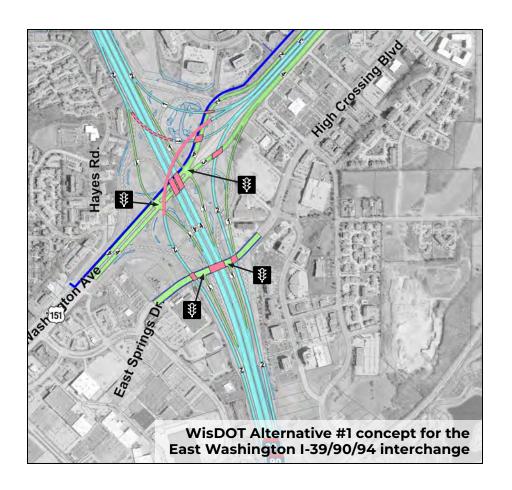
The City has provided comments to WisDOT on the proposed Interstate expansion. The City is suggesting, at a maximum, the addition of a "flex" lane in each direction, which is only open during heavier traffic times. Madison also suggests removal of the separate collector-distributor lanes.

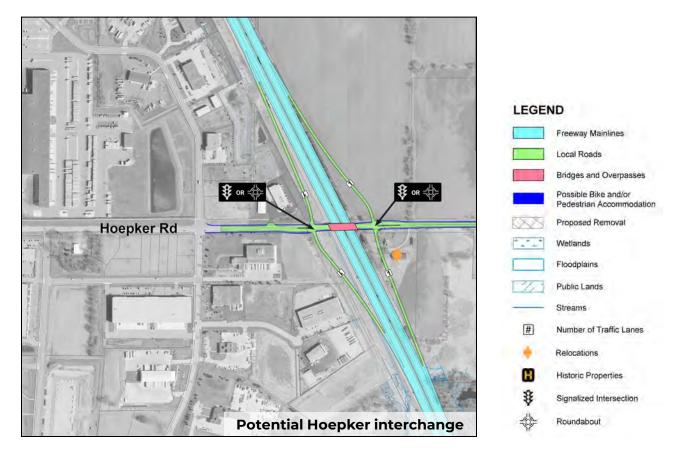
Existing Interchanges

The Interstate project would also reconfigure each interchange. The four existing interchanges adjacent to the Northeast Area are being considered for the following:

- Highway 51 interchange: generally maintain the existing configuration. A new north-south shared-use path through the interchange would be added.
- Highway 151 (East Washington Avenue) interchange: a significant reconfiguration and expansion.
- High Crossing Boulevard partial interchange: generally maintain the existing configuration.
- Badger Interchange (where I-94 meets I-39/90): a significant reconfiguration that would make the interchange taller, but the footprint slightly less expansive.

The Highway 151 (East Washington Avenue) interchange would be significantly different. WisDOT and the City's preferred configuration is a conventional diamond interchange with traffic signals along East Washington Avenue. This configuration does not have any "free flow" on and off ramps that currently exist. Free flow ramps contribute to higher traffic speeds on East Washington Avenue. Reconfiguration of the interchange will help transition the eastern end of East Washington Avenue from a higher speed highway to a City street. Lower speeds will be safer for pedestrians and bicyclists traveling to destinations in





this area, including the BRT stations. WisDOT has also incorporated a shared-use path along the north side of the proposed interchange to connect the east and west sides of the Interstate.

Potential Hoepker Interchange

A potential new interchange is being considered at Hoepker Road. An interchange at this location would improve access to regional medical facilities located in the American Center such as UW Health's East Madison Hospital and Eastpark Medical Center. It would also benefit employers such as American Family Insurance and Alliant Energy while potentially facilitating further development on underutilized sites in the American Center.

An interchange would also serve planned residential development to the northeast of a potential interchange. The City has received residential development proposals and DeForest Area School District has purchased a property for a future school site. This area will experience increased airport noise based on the Airport's latest sound estimates.

Beyond these two areas, future City of Madison development in the larger interchange area is somewhat limited by existing development, the Airport and Airport height restrictions, the City's future municipal boundary with Sun Prairie, and environmental areas associated with Cherokee Marsh, Token Creek, and Starkweather Creek.

WisDOT generally supports a Hoepker interchange because it would reduce traffic on Highway 51 north of the Airport. It would also reduce traffic traveling through the Interstate/Highway 151 interchange. The City of Sun Prairie supports a Hoepker interchange as it would provide a connection between the Interstate and west side of the community, including the Prairie Lakes commercial area and adjacent residential areas.

Hoepker Road to the east of the Interstate, and Portage Road in this area, are currently two-lane, rural roads. These roads would need improvements to accommodate increased traffic from an interchange. Preliminary WisDOT traffic projections for 2050 indicate an interchange would increase future Hoepker Road traffic levels east of the Interstate by about 5,000 vehicles per day. Projected 2050 traffic levels on Portage Road, between Hoepker Road and Hanson Road/Eastpark Boulevard, would increase by about 2,000 vehicles per day.

WisDOT may require local municipalities to pay for a portion of the interchange's cost. Madison's interchange assessment policy requires area property owners to pay for a portion of the cost based on how much traffic their land use generates. Several Dane County municipalities have funded new highway interchanges through tax increment financing, where the tax revenue generated from increased development covers the cost.

Prioritized Non-Motor Vehicle Crossings

To increase pedestrian and bicycle connectivity across the Interstate, the <u>City has emphasized and prioritized crossings</u> that should be added as part of this Interstate project. The highest priority crossings within the Northeast Area are along the rail line north of Lien Road, along East Washington Avenue/Highway 151, and between Hayes Road/DiLoreto Drive and Eastpark Boulevard. Potential

pedestrian and bicycle crossings near Anderson and Hoepker Roads are slightly lower priorities. Like interchanges, WisDOT may require the City to pay for a portion of the cost.

The City also requests that each street crossing of the Interstate include sidewalk and bicycle accommodations on both sides of the street. Currently, the Hanson Road crossing is only the crossing that has full accommodations.

Sound Barriers

Over time, sound walls have been constructed along portions of the Interstate and Beltline as part of expansions of these highways. WisDOT is evaluating areas along the Interstate that do not have sound barriers, but have higher numbers of residential units and other noise-sensitive land uses such as parks, to determine whether they qualify. In areas that qualify, property owners and residents vote to determine whether a sound barrier is ultimately constructed for that segment of the Interstate. Sound barriers are typically up to 24 feet tall and reduce sound for areas within a few hundred feet of the wall.

Actions

Road Connectivity and Traffic Circulation

- 1. Officially Map future public streets as shown on the Street Network Map and reserve sufficient public right-of-way to implement the planned street network.
- 2. Consider short and long-term improvements to Fair Oaks Avenue, Portage Road, Thompson Drive and other wide neighborhood streets that reduce unneeded street width to slow vehicular traffic, allow for tree plantings, improve bike, and pedestrian experience and reduce impervious surface. Short-term improvements could include bump-outs to shorten crossing distances and to allow for tree plantings, while long-term reconstructions could reflect Complete Green Street typologies.
- Remove the street segments as shown on the Street Network Map and transition underutilized or unutilized roads or right-of-way into developed areas that have buildings that are close to the street and easily accessible to pedestrians.

Pedestrian and Bicycle Network

- 4. Add sidewalks and other connections shown on the Pedestrian Network Map to improve connectivity and safety. Seek grants or other funding sources to add Priority Connections on high traffic corridors, near schools, or other primary pedestrian routes.
- 5. Add pedestrian and bicycle improvements at the intersections shown on the Pedestrian Network map to improve safety. Highest priority crossings are those on Highway 51 and East Washington Avenue.
- 6. Implement the bicycle network connections shown on the Bicycle Network Map to improve connectivity and safety.
- 7. Update the Complete Green Streets Guide's Street Type Map to incorporate changes to the street types to ensure that streets are designed according to the existing and planned surrounding context.

Partnerships

- A. Work with WisDOT to ensure changes to the Highway 51 corridor aligns with the City's vision for land use and transportation as discussed in this section:
 - i. At East Washington Avenue, the City opposes the closures of Rowland Avenue and MacArthur Road.
 - ii. At Lexington and Commercial Avenues, the City recommends the three-legged intersection.
 - iii. At Highway 30, the City recommends the conventional interchange.
 - iv. At Anderson Street, ensure the intersection design supports comfortable and safe bike and pedestrian crossings, as well as BRT access to Mendota Street.
 - v. The City recommends WisDOT include the shared-use path on the east side of Highway 51 from Kinsman Boulevard to Anderson Road. If this is not possible, WisDOT should include and fund an alternative path or onstreet protected bike lane on a parallel route such as Portage Road.
 - vi. The design should support existing and future redevelopment by limiting right-of-way expansion. If additional right-of-way is needed, the City should work with the WisDOT to acquire surplus parcel remnants and facilitate redevelopment through a request for proposal process.
- B. Work with WisDOT to ensure changes to the Interstate/East Washington Avenue intersection aligns with the City's vision for land use and transportation.
 - i. Reduce speeds on East Washington Avenue. Free flow ramps and lanes from the Highway 151 freeway to East Washington Avenue and the lack of signals at the interchange contribute to speeds that are not appropriate for a city street. Madison's Vision Zero program recently reduced the speed limit on this section of East Washington Avenue to a more appropriate 35 mph. Alternatives that extend an urban East Washington Avenue with signals will help reduce speeds.
 - ii. Improve connectivity for all modes of travel, including to The American Center, the High Crossing area, and across East Washington Avenue near Hayes Road. The existing interchange design isolates each quadrant, making connections difficult without a car. It is very difficult to access The American Center without using a freeway and it is almost impossible by bike or as a pedestrian. Providing an urban street connection to The American Center helps connect it to the whole of Madison.
 - iii. Reduce noise impacts to surrounding properties by minimizing elevated ramps. Generally, ramps that are not elevated have less potential to transmit highway noise.
 - iv. Transition the character of East Washington Avenue to an urban street to enhance development opportunities. This includes adding sidewalks, street trees, improved street crossings, allowing driveway access, and reducing travel lanes between Stoughton Road and Interstate 39/90/94.
 - v. Changes should enhance connectivity for Bus Rapid Transit (BRT) Service.

Neighborhoods and Housing

Comprehensive Plan Goals

This Plan aims to advance the following goals:

- Madison will be a safe and welcoming city of strong and complete neighborhoods that meet the needs of all residents.
- Madison will have a full range of quality and affordable housing opportunities throughout the city.

Madison is often recognized as a desirable place to live and experiences steady population growth. Some of the main factors driving people to move to Madison include the strong economy and job market, educational opportunities, cultural amenities, and overall high quality life. Since 2010, new housing development has not kept up with demand and has resulted in Madison having one of the lowest residential vacancy rates in the country. The persistent housing shortage increases the cost of housing and makes finding a place to live in Madison difficult. The current housing shortage is likely to continue despite ongoing efforts to increase housing availability through new construction.

The Comprehensive Plan recommends a majority of future growth to occur as infill and redevelopment. Madison is gradually losing the ability to expand outward. In addition, extending City services for development at the edge of the city is more costly. For these reasons, the City's ability to accommodate growth through redevelopment is increasingly important. Future growth should be focused along activity centers and corridors identified in the Comprehensive Plan. See the Growth Priority Areas map. One such location is the East Towne Mall area, which is roughly 300 acres of parking and single-story commercial buildings. This activity center alone could accommodate approximately 12,000 new housing units over time if it were redeveloped following Plan recommendations. Redevelopment on underutilized parking areas or commercial sites also provides a benefit of not displacing residents.

Affordability

The need for housing that is affordable to low- and moderate-income households was one of the most common issues identified by stakeholders during the planning process. The Northeast Area is relatively affordable compared to many parts of Madison. Of the twelve Area Plan geographies, the Northeast Area has the lowest average value for owner-occupied houses and rental houses.1 The median rent was \$1,013 per month compared to Madison's median rent of \$1,189².

The City does not currently have the ability to regulate rents or require new developments to include affordable housing. Because of this, the City's most impactful way to increase the supply of new income- and rent-restricted affordable rental housing is through financial assistance programs offered by the Community Development Division (CDD) and Economic Development

What We Heard

- 1. Increase the supply of affordable housing.
- 2. Have a mix of housing types and sizes, including housing units large enough to support large families.
- 3. Have complete neighborhoods with easy access to amenities such as parks, schools, and stores.
- 4. Improve the quality of existing rental housing.
- 5. Ensure sufficient support for unhoused residents.
- 6. Community concerns about spillover effects from the current Zeier homeless shelter and the planned shelter at Bartillon Drive.

Affordable housing is housing for which occupants are paying no more than 30 percent of gross household income for housing costs, including utilities. Households with costs exceeding 30% of income are considered housing cost-burdened. The City's owner-occupant and rental development subsidy programs are primarily designed to reduce housing cost burdens for renter households with incomes at or below 60% of Dane County's median income (CMI) and for owner-occupied households at or below 80% of CMI.

¹ Madison Neighborhood Indicators Project 2022

² Source: American Community Survey 2021 5-year estimate

Northeast Area City of Madison Since 2000 15.7% 25.2% 1980–1999 33.4% 22.3% 1960–1979 35.6% 24.8% 1940–1959 11.5% 13.8% Before 1940 4.8% 13.9%

Source: Census/American Community Survey

Division (EDD). The Affordable Housing Fund-Tax Credit (AHF-TC) and Tax Increment Financing (TIF) programs work in cooperation with the Wisconsin Housing and Economic Development Authority's Low-Income Housing Tax Credits as the City's primary tools for increasing the supply of affordable rental housing. The Affordable Housing Fund prioritizes areas well-served by transit as it can reduce household transportation costs. See the AHF-TC Map. Within the Northeast Area, the East Washington Avenue BRT corridor is generally identified as a preferred location for development of affordable housing. An exception is west of Fair Oaks Avenue, which has

limited eligibility resulting from airport noise impacts. CDD also provides financial support to smaller-scale, non-tax credit affordable rental developments, including housing cooperatives, and to affordable homeownership opportunities.

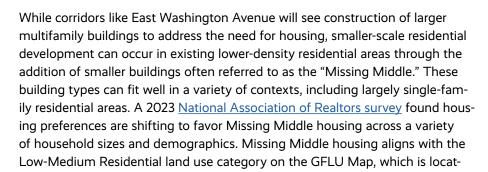
Stakeholders also discussed expanding support and resources for Madison residents who are unhoused. Some stakeholders raised concerns about the impact of the previous encampment of unsheltered individuals at Reindahl Park during the COVID-19 pandemic. During the pandemic, many people avoided congregate shelters, leading to an increase in unsheltered homelessness. After closing the temporary permissible encampments in 2021, the City implemented alternative options, including a temporary campground at Dairy Drive, a short-term hotel program, and an improved congregate shelter. In addition, the temporary shelter at Zeier Road is providing time to plan and construct a permanent shelter on Bartillon Drive. There is not a simple solution to end the complex issue of homelessness and housing instability. Instead, there are multiple ways of housing people to meet their needs and to work toward permanent housing opportunities.



Existing Missing Middle homes in the Grandview Commons neighborhood

Mix of Housing Types

As current residents age, new residents move to Madison in large numbers, and racial and ethnic diversity continue to increase, it is important to have housing options to accommodate all stages of life and living arrangements. In addition to increasing the supply of new affordable rental housing, redevelopment and infill should include market rate apartments and homeownership opportunities, including condominiums. The Northeast Area's split between rental and owner occupancy is similar to Madison overall, but the demand for owner-occupied housing remains high. The City's CDD also provides financial support for owner-occupied development, including co-housing, condominiums, and community land trusts.





Rendering of the future homeless shelter at Bartillon Drive

ed at select areas along Portage Road, Thompson Drive, and areas near East Washington Avenue.

Complete Neighborhoods

Building housing alone does not create complete neighborhoods. Elements like public parks, schools, gathering spaces, local businesses, and access to healthy food are also needed. The Northeast Area has four full-service grocery stores and several smaller convenience stores, specialty stores and ethnic grocers. However, the location of those stores can create barriers for residents without cars, especially those living in the Sandburg area. For more information, see the City's Food Access Improvement Areas map.

Schools are also catalysts for establishing strong neighborhoods, as children's activities, daily social interactions, and community projects create community connections. The Northeast Area is primarily within the Madison Metropolitan School District (MMSD). This includes the Hawthorne and Sandburg Elementary attendance areas, which area affiliated with Sherman and O'Keeffe Middle

cv Woods Dane County Regional Airport 8151B

The City of Madison Community **Development Division's Housing** Development and Financing Programs provide loans to for-profit and non-profit housing developers for the construction and rehabilitation of income- and rent-restricted affordable rental and owner-occupied housing.

Complete neighborhoods are neighborhoods that include a range of housing types and costs, neighborhood-serving businesses, stores, services, schools, and places of worship. These elements are ideally accessible by foot, bicycle, or transit through a network of well-connected streets and blocks, usable public spaces, and a system of connected parks, paths, and greenways.

Northeast Area Plan **Elementary School Attendance Areas**

School

Elementary Attendance Area

Hawthorne Elementary Kennedy Elementary

Sandburg Elementary

Schenk Elementary Windsor Elementary (DeForest Area School District)

Meadow View Elementary (Sun Prairie Area School District) Schools and East High School. The Schenk Elementary and Kennedy Elementary attendance areas are affiliated with Whitehorse Middle School and LaFollette High School.

The northern half of the Northeast Area is in DeForest Area School District's (DASD) Windsor Elementary attendance area. The area east of Zeier Road falls within the Sun Prairie Area School District's (SPASD) Meadow View Elementary attendance area. Both DASD and SPASD own potential elementary school sites just outside the Northeast Area that would likely serve Northeast Area residents. DASD owns a site near the intersection of Portage Road and Hoepker Road. SPASD owns a site at the intersection of Felland Road and Autumn Lake Parkway.

Actions

- Incentivize affordable housing, particularly on the East Washington Avenue and BRT corridors, through the Affordable Housing Fund, Land Banking initiative, and Tax Increment Financing.
- 2. For developments receiving City funding, incentivize a wider mix of rental unit sizes, including those large enough to support larger and multigenerational families (3-4+ bedrooms).
- 3. Consider designating a neighborhood in the Northeast Area as a <u>targeted</u> area for the <u>Rental Rehab Loan Program</u> to further incentivize improvements to aging rental housing properties in lower-income communities.
- 4. Invest in food retail through funding and technical assistance to store operators through the Healthy Retail Access Program and SEED Grants, particularly neighborhoods north of East Washington Avenue which are identified as Food Access Improvement Areas (see Food Access Improvement Areas map).

Partnerships

- A. Establish a community advisory board with representatives from the homeless shelter at Bartillon Drive, local residents, City and County staff, businesses, and community organizations to provide ongoing feedback and guidance for shelter operations and the shelter's impact on the neighborhood.
- B. Create a dedicated communication channel such as an email address or a hotline for residents/businesses to report issues or concerns related to operations at the homeless shelter at Bartillon Drive.
- C. Pursue additional partnerships with Dane County Human Services and nonprofits to ensure renters in the Northeast Area have access to support needed to thrive, including onsite support services. This may include exploring an increase in funding for Dane County's Joining Forces for Families program to expand services available to the Northeast Area.

Economy and Opportunity

Comprehensive Plan Goals

This Plan aims to advance the following goals:

- Madison will have a growing, diversified economy that offers opportunity for businesses and residents to prosper.
- Madison will have equitable education and advancement opportunities that meet the needs of each resident.

The Northeast Area is a pivotal employment hub, supporting 1,000 businesses and other entities with approximately 20,000 jobs¹. Over 30 entities have more than 100 employees. While employment growth in the Northeast Area is uncertain, the Wisconsin Department of Workforce Development projects an

11.2% increase in employment in and around Dane County between 2020 and 2030.

Despite its economic significance, some areas within the planning area face significant economic challenges. Over 25% of Sandburg and approximately 20% of Burke Heights and Mayfair Park residents have incomes below federal poverty thresholds2, emphasizing the need for targeted economic intervention. Burke Heights has one of the lowest median incomes in the city, at slightly over \$39,000³.

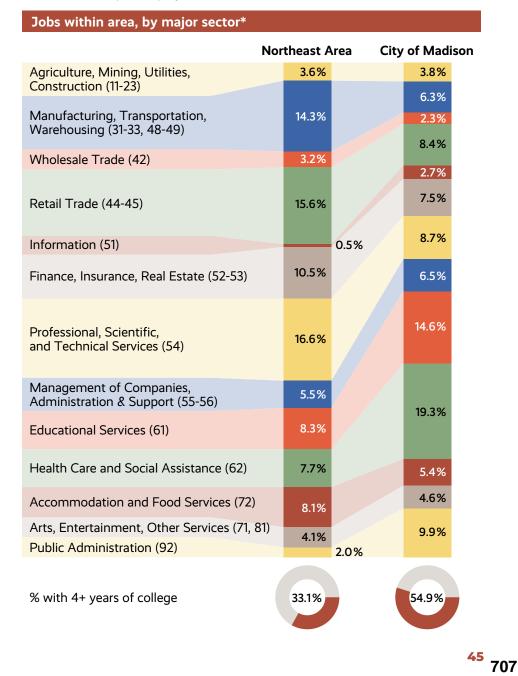
Industrial Development

Industrial land accounts for 14% of the Northeast Area. The Center for Industry and Commerce and the Interstate Commerce Park, both industrial hubs, are situated on the east side of Highway 51. The area northwest of Hoepker Road and Highway 51 is within the Town of Burke and currently lacks access to water and sewer services. According to the Town of Burke Intergovernmental

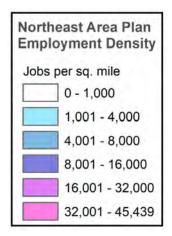
- 1 Source: OnTheMap 2021 data
- 2 Source: Census/American Community
- 3 Source: Neighborhood Indicators Project

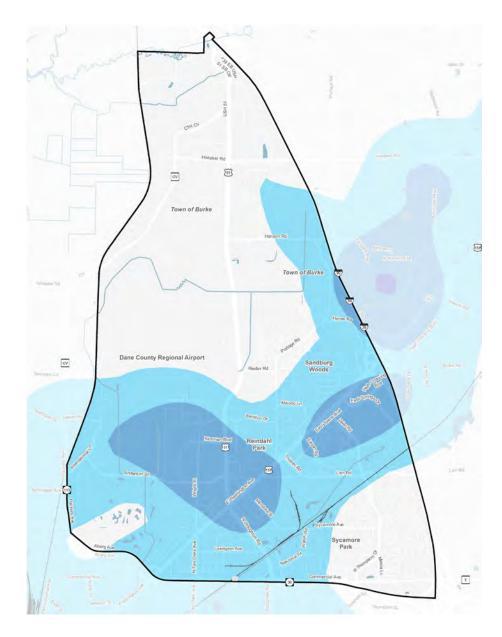
What we Heard

- 1. Promote local business growth, emphasizing the significance of fostering local entrepreneurship and neighborhood-serving activities.
- 2. Improve access to essential services like grocery stores and childcare to support families.



Northeast Area Plan - Draft





Agreement, the Village of DeForest is responsible for providing water service to the area, but the area will become part of Madison in 2036. This arrangement complicates the development process and requires coordination with multiple municipalities for utilities and approvals. See the Effective Government chapter for more information about intergovernmental cooperation.

Service and Retail Sector

Between 2016 and 2021, service and retail employment in the Northeast Area declined by over 1,400 jobs⁴. The decline is likely due to a number of factors, such as online shopping, retail growth in neighboring Sun Prairie, and lasting impacts from the COVID-19 pandemic. Several large vacant spaces are in and around the East Towne area, including the former Boston Store and Sears stores. While these spaces could be repurposed, they could also be redeveloped into large amounts of housing, and new residents would support existing and future businesses.

⁴ Source: OnTheMap 2021 data

Local Business Survey

As part of the planning process, the City's Office of Business Resources conducted a business survey to understand perspectives within the area. With the assistance of the Northeast Area Community Navigators, responses from a total of 30 businesses indicated the following:

- Customer Base and Transportation: Most customers come from outside the planning area by driving or taking transit.
- Parking as a Vital Resource: With most customers driving, businesses heavily depend on parking facilities for their operations.
- Workforce Challenges: Over 55% of respondents discussed the difficulty of finding and hiring qualified employees. The workforce composition reveals that only 25% are full-time employees, with 13% being seasonal. Some respondents also mentioned a shortage of employees in the trades, such as carpentry and construction.
- Location Appeal and Concerns: Some business owners appreciate the location, particularly due to ample parking space. Others discussed airport noise as a notable drawback, negatively impacting the overall business environment.
- Community Improvement Suggestions: Several businesses identified areas of improvement, emphasizing the need for enhanced bus service and bike paths, which can help with hiring and retaining employees.
- Challenges Linked to Homelessness and Crime: Some indicated the rise in homelessness and crime rates in the area is adversely affecting their businesses.
- Lower incomes in the Area: Some respondents suggested that the limited discretionary income could potentially impact local businesses. This implies that businesses might benefit from a diverse mix of income levels in the area.



Northeast Area Plan Tax Increment District (TID) Tax Increment District #52

Actions

- 1. Establish a Greater East Towne Area Implementation Team to assist its redevelopment while also addressing vacant business spaces and parking lots.
- Promote awareness and continue implementation of small business development programs funded through Tax Increment District (TID) #52 and other future area TIDs. These programs include the Facade Grant Program, Building Improvement Grant Program, Small Cap TIF Program, and Commercial Ownership Assistance Program.
- 3. Utilize Land Banking program to buy underutilized properties to facilitate redevelopment that includes employers.
- Concentrate City resources on enhancing the business area along the International Lane corridor south the airport. Optimize available commercial spaces to their full potential to attract investment and foster economic growth.
- Actively promote the <u>Foreign Trade Zone</u> adjacent to Dane County Regional Airport to attract businesses that offer stable employment opportunities. Efforts should focus on attracting more businesses to create employment opportunities for local residents.

Partnerships

- A. Address the childcare needs of working families by collaborating with local childcare providers and community organizations to enhance access to quality childcare services.
 - i. Broaden the eligibility of Madison College's childcare facility and bring in providers such as Reach Dane and others to commercial spaces of new mixed-use buildings such as the former Bimbo bakery site.
 - ii. Assist with creation of childcare cooperatives and in-home childcare and encourage employers in the area to provide on-site childcare.
- B. Explore creating a program similar to the Madison Public Market's "Market Ready" program to help new businesses locate within commercial space in the Plan area:
 - i. Explore gap assistance to help businesses displaced by redevelopment afford ground-floor space in new mixed-use buildings.
 - ii. Conduct an equity-based market study of the area to further explore strategies for strengthening the economy, with an emphasis on ensuring that underrepresented populations are included in redevelopment, ownership, entrepreneurship, and employment opportunities.
 - iii. Incorporate networking, mentorship, and collaboration opportunities as part of business planning.
 - iv. Work with local commercial brokers and property owners to connect small businesses to available space as they "graduate" from the program.
- C. Support creation of an East Washington Avenue Business Association to connect business owners with each other and with the City to facilitate economic growth and development.
- D. Explore creating a business or neighborhood improvement district along East Washington Avenue to help make public spaces more welcoming to residents, maintain amenities, facilitate events, and promote the area to customers.

Culture and Character

Comprehensive Plan Goals

This Plan aims to advance the following goals:

- Madison will be a vibrant and creative city that values and builds upon its cultural and historic assets.
- Madison will have a unique character and strong sense of place in its neighborhoods and the city as a whole.

Through an examination of community activities, architectural elements, and the impact of local organizations, this Plan aims to provide recommendations that contribute to the culture and character of the Northeast Area. It emphasizes the vision of a central urban plaza that celebrates diverse cultures within the existing East Towne Mall area, proposes changes to Urban Design District (UDD) 5, assigns new potential design guidelines to three additional sub-areas, and explores opportunities for inclusive art and entertainment to enhance vibrancy.

The Greater East Towne Area Plan outlined a vision for the transformation of areas around and including the mall, setting the stage for dynamic growth and revitalization. This Plan recommends expanding the geographical boundaries of UDD 5 and revising UDD 5's recommendations to achieve a vibrant and inclusive urban environment.

The city's population is increasingly diverse. Enhancing the distinctive qualities that define the Northeast Area will ensure it remains inclusive and welcoming to individuals of every race, ethnicity, gender, age, and ability. The Comprehensive Plan advocates for dynamic and inviting spaces, preservation of historic sites, the creation of secure and affirming community spaces, the integration of public art, and the facilitation of opportunities for learning, creativity, collaboration, and arts enjoyment.

The Northeast Area has several of Madison's eastern gateways, with two pivotal intersections: East Washington Avenue at Interstate 39/90/94, and Highway 51 and Interstate 39/90/94. These intersections are the initial impression of the city, underscoring the importance of recommendations that enhance the area's distinct character. There is significant potential to enhance the Northeast Area's walkability and vibrancy, primarily within and around the Highway 51 corridor, East Washington Avenue, Commercial Avenue, and East Towne Mall.

What we Heard

- 1. Create welcoming and safe neighborhoods in the Northeast area by making the streets look nice, adding community art, using signs in different languages, and asking residents for their ideas in the planning process.
- 2. Design a culturally-affirming central green and open space that accommodates diverse public activities and events.
- 3. Explore opportunities for the development of inclusive art concepts and cultural entertainment to enhance vibrancy throughout the area.



Amund Reindahl's barn, located in Reindahl park

Historical Resources

There are no City-designated landmarks or properties listed in the National Register of Historic Places in the Northeast Area. However, staff has identified several sites with architectural or historic interest which may be eligible for designation (see appendix: Historical Resource Survey). There are also several previously identified archaeological sites, several of which are human burial sites. These sites may require additional review and consultation with the Ho-Chunk Nation and Office of the State Archaeologist. The archaeological sites could also be eligible for local or federal historic designation. Properties listed in the National Register are potentially eligible for preservation tax credits for work on buildings, and burial sites are eligible for a property tax exemption.

Actions

- Revise and update Urban Design District (UDD) 5 to require new development to be pedestrian-oriented, including comfortable spaces and visually appealing elements along East Washington Avenue. See proposed UDD 5 revisions at the end of this chapter.
- Support a variety of cultural and entertainment venues as the area redevelops. This should include spaces that vary in size and are well suited for a variety of visual and performing arts to make them easily accessible to area residents.
- 3. Add art and placemaking elements through expanded use of the utility box wrap program.
- 4. Provide residents with information and assistance about how to create and host festivals or community events within Reindahl and Sycamore Parks.
- 5. Add benches along Starkweather Creek.
- 6. If East Towne Mall redevelops, add a prominent art piece within the planned urban plaza and connect the new neighborhood with Sycamore Park through arts programs such as Art in Public Places and Poetry in Sidewalks.
- 7. Inform property owners who can officially document sites identified during historic resource surveys (see <u>Historic Resources Map</u>) with historical value through the National Register process.





Utility box wrap at Regent St

Former Poet Laureate, Angela Trudell Vasquez is kneeling in front of her poem, "The Other America Tour...," at the sidewalk along Atwood Ave.

Partnerships

- A. Provide a placemaking best practices guide to businesses and organizations in the area to provide an alternative use of vacant sites and underutilized parking lots.
- B. Collaborate with local businesses and community organizations to beautify the area by addressing unsightly businesses and wasted space like parking lots, areas with excessive amounts of litter, or buildings that are poorly maintained. This may involve current City initiatives like the Facade Improvement Program, Neighborhood Grant Program, and Arts grants.
- C. Improve the Northeast Area's primary gateways at Interstate 39/90/04 and Highway 51 and Interstate 39/90/94 and East Washington Avenue. Enhancements may include:
 - i. Facilitate the construction of higher-density buildings at these key corners to enhance the appearance and create a sense of enclosure. This approach aims to reduce vehicle speed, signaling to drivers that they have transitioned from a highway to city streets.
 - ii. Where possible, introduce visually appealing landscaping and streetscaping elements to establish a more inviting atmosphere at these entrances.
 - iii. Incorporate public art installations or sculptures to infuse the gateways with cultural and artistic elements. This aims to cultivate a distinctive identity and foster community pride.
- D. Invite residents of all backgrounds to participate in a community-based process to design public gathering spaces and streetscapes within or around the existing East Towne Mall area and Commercial Core so this commercial area redevelops into more of a neighborhood-serving place, where people will be comfortable expressing their culture and feel that they belong.
- E. Consider providing art mini-grants and how-to guides to local businesses for permanent exhibition materials, such as art hanging systems and lighting, to generate more opportunities for local artists to share their work with the public.
- F. Work with Dane County and residents to identify community-oriented interim uses for the greenspace in the Wright Street triangle, such as picnic areas, temporary outdoor fitness equipment, and public art installations.
- G. Enhance the aesthetics of vacant facades by installing murals that resonate with the community, such as the telecommunications building at Rowland Avenue and East Washington Avenue.
- H. Explore opportunities for the temporary use of vacant or transitional spaces to support emerging artists while adding to the vibrancy of the area. This may include pop-up galleries, artist-in residencies, and murals.
- I. Provide spaces where people are comfortable expressing their culture and feel that they are part of a community. This applies to streets, sidewalks, paths, transit stops, and other public amenities as well as private development. It may include physical amenities such as multilingual signage, permanent or temporary art, poetry in sidewalks, and murals.



Madison Mural at a parking lot at Monroe St and Commonwealth Ave.

Urban Design District 5

Urban design is also an important contributor to culture and character. Madison's Urban Design Districts (UDDs) contribute to the aesthetics of the built environment in addition to some aspects of urban design regulated in the zoning code. The City's UDDs generally ensure development contributes to creating a high quality, well-designed built environment within certain areas of the city. There are eight UDDs that are primarily located along the city's major transportation corridors. Each Urban Design District is unique and establishes mandatory requirements and advisory guidelines for new development, buildings, remodeling, or additions to existing buildings. Some districts are more detailed than others. While the requirements and guidelines are different in each district, generally they address building design (height, setbacks, and stepbacks), design and quality of exterior materials and architectural detailing, lighting, signage, the design of private open space, land-scaping, and screening.

Portions of UDD 5 are within the Northeast Planning Area. It currently runs along East Washington Avenue, generally from Interstate I-90/94/39 to Sixth Street. This Plan recommends revisions to UDD 5 which are anticipated to be further studied and refined as part of a separate, citywide UDD review process. This Plan recommends expanding the Urban Design District and establishing different recommendations for various sub-areas.

Revise and update Urban Design District (UDD) 5 to require that development
is pedestrian-oriented, including comfortable spaces and visually appealing elements along East Washington Avenue, as well as other public streets and views.
As noted in this chapter, revisions to the Urban Design District should create
design standards and guidelines that recognize unique planning areas, including
the recommended design sub-areas noted in this chapter.



- 2. Extend UDD 5 to the Interstate and explore expanding it to include key sites south of East Washington Avenue, the East Towne Mall area, and Highway 51.
- Incorporate Transit-Oriented Development (TOD) overlay district features to UDD 5 requirements and guidelines, such as minimum building heights of two stories.
- 4. Separate UDD 5 by character into three distinct sub-areas to better guide development along the East Washington Avenue corridor, as illustrated in the Proposed Urban Design District (UDD) 5 Changes Map.
- 5. While the western portion of UDD 5 is outside the Northeast Area, given its distinct character, this area should also be considered as an individual sub-area.

The Urban Design District Changes Map recommends various boundary adjustments to UDD 5 to reflect current property lines and include areas expected to see a high level of construction activity.

UDD 5 Design Guidelines - Recommended Updates

- Update the list of approved building materials and design guidelines to reflect the current standards of the City, particularly focusing on the Urban Design Commission's "purpose and intent" to encourage and promote high-quality design in new buildings, including guidance that:
 - a. Buildings should be designed so uses, entrances, and design elements provide amenities to enhance the visual and pedestrian character of the street.
 - b. Street facing facades that lack architectural detail, variety, and windows are discouraged.
 - c. Use changes in building plane to create porches, balconies, roof terraces, entrances, and other active uses are encouraged for residential units/buildings.
 - d. Larger buildings should incorporate facade modulation and changes in plane.
 - e. Porches and balconies should be well-integrated and provide usable open space for tenants.
 - f. If material changes are proposed, they should generally occur at inside corners or be delineated by a specific transitional detail.
 - g. Encourage developers to incorporate artists into the design team to integrate works of art into their projects.
- 2. Site layout and building orientation guidelines at Commercial Core areas are encouraged to have outdoor patios, outdoor dining, awnings, and display windows.



The Madison Youth Arts building on East Mifflin Street exemplifies effective use of facade articulation, offering diverse design elements that enhance visual appeal and create an inviting atmosphere for pedestrians.



Example of Missing Middle housing



Pedestrian and bike friendly sidewalk with green buffer in Saint Paul, Minnesota.

Potential UDD 5 Sub-Areas

UDD 5. Sub-Area 1 Design-Related Considerations: East Washington Avenue Corridor – Aberg Avenue to Thierer Road

This sub-area is characterized by neighborhood retail and lower-scale development. Development in this area should support neighborhood-serving commercial uses to create complete neighborhoods and provide walkable amenities to residents in the area.

- 3. Support "Missing Middle" residential development such as townhomes, and preserve naturally-occurring affordable housing.
- 4. Proximity to institutional and educational uses should be considered in the development of housing and connections to open spaces.

UDD 5, Sub-Area 2 Design-Related Considerations: East Washington Avenue Corridor – Thierer Road to Interstate

This sub-area is characterized by its regional connectivity. Development in this area is recommended for high-intensity, mixed-use development that serves and supports the larger community and region.

- 5. Support Community Mixed-Use development to create complete neighborhoods and provide walkable amenities to residents in the area.
- 6. Given its location at a major highway interchange, sub-area 2 will remain a gateway to Madison. As such, guidelines and requirements should promote a higher level of design aesthetic (high-quality materials, incorporate visual interest, articulation, and modulation in building design, etc.) and encourage appropriate transitions to less intensive development areas.

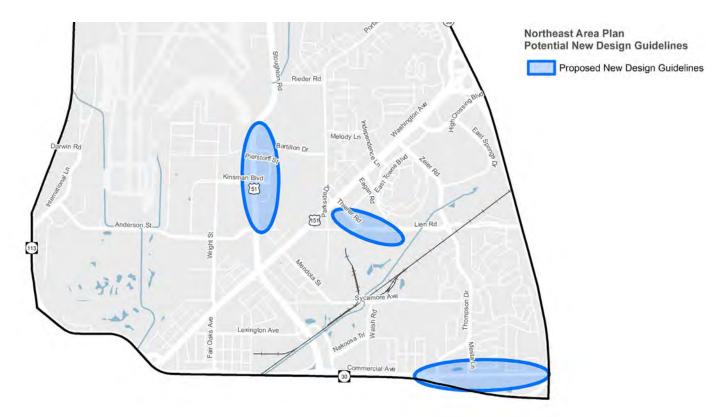
UDD 5, Sub-Area 3 Design-Related Considerations: Greater East Towne Area

This sub-area is intended to create a more multi-modal, transit-oriented development pattern that includes new high-density uses accessed from and oriented to a new local public street network. The area should include a high degree of pedestrian amenities intended to support successful mixed-use redevelopment.

- 7. Development in or near the existing East Towne Mall should consider creating an urban plaza and pedestrian features through and to open spaces when locating building entrances, enhanced sidewalk connections, and pedestrian-orientated uses.
- 8. Construct earth berms along Interstate 39/90/94 or plant dense vegetation such as trees, shrubs, and bushes that serve as both a visual and sound barrier. The irregular surface helps to deflect and absorb sound waves.
- 9. Whenever possible, integrate noise reduction features into the architectural design of residential buildings facing the Interstate and Highway 30. This may include the strategic placement of windows, use of double-glazed glass, and soundproofing materials in walls and roofs.

Additional Design Guidelines

As shown in the Potential New Design Guidelines Map, three areas were identified as future destinations that could benefit from better building and public space design. They also aim to make it easier to get around, add more green-

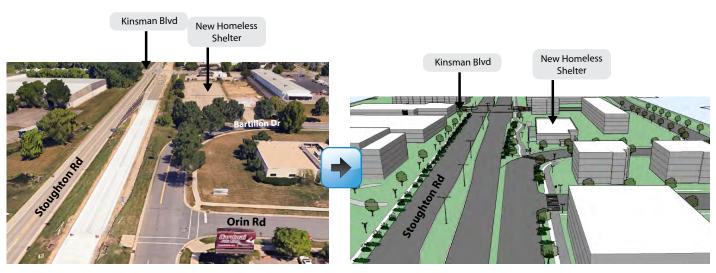


ery, and include features that make spaces feel welcoming and comfortable for people.

These new guidelines are intended to create more attractive and engaging public areas through building redevelopments and street enhancements. The following guidelines will inform proposed infrastructure projects and proposed private developments.

Highway 51 Proposed Design Guidelines

• Incorporate pedestrian-friendly elements such as well-designed lighting fixtures and wider sidewalks to enhance the pedestrian experience.



Note: This image does not intend to illustrate changes of the intersection of Kinsman Blvd and Stoughton Rd. These suggested changes within private land will only occur if property owners decide to redevelop.

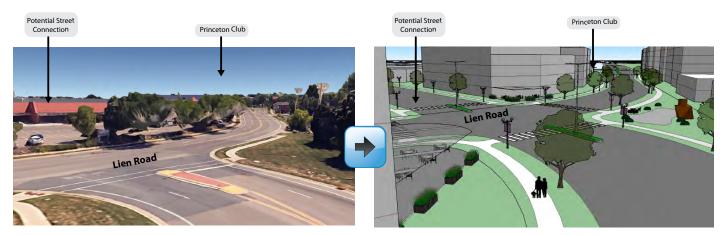
- Relocate utilities underground to eliminate visual clutter, enhance the streetscape, and contribute to a cleaner, more modern aesthetic. This includes transitioning electrical poles from overhead to underground.
- Install clear and well-designed wayfinding signage to guide pedestrians, cyclists, and drivers efficiently. This can include directional signs, informational kiosks, and maps at key points.



- Incorporate an art piece and/or water feature at the intersection of Lien Road and Thierer Road. The artwork should fit with the surroundings, avoiding obstruction of visibility. The chosen piece should serve as a reflection of the cultural and natural character of the area, enhancing the overall aesthetic appeal and local identity.
- Encourage outdoor dining facing the corridor to increase foot traffic for first floor businesses.
- Provide incentives to property owners to replace wire fences with natural barriers like trees, bushes, or water features.
- Build a more visible pedestrian crossing at the Lien Road and Thierer Road intersection, as well as a mid-block pedestrian crossing between Lien Road and Eagan Road.



Bell Street Park, in Seattle, features a well-defined pedestrian crossing that includes greenery and inviting lighting for pedestrians and bicyclists.



Note: These suggested changes within private land will only occur if property owners decide to redevelop.

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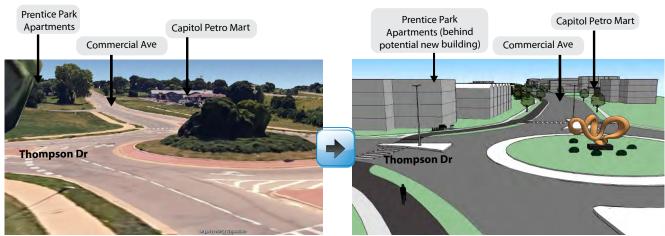
Commercial Avenue Proposed Design Guidelines

- Maximize the use of existing right-of-way to provide bike/pedestrian amenities for adjacent commercial and residential sites.
- Buildings should have storefronts and entrances that are welcoming to pedestrians, such as attractive landscape, inviting signage, well-maintained facades, or outdoor seating areas along Commercial Avenue.
- Implement universal design principles to make the street accessible to people of all abilities, such as detectable warning pads and acoustic pedestrian crossing signals.
- Build pedestrian crossings at Mesta Lane and Eagle Crest Drive for better connectivity to the Ridgewood Neighborhood.
- Consider integrating public art and installations along Commercial Avenue to add cultural and visual interest.
- Implement effective street lighting to enhance visibility and safety, especially during evening hours.

Potential Commercial Avenue Reconfiguration



Note: Redevelopment of right-of-way could also occur to the south or both sides of Commercial Avenue.



Note: These suggested changes within private land will only occur if property owners decide to redevelop.

Green and Resilient

What We Heard

- Improve accessibility to and the experience within parks, open spaces, and natural areas.
- Ensure future growth happens with sustainability as a consideration by appropriately managing stormwater, protecting native species, woodlands, and wetlands, and minimizing the carbon footprint of new buildings and their users.
- Acknowledge climate change and prepare for more frequent and intense weather events, including both precipitation and heat.
- Address past environmental contamination with solutions that protect the health of Madison residents.

Park Expansions

58

Specific Park Expansion Areas identifies locations were future park space is expected.

General Park Expansion Areas show where future park space is desired, but a precise location can't be determined due to land availability, lot patterns or other factors.

Comprehensive Plan Goals

This Plan aims to advance the following goals:

- Madison will be a leader in stewardship of our land, air, and water resources.
- Madison will have a model park and open space system that preserves our significant natural features and offers spaces for recreation and bringing residents together.

Natural areas, open spaces, and parks are major features in the Northeast Area and were frequently mentioned as what makes the area special. Residents also mentioned the need to grow sustainably in the coming years, including protecting wetlands, preserving and adding trees, and avoiding growth near sensitive areas. This chapter addresses the intersection between the built environment and the natural environment, including parks and open spaces, sustainability, ecology, and stormwater management.

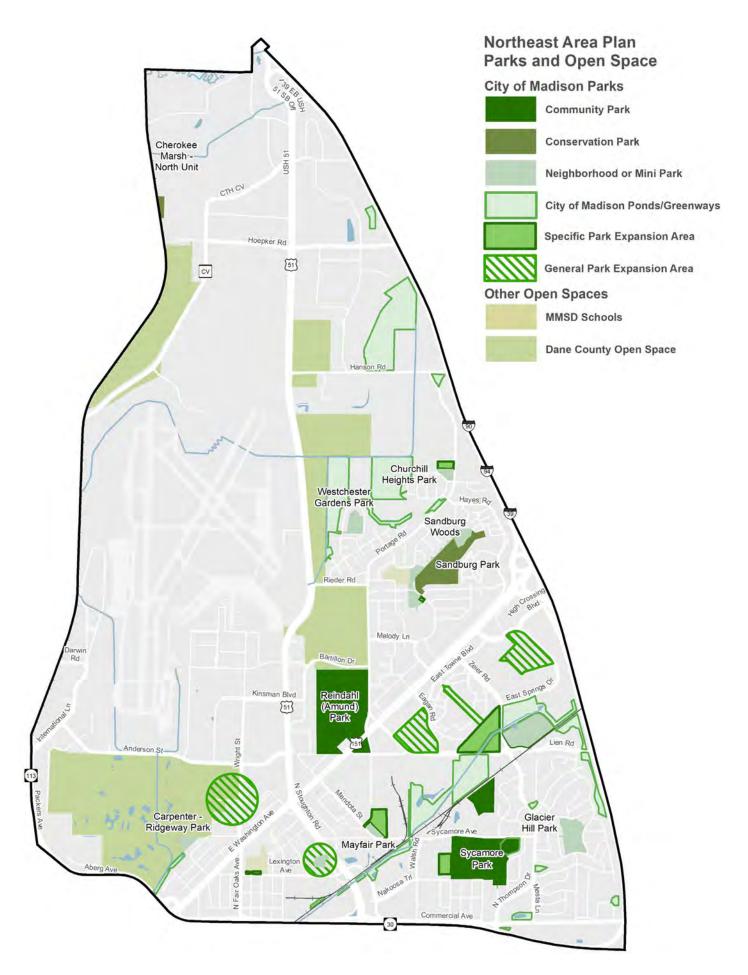
Parks and Greenways

The Northeast Area's variety of parks provide opportunities for a wide range of active and passive outdoor activities, from basketball to birdwatching. The area has 10 parks covering 246 acres that serve its nearly 15,000 residents (17 acres per 1,000 residents), and 82% of households in the Northeast Area are within a quarter mile of a park. However, there is an area north of East Washington Avenue in the Truax Neighborhood lacking in park access. New park space in this area is desired, but much of the vacant land in the area is controlled by the Dane County Regional Airport, which leaves limited options for City park acquisition. Along with addressing park-deficient areas, new park spaces will be needed to serve the recreational needs of the area's future population.

While the variety of amenities in the area's parks are appreciated by residents, continued investments will need to be made to keep pace with the growing population of the area and the changing recreational preferences of residents. For example, Reindahl Park includes the City's only full-size Cricket field and hosts the most soccer fields on the east side. These community park amenities attract residents from across the east side of Madison and beyond. While mini and neighborhood parks tend to focus on serving surrounding neighborhoods, they can also feature unique recreational opportunities that draw from a larger area. Future changes to the City's parks, such as new playgrounds, shelters, and paths are guided by Park Development Plans (for mini and neighborhood parks) and Master Plans (for community parks), which are updated approximately every 10 years.

Reindahl Park

Future changes at Reindahl Park are guided by the <u>2023 Reindahl Park Master Plan</u>. The Master Plan includes the Madison Public Library's Imagination Center, which will include library and community facilities and an indoor, year-round, reservable park shelter. The Imagination Center is scheduled for



Types of Parks

A **mini park** is a small park (generally < 5 acres) with limited recreational amenities that serves the immediate surrounding area.

A **neighborhood park** is generally 5+ acres and serves as the recreational focus of the neighborhood, with more recreational amenities than a mini park.

A **community park** is usually 20+ acres that is designed to serve/attract residents from beyond the surrounding neighborhood.

A **conservation park** is primarily managed to preserve Madison's native landscapes, plants, and animal populations for the careful use and enjoyment of visitors.

Type of Park	# of Parks	Acres in Northeast Area
Mini Parks	3	9
Neighborhood Parks	4	41
Community Parks	2	162
Conservation Parks	1	34
Total	10	246



Reindahl Park



Sycamore Park

construction in 2025. Other elements of the Master Plan, including a new playground, expanded community gardens, and athletic facility upgrades will be implemented over the next 10-15 years as funding and resources allow.

Sycamore Park

The 2023 Sycamore Park Master Plan recommends new trails, a bicycle pump track, and improved connections to the wooded section of the park north of Sycamore Avenue. Other planned improvements include dog park enhancements, a sun shelter, and a restroom building. The adopted Master Plan does not include the recommended extension of Nakoosa Trail through the park to Jana Lane. However, the extension was reviewed and is supported by the Parks Division.

Sandburg Woods Conservation Park

This conservation park features hilly trails through mature forest on the north side of a drumlin. The variety of ecosystems within this conservation park are protected and managed in accordance with the Parks Division's Land Management Plan.

Neighborhood and Mini Parks

Alongside the Northeast Area Plan, the Parks Division completed Park Development Plans for each neighborhood and mini park in the Northeast Area. These address park-specific needs for new amenities and the potential relocation of existing park amenities. All parks will be considered for improvements to make park features more accessible for individuals with physical limitations, improve wildlife habitat, and incorporate recommendations from the 2020 Parks Division Madison Bicycle Adventure Trail Network (Mad-BAT) plan. For more information, refer to individual Park Development Plans.

- Brigham Park
- Carpenter Ridgeway Park
- Churchill Heights Park
- Glacier Hill Park
- Mayfair Park
- <u>Sandburg Park</u> (Note: Sandburg Park is next to but does not include Sandburg Woods Conservation Park spaces)
- Westchester Gardens Park

Future Park Spaces

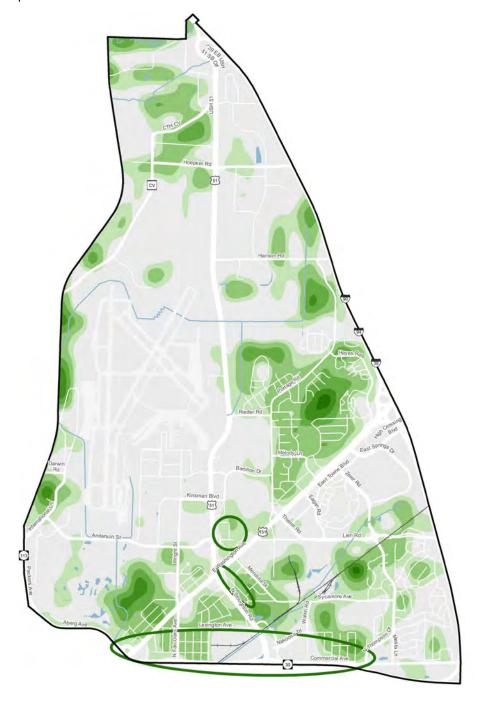
Several new parks are planned in the Northeast Area, particularly around the East Towne Mall area. As these future park spaces are acquired, the park development or master planning processes should be community-based and engage a diverse group of community members to establish spaces that are welcoming and culturally relevant for all city residents. These new parks should incorporate spaces and features that accommodate special events and programing like concerts, festivals, and farmers markets. Refer to Culture and Character actions and partnerships for additional information.

Trees and Canopy Coverage

The Northeast Area's 11% tree canopy coverage is one of the lower coverage rates among the City's 12 plan areas, and about half the level of Madison as a whole. This lack of coverage is due to the airport, large commercial areas, areas within parks such as athletic fields and reclaimed landfills, and wetland areas where trees don't grow. Regardless, it is a priority to add trees to both public and private land. There are gaps in street tree coverage, mostly in the neighborhoods around Fair Oaks Avenue, but also on streets which abut Highway 30. Trees in those locations could establish a buffer between the highway and nearby residential areas. Redevelopment of large commercial properties could lead to increased tree plantings and total canopy coverage, since new development regulations include higher standards for the number of trees that must be provided on a site.



Adequate tree coverage along West Washington Avenue







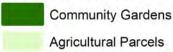
Example of a Rain Garden

Stormwater Management and Flood Reduction

Stormwater management and flooding saw increased attention from many community members after a 2018 rain event where 12 inches of rain fell over a 24-hour period in portions of Madison. While the worst impacts occurred on the west side, the intersection of Highway 51 and Lexington Avenue in the Northeast area was impassable for several hours. This area continues to flood on occasion. The Engineering Division's <u>Starkweather Creek Watershed Study</u> covers nearly all of the Northeast Area and will recommend improvements to minimize flooding from more frequent, large rain events.



Northeast Area Plan Urban Agriculture



Water Quality

Part of growing sustainably is addressing contamination from past developments and practices. Redevelopment projects often include environmental cleanups that address contamination from past developments like gas stations, laundromats, or industrial uses. Redevelopment is often the best way to clean or remediate sites, since it creates financial motivation to do so. The highest profile contamination issue in the Northeast Area is Per- and polyfluoroalkyl substances (commonly referred to as PFAS) related to past use of firefighting foam at Dane County Regional Airport. That contamination led to the shutdown of Madison Water Utility's Well 15, which along with Well 29, provides water for the Northeast Area. Well 15 remains offline while a PFAS treatment system is under development. Improvements at the well facility, including installation of PFAS treatment equipment, began in 2024. Madison Water Utility will provide regular updates on construction progress, water testing, and operational status on the Well 15 project website.



Madison Water Utility's Well 15

Urban Agriculture

The Northeast Area has several different types of urban agriculture. Community garden plots are the smallest scale and are located in Reindahl Park and at the Community Development Authority's Truax Housing property. While on public land, the plots are managed by Rooted (Community Groundworks). The Parks Division often receives requests for additional community garden spaces, and these are forwarded to Rooted. Once Rooted determines there is enough demand for new garden spaces, it works with the Parks Division to identify appropriate locations. Once approved, these locations are then added to Rooted's lease with the City. Approximately eight acres of private market gardens (small scale commercial agriculture) are located west of County Highway CV at Hoepker Road. There are several large properties in the Northeast Area planted with row crops. Some are planted as an interim use with eventual development planned, while others are unlikely to develop. Dane County Regional Airport owns approximately 150 acres of land within unbuildable Runway Protection Zones that are currently rented for agriculture. While these are currently planted with row crops, it's possible they could transition to other types of agriculture, such as Community Supported Agriculture (CSA) farms or similar.



Parks and Open Space

- 1. Parkland Acquisition and Expansion: Improve access to, visibility of, and usability of City park spaces through new park development and property acquisition as shown on the Parks and Open Space Map.
- 2. Park Specific Improvements: The following recommendations address issues discussed during the Northeast Area Plan process and were not previously addressed in Parks master planning processes:
 - a. Sandburg Park
 - i. Add a path connection to Sandburg Woods from vacant property on Forest Run Road when it develops.
 - ii. Better define edges between park land and private properties on the north side of Sandburg Park.



Community garden at Leopold Park



Row crops planted in the Runway Protection Zone





Sycamore Dog Park

iii. If apartment communities on Portage and Hayes Roads redevelop, establish a new street which abuts the park to improve visibility, access, and uses.

b. Sycamore Park

- i. Extend Nakoosa Trail to Jana Lane, as a phased project, to provide greater visibility, access, and use of the southern portion of the park. Extending Nakoosa Trail to the east of Pulley Street will require minor park configuration changes that should be addressed as part of a future master planning process.
- ii. In the interim, consider changes to the shared-use path intersection with Nakoosa Trail to address reports of unauthorized motor vehicle use on the path.
- 3. Improve access to publicly-owned natural areas to provide passive recreation opportunities:
 - Provide public access to the wetland area north and west of Portage Road when adjacent vacant property develops.
 - b. Establish public access to the wetland area along the rail corridor south of Lien Road as part of the planned Hiestand Path.
 - c. Seek public access along Starkweather Creek between Highway 30 and Commercial Avenue via easement or property acquisition.

Trees and Canopy Coverage

- 4. Proactively evaluate identified areas on the Urban Forestry Map for potential street tree planting sites, with a particular emphasis on the Highway 30/Commercial Avenue frontage.
- Evaluate City stormwater parcels for plantings and ecological restoration consistent with the Stormwater Utility Vegetation Management Plan once adopted.

Stormwater Management and Flood Reduction:

6. As part of the Starkweather Creek Watershed Study, develop stormwater management and watershed protection recommendations. Investigate solutions to areas with chronic flooding such as at Lexington Avenue and Highway 51 and in the "triangle" area between the East Washington Avenue Frontage Road and the Commercial Avenue Service Road.

Green and Resilient Partnerships

- A. Explore opportunities with Dane County to increase use of the County-owned greenspace south of Anderson Street and west of Wright Street since this area is park-deficient.
- B. Work with neighborhood groups, cultural organizations, school classes, and other stakeholders to improve and expand the walking trails that connect Carpenter-Ridgeway Park to Starkweather Creek.
- C. Pursue tree planting options for unneeded highway right-of-way as part of Wisconsin Department of Transportation studies.

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Effective Government

Comprehensive Plan Goals:

This Plan aims to advance the following goals:

- Madison will have efficient and reliable public utilities, facilities, and services that support all residents.
- Madison will collaborate with other governmental and nongovernmental entities to improve efficiency and achieve shared goals.

The Northeast Area's population increased by approximately 900 residents between 2010 and 2020. Much of the planning area, including areas in the Town of Burke, have been largely developed. Most of the area has the full complement of City services, such as police and fire, utilities, waste management, and snow removal. Town of Burke properties are served by a combination of Town of Burke, City of Madison, and Village of DeForest services. Madison provides fire and emergency medical services to the Town as well as water and sanitary sewer to parts of the Town.

The City will continue to provide a full range of services to neighborhoods (including former Town properties attaching to the City of Madison), while optimizing where community facilities are located to balance provision of services with the long-term operating costs and the growing and changing population of the Northeast Area. Select areas may receive water and sewer service from the Village of Deforest, as specified in the Town of Burke, Village of DeForest, City of Sun Prairie and City of Madison Cooperative Plan. However, new connections in the DeForest service area are not guaranteed after properties attach to the City of Madison. See the cooperative plan for more information.

The Reindahl Imagination Center and homeless shelter at Bartillon Drive are the latest City facilities to be planned in the Northeast Area. Additional adjustments to services will likely be needed in the future as the Northeast Area and the city continue to grow.

A further important focus on the provision of services is to provide individuals and groups, especially underrepresented populations, with the tools to effectively gain access to Madison's services and resources. The Northeast Area is well-situated to provide satellite services to residents that would otherwise need to travel to City offices located downtown. Situated between downtown and the edge of the city, the Northeast area could host City services that would otherwise require a visit downtown. Bus Rapid Transit (BRT) service through the area could provide improved access to such a satellite service facility.

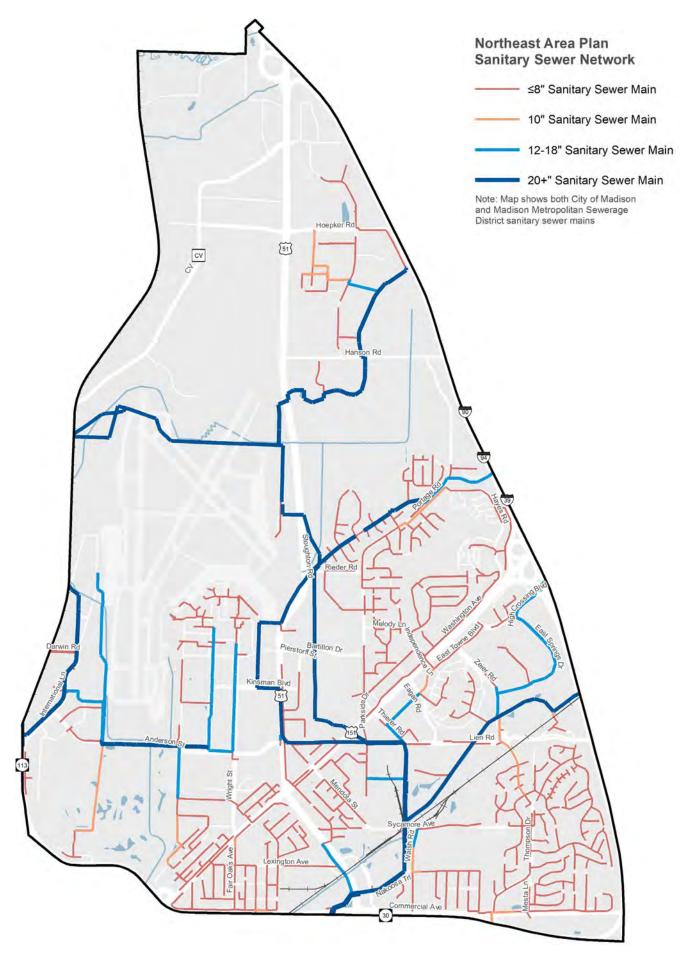
Redevelopment of lower density uses into higher density uses results in changing demands for City services and infrastructure. Adding residential uses into previously low-density or commercial areas requires a different set of services such as parks and pedestrian and bicycle facilities. Additionally, residential uses require a comparatively higher amount of water, which could result in the need to upsize water mains and sanitary sewer mains as redevelopment transitions

What We Heard

- Residents perceive public investments have lagged in the Northeast Area compared to rest of the city.
- 2. Have a smooth transition of municipal services when the final attachment of the Town of Burke occurs in 2036.
- 3. Improve maintenance of City infrastructure within the planning area.



City of Madison recycling service



66

low-density commercial and office space to mixed-use development or multifamily residential development (see Map: Sanitary Sewer Network). Upsizing of sanitary sewer mains is most likely needed for properties served by eight-inch diameter mains, which are generally the City's smallest diameter mains.

Maximizing the efficiency of facilities and services can best be achieved through careful coordination among City agencies and potentially with other municipalities, specifically the City of Sun Prairie, Village of DeForest, and Town of Burke.

[51] cv Town of Burke Dane County Regional Airport Bartillon De Reindahl [51]

Town of Burke, Village of DeForest, City of Sun **Prairie, City of Madison Cooperative Plan**

- Town of Burke dissolves 2036, properties attach to DeForest, Sun Prairie, and Madison. See Intergovernmental Boundary Agreements Map for lands attaching to Madison.
- Town properties can attach to Madison if requested by the property owner and approved by the City.
- Specific Town properties are protected from early attachment until 2036 to maintain tax base for the Town unless approved by the Town. See Intergovernmental Agreement Map.
- Madison will provide the full range of City services to Town properties attaching to the City except for properties located near Highway 51 and Interstate 39/90/94. DeForest may provide water and sanitary sewer to these properties per the agreement.
- There is a restriction on the number of housing units that can be built each year within the portion of the DeForest Area School District that is attaching to Madison.

Northeast Area Plan Madison-Burke-Sun Prairie-DeForest Cooperative Plan

Town of Burke Protected Areas

Town of Burke Parcels Attaching to Madison (Final Attachment 2036)

Actions

- 1. Explore satellite facilities for government services to accommodate customer schedules and needs, potentially at the Reindahl Imagination Center.
- 2. Conduct a review of community service and facility needs after the Reindahl Imagination Center is built and new housing is present in the planning area. If gaps are discovered, the Community Development Division should explore a service provider facility (for example, The Village on Park) where providers can rent space and offer: employment services, business development and computer skills training, preventive health services, food and essentials pantry, and fitness and recreation.
- 3. Develop a comprehensive infrastructure plan in advance of Town of Burke properties attaching in 2036. The plan should identify infrastructure needed to support continued industrial development in area north of the airport and capitalize on the City's successful partnerships with the Interstate Commerce Park area and the Center for Industry and Commerce Owners Association.

Partnerships

- A. Work with the applicable school districts to address pedestrian, bicycle, and traffic safety issues around schools when street repaving, reconstruction, or other changes to the right-of-way are being considered. Improvements may include things like signage, increasing crosswalk visibility, modifying traffic patterns, increasing nearby sidewalk connectivity, and implementing safe all-ages-and-abilities bicycle connections.
- B. Pursue changes to the residential building permit restriction in the Burke Cooperative Plan.
- C. Pursue an agreement with Village of DeForest to use existing DeForest sanitary and water infrastructure to serve properties northeast of Hoepker Road and Highway 51.
- D. Work with Federal Government, Air National Guard, Federal Aviation Administration (FAA), and Dane County Regional Airport to secure soundproofing funding for residential buildings and other noise-sensitive uses affected by higher levels of airport noise.

68

Health and Safety

Comprehensive Plan Goals:

This Plan intends to advance the following goals:

- Madison will be a place where all residents have equitable health outcomes.
- Madison will be a place where residents and visitors are safe at home and feel welcome in the community.

The Comprehensive Plan promotes access to comprehensive mental and physical health services, healthy and secure living environments, and the implementation of public health strategies to prevent violence.

The recommendations in this chapter aim to enhance safety and mitigate noise and air quality issues in the Northeast Area, ultimately improving residents' quality of life and fostering a more vibrant and sustainable community. To implement these recommendations, it is essential that the City, specifically Public Health Madison & Dane County and the Madison Police Department, continue to engage with the local community and other stakeholders to ensure a collaborative and effective approach.

Health

According to the national City Health Dashboard, health conditions in the Northeast area in 2020 were comparatively poorer than the city as a whole. Individuals experience obesity at a higher rate than the citywide average. Northeast Area residents had lower levels of physical activity compared to the city as a whole. Levels of particulate matter (air pollution) in the northern portion of the planning area are some of the highest in the city.

Safety

Safety concerns were frequently expressed by Northeast Area residents. According to the Neighborhood Indicators Project 2022 Edition, the Northeast Area had the highest number of verified criminal offenses in Madison. Madison Police Department records indicate a spike in calls for service and verified criminal offenses within the Northeast Area in 2022, with most calls being for thefts, disturbances, drug incidents, investigations, and burglaries. The increase in calls for service and criminal offenses underscore the need for comprehensive community engagement and targeted strategies to address these issues.

Actions

- 1. Establish one or two Neighborhood Resource Teams (NRTs) to better connect City and community resources to areas experiencing challenges. NRTs also increase opportunities for stakeholders to provide feedback with City and community officials to proactively address safety concerns and other neighborhood issues. See "Potential NRT Area" map.
- 2. Incorporate SafeGrowth strategies that empower neighborhood residents involving them with city government agencies in developing solutions to safety concerns that have been identified.

What We Heard

- 1. Address drug-dealing.
- 2. Add lighting on Reindahl Park bike path, along Portage Road, and other locations to deter crime and increase the sense of safety.
- 3. Address safety concerns on Hayes Road near existing hotels where suspicious behaviors have been reported (frequent short visits, abnormal trash, or debris and unusual odors).
- 4. Ensure adequate services are provided for unhoused and lower-income residents to avoid unintended neighborhood impacts.
- 5. Mitigate issues related to highway and airport noise.
- 6. Improve air quality.





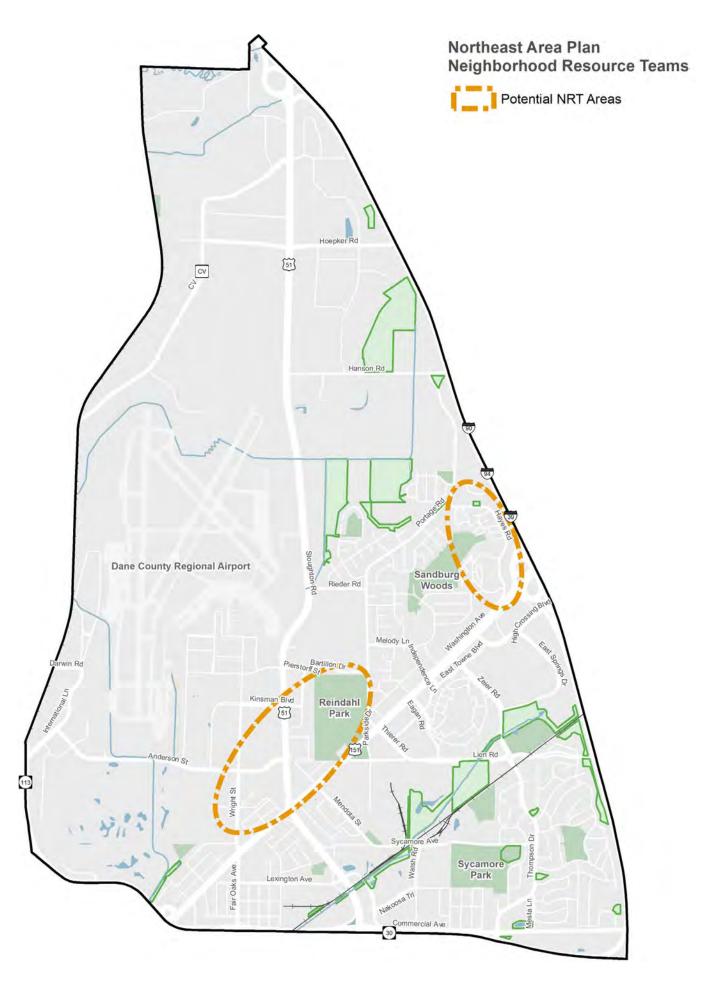
One goal of the City's Vision Zero program is to ensure a safe commute for school children.

- Conduct a comprehensive lighting audit in the East Washington Avenue, Mendota Street, Sycamore bike path, and Sandburg Woods bike path areas. Install additional lighting where needed to improve safety and deter theft and drug related offenses.
- 4. Selectively remove trees and brush to increase visibility along businesses abutting Starkweather Creek and Sycamore Park.
- 5. Require sound mitigation (sound insulating materials and construction techniques) on new developments receiving City funding that are located within the Dane County Regional Airport's 65 db DNL noise contour.
- 6. Encourage new buildings within 200 feet of Interstate 39/90/94 and Highway 30 to use materials and insulation that keep interior noise levels below 52 decibels as per state code TRANS 405. Design sites to shield outdoor spaces from noise above 67 decibels. Developments near the highway should follow setback and noise regulations outlined in Wisconsin statutes (TRANS 233, TRANS 405), and MGO Section 16.23.
- 7. Increase pedestrian safety for children traveling to Sandburg Elementary School, with a focus on East Washington Avenue..
- 8. Conduct Vision Zero Speed Analyses while actively enforcing traffic regulations along school routes. Particular emphasis should be placed on streets such as East Washington Avenue, East Towne Boulevard, Packers Avenue, and other residential areas seeing frequent pedestrian activity and experiencing elevated risks of serious traffic-related injuries and fatalities.

Partnerships

- A. Work with WisDOT to explore and implement noise mitigation projects along Highway 30, Highway 51, and Interstate 39/90/94 frontages.
- B. Examine traffic management strategies along Lexington Avenue to slow vehicle speeds in close proximity to Hawthorne Elementary School. Prioritize key intersections such as Powers-East Washington-Lexington Avenues, North Fair Oaks and Lexington Avenues, and Mayfair and Lexington Avenues.
- C. Broaden the availability of cooling options in the Northeast Area for residents without access to air conditioning. Establish partnerships with the East Madison Community Center, local businesses, places of worship, and other community entities to designate them as officially recognized cooling sites.
- D. Collaborate with resident leaders to continue to assess environmental factors that may contribute to or protect against violence, such as dimly lit areas, abandoned buildings, and vacant parking lots.

72 Draft - City of Madison



Community Action Strategy

Introduction

The City receives federal funding to support community development initiatives primarily benefiting low- to moderate-income residents. A portion of this funding is used to connect with residents in these areas to plan for and implement projects and activities that enhance the quality of life in their neighborhoods. These areas are defined by the U.S. Department of Housing and Urban Development and are eligible to receive federal funding to implement recommendations through the Community Development Block Grant (CDBG) program.

The Northeast Area Plan includes three eligible areas: Sandburg, Burke Heights, and Hawthorne-Truax. The Hawthorne-Truax Community Action Strategy (CAS) recommendations were developed through the 2023 Hawthorne-Truax Neighborhood Plan and are incorporated into this Plan.

While Area Plans focus on guiding physical changes over a large area, CAS recommendations focus on specific neighborhoods and include additional topics such as:

- Capacity Building: Enhancing the ability of individuals, organizations, and communities to effectively advocate for their needs and implement relevant projects and activities, and also providing information to community members about public services, programs, and projects that could affect them.
- Community and Stakeholder Relationships: Building relationships between the City and community that are open, understanding, and cooperative helps ensure a more inclusive and responsive approach to plan implementation. Such relationships are built over time and can be powerful catalysts for carrying out community-based recommendations for things like business improvement districts, neighborhood-oriented destinations, and community gatherings.
- Small Scale Physical Improvements: Modest enhancements to specific sites such as street infrastructure, green spaces, public facilities, art installations, traffic calming, community clean-up initiatives, or other community programming. These improvements aim to enhance local aesthetics, functionality, and community engagement.

CAS Project Funds

The City's Community Development Block Grant (CDBG) Committee budgeted a total of \$400,000 in federal CDBG funds to facilitate the implementation of eligible projects in the Sandburg and Burke Heights neighborhoods. The CDBG Committee previously budgeted \$250,000 for the Hawthorne-Truax area.



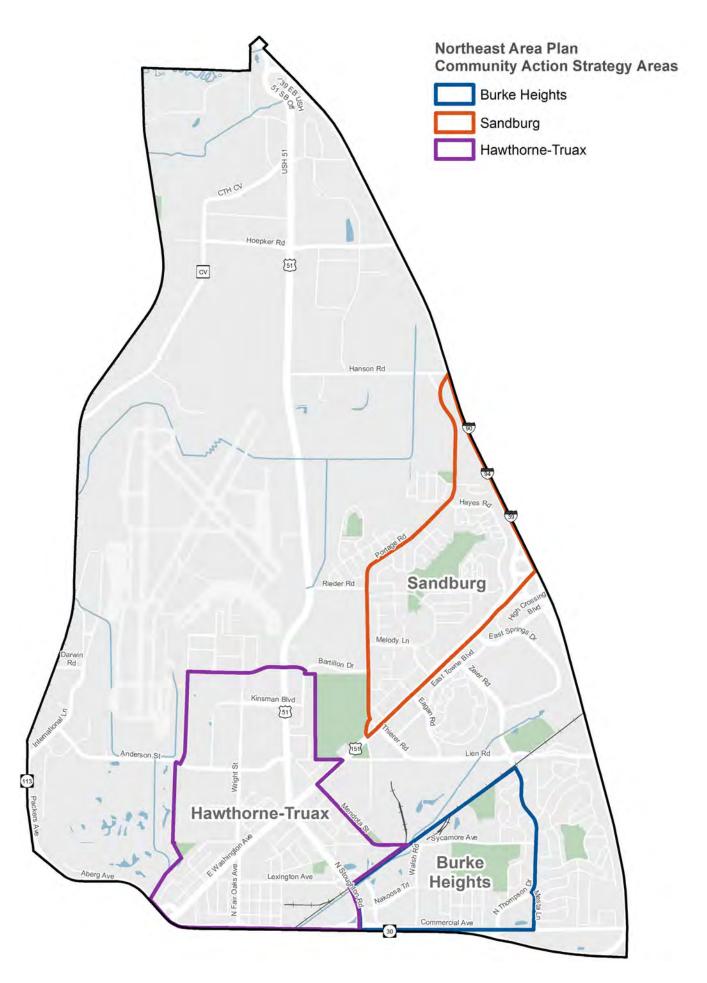
Capacity building example - City resources workshop at Senior Center



Community relationships example - Clean-up neighborhood activity with the youth



Small physical improvements example - sidewalk repair





Sandburg CAS Recommendations: Capacity Building

- 1. Create a cultural inclusion plan for developers that would provide guidance on promoting a diverse mix of tenants.
- 2. Increase promotion of City residential assistance programs, including rental and utility payment support, the property tax assistance program for seniors, and low-cost mortgage and rehabilitation loans.
- 3. Strengthen Public Health's "Reduce Drug Harm and Violence" initiative to expand service to the area for mental and physical health, domestic violence, and violence prevention and intervention.

Community and Stakeholder Relationships

- 4. Develop marketing strategies to draw activity and employment to the business areas along East Washington Avenue.
- Continue building relationships with economic development partners, especially those with ties to Black, Indigenous, and people of color (BIPOC) communities, including the Madison Black Chamber of Commerce, Latino Chamber of Commerce of Dane County, and the Hmong Wisconsin Chamber of Commerce.
- 6. Identify and grow opportunities for entrepreneurs to locate along East Washington Avenue and the Greater East Towne Mall Area.
- 7. Work with local commercial brokers and property owners to connect small businesses to available space as they "graduate" from an incubator.
- 8. Conduct an equity-based market study of the area to further explore strategies for strengthening the economy, with an emphasis on ensuring that people who have been traditionally excluded are included in redevelopment and employment opportunities.
- Establish relationships between the Sandburg Neighborhood Association and Dane County's Joining Forces for Families and Public Health's Violence Prevention Initiatives to help residents connect to services.

Small Scale Physical Improvements

- 10. Integrate pedestrian and bicycle infrastructure like benches, pedestrian-oriented lighting, and bike racks into terrace areas, especially in and around East Washington Avenue.
- 11. Encourage expansion of bicycle share (B-Cycle) facilities to the area.
- 12. Support temporary use of vacant spaces to assist emerging artists while adding to the vibrancy of the area. This may include artist-in-residency spaces, pop-up galleries, and public art.



B-Cycle station

CAS Project Expenditures

The following table includes a list of potential projects prioritized by residents with locations and cost estimates. The City will work on implementing the projects in the order shown, and in some cases may encounter obstacles that require moving to the next project in the list. The intention is to fully implement these projects over time using either CDBG funding alone, CDBG funding in combination with the City budget and/or other funds, or entirely through other means.

Priority	Project	Location	Cost Estimate
1	Lighting and safety	 Lighting Bike and walking paths (Sandburg Woods) Safety: traffic calming and safer crossings along: Portage Road between East Washington Avenue and Reider Road Independence Lane Anniversary Lane Melody Lane and Bartillion Drive Dwight Drive (has 6 speed humps but needs sidewalk) Mayfair Park North Thompson Drive bump outs, continental crosswalks, islands 	 Rectangular rapid flashing beacons (RRFB) at Dwight Drive \$10,000 Dwight Drive curb bump out at Reindahl path \$25,000 Melody Lane and/or Independence Lane speed humps \$10,000 each Pedestrian island at Portage Road and Melody Lane \$10,000 Maximum speed signs \$2,000 Mayfair Park RRFB and curb ramp \$20,000
2	Public gathering spaces	Sandburg ParkBenches in Sandburg Woods	 Art in alcoves along path \$30,000+ (depending on length) Benches \$4,000 each
3	Shelters and pavilions in parks	Sandburg Park	Depending on size
4	Permanent games in parks	Sandburg ParkWestchester Gardens Park	 Sandburg – Additional play equipment \$15,000 Sandburg – Nature play area \$50,000 Westchester – Community patio with shelter \$150,000
5	Community garden	 Sandburg – community coordination with Rooted required 	Water supply and shed

Burke Heights





Parks Alive program

Burke Heights CAS Recommendations: Capacity Building

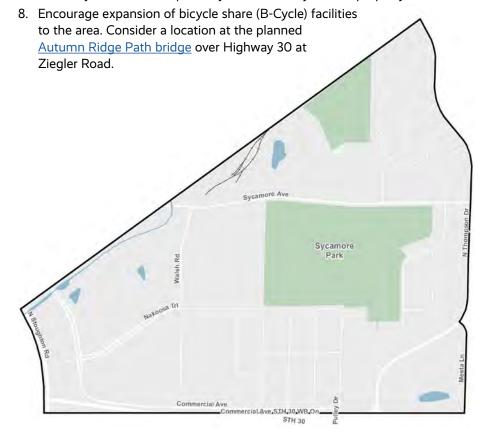
- 1. Include Sycamore Park as a primary location for community programming to help build relationships in the community (e.i., Parks Alive).
- 2. Increase promotion of City residential assistance programs, including rental and utility payment support, the property tax assistance program for seniors, and low-cost mortgage and rehabilitation loans.
- 3. Create a cultural inclusion plan for developers that would provide guidance on promoting a diverse mix of tenants.

Community and Stakeholder Relationships

- 4. Establish relationships between the Burke Heights Neighborhood Association and Dane County's Joining Forces for Families and Public Health's Violence Preventions Initiatives to help residents connect to services.
- 5. Connect the Burke Heights Neighborhood Association and residents with the Parks Division, Madison Public Library, and Mobile Madison to help residents host neighborhood events at Sycamore Park.

Small Scale Physical Improvements

- 6. Coordinate community clean-up events along primary neighborhood streets like Commercial Avenue and Sycamore Avenue.
- 7. Support the establishment of neighborhood planting areas to be maintained by volunteers on privately-owned or City-owned property.



CAS Project Expenditures

The following table includes a list of potential projects prioritized by residents with locations and cost estimates. The City will work on implementing the projects in the order shown, and in some cases may encounter obstacles that require moving to the next project in the list. The intention is to fully implement these projects over time using either CDBG funding alone, CDBG funding in combination with the City budget and/or other funds, or entirely through other means.

Priority	Project	Location	Cost Estimate
1	Lighting and safety	 Sycamore Park Neighborhoods surrounding Sycamore Park Speed limit signs along Sycamore Avenue and Mendota Street 	
2	Community garden	 Sycamore Park – Community coordination with Rooted required Fruit trees 	Water supply and shed
3	3 Bicycle share (B-cycle) • Along East Washington Avenue station		\$1,800 per dockRepair stations \$3,000
4	Murals/art installations	Interactive art	• \$30 per sq ft
5	Asphalt art along paths	 Sycamore Park dog park path 	• \$25 per sq ft

Hawthorne-Truax CAS Recommendations: Capacity Building

- Support the establishment, capacity-building, and diversification of groups working to strengthen neighborhood cohesion and connections by sharing tools, tips, and other resources that help these groups become more inclusive and welcoming to the area's diverse residents.
- 2. Share guidance, information and resources with local groups and neighborhood associations to build capacity, support emerging groups, and empower diverse voices. Provide technical support and resources to assist residents in re-activating the Hawthorne Neighborhood Association.
- 3. Increase promotion of City residential assistance programs including rental and utility payment support, the property tax assistance program for seniors, and low-cost mortgage and rehabilitation loans.
- 4. Strengthen Public Health's "Reduce Drug Harm and Violence" initiative to expand service to the plan area for mental and physical health, domestic violence, and violence prevention and intervention.



Community and Stakeholder Relationships

- Increase awareness and access to existing community programs and services and explore expanding them in locations such as the community rooms of new housing developments, Hawthorne Elementary School, the Reindahl Imagination Center, and through co-location of community-based organizations.
- 6. Support Joining Forces for Families and East Madison Community Center in their efforts to address Truax resident needs like affordable childcare, healthcare, and nutritious food.
- 7. Work together with the community and utilize City support and resources such as Parks Alive, Carts in Parks, and Madison Arts Commission funding and festival planning assistance to establish neighborhood events and activities in parks and open space, Truax apartment greenspace, and other gathering places. Feature food, music, crafts, artwork, and cultural performances that celebrate the diverse backgrounds of residents.
- 8. Coordinate with area financial institutions to explore creating financial literacy programming for residents including adults, youth, and school age kids.
- Partner with East Madison Community Center, Project Babies, RISE Wisconsin, the Lullaby program through the Overture Center, and other area organizations to provide support for pregnant people, babies, and young children.
- 10. Work with community partners to support and expand Hawthorne Community School. Consider establishing Open Schoolhouse evenings that offer community programs and resources.
- 11. Expand the Hawthorne Elementary Walking School Bus to serve students from the Carpenter-Ridgeway and Hawthorne neighborhoods.
- 12. Create additional teen activities and programing, similar to the Goodman Center Teen Loft, at the East Madison Community Center or in other community spaces.
- 13. Explore hosting community service and health fairs at Access Community Health Center, East Madison Community Center, Hawthorne Elementary School, and other neighborhood locations.
- 14. Support organizations such as New Bridge Madison and Goodman Community Center to bring programming and services for older adults to the area, including community events such as neighborhood concerts.

Small Scale Physical Improvements Recommendations

- 15. Explore possibilities for adding murals to blank building facades, such as the telecommunications building at Rowland Avenue and East Washington Avenue.
- 16. Work with Dane County to identify community-oriented interim uses for the greenspace in the Wright Street triangle.
- 17. Add one or more canoe and kayak launches along the west branch of the Starkweather Creek.
- 18. Create urban arts districts along Lexington, and Mayfair Avenues to support the establishment of artist workshops, music studios, and makerspaces.



Wright Street Green Open Space



Downtown LA Arts District

CAS Project Expenditures

The following table includes a list of community-driven projects as prioritized by residents with potential locations and cost estimates. City staff will work on implementing the projects in the order shown, and in some cases may encounter stumbling blocks that require moving to the next project in the list. The intention is to fully implement all of these projects over time using either CDBG funding alone, CDBG funding in combination with the City budget and/or other funds, or entirely through other means.

Priority	Project	Location	Cost Estimate
1	Youth recreation facilities	 Highest priority need is near Truax apartments, though location must be identified 	Basketball court: \$100,000Bike/skate facility: \$400,000
2	Lighting and other enhancements in tunnels and underpasses	 Tunnel under East Washington Avenue at Wright Street/Fair Oaks Avenue and the Marsh View Path under Highway 30 	East Washington tunnel: \$15,000Marsh View Path: \$100,000
3	Hawthorne Elementary School playground enhancements	Walking path around playgroundAccessible playground equipmentMural	 Walking path: \$50,000 Accessible playground equipment: \$125,000 Mural: \$100,000
4	Community gathering spaces	 Potentially include the Wright Street triangle and parklets on Fair Oak Avenue 	Wright Street triangle: \$25,000Fair Oaks parklets: \$25,000
5	Expand access to childcare services	 Renovation of childcare space at East Madison Community Center 	 TBD, possibly in coordination with future CDA Truax redevelopment phase

82

Appendix

Historic Resource Survey	84
Action Matrix	88
Glossary	94

Northeast Area Plan - Draft

Historic Resource Survey

Methodology

To identify potential historic resources in the Northeast Area, staff completed a visual survey of properties in May 2023 and noted properties of potential architectural significance. Staff then completed additional research using files from the Wisconsin Historical Society, City of Madison preservation and building permit files, Sanborn maps, archival phone directories, and US Census records.

Findings

The area contains no City-designated landmarks and no properties listed in the National Register of Historic Places. Of the buildings noted for architectural or historic interest, that information is included as a note for that property in case property owners are interested in pursuing a future historic designation. Many of these are buildings that were places of worship and schools, which often serve as the location for significant architecture in a community, in addition to being the location of community gatherings and activities.

Staff did not complete an assessment of the buildings at Truax Field Air National Guard Base as the potential for historic designation or preservation is best handled as part of a federal process through the National Historic Preservation Act.

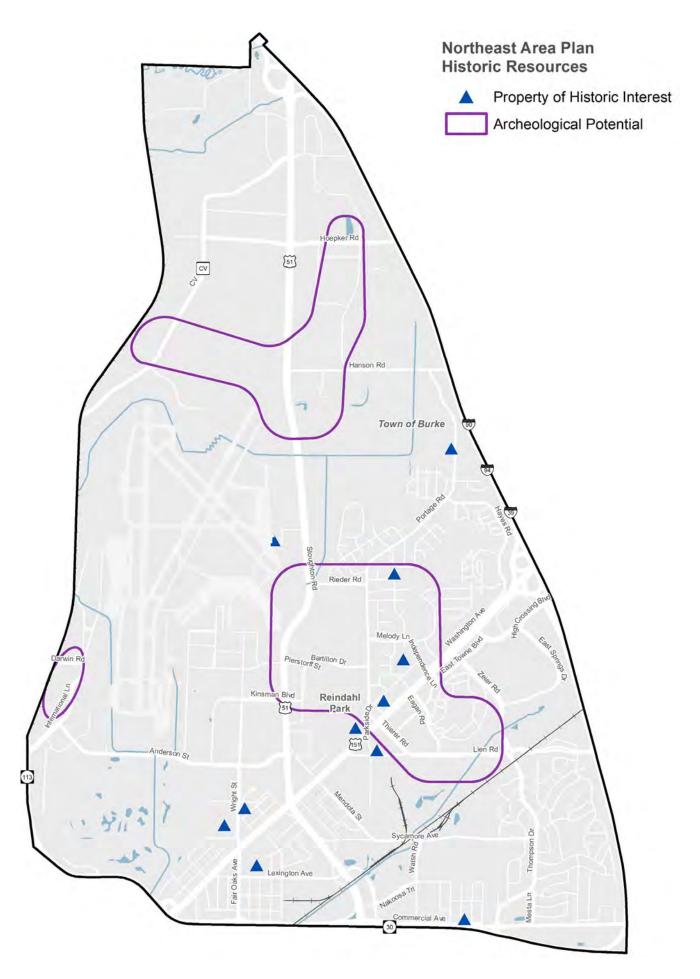
As Madison is the site of long-term human occupation, there are also several archaeological sites within the Northeast Area, several of which are known human burial sites. Most of these are associated with the Ho-Chunk people and any work that impacts these properties should be done in consultation with the Tribe during the earliest stages of the planning process. Any of the locations that are known human burial sites will need to secure a Request to Disturb from the Wisconsin Historical Society before engaging in ground-disturbing activity.

Recommendations

There are several individual properties that could benefit from having their story officially documented through the National Register process. As it is an honorary process, it does not place additional regulations on private property owners but does provide additional considerations for government undertakings that may impact their properties and opens the possibility for preservation tax credits to help property owners adapt their properties for new and ongoing uses. The preservation tax credit program is very popular, as there is both a State and Federal program.

For properties with known human burial sites on them, there is the <u>Human Burial Site Tax Exemption</u> process that makes the undevelopable portion of a property exempt from taxes. For that work, property owners should contact the Wisconsin Office of the State Archaeologist.

Historic preservation does not prevent development or changes of use, but it does shape how redevelopment happens. It is not a tool for exclusionary zoning. Properties with possible historic value are opportunities for adaptive reuse of places of architectural or historic significance so that these places could be sensitively integrated into the ongoing growth of Madison. Sharing Madison's unique culture and character through historic preservation, representing the history of Madison's diversity, and building reuse as an environmental sustainability strategy are priorities in of the Madison Preservation Plan.



Properties of Historic Interest

Name and Address	Area of Sig- nificance	Notes
Motel Royal 705 Redland Dr	History & Architecture	Constructed in 1950, this motorcourt style motel has changed uses to apartments, but still retains a great deal of historic integrity. This property was surveyed by WisDOT in 2014 and 2018
Islamic Center of East Madison (historic: Parkside Presbyterian)	Architecture	Constructed in 1958, this MidCentury religious building was designed by noted architect W.V. Kaeser
4002 Lien Rd		
Amund Reindahl Barn 3900 E Washington Ave	History & Architecture	The barn is all that remains of Amund Reindahl's farm, which he purchased in 1901. Reindahl bought the farm in 1901, and much of the land is now a City park.
H.P. Hall House	History 0	• •
4117 Dwight Dr	History & Architecture	The Greek Revival house was constructed in the 1850s as the home of H.P. Hall, who had a large farm in the area. The building has several additions off the rear of the building and now serves as a nursing home.
H.P. Hall Bird Effigy	Archaeology	Once the site of a large bird effigy mound on the farm of H.P. Hall, years of farming and subsequent development have largely destroyed remains of the mound, but there is still the potential for human burials remaining. Any ground-disturbing activity requires archaeological monitoring and a Request to Disturb from the Office of the State Archaeologist.
O'Connell Memorial US Army Reserve Center 1439 Wright St	History & Architecture	The two buildings associated with the USARC were constructed in 1961 and feature a training and administration building towards the front of the property and a multi-bay vehicle maintenance shop at the rear of the property. The buildings have had few significant changes to them over time and are indicative of post-WWII military infrastructure in smaller cities like Madison.
Wisconsin National Guard Headquarters Armory 1420 Wright St	History & Architecture	This Late Modern Building was constructed in 1956 and was home to the Wisconsin National Guard Headquarters Armory until 1994. This building is indicative of post-WWII military infrastructure in smaller cities like Madison. The building has had very few significant changes over time and is able to convey its historic associations.
Madison Municipal Airport Hangar	History & Architecture	Constructed by the Works Progress Administration in the Rustic style, this airport hangar was once paired with a municipal airport terminal building.
3400 N Stoughton Rd		

86 Draft - City of Madison 746

Name and Address	Area of Sig- nificance	Notes
Lucinda & Samuel Messer- schmidt House	History & Architecture	Constructed in 1930, this English Revival house is unusual for the Madison area in that it is clad in fieldstone. The house was designed by Samuel Massacrashmidt who lived at this bours with his
5487 Portage Rd		signed by Samuel Messerschmidt, who lived at this house with his wife Lucinda. In 2000, the building was relocated to this site from where it had been a part of a farmstead at 5723 STH 51.
Hoepker Scatter	Archaeology	Previous investigations have yielded information about early Anglo settlement of this site
Truax Air Park Mound	Archaeology	The mound site is protected with a conservation easement held by the Madison Trust for Historic Preservation. This is a Native Ameri- can mound and burial site.
Sandburg Elementary School 4114 Donald Dr	Architecture	The MidCentury Modern school building was designed by Weiler, Strang, McMullin and Associates and constructed in 1968
East Side Evangelical Lutheran Church	Architecture	The MidCentury Modern church was designed by J. Flad & Associates and constructed by Vogel Brothers in 1963
2310 Independence Ln		
Hawthorne School	Architecture	The MidCentury Modern school was designed by Law, Law, Potter & Nystrom and constructed by Findorf Construction in 1958
3344 Concord Ave		

Action Matrix

	Land Use Actions	Agencies
1	Update the Comprehensive Plan Generalized Future Land Use Map to be consistent with the Future Land Use map in this Plan.	Planning
2	Implement maximum building height recommendations shown on the Building Height Map with a mechanism for allowing bonus stories for developments that achieve specific affordability or sustainability parameters, similar to downtown height bonuses in City ordinances (Madison General Ordinances 28.071(2)(a)2).	Planning, Zoning
3	Require ground floor commercial space or other active uses in the locations shown on the Commercial Core Areas Map.	Planning
4	Initiate a follow-up process to consider zoning changes shown on the Proactive Rezoning Map. Ensure all impacted property owners are aware of potential zoning changes.	Planning, Zoning
5	Explore transitioning Planned Developments approved prior to 2013 to a matching conventional zoning district to improve transparency, usability, and consistency of regulations while reducing administrative burdens and inquiry response time.	Planning, Zoning
	Transportation Actions	Agencies
1	Officially Map future public streets as shown on the Street Network Map and reserve sufficient public right-of-way to implement the planned street network.	Planning, Engineering
2	Consider short and long-term improvements to Fair Oaks Avenue, Portage Road, Thompson Drive and other wide neighborhood streets that reduce unneeded street width to slow vehicular traffic, allow for tree plantings, improve bike, and pedestrian experience and reduce impervious surface. Short-term improvements could include bump-outs to shorten crossing distances and to allow for tree plantings, while long-term reconstructions could reflect Complete Green Street typologies.	Engineering, Traffic Engineering
3	Remove the street segments as shown on the Street Network Map and transition underutilized or unutilized roads or right-of-way into developed areas that have buildings that are close to the street and easily accessible to pedestrians.	Engineering, Traffic Engineering
4	Add sidewalks and other connections shown on the Pedestrian Network Map to improve connectivity and safety. Seek grants or other funding sources to add Priority Connections on high traffic corridors, near schools, or other primary pedestrian routes.	Engineering, Traffic Engineering
5	Add pedestrian and bicycle improvements at the intersections shown on the Pedestrian Network map to improve safety. Highest priority crossings are those on Highway 51 and East Washington Avenue.	Engineering, Traffic Engineering
6	Implement the bicycle network connections shown on the Bicycle Network Map to improve connectivity and safety.	Engineering, Traffic Engineering
7	Update the Complete Green Streets Guide's Street Type Map to incorporate changes to the street types to ensure that streets are designed according to the existing and planned surrounding context.	Transportation, Traffic Engineering
	Neighborhoods and Housing Actions	Agencies
1	Incentivize affordable housing, particularly on the East Washington Avenue and BRT corridors, through the Affordable Housing Fund, Land Banking initiative, and Tax Increment Financing.	Community Development, Economic Development
2	For developments receiving City funding, incentivize a wider mix of rental unit sizes, including those large enough to support larger and multigenerational families (3-4+ bedrooms).	Community Develop- ment, Planning

88 Draft - City of Madison 748

3	Consider designating a neighborhood in the Northeast Area as a targeted area for the Rental Rehab Loan Program to further incentivize improvements to aging rental housing properties in lower-income communities.	Community Development
4	Invest in food retail through funding and technical assistance to store operators through the Healthy Retail Access Program and SEED Grants, particularly neighborhoods north of East Washington Avenue which are identified as Food Access Improvement Areas (see Food Access Improvement Areas map).	Economic Development
	Economy and Opportunity Actions	Agencies
1	Establish a Greater East Towne Area Implementation Team to assist its redevelopment while also addressing vacant business spaces and parking lots.	Economic Develop- ment, Planning
2	Promote awareness and continue implementation of small business development programs funded through Tax Increment District (TID) #52 and other future area TIDs. These programs include the Facade Grant Program, Building Improvement Grant Program, Small Cap TIF Program, and Commercial Ownership Assistance Program.	Economic Development
3	Utilize Land Banking program to buy underutilized properties to facilitate redevelopment that includes employers.	Economic Development
4	Concentrate City resources on enhancing the business area along the International Lane corridor south the airport. Optimize available commercial spaces to their full potential to attract investment and foster economic growth.	Economic Development
5	Actively promote the Foreign Trade Zone adjacent to Dane County Regional Airport to attract businesses that offer stable employment opportunities. Efforts should focus on attracting more businesses to create employment opportunities for local residents.	Economic Development
	Culture and Character Actions	Agencies
1	Revise and update Urban Design District (UDD) 5 to require new development to be	Planning
'	pedestrian-oriented, including comfortable spaces and visually appealing elements along East Washington Avenue. See proposed UDD 5 revisions at the end of this chapter.	Flammig
2	pedestrian-oriented, including comfortable spaces and visually appealing elements along	Planning, Economic Development
	pedestrian-oriented, including comfortable spaces and visually appealing elements along East Washington Avenue. See proposed UDD 5 revisions at the end of this chapter. Support a variety of cultural and entertainment venues as the area redevelops. This should include spaces that vary in size and are well suited for a variety of visual and per-	Planning, Economic
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3	pedestrian-oriented, including comfortable spaces and visually appealing elements along East Washington Avenue. See proposed UDD 5 revisions at the end of this chapter. Support a variety of cultural and entertainment venues as the area redevelops. This should include spaces that vary in size and are well suited for a variety of visual and performing arts to make them easily accessible to area residents. Add art and placemaking elements through expanded use of the utility box wrap program. Provide residents with information and assistance about how to create and host festivals or community events within Reindahl and Sycamore Parks.	Planning, Economic Development Planning Planning, Parks
3 4 5	pedestrian-oriented, including comfortable spaces and visually appealing elements along East Washington Avenue. See proposed UDD 5 revisions at the end of this chapter. Support a variety of cultural and entertainment venues as the area redevelops. This should include spaces that vary in size and are well suited for a variety of visual and performing arts to make them easily accessible to area residents. Add art and placemaking elements through expanded use of the utility box wrap program. Provide residents with information and assistance about how to create and host festivals or community events within Reindahl and Sycamore Parks. Add benches along Starkweather Creek. If East Towne Mall redevelops, add a prominent art piece within the planned urban plaza and connect the new neighborhood with Sycamore Park through arts programs such as	Planning, Economic Development Planning Planning, Parks Parks, Engineering Planning, Economic
2 3 4 5 6	East Washington Avenue. See proposed UDD 5 revisions at the end of this chapter. Support a variety of cultural and entertainment venues as the area redevelops. This should include spaces that vary in size and are well suited for a variety of visual and performing arts to make them easily accessible to area residents. Add art and placemaking elements through expanded use of the utility box wrap program. Provide residents with information and assistance about how to create and host festivals or community events within Reindahl and Sycamore Parks. Add benches along Starkweather Creek. If East Towne Mall redevelops, add a prominent art piece within the planned urban plaza and connect the new neighborhood with Sycamore Park through arts programs such as Art in Public Places and Poetry in Sidewalks. Inform property owners who can officially document sites identified during historic resource surveys (see Historic Resources Map) with historical value through the National	Planning, Economic Development Planning Planning, Parks Parks, Engineering Planning, Economic Development
2 3 4 5 6	East Washington Avenue. See proposed UDD 5 revisions at the end of this chapter. Support a variety of cultural and entertainment venues as the area redevelops. This should include spaces that vary in size and are well suited for a variety of visual and performing arts to make them easily accessible to area residents. Add art and placemaking elements through expanded use of the utility box wrap program. Provide residents with information and assistance about how to create and host festivals or community events within Reindahl and Sycamore Parks. Add benches along Starkweather Creek. If East Towne Mall redevelops, add a prominent art piece within the planned urban plaza and connect the new neighborhood with Sycamore Park through arts programs such as Art in Public Places and Poetry in Sidewalks. Inform property owners who can officially document sites identified during historic resource surveys (see Historic Resources Map) with historical value through the National Register process.	Planning, Economic Development Planning Planning, Parks Parks, Engineering Planning, Economic Development Planning

Northeast Area Plan - Draft

a.	Sandburg Park	
i.	Add a path connection to Sandburg Woods from vacant property on Forest Run Road when it develops.	Parks, Engineering
ii.	Better define edges between park land and private properties on the north side of Sandburg Park.	Parks, Engineering
iii.	If apartment communities on Portage and Hayes Roads redevelop, establish a new street which abuts the park to improve visibility, access, and uses.	Traffic Engineering, Engineering
b.	Sycamore Park	
i.	Extend Nakoosa Trail to Jana Lane, as a phased project, to provide greater visibility, access, and use of the southern portion of the park. Extending Nakoosa Trail to the east of Pulley Street will require minor park configuration changes that should be addressed as part of a future master planning process.	Traffic Engineering, Parks, Engineering
ii.	In the interim, consider changes to the shared-use path intersection with Nakoosa Trail to address reports of unauthorized motor vehicle use on the path.	Traffic Engineering, Parks, Engineering
3	Improve access to publicly-owned natural areas to provide passive recreation opportunities:	Parks, Engineering
a.	Provide public access to the wetland area north and west of Portage Road when adjacent vacant property develops.	Parks, Engineering
b.	Establish public access to the wetland area along the rail corridor south of Lien Road as part of the planned Hiestand Path.	Parks, Engineering
C.	Seek public access along Starkweather Creek between Highway 30 and Commercial Avenue via easement or property acquisition.	Parks, Engineering
4	Proactively evaluate identified areas on the Urban Forestry Map for potential street tree planting sites, with a particular emphasis on the Highway 30/Commercial Avenue frontage.	Streets/Forestry, Engineering
5	Evaluate City stormwater parcels for plantings and ecological restoration consistent with the Stormwater Utility Vegetation Management Plan once adopted.	Engineering
6	As part of the Starkweather Creek Watershed Study, develop stormwater management and watershed protection recommendations. Investigate solutions to areas with chronic flooding such as at Lexington Avenue and Highway 51 and in the "triangle" area between the East Washington Avenue Frontage Road and the Commercial Avenue Service Road.	Engineering
	Effective Government Actions	Agencies
1	Explore satellite facilities for government services to accommodate customer schedules and needs, potentially at the Reindahl Imagination Center.	Engineering, Library, Parks
2	Conduct a review of community service and facility needs after the Reindahl Imagination Center is built and new housing is present in the planning area. If gaps are discovered, the Community Development Division should explore a service provider facility (for example, The Village on Park) where providers can rent space and offer: employment services, business development and computer skills training, preventive health services, food and essentials pantry, and fitness and recreation.	Community Development, Economic Development
3	Develop a comprehensive infrastructure plan in advance of Town of Burke properties attaching in 2036. The plan should identify infrastructure needed to support continued industrial development in area north of the airport and capitalize on the City's successful partnerships with the Interstate Commerce Park area and the Center for Industry and Commerce Owners Association.	Planning, Engineering

90 Draft - City of Madison 750

	Health and Safety Actions	Agencies
1	Establish one or two Neighborhood Resource Teams (NRTs) to better connect City and community resources to areas experiencing challenges. NRTs also increase opportunities for stakeholders to provide feedback with City and community officials to proactively address safety concerns and other neighborhood issues. See "Potential NRT Area" map.	Civil Rights
2	Incorporate SafeGrowth strategies that empower neighborhood residents involving them with city government agencies in developing solutions to safety concerns that have been identified.	Police, Public Health
3	Conduct a comprehensive lighting audit in the East Washington Avenue, Mendota Street, Sycamore bike path, and Sandburg Woods bike path areas. Install additional lighting where needed to improve safety and deter theft and drug related offenses.	Engineering, Police
4	Selectively remove trees and brush to increase visibility along businesses abutting Starkweather Creek and Sycamore Park.	Engineering
5	Require sound mitigation (sound insulating materials and construction techniques) on new developments receiving City funding that are located within the Dane County Regional Airport's 65 db DNL noise contour.	Planning, Community Development
6	Encourage new buildings within 200 feet of Interstate 39/90/94 and Highway 30 to use materials and insulation that keep interior noise levels below 52 decibels as per state code TRANS 405. Design sites to shield outdoor spaces from noise above 67 decibels. Developments near the highway should follow setback and noise regulations outlined in Wisconsin statutes (TRANS 233, TRANS 405), and MGO Section 16.23.	Planning, Zoning
7	Increase pedestrian safety for children traveling to Sandburg Elementary School, with a focus on East Washington Avenue	Traffic Engineering
8	Conduct Vision Zero Speed Analyses while actively enforcing traffic regulations along school routes. Particular emphasis should be placed on streets such as East Washington Avenue, East Towne Boulevard, Packers Avenue, and other residential areas seeing frequent pedestrian activity and experiencing elevated risks of serious traffic-related injuries and fatalities.	Traffic Engineering, Transportation
	Sandburg CAS Recommendations:	Agencies
1	Create a cultural inclusion plan for developers that would provide guidance on promoting a diverse mix of tenants.	Community Develop- ment, Planning
2	Increase promotion of City residential assistance programs, including rental and utility payment support, the property tax assistance program for seniors, and low-cost mortgage and rehabilitation loans.	Community Development
3	Strengthen Public Health's "Reduce Drug Harm and Violence" initiative to expand service to the area for mental and physical health, domestic violence, and violence prevention and intervention.	Public Health
4	Develop marketing strategies to draw activity and employment to the business areas along East Washington Avenue.	Economic Development
5	Continue building relationships with economic development partners, especially those with ties to Black, Indigenous, and people of color (BIPOC) communities, including the Madison Black Chamber of Commerce, Latino Chamber of Commerce of Dane County, and the Hmong Wisconsin Chamber of Commerce.	Economic Development
6	Identify and grow opportunities for entrepreneurs to locate along East Washington Avenue and the Greater East Towne Mall Area.	Economic Development
7	Work with local commercial brokers and property owners to connect small businesses to available space as they "graduate" from an incubator.	Economic Development

Northeast Area Plan - Draft

8	Conduct an equity-based market study of the area to further explore strategies for strengthening the economy, with an emphasis on ensuring that people who have been traditionally excluded are included in redevelopment and employment opportunities.	Economic Development
9	Establish relationships between the Sandburg Neighborhood Association and Dane County's Joining Forces for Families and Public Health's Violence Prevention Initiatives to help residents connect to services.	Joining Forces for Families, Public Health, NRT
10	Integrate pedestrian and bicycle infrastructure like benches, pedestrian-oriented lighting, and bike racks into terrace areas, especially in and around East Washington Avenue.	Engineering
11	Encourage expansion of bicycle share (B-Cycle) facilities to the area.	Traffic Engineering
12	Support temporary use of vacant spaces to assist emerging artists while adding to the vibrancy of the area. This may include artist-in-residency spaces, pop-up galleries, and public art.	Planning
	Burke Heights CAS Recommendations:	Agencies
1	Include Sycamore Park in the City's Parks Alive program.	Parks, NRT
2	Increase promotion of City residential assistance programs, including rental and utility payment support, the property tax assistance program for seniors, and low-cost mortgage and rehabilitation loans.	Community Development
3	Create a cultural inclusion plan for developers that would provide guidance on promoting a diverse mix of tenants.	Planning
4	Establish relationships between the Burke Heights Neighborhood Association and Dane County's Joining Forces for Families and Public Health's Violence Preventions Initiatives to help residents connect to services.	Joining Forces for Families, Public Health, NRT
5	Connect the Burke Heights Neighborhood Association and residents with the Parks Division, Madison Public Library, and Mobile Madison to help residents host neighborhood events at Sycamore Park.	Planning, Parks
6	Coordinate community clean-up events along primary neighborhood streets like Commercial Avenue and Sycamore Avenue.	Burke Heights Neighborhood Association
7	Support the establishment of neighborhood planting areas to be maintained by volunteers on privately-owned or City-owned property.	Parks, Engineering, Burke Heights Neigh- borhood Association
8	Encourage expansion of bicycle share (B-Cycle) facilities to the area. Consider a location at the planned Autumn Ridge Path bridge over Highway 30 at Ziegler Road.	Traffic Engineering
	Hawthorne-Truax CAS Recommendations:	Agencies
1	Support the establishment, capacity-building, and diversification of groups working to strengthen neighborhood cohesion and connections by sharing tools, tips, and other resources that help these groups become more inclusive and welcoming to the area's diverse residents.	Planning
2	Share guidance, information and resources with local groups and neighborhood associations to build capacity, support emerging groups, and empower diverse voices. Provide technical support and resources to assist residents in re-activating the Hawthorne Neighborhood Association.	Planning
3	Increase promotion of City residential assistance programs including rental and utility payment support, the property tax assistance program for seniors, and low-cost mortgage and rehabilitation loans.	Community Development
4	Strengthen Public Health's "Reduce Drug Harm and Violence" initiative to expand service to the plan area for mental and physical health, domestic violence, and violence prevention and intervention.	Public Health

92 Draft - City of Madison 752

5	Increase awareness and access to existing community programs and services and explore expanding them in locations such as the community rooms of new housing developments, Hawthorne Elementary School, the Reindahl Imagination Center, and through co-location of community-based organizations.	Hawthorne Neighbor- hood Association
6	Support Joining Forces for Families and East Madison Community Center in their efforts to address Truax resident needs like affordable childcare, healthcare, and nutritious food.	Community Development
7	Work together with the community and utilize City support and resources such as Parks Alive, Carts in Parks, and Madison Arts Commission funding and festival planning assistance to establish neighborhood events and activities in parks and open space, Truax apartment greenspace, and other gathering places. Feature food, music, crafts, artwork, and cultural performances that celebrate the diverse backgrounds of residents.	Hawthorne Neighborhood Association
8	Coordinate with area financial institutions to explore creating financial literacy programming for residents including adults, youth, and school age kids.	Economic Development
9	Partner with East Madison Community Center, Project Babies, RISE Wisconsin, the Lullaby program through the Overture Center, and other area organizations to provide support for pregnant people, babies, and young children.	Hawthorne Neighborhood Association
10	Work with community partners to support and expand Hawthorne Community School. Consider establishing Open Schoolhouse evenings that offer community programs and resources.	Hawthorne Neighbor- hood Association
11	Expand the Hawthorne Elementary Walking School Bus to serve students from the Carpenter-Ridgeway and Hawthorne neighborhoods.	MMSD
12	Create additional teen activities and programing, similar to the Goodman Center Teen Loft, at the East Madison Community Center or in other community spaces.	East Madison Community Center
13	Explore hosting community service and health fairs at Access Community Health Center, East Madison Community Center, Hawthorne Elementary School, and other neighborhood locations.	Hawthorne Neighbor- hood Association
14	Support organizations such as New Bridge Madison and Goodman Community Center to bring programming and services for older adults to the area, including community events such as neighborhood concerts.	Community Development
15	Explore possibilities for adding murals to blank building facades, such as the telecommunications building at Rowland Avenue and East Washington Avenue.	Planning
16	Work with Dane County to identify community-oriented interim uses for the greenspace in the Wright Street triangle.	Planning, Parks
17	Add one or more canoe and kayak launches along the west branch of the Starkweather Creek.	Engineering, Parks
18	Create urban arts districts along Lexington, and Mayfair Avenues to support the establishment of artist workshops, music studios, and makerspaces.	Planning

Northeast Area Plan - Draft

Glossary

Accessory dwelling unit: A second dwelling unit contained within a single-family dwelling or within a detached building located on the same lot as a single-family dwelling. This definition includes accessory buildings constructed in connection with a private garage or a private garage converted into a dwelling unit.

Activity Center: An intensively developed area that is the visual and/or functional center of a neighborhood(s) or a district. Activity centers are typically comprised of a mix of land uses developed at a higher intensity than the surrounding area including residential, commercial, employment, civic, institutional, and parks and open space uses.

Affordable housing: Housing for which the occupant(s) are paying no more than 30 percent of gross household income for housing costs, including utilities. Households with costs exceeding 30% of income are considered housing cost-burdened. The City of Madison's owner-occupant and rental development subsidy programs are primarily designed to reduce housing cost burdens for renter households with incomes at or below 60% of Dane County's median income (CMI) and for owner-occupied households at or below 80% CMI.

Affordable Housing Fund: A City of Madison program to provide loans and grants to for-profit and non-profit housing developers for the construction of new affordable rental housing.

Berm: A linear mound generally built to screen views, define areas, or direct stormwater.

Bicycle share (B-Cycle): A kiosk-based bike rental system offering hourly, daily, or subscription-based usage.

Big box retail: A physically large retail store with at least 75,000 square feet of floor area and a regional sales market. They are usually part of a national or regional chain of stores. Some examples include Walmart, Target, Best Buy, and Home Depot. (Source: APA: A Planners Dictionary)

BIPOC: An umbrella term for people of color, which stands for Black, Indigenous, and people of color. The term acknowledges that not all people of color face equal levels of injustice, recognizing that Black and Indigenous people are severely impacted by systemic racial injustices. (Source: Merriam-Webster)

Bump out: An extension of a raised curb into a roadway, typically a parking lane, to create additional terrace or sidewalk space. This can be used to shorten pedestrian crossing distances or create additional space for street trees while encouraging lower vehicular speeds.

Bus Rapid Transit (BRT): is a frequent, faster, and more reliable bus system that uses larger buses to transport more riders. BRT's features include frequent all-day service and direct routes with fewer stops. It utilizes special traffic signals to help buses get through intersections faster, dedicated bus lanes, and stations with off-board fare payment kiosks.

Capital Area Regional Planning Commission (CARPC): One of nine commissions in Wisconsin established to coordinate planning and development among area municipalities. CARPC develops and promotes regional plans, provides objective information and professional planning services, and focuses local attention on issues of regional importance. CARPC carries out land use planning and areawide water quality management planning for the greater Madison region. State statutes charge it with the duty of preparing and adopting a master plan for the physical development of the region. The Department of Natural Resources contracts with the Commission to maintain a continuing areawide water quality management planning process to manage, protect, and enhance the water resources of the region. (Source: CARPC)

Commercial Core: Street frontages where future development proposals are required to include ground floor commercial spaces.

Community Development Block Grant (CDBG): A federal program which provides annual grants to states and cities for affordable housing, anti-poverty, infrastructure, and planning activities that benefit low- to moderate-income persons.

Community garden: An area of land or space managed and maintained by a group of individuals to grow and harvest food crops and/or non-food, ornamental crops, such as flowers, for personal or group use, consumption or donation.

Community land trust: nonprofit organizations whose primary objective is the creation of homes that remain permanently affordable, providing successful homeownership opportunities for generations of lower income families. Under the community land trust (CLT) model, homeowners purchase their house, but not the land (this lowers the purchase price). The land is leased by the homeowner from the land trust. When homeowners sell, 75% of the appreciated value stays with the house, so it's more affordable for the next buyer. The CLT homeowner also gets to keep the equity they invested into the home plus 25% of the increase in value. (Sources: Madison Area Community Land Trust and Grounded Solutions Network)

Commercial Ownership Assistance program: The City's Commercial Ownership Assistance Program (COA) is focused on helping business owners expand their enterprises by transitioning from renting space to owning commercial property for their business. Priority is given to applicants who are people of color, immigrants, women, the disabled, veterans and any other underrepresented groups.

Complete neighborhood: are neighborhoods that include a range of housing types and costs, neighborhood-serving businesses, stores, services, schools, and places of worship. These elements are ideally accessible by foot, bicycle, or transit through a network of well-connected streets and blocks, usable public spaces, and a system of connected parks, paths, and greenways.

Continental crosswalk: A crosswalk with a more visible type of striping (painting), consisting of thick white stripes parallel to the direction of vehicular travel.

Cost-burdened: a household is cost-burdened when they are paying too much for their home (when total housing costs exceed 30% of their gross monthly income).

Drumlin: A linear or oval-shaped hill created by the streamlined movement of glacial ice sheets across rock debris. (Source: Britannica)

Easement: A legal tool that grants one party the right to use property that another party owns and possesses. (Sources: Investopedia, Merriam-Webster Dictionary)

Exclusionary zoning: A term applied to zoning standards, districts, or policies that seek to prevent people of certain races, ethnicities, or income levels from buying homes or living in specific areas or neighborhoods. This could include extensive use of exclusively single-family districts, large minimum lot or open space sizes, and narrow occupancy (household size) rules. (Source: Planetizen)

Facade modulation: Stepping back or extending forward a portion a building's facade. This can be done with vertical divisions using different textures or materials or adding features such as dormers, stepped roofs, arcades, awnings, and window bays.

Foreign Trade Zone (FTZ): a specific area within the United States, located in or near a port of entry, where certain types of merchandise can be imported without going through formal customs entry procedures or paying import duties. FTZs were designed to encourage international trade and U.S. employment. (Sources: Dane Country Regional Airport and Port of Seattle)

Frontage road: A street adjacent to a freeway, expressway, or arterial street separated therefrom by a dividing strip and providing access to abutting properties. (Source: APA: A Planners Dictionary)

Grade-separated intersection: an intersection where one roadway or lane travels above or below other roadways by separating where they cross each other with an overpass or underpass.

Greenway: Linear corridors of land and water and the natural, cultural, and recreational resources they link together. (Source: Massachusetts Office of Energy and Environmental Affairs)

Healthy Retail Access Program: A program created by Madison's Food Policy Council that provides funds for healthy retail projects that aim to improve access to affordable, healthy, and culturally appropriate food and retail within underserved areas.

Historic district: A significant concentration, linkage, or continuity of sites, buildings, structures, or objects united historically or aesthetically by plan or physical development. A local, state, or the federal government can officially recognize districts. (Source: U.S. National Park Service)

Housing cooperative: A residence that is collectively owned and controlled by its members: the people who live in the housing. The buildings typically have private bedrooms but shared spaces, such as common kitchens and recreation areas. Members often share responsibility for cooking, daily chores, and property maintenance.

Impervious surface: Any hard-surfaced area that does not readily absorb or retain water, including but not limited to building roofs, parking and driveway areas, graveled areas, sidewalks, and paved recreation areas. (Source: APA: A Planners Dictionary)

Income- and rent-restricted housing: a type of housing where a specific number of units in a building are reserved for low-income households. A qualifying household's income level must be at or below a specific level for that household's

size, such as 60% of the county median income. A limit is also set on the monthly rent to ensure that housing is more affordable for those households, often due to some form of subsidy. These restrictions are typically enforced through a Land Use Restriction Agreement.

Incubator: An area, commercial space, and/or building designated for the cultivation and enhancement of new or future businesses.

Infill development: Development of vacant or underused lots that are surrounded by developed areas.

Land Banking: A City program used to acquire land and buildings that could be used for future economic development, affordable housing projects, and other City uses. The goal is to acquire strategic properties for future purposes that might include: assisting displaced businesses, reducing blight, stabilizing housing markets, improving the quality of life of residents and neighborhoods, and preserving land for City purposes.

Makerspace: A term used to describe a place where people gather to share resources and knowledge, work on projects, network, and build. This could include artist studios, small-scale fabrication spaces, workshops, commercial kitchens, or similar spaces.

Market-rate housing: Housing that does not have any restrictions on rent or household income.

Missing Middle Housing: A range of housing types scaled between single-family detached houses and larger apartment buildings. Housing types that are considered as part of the missing middle include duplexes, triplexes, four-units, rowhouses, live-work buildings, accessory dwelling units (ADUs), clustered small homes like bungalow courts, and some small apartment buildings. The scale of missing middle housing is compatible with most existing single-family residential areas.

Mixed-use: A building or groups of buildings designed to encourage a diversity of compatible land uses, which include a mixture of two or more of the following uses: residential, office, retail, recreational, light industrial, and other miscellaneous uses. (Source: City of Beaverton, OR)

Neighborhood Development Plan (NDP): A plan prepared for largely undeveloped land on the city's edge. NDPs are adopted as supplements of the Comprehensive Plan and include recommendations for land use, transportation, parks and open space, and utilities.

Neighborhood Plan: A plan prepared for an already-developed area of the city that includes recommendations for land use, urban design, transportation, parks, placemaking, and other improvements/investments/changes to a given area. Neighborhood plans can encompass more than one neighborhood and are generally adopted as supplements to the Comprehensive Plan.

Neighborhood Resource Teams (NRTs): A citywide effort to coordinate and improve the delivery of City services to Madison's neighborhoods. NRTs provide a regular forum for City employees to meet, discuss, and support each other's efforts in delivering excellent City services. NRT membership can include alders, City staff, and non-City staff participants.

Official Map: A formal public record used to indicate where a government is likely to require right-of-way, easements, or land for future roads, drainageways, utilities, or recreation facilities. Within officially mapped areas, a property owner maintains control and use of their property but building permits cannot be issued. Official Mapping is established in Wisconsin State statute 62.23(6). (Source: UW Extension)

Operating costs: Expenses associated with the maintenance and administration of a business or government on a day-today basis, such as salaries. (Source: Investopedia)

Per- and polyfluoroalkyl substances (PFAS): A group of chemicals used to make products that resist heat, oil, stains, grease, and water. They are long lasting chemicals, which break down very slowly over time and exposure to them may be linked to harmful health effects. (Sources: US Environmental Protection Agency and Centers for Disease Control and Prevention)

Placemaking: Creation of an environment that fosters community, stimulates interaction, encourages entrepreneurship, generates innovation, and nurtures humanity. (Source: Project for Public Spaces)

Proactive rezoning: Proactive rezoning is when the City, rather than a property owner or developer, proposes to rezone land so that it is consistent with plan recommendations. When the City proposes to proactively rezone areas during a planning process, it informs property owners about the recommendation and whether it could affect them.

Property Tax Assistance for Seniors Program: A City program that pays all or a portion of property taxes for qualified homeowners who are over the age of 65 and own a single-family residence within the City of Madison.

Rectangular rapid flashing beacons (RRFBs): A flashing signal activated by pedestrians that alerts drivers to yield to crossing pedestrians.

Redevelopment: Construction of a new building where a building already exists.

Rental Rehab Loan Program: City of Madison program which offers financial assistance, including low-interest loans, for Madison property owners to renovate and improve rental housing.

SafeGrowth: A philosophy built on the belief that healthy and functioning small neighborhoods provide the safest way to build cities in the 21st Century. It is based on the premise that crime is best tackled within small neighborhoods by harnessing the creative energy of functioning neighborhood groups, by employing the latest crime prevention methods, and by adopting an annual SafeGrowth® Plan to address crime and fear. (Source: SafeGrowth.org)

SEED Program: A City of Madison program administered by the Madison Food Policy Council that provides grants to improve the local food system and make food more accessible to Madison residents.

Sense of Place: The characteristics of a location that make it readily recognizable as being unique and different from its surroundings and that provides a feeling of belonging to or being identified with that particular place. (Source: Scottsdale, AZ)

Setbacks: A building design where there are fewer stories closer to the lot line (for example, near sidewalks and adjacent properties) than the rest of the building.

Shared-use path: a path or lane shared by pedestrian, bicycle, and other non-motorized users.

Smaller-scale housing: Also referred to as missing middle housing, it is a range of smaller multi-unit or clustered housing types compatible in scale with single-family homes. (Source: Opticos Design, Inc.)

Stepbacks: A building design element that is typically applied to the upper-story of a development to establish compatibility with surrounding development and maintain a pedestrian-oriented scale. A stepback requires that any portion of a building above a certain height is further pushed-in towards the center of the property.

Stormwater: Untreated runoff from rainfall and snowmelt. It flows across impervious surfaces (such as streets), through fields, and over construction sites, crossing municipal boundaries and can carry contaminants to lakes and streams. (Source: Dane County Office of Lakes & Watersheds)

Tabletop crossing or intersection: An intersection where the roadway ramps up to the sidewalk level to create greater visibility for pedestrians, improve usability for individuals who are mobility impaired, and slow vehicles down. Also known as a raised crossing or intersection.

Tax Increment Financing (TIF): A governmental finance tool to provide funds to construct public infrastructure, promote development opportunities, and expand the tax base.

Terrace: The space between the sidewalk and the curb along a street.

Through movement: within an intersection, the through movement refers to vehicles or users going straight and not turning onto the cross street.

Transit-Oriented Development (TOD): Compact, walkable, mixed-use development that is centered on quality public transit. It typically includes a mix of housing, office, retail, neighborhood amenities, and other uses within walking distance of a transit station. TOD reduces the need for driving by creating compact, vibrant, walkable neighborhoods with convenient access to activities and destinations connected by transit.

Tree canopy: The part of a city or area that is shaded by trees; The layer of leaves, branches, and stems of trees that obscure the ground when viewed from above. (Source: Center for Watershed Protection)

Underrepresented groups: Groups of people with a common race, ethnicity, immigration status, age, income level, gender identity, or sexual orientation who have not typically participated in City decision-making processes corresponding with the proportion of the population they comprise. These groups have often experienced discrimination or marginalization based on their identity.

Urban agriculture: The production of food for personal consumption, market sale, donation, or educational purposes within cities and suburbs.

Urban form: the patterns of building height and development intensity as well as the structural elements that define an area physically, such as natural features, transportation corridors, open space, public facilities, and other elements. (Source: City of Los Angeles)

Water quality: The condition of water, including its chemical, physical, and biological characteristics with respect to its expected use, for example, drinking, swimming, or fishing. (Source: Florida Brooks National Marine Sanctuary, Key West, Florida)

Watershed: Watersheds are an area of land that drain to the same location (the outlet).

Wayfinding: Wayfinding refers to information systems that guide people through a physical environment and enhance their understanding and experience of the space. (Source: The Society for Experiential Graphic Design)

Zoning Code: An ordinance that regulates land use, lot size, building placement, building height, and other aspects of the development of land.



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84383

File ID: 84383 File Type: Ordinance Status: Council New

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 07/10/2024

Enactment Number:

File Name: Adopt the West Area Plan Final Action:

Title: Adopting an amendment to the Comprehensive Plan related to the West Area

Plan.

Notes: 6884WestAreaPlan

Sponsors: John P. Guequierre Effective Date:

Attachments: West Area Plan Introduction Draft, West Plan Phase

1 Feedback Summary, West Plan Phase 2 Feedback Summary, West Plan Phase 3 Feedback Summary, West Plan Phase 4 Feedback Summary, Resolution 84377-Adopting West Area Plan, West Area

Adopting Memo.pdf

Author: Kate Smith Hearing Date:

Entered by: mglaeser@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Attorney's Office	07/10/2024	Referred for				
			Introduction				
	Action Text: This Ordinance was Referred for Introduction						
	Notes:	tes: Plan Commission- Public Hearing (8/26/24), Economic Development Committee (7/17/24), Transportation					
	Commission (7/31/24), Landmarks Commission (8/5/24), Board of Parks Commissioners (8/14/24), Urban Design Commission (8/14/24), Common Council (9/10/24)						

Text of Legislative File 84383

Fiscal Note

Flscal note pending.

Title

Adopting an amendment to the Comprehensive Plan related to the West Area Plan.

Body

DRAFTER'S ANALYSIS: This ordinance adopts the West Area Plan.

The Common Council of the City of Madison do hereby ordain as follows:

WHEREAS, on August 2, 2022 the City of Madison Common Council authorized the Planning Division's shift to a planning framework with twelve (12) Area Plans that cover the entire city and are updated every ten (10) years (RES-22-00577); and

WHEREAS, the City of Madison Common Council authorized the Planning Division to prepare the West Area Plan on January 3, 2023 (RES-23-00021); and

WHEREAS, the planning framework states that the future land use map developed through each area plan process will directly update the Comprehensive Plan's Generalized Future Land Use Map; and

WHEREAS, the City of Madison Common Council adopted a public participation plan for the West Area Plan on July 25, 2023 (RES-23-00516), as required by Wis. Stat. § 66.1001(4) to amend the Comprehensive Plan, which described how the City intended to make the process accessible to all; and

WHEREAS, the public participation process included engagement through a project website, interactive mapping exercise, virtual meetings, in-person meetings, emailed project updates to interested parties, distribution of informational materials, focus groups, neighborhood association visits, social media posts, traditional media, and board/committee/commission progress updates throughout the process; and

WHEREAS, the public participation process also included contracting with several Community Partners and coordinating with the City's Neighborhood Navigators to engage traditionally underrepresented residents through focus groups and participation in community events and activities; and

WHEREAS, notice was provided pursuant to Wis. Stat. § 66.1001(4)(d) and the Plan Commission held a public hearing on the proposed amendment to the Comprehensive Plan on August 26, 2024; and

WHEREAS, the Plan Commission, by a majority vote of the entire Commission recorded in its official minutes, adopted a resolution recommending the Common Council adopt an amendment to the City of Madison Comprehensive Plan's Generalized Future Land Use Map, Bicycle Facilities Map, and relevant text and maps in the Comprehensive Plan that reference area plans, including underlying plans that are archived; and

NOW, THEREFORE BE IT RESOLVED the Common Council of the City of Madison does, by the enactment of this ordinance, formally amend the City of Madison Comprehensive Plan's Generalized Future Land Use Map, Bicycle Facilities Map, maps and text that reference underlying archived plans, and any amendments to the Plan made from the Common Council floor, pursuant to Wis. Stat. § 66.1001(4)(c).

BE IT FINALLY RESOLVED that staff is directed to implement the actions contained in the West Area Plan.

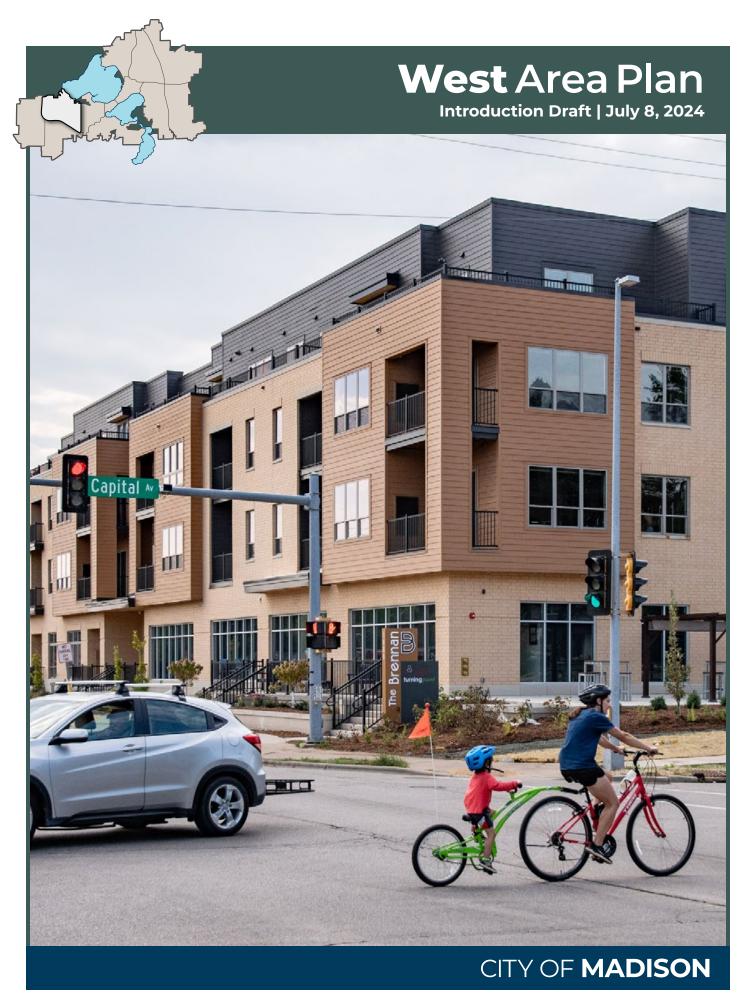


Table of Contents

Acknowledgements1
Introduction
Land Use
Transportation
Neighborhoods and Housing30
Economy and Opportunity34
Culture and Character
Green and Resilient46
Effective Government54
Health and Safety
Appendix56

Adopted by the Common Council on (Date)

Resolution RES24XXX Legistar file # XXXXX

Acknowledgements

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Mark Fitzgerald Matt Frater Jaymes Langrehr

Police

Captain Kelly Beckett
Capitan Jason Freedman

Fire

Assistant Chief Scott Bavery Chief Chris Carbon Bill Sullivan

Public Health

Melinda Knuth Justin Svingen

Economic Development

Mike Miller Saran Ouk

Community Partners

African Center for Community Development Claire Baker Francis Medrano Sedgwick Smith

Introduction



Planning Framework

The West Area Plan is one of the initial two area plans prepared following the <u>City's Planning Framework</u> adopted by the Common Council on August 2, 2022. The framework covers a consistent set of topics and provides clear, high-level plan actions for specific physical improvements. The framework established 12 distinct geographies, each of which will be updated approximately every 10 years. Many actions in this Plan can be implemented over the next decade, but some actions are included that either have an uncertain timeline or may take longer than a decade to implement.

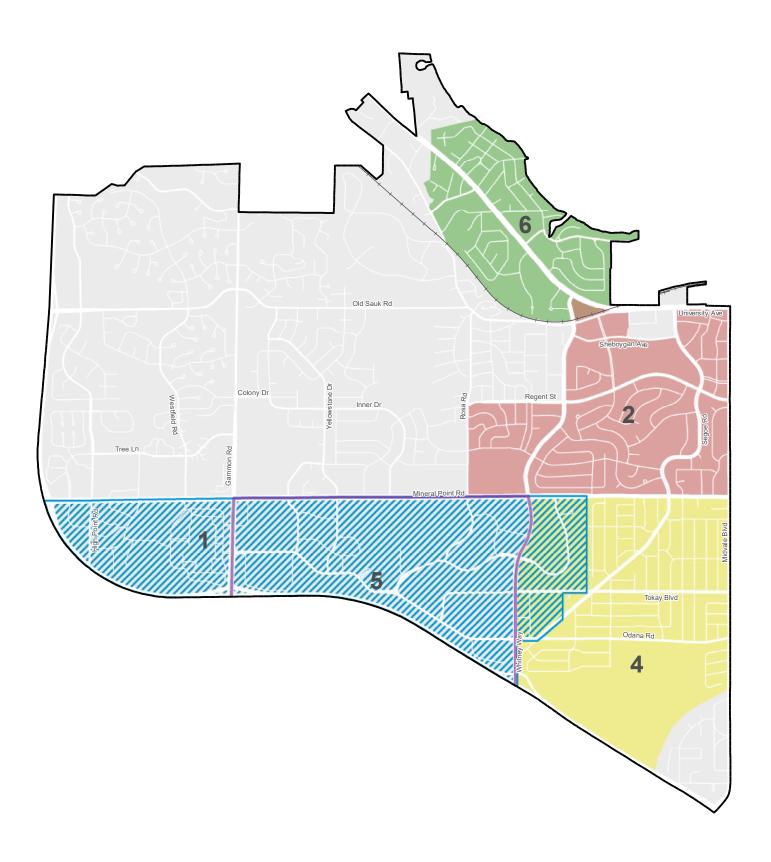
Previously Adopted Underlying Plans

While this Plan is a new effort under the Planning Framework, it is not the first plan to be prepared within the area. The Common Council adopted five plans to guide City and neighborhood decision-making in portions of the West Area since 2006. The five plans have been important for their neighborhoods over the past 15-plus years. However, the Planning Framework provides a more equitable approach to creating plans. Under the previous approach certain areas of the city received greater attention while others never had a plan beyond the citywide Comprehensive Plan. Providing full coverage of a growing city and regular updates to reflect changing conditions required a new approach to planning. This approach also creates greater clarity under a single Area Plan, rather than underlying plans with different and sometimes inconsistent recommendations for a particular geography.

Part of the West Area Plan process involved reviewing previously adopted plans with a focus on potential physical or regulatory changes specific to the West Area, such as land use, zoning, building height, transportation, parks and open space, public utilities, and stormwater infrastructure. Recommendations that simply repeated existing Citywide policies, have already been implemented, are inconsistent with current City policy, or were to be implemented by non-City entities were not incorporated in this Plan. The underlying plans were then archived: they will no longer guide development review, future City projects, policies, budgets, or work plans. Going forward, the West Area Plan will guide decisions. Archived plans for the West Area are:

- 1. Odana Area Plan (2021)
- 2. University Hill Farms Neighborhood Plan (2016)
- 3. Hoyt Park Area Neighborhood Plan (2014)
- 4. Midvale Heights/Westmorland Joint Neighborhood Plan (2009)
- 5. Southwest Neighborhood Plan (2008)
- 6. Spring Harbor Neighborhood Plan (2006)

The portion of the Hoyt Park Area Neighborhood Plan that underlies the West Area is not shown on the map since it was replaced by the University Hill Farms



West Area Plan

³ 765



Neighborhood Plan. Only the portions of the Midvale Heights/Westmorland Joint Neighborhood Plan and the Southwest Neighborhood Plan that underly the West Area Plan boundary are considered archived.

Relationship between the West Area Plan and the Comprehensive Plan

The Comprehensive Plan includes citywide policies and priorities for investment based on feedback received through an extensive community engagement effort. Recommendations are grouped by the seven elements illustrated below. The Comprehensive Plan also includes policies that guide future growth in a way that supports citywide policies.

The West Area Plan applies policies from the Comprehensive Plan, and other citywide initiatives, into specific recommendations for the West Area geography based on a closer evaluation of the area and additional community engagement.

While Area Plans help guide future growth, they don't automatically create or require development. Instead, these Plans guide the City's review process when development is proposed. Proposals that generally follow plan recommendations are likely to be approved. Those that don't face greater scrutiny, and approval is less likely. Plans also provide a guide for future City projects and programs, and future City budgets will determine the timing of those actions City budgets will determine the timing of those actions.



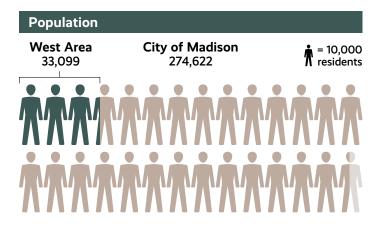
The West Area

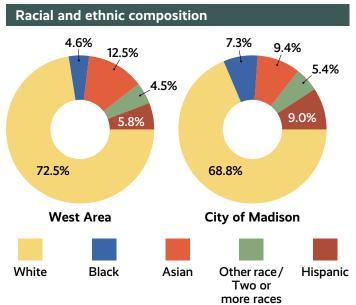
The City of Madison is located within the ancestral homelands of the Ho-Chunk people, who have lived in this area for time immemorial. The West area is over 5,500 acres (8.7 square miles), bounded by the West Beltline Highway to the west and south, Midvale Boulevard to the east and the Village of Shorewood Hills, Lake Mendota, and the City of Middleton to the north. The area features several of Madison's largest commercial and employment centers, including Hilldale Shopping Center, West Towne Mall, University Research Park, and the Odana Road Corridor. The planning area is also crisscrossed or bordered by some of the busiest roads and transit corridors in the region, including University Avenue, Mineral Point Road, Whitney Way, Gammon Road, and the West Beltline Highway.

People are at the heart and soul of a community, and there are just over 30,000 residents in the West Area. People of color make up 27.5% of the population. Residents of Asian backgrounds comprise 12.5% of the population, many of whom live in the Sheboygan Avenue, Segoe Road, and Oakbridge areas. Residents identifying as Black are 4.6% of the population, many living in the Tree Lane and Wexford Ridge areas (2020 Census). Marsh The West Area has a strong social network of community-building groups, agencies, and insti-Old Sauk Rd Owen Sheboygan Ave Conservation Rennebohm Park Colony Dr Regent St Rd Rd Garnei Tokay Blvd **West Area Plan** Odana Rd **Planning Area** Odana Hills Area Plan Boundary tutions that offer programming, services, entertainment, and other resources. There are 27 registered neighborhood groups that vary in structure, mission, and agenda (see the City's Neighborhoods Map). Some host occasional social events while others organize year-round activities, community improvement projects, engagement in city projects, review of development proposals, and more. The Alicia Ashman Public Library is located within the West Area and Sequoya Commons Public Library is just outside it. There are a number of community-based organizations, schools, and places of worship in the Area

West Area Plan

that also build community. The West Area has four public elementary schools





Income and Household Economics

	West Area	City of Madison
Poverty rate (families)	7.0%	5.9%
Median income	\$82,401	\$70,466
Homeownership (Tenure)	52.7%	44.5%
Cost-burdened households – owners	25.5%	19.8%
Severely cost-burdened households – owners	10.2%	6.8%
Cost-burdened households – renters	39.1%	46.5%
Severely cost-burdened households – renters	21.2%	25.5%

(and portions of six school attendance areas), two middle schools, and one high school in the Madison Metropolitan School District. There are also approximately 20 places of worship. In addition to spiritual support and religious affiliation, some offer community gathering space, pre-school classes, day care, social activities, blood drives, food pantries, health screenings, community gardens, and more.

Development History and Character

The earliest annexation of land to the City in the West Area occurred in 1947, when part of today's Summit Woods Neighborhood and the Odana Hills Golf Course were added to the city, followed by what would later become Hilldale and Rennebohm Park. The city continued to grow to the south and west through the 1950s, 1960s, and 1970s. Other than portions of University Research Park, which is a former UW Madison farm that began to develop in the late 1980s, the West Area was largely built out by the mid-1990s.

The area contains a variety of post-World War II development styles in a relatively compact area. University Hill Farms, to the southwest of Midvale Boulevard and University Avenue, was the first major development in the area, and was constructed in the 1950s and 1960s. Development continued to the south and west, with the gridded streets of the Midvale Heights neighborhood and the curvilinear streets of Faircrest and Parkwood Hills. The West Towne Mall regional shopping center was constructed at what was the edge of the city in the early 1970s, serving both new residents and drawing retail out of downtown. Development of the Walnut Grove, Sauk Creek, and Wexford Village neighborhoods, which include cul-de-sacs and development on private roads, followed in the late 1970s through the late 1980s. Other than a few scattered undeveloped properties, the only major piece of undeveloped land remaining in the West Area is a 35-acre parcel of land owned by TruStage (formerly CUNA) located south of Mineral Point Road between University Research Park and Oakwood Village.

The redevelopment of the parking lots surrounding Hilldale Mall in the mid-2000s kicked off a number of significant redevelopment projects that have increased the density of the West Area. Major redevelopment projects over the last 15 years include University Crossing at

Whitney Way and University Avenue, redevelopment of State-owned land as the Madison Yards development between Sheboygan Avenue and University Avenue, the completion of several major apartment buildings in the "University Park" project at the former Westgate Mall, and several major apartment/mixed-



use buildings in the Grand Canyon Drive/Yellowstone Drive area north of Odana Road. With Bus Rapid Transit (BRT) coming to the West Area in late 2024, the demand for further residential and mixed-use redevelopment is expected to continue.

Planning Process and Public Engagement

The West Area Plan process officially began with Common Council authorization on January 3, 2023. There were five planning phases, beginning with background data gathering and review of underlying sub-area plans. There were numerous opportunities for public participation. City staff hosted virtual and in-person public meetings in winter, spring and summer 2023, and invited people to share feedback through an online Interactive Map and Community Survey. City staff also conducted a business survey to learn about issues and priorities of business owners. In spring 2023 staff engaged with area teens through UW-Madison's PEOPLE Program class on urban planning at Vel Phillips Memorial High School. Youth engagement continued in the summer with the Youth Action internship program at Lussier Community Education Center.

Community Partners who have engagement expertise and trusted relationships with Black, Indigenous and People of Color communities were hired through a "Call for Commu-

nity Partners" to ensure the voices of people who are traditionally underrepresented in city processes were heard. Community Partners included the African Center for Community Development, Claire Baker, Francis Medrano, and Sedgwick Smith. They conducted a variety of outreach and education activities from

Hilldale Mall



The Madison Yards project redeveloped a former Wisconsin DOT parking lot into apartments, offices, and a grocery store.



New affordable housing replaced the former Westgate Mall at Whitney Way and Tokay Boulevard.



August 2023 public meeting at Rennebohm Park



May 2024 West Area Plan open house

May through November 2023, including helping city staff plan and facilitate focus groups, and providing education and outreach at festivals, fairs, and other activities in the community.

The final phase of the planning process ran from March 2024 through early June 2024 and included four virtual public meetings and four in person public meetings. The draft Plan was also posted to the <u>project webpage</u> for online review and feedback, and plan information was available at Alicia Ashman Library, Sequoya Library, and the Lussier Community Education Center. Themes that emerged from public feedback for each of the Plan's topic areas are included within the introductions of each chapter.

While adopted citywide plans (like the Comprehensive Plan) and adopted underlying plans served as the starting point for West Area Plan actions, public feedback provided additional insights into what people value and what they feel are the most important assets and opportunities. This helped identify themes and develop actions that are relevant to the community, while also being grounded in the realities of what is feasible for the City to implement and what can be implemented by community partners.

In some cases, conflicting feedback had to be balanced when drafting actions. For example, some residents felt that more housing is needed, while others felt enough development has already occurred in the West Area.

A final draft Plan was developed for review and approval by City Boards, Committees, Commissions, and the Common Council after the final phase of public engagement. The adopted Plan will be implemented over time by City agencies and other governmental entities, community organizations, and other stakeholders. Policy makers are responsible for the allocation of resources for the entire City, so funding for the West Area Plan's City-led actions will

Planning Process and Public Engagement



be weighed against other projects citywide. Securing funding from outside sources, leveraging other available funding, or combining projects will help in implementing some of the actions in this Plan.

Plan Format

The Plan is structured around the Comprehensive Plan's seven elements, with each element generally having four sections:

- 1. An introduction, covering background information and current conditions
- 2. What We Heard, including major themes in feedback and comments
- 3. Actions, which focus on City-led items
- 4. Partnerships, which focus on items led by entities other than the City but may have some City involvement or contributions

While public feedback themes were generally used to inform actions and partnerships, not all summarized themes have corresponding actions or partnerships. Some feedback themes contradicted each other. Some themes conflicted with adopted City policy, such as the Comprehensive Plan. Lastly, some themes addressed topics the City has little control over. Actions in each chapter are listed by number, but do not have a priority order.

Comprehensive Plan Goals

This Plan aims to advance the following goals from the Comprehensive Plan:

- Madison will be comprised of compact, interconnected neighborhoods anchored by a network of mixed-use activity centers.
- Madison will have a safe, efficient, and affordable regional transportation system that offers a variety of choices among transportation modes.

The planning is comprised of employment, retail, institutional/governmental, parks and open space, and residential uses, with a limited amount of vacant land. Residential uses cover approximately 44% of the Area. Most of this residential land is zoned for single-family residential, with only 1% zoned for 2 to 4 dwelling units and 5% zoned for multi-family housing (greater than four units per building). There are approximately 16,200 dwelling units in the planning area, 42% of which are single-family dwellings (7% higher than the city as a whole), 1% are two-family dwellings (4% lower than the city), and 57% are multi-family dwelling units (3% lower than the city).

Commercial uses make up approximately 15% of the area's land use, while Parks and Open Space make up approximately 12%. The vast majority of the commercial uses are clustered in and around Hilldale Shopping Center, at Whitney Way and the Beltline, along Odana Road, and in the West Towne Mall area. Employment uses are clustered in University Research Park and areas to the west along Odana Road. The combination of shopping and employment nodes make the West Area a destination for the region, attracting shoppers and employees from Dane County and beyond.

Land Use	Acres	% Of Total
Residential	2,447	44%
Commercial	814	15%
Institutional/Governmental	296	5%
Industrial	2	~0%
Park and Open Space	648	12%
Agriculture/Vacant/Under Construction	102	2%
Utilities, Transportation	1,269	23%
Total	5,578	100%

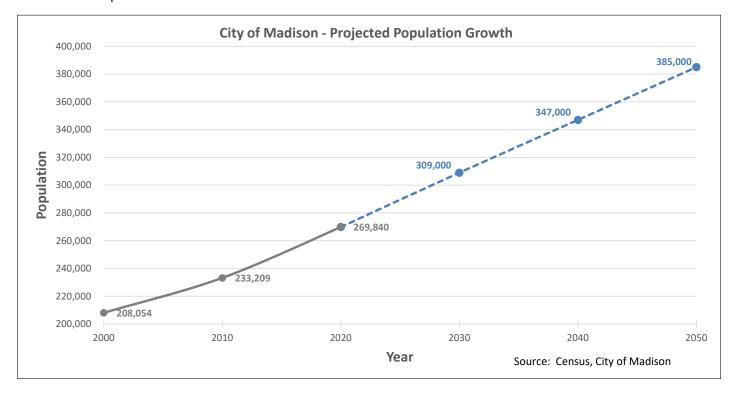
Source: Capital Area Regional Planning Commission (CARPC) 2020 Land Use

Future Growth

According to the Comprehensive Plan, the city is projected to add 115,000 new residents and 50,000 jobs between 2020 and 2050. While this growth will be spread across the entire city, a portion will occur within the West Area. The Comprehensive Plan emphasizes accommodating projected growth through redevelopment of underutilized sites, particularly on transit corridors. Such corridors and Activity Centers are identified as "Growth Priority Areas" in the Comprehensive Plan and include corridors like Mineral Point Road and University Avenue, along with Activity Centers like Hilldale and West Towne Mall. Accommodating growth through redevelopment makes use of existing infrastructure, reduces dependency on automobile travel, and preserves farmland, among other benefits.

Much of the redevelopment within the West Area to date has occurred along transit corridors, a trend that this Plan anticipates will continue. The Generalized Future Land Use (GFLU) Map in the Plan recommends the most intensive planned land uses along BRT lines, including Mineral Point Road and Sheboygan Avenue. There are many underutilized properties and large surface parking lots along the BRT line recommended for increased development intensity. See the Land Use Vision map for overall future land use themes.

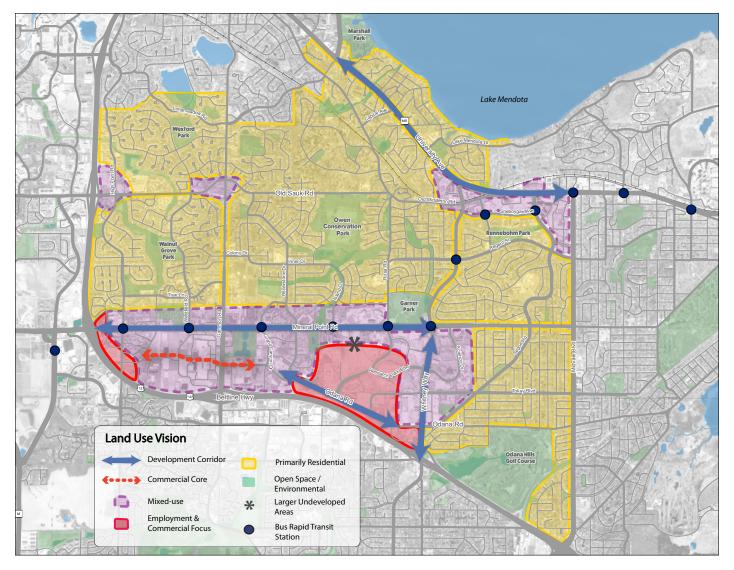
In addition to allowing more intensity along major corridors, places of worship, which have traditionally been mapped as "Special Institutional" or "Low Residential" in past plans, have been shifted to land use categories that allow mixed-use or multi-family residential development. If the institutions – many of which are along major corridors – should ever choose to pursue additional development on a portion of their property, consolidate, relocate, or close, more intense redevelopment can be considered.



Recommended Future Land Use and Growth Areas

The GFLU Map establishes a framework for how the West Area can grow and evolve over time. The map identifies broad categories of development such as residential, commercial, or mixed-use. The future land use categories guide future growth and establish the basis for how the City reviews development proposals. The land use mapping and actions in this Plan embody several of the Comprehensive Plan's citywide strategies, including:

- Concentrating the highest intensity development on major transit corridors and at activity centers;
- Facilitating compact growth to reduce development of farmland;
- Increasing the amount of available housing;
- Supporting the development of a wider mix of housing types, sizes, and costs.



While the GFLU Map establishes a future vision, there isn't a defined timeline for change. Many factors impact development, making it hard to predict where and when it might occur. The overall economy, market demand, available properties, financing, and developer priorities all influence the timing and location of development proposals.

The larger areas that meet the Comprehensive Plan's strategy of concentrating the highest intensity development on major transit corridors and at Activity Centers are shown as mixed-use on the Land Use Vision map, and include West Towne Mall, University Research Park, and the Hilldale area. While these areas have seen varying intensities of redevelopment already, significant portions remain as low-density uses and/or surface parking lots. Major "Development Corridors" shown on the Land Use Vision map are University Avenue, Mineral Point Road, Odana Road, and Whitney Way from Mineral Point to the Beltline. These mixed-use areas and corridors will see the majority of redevelopment in the West Area. However, there are other smaller properties and areas that can be appropriate for redevelopment, consistent with the planned land uses on the GFLU map.

The land use categories on the GFLU map are consistent with the Comprehensive Plan categories and are intended to guide decisions on zoning and

redevelopment proposals. However, they are not zoning designations. Future discussions and decisions on zoning and redevelopment proposals should be focused on whether the proposed changes are consistent with the GFLU Map, the actions in this Plan, and the goals, strategies, and actions in the Comprehensive Plan. Regent St Tokay Blvd **West Area Plan Generalized Future Land Use Bus Rapid Transit (BRT) Station** High Residential (HR) **Commercial Core** Neighborhood Mixed Use (NMU) Planned Streets Community Mixed Use (CMU) **Employment or Residential Only Mixed Use Area** Regional Mixed Use (RMU) **Generalized Future Land Use** General Commercial (GC) Low Residential (LR) Employment (E)

West Area Plan

Parks and Open Space (P)

Special Institutional (SI)

Low-Medium Residential (LMR)

Medium Residential (MR)

Implementation and Development Regulation **Maximum Building Heights**

The Comprehensive Plan provides height guidance for most land uses. A select few land use categories don't have recommended height ranges or allow Area Plans to recommend heights that may differ from those ranges. Mapping maximum building heights for these categories is intended to increase predictibility and simplify the development review process by communicating recommended height limits for these categories. The maximum building height recommendations consider the intensity of development for each land use category, adjacent land uses, desired urban form, and proximity to amenities like transit and parks. Refer to the Comprehensive Plan for guidance on areas without identified maximum heights on this map.

Additionally, some maximum building heights in this Plan are taller than the Morey Airport maximum height map in the northwestern portion of the West Area. If a project seeks to exceed Morey height limits a variance must be secured from the City of Middleton.

Commercial Core

The GFLU Map identifies several mixed-use categories focused on major corridors. These areas are planned for a mixture of residential, commercial, and em-



Images below illustrate types of development commonly found in each category. More information on each of the land use categories can be found in the

Land use categories

Comprehensive Plan.

*In select conditions, Low Residential may allow up to 30 DU/ac and three stories. In select conditions, Low Medium Residential may allow up to 70 DU/ac and four stories.



Low Residential (LR)







Employment (E)

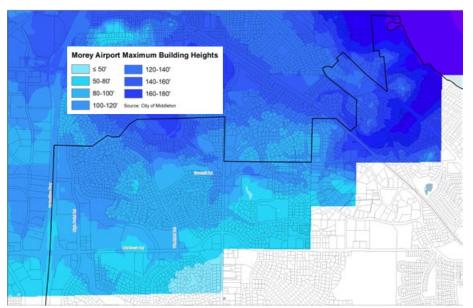


Neighborhood Mixed-Use (NMU)









Morey Airport height regulations may impact development in the northwest part of the West Area

ployment uses to achieve an active and vibrant place. While a mixture of uses is envisionsed, mixed-use buildings with retail or other commercial uses on the ground floor are generally not required. Within these areas, commercial uses are seen as appropriate as part of a broader use mix, but not every location can support ground floor commercial businesses. Visibility and enough residents and customers nearby are needed to support the businesses that provide goods and services. Future buildings along Commercial Core frontages, however, should have ground floor commercial uses (retail, restaurant, service, or office uses). These areas appear capable of supporting commercial spaces and are located at important nodes and along major corridors.

Employment or Residential Only Mixed-Use

A few areas on the GFLU Map are designated as "Employment or Residential Only Mixed Use Area." These areas should not include retail nodes, instead focusing on residential and/or employment land uses.

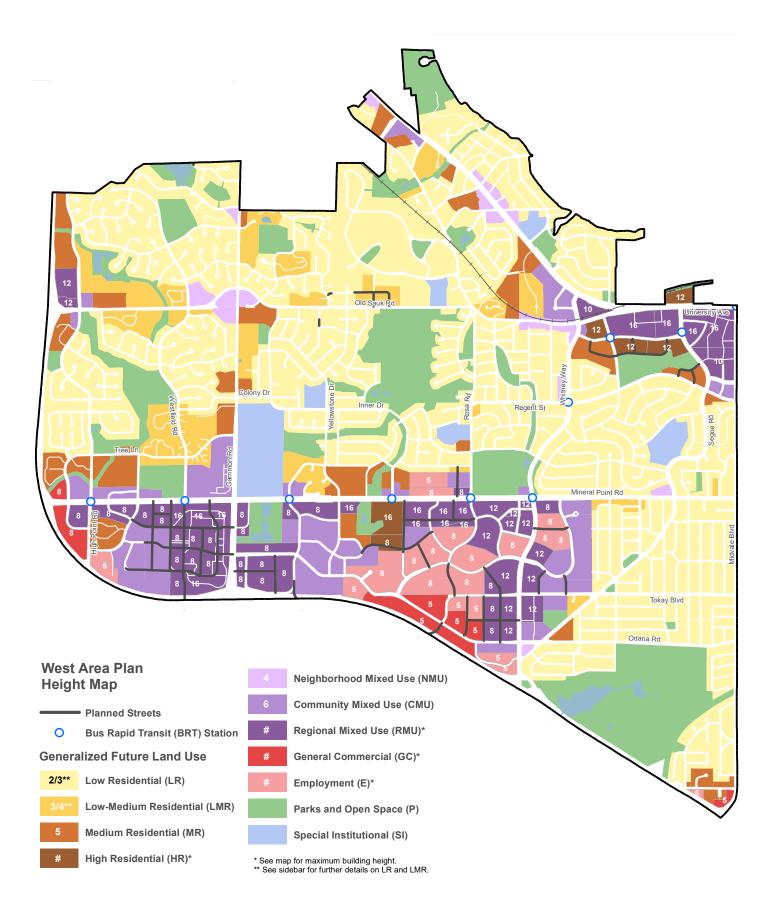
Aligning Future Land Use and Zoning

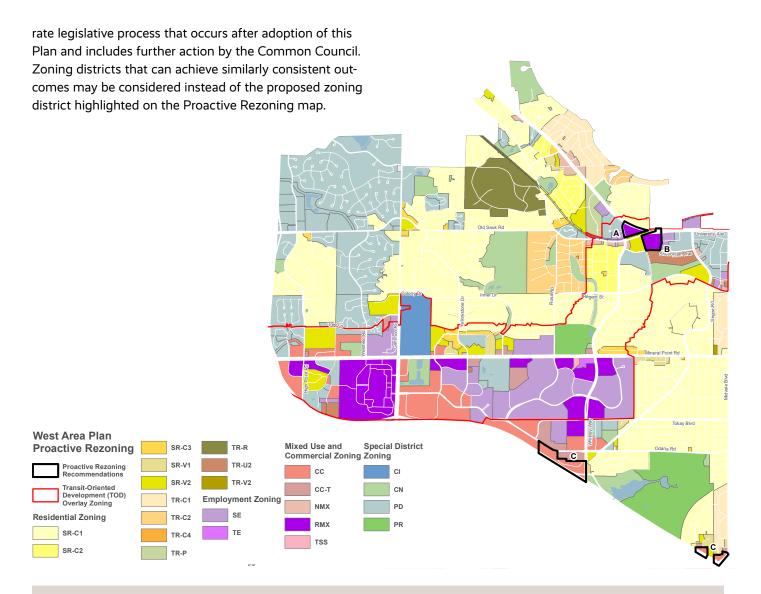
Zoning plays a key role in the implementation of adopted plans. Several recent modifications to the zoning code facilitate higher-density and compact forms of development across the city, including in the West Area. These modifications included increasing the number of dwelling units allowed in mixed-use buildings without conditional use approval and creation of a Transit Oriented Development (TOD) overlay zoning district. TOD encourages development of new housing units along the City's primary transit routes through increases in allowed density and elimination of minimum parking requirements. The TOD overlay also includes increased requirements to improve the design of new development. Portions of the West Area are located within the TOD Overlay District.

This Plan recommends several properties for proactive rezoning. Proactive rezoning is a City-initiated process that rezones property to a district consistent with this Plan's GFLU Map. Implementation of proactive rezoning is a sepa-

What We Heard

- More mixed-use development and high density at appropriate locations.
- 2. Avoid over-developing and losing neighborhood character.
- Ground floor development at the corner of Regent/Whitney Way and along Midvale Boulevard should include active uses.
- Desire not to include retail in areas of low-density development, while others would lke retail that they can walk to.
- Create more walkable spaces.
 Some areas that can benefit from improved walkability are West Towne Mall area, parks, the BRT route near employment and residential areas, and the Madison Yards area.
- More options for low density housing that isn't single-family are desired, such as Missing Middle housing and smaller 2–3 story apartments.
- A balance between single-family and multi-family development is needed.
- Some skepticism about changing planned land use from Low Residential (LR) to other residential or mixed-use categories that would allow for more intense future development.
- A mix of feedback between some people wanting more housing and businesses while others who don't want more construction.
- 10. Appreciation for the mix of uses within the West Area, with retail, employment, restaurants in proximity to, or part of, neighborhoods.





Planned Development Zoning

Many areas in the city have a site-specific zoning called Planned Development (PD). PDs are intended for unique buildings or development projects that can't be approved using conventional zoning districts. These developments don't need to follow other zoning requirements and establish their own unique and detailed zoning code addressing uses, building placement, height, landscaping, parking, etc.

PDs were heavily used prior to the adoption of the current zoning code in 2013. The previous code, adopted in 1966, was so burdensome, complex, and out-of-date that PD zoning was routinely used for relatively typical projects, including single-family homes and small apartment buildings. Over the years, the City approved over 1,500 PDs, and while they simplified the approval process, they create long-term challenges for residents, property owners, and the City. Zoning

codes are amended over time to address new issues or priorities that emerge, but PD files are rarely updated and reflect the time when they were adopted. Zoning code updates allowing accessory dwelling units and the keeping of chickens, or those requiring electric vehicle charging or bird-safe glass, do not apply to these PDs. The zoning text for PDs are only available in person (not online, like the rest of the code), and review and approval for minor changes like the addition of a shed can take weeks instead of minutes. The current zoning code is much more adept at handling these now-typical developments that were previously approved as PDs. Transitioning PDs approved under the old code to conventional zoning districts will improve transparency and usability for residents and property owners while improving the ability of the City to respond quickly to inquiries and routine applications like fences, sheds, and additions.

The West Area also has many properties with Planned Development (PD) zoning. Each PD has a unique set of regulations which is cumbersome for both residents and the City. This Plan recommends developments zoned PD - the bulk of which are along or west of Gammon Road - be reviewed for potential transition into conventional zoning districts to simplify zoning applications for residents and simplify administration for staff.

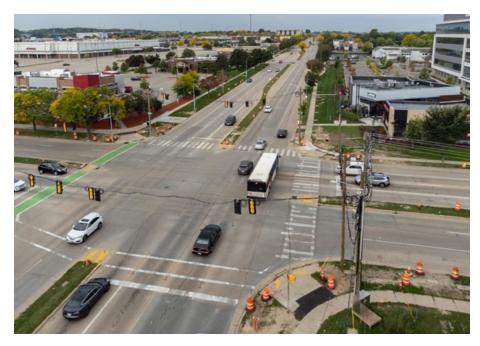
Actions

- 1. Update the Comprehensive Plan Generalized Future Land Use Map to be consistent with the Future Land Use map in this Plan.
- 2. Rezone properties identified in the Proactive Rezoning map to implement the goals of this Plan and encourage development consistent with the actions in this Plan:

Site	GFLU	Existing Zoning	Potential Zoning	Notes
Α	RMU	NMX	RMX	Create consistency with GFLU Map; allow for more intense development in close proximity to transit.
В	RMU	SE	RMX	Create consistency with GFLU Map; allow for more intense development in close proximity to transit.
С	GC/E	CC-T	CC	Change to discourage residential along Beltline Highway.

- 3. Review Planned Development (PD) zoning covering development along and/or west of Gammon Road for a potential transition to conventional zoning to simplify resident applications to the City for minor projects and simplify staff administration.
- 4. Implement maximum building height recommendations shown on the Building Height Map, with a mechanism for allowing bonus stories for developments that achieve specific affordability or sustainability parameters, similar to downtown height bonuses in City ordinances (Madison General Ordinances 28.071(2)(a)2).
- 5. Create a new commercial and employment zoning district with limited allowable residential development for areas along the West Beltline Highway and similar areas elsewhere in the City.
- 6. Require ground floor commercial uses in the locations shown as Commercial Core on the GFLU Map.

Transportation



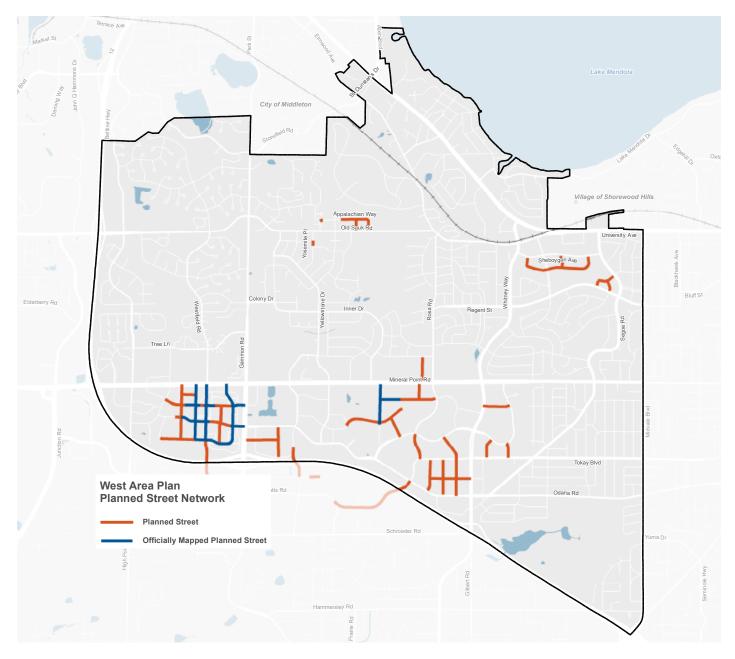
The West Area's transportation system is anchored by the West Beltline Highway, Old Sauk Road, Mineral Point Road, Gammon Road, University Avenue, Whitney Way, and Midvale Boulevard. Transit service is anchored by the City's first BRT route, which runs on Mineral Point Road, Whitney Way, Sheboygan Avenue, Segoe Road, and University Avenue. Local transit routes run along Old Sauk Road, Tokay Boulevard, and Odana Road, among other major streets. The bicycle network has significantly improved in recent years, with on-street facilities added to Whitney Way, Odana Road, and a portion of Tokay Boulevard to supplement existing routes on Segoe Road and Midvale Boulevard from the Beltline to Mineral Point Road. Shared-use paths are part of the bicycle network as well, though the path network has gaps. People report a lack of north-south bike connectivity in the West Area. The sidewalk network is generally robust, with some exceptions in neighborhoods like Spring Harbor, Glen Oak Hills, and Crestwood.

Street Network

The street network is made up of major roads with limited connections to neighborhoods and commercial areas. Many area neighborhoods have local street networks that are internally focused and feature winding, circuitous routes, or cul-de-sac and dead-end configurations. This concentrates traffic on fewer streets, and increases travel time for all modes of transportation. An exception is the Midvale Heights Neighborhood, which has a grid street system and multiple connections to major roads. Commercial and employment areas like West Towne Mall and University Research Park were designed around cars. That is reflected in a street network with large blocks, private drives, few through streets, and limited connections to major streets.

What We Heard

- 1. Better road, bicycle and pedestrian connectivity is needed.
- 2. With all transportation improvements, address the needs of people of all ages, backgrounds and abilities.
- Safer, more efficient and enjoyable pedestrian and bicycle transportation, especially for students travelling to and from area schools and after-school programs.
- 4. Safer and more direct northsouth pedestrian and bike connections that also feed into east-west connections.
- Appreciation for how walkable and bikeable neighborhoods are within the area, with access to transit and destinations (restaurants, shops, services).
- 6. Better walkability within neighborhoods.
- Safer, more direct path connections between neighborhoods and commuter paths that take people to employment and shopping areas within and outside the West Area.
- 8. No shared-use path in the Sauk Creek Greenway, while others would like a shared-use path there.
- Appreciation for the existing transit accessibility to the UW-Madison campus and downtown, while also desiring further transit service frequency and additional routes.
- 10. Safer and more comfortable bicycle facilities like protected lanes with barriers.



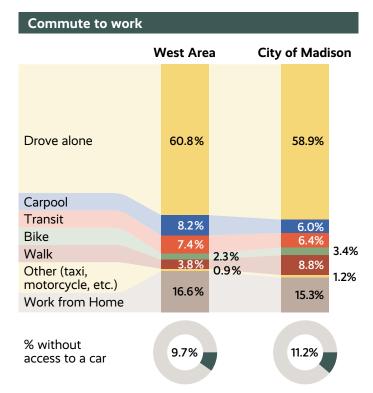
Changes to the street network are recommended to create walkable, pedestrian-oriented places, better connect neighborhoods, and distribute vehicular traffic over multiple routes. The largest concentration of planned streets is within the West Towne Mall area. These streets are needed to support that area's future growth and should be built as redevelopment occurs. Other street connections, such as Appalachian Way and Yosemite Place, would better link existing neighborhoods.

Transit

Transit service in the West Area is robust, with Metro Transit's east-west BRT route replacing Route A. BRT Route A provides 15-minute service starting at Junction Road, travelling along Mineral Point Road, Whitney Way, Sheboygan Avenue and University Avenue, through campus and downtown, and along East Washington Avenue to East Towne Mall (see the City's BRT Map). It serves high ridership areas, including West Towne Mall, surrounding commercial areas, Vel Phillips Memorial High School, Ezekial Gillespie Middle School, Sheboygan

Avenue apartments, and the Hilldale area. The BRT system includes dedicated bus lanes along much of its length, passenger stations, and large electric buses.

Metro Transit implemented its Transit Network Redesign in 2023 (see the Metro Transit Route Map). The new system reduces the number of routes and stops and increases bus frequency to improve reliability and efficiency. In addition to Rapid Route A, the West Area is served by routes D, E, F, J, H, R, 28, and 38. Mineral Point Road and some portions of Whitney Way and University Avenue have 15-minute bus service on weekdays and Saturday. Tokay Boulevard, portions of Whitney Way, Old Sauk Road, University Avenue, and Odana Road have 30-minute weekday/Saturday service. North Gammon Road and a portion of Old Sauk Road has 60-80-minute service. The Transit Network Redesign has led to mixed experiences for transit riders with some preferring it over the old system, while others find it difficult and sometimes unsafe to access stops.



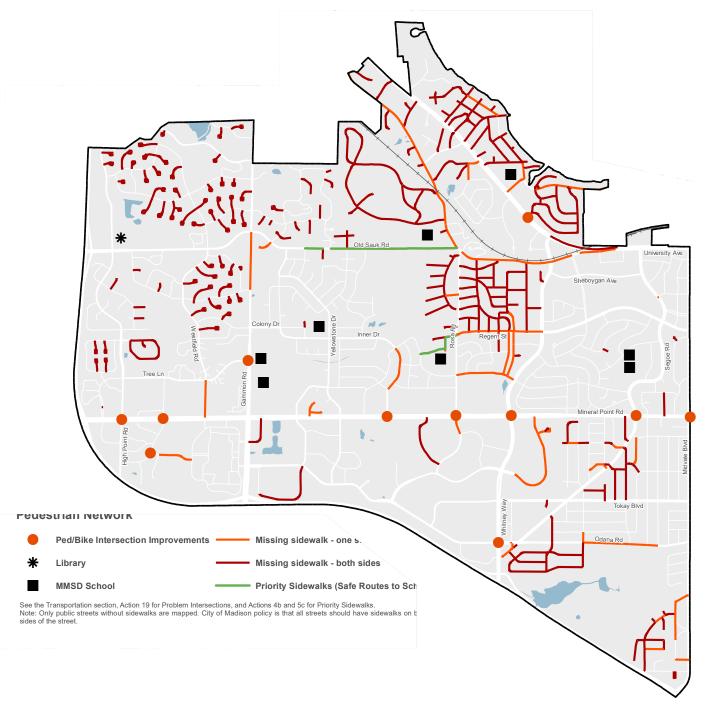
Bike and Pedestrian Network

About 75% of the West area's street network includes sidewalks on one or both sides of the street.

However, a number of neighborhoods were constructed without sidewalks, including Spring Harbor, Glen Oak Hills, Crestwood, Highlands, Wexford Village, Sauk Creek, and Walnut Grove. It is City policy to add sidewalks as streets are reconstructed, so over time these areas will become safer, more enjoyable places to walk. The City's Complete Green Streets Guide will guide reconstruction, including how the space within the right-of-way will be used for the street, terrace, sidewalk or other pedestrian/bicycle infrastructure, in context with the surroundings.

Some major streets in the West Area include bike lanes. While there are several shared-use paths in the West Area, such as the University Avenue Path, Blackhawk Path, Southwest Path, and West Towne Path, portions of the area lack connectivity for bikes, especially north to south. Further, bike lanes on heavily travelled streets and in multi-lane intersections feel unsafe for many bicyclists and the motorists that drive alongside them. There are also large greenspaces, like the Walnut Grove Greenway, Sauk Creek Greenway, and Owen Conservation Park, that limit connectivity as they span multiple blocks and do not include through-paths accessible to people of all ages and abilities.

The City established an initial "All Ages and Abilities Bike Network" map in the Complete Green Streets Guide. The Network identifies streets and paths that are intended to provide a complete network of low-stress bikeways between neighborhoods, key destinations in the city, and to adjacent municipalities. Some streets or paths may need to be built or improved to provide low-stress connections, but the map serves as the long-range plan for a full network. The initial All Ages and Abilities Bike Network map is planned to be updated in the coming years. Information in this Plan on recommended bicycle facilities will inform changes to the All Ages and Abilities map.



Other Transportation Initiatives

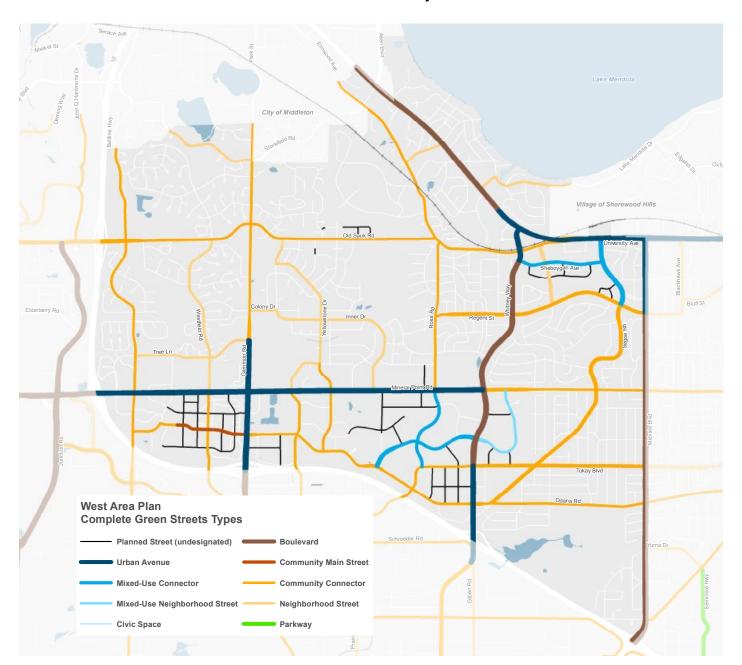
The High Injury Network Map shows street segments with elevated levels of serious injuries and fatalities. Areas where people tend to walk, bike and board transit are of particular concern because collisions between motor vehicles and people result in more serious injuries. This includes the street segments shown near Hilldale Shopping Center, Sheboygan Avenue, the commercial area along South Whitney Way between Tokay Boulevard and the West Beltline Highway, Gammon Road near Vel Phillips Memorial High School and Ezekiel Gillespie Middle School, and the Midvale Boulevard/Southwest Path intersection.

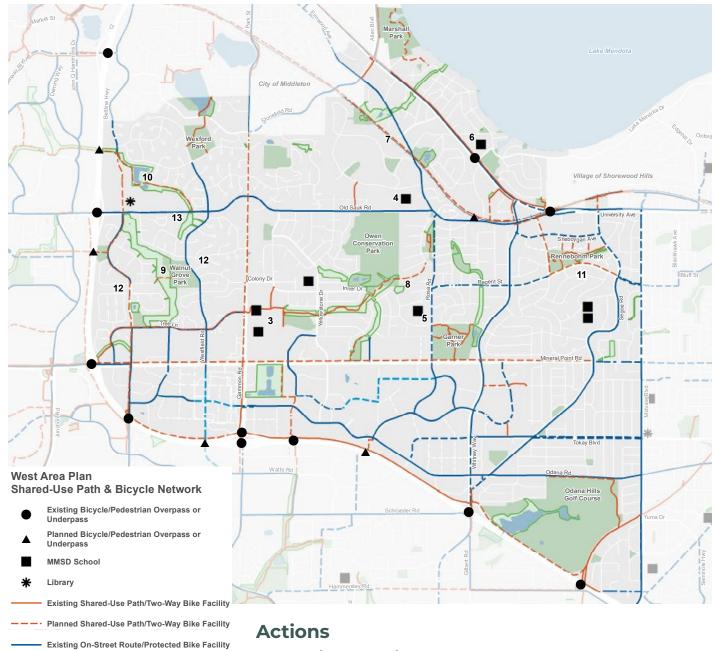
The City's <u>Vision Zero campaign</u> and <u>Safe Streets Madison program</u> focus on eliminating traffic deaths and serious injuries on city streets by improving public infrastructure. The West Area Plan's transportation actions complement

these initiatives. Vision Zero's main goal is eliminating traffic fatalities throughout the city by 2035. This includes redesigning high injury and fatality street segments to slow vehicle speeds and make intersections safer for people walking, biking, and driving. Safe Streets Madison emphasizes traffic safety measures such as speed humps, mini traffic circles, and pedestrian refuge islands. Further initiatives that will be complementary include the upcoming All Ages and Abilities and Pedestrian Network Plans. In all of these initiatives, there is an emphasis on improvements in areas where people with low incomes and people of color live since they face disproportionate impacts from the historic focus on automobile infrastructure.



Bike lanes and a pedestrian refuge island on Westfield Road at Colony Drive





Pedestrian and Bicycle Network

Planned On-Street Route/Protected Bike Facility

Planned On-Street Route/Protected Bike Facility

(planned roads)

Greenway

City of Madison Park

Note: See text the Transportation actions for numbered

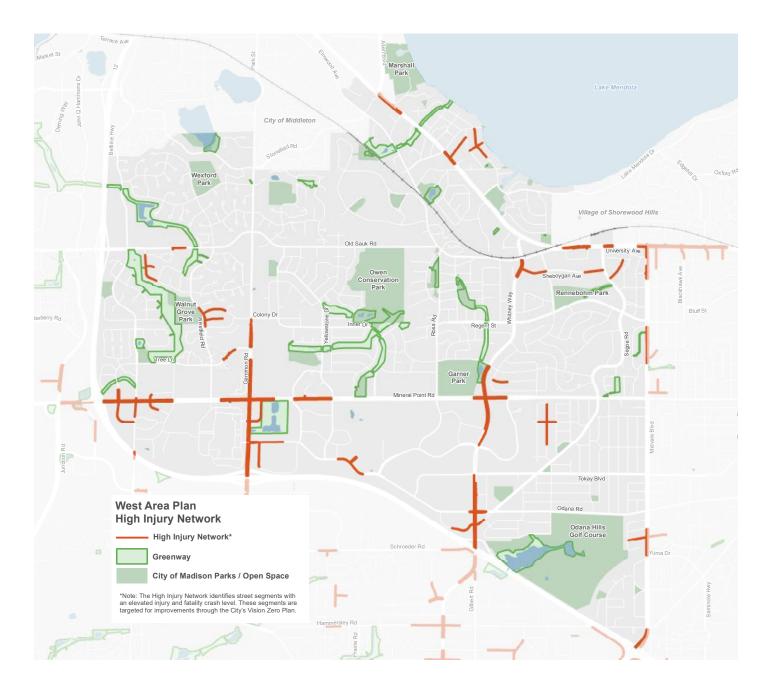
- Close gaps in the sidewalk network shown on the Sidewalk Network Map through the Safe Streets Madison Program, as streets are reconstructed, and as priority sidewalk additions can be constructed through state and federal grants (see priority sidewalk actions 4 b. and 5 c. below)
- 2. Implement the pedestrian and bicycle improvements shown on the Shared-Use Path and Bicycle Network Map. Further description is included below with the actions corresponding to the numbers on the map.
- 3. Vel Phillips Memorial High School and Ezekiel Gillespie Middle School
 - a. Study the addition of a left turn arrow from northbound N. Gammon Rd. into Tree Ln. to the traffic signal at the Tree-Gammon intersection.
 - b. Add a shared-use path along the east side of N. Gammon Rd. from Mineral Point Rd. to the City of Middleton. Starting at Tree Lane or Colony Drive, narrow Gammon Road to one lane of traffic in each direction with a center turn lane to reduce speeding, increase safety, and allow for construction of the shared-use path.

a center turn lane to reduce speeding, increase safety, and allow for construction of the shared-use path.

- c. Consider narrowing travel lanes and other design changes on N. Gammon Rd. between Mineral Point Rd. and Tree Ln. to calm traffic and enhance safe connections to schools.
- d. Coordinate with the Madison Metropolitan School District (MMSD) on an improved east-west shareduse path connection through the high school and middle school property.
- e. Work with the Wisconsin Department of Transportation (WisDOT) to clarify the lane markings and signage at the Beltline Hwy. ramps at Gammon Rd. to improve pedestrian and bicyclist safety and comfort.



Gammon Road at Sawmill Road and Ponwood Circle





Southwest Path at Midvale Boulevard

- f. When WisDOT pursues replacement of the Beltline Hwy. bridge at S. Gammon Rd., work with them to extend the wide sidewalk/shared-use path under the highway.
- g. Work with WisDOT to add lighting under the S. Gammon Rd. overpass.
- h. Improve pedestrian and bike access and safety at the Mineral Point Rd. intersections with S. High Point Rd., S. Gammon Rd., and Grand Canyon Dr.

4. Crestwood Elementary School

- a. Review MMSD's school travel plan and flow of people walking, biking and driving to the school to improve safety at the school.
- Evaluate options to add or enhance ped/bike facilities on Old Sauk Rd where they are missing, through street reconstruction and the Safe Streets Madison Program.

5. Stephens Elementary School

- a. In coordination with MMSD, mark bike lanes on N. Rosa Rd. while adding bump outs for school busses and installing more durable terrace paving for students getting on and off busses.
- b. Consider adding an alternate student drop-off/pick-up area at the back of the school on Cable Ave.
- c. Add a priority sidewalk on the remainder of the north side of Cable Ave., the west side of Beach St. and the north and south sides of Anchorage Ave. between Beach St. and Rosa Rd., along with crosswalks at the Cable Ave. and Beach St. intersection, as part of the Safe Streets Madison Program.

6. Spring Harbor Middle School

- Replace the pedestrian underpass beneath the railroad between Old Middleton Rd. and Craig Ave. and add lighting and a new pathway extending from Craig Ave.
- 7. If the rail corridor becomes inactive, work with WisDOT to study the feasibility of converting the corridor to a shared-use path that parallels University Ave., like the Southwest Path.
- 8. Construct a shared-use path from Island Dr. through the southeast corner of Owen Conservation Park to Bordner Dr. in coordination with the Crestwood Neighborhood Association.
- 9. During the Sauk Creek Greenway Corridor Plan, use detailed engineering data to consider the impacts and benefits of adding an All Ages and Abilities shared-use path to make an east-west connection across the greenway to Walnut Grove Park. Any All Ages and Abilities path should be designed using environmentally sensitive best practices and minimizing impacts to trees and other vegetation, wildlife habitat, and adjacent properties.
- 10. When the Wexford Pond Greenway comes up for reconstruction, use environmentally sensitive best practices to design a shared-use path through the greenway. Where possible, the path should follow the existing maintenance path to provide continued access for greenway management, and it should provide a safe, accessible, and enjoyable route for bicyclists, pedestrians, people in wheelchairs, and other users, while minimizing impacts to wildlife habitat, vegetation, trees and adjacent properties.

- 11. Convert existing five-foot wide sidewalks to 10' wide shared-use paths along Regent St. from N. Segoe Rd. to Eau Claire Ave. and along Eau Claire Ave. from Old Middleton Rd. to Regent St. while maintaining or widening existing terraces and minimizing tree impacts. This would facilitate bicycle connections without removing on-street parking that is needed to serve Rennebohm Park. Shared-use path implementation should occur in concert with additional dedication of parkland for Rennebohm Park.
- 12. Reduce on-street parking on High Point Rd. and Westfield Rd. between Old Sauk Rd. and Mineral Point Rd. to allow for construction of all ages and abilities bike facilities.
- 13. Extend the one lane in each direction configuration of Old Sauk Road from Westfield Road to High Point Road while adding a center turn lane and buffering the existing bicycle lanes.
- 14. Integrate pedestrian and bicycle infrastructure like benches, pedestrian-oriented lighting, and bike racks into terrace areas adjacent to high-traffic pedestrian and bicycle routes, especially in and around the "Commercial Core" areas shown on the GFLU Map.
- 15. Integrate facilities like bike racks, bike lockers and BCycle stations in street reconstruction and development projects along BRT corridors and at BRT stations.
- 16. Implement bicycle wayfinding.
- 17. Work with the City of Middleton to:
 - a. Extend a shared-use path from Camelot Drive through Marshall Park to connect with Middleton Beach Road. Note: the line shown on the Shared-Use Path & Bicycle Network map represents the connection but may not be the final route.
 - Ensure cross-border bicycle connectivity as both communities continue to improve bicycle facilities, including lanes on John Q. Hammons Dr., paths or lanes on N. High Point Rd., and a possible path along N. Gammon Rd.
- 18. Work with WisDOT to improve pedestrian and bicycle connectivity across the Beltline by:
 - Integrating new Beltline crossings shown on the Shared-Use Path and Bicycle Network Map into any future Beltline construction/reconstruction to enhance connectivity for pedestrians and bicyclists.
 - b. Improving conditions for transit, bicyclists, and pedestrians at Beltline interchanges.
 - c. Improving conditions of the Beltline Hwy. underpass at Struck St. and making needed improvements for safety and comfort of pedestrians and bicyclists such as upgrades to pavement, drainage, and lighting.
- Improve pedestrian and bicyclist safety at the major intersections identified on the Pedestrian Network map.



Old Sauk Road at Farmington Way



The Struck Street underpass of the Beltline is an important connection for pedestrians and bicyclists, but is in need of upgrades.



Existing Conditions

20. Improve pedestrian safety along Segoe Rd. by by removing Laub Ln. from Berwyn Dr. to S. Segoe Rd. (see diagram below) and by reducing pedestrian exposure and crossing distance with bump-outs, pedestrian refuge islands, and continental crosswalk markings, especially where Segoe Rd. crosses Wedgewood-Manor Cross, Tokay Blvd., and Tocora-Hilltop.



Proposed Intersection Improvements

Road Connectivity and Traffic Circulation

- 21. Update the Complete Green Streets Guide to reflect the Complete Green Streets Types Map in this Plan.
- 22. Preserve the opportunity to connect Middleton St. in the future.
- 23. Improve road connectivity with the following street segment changes (also see the Planned Streets Map):
 - a. Study the transformation of St. Dunstan's Dr. to a two-way street from Allen Blvd. to Old Middleton Rd. if the St. Dunstan's Episcopal Church site is proposed for redevelopment.
 - b. Connect Yosemite Place to Yosemite Trail to improve connectivity south of Old Sauk Rd.
 - c. Contingent on redevelopment proposed in the area, extend Appalachian Way to the east, then south to connect to Old Sauk Rd. Add a new north-south street alongside the Cooper Lane Bike Path to connect with Appalachian Way extended.
 - d. If properties north of Rennebohm Park redevelop, reserve property to create a narrow, pedestrian- and bicyclist friendly street to the north of the park to break up a large block, provide the framework to orient buildings toward the public park, and create alternate access to the properties fronting Sheboygan Ave. The street should have the minimum allowable speed limit and be designed to minimize non-local through traffic.



Partnerships

A. Work with WisDOT to:

- i. Add new Beltline crossings shown on the Planned Streets Map and the Shared-Use Path to enhance connectivity for cars, pedestrians, bicyclists, and transit, shorten trips, and provide alternative north-south routes to Beltline interchanges at S. Gammon Rd. and S. Whitney Way.
- ii. Contribute Beltline planning to ensure that City interests are represented as part of WisDOT processes.
- B. Explore public-private partnerships with area commercial property owners to create a Park and Ride system that makes better use of underutilized parking lots on the weekdays.
- C. Work with the Madison Metropolitan School District to increase lighting along the north-south shared-use path that runs along the east side of the Ezekial Gillespie Middle School property and connects to Inner Dr.
- D. Work with the MMSD and school principals to address pedestrian, bicycle, and traffic safety issues around schools when street repaving, reconstruction, or other changes to the right-of-way are being considered. Strategies may include signage, increasing crosswalk visibility, modifying traffic patterns, increasing nearby sidewalk connectivity, and implementing safe All Ages and Abilities bicycle connections.

Planned Street (conceptual location; if properties redevelop)

Potential Park Expansion (conceptual location; if properties redevelop)

Planned Shared-Use Path (conceptual location; if properties redevelop)

Neighborhoods and Housing

Comprehensive Plan Goals

This Plan intends to advance the following goals from the Comprehensive Plan:

- Madison will be a safe and welcoming city of strong and complete neighborhoods that meet the needs of all residents.
- Madison will have a full range of quality and affordable housing opportunities throughout the city.

This Plan seeks to strengthen neighborhoods in the West Area and provide opportunities for new housing as the Area continues to evolve. Madison's strong job market and high quality of life has led to consistent population growth. However, a limited housing supply leads to limited housing options and high housing costs. With Madison anticipated to add about 115,000 new residents between 2020 and 2050 - or about 3,800 new residents per year - housing supply issues may continue despite ongoing efforts to improve housing availability.

The Comprehensive Plan directs a majority of future growth to occur as infill and redevelopment. Madison is gradually losing the ability to expand outwards and extending City services for development at the edge of the city is more costly. For these and other reasons, the City's ability to accommodate growth through redevelopment is increasingly important. The West Area provides an opportunity to build a significant amount of new housing in close proximity to jobs, shopping, services and transit. There is also an opportunity to diversify the West Area's housing stock, which includes an abundance of areas comprised solely or almost entirely of single-family homes. Additionally, large parts of the West Area are low-density commercial land uses. Much of the redevelop-

> ment in the West Area will occur on these auto-oriented commercial sites and underutilized parking areas, which does not displace current residents.

As current residents age, young residents move to Madison in large numbers (an increase of approximately 14,000 more 20- to 40-year-olds in the past ten years), and racial and ethnic diversity increases, it is important to have housing options to accommodate all stages of life and living arrangements. While household size has been declining for decades in both Madison and Wisconsin and the number of families with children has remained flat compared to overall household growth, a mix of 2-, 3-, and 4-bedroom units for families should be thoughtfully included, especially near schools and at affordable prices.

% of housing by construction date

	West Area	City of Madison
Since 2000	11.5%	25.2%
1980-1999	29.8%	22.3%
1960–1979	39.8%	24.8%
1940-1959	15.3%	13.8%
Before 1940	3.6%	13.9%



The Monticello Apartments on Sheboygan Avenue

Affordability

The City's most effective way to increase the supply of new income- and rent-restricted affordable rental housing is through the financial assistance programs offered by the Community and Economic Development Divisions (CDD and EDD). The Affordable Housing Fund-Tax Credit (AHF-TC) and Tax Increment Financing (TIF) Programs are

the City's primary tools for impacting affordable rental housing development. The AHF-TC is especially impactful when used in cooperation with the Wisconsin Housing and Economic Development Authority's Low-Income Housing Tax Credits. CDD also provides financial support to smaller scale, non-tax credit affordable rental developments like housing cooperatives. New lower cost or

subsidized housing should be in areas that already have a high level of resources and amenities, including transit. Areas along University Avenue, Sheboygan Avenue, Whitney Way, and Mineral Point Road are particularly well-positioned for this type of development. However, it is often more cost-efficient to preserve existing naturally affordable older housing stock than to build new low-cost housing. While naturally-occurring affordability necessarily requires a "hands-off" stance from the City, maintenance is obviously required and it may be lost altogether as prices continue to increase if additional housing is not built to meet demand. State statutes and court decisions do not allow the City to enact rent control or require new developments include affordable housing.

Mix of Housing Types

In addition to larger multifamily buildings constructed in activity centers and corridors identified in the Comprehensive Plan's Growth Priority Areas Map, lower-impact residential development can occur in existing lower-density residential areas through the addition of smaller buildings often referred to as the "Missing Middle." These building types are designed to fit with predominantly single-family residential areas. On the recommended GFLU Map Low-Medium Residential areas are primarily intended to accommodate Missing Middle. Missing Middle housing types can also be added to Medium Residential areas and in Low Residential areas located near activity centers or along transit corridors.

In addition to the different physical characteristics of housing, such as size of units and number of units in a building, it is also important for a variety of tenancy options be available. The most common tenancy arrangements are fee-simple ownership (often for single-family residences) and rental apartments. Co-housing, condominiums, housing cooperatives, and community land trusts can, however, provide further housing choice to residents.

Complete Neighborhoods

Awareness and access to existing community programs and services, especially for youth and seniors, is important for neighborhood health. Several community facilities provide services along the southern portion of the area such as Madison School and Community Recreation (MSCR) - Odana, Madinah Community Center, and Huckleberry and Persimmon. Except for the Alicia Ashman Library and Lussier Community Education Center, there are large geographic areas without support in the rest of the West Area. Residents of the area desire more community facilities where people can obtain programing and support including childcare, youth activities, job training, senior care, managing living expenses, and other family needs.

Schools are also catalysts for establishing strong neighborhoods, as children's activities, daily social interactions, and community projects create connections. The West Area has a strong network of schools, with four public elementary schools (and portions of six attendance areas), two middle schools, and one high school in the Madison Metropolitan School District.

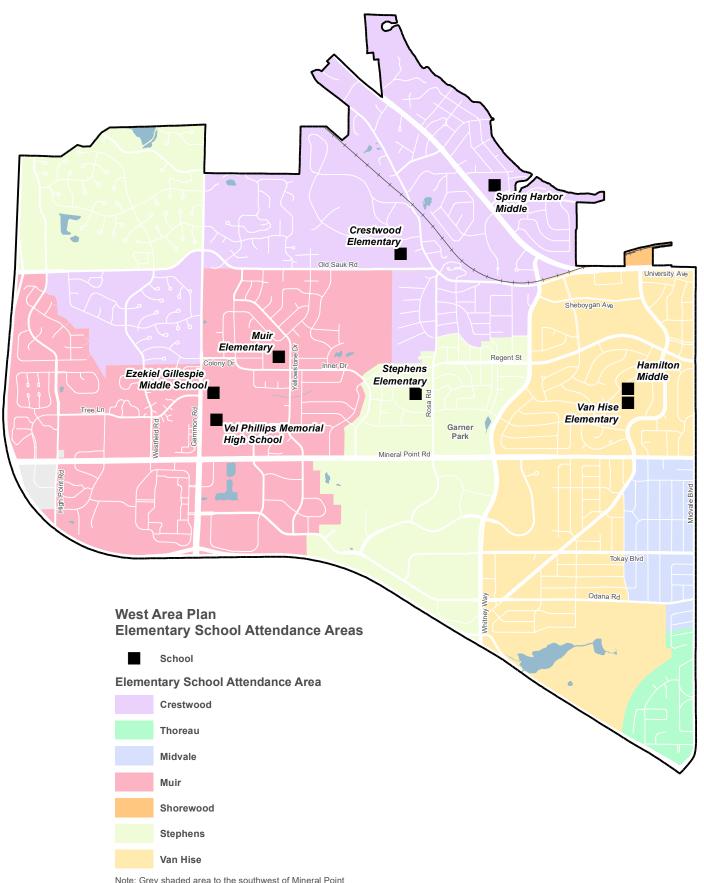
Finally, access to healthy food is another important consideration in healthy neighborhoods. Four full-service grocery stores are located within the West Area, with several more just outside the planning boundary. There are also several smaller convenience stores and ethnic grocers in and around the Area. Seasonal farmers markets occur at University Research Park and Hilldale Mall.

Missing Middle housing is a range of multi-unit or clustered housing types scaled between single-family detached houses and larger apartment buildings, such as rowhouses, small multifamily buildings, tiny homes, bungalow courts, courtyard apartment buildings, accessory dwelling units, and live-work buildings.

Complete neighborhoods are neighborhoods that include a range of housing types and costs, neighborhood-serving businesses, stores, and services; schools, and places of worship. Neighborhood components are ideally accessible by foot, bicycle, or transit through a network of well-connected streets and blocks, usable public spaces, and a system of connected parks, paths, and greenways.

Affordable housing is housing for which the occupants are paying no more than 30 percent of gross household income for housing costs, including utilities. Households with housing costs exceeding 30% of income are considered housing cost-burdened. The City of Madison's direct and development subsidy programs are primarily designed to reduce housing cost burdens for renter households with incomes at or below 60% county median income (CMI) and for owner-occupied households at or below 80% CMI.

The City of Madison Community
Development Division's Housing
Development & Financing Program provides loans to for-profit and non-profit housing developers for the construction and rehabilitation of new income and rent-restricted affordable rental and owner-occupied housing.



Note: Grey shaded area to the southwest of Mineral Point Road and High Point Road is in the Middleton-Cross Plains Area School District.

Actions

- Work with developers to encourage a wider mix of apartment unit sizes, including smaller units for younger and older households, as well as units large enough to support larger families (three or more bedrooms) in proximity to schools.
- Encourage and incentivize development of smaller-scale "Missing Middle" housing types in Low-Medium Residential (LMR) areas on the GFLU Map, especially in areas near schools, parks, and transit service, through zoning ordinance changes.
- For new housing partially funded by the City (through land banking, the Affordable Housing Fund, TIF) work with partners to distribute housing at costs affordable to a variety of households throughout the area, with a focus on areas along the BRT corridor.
- Development along the Beltline Highway should include a variety of noise mitigation improvements. Limit or entirely restrict City financial support for affordable housing in these locations, unless significant noise mitigation measures are included.
- 5. Increase access to information regarding First Time Home Buyer Assistance programs for rental and utility payment support, the City's property tax assistance program for seniors, and the City's low-cost mortgage and rehabilitation loans, and historic preservation tax credit programs for existing residents and those looking to move to the West Area.
- If the large residential parcels on Sheboygan Avenue or bordering Rennebohm Park redevelop, new residential development should connect to the park and surrounding amenities and include community spaces serving residents.

Partnerships

A. Increase awareness and access to existing community programs and services, especially for youth and seniors, and explore expanding them in such locations as the community rooms of new housing developments, Lussier Community Education Center, and through co-location of community-based organizations.

What We Heard

- The need for housing from public feedback and recommendations from underlying plans generally fell into the following five categories of desired housing types:
 - a. Affordable housing, including affordable homeownership opportunities.
 - A variety of tenancy structures in addition to just fee-simple ownership and rental.
 - A wider mix of unit sizes, including those large enough to support families with children.
 - d. Housing for all stages of life.
 - e. Smaller-scale multi-family buildings, including building types commonly referred to as the Missing Middle.
- The desire for more/expanded community facilities such as community centers, libraries, and daycare opportunities.
- 3. Appreciation of quiet neighborhoods/streets that are still close to shops and restaurants.

Economy and Opportunity

Comprehensive Plan Goals

This Plan aims to advance the following goals from the Comprehensive Plan:

- Madison will have a growing, diversified economy that offers opportunity for businesses and residents to prosper.
- Madison will have equitable education and advancement opportunities that meet the needs of each resident.

The West Area plays an outsized employment role within the city and region. Of the Area's approximately 30,000 residents, approximately 15,000 are in the workforce. The Area hosted approximately 23,600 jobs in 2020, up by more than 3,000 over the previous decade (Census On The Map 2021). Job sectors where the Area has a higher proportion than the City as a whole include retail trade, finance and insurance, and professional/scientific/technical services. The area has fewer than its fair share of jobs in the educational services sector. Employees working in the West Area generally reflect citywide ratios as far as educational attainment, race, and ethnicity.

Major employment nodes include the approximately 4,200 jobs within University Research Park, which is a hub for biotechnology companies in Wisconsin. Many Research Park jobs are high-paying. The West Area includes two of the region's most successful retail areas: West Towne Mall and Hilldale Mall, both of which attract shoppers from around southcentral Wisconsin. There are numerous health care clinics, along with a concentration of dentistry practices at Old Sauk and Gammon Roads. Financial services, led by TruStage (formerly CUNA) along Mineral Point Road, are a significant employer as well. The State



TruStage recently invested in a new training building along Mineral Point Road, but the company's overall employment in the area is decreasing, resulting in demolition of one of their office buildings.

of Wisconsin continues to consolidate multiple state agencies into the new Hill Farms State Office Building, including WisDOT's headquarters. Additionally, the State of Wisconsin Investment Board has moved from downtown to a new office building in the Hilldale area.

Office Development

The COVID-19 pandemic significantly increased remote work, generally reducing office occupancy. This shift has heavily affected older office

space. Older ("Class C") office space in the Yellowstone-Grand Canyon Drive area is receiving increased interest from developers looking to bring multifamily housing and mixed-use development to an area that, until recently, has not had any residences west of Yellowstone Drive. Other areas with small, relatively old office buildings, like Midvale Boulevard south of Hilldale and the Odana Road-Medical Circle area, are also seeing redevelopment interest.

Pre- and post-COVID 19, the West Area has experienced a relatively strong market for new office space, led by the Hill Farms State Office Building, along with new lab space constructed by University Research Park and Exact Sciences' new headquarters in University Research Park. Navitus Health Solutions also anchors a large office building at the corner of Gammon and Mineral Point Roads that was completed in 2018.



The State of Wisconsin Investment Board moved from its downtown office to a new location within the Madison Yards development close to Hilldale Mall.

While TruStage has been a long-time employment anchor in the area and completed a major new training center in 2022, it has been reducing its workforce. It has demolished a major office building at the corner of Rosa and Mineral Point Roads.

Service and Retail Sector

The changing retail landscape, which was accelerated by COVID, is impacting the West Towne area more significantly than the Hilldale area, with small strip malls along the Odana Road corridor attracting developer interest for multifamily residential and mixed-use projects. A long-term trend of big box retail being adaptively reused for entertainment, fitness, and services started pre-COVID with Dave and Busters taking over a portion of a former Sears store and Urban Air Adventure Park taking over a



A former drive-through bank on Odana Road was redeveloped into the Urbana mixed-use project.

Jobs within area, by major sector

	West Area		of Madi	ison
Agriculture, Mining, Utilities, Construction (NAICS #11-23)	3.7%	0.6% 1.3%	3.8%	
Manufacturing, Transportation,			6.3%	
Warehousing (31-33, 48-49) Wholesale Trade (42)	16.0%		2.3%	
Retail Trade (44-45)			8.4%	
Information (51)		2.0%	2.7%	
Finance, Insurance, Real Estate (52-53)	13.3%		7.5%	
			8.7%	
Professional, Scientific, and Technical Services (54)	14.5%		6.5%	
Management of Companies, Administration & Support (55-56)	4.1%		14.6%	
Educational Services (61)	4.2%			
Health Care and Social Assistance (62)	19.3%		19.3%	
Accommodation and Food Services (72)	7.6%		5.4%	
Arts, Entertainment, Other Services (71, 81)	5.2%		4.6%	
Public Administration (92)	8.2%		9.9%	

% with 4+ years of college

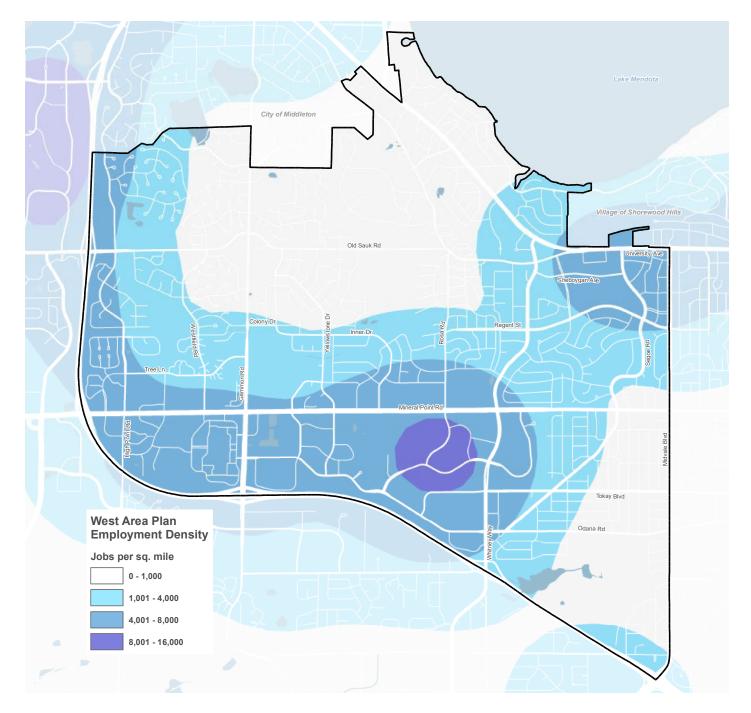
63.4%

54.9%

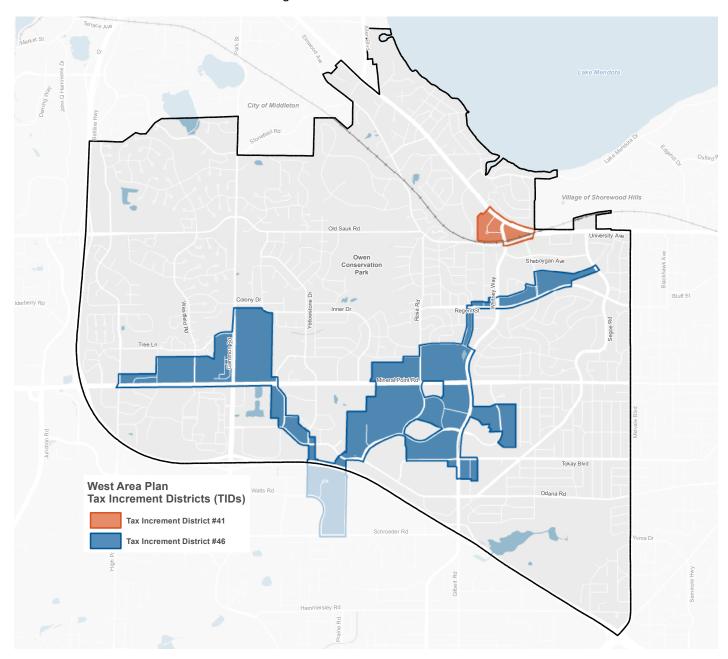


Two small office buildings along Midvale Boulevard were demolished to construct the Manchester on Midvale apartment building.

former Toys R Us store. The trend is continuing in the West Towne area, with Crunch Fitness moving into a former Shopko and MSCR filling a high-turnover space along the Beltline. T.J. Maxx moving across Whitney Way opened the former Westgate Mall property for redevelopment with almost 500 units of multifamily housing. Similar consolidation elsewhere in the West Area can both help fill vacant retail space and open land to address the city's ongoing need for housing. While these changes all illustrate the evolving retail landscape, they also demonstrate the overall adaptability, health, and desirability of the West Area, as many vacant or underutilized spaces are adaptively reused or redeveloped relatively quickly.



The restaurant sector is generally strong within the West Area, with a concentration of franchise locations in and around West Towne Mall. Local restaurants are more common to the east, along Odana Road and Whitney Way. Hilldale Mall hosts a mixture of local restaurants and higher-end national chains.



Actions

- Analyse land banking within the concentration of commercial development south of Mineral Point Road as a potential method for reusing existing buildings for affordable local business space (potentially as part of an incubator) and for redeveloping surplus surface parking into affordable housing.
- 2. Monitor the performance of TIDs #41 and #46 to determine whether amendments are financially feasible to fund affordable housing, non-assessable infrastructure (bicycle network improvements, pedestrian safety improvements, new streets, stormwater management improvements, etc.),

What We Heard

- Maintain some affordable space for small businesses as it changes and grows through increased redevelopment.
- Add to business diversity in areas currently lacking in services and restaurants through implementing additional mixed-use development.
- Finding staff can be a challenge in some sectors, with the lack of affordable housing a contributing factor.
- 4. The area has easy access to downtown and is convenient to shopping, dining, and many employment areas.
- 5. An appreciation of the current small businesses in the area and a desire for more local businesses, especially within walking distance of residents.

- creation of a business incubator, land banking, small business assistance, and other economic development initiatives in and around these TIDs.
- 3. As part of TID #41 and #46 evaluation, determine whether a commercial rent assistance or guarantee program to assist small and emerging businesses is financially feasible, particularly to allow BIPOC-owned businesses afford space in areas where rent increases may occur. Gap assistance could be used to help businesses displaced by redevelopment afford ground-floor space in new mixed-use buildings.
- 4. Form a staff team to review the possibility of a new TID in the West Towne area/Mineral Point Road corridor to support redevelopment, stormwater management improvements, and other non-assessable infrastructure that is consistent with City plans.
- 5. Increase affordable childcare options, taking advantage of existing commercial spaces where non-profit childcare providers could locate, ideally along transit corridors and close to employment areas.

Partnerships

- A. Explore the development of a business incubator, with lower cost space, shared resources and expertise needed to help grow local businesses.
 - i. Partner with business, economic development, and community organizations, such as Latino, Black, Hmong, and Madison Chambers of Commerce, Wisconsin Women Business Initiative Corporation, and established incubators, to ensure an incubator meets the needs of BI-POC-owned businesses and fosters opportunities for long-term success.
 - Consider integrating an incubator with land banking, using renovated commercial space to grow local businesses and retain affordable options.
 - iii. Incorporate networking, mentorship, and collaboration opportunities as part of business incubator planning.
 - iv. Integrate youth mentorship/training into incubator space, potentially in partnership with nearby Vel Phillips Memorial High School.
 - v. Explore creating a program similar to the Public Market's "Market Ready" as part of incubator planning to help new businesses and small businesses prepare for incubator occupancy.
 - vi. Work with local commercial brokers and property owners to connect small businesses to available space as they "graduate" from an incubator.
- B. As business areas transition to a more connected, pedestrian-friendly environment, assist the creation of a neighborhood business association or BID (business improvement district) to maintain amenities and facilitate events.
- C. Continue building relationships with and supporting economic development partners, especially those with ties to historically underrepresented communities. For example, the Madison Black Chamber of Commerce, Latino Chamber of Commerce of Dane County, and the Hmong Wisconsin Chamber of Commerce have vibrant memberships, and the City should collaborate with them, and other similar business and economic development groups, to help them grow opportunities for entrepreneurs to locate in the West Area.

Culture and Character

The Comprehensive Plan encourages vibrant and inviting places, preserving **Comprehensive Plan** historic and special places, creating safe and affirming community spaces, pub-Goals lic art, and providing opportunities to learn, create, collaborate, and enjoy the arts. This Plan seeks to lend further detail to those goals and strategies, realiz-This Plan aims to advance the ing that neighborhoods across Madison vary not only by size, but also through following goals from the Compretheir culture and character. The character of the area can be found in the activhensive Plan: ities at parks, community gardens, architecture of the built environment, pools, · Madison will be a vibrant and the work of community-based organizations, and other aspects of what makes creative city that values and a neighborhood a neighborhood. Building on and maintaining a sense of place builds upon its cultural and reflective of businesses, history, and connection between residents and their historic assets. community creates strong and resilient communities. This Plan looks to balance Madison will have a unique growth and change with historic and cultural character and strong sense of resources to preserve the Area's history place in its neighborhoods and while making it welcoming for current the city as a whole. and new residents. Sheboygan Ave Rennebohm Park **West Area Plan Community Gathering Places** Library/Community Center Place of Worship Recreation/Entertainment

The cultural assets of the Area significantly contribute to neighborhoods – these assets can come in the form of religious institutions, neighborhood institutions, schools, businesses, and other gathering places, both public and private. Preschool of the Arts, Madison Ballet, and the Camera Company are just some examples of the cultural assets in the planning area. Parks oftentimes host cultural events as well. For example, the Capitol City Band holds an annual summer concert series in the Rennebohm Park shelter that brings together residents of different cultures and ages, and Garner Park hosts the annual Opera in the Park event. More community events could be undertaken by establishing Neighborhood Improvement Districts to fund physical and placemaking activities within the area.



The vacant former Copps grocery store is in Urban Design District 3.



Madison Yards is within Urban Design District 6.

Urban Design Districts

Urban design is also an important contributor to culture and character. The city's Urban Design Districts contribute to the aesthetics of the built environment in addition some aspects of urban design regulated in the zoning code. The city's Urban Design Districts generally ensure development contributes to creating a high quality, well-designed built environment within certain areas of the city. There are eight Urban Design Districts that are primarily located along the city's major transportation corridors. Each Urban Design District (UDD) is unique

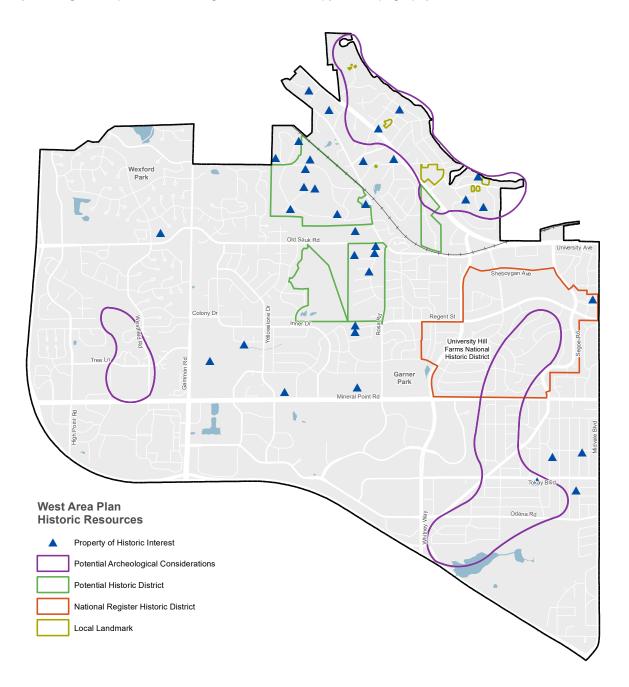
and establishes requirements and guidelines for new development, buildings, remodeling, or additions to existing buildings. Some districts are more prescriptive than others. While the requirements and guidelines are different in each district, generally they address building design (height, setbacks, and stepbacks), design and quality of exterior materials and architectural detailing, lighting, signage, the design of private open space, landscaping, and screening.

There are two UDDs in the West Area: UDD 3 and UDD 6. UDD 3 is southwest of the Tokay Boulevard and Whitney Way intersection and is characterized by commercial and employment development. UDD 6 is located along University Avenue from Middleton to University Bay Drive and is characterized by a wide variety of older strip commercial and newer mixed-use development, with some single-family residential included in the boundary in the western half of the district. This plan outlines actions related to the review/re-evaluation of UDD 3 and UDD 6 boundaries and the guidelines and requirements enumerated in the districts. While outlined in this plan UDD-related actions are intended to be part of a separate, citywide UDD review process.

Historic Resources

The West Area encompasses many sites and resources rich in history and culture. Residents expressed appreciation of these sites, the history of the West Area, scenic views from local vantage points, and the desire for more public art and art opportunities. There are several designated City of Madison landmarks or properties listed in the National Register of Historic Places as shown on the Historic Resources Map. Additionally, this Plan lists properties of potential architectural or historic significance and outlines the process if a property owner wishes to pursue designation (see the Historic Resources Review in the Appendix for further details). There are also archaeological sites in the West

Area, several of which are human burial sites. These sites may require additional review and consultation with the Ho-Chunk Nation and Office of the State Archaeologist. The archaeological sites could also be eligible for local or Federal historic designation. Properties listed in the National Register are potentially eligible for preservation tax credits for work on buildings, and burial sites are eligible for a property tax exemption. As part of this planning process, staff conducted a viewshed analysis of vantage points of the Capitol from publicly accessible locations within the Area. However, views from the Area were limited by existing development, street alignments, tree canopy, and topography.



What We Heard

- 1. Preserve the history and character of neighborhoods while looking to the future.
- Residents like and want to see more cultural diversity within the community.
- 3. More inclusive community events, spaces, and resources.
- 4. Expand arts infrastructure, opportunities, and spaces on the westside.
- 5. Integrate more public art throughout the area.
- 6. Preserve viewsheds of greenspaces, lakes, and the Capitol.

Actions

- Review UDD 3 (MGO section 33.24(10)) to determine whether it should be revised or repealed. Many requirements for UDD 3 development are adequately addressed by the current zoning code, which was adopted approximately 25 years after UDD 3. Other ordinance language governing UDD 3, such as the Statement of Purpose that says the requirements were established, in part, to "assure that future development will complement the existing development in the district," contradict the actions in this Plan, which expressly seeks to transition the area away from its current auto-oriented form.
- 2. Changes to UDD 6 (MGO section 33.02(13)) should be analyzed as part of a larger citywide UDD review to:
 - a. Revise the boundaries to not include single-family development within the district boundary and add existing multifamily development to the south of Sheboygan Avenue and existing commercial development between Hilldale Mall, Midvale Blvd., Regent St., and Sheboygan Ave. to the district boundary.
 - b. Align regulations with the TOD overlay zoning district.
 - c. Explore whether UDD 6 should be split into subareas to account for the widely varied characteristics and surroundings along the three-plus mile long corridor.
 - d. Consider establishing minimum and maximum building heights in some or all of UDD 6 (note there is already a two-story minimum height in the portions of the UDD that are within the TOD overlay zoning district).
 - e. Integrate design standards into the "Building Design" requirements to ensure buildings fronting shared-use paths address the path with architectural elements and building access at the same design level as the front of the building.
 - f. Add additional building and site design elements to the "Building Design" requirements to require ground floor residential units facing public right-of-way have individual unit entrances from the sidewalk.
 - g. Add an element to the "Building Design" guidelines to provide direct pedestrian connection(s) from buildings to adjacent public parks and/or shared-use paths.
- Integrate public art into upcoming public construction projects, including Sidewalk Poetry, Utility Box Art Wraps, and public art at BRT stations, along gateway corridors, and along bike paths. For large-scale construction projects, explore hosting workshops with the community to identify locations for public art.
- Implement the "Art Through City Building" concepts as described in the <u>City's Public Art Framework</u>, <u>Cultural Plan</u>, and other adopted policies to make the West Area more welcoming to people of every race, ethnicity, gender, age, and ability.

Partnerships

- A. Work with the property owners and managers of the apartments on Sheboygan Avenue to identify opportunities to partner on placemaking opportunities to enliven the area and engage the community.
- B. Work with neighborhoods eligible for creation of a local historic district and/ or a national register historic district to establish district(s). National Register or local historic district nominations should be initiated by the neighborhood, with technical assistance provided by the City. Note that National

Register districts allow residents to apply for tax credits, whereas local historic districts establish design requirements that shape future reinvestment and development in these neighborhoods (see the Historic Resources Review in the Appendix for more information).

- C. The property at 6405 Mineral Point Road hosts the historic Otto & Louisa Toepfer Farm house, which is now part of Otto's Restaurant & Bar. Should changes or development be proposed for this site, the property owner is encouraged to adaptively reuse the historic building.
- D. Encourage the creation of neighborhood-oriented events in underserved areas, such as block parties, food cart nights, or summer concert series, to bring residents together more frequently. Such events should focus promotion on underserved communities.
- E. Work with area property owners and nonprofits to add space for artist(s)-in-residence, affordable artist studios, exhibitions, performances, and rehearsals.
- F. Work with University Research Park and UW-Madison Arts staff to explore possibilities for locating arts space within or close to the Research Park. Space could take the form of studios, partnerships with businesses, an artist residency, or other formats.

Design Guidelines

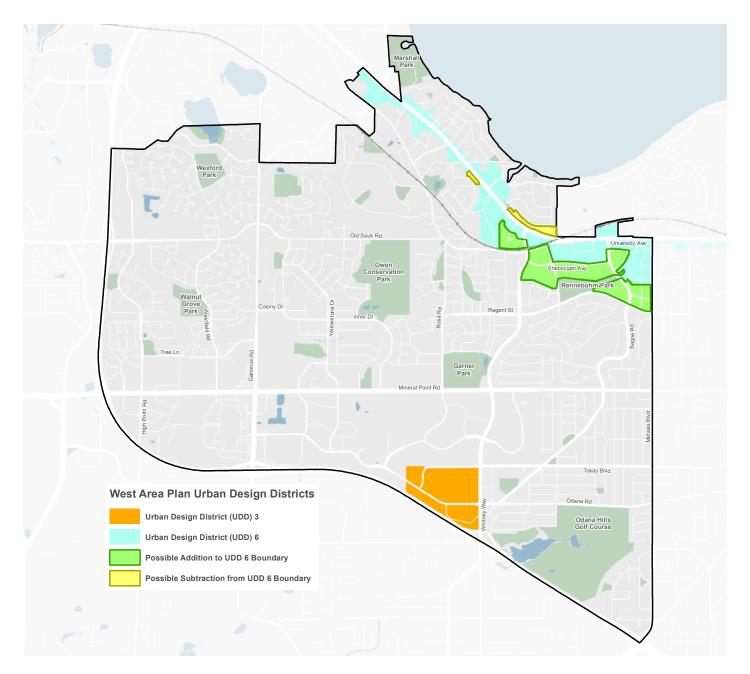
The West Area contains several prominent retail and employment destinations, like the West Towne Mall area, Hilldale area, and University Research Park, along with major multifamily developments, like Sheboygan Avenue. Throughout the Area redevelopment and development within mixed-use, employment, medium residential, and high residential land uses on the GFLU Map should reference the following design guidelines to create an engaging public realm with development that is more transit, pedestrian, and bicycle friendly than the traditional suburban style of development prevalent along some corridors within the Area.

Building Height, Stepbacks, and Setbacks

- 1. Buildings fronting "Commercial Core" areas, as shown on the GFLU Map, should be set back the minimum distance required by zoning to create a consistent, engaging, and walkable street frontage.
- Buildings not fronting Commercial Core areas should generally be minimally set back to maintain walkability and create easy pedestrian connections to the street and nearby transit. Substantial variations in setbacks between adjoining parcels should be avoided.
- 3. Buildings over five floors set back less than 10 feet should step back a minimum of 15 feet from the minimum setback line above the fifth floor along rights-of-way that are under 110' wide (for reference, Mineral Point Road and Whitney Way are over 110' wide, while Odana Road and Old Sauk Road are less than 110' wide).

Building Materials and Articulation

- 4. Material changes should not be made within the same plane without a programmatic change or a minimum notable relief.
- Buildings should be designed so that uses, entrances, and design elements provide amenities to enhance the visual and pedestrian character of the street.
- 6. Blank building walls that lack architectural detail, variety, and windows along street-facing facades should be avoided.



- 7. Modulation to create porches, balconies, roof terraces, entrances, and other active uses are encouraged for residential units/buildings.
- 8. Porches and balconies should be well integrated within the design and provide usable open space for residential tenants.

Site Layout & Building Orientation

- 9. New buildings should be oriented on sites to maximize pedestrian, transit, and bicyclist use while shielding parking from the public realm.
- 10. All ground floor commercial uses shall have usable entrances facing a public street that are open to the public during businesses' regular hours.
- 11. Where possible, parking should be placed under buildings (below grade), in parking structures, or behind buildings. Parking should be shielded from streets by landscaping, walls, or fences, and should not be placed between a building and the street. Above-ground structured parking should be shielded from public streets by liner buildings.

- 12. All ground floor residential units facing a street shall have their own street entrance with a front porch/stoop.
- 13. Building entrances should be designed as the focal points of the front facade, and should utilize overhangs, porches, stoops or other elements to add a pedestrian rhythm to the street façade.
- 14. "Commercial Core" areas on the GFLU Map should have active ground floor commercial uses (retail, restaurant, service, or office uses) and are encouraged to have outdoor patios, dining, awnings, and display windows.
- 15. Where possible, locate the tallest mass of a building to minimize shading of the sidewalk/street (However, other guidelines should not be compromised to meet this element).

Green and Resilient

Comprehensive Plan Goals

This Plan aims to advance the following goals from the Comprehensive Plan:

- Madison will be a leader in stewardship of our land, air, and water resources.
- Madison will have a model park and open space system that preserves our significant natural features and offers spaces for recreation and bringing residents together.

The West Area's variety of parks and greenspace provide residents excellent opportunities to participate in a wide range of active and passive outdoor activities, from basketball to birdwatching. Several parks in the area attract residents from around the city and the region, like Odana Hills Golf Course, Owen Conservation Park, and Garner Park, with its Opera in the Park event.

This chapter also covers elements of a green and resilient city aside from parks, such as tree canopy cover and stormwater management. While other sustainability measures, like renewable energy and energy efficient buildings, also have a major role to play in a green and resilient city, they are more effectively addressed on a citywide basis through the Comprehensive Plan and Sustainability Plan. This Plan focuses on elements specific to the West Area.

Parks and Greenways

Overall, the Area has an excellent system of parks, with a total of 540 acres in 36 parks. There is approximately 18 acres of parkland per 1,000 residents. 78% of households are within a quarter mile of a park. However, there is an area south of Mineral Point Road lacking park access that is emerging as a new neighborhood with residential units replacing former retail and office develop-

ment. This area around Grand Canyon Drive and Yellowstone Drive requires new park space to serve the growing number of residents that do not have access to nearby park space.

While the variety of amenities in the Area's parks are appreciated by residents, continued investments will need to be made to keep up with the growing population of the area and the changing preferences of residents. For example, the City's first dedicated pickleball courts were added to Garner Park in 2016 to meet the exploding popularity of the sport, and a playground in Rennebohm Park was replaced with an inclusive playground in 2023. Community park amenities like those found in Garner and Rennebohm parks can attract residents from across the West Side. While mini and neighborhood parks tend to focus on serving the immediately surrounding neighborhoods, they can also sometimes feature unique recreational opportunities which can draw from a large area. Investments and changing amenities will be guided by Parks Development Plans (PDPs) for mini and neighborhood parks and master plans for community parks. The Parks Division completed West Area PDPs for all mini and neighborhood parks alongside the West Area Plan, and master plans for community parks

The West Area has about 207 acres of greenways that are maintained by the Engineering Division for stormwater

are expected to follow over the next decade.

Types of Parks

A **mini park** is a small park (generally < 5 acres) with limited recreational activities that serves the immediate surrounding area.

A **neighborhood park** is generally 5+ acres and serves as the recreational focus of the neighborhood, with more recreational amenities than a mini park.

A **community park** is usually 20+ acres that is designed to serve/attract residents from beyond the surrounding neighborhood.

A **conservation park** is primarily managed to preserve Madison's native landscapes, plants, and animal populations for the careful use and enjoyment of visitors.

Type of Park	# of Parks	Acres in West Area
Mini Parks	14	28
Neighborhood Parks	8	65
Community Parks	5	140
Conservation Parks	3	118
Golf Course (Odana Hills)	1	171
Open Space/Not Classified	4	18
Total	36	540

management. In some limited cases parks and greenways overlap. While the main purpose of greenways are for stormwater management, they also provide significant wildlife habitat and tree canopy in some locations. Engineering has recently completed a series of watershed studies that cover the West Area to help prioritize greenway and stormwater management projects to address runoff from more intense storms due to climate change.

Trees and Canopy Coverage

on single-family or duplex lots but does

require landscaping plans for multi-

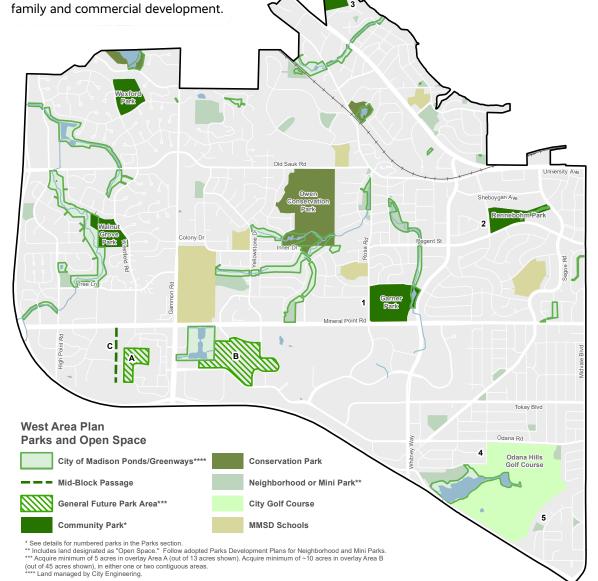
Overall, the Area has about 31% canopy coverage, with most coverage to the north of Mineral Point Road and east of University Research Park. West Area parks and greenways – especially conservation parks like Owen – play a strong role in the area's tree canopy, but ultimately City terrace trees and canopy on private property play a larger role. The City does not regulate trees



Rennebohm Park playground



Garner Park Sneite



Current landscaping requirements for commercial development are more stringent than when most of the West Area originally developed. The requirements should result in more canopy on private property over the long term as properties, some of which have substantial surface parking lots, are redeveloped. Maintaining terrace trees is a citywide effort, but over the long term there will be an opportunity to add more terrace trees in the Area as properties are redeveloped and the planned street network is implemented. This will break up large blocks and expanses of surface parking.



Capital Avenue street end at Lake Mendota

Stormwater Management and Flood Reduction

Trees, parking, and greenspace all have an impact on stormwater management, which continues to be a concern within the Area. The City's stormwater management ordinance was revised in 2020 to require better stormwater management on redeveloping properties. Still, the Area contains some of the more challenging watersheds in the city for stormwater management, such as a portion of the Willow Creek watershed, which drains stormwater from a low point at University Avenue and Midvale Boulevard that would not drain if not for two large storm sewer pipes, one of which was completed in 2023. While significant upgrades in stormwater management have been made, more investment in the system is needed. The watershed studies completed in the Area and across the city will help prioritize the most effective solutions for

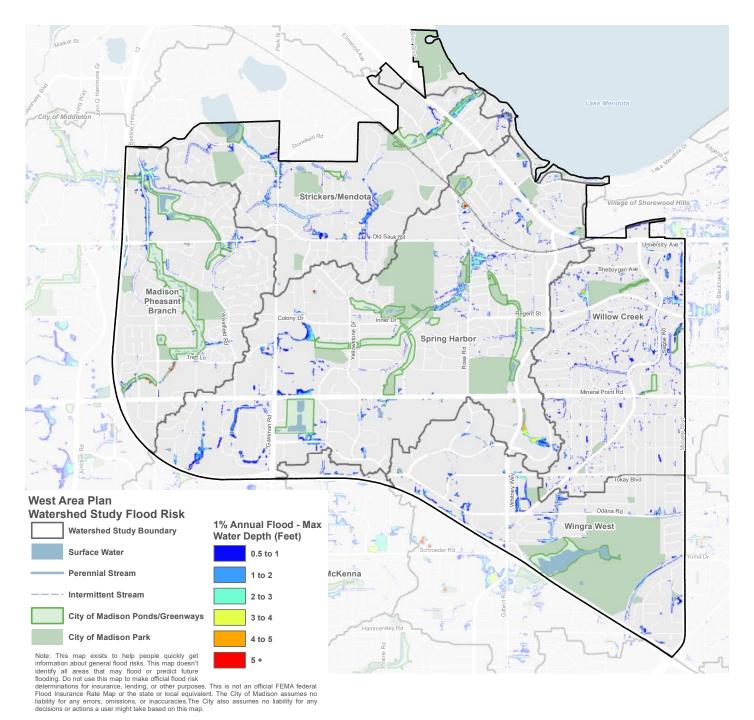
the City to implement in greenways and for storm sewers. With so much of the area developed as single-family housing, homeowners can also help improve water quality and decrease flooding in small events by implementing green infrastructure projects like rain gardens.

Water Quality

The City's 2020 update to its stormwater management ordinance, combined with recommendations to improve stormwater management in completed watershed plans, will help improve surface water quality. Compared to older parts of the city and areas with more of an industrial history, the West Area has relatively few sources of groundwater contamination, according to the Wisconsin Department of Natural Resources' Remediation and Redevelopment Database. The highest number of cases are associated with gas station and auto repair businesses that have been remediated and closed. An ongoing groundwater quality challenge is the chloride contamination from road salt that is impacting Well 14, along University Avenue. The Water Utility has modified the well to draw more water from the lower aquifer to reduce chloride. Additionally, the Streets Division has reduced winter salt use citywide. Annual use has dropped from the 12,000-16,000 ton range prior to the winter of 2014-15 to a 6,000-10,000 ton range since that time. While the reduction will help water quality in the long term, groundwater can see delayed results from changes to street salt use. If chloride concentration in Well 14 does not drop, a new well may need to be considered. The Water Utility currently owns land close to the Whitney Way-Mineral Point Road intersection that could be used to address the issue.

Urban Agriculture

The Area is largely developed. The largest undeveloped property is along Mineral Point Road between University Research Park and Oakwood Village. It is privately owned by TruStage, and is likely to be developed in the future. Overall, the Area lacks a substantial amount of space for urban agriculture. However, opportunities exist for smaller-scale community gardens on private land, such as the gardens at Madison Christian Community along Old Sauk Road, and through community group partnerships with the the City's Parks Division, as is in place for the community gardens at Rennebohm Park. Community gardens are allowed under the parks and recreation zoning district and all commercial, mixed-use, residential, and employment zoning districts.



What We Heard

- Appreciation for the variety of parks, natural areas, greenways, and other greenspaces throughout the area, and the trails, recreational activities, and wildlife within those spaces.
- A strong tree canopy on private lots and public greenspaces, with a desire to retain and/or improve canopy coverage, especially south of Mineral Point Road.
- A mix of desires for adding more biking/mountain biking facilities in parks and greenspaces vs. a desire to keep those spaces bike-free.
- A desire for more facilities like drinking fountains and restrooms in parks and different/ additional recreational facilities/ amenities like more pickleball courts.
- 5. A general goal to "sustain and maintain" current parks while also looking for opportunities to add more park space as additional housing is constructed.
- The importance of proper stormwater management, reducing pesticide use, and reducing salt use to maintain or improve water quality.

Actions

Parks

- 1. Garner Park:
 - Explore additional trails in Garner Park as part of any future improvements for the park.
- 2. Rennebohm Park:
 - Require dedication of additional parkland as part of any redevelopment of land adjoining the park that increases the number of residential dwelling units.
 - b. Consider the following elements as part of future upgrades to the park: addition of a water play feature, improvements to the shelter, additional seating near the shelter/tennis courts/paths, and path lighting.
 - c. Provide pedestrian and bicycle access to the park from Sheboygan Avenue, potentially as part of additional north-south and east-west streets (see planned future streets in the Land Use and Transportation chapter).
- 3. Marshall Park: Increase access to the park from the south by constructing a pedestrian/bicycle connection to the park from Lake Mendota Drive/Camelot Drive. Note: the line shown on the Shared-Use Path & Bicycle Network map represents the connection but may not be the final route.
- 4. Implement changes in Odana Hills Golf Course and adjacent park land to:
 - a. Reinvest in the Course in a manner that is aligned with the Task Force on Municipal Golf's recommendations to provide a sustainable, high-quality 18-hole course that is environmentally and financially sustainable and inclusive of year-round diverse golf park programming.
 - b. Work to integrate stormwater management into the Course to protect neighboring properties, enhance the natural environment, and build off of volunteer-driven efforts to promote a more sustainable landscape.
 - c. Replace the existing clubhouse with a year-round, fully accessible facility that serves a broader public purpose beyond golf.
 - d. Take a holistic approach to managing Odana Hills Park (west), Odana Hills Park (east), and Odana Hills Golf Course to expand the variety of recreational opportunities available throughout the combined properties.
- 5. Zook Park: Increase connectivity between the park, surrounding neighborhood, and the Southwest Commuter Path via a paved path.
- 6. Initiate Park Master Plans within the next 10 years for community parks in the planning area (Garner, Marshall, Rennebohm, Walnut Grove, Wexford).
- 7. Implement <u>adopted Park Development Plans</u> for neighborhood and mini parks over the next 15 years.
- 8. Improve public lake access at lake access courts and parks north of Lake Mendota Drive.
 - a. Construct an ADA-compliant accessible path at the most appropriate location to increase inclusive lake access.
 - b. Add signage (in addition to the existing standard green street signs) to indicate public lake access.
 - c. Work with the Ho-Chunk Nation to place educational plaques at courts and/or along Lake Mendota Drive to describe and illustrate the history of the Ho-Chunk Nation in the area.
- 9. Create a small neighborhood park with a variety of outdoor recreation opportunities south and west of University Avenue and west of Spring Harbor

- Drive to serve this part of the West Area, which is underserved by existing parks.
- Look for opportunities to install recreational biking facilities in the Area, such as skill tracks, pump tracks, and single track biking facilities, in alignment with Madison Bicycle Adventure Trail (MadBAT) goals.
- 11. Create three new parks that are a minimum of five acres each to serve future residents in redeveloping commercial areas. The General Future Park Area overlay shown on the map designates the general area within which a smaller portion of the land will be sited for a park or open space in the future. The large general overlay is necessary because at this point it is not known where the park and open space will be sited.
 - a. One park should be as centrally located as possible amongst any future redevelopment of West Towne Mall and the big box stores to the west of the mall. While the block containing the Von Maur site would be ideal if the mall were redeveloped all at once, Von Maur is the most recent investment in the area and therefore likely to remain over the mid- or longterm. Alternative locations are shown within the overall western "general future park area" labeled as "A" on the Parks and Open Space Map. This park may need to be two smaller parcels connected by a green corridor.
 - b. An urban feel, like Lisa Link Peace Park or McPike
 Park, should be considered for the western park
 (A on the map), due to its location amidst more
 intensive planned mixed-use development and the
 possibility of serving residents, patrons of nearby
 businesses, and visitors to the area. A design that
 can accommodate special events and programming,
 such as farmers' markets, festivals, and concerts is encouraged.



Marshall Park



Garner Park Pickleball courts

c. Two other parks should be created between the stormwater ponds and Oakwood Village. See the eastern green hatched area labeled as "B" on the Parks and Open Space Map for the generally acceptable location for these parks. The Parks Division may opt to proceed with one larger park,

if that is found to be preferable based on property availability.

- d. Park space should be acquired through purchase of property and through parkland dedication which is required of new residential development. Parkland dedication may be required of projects when they are proposed adjacent to dedicated parkland in either area A or area B on the map.
- e. The eastern park(s) (B on the map) may be developed with a focus on serving the local neighborhood and would ideally include park space adjacent to the major stormwater facility south of Mineral Point Road and west of Gammon Place.
- f. Future designs and amenities for new park land will be determined via park development planning processes undertaken by the Parks Division. Such a planning process will proactively engage a diverse group of residents, including students from nearby Ezekiel Gillespie and Vel Phillips Memorial schools. The park planning process will consider amenities and park names that are welcoming to BIPOC residents.

Trees and Canopy Coverage

- 12. Enhance the tree canopy on public right-of-way. The biggest opportunity for increasing street tree canopy on existing streets is within University Research Park.
- 13. As recommended in the Complete Green Streets Guide, terrace widths on new streets planned for the area should be wide enough to accommodate healthy canopy trees and/ or bioswales, typically requiring a minimum of 10 feet. Expansion of existing substandard terraces (generally less than eight feet) should be accommodated through additional dedication of right-ofway whenever possible.
- 14. Work with Oakwood Village to explore the prospects of an official, publicly accessible "green corridor" connecting Nautilus Point Park through the Oakwood campus to planned City park space in the Grand Canyon Dr. - Yellowstone Dr. area and to the stormwater ponds to the west.
- 15. Evaluate stormwater utility parcels for plantings and ecological restoration consistent with the Stormwater Utility Vegetation Management Plan once adopted.

Stormwater Management and Flood Reduction

- 16. Implement recommendations included in the City's watershed studies to reduce flooding and create a more resilient stormwater management system. Pursue federal funding for stormwater management improvements in watersheds that face the most severe challenges, such as Willow Creek.
- 17. Focus City funding for public and private green infrastructure projects on watersheds that face the most severe challenges, such as Willow Creek and Spring Harbor, or in areas near the lakeshores or have concerns with water quality. Pair funding with increased publicity efforts surrounding City initiatives to boost green infrastructure on private property.
- 18. Implement innovative stormwater management techniques as part of future right-of-way design for both new street construction planned for the West Towne area if/when any redevelopment occurs, other new planned streets in University Research Park, and reconstruction of existing streets.
- 19. Projects in the four planned blocks directly south of the entrance to Sunset Memory Gardens should explore placing stormwater management measures mid-block, as a contiguous feature with accompanying mid-block pedestrian connections (see the "Mid-Block Passage" labeled "C" on the Parks and Open Space Map).
- 20. Consider integration of stormwater management into future park space for large storms as a natural feature in a way that does not compromise the amenities and use of the park outside of large storm events.
- 21. Work with the City of Middleton on cross-border stormwater management issues, especially regarding the Pheasant Branch watershed and Stricker Pond.
- 22. Work with the Village of Shorewood Hills on improving stormwater management in the Willow Creek watershed to reduce incidences of flooding along University Avenue and Midvale Boulevard in the face of increasing annual rainfall.

Sustainability & Urban Agriculture

23. Remove the north side of Vernon Boulevard in favor of additional greenspace for a community garden or civic space.

Partnerships

- A. While the City has a significant role to increase the West Area's canopy coverage from its existing 31% to the 40% goal established in the <u>Urban Forestry Task Force's report</u> through plantings on public property, the increased coverage will not be achieved without action on private property. The City should work with major property owners in the West Area, such as MMSD and University Research Park, to increase canopy on non-City land.
- B. Explore partnership opportunities with community groups to create additional community gardens, especially in proximity to concentrations of multifamily housing.

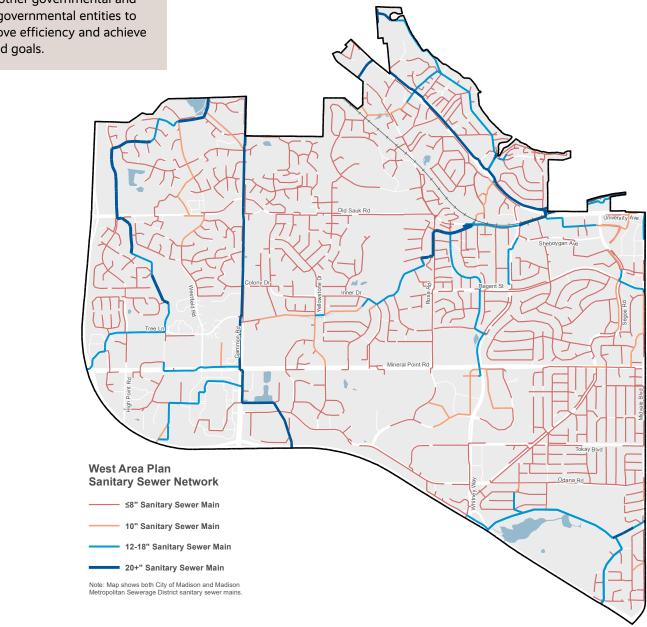
Effective Government

Comprehensive Plan Goals

This Plan aims to advance the following goals from the Comprehensive Plan

- · Madison will have efficient and reliable public utilities, facilities, and services that support all residents.
- Madison will collaborate with other governmental and non-governmental entities to improve efficiency and achieve shared goals.

The West Area has been largely developed since the mid-1990s. With that has come the full complement of City services, such as police and fire protection, utilities, waste management, snow removal, and libraries. The City will continue to provide a full range of services to all neighborhoods, while optimizing where community facilities are located in the Area and surrounding areas. Optimization is needed to balance provision of services with the long-term costs to operate facilities and the growing and changing population of the West Side. For example, the the Police Department's West District was split when the Midtown Police Station was completed in 2018 to better cover West Madison due to rapid growth on the far West Side. A public works campus is also taking shape to the west of the Beltline to improve service delivery on the West Side as land continues to develop west of the Beltline.



Redevelopment of retail and office uses into residential results in changing demands for City services and infrastructure. For example, City sanitary sewer mains may need upsizing as redevelopment transitions low-density commercial and office space to mixed-use development or multifamily residential development. Upsizing of sanitary sewer mains is most likely to be needed when properties are served by a main of eight inches in diameter - the smallest mains in the Area. Additional adjustments to services will likely be needed in the future as the West Area, and the city, continue to grow.

Maximizing the efficient provision of facilities and services can be achieved through careful coordination among City agencies and with other municipalities. Examples include some residents on the north side of the Area visiting the Middleton Library and the City of Madison Fire Department providing service to the Village of Shorewood Hills. Such cooperation can help avoid costly duplication of services. A further important focus is to provide customers, especially underrepresented populations, with the tools to effectively gain access to Madison's services and resources.

What We Heard

- 1. Residents expressed a desire to continue engaging with the City as details and designs are developed to implement plan actions.
- 2. Communication around plan implementation is needed.
- 3. Access to schools, libraries, and other amenities is appreciated.

Actions

- 1. Review sanitary sewer capacity needs related to anticipated development/ redevelopment and pair capacity upgrades with road construction projects where possible.
- 2. Study water utility service area #8 (West Towne area and north of West Towne) to determine whether a new well is needed to serve the area.

Partnerships

- A. Work with MMSD and the Lussier Community Education Center to facilitate expansion of Lussier to provide additional community space and services to area residents and students.
- B. Explore opportunities to secure/reserve space to serve as a community center, neighborhood center, or meeting space for future residents of a new Odana neighborhood, potentially in conjunction with, or close to, new park space.



Lussier Community and Education Center

Health and Safety

Comprehensive Plan Goals

This Plan aims to advance the following goals from the Comprehensive Plan

- Madison will be a place where all residents have equitable health outcomes.
- Madison will be a place where residents and visitors are safe at home and feel welcome in the community.

Madison strives to be a place where all residents have equitable health outcomes, residents and visitors are safe, and all feel welcome in the community. To support these goals, the Comprehensive Plan encourages more community partnerships, better access to mental and physical health services, healthy and safe living environments, and public health approaches to violence prevention.

According to the <u>Neighborhood Indicators Project 2022 data</u>, the West Area is relatively safe compared to the city as a whole. Despite having 11% of the City's population and a significant amount of commercial development, only 9% of property related offenses (which includes thefts, burglary, stolen vehicles and such other offenses) were reported. The Area also only had 6% of the City's crimes against people and 5% crimes against society. Crimes against society include narcotics, liquor law, gambling, weapons and prostitution.

The Area also had only 11% of citywide crashes, 12% of emergency medical services calls, and 9% calls for Fire service. With regard to infant health, several measures indicate the Area is doing very well compared to the rest of the city. Only 11% of births are pre-term and only 10% received less than adequate prenatal care. The Area also had very few property maintenance and zoning violations, with only 5% of reported citywide violations.

The following actions support health and safety for residents, with a focus on physical changes. This includes building off of strategies and actions the City is already engaged with through the All Ages and Abilities Bike Network Plan, Vision Zero Program, Violence Prevention Plan, and Community Health Improvement Plan. Implementation of these initiatives will help improve health and safety in the Area through things like neighborhood friendly infrastructure and street design, building safe public spaces by providing buffers and green spaces that separate public areas from transportation corridors, providing protected on-street facilities for vulnerable users in street design, and focusing on All Ages and Abilities facilities when possible while encouraging human centered transportation design such as slow streets and shared streets, and closing gaps in the pedestrian and bicycle network. Health and Safety also covers equitable outcomes for residents, and this includes continued citywide efforts that will support Area residents such as expanding resources to build and maintain a strong, connected fiber network and radio communication system for existing and future City operations.

Actions

- Monitor driving behavior and speed and enforce traffic regulations on streets along school routes including N. Gammon Rd., Tree Ln., Old Sauk Rd., Rosa Rd., Segoe Rd., and other areas where people walk frequently and feel endangered and/or are experiencing higher levels of serious traffic related injuries and fatalities (see the High Injury Network map).
- 2. Conduct Vision Zero speed analyses on Indian Hills, High Point Rd., Tree Ln., Whitney Way., Mineral Point Rd., Old Sauk Rd., Rosa Rd. and Segoe Rd. and explore potential traffic calming improvements such as road diets and speed bumps; pair these actions with speed enforcement.
- 3. Increase opportunities for West District Police and community members to engage and build trust with residents of color and people living with lower incomes including the Tree Ln., Oakbridge and Wexford Ridge areas. Activities could include Coffee with a Cop, and Police participation with youth recreation programs, block parties and cultural events and activities.
- 4. Include the Tree Ln. and Wexford Ridge neighborhood as a potential new Neighborhood Resource Team area to improve equitable outcomes by focusing City resources and building stronger community connections for the growing population of people of color and those living with lower incomes.
- 5. Encourage new buildings within 200 feet of the Beltline to use materials and insulation that keep interior noise levels below 52 decibels as per state code TRANS 405. Design sites to shield outdoor spaces from noise above 67 decibels. Developments near the Beltline should follow setback and noise regulations outlined in Wisconsin statutes (TRANS 233, TRANS 405), and MGO Section 16.23.

Partnerships

- A. Work with MSCR and Lussier Community Education Center (LCEC) to improve access to health and recreation programs at MSCR Odana, LCEC, schools, and other locations such as walking/running groups, yoga, meditation, and healthy cooking and nutrition classes.
- B. Work with resident leaders to continue to monitor environmental factors that may contribute to, or protect against violence (e.g. dimly lit areas, abandoned buildings, increased green space, etc.) and use violence prevention resources such as the Madison Dane County Violence Prevention: A Roadmap to Reducing Violence Report to address related issues.
- C. Work with the National Interscholastic Cycling Association and other organizations that offer recreational activities outside of schools, to welcome, encourage, and support participation by middle and high school students of color.
- D. Expand options in the Area where residents without air conditioning can go to get relief from the heat by partnering with LCEC, area businesses, centers of worship, and others to establish them as City-recognized cooling sites.
- E. Partner with Pepartnership Inc. to establish a personal essentials pantry in the Tree Ln. and Wexford Neighborhood area.
- F. Create partnerships between neighborhood groups, YMCA, MSCR and area schools to establish youth recreation programs at Haen Family Park and nearby school open space for activities like flag football, basketball and soccer.
- G. Work with residents, neighborhood groups, Madison Arts and non-profit organizations to activate West Area parks and other greenspaces in Oakbridge, Tree Ln., and Wexford Ridge areas for people of all ages with musical performances, community meals, arts activities and recreation.

What We Heard

- There is speeding and unsafe driving on some area roads, which makes walking and biking unpleasant, especially for kids walking, biking and skateboarding to school and people with limited mobility.
- 2. Low lighting along school routes is a concern.
- 3. Pockets of gun violence and stolen cars exist in the area.
- 4. There is a lack of information sharing and communication between the City and community.
- 5. Activities for teenagers are lacking.
- 6. People of color and those living with lower incomes can tend to feel distrustful of Police due to negative experiences.

Appendix

Historic Resources Review	7
Plan Implementation Summary62	2
Glossary	3

Historic Resources Review

Staff completed a "windshield survey" (observing properties while driving through the area) on November 16, 2022 to assess historic resources in the West Area. Several properties with potential architectural significance were noted during the survey. Staff conducted additional research using files from the Wisconsin Historical Society, City of Madison preservation and building permit files, historic phone directories, and US Census records to supplement the windshield survey.

There are several known archaeological sites throughout the planning area, with most relating to early Native American history. These sites are either mound locations, which are culturally significant human burial sites, or historic village locations. Some of these sites have been disturbed by subsequent development, but further ground-disturbing work may require additional review and consultation with the Ho-Chunk Nation and the Office of the State Archaeologist. While there is a concentration of archaeological sites along the shore of Lake Mendota, there are other sites throughout the planning area, as Madison has been the site of long-term human occupation. There is a Human Durial Site Tax Exemption process for properties with known human burial sites that makes the undevelopable portion of a property tax exempt. Owners of such property should contact the Wisconsin Office of the State Archaeologist and the Madison Assessor's Office.

The area also contains six designated City of Madison landmarks: three for history, one for architecture and history, and two for archaeology. The West Area also contains the University Hill Farms National Register Historic District. Being listed on the National Register is an honorary designation that comes with Federal financial incentives to encourage adaptive reuse of historic buildings. The City's Preservation Planner identified 35 individual properties that warranted additional investigations to explore either National Register or local historic designations. There are also four neighborhoods that warrant additional investigations for possible National Register or local historic district designation: Crestwood, Blackhawk Park, Highlands, and Odana Hills.

Crestwood was developed in the 1930s and contains an excellent collection of Midcentury architecture with houses designed by Frank Lloyd Wright, Marshall Erdman, and Norman Kandl. It is also the location of the Carson and Beatrice Gulley House. Carson Gulley was a noted African American chef and TV personality. His story is detailed in the City's <u>Underrepresented Communities Historic Resources Survey</u>.

Blackhawk Park has previously had an intensive survey and the Wisconsin State Historic Preservation Office has determined it to be eligible to the National Register of Historic Places. This area is currently known as the Trillium Homes Condominium Association but was originally developed by the Harnischfeger Corporation with simple Minimal Traditional style, post-World War II detached housing.

The Highlands has a diverse collection of architect-designed homes from a variety of time periods. Given the number of properties called out as possibly being architecturally significant, it is likely worthwhile to investigate a possible National Register historic district designation.

Odana Hills features a grouping of early Midcentury Modern residential architecture that may warrant additional investigations into possibly becoming a National Register Historic District.

There are several individual properties and some neighborhoods that could benefit from having their story officially documented through the National Register process. As it is an honorary process, it does not place additional regulations on private property owners but does provide additional considerations for government-funded or permitted projects for properties and opens the possibility for preservation tax credits to help property owners adapt their properties for new and ongoing uses.

Historic Preservation does not prevent development or changes of use, but it does shape how redevelopment happens. It is not a tool for exclusionary zoning. Properties with possible historic value are opportunities for adaptive reuse of places of architectural or historic significance so that these places could be sensitively integrated into the ongoing growth of Madison. Sharing Madison's unique culture and character through historic preservation, representing the history of Madison's diversity, and building reuse as an environmental sustainability strategy are priorities in of the Madison Preservation Plan.

Designated Landmarks

Name and Address	Area of Significance	Notes
Thorstrand Estate/Mary North House 1 Thorstrand Rd	History	Daughter of Magnus & Annie Swenson. One of two houses on family estate, both designed by Law & Law
Thorstrand Estate/ Magnus Swenson House 2 Thorstrand Rd	History	Founder of UW College of Agriculture, advances in sugar production
John R. Commons House 1645 Norman Way	Architecture & History	House designed by Cora Tuttle. John Commons was a UW economist, significant contributions to labor history and social legislation
Hickory Hill House 1721 Hickory Dr	History	Built in 1842 by John Hicks. Home of noted conservationist, Walter Scott, for 40 years with significant tree plantings on the property during his tenure
Merrill Springs Mound Group II	Archaeology	Listed in the National Register for its significance in Native American history
Spring Harbor Mound Group	Archaeology	Listed in the National Register for its significance in Native American history

National Register

Name and Address	Area of Significance	Notes
University Hill Farms	Architecture & History	Listed in the National Register for its significance in Urban Planning and its Midcentury architecture.

Properties of Historic Interest

Name and Address	Area of Significance	Notes
Crestwood Neighborhood	Architecture & History	Early neighborhood association, collection of International and Midcentury architecture, should explore for possible National Register of Historic Places designation
Thomas & Eva Laufer House 5710 Arbor Vitae Pl	Architecture	International style, designed by Marshall Erdman
Mabel & Rolfe B. Sawtelle House 5705 Arbor Vitae Pl	Architecture	International style
John S. Bordner House 5746 Bittersweet Pl	Architecture	International style, designed by Norman Kandl
Beth Whitaker & Mary Schlaefer House 5818 Anchorage Ave	Architecture	Wrightean style, designed by Marshall Erdman
Eugene & Mary Van Tamelen House 5817 Anchorage Ave	Architecture	Usonian style. Frank Lloyd Wright architect, Marshall Erdman contractor
St. Dunstan's Church 6205 University Dr	Architecture	Midcentury Modern, Graven, Kenney & Iverson architects, Wengler addition
Eric Vogelman House 6109 Quetico Dr	Architecture	Vogelman's house and architect for Midcentury/Wrightean structure
Louise O. Kloepper & Mary A. Lescohier House 1621 Capital Ave	Architecture & History	Midcentury Modern/Wrightean structure, William Westley Peters architect. Designed for Mary Lescohier in recognition for her support of Monona Terrace in the 1950s
Dale Heights Presbyterian Church 5501 University Ave	Architecture	Midcentury Modern church, William Kaeser architect
Blackhawk Park Historic District 1 Craig-147 Craig Ave	Architecture & History	Determined eligible to the National Register as a historic district for its development history and architecture
Erdman and Associates Office and Shop 5117 University Ave	Architecture & History	Usonian style. William Kaeser architect, Marshall Erdman builder. Offices and shop of Marshall Erdman's architecture firm
Warner Farmhouse/Merrill Springs Farm 5010 Risser Rd	History	House constructed in 1858 for Rev. Louis Jenkins. Property named for longtime caretaker, Alf Merrill
Sunflower House/James Dresser House 5126 Tomahawk Tr	Architecture	Expressionist style house built by architect James Dresser as his residence.
Merrill Spring Park & Cistern 5102 Spring Ct	Engineering & Landscape Architecture	Sandstone cistern constructed by the Wisconsin Emergency Relief Administration (WERA) in 1934
Harold & Lorraine White House 1705 Hickory Dr	Architecture	Tudor Revival style Sears & Roebuck kit house with excellent historic integrity
David Robert Cheney House 6214 N Highlands Ave	Architecture	Wrightean house designed by engineer David Cheney using wood reclaimed from an earlier bridge project.
CUNA Mutual Insurance Building 5910 Mineral Point Rd	Architecture	International style office building, William Kaeser architect
Marbella Apartment Complex 6302 Mineral Point Rd	Architecture	New Formalist apartment building constructed in 1972 as a luxury apartment project. James Browing architect, Keith Parr landscape architect.

Name and Address	Area of Significance	Notes
James Madison Memorial High School 201 S Gammon Rd	Architecture	Midcentury Modern educational building. Grave, Kenney & Iverson architects
Orley W. Jensen House 6605 Gettysburg Dr	Architecture	Shed style residential structure
Owen Park 310 Jetty Dr	History & Archaeology	The caretakers house and rootcellar remain on the property and there is the potential for archaeological information in the former location of the house and barn. This property was associated with Professor Edward Owen, experimental agricultural uses in the City, and private development of the area with the intention of it becoming a park.
Highlands Mendota Beach School 5930 Old Sauk Rd	Architecture	Midcentury school, William Kaeser architect
George Kaeser Residence 5725 Old Middleton Rd	Architecture	William Kaeser designed this Wrightean residence for his brother George Kaeser
William Grubb Farmhouse/ W.H. Lighty House 6010 S Highlands Ave	History	Farmhouse constructed ca. 1860 for William Grubb. Property was later purchased by W.H. Lighty who developed correspondence courses for the UW Extension Office, beginning the education program for the Extension service.
Ronald & Louise Mattox Residence 1115 Willow Ln	Architecture	French Provincial style residence designed by architect Jerome Cerny
Professor Edward & Laura Kemers Residence 1102 Willow Ln	Architecture	Arts & Crafts style house designed by architect Frank Riley
Susan King & Robert Horowitz House 6217 N Highlands Ave	Architecture	Postmodern residence designed by architect Hardy Holzman Pfeiffer
Charles W. Curtis House 1406 W Skyline Dr	Architecture	Usonian house constructed in 1959 by architect Marshall Erdman
Keith Yelinek House 1 Larch Cir	Architecture	Shed style residential structure constructed by Wil-Mac Builders
Gertrude Herman House 1425 Skyline Dr	Architecture	Wrightean house designed by Dohm Construction
Highlands Neighborhood	Architecture	Collection of significant architectural residences should be explored for National Register eligibility
Madison Christian Church 7118 Old Sauk Rd	Architecture	Exaggerated Modern religious architecture, designed by Strang Partners
Carson & Beatrice Gulley House 5701 Cedar Pl	History	Associated with the life of noted African-American chef and television personality, Carson Gulley
Roger E. McMullin House 4509 Ames St	Architecture	Usonian house constructed in 1955 by architect Roger McMullin as his residence
Vic H. Peterson House 509 Piper Dr	Architecture	Usonian house designed by builder V.H. Peterson in 1953
Muriel & Dr. Royal Rotter House 614 Odell St	Architecture	Wrightean house designed by builder Herman E. Postweiler in 1955. Explore significance of houses at 610 and 618 Odel as possibly same builder with different architecture than rest of neighborhood. Perhaps a mini district
Oswald B Anderson House 602 Hilltop Dr	Architecture	New England Colonial Revival house built in 1958 by builder F.J. Vivian

Name and Address	Area of Significance	Notes
John Lundeen House 5701 Lake Mendota Dr	Architecture	Organic architecture, H. Fritz architect, house was sited and constructed so as to protect the existing trees on the property, which required a variance when it was constructed in 1969.
Odana Hills Neighborhood	Architecture	Collection of early Midcentury residential buildings. Further investigation of possible National Register Historic District is recommended.
Anchor Bank Building 302 N Midvale Blvd	Architecture	Anchor Savings & Loan was designed by Graven, Kenney, & Iverson. Constructed in 1966 and expanded in 1979, it is an excellent example of Contemporary Architecture.

Plan Implementation Summary

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	Land Use Actions	Agencies
1	Update the Comprehensive Plan Generalized Future Land Use Map to be consistent with the Future Land Use map in this Plan.	Planning
2	Rezone properties identified in the Proactive Rezoning map to implement the goals of this Plan and encourage development consistent with the actions in this Plan. Ensure all impacted property owners are aware of potential zoning changes.	Planning, Zoning
3	Review Planned Development (PD) zoning covering development along and/or west of Gammon Road for a potential transition to conventional zoning to simplify resident applications to the City for minor projects and simplify staff administration.	Planning, Zoning
4	Implement maximum building height recommendations shown on the Generalized Future Land Use Map via zoning, with a mechanism for allowing bonus stories for developments that achieve specific affordability or sustainability parameters, similar to downtown height bonuses in City ordinances (Madison General Ordinances 28.071(2)(a)2).	Planning
5	Create a new commercial and employment zoning district with limited allowable residential development for areas along the West Beltline Highway and similar areas elsewhere in the City.	Planning, Zoning
6	Require ground floor commercial uses in the locations shown as Commercial Core on the Future Land Use Map.	Planning
	Transportation Actions	Agencies
1	Close gaps in the sidewalk network shown on the Sidewalk Network Map through the Safe Streets Madison Program, as streets are reconstructed, and as priority sidewalk additions can be constructed through state and federal grants (see priority sidewalk actions 4 b. and 5 c. below)	Engineering, Traffic Engineering
2	Implement the pedestrian and bicycle improvements shown on the Shared-Use Path and Bicycle Network Map. Further description is included below with the actions corresponding to the numbers on the map.	Engineering, Traffic Engineering
3	Vel Phillips Memorial High School and Ezekiel Gillespie Middle School	
a.	Study the addition of a left turn arrow from northbound N. Gammon Rd. into Tree Ln. to the traffic signal at the Tree-Gammon intersection.	Traffic Engineering
b.	Add a shared-use path along the east side of N. Gammon Rd. from Mineral Point Rd. to the City of Middleton. Starting at Tree Lane or Colony Drive, narrow Gammon Road to one lane of traffic in each direction with a center turn lane to reduce speeding, increase safety, and allow for construction of the shared-use path.	Engineering, Traffic Engineering
C.	Consider narrowing travel lanes and other design changes on N. Gammon Rd. between Mineral Point Rd. and Tree Ln. to calm traffic and enhance safe connections to schools.	Traffic Engineering
d.	Coordinate with the Madison Metropolitan School District (MMSD) on an improved eastwest shared-use path connection through the high school and middle school property.	MMSD, Engineering
e.	Work with the Wisconsin Department of Transportation (WisDOT) to clarify the lane markings and signage at the W. Beltline Hwy. ramps at Gammon Rd. to improve pedestrian and bicyclist safety and comfort.	Traffic Engineering
f.	When WisDOT pursues replacement of the W. Beltline Hwy. bridge at S. Gammon Rd., work with them to extend the wide sidewalk/shared-use path under the highway.	Traffic Engineering
g.	Work with WisDOT to add lighting under the S. Gammon Rd. overpass.	Engineering, Traffic Engineering

4	Crestwood Elementary School	
a.	Review MMSD's school travel plan and flow of people walking, biking and driving to the school to improve safety at the school.	Traffic Engineering
b.	Evaluate options to add or enhance ped/bike facilities on Old Sauk Rd. where they are missing, through street reconstruction and the Safe Streets Madison Program.	Engineering, Traffic Engineering
5	Stephens Elementary School	
a.	In coordination with MMSD, mark bike lanes on N. Rosa Rd. while adding bump outs for school busses and installing more durable terrace paving for students getting on and off busses.	Engineering, Traffic Engineering
b.	Consider adding an alternate student drop-off/pick-up area at the back of the school on Cable Ave.	Engineering, Traffic Engineering
C.	Add a priority sidewalk on the remainder of the north side of Cable Ave., the west side of Beach St. and the north and south sides of Anchorage Ave. between Beach St. and Rosa Rd., along with crosswalks at the Cable Ave. and Beach St. intersection, as part of the Safe Streets Madison Program.	Engineering, Traffic Engineering
6	Spring Harbor Middle School	
a.	Replace the pedestrian underpass beneath the railroad between Old Middleton Rd. and Craig Ave. and add lighting and a new pathway extending from Craig Ave.	Engineering, Traffic Engineering
7	If the rail corridor becomes inactive, work with WisDOT to study the feasibility of converting the corridor to a shared-use path that parallels University Ave., like the Southwest Path.	Engineering, Traffic Engineering
8	Construct a shared-use path from Island Dr. through the southeast corner of Owen Conservation Park to Bordner Dr. in coordination with the Crestwood Neighborhood Association.	Engineering, Parks
9	During the Sauk Creek Greenway Corridor Plan, use detailed engineering data to consider the impacts and benefits of adding an All Ages and Abilities shared-use path to make an east-west connection across the greenway to Walnut Grove Park. Any All Ages and Abilities path should be designed using environmentally sensitive best practices and minimizing impacts to trees and other vegetation, wildlife habitat, and adjacent properties.	Engineering, Traffic Engineering
10	When the Wexford Pond Greenway comes up for reconstruction, use environmentally sensitive best practices to design a shared-use path through the greenway. Where possible, the path should follow the existing maintenance path to provide continued access for greenway management, and it should provide a safe, accessible, and enjoyable route for bicyclists, pedestrians, people in wheelchairs, and other users, while minimizing impacts to wildlife habitat, vegetation, trees and adjacent properties.	Engineering, Traffic Engineering
11	Convert existing five-foot wide sidewalks to 10' wide shared-use paths along Regent St. from N. Segoe Rd. to Eau Claire Ave. and along Eau Claire Ave. from Old Middleton Rd. to Regent St. while maintaining or widening existing terraces and minimizing tree impacts. This would facilitate bicycle connections without removing on-street parking that is needed to serve Rennebohm Park. Shared-use path implementation should occur in concert with additional dedication of parkland for Rennebohm Park.	Engineering, Traffic Engineering
12	Reduce on-street parking on High Point Rd. and Westfield Rd. between Old Sauk Rd. and Mineral Point Rd. to allow for construction of all ages and abilities bike facilities.	Engineering, Traffic Engineering
13	Extend the one lane in each direction configuration of Old Sauk Road from Westfield Road to High Point Road while adding a center turn lane and buffering the existing bicycle lanes.	Engineering, Traffic Engineering

14	Integrate pedestrian and bicycle infrastructure like benches, pedestrian-oriented lighting, and bike racks into terrace areas adjacent to high-traffic pedestrian and bicycle routes, especially in and around the "Commercial Core" areas shown on the GFLU Map.	Engineering, Traffic Engineering
15	Integrate facilities like bike racks, bike lockers and BCycle stations in street reconstruction and development projects along BRT corridors and at BRT stations.	Engineering, Traffic Engineering
16	Implement bicycle wayfinding.	Engineering, Traffic Engineering
17	Work with the City of Middleton to:	
a.	Extend a shared-use path from Camelot Drive through Marshall Park to connect with Middleton Beach Road. Note: the line shown on the Shared-Use Path & Bicycle Network map represents the connection but may not be the final route.	Engineering, Parks
b.	Ensure cross-border bicycle connectivity as both communities continue to improve bicycle facilities, including lanes on John Q. Hammons Dr., paths or lanes on N. High Point Rd., and a possible path along N. Gammon Rd.	Traffic Engineering
18	Work with WisDOT to improve pedestrian and bicycle connectivity across the Beltline by:	
a.	Integrating new Beltline crossings shown on the Shared-Use Path and Bicycle Network Map into any future Beltline construction/reconstruction to enhance connectivity for pedestrians and bicyclists.	Engineering, Traffic Engineering
b.	Improving conditions for transit, bicyclists, and pedestrians at Beltline interchanges.	Engineering, Traffic Engineering
C.	Improving conditions of the W. Beltline Hwy. underpass at Struck St. and making needed improvements for safety and comfort of pedestrians and bicyclists such as upgrades to pavement, drainage, and lighting.	Engineering, Traffic Engineering
19	Improve pedestrian and bicyclist safety at the major intersections identified on the Pedestrian Network map.	Engineering, Traffic Engineering
20	Improve pedestrian safety along Segoe Rd. by by removing Laub Ln. from Berwyn Dr. to S. Segoe Rd. and by reducing pedestrian exposure and crossing distance with bump-outs, pedestrian refuge islands, and continental crosswalk markings, especially where Segoe Rd. crosses Wedgewood-Manor Cross, Tokay Blvd., and Tocora-Hilltop.	Engineering, Traffic Engineering
21	Update the Complete Green Streets Guide to reflect the Complete Green Streets Types Map in this Plan.	Engineering, Traffic Engineering
22	Preserve the opportunity to connect Middleton Street in the future.	Engineering
23	Improve road connectivity with the following street segment changes (also see the Planned Streets Map):	Engineering, Traffic Engineering
a.	Study the transformation of St. Dunstan's Drive to a two-way street from Allen Blvd. to Old Middleton Rd. if the St. Dunstan's Episcopal Church site is proposed for redevelopment.	Engineering, Traffic Engineering
b.	Connect Yosemite Place to Yosemite Trail to improve connectivity south of Old Sauk Rd.	Engineering, Traffic Engineering
		Engineering,
C.	Contingent on redevelopment proposed in the area, extend Appalachian Way to the east, then south to connect to Old Sauk Road. Add a new north-south street alongside the Cooper Lane Bike Path to connect with Appalachian Way extended.	Traffic Engineering
c.	then south to connect to Old Sauk Road. Add a new north-south street alongside the	•

e.	If properties north of Rennebohm Park redevelop reserve property to create a narrow, pedestrian- and bicyclist friendly street to the north of the park to break up a large block, provide the framework to orient buildings toward the public park, and create alternate access to the properties fronting Sheboygan Ave. The street should have the minimum allowable speed limit and be designed to minimize non-local through traffic.	Engineering, Traffic Engineering, Parks
	Transportation Partnerships	Agencies
Α	Work with WisDOT to:	
i.	Add new Beltline crossings shown on the Planned Streets Map and the Shared-Use Path to enhance connectivity for cars, pedestrians, bicyclists, and transit, shorten trips, and provide alternative north-south routes to Beltline interchanges at S. Gammon Rd. and S. Whitney Way.	Engineering, Traffic Engineering
ii.	Contribute Beltline planning to ensure that City interests are represented as part of Wis- DOT processes.	Engineering, Traffic Engineering
В	Explore public-private partnerships with area commercial property owners to create a Park and Ride system that makes better use of underutilized parking lots on the weekdays.	Engineering, Traffic Engineering
С	Work with the Madison Metropolitan School District to increase lighting along the north-south shared-use path that runs along the east side of the Ezekial Gillespie Middle School property and connects to Inner Dr.	Madison Area MPO, private property owners
D	Work with the MMSD and school principals to address pedestrian, bicycle, and traffic safety issues around schools when street repaving, reconstruction, or other changes to the right-of-way are being considered. Strategies may include signage, increasing crosswalk visibility, modifying traffic patterns, increasing nearby sidewalk connectivity, and implementing safe All Ages and Abilities bicycle connections.	Traffic Engineering, Engineering, MMSD
	menting sale All Ages and Abilities bicycle conflections.	
	Neighborhoods and Housing Actions	Agencies
1		Agencies Community Development, Economic Development
1	Neighborhoods and Housing Actions Work with developers to encourage a wider mix of apartment unit sizes, including smaller units for younger and older households, as well as units large enough to support larger	Community Development, Economic
	Neighborhoods and Housing Actions Work with developers to encourage a wider mix of apartment unit sizes, including smaller units for younger and older households, as well as units large enough to support larger families (three or more bedrooms) in proximity to schools. Encourage and incentivize development of smaller-scale "Missing Middle" housing types in Low-Medium Residential (LMR) areas on the GFLU Map, especially in areas near	Community Development, Economic Development Community Development,
2	Neighborhoods and Housing Actions Work with developers to encourage a wider mix of apartment unit sizes, including smaller units for younger and older households, as well as units large enough to support larger families (three or more bedrooms) in proximity to schools. Encourage and incentivize development of smaller-scale "Missing Middle" housing types in Low-Medium Residential (LMR) areas on the GFLU Map, especially in areas near schools, parks, and transit service, through zoning ordinance changes. For new housing partially funded by the City (through land banking, the Affordable Housing Fund, TIF) work with partners to distribute housing at costs affordable to a variety of	Community Development, Economic Development Community Development, Planning Community
3	Neighborhoods and Housing Actions Work with developers to encourage a wider mix of apartment unit sizes, including smaller units for younger and older households, as well as units large enough to support larger families (three or more bedrooms) in proximity to schools. Encourage and incentivize development of smaller-scale "Missing Middle" housing types in Low-Medium Residential (LMR) areas on the GFLU Map, especially in areas near schools, parks, and transit service, through zoning ordinance changes. For new housing partially funded by the City (through land banking, the Affordable Housing Fund, TIF) work with partners to distribute housing at costs affordable to a variety of households throughout the area, with a focus on areas along the BRT corridor. Development along the Beltline Highway should include a variety of noise mitigation improvements. Limit or entirely restrict City financial support for affordable housing in these	Community Development, Economic Development Community Development, Planning Community Development Community Community Community Community
3	Neighborhoods and Housing Actions Work with developers to encourage a wider mix of apartment unit sizes, including smaller units for younger and older households, as well as units large enough to support larger families (three or more bedrooms) in proximity to schools. Encourage and incentivize development of smaller-scale "Missing Middle" housing types in Low-Medium Residential (LMR) areas on the GFLU Map, especially in areas near schools, parks, and transit service, through zoning ordinance changes. For new housing partially funded by the City (through land banking, the Affordable Housing Fund, TIF) work with partners to distribute housing at costs affordable to a variety of households throughout the area, with a focus on areas along the BRT corridor. Development along the Beltline Highway should include a variety of noise mitigation improvements. Limit or entirely restrict City financial support for affordable housing in these locations, unless significant noise mitigation measures are included. Increase access to information regarding First Time Home Buyer Assistance programs for rental and utility payment support, the City's property tax assistance program for seniors, and the City's low-cost mortgage and rehabilitation loans, and historic preservation tax	Community Development, Economic Development Community Development, Planning Community Development Community Development Community Community Community Community Community Community Community

Increase awareness and access to existing community programs and services, especially for youth and seniors, and explore expanding them in such locations as the community rooms of new housing developments, Lussier Community Education Center, and through co-location of community-based organizations. **Economy and Opportunity Actions Agencies** 1 Analyse land banking within the concentration of commercial development south of Min-Economic eral Point Road as a potential method for reusing existing buildings for affordable local Development, business space (potentially as part of an incubator) and for redeveloping surplus surface **Planning** parking into affordable housing. 2 Monitor the performance of TIDs #41 and #46 to determine whether amendments are Economic financially feasible to fund affordable housing, non-assessable infrastructure (bicycle net-Development work improvements, pedestrian safety improvements, new streets, stormwater management improvements, etc.), creation of a business incubator, land banking, small business assistance, and other economic development initiatives in and around these TIDs. 3 As part of TID #41 and #46 evaluation, determine whether a commercial rent assistance Economic or guarantee program to assist small and emerging businesses is financially feasible, par-Development ticularly to allow BIPOC-owned businesses afford space in areas where rent increases may occur. Gap assistance could be used to help businesses displaced by redevelopment afford ground-floor space in new mixed-use buildings. 4 Form a staff team to review the possibility of a new TID in the West Towne area/Mineral Economic Point Road corridor to support redevelopment, stormwater management improvements, Development and other non-assessable infrastructure that is consistent with City plans. 5 Increase affordable childcare options, taking advantage of existing commercial spaces Economic where non-profit childcare providers could locate, ideally along transit corridors and close Development to employment areas. **Economy and Opportunity Partnerships Agencies** Α Explore the development of a business incubator, with lower cost space, shared resources and expertise needed to help grow local businesses. a. Partner with business, economic development, and community organizations, such as Latino, Black, Hmong, and Madison Chambers of Commerce, Wisconsin Women Business Initiative Corporation, and established incubators, to ensure an incubator meets the needs of BIPOC-owned businesses and fosters opportunities for long-term success. Consider integrating an incubator with land banking, using renovated commercial space to grow local businesses and retain affordable options. Incorporate networking, mentorship, and collaboration opportunities as part of business incubator planning. Integrate youth mentorship/training into incubator space, potentially in partnership with d. nearby Vel Phillips Memorial High School. Explore creating a program similar to the Public Market's "Market Ready" as part of incubator planning to help new businesses and small businesses prepare for incubator occupancy. Work with local commercial brokers and property owners to connect small businesses to

66 City of Madison

As business areas transition to a more connected, pedestrian-friendly environment, assist the creation of a neighborhood business association or BID (business improvement dis-

available space as they "graduate" from an incubator.

trict) to maintain amenities and facilitate events.

В

С	Continue building relationships with and supporting economic development partners, especially those with ties to historically underrepresented communities. For example, the Madison Black Chamber of Commerce, Latino Chamber of Commerce of Dane County, and the Hmong Wisconsin Chamber of Commerce have vibrant memberships, and the City should collaborate with them, and other similar business and economic development groups, to help them grow opportunities for entrepreneurs to locate in the West Area.	Economic Development with Madison Black Chamber of Commerce, Latino Chamber of Commerce of Dane County and the Hmong Wisconsin Chamber of Commerce
	Culture and Character Actions	Agencies
1	Review UDD 3 (MGO section 33.24(10)) to determine whether it should be revised or repealed. Many requirements for UDD 3 development are adequately addressed by the current zoning code, which was adopted approximately 25 years after UDD 3. Other ordinance language governing UDD 3, such as the Statement of Purpose that says the requirements were established, in part, to "assure that future development will complement the existing development in the district," contradict the actions in this Plan, which expressly seeks to transition the area away from its current auto-oriented form.	Planning
2	Changes to UDD 6 (MGO section 33.02(13)) should be analyzed as part of a larger citywide UDD review to:	
a.	Revise the boundaries to not include single-family development within the district boundary and add existing multifamily development to the south of Sheboygan Avenue and existing commercial development between Hilldale Mall, Midvale Blvd., Regent St., and Sheboygan Ave. to the district boundary.	Planning
b.	Align regulations with the TOD overlay zoning district.	Planning, Zoning
C.	Explore whether UDD 6 should be split into subareas to account for the widely varied characteristics and surroundings along the three-plus mile long corridor.	Planning
d.	Consider establishing minimum and maximum building heights in some or all of UDD 6 (note there is already a two-story minimum height in the portions of the UDD that are within the TOD overlay zoning district).	Planning
e.	Integrate design standards into the "Building Design" requirements to ensure buildings fronting shared-use paths address the path with architectural elements and building access at the same design level as the front of the building.	Planning
f.	Add additional building and site design elements to the "Building Design" requirements to require ground floor residential units facing public right-of-way have individual unit entrances from the sidewalk.	Planning
g.	Add an element to the "Building Design" guidelines to provide direct pedestrian connection(s) from buildings to adjacent public parks and/or shared-use paths.	Planning
3	Integrate public art into upcoming public construction projects, including Sidewalk Poetry, Utility Box Art Wraps, and public art at BRT stations, along gateway corridors, and along bike paths. For large-scale construction projects, explore hosting workshops with the community to identify locations for public art.	Planning
4	Implement the "Art Through City Building" concepts as described in the <u>City's Public Art Framework</u> , <u>Cultural Plan</u> , and other adopted policies to make the West Area more welcoming to people of every race, ethnicity, gender, age, and ability.	Planning

	Culture and Character Partnerships	Agencies
A	Work with the property owners and managers of the apartments on Sheboygan Avenue to identify opportunities to partner on placemaking opportunities to enliven the area and engage the community.	
В	Work with neighborhoods eligible for creation of a local historic district and/or a national register historic district to establish district(s). National Register or local historic district nominations should be initiated by the neighborhood, with technical assistance provided by the City. Note that National Register districts allow residents to apply for tax credits, whereas local historic districts establish design requirements that shape future reinvestment and development in these neighborhoods (see the Historic Resources Review in the Appendix for more information).	
С	The property at 6405 Mineral Point Road hosts the historic Otto & Louisa Toepfer Farm house, which is now part of Otto's Restaurant & Bar. Should changes or development be proposed for this site, the property owner is encouraged to adaptively reuse the historic building.	
D	Encourage the creation of neighborhood-oriented events in underserved areas, such as block parties, food cart nights, or summer concert series, to bring residents together more frequently. Such events should focus promotion on underserved communities.	
Е	Work with area property owners and nonprofits to add space for artist(s)-in-residence, affordable artist studios, exhibitions, performances, and rehearsals.	
F	Work with University Research Park and UW-Madison Arts staff to explore possibilities for locating arts space within or close to the Research Park. Space could take the form of studios, partnerships with businesses, an artist residency, or other formats.	
	Green and Resilient Actions	Agencies
1	Garner Park:	
a.	Explore additional trails in Garner Park as part of any future improvements for the park.	Parks, Engineering
2	Rennebohm Park:	
a.	Require dedication of additional parkland as part of any redevelopment of land adjoining the park that increases the number of residential dwelling units.	Planning, Parks
b.	Consider the following elements as part of future upgrades to the park: addition of a water play feature, improvements to the shelter, additional seating near the shelter/tennis courts/paths, and path lighting.	Traffic Engineering, Parks
C.	Provide pedestrian and bicycle access to the park from Sheboygan Avenue, potentially as part of additional north-south and east-west streets (see planned future streets in the Land Use and Transportation chapter).	Parks, Engineering, Traffic Engineering
3	Marshall Park: Increase access to the park from the south by constructing a pedestrian/bicycle connection to the park from Lake Mendota Drive/Camelot Drive. Note: the line shown on the Shared-Use Path & Bicycle Network map represents the connection but may not be the final route.	
4	Implement changes in Odana Hills Golf Course and adjacent park land to:	Parks, Engineering
a.	Reinvest in the Course in a manner that is aligned with the Task Force on Municipal Golf's recommendations to provide a sustainable, high-quality 18-hole course that is environmentally and financially sustainable and inclusive of year-round diverse golf park programming.	
b.	Work to integrate stormwater management into the Course to protect neighboring properties, enhance the natural environment, and build off of volunteer-driven efforts to promote a more sustainable landscape.	

C.	Replace the existing clubhouse with a year-round, fully accessible facility that serves a broader public purpose beyond golf.	
d.	Take a holistic approach to managing Odana Hills Park (west), Odana Hills Park (east), and Odana Hills Golf Course to expand the variety of recreational opportunities available throughout the combined properties.	
5	Zook Park: Increase connectivity between the park, surrounding neighborhood, and the Southwest Commuter Path via a paved path.	Engineering, Parks
6	Initiate Park Master Plans within the next 10 years for community parks in the planning area (Garner, Marshall, Rennebohm, Walnut Grove, Wexford).	Parks
7	Implement <u>adopted Park Development Plans</u> for neighborhood and mini parks over the next 15 years.	Parks
8	Improve public lake access at lake access courts and parks north of Lake Mendota Drive.	Parks
a.	Construct an ADA-compliant accessible path at the most appropriate location to increase inclusive lake access.	
b.	Add signage (in addition to the existing standard green street signs) to indicate public lake access.	
C.	Work with the Ho-Chunk Nation to place educational plaques at courts and/or along Lake Mendota Drive to describe and illustrate the history of the Ho-Chunk Nation in the area.	
9	Create a small neighborhood park with a variety of outdoor recreation opportunities south and west of University Avenue and west of Spring Harbor Drive to serve this part of the West Area, which is underserved by existing parks.	Parks
10	Look for opportunities to install recreational biking facilities in the Area, such as skill tracks, pump tracks, and single track biking facilities, in alignment with Madison Bicycle Adventure Trail (MadBAT) goals.	Parks
11	Create three new parks that are a minimum of five acres each to serve future residents in redeveloping commercial areas. The General Future Park Area overlay shown on the map designates the general area within which a smaller portion of the land will be sited for a park or open space in the future. The large general overlay is necessary because at this point it is not known where the park and open space will be sited.	Parks
a.	One park should be as centrally located as possible amongst any future redevelopment of West Towne Mall and the big box stores to the west of the mall. While the block containing the Von Maur site would be ideal if the mall were redeveloped all at once, Von Maur is the most recent investment in the area and therefore likely to remain over the mid- or long-term. Alternative locations are shown within the overall western "general future park area" labeled as "A" on the Parks and Open Space Map. This park may need to be two smaller parcels connected by a green corridor.	Parks
b.	An urban feel, like Lisa Link Peace Park or McPike Park, should be considered for the western park (A on the map), due to its location amidst more intensive planned mixed-use development and the possibility of serving residents, patrons of nearby businesses, and visitors to the area. A design that can accommodate special events and programming, such as farmers' markets, festivals, and concerts is encouraged.	Parks
C.	Two other parks should be created between the stormwater ponds and Oakwood Village. See the eastern green hatched area labeled as "B" on the Parks and Open Space Map for the generally acceptable location for these parks. The Parks Division may opt to proceed with one larger park, if that is found to be preferable based on property availability.	Parks
d.	Park space should be acquired through purchase of property and through parkland dedication which is required of new residential development. Parkland dedication may be required of projects when they are proposed adjacent to dedicated parkland in either area A or area B on the map.	Parks

e.	The eastern park(s) (B on the map) may be developed with a focus on serving the local neighborhood and would ideally include park space adjacent to the major stormwater facility south of Mineral Point Road and west of Gammon Place.	Parks
f.	Future designs and amenities for new park land will be determined via park development planning processes undertaken by the Parks Division. Such a planning process will proactively engage a diverse group of residents, including students from nearby Ezekiel Gillespie and Vel Phillips Memorial schools. The park planning process will consider amenities and park names that are welcoming to BIPOC residents.	Parks
12	Enhance the tree canopy on public right-of-way. The biggest opportunity for increasing street tree canopy on existing streets is within University Research Park.	Engineering, Traffic Engineering
13	As recommended in the Complete Green Streets Guide, terrace widths on new streets planned for the area should be wide enough to accommodate healthy canopy trees and/ or bioswales, typically requiring a minimum of 10 feet. Expansion of existing substandard terraces (generally less than eight feet) should be accommodated through additional dedication of right-of-way whenever possible.	Traffic Engineering, Engineering
14	Work with Oakwood Village to explore the prospects of an official, publicly accessible "green corridor" connecting Nautilus Point Park through the Oakwood campus to planned City park space in the Grand Canyon Dr. – Yellowstone Dr. area and to the stormwater ponds to the west.	Engineering
15	Evaluate stormwater utility parcels for plantings and ecological restoration consistent with the Stormwater Utility Vegetation Management Plan once adopted.	Engineering
16	Implement recommendations included in the City's watershed studies to reduce flooding and create a more resilient stormwater management system. Pursue federal funding for stormwater management improvements in watersheds that face the most severe challenges, such as Willow Creek.	Engineering
17	Focus City funding for public and private green infrastructure projects on watersheds that face the most severe challenges, such as Willow Creek and Spring Harbor, or in areas near the lakeshores or have concerns with water quality. Pair funding with increased publicity efforts surrounding City initiatives to boost green infrastructure on private property.	Engineering
18	Implement innovative stormwater management techniques as part of future right-of-way design for both new street construction planned for the West Towne area if/when any redevelopment occurs, other new planned streets in University Research Park, and reconstruction of existing streets.	Engineering
19	Projects in the four planned blocks directly south of the entrance to Sunset Memory Gardens should explore placing stormwater management measures mid-block, as a contiguous feature with accompanying mid-block pedestrian connections (see the "Mid-Block Passage" labeled "C" on the Parks and Open Space Map).	Engineering
20	Consider integration of stormwater management into future park space for large storms as a natural feature in a way that does not compromise the amenities and use of the park outside of large storm events.	Engineering
21	Work with the City of Middleton on cross-border stormwater management issues, especially regarding the Pheasant Branch watershed and Stricker Pond.	Engineering
22	Work with the Village of Shorewood Hills on improving stormwater management in the Willow Creek watershed to reduce incidences of flooding along University Avenue and Midvale Boulevard in the face of increasing annual rainfall.	Engineering
23	Remove the north side of Vernon Boulevard in favor of additional greenspace for a community garden or civic space.	Engineering

	Green and Resilient Partnerships	Agencies
A	While the City has a significant role to increase the West Area's canopy coverage from its existing 31% to the 40% goal established in the Urban Forestry Task Force's report through plantings on public property, the increased coverage will not be achieved without action on private property. The City should work with major property owners in the West Area, such as MMSD and University Research Park, to increase canopy on non-City land.	
В	Explore partnership opportunities with community groups to create additional community gardens, especially in proximity to concentrations of multifamily housing.	
	Effective Government Actions	Agencies
1	Review sanitary sewer capacity needs related to anticipated development/redevelopment and pair capacity upgrades with road construction projects where possible.	Engineering
2	Study water utility service area #8 (West Towne area and north of West Towne) to determine whether a new well is needed to serve the area.	Water Utility
	Effective Government Partnerships	Agencies
Α	Work with MMSD and the Lussier Community Education Center to facilitate expansion of Lussier to provide additional community space and services to area residents and students.	MMSD, Lussier Community Education Center
В	Explore opportunities to secure/reserve space to serve as a community center, neighborhood center, or meeting space for future residents of a new Odana neighborhood, potentially in conjunction with, or close to, new park space.	Community Development, Planning
	Health and Safety Actions	Agencies
1	Monitor driving behavior and speed and enforce traffic regulations on streets along school routes including N. Gammon Rd., Tree Ln., Old Sauk Rd., Rosa Rd., Segoe Rd., and other areas where people walk frequently and feel endangered and/or are experiencing higher levels of serious traffic related injuries and fatalities (see the High Injury Network map).	West District Police
2	Conduct Vision Zero speed analyses on Indian Hills, High Point Rd., Tree Ln., Whitney Way., Mineral Point Rd., Old Sauk Rd., Rosa Rd. and Segoe Rd. and explore potential traffic calming improvements such as road diets and speed bumps; pair these actions with speed enforcement.	West District Police, Traffic Engineering
3	Increase opportunities for West District Police and community members to engage and build trust with residents of color and people living with lower incomes including the Tree Ln., Oakbridge and Wexford Ridge areas. Activities could include Coffee with a Cop, and Police participation with youth recreation programs, block parties and cultural events and activities.	West District Police
4	Include the Tree Ln. and Wexford Ridge neighborhood as a potential new Neighborhood Resource Team area to improve equitable outcomes by focusing City resources and building stronger community connections for the growing population of people of color and those living with lower incomes.	Department of Civil Rights
5	Encourage new buildings within 200 feet of the Beltline to use materials and insulation that keep interior noise levels below 52 decibels as per state code TRANS 405. Design sites to shield outdoor spaces from noise above 67 decibels. Developments near the Beltline should follow setback and noise regulations outlined in Wisconsin statutes (TRANS 233, TRANS 405), and MGO Section 16.23.	Planning

Health and Safety Partnerships Agencies Α Work with MSCR and Lussier Community Education Center (LCEC) to improve access to health and recreation programs at MSCR Odana, LCEC, schools, and other locations such as walking/running groups, yoga, meditation, and healthy cooking and nutrition classes. В Work with resident leaders to continue to monitor environmental factors that may contribute to, or protect against violence (e.g. dimly lit areas, abandoned buildings, increased green space, etc.) and use violence prevention resources such as the Madison Dane County Violence Prevention: A Roadmap to Reducing Violence Report to address related issues. C Work with the National Interscholastic Cycling Association and other organizations that offer recreational activities outside of schools, to welcome, encourage, and support participation by middle and high school students of color. D Expand options in the Area where residents without air conditioning can go to get relief from the heat by partnering with LCEC, area businesses, centers of worship, and others to establish them as City-recognized cooling sites. Ε Partner with Pepartnership Inc. to establish a personal essentials pantry in the Tree Ln. and Wexford Neighborhood area. F Create partnerships between neighborhood groups, YMCA, MSCR and area schools to establish youth recreation programs at Haen Family Park and nearby school open space for activities like flag football, basketball and soccer. G Work with residents, neighborhood groups, Madison Arts and non-profit organizations to activate West Area parks and other greenspaces in Oakbridge, Tree Ln., and Wexford Ridge areas for people of all ages with musical performances, community meals, arts activities and recreation.

Glossary

West Area Plan

Accessory dwelling unit: A second dwelling unit contained within a single-family dwelling or within a detached building located on the same lot as a single-family dwelling. This definition includes accessory buildings constructed in connection with a private garage or a private garage converted into a dwelling unit.

Activity Center: An intensively developed area that is the visual and/or functional center of a neighborhood(s) or a district. Activity centers are typically comprised of a mix of land uses developed at a higher intensity than the surrounding area including residential, commercial, employment, civic, institutional, and parks and open space uses.

Affordable housing: housing for which the occupant(s) are paying no more than 30 percent of gross household income for housing costs, including utilities. Households with costs exceeding 30% of income are considered housing cost-burdened. The City of Madison's owner-occupant and rental development subsidy programs are primarily designed to reduce housing cost burdens for renter households with incomes at or below 60% of Dane County's median income (CMI) and for owner-occupied households at or below 80% CMI.

Affordable Housing Fund: A City of Madison program to provide loans and grants to for-profit and non-profit housing developers for the construction of new affordable rental housing.

Berm: A linear mound generally built to screen views, define areas, or direct stormwater.

Bicycle share (B-Cycle): A kiosk-based bike rental system offering hourly, daily, or subscription-based usage.

Big box retail: A physically large retail store with at least 75,000 square feet of floor area and a regional sales market. They are usually part of a national or regional chain of stores. Some examples include Walmart, Target, Best Buy, and Home Depot. (Source: APA: A Planners Dictionary)

BIPOC: An umbrella term for people of color, which stands for Black, Indigenous, and people of color. The term acknowledges that not all people of color face equal levels of injustice, recognizing that Black and Indigenous people are severely impacted by systemic racial injustices. (Source: Merriam-Webster)

Bump out: An extension of a raised curb into a roadway, typically a parking lane, to create additional terrace or sidewalk space. This can be used to shorten pedestrian crossing distances or create additional space for street trees while encouraging lower vehicular speeds.

Bus Rapid Transit (BRT): is a frequent, faster, and more reliable bus system that uses larger buses to transport more riders. BRT's features include frequent all-day service and direct routes with fewer stops. It utilizes special traffic signals to help buses get through intersections faster, dedicated bus lanes, and stations with off-board fare payment kiosks.

Capital Area Regional Planning Commission (CARPC): One of nine commissions in Wisconsin established to coordinate planning and development among area municipalities. CARPC develops and promotes regional plans, provides objective information and professional planning services, and focuses local attention on issues of regional importance. CARPC carries out land use planning and areawide water quality management planning for the greater Madison region. State statutes charge it with the duty of preparing and adopting a master plan for the physical development of the region. The Department of Natural Resources contracts with the Commission to maintain a continuing areawide water quality management planning process to manage, protect, and enhance the water resources of the region. (Source: CARPC)

Commercial Core: Street frontages where future development proposals are required to include ground floor commercial spaces.

Community Development Block Grant (CDBG): A federal program which provides annual grants to states and cities for affordable housing, anti-poverty, infrastructure, and planning activities that benefit low- to moderate-income persons.

Community garden: An area of land or space managed and maintained by a group of individuals to grow and harvest food crops and/or non-food, ornamental crops, such as flowers, for personal or group use, consumption or donation.

Community land trust: nonprofit organizations whose primary objective is the creation of homes that remain permanently affordable, providing successful homeownership opportunities for generations of lower income families. Under the com-

munity land trust (CLT) model, homeowners purchase their house, but not the land (this lowers the purchase price). The land is leased by the homeowner from the land trust. When homeowners sell, 75% of the appreciated value stays with the house, so it's more affordable for the next buyer. The CLT homeowner also gets to keep the equity they invested into the home plus 25% of the increase in value. (Sources: Madison Area Community Land Trust and Grounded Solutions Network)

Commercial Ownership Assistance program: The City's Commercial Ownership Assistance Program (COA) is focused on helping business owners expand their enterprises by transitioning from renting space to owning commercial property for their business. Priority is given to applicants who are people of color, immigrants, women, the disabled, veterans and any other underrepresented groups.

Complete neighborhood: are neighborhoods that include a range of housing types and costs, neighborhood-serving businesses, stores, services, schools, and places of worship. These elements are ideally accessible by foot, bicycle, or transit through a network of well-connected streets and blocks, usable public spaces, and a system of connected parks, paths, and greenways.

Continental crosswalk: A crosswalk with a more visible type of striping (painting), consisting of thick white stripes parallel to the direction of vehicular travel.

Cost-burdened: a household is cost-burdened when they are paying too much for their home (when total housing costs exceed 30% of their gross monthly income).

Drumlin: A linear or oval-shaped hill created by the streamlined movement of glacial ice sheets across rock debris. (Source: Britannica)

Easement: A legal tool that grants one party the right to use property that another party owns and possesses. (Sources: Investopedia, Merriam-Webster Dictionary)

Exclusionary zoning: A term applied to zoning standards, districts, or policies that seek to prevent people of certain races, ethnicities, or income levels from buying homes or living in specific areas or neighborhoods. This could include extensive use of exclusively single-family districts, large minimum lot or open space sizes, and narrow occupancy (household size) rules. (Source: Planetizen)

Foreign Trade Zone (FTZ): a specific area within the United States, located in or near a port of entry, where certain types of merchandise can be imported without going through formal customs entry procedures or paying import duties. FTZs were designed to encourage international trade and U.S. employment. (Sources: Dane Country Regional Airport and Port of Seattle)

Frontage road: A street adjacent to a freeway, expressway, or arterial street separated therefrom by a dividing strip and providing access to abutting properties. (Source: APA: A Planners Dictionary)

Grade-separated intersection: an intersection where one roadway or lane travels above or below other roadways by separating where they cross each other with an overpass or underpass.

Greenway: Linear corridors of land and water and the natural, cultural, and recreational resources they link together. (Source: Massachusetts Office of Energy and Environmental Affairs)

Healthy Retail Access Program: A program created by Madison's Food Policy Council that provides funds for healthy retail projects that aim to improve access to affordable, healthy, and culturally appropriate food and retail within underserved areas.

Historic district: A significant concentration, linkage, or continuity of sites, buildings, structures, or objects united historically or aesthetically by plan or physical development. A local, state, or the federal government can officially recognize districts. (Source: U.S. National Park Service)

Housing cooperative: A residence that is collectively owned and controlled by its members: the people who live in the housing. The buildings typically have private bedrooms but shared spaces, such as common kitchens and recreation areas. Members often share responsibility for cooking, daily chores, and property maintenance.

Impervious surface: Any hard-surfaced area that does not readily absorb or retain water, including but not limited to building roofs, parking and driveway areas, graveled areas, sidewalks, and paved recreation areas. (Source: APA: A Planners Dictionary)

Income- and rent-restricted housing: a type of housing where a specific number of units in a building are reserved for low-income households. A qualifying household's income level must be at or below a specific level for that household's size, such as 60% of the county median income. A limit is also set on the monthly rent to ensure that housing is more affordable for those households, often due to some form of subsidy. These restrictions are typically enforced through a Land Use Restriction Agreement.

Incubator: An area, commercial space, and/or building designated for the cultivation and enhancement of new or future businesses.

Infill development: Development of vacant or underused lots that are surrounded by developed areas.

Land Banking: A City program used to acquire land and buildings that could be used for future economic development, affordable housing projects, and other City uses. The goal is to acquire strategic properties for future purposes that might include: assisting displaced businesses, reducing blight, stabilizing housing markets, improving the quality of life of residents and neighborhoods, and preserving land for City purposes.

Makerspace: A term used to describe a place where people gather to share resources and knowledge, work on projects, network, and build. This could include artist studios, small-scale fabrication spaces, workshops, commercial kitchens, or similar spaces.

Market-rate housing: Housing that does not have any restrictions on rent or household income.

Missing Middle Housing: A range of housing types scaled between single-family detached houses and larger apartment buildings. Housing types that are considered as part of the missing middle include duplexes, triplexes, four-units, row-houses, live-work buildings, accessory dwelling units (ADUs), clustered small homes like bungalow courts, and some small apartment buildings. The scale of missing middle housing is compatible with most existing single-family residential areas.

Mixed-use: A building or groups of buildings designed to encourage a diversity of compatible land uses, which include a mixture of two or more of the following uses: residential, office, retail, recreational, light industrial, and other miscellaneous uses. (Source: City of Beaverton, OR)

Neighborhood Development Plan (NDP): A plan prepared for largely undeveloped land on the city's edge. NDPs are adopted as supplements of the Comprehensive Plan and include recommendations for land use, transportation, parks and open space, and utilities.

Neighborhood Plan: A plan prepared for an already-developed area of the city that includes recommendations for land use, urban design, transportation, parks, placemaking, and other improvements/investments/changes to a given area. Neighborhood plans can encompass more than one neighborhood and are generally adopted as supplements to the Comprehensive Plan.

Neighborhood Resource Teams (NRTs): A citywide effort to coordinate and improve the delivery of City services to Madison's neighborhoods. NRTs provide a regular forum for City employees to meet, discuss, and support each other's efforts in delivering excellent City services. NRT membership can include alders, City staff, and non-City staff participants.

Official Map: A formal public record used to indicate where a government is likely to require right-of-way, easements, or land for future roads, drainageways, utilities, or recreation facilities. Within officially mapped areas, a property owner maintains control and use of their property but building permits cannot be issued. Official Mapping is established in Wisconsin State statute 62.23(6). (Source: UW Extension)

Operating costs: Expenses associated with the maintenance and administration of a business or government on a day-to-day basis, such as salaries. (Source: Investopedia)

Per- and polyfluoroalkyl substances (PFAS): A group of chemicals used to make products that resist heat, oil, stains, grease, and water. They are long lasting chemicals, which break down very slowly over time and exposure to them may be linked to harmful health effects. (Sources: US Environmental Protection Agency and Centers for Disease Control and Prevention)

Placemaking: Creation of an environment that fosters community, stimulates interaction, encourages entrepreneurship, generates innovation, and nurtures humanity. (Source: Project for Public Spaces)

Proactive rezoning: Proactive rezoning is when the City, rather than a property owner or developer, proposes to rezone land so that it is consistent with plan recommendations. When the City proposes to proactively rezone areas during a planning process, it informs property owners about the recommendation and whether it could affect them.

Property Tax Assistance for Seniors Program: A City program that pays all or a portion of property taxes for qualified homeowners who are over the age of 65 and own a single-family residence within the City of Madison.

Rectangular rapid flashing beacons (RRFBs): A flashing signal activated by pedestrians that alerts drivers to yield to crossing pedestrians.

Redevelopment: Construction of a new building where a building already exists.

Rental Rehab Loan Program: City of Madison program which offers financial assistance, including low-interest loans, for Madison property owners to renovate and improve rental housing.

SafeGrowth: A philosophy built on the belief that healthy and functioning small neighborhoods provide the safest way to build cities in the 21st Century. It is based on the premise that crime is best tackled within small neighborhoods by harnessing the creative energy of functioning neighborhood groups, by employing the latest crime prevention methods, and by adopting an annual SafeGrowth® Plan to address crime and fear. (Source: SafeGrowth.org)

SEED Program: A City of Madison program administered by the Madison Food Policy Council that provides grants to improve the local food system and make food more accessible to Madison residents.

Sense of Place: The characteristics of a location that make it readily recognizable as being unique and different from its surroundings and that provides a feeling of belonging to or being identified with that particular place. (Source: Scottsdale, AZ)

Setbacks: A building design where there are fewer stories closer to the lot line (for example, near sidewalks and adjacent properties) than the rest of the building.

Shared-use path: a path or lane shared by pedestrian, bicycle, and other non-motorized users.

Smaller-scale housing: Also referred to as missing middle housing, it is a range of smaller multi-unit or clustered housing types compatible in scale with single-family homes. (Source: Opticos Design, Inc.)

Stepbacks: A building design element that is typically applied to the upper-story of a development to establish compatibility with surrounding development and maintain a pedestrian-oriented scale. A stepback requires that any portion of a building above a certain height is further pushed-in towards the center of the property.

Stormwater: Untreated runoff from rainfall and snowmelt. It flows across impervious surfaces (such as streets), through fields, and over construction sites, crossing municipal boundaries and can carry contaminants to lakes and streams. (Source: Dane County Office of Lakes & Watersheds)

Tabletop crossing or intersection: An intersection where the roadway ramps up to the sidewalk level to create greater visibility for pedestrians, improve usability for individuals who are mobility impaired, and slow vehicles down. Also known as a raised crossing or intersection.

Tax Increment Financing (TIF): A governmental finance tool to provide funds to construct public infrastructure, promote development opportunities, and expand the tax base.

Terrace: The space between the sidewalk and the curb along a street.

Through movement: within an intersection, the through movement refers to vehicles or users going straight and not turning onto the cross street.

Transit-Oriented Development (TOD): Compact, walkable, mixed-use development that is centered on quality public transit. It typically includes a mix of housing, office, retail, neighborhood amenities, and other uses within walking distance of a transit station. TOD reduces the need for driving by creating compact, vibrant, walkable neighborhoods with convenient access to activities and destinations connected by transit.

Tree canopy: The part of a city or area that is shaded by trees; The layer of leaves, branches, and stems of trees that obscure the ground when viewed from above. (Source: Center for Watershed Protection)

Underrepresented groups: Groups of people with a common race, ethnicity, immigration status, age, income level, gender identity, or sexual orientation who have not typically participated in City decision-making processes corresponding with the

proportion of the population they comprise. These groups have often experienced discrimination or marginalization based on their identity.

Urban agriculture: The production of food for personal consumption, market sale, donation, or educational purposes within cities and suburbs.

Urban form: the patterns of building height and development intensity as well as the structural elements that define an area physically, such as natural features, transportation corridors, open space, public facilities, and other elements. (Source: City of Los Angeles)

Water quality: The condition of water, including its chemical, physical, and biological characteristics with respect to its expected use, for example, drinking, swimming, or fishing. (Source: Florida Brooks National Marine Sanctuary, Key West, Florida)

Watershed: Watersheds are an area of land that drain to the same location (the outlet).

Wayfinding: Wayfinding refers to information systems that guide people through a physical environment and enhance their understanding and experience of the space. (Source: The Society for Experiential Graphic Design)

Zoning Code: An ordinance that regulates land use, lot size, building placement, building height, and other aspects of the development of land.

January through June 2023 - Community Feedback Summary

COMMENT	FEEDBACK SOURCE	MULTIPLER*	AREA/DETAILS
LAND USE - Assets			
Lots of places to shop and eat	(KOF), (KOM), (CS)	17	Hilldale
Like the lack of congestion	(KOF)	3	
Like the views and vistas	(KOF), (KOM), (SHNA)	3	
Support infill redevelopment	(CS)	2	
LAND USE - Recommendations			
Want lower-density/lower-rise buildings	(KOF), (CS)	44	Odana, against multi-family housing, limit multi-story apartments that crowd sidewalks and limit views, avoid over development, want single family homes, keep under 4 stories, want less low-income housing, too many condos/apartments
Want more mixed-use	(KOF), (KOM), (CS)	28	rezone old commercial into mixed use, more development in Odana, want more food options in commercial spaces, fit economy within housing stock, combine small scale rentals into single family areas to prevent large scale rental from becoming run-down, more like Hilldale, use TIF funding, small business on ground floor of apartments
Want higher density	(KOF), (KOM), (CS)	21	against parking lots, less single family, more density along major roads, near BRT and activity centers
City should be beautiful	(CS)	7	·
Convert old commercial to new development	(CS)	7	West Towne
Prioritize people not cars	(KOF), (CS)	4	
Need corridor plans for major roads	(SHNA), (CS)	3	Transportation corridors
Preserve farmland	(CS)	2	
Transition to structured parking	(CS)	2	away from single family areas
TRANSPORTATION - Assets			
Like walkability	(KOF), (KOM), (CS)	32	around city, on quiet trails, to schools, shaded
Like existing bike connections	(KOF), (KOM), (CS)	14	Odana, around parks, bike trails accessible to housing
Like the bus service	(KOF), (CS)	9	BRT
Like the lowered speed limits; want more lowered	(KOF), (CS)	7	lower on Tree Lane and Westfield Rd, South Highlands Ave, Old Middleton
TRANSPORTATION - Recommendations			
Expand bus service	(KOF), (CS)	42	reach areas not along Route A, make closer stops to Tokay instead of ending on Odana, in low-income areas, redesign makes it too far to walk

Expand bike pathways	(KOF), (KOM), (CS), (OH)	36	Old Middleton Rd at University Ave ramp, to downtown, north-south connections, connecting to bus stops/BRT, to West Towne Mall, Southwest Commuter Path to Hill Farms, access to Beltline Path (Craig Ave to Huegel Elementary), prioritize in high traffic areas, Whitney Way, Want Segoe to be predominately biking road (with separated lanes), trails from near west to far west, fill in gaps, connect existing paths, all age pathways,
Safety for bikers and pedestrians	(KOF), (KOM), (CS)	34	buffers between cars and bikes, buffers between bikes and people, reduce bike barriers, more off road paths, pedestrian safety on sidewalks (from bikers, skate boards, scooters), bikers don't stop at stop signs
Need improved safety at intersections and crosswalks for bikes & pedestrians	(KOF), (KOM), (CS)	24	Indiana Hills, High Point Road, Mineral Point, Whitney Way and Segoe
Expand walkability	(KOF), (CS)	21	West Towne Mall, to parks, BRT near employment and residential, Hill Farms
Reduce traffic	(KOF), (KOM), (CS)	14	Reduce speeds and lanes on arterial roads; put pedestrians, bikes, and transit first
Decrease car lanes	(CS)	7	
Sidewalk improvement and infill	(KOF), (CS)	6	High Point Road
Concern over lack of parking	(KOM), (CS)	5	
Street Improvements	(KOM), (CS)	6	Spring Harbor St, Old Sauk Rd, South Highlands Ave (potholes)
Overboard on speed limit restriction with minimal enforcement	(CS)	3	
Car Intersection improvements	(CS)	2	Left hand turn at Old Sauk and High Point, need wide enough lanes for safety
Sidewalks comes with downsides	(CS)	1	Cut mature trees, little car traffic can manage bikers and walkers without dedicated lanes
Decrease stoplights and pedestrian crossings	(CS)	1	Routes to downtown, particularly University Ave
Increase Metro access for people with disabilities	(CS)	1	
NEIGHBORHOODS & HOUSING - Assets			
Good diversity of current housing stock	(KOF), (CS)	6	keep the single family homes
Like the amount of affordable housing	(KOF), (CS)	4	
Like mix of residential and commercial sites	(CS)	1	good balance
Like ample amount of lot size	(CS)	1	
Like the Sequoya Commons	(KOF)	1	
NEIGHBORHOODS & HOUSING - Reccomendations			
Want more affordability	(KOF), (CS)	63	address property taxes, along bus lines, want it to be evenly distributed across city, not just concentrated in certain areas, for seniors, outside of Section 8 program
Want more diverse housing stock	(KOF), (CS)	17	ADUs, Townhouses and condos, missing middle, multi-family, senior options, single family

Need to increase housing development	(KOF), (KOM), (CS)	16	Along transit corridors, Hilldale, Odana, redevelopment of mall, convert non-active businesses/churches to more housing (Whitney Way, Copps), a must in the right areas, TOD is too broad of a brush, incentivize development in areas that can better accommodate it vs. an incremental increase (SF to MF), on vacant or underdeveloped commercial parcels
Want more interesting building facades	(KOF), (CS)	12	commercial and residential
Against multi-family buildings	(KOF), (CS)	6	Crime, not enough green spaces, too much multi-residential, limit public housing, want single family homes predominantly
Don't want buildings set right up to the sidewalk	(KOF), (KOM), (SHNA), (CS)	4	
More subsidized housing that is well managed	(CS)	2	Need low income housing
Less subsidized housing	(CS)	2	crime concerns
Air B&Bs taking up too much space	(KOF)	1	
Redevelop existing offices and apartments	(KOM)	1	
Increase updates to apartment buildings	(CS)	1	encourage apartment owners to update to heat pumps & higher building insulation & window standards for existing buildings
GREEN & RESILIENT - Recommendations			
Sustainability in all aspects	(KOM), (CS)	30	incentivize private owners of massive paved parking lots to install underground holding tanks, permeable pavement & rain gardens, improve recycling, city wide composting, include environmental learning standards in school, fight climate change
Plant more trees	(CS)	26	education on importance of trees (value, cooling, carbon capture)
Improve water quality and natural areas/habitats	(CS)	21	protect natural growth, improve lake conditions, leaf pick up, clean and safe drinking water
Preserve animal habitats	(CS)	2	
Address climate change and storm water runoff	(KOF), (KOM), (CS), (OH)	18	Hill farms, use natural storm water management, flooding issues on Kenosha, Research Park, better storm water collection
Reduce pollution	(CS)	10	chemicals in parks and on lawns, no gas lawn equipment, reduce salt use, PFAS
Renewable energy	(CS)	9	
Want less light pollution	(KOM), (CS)	8	concern over lighting in parks that will disturb natural areas, encourage Dark Sky outdoor lighting
Streets should be an ecosystem	(KOF), (CS), (SHNA)	6	maintain tree cover, replant trees that are removed, native flowers in medians, encourage native plantings in people's yards
Bring gardens closer to residents through schools, backyards, and parks	(KOF), (CS)	2	
Add nature barriers	(KOM), (CS)	3	between housing and beltline, between public and private properties
Existing housing stock is old and energy inefficient	(KOF)	1	
Increase Resiliency	(CS)	1	better infrastructure now to protect the community
PARKS & GREENSPACE - Assets			

Like the current parks/greenspaces	(KOF), (KOM), (CS)	105	Spring Harbor, open spaces, Walnut Grove park, Sauk Creek, Kenosha Parkway, Glen oaks, Bordner park, dog park, greenways, Owen Conservancy, Garner,
Like the amount of trees	(KOF), (KOM), (CS)	38	
Like being close to the lake, ponds, parks, and greenways	(KOF), (CS)	9	Kettle pond- connection to nature, like access to parks, Access to university and lakeshore preserve
Nature programing	(KOM)	2	ice rinks in winter, activities in Rennebohm Park
PARKS & GREENSPACE - Reccomendations			
Increase/preserve greenspaces and parks in dense areas	(KOF), (KOM), (CS)	79	maintain current parks, create pocket parks, willing to up-zone to get more parks, more trees at Garner Park, new park in the area of Odana Rd., make Garner Park a regional Central Park, more no mow areas
Concern about Sauk Creek Greenway reconstruction (bike path and tree removal)	(CS)	69	want to keep trees, would be a hazard to pedestrians (kids, elderly, dogs), would harm wildlife, concerned about home valuation, don't want bike path, want Greenway to be quiet place to explore and think bike paths should be closer to roads (Farmington and Sauk Creek Drive), acknowledge that storm water needs to be addressed, new bike route near Walnut Grove ahead of, during, and after construction
Park amenity improvements and maintenance	(KOF), (KOM), (CS)	68	more accessible bathrooms, more shade, more drinking water with bottle fillers (new and replacements), more seating, seating in shade, add nature playgrounds, pickle ball, improved tennis court, mobility accommodations for older adults in parks and trails, dog parks, maximize open space, intergenerational playgrounds, permanent infrastructure in Bordner Park and Glen Canyon Park, ice rinks, volleyball courts, partner with neighborhood associations and residents to take care of parks, kayak/canoe launch at Spring harbor, Skyline park playground (dangerous chipped areas), recycle bins
Against tree removal	(KOF), (CS)	30	Kenosha greenway
Environmental stewardship	(KOF), (KOM), (CS)	25	Newberry Park & Research Park; add native vegetation & access to conservancy areas, Reduce invasive species (Sauk Creek Greenway), Address overgrown area and stagnant water on Julia St, preservation of Sauk Creek woods, Protect existing habitats and wildlife, concerned about Garner woods, clear wild parsnip near waterways, reduce chemical treatments in parks
Want more connected and intentional greenways	(KOF), (KOM), (CS)	15	Connect parks through bike trails, make Odana Hills golf course more multi-functional, more multi-functional walking paths through greenways, more mountain bike trails through greenways and along paths
Don't divide parks with bike trails	(KOF), (CS)	15	Walnut Grove
Want Sauk Creek greenway reconstruction	(CS)	6	want trees to be replanted near walking path
Need parking at Walnut Grove	(KOF)	1	

ECONOMY & OPPORTUNITY - Assets			
Proximity to downtown amenities	(CS)	23	
Like the quality of facilities	(KOF), (KOM), (CS)	12	schools, libraries, Urgent care, BRT, garden club
Locations of businesses	(KOF), (CS)	11	Hilldale, convenient to the bus, grocery stores and Target
ECONOMY & OPPORTUNITY - Recommendations			
More restaurants, coffee shops, clinics	(CS)	20	want more unique development: rooftops, More development around transit, add green spaces around businesses. build more businesses/cafes into neighborhoods
Fill in vacant commercial spaces with small scale businesses and community resources	(KOF), (KOM), (CS)	16	need more childcare, more restaurants and coffee shops, less CBD, liquor stores, and chiropractors - want more community supported services, more shared spaces
Food access	(KOF), (KOM), (CS)	9	more farmers markets, walkable food access in Sequoya Commons, restaurants, cafes, and food trucks near BRT stops, incentivize grocery stores and healthy food options to expand to keep up with growing population, more restaurants and cafes, grocery stores within biking distance
Support marginalized and local business owners/workers	(CS)	6	support marginalized entrepreneurs through cutting red tape and offering support of city navigators, MBE, WBE, DBE, hire handicapped
More schools	(CS)	5	
Concern over commercial displacement	(KOM), (CS)	4	support business owners, support small businesses, find location for small offices
Internet	(KOF), (CS)	3	Make work from home easier, municipal internet or improve private coverage
More employment	(CS)	3	near new/dense housing, one of few places to add employment at scale
HEALTH & SAFTEY - Assets			
Quiet	(CS)	23	
Safe and low-crime	(KOF), (KOM), (CS)	16	
Like that much of the area is close to medical services	(CS)	1	
HEALTH & SAFTEY - Recommendations			
Speeding and vehicle accidents	(CS)	42	reduce speeding: Whitney and Mineral Point, Odana, Old Sauk Road, stop DUIs
Reduce crime	(CS)	33	against guns, want more cops with body cams, lighted sidewalks, pathways, and parking, support police and first responders, less crime on pathways/bike trails in Tree Lane Apt and Wexford Ridge area, concern over bike paths near residential homes
Community services	(KOM), (CS)	16	alternatives to police, will need more community services if new families are brought in, address "panhandlers" at intersections, Good Neighbor Project, mental health crisis, social workers, help homeless, more community leaders

		1	
More police enforcement	(CS)	7	Tree Lane Housing, Walgreens and Kwik Trip, need officers building rapport in neighborhoods - driving squad cars feels isolating
Enforcement of building codes	(CS)	1	Fire safety
Walkability promotes health & safety	(CS)	1	
CULTURE & CHARACTER - Assets			
Good people in community	(KOF), (KOM), (CS)	33	
Like the charming character/suburban feel	(KOF), (CS)	15	
Like diversity of people	(CS)	7	
CULTURE & CHARACTER - Recommendations			
Want more local/community involvement	(CS)	18	maintaining parks, health and safety, in development decisions, in creation of Sauk Creek Plans
Maintain character of neighborhood	(KOF), (CS)	6	need to preserve history while looking to future
Want more support	(CS)	7	childcare, living expenses, job training, elder care, community run commercial facilities, to meet family needs
Want more diversity in neighborhood	(CS)	6	
Want more cultural facilities and art	(KOF), (CS)	5	libraries, schools
City is more focused on development than quality of life	(KOF), (CS)	4	promote local developers who care about the community
Want more DEI in planning practices	(CS)	3	
Want attention on place making	(CS)	1	
Want more youth activities	(CS)	11	

Phase 1 Engagement Feedback

(KOF) Virtual Meetings February 6, 12pm & February 13, 6pm

(KOM) In- Person Meeting March 9

(SHNA) Spring Harbor Neighborhood Association Meeting April 27

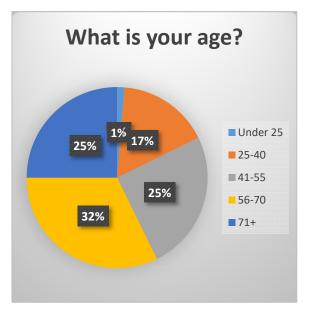
(OH) Open House- May 10 Written Comments

(OCM) Online Commenting Map Comments

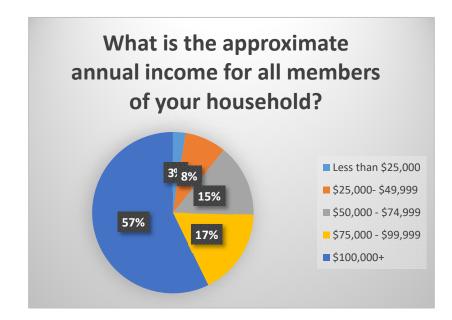
(CS) Community Survey Comments

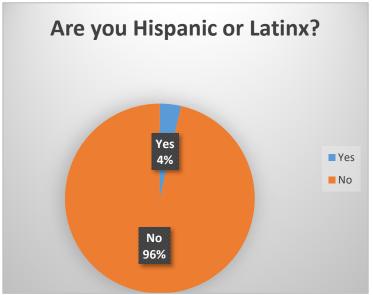
*Note: The multiplier refers to the number of time the comment was shared.

Demographic questions were asked of community survey respondents. Those are summarized here, and are generally consistent with the virtual and in-person meetings of February and March.









6/30/2023

Phase 2: mid-July 2023-August 2023. Staff has reviewed, categorized, and summarized feedback received thus far from various methods of public engagement (see the key at the end of the summary for a full description of the feedback source abbreviations). The "multiplier" column indicates how many times similar comments were received.

West Area Plan - Community Feedback Summary

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Land Use and Transportation: Compact Land Efficient Transportation			
Expand bike pathways/protected lanes)	(LYA), (EVM), (LVM), (RPM), (CS), (CP)	92	Connecting Madison bike infrastructure to Middleton on Gammon Rd, Midvale, behind Odana golf course, connect good neighborhood path to old Middleton, both sides of Bluff St, N-S connection to Odana and southwest path, Hamersley, Van Hise, Hamilton
Expand walkability	(LYA), (EVM), (RPM), (CS), (CP)	53	
Want more density	(EVM), (RPM), (CS)	52	Along transit lines, convert more LR to LMR, reduce parking, more LMR, MR, and NMU instead of LR
Safety for bikers and pedestrians	(EVM), (RPM), (CS),(CP)	48	Bikers need to obey traffic laws, keep cars out of bike/turning lanes, dedicated bike lane on Hamersley, reduce on-street parking, prefer protected bike lanes to shared lanes, Nakoma curb cuts are dangerous, more all ages and abilities, more lighting at S Gammon Rd, High Point Rd, Beltline and adjacent areas
Expand bus service	(CS), (CP)	40	New service doesn't meet some needs, should be expanded to low-income/high density areas, expand east-west connections, concerns about number of bus stops in area
Want more mixed-use	(RPM), (CS)	35	Activate ground floors, corner of regent/Whitney Way, along Midvale, Odana, want walkable spaces, don't put in areas of low development density
Speed limit reduction and enforcement	(EVM), (RPM), (CS)	32	Road diets and speed bumps, Tree Lane speeds, 20 is plenty, Whitney Way, Rosa Rd., Mineral Point Road, Segoe, Old Sauk

No Sauk Creek Bicycle Route	(EVM), (RPM), (CS)	27	Don't want lights or concrete path, distribution of water management and natural habitat, preserve trees, afraid city won't maintain, steep incline at start of path would make dangerous, Tree Lane/High Point intersection dangerous, prioritize floodplain, creek, trees, and climate over maintenance or bike trails, dangerous to pedestrians walking in area
Intersection and crosswalk improvements	(EVM), (LVM), (RPM), (CS), (CP)	26	Midvale Ave/University Ave, University Ave West of Whitney way, Genomic/Mineral Pt, new cross streets along Mineral Pt, traffic signal at Regent and Segoe, difficult to cross Segoe, Odana west of Whitney Way, Old Middleton/ N Whitney Way, Karen Arms/Regent, Segoe and Hilltop, Whitney Way/Gilbert, South of Mineral Point Rd along Gammon Rd
Maximize low-density	(RPM), (CS)	19	More options for low density housing that isn't single family, balance of single and multi-family, pool and churches should go to LR or LMR not MR, more 2-3 story apartments
Prioritize people over cars	(EVM), (RPM), (CS)	18	
Sidewalk improvements	(LYA), (EVM), (LVM), (RPM), (CS), (CP)	13	Summit Woods, Odana, Hilltop, bury power lines for trees, increase snow removal in winter near bus stops, widen sidewalks, along Memorial
Reduce traffic	(CS)	12	traffic calming on Whitney Way and at Beltline interchanges
Street Improvements	(LYA), (RPM), (CP)	12	Road repairs needed on Old Sauk, keep traffic moving
More B-cycle stations	(LYA), (RPM)	11	
Support High Point Road Bike Connections	(LVM), (RPM)	9	Want one lane each way, prefer protected or off road (5), prefer shared lane (2)
Parking concerns	(RPM), (CS)	7	on-street parking: unsafe for bikers, blocking curbs and hydrants, if city decreases parking lanes, need to work with apartments to direct cars elsewhere
Support Sauk Creek Bicycle Route	(LVM), (RPM), (CS)	6	On street bicycle facilities will only be so safe, need alternative, restructure but support trees
Support Westfield Bicycle Route	(RPM), (CS)	6	Extend through mall and across beltline, want protected lanes and removal of parking
Concern over planned streets	(EVM)	1	
Expand bicycle wayfinding	(CS)	1	

No Westfield Bike Lanes	(RPM)	1	Don't want on-street parking removed
NEIGHBORHOODS & HOUSING: Complete Neighborhoods Housing			
Want more affordability	(RPM), (CS), (CP)	49	Affordability only in certain areas will segregate people
Want more diverse housing stock	(EVM), (CS)	25	condos, duplexes, rent to own, co-op, ADU, diverse facades, missing middle
More housing development	(EVM), (CS)	20	
Require setbacks and natural buffers	(RPM)	3	Additional setbacks for residential units, need plant buffer between high and low zones
ECONOMY & OPPORTUNITY: Growing Economy Equitable Education and Advancement			
More businesses like retail and restaurants	(LYA), (CS)	35	
Support marginalized and small business owners/workers	(RPM), (CS)	16	Food trucks
More accessible options for employment for adults and youth	(LYA), (CP)	10	More youth internships, jobs, opportunities
CULTURE & CHARACTER: Cultural Vibrance Unique Character			
Inclusive community events and spaces	(LYA), (CP)	10	Expand Lussier Community Education Center as it is important to community and offers a lot of series to community and needs more space to better serve community
Youth facilities and activities	(LYA), (CP)	10	
Bring food trucks to parks	(LYA)	10	
Aquariums	(LYA)	10	
Maintain community and welcoming culture	(EVM), (RPM), (CS)	6	Midvale Heights,
Want more diversity in neighborhood	(CS)	5	
Community Resources	(CP)	1	Lack of resources and information dissemination in west area
GREEN & RESILIENCE: Natural Resources Parks and Recreation			
Increase/preserve greenspaces and parks in dense areas	(LYA), (RPM), (CS)	62	Add more playgrounds
More tree canopy cover	(LYA), (EVM), (RPM), (CS)	51	Bury power lines to make room, want city to provide trees for free, more trees needed around west towne mall
Address climate crisis	(RPM), (CS)	33	
Park amenity improvements and maintenance	(EVM), (CS), (CP)	25	Add shelters to parks that don't have them already, more parks for dogs, public bathroom upgrades, more amenities at Garner Park for families and children
Improve water quality and natural areas/habitats	(CS)	20	
Maintenance and protection of greenways	(LVM), (RPM), (CS)	18	need more maintenance in over grown areas

Stormwater Management	(RPM), (CS)	15	natural methods, concerned about flood control from new development (Brennan, harbor view, UW clinic)
Reduce pollution	(CS)	12	PFAS, road salt, air quality
Energy efficiency	(CS)	12	homes, electric vehicle charging, help residence install solar panels, tax programs for businesses
Cleaner beaches	(LYA)	10	
More dog friendly trails and spaces	(LYA)	10	
Native plantings in parks and greenspaces	(CS)	9	
Want more connected and intentional parks and greenways	(EVM), (LVM), (CS)	8	Like advocated for in 1961 plan, only purpose is not stormwater management
Sustainable development	(CS)	7	Green architecture, LEED qualities
Urban Agriculture	(LVM), (CS)	3	Community garden, edible trees and vegetation in parks,
EFFECTIVE GOVERNMENT: Efficient Services Regional Cooperation Community Facilities			
Improved police enforcement	(CS)	17	Regular and community policing, body cams
Reduce crime	(CS)	13	stolen cars, gun violence
Expand Lussier Community Center	(LYA)	15	
More fire stations	(LYA)	10	
Teenage clubs for sports (iceskating and rollerblading), games, and entertainment	(LYA)	10	
Need homeless shelter; animal shelters	(LYA)	10	
Increase lighting	(LYA)	10	Between Memorial HS and residential south of Beltline (underpass)
Diverse Participation	(EVM), (CS)	3	Survey/public meeting participants are self selected and majorly represent older, wealthy people- Need more representation
Incorporate underlying plans	(EVM)	1	
More libraries	(RPM)	1	In high densities and for transit dependent populations (Sheboygan and BRT lines)

Phase 2 Engagement

(LYA) Youth Action Interns - July 18

(EVM) Virtual Meeting July 18 (evening)

(LVM) Virtual Meeting July 26 (noon)

(RPM) In- Person Meeting/Open House August 2 (evening)

(CS) Community Survey Comments

(CP) Community Partner Comments (The West Area Plan hired Community Partners to focus and highlight feedback from underrepresented communities during this planning process. Partners reached over 275+

Phase 3: August 2023-April 19, 2024

Staff has reviewed, categorized, and summarized public feedback received from August 2023 to April 19, 2024 through the activities shown in the legend at the bottom of the document. The legend also includes abbreviations used within the document, to identify where the various comments came from.*

Public comments received during Board, Committee and Commission meetings and third-party gatherings like neighborhood association meetings, friends groups, and other activities are not included. Board, Committee and Commission meeting recordings are available via the West Area Plan project webpage.

The "multiplier" column approximates how many times similar comments were received. There were over 1000+ online comments received. In certain cases, individuals may have submitted the same or similar comment multiple times. If the comment is not within the scope of what the West Plan covers, staff shared the comments with the appropriate city agency.

*The details provided below are only from comments directly received by the City. There have been external community engagement efforts done like the Highlands Community Survey of property owner households and a Sauk Creek Greenway survey regarding the proposed shared-use path and the results of those surveys are not included below.

West Area Plan - Community Feedback Summary

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Land Use			
Support increased density/housing density	(EIM12) (LCD) (EF) (OC) (EVM18)	64	Like redeveloping commercial areas to increase housing density, remove barriers to building multifamily buildings, this just forces property owners to get special approval for rezoning and variances, which only deters them from constructing the desperately needed housing, too much LR land use shown & not enough mixed-use, require more condos/townhomes, there's no excuse for so much of area to be exclusively single family, allow mixed use, there's no way that this little amount of suggested rezoning by the planning department would ever need meet the growth needs of the area, consider rezoning areas along arterials to include mid density housing and mixed use, more efficient use of resources and less environmental impact, bring more diversity to neighborhoods, allow duplex, triplex, and rowhouses in single-family areas, stop protecting low density SF, Plan does not go far enough to increase MF housing, increase density near shared use path to provide destinations and reason to use paths, promote walkability and bikeability, suggested locations for density: near Garner Park/Segoe corridor (LMR)/Hilldale/Gammon Rd/Old Sauk Rd/Odana Rd/Tokay Blvd/West Towne area (where folks won't complain about it being in their backyard)/University/Midvale
Oppose higher density	(EIM12) (EF) (OC) (EVM18) (HFNA)	19	Do not want higher density of residents, multi-family units impacts safety of current residents and changes character of neighborhood, no development that impacts schools, higher density within built up parts of city puts too much pressure on transportation infrastructure and drives down property values, light pollution, traffic, sewer, water, noise, parking, don't need more 10+ story buildings, people want single-family houses, areas already congested, concerns development will eliminate tree canopy, locations with concerns: Old Sauk, University Ave, Whitney Way, Old Middleton Road

1

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Old Sauk Rd.	(OC) (EVM5)	4	Agree with all actions that allow missing middle housing, remove escalator clause for LR, LMR and MR designations in Old Sauk Rd. area, add a "House-like development pattern" map note (similar to the Isthmus) for LMR on Old Sauk Rd farm property, don't build an apt. complex on Old Sauk Road, the development information mtg indicated the development will require residents to pay for onsite parking-this will push residents to park on nearby residential streets
Oppose West Plan Height Map	(OC)	2	Keep residential areas lower height and in-style with the residential neighborhood. Location: south side of Sheboygan Ave
Support West Plan Height Map	(EF) (OC)	9	Locations: West Towne Mall (increase heights), Western edge of Eau Claire Ave (2-5 stories), Buffalo Trl. (2 stories to protect single-family housing), SW corner of Mineral Point Road and Rosa Rd, Red Cross site
Support Hill Farms pool rezone [staff note: a change to GFLU, not zoning, is shown in public review draft]	(OC)	1	Homeowner in Hill Farms-fully support changes in zoning to increase housing density. Madison needs more housing and there's plenty of space in this part of town.
Oppose Hill Farms Pool rezone [staff note: a change to GFLU, not zoning is shown in public review draft]	(EIM12) (EF) (OC)	19	Take up rezoning at the time of an actual proposal, it is not close to transit or major streets, far from the nearest multifamily development (across a wide empty marsh), there are SF homes here not apartments, need more public pools, should be designated as park/green space
Approve expanding mixed use in residential areas	(LCD) (EF) (EVM18) (EVM5)	17	Want closer amenities, create walkable neighborhoods, integrate residential and commercial, allow heterogeneity along arterial streets, will bring better quality of life, likely improve many homeowners' property values, great way to reduce carbon emissions, want small apartment buildings/mixed use like Sequoya Commons sprinkled all throughout the neighborhood, stop protecting low-density single-family development
Oppose expanding mixed use in residential areas	(EIM12) (EF) (OC)	8	Concern the change will: encourage the loss of single family homes and churches, result in less options for homeownership, encourage developers to buy out current homes, increase traffic, overcrowd schools, less parking on the street, more noise, more light pollution, bring more problems, impact the quality of life of residents in area and health of the neighborhood, cause possible stormwater issues, lose privacy with buildings overlooking yards/homes, bring down property values, negatively affect those in lower social economic status ability to own home, plans should not focus on intensive mixed use development but affordable owner occupied housing for community service professionals, should focus using underutilized commercial spaces for additional housing density, maintain single family residential neighborhoods
Support changing single-family areas	(OC)	5	Areas around Hilldale ripe for upzoning - there's no excuse for so much of that area to be exclusively single family, Odana and Tokay need to be upzoned from single family to mid density and mixed use, there's way too much detached single family housing along major roads/arterials, consider rezoning areas along arterials to include mid density housing and mixed use, LR = selfish

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Oppose changing single-family areas	(EF) (HFNA) (OC) (CFG)	15	Preserve and encourage single family housing and ownership, do not want higher height development next to single family homes, rental units do not contribute to an investment to the neighborhood community, rental occupants are less likely to have and raise families with children, single family houses are more affordable for families, don't want high density in areas, use vacant or underdeveloped commercial or existing mixed use space that could accommodate med-high density residential instead; Locations: NMU near Hickory Dr. and University Ave., properties between Wynnwood Way and Whitney Way, Old Sauk Rd (stop escalator clause), generally oppose changes from SF to NMU or higher density residential
Concern about proposed GFLU changes	(EF) (EIM12) (OC)	26	Concerns: how change will impact current property owners, trees/wildlife, creating more access points causing more traffic congestion, lower property values, create more problems, will create urban high density/overdevelopment, change character of neighborhood, will not encourage missing middle development. Locations with concerns: Low-Medium Residential for 600 block of Dearholt Rd- want LR with SR-C3 zoning potentially, NW Block at Whitney and Regent-LMR instead, Dearholt should remain LR, Hillcrest should not be MR, N Gammon (200 Block), Wynwood Way, Whitney Way, LR to LMR along northside of Old Sauk Rd., MR between Old Sauk Rd. and Gammon should remain LMR, MR along Rosa Rd and South Hill Drive - should not developed beyond R-1, MR on Gammon Rd and Colony Dr is overdevelopment and should be LR, Beltline and Hammersley, limit population density along Old Sauk between Gammon and Old Middleton due to 2 lane street, Crestwood Elementary - steep hill, keep MR along western edge of Eau Claire Ave between Sheboygan and Regent and be sensitive to redevelopment to protect single family homes along Buffalo Trl., MR at Segoe Rd and Mineral Point Rd
Support proposed GFLU changes	(EF) (OC) (EVM18)	10	Good to have more medium/high density residential, need to allow small stores in neighborhoods so they are walkable - too much driving is required to get basics/groceries in many developments, LMR for Old Sauk Rd but oppose using the escalator clause to increase density, expand NMU area at Old Sauk and Gammon
Increase proposed GFLU changes	(OC)	10	GFLU changes are small-support more, too much low residential, low residential should be eliminated entirely to allow higher densities, add more LMR along streets like Whitney Way, Mineral Point, Regent, etc., would like to see more mixed use within residential areas focused on social life like shops and cafes within low/medium residential areas, allow up to 5 stories and mixed use along Whitney Way, allow 4 stories and mixed use at Midvale intersections (like Sequoya Commons), allow 7 stories at south end of Midvale, more intensity along Whitney Way - area is currently monotonous

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Opposed/concern - proposed zoning changes	(EIM12) (OC) (AVM4)	26	Want development to not change character of neighborhood, parks should not be rezoned unless to specially promote their use as community space in neighborhoods, residents near any proposed rezoning should have say in advance of a developer proposing a project, 4 stories at Whitney/Regent is too tall, rather than proactively rezoning Area A - seems like land use change is better, rezone of Area E seems not to affect much - make sure it doesn't negatively affect YMCA and affordable childcare, opposed to blanket rezoning rules - city should be zoned to retain beautiful character this city has now, don't understand how some lots are selected and some are not, cuts homeowners out of zoning decisions, Locations of concerns: NW Block at Whitney and Regent (Single Family)/NW Block at Whitney and Wynnwood Way/Sheboygan Ave, Rezone Area D (parking concerns)/Rezone Area A
Support proposed zoning changes	(EIM12) (EF) (OC) (EVM18) (HFNA)	37	Regional Mixed Use shown in Proactive Rezone Map areas B and C, zoning plans emphasize densifying housing and service with focus on providing affordable and missing middle housing, help reduce climate warming emissions and environmental sustainability, help reach equity and inclusion goals by providing affordable housing of the City, help not push low-income people to outskirts of the city to find housing, rezoning to prepare for future growth is a good idea - better now than when the need is critical, Madison desperately needs it, hope zoning changes can help people afford to live and stay in Madison and support local businesses, support proactive rezoning at Regent & Whitney, support B, C, E as long as existing businesses do not end up paying high lease costs
Increase proposed zoning changes	(EF) (OC) (EVM18) (HFNA)	34	We should eliminate single family zoning like other cities, automatically approve upzones and transfers from single use to mixed use zoning, landowners should be able to densify by right, loosen zoning along throughfares to address ingrained racism and climate change, will allow more amenities, SR-C1 zoning mandates car use - we need more mixed-use neighborhoods for walkability, doesn't go far enough to allow for willing redevelopment, critical need now, entire city needs to be upzoned to allow more homes for people, support hierarchical zoning allowing housing in all zones, allow duplex, triplex, and rowhouses in single-family areas, all residential zoning should be increased by one level similar to TOD, single-family zoning is too restrictive, add more multi use zoning to allow for offices, restaurants, grocery stores, etc. to be near more people to allow for walkability and bikeability. Locations advocated for increases: upzoning generally along major bus and transit corridors, Midvale Heights to support more missing middle and mixed-use, Whitney Way corridor, area around Rennebohm, increase RMX along University Ave, upzone all SR-C1/SR-C2/SR-C3 to SR-V1 to support Missing Middle housing, golf courses to allow for living spaces, upzone Segoe Rd to allow rowhouses, Old Sauk area, upzone low density residential areas in SF zoning along Whitney Way from Regents to Mineral Point Rd., especially near intersections, single-family area next to higher density areas of Hill Farms and Hilldale, all of Whitney Way should be MR and mixed-use, residential area along northside of Odana Rd. between Dearholt and Wedgewood could become SR-C3 since it's near a grocery store

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Support rezoning or future land use change for churches [staff note: the initial public review draft had only proposed proactive rezoning for two sites; others were proposed for future land use map changes]	(EIM12) (EF) (EVM18) (AVM4) (HFNA)	10	If churches choose to leave, area should be Low-Medium residential to allow for townhomes or condos for homeownership, churches are near BRT, could hold off change until BRT has been running for some years, could help extend their stay because being rezoned allows them to take advantage of new financing, like mixed use recommendation to maintain community supporting area/third spaces, ensure that development is pedestrian-oriented and includes viable neighborhood businesses, to encourage this area to develop as a complete neighborhood (not just additional housing)
Oppose rezoning or future land use change for churches [staff note: the initial public review draft had only proposed proactive rezoning for two sites; others were proposed for future land use map changes]	(EIM12) (EF) (HFNA) (OC)	26	Concerns: churches will sell to developers and lead to no churches in the neighborhood, what will repurposing of churches will look like, how the change will impact the church financially or influence churches to move or stay, encourage loss of single family homes, will contribute to lack of opportunity for homeownership, parking, churches should be Low-Medium residential, take up rezoning at the time of an actual proposal, churches are community serving spaces, change in future land use unnecessary because the Comprehensive Plan already addresses it, single family or duplex homes with backyards generate a real feeling of long term residency and community and apartment buildings do not
Oppose rezone of Highlands	(EF) (OC) (MHCA)	30	Concerns: shrink minimum lot sizes, loss of historic layout and design of neighborhood - O.C. Simmonds design, impact tree canopy of neighborhood, alter building requirements, loss of park-like setting for residents and community members who enjoy walking through the neighborhood will change character of Highlands neighborhood/one of Madison's premier residential neighborhood, will not add any meaningful quantity of new homes for Madison, will lead to more housing, more pressure on existing low volume roads and need for larger roads with curb and gutter, rezoning Highlands will add high end housing that isn't needed, people who live there paid a hefty price for their neighborhood of spacious lots
Support rezone of Highlands	(OC)	3	Small change to zoning would not significantly impact the neighborhood, the existing zoning type simply should not exist in an urban area, still private residential property that cannot be enjoyed by the community, can't guarantee that these lots actually preserve tree cover, SR-C1 is restricted to 2-stories and does not have any goals concerning the preservation of trees
Clarify land use map	(OC)	2	Include escalator clause on all GFLU maps, on map it looks like you have expanded the pool area to the homes next door and changed the zoning

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Expand bike/protected shared-use paths	(EIM12) (EF) (OC) (EVM18) (HFNA)	32	Painted lines with no physical barrier are not as safe, generally support bicycle paths that are not shared with a street or have a physical barrier from traffic, use quieter streets for shared use routes rather than heavily traveled streets, Old Sauk Road needs a fully protected bike lane, bike path on Segoe south of Regent is unprotected and drivers will go into bike lane at high speeds, no SE/NW bike route at all south of Regent/Old Sauk and is major gap in bike network. Add bike path: behind Queen of Peace Church for West/East connection, connectors and bus/BRT routes, to connect path behind Memorial High Schools to other paths, along railroad line, to connect Boundary Park to Skyline/Highlands neighborhoods, in Greenway connecting Old Middleton to Camelot, to complete West Towne area path west of Gammon, to complete bike lane network through URP
Support Gammon Rd. shared-use path	(OC)	6	Avoid this area currently because of poor biking facilities, path under Beltline should be improved as noted in plan text, slip lanes are unsafe and cars are always in cross walk, it should be a protected lane
Concern about bike lanes	(EIM12) (EF) (OC)	4	Bike lane at Glenway ends in traffic, sidewalk conversion on N. Segoe Road and Old Middleton does not facilitate bicycle connections and will decrease parkland, bike lanes on Odana/Midvale intersection disappear, shared use path on Regent St. is of minimal benefit since it does not extend to Whitney Way and removes trees along Regent St.
Oppose bike lanes/bike infrastructure	(EIM12) (OC) (EVM18)	9	Bikes are moving road hazards, winters mean bike infrastructure not needed, protected lanes are not welcoming - they are unsafe and provide a false sense of security, adding so many bike paths across Madison for small subset of population that bike, destroying nature for paving. Locations of concerns: Mineral Point Road, path along Regent by Rennebohm Park (not needed and will cut down trees where traffic is low), Eau Claire Road near Rennebohm Park, already a path along University Avenue
Approve of West Plan addressing pedestrian, bike, and transit/bus needs	(EIM12) (EF) (OC) (AVM4) (EVM5)	32	Like more safe bike routes, improved bus lines and bus stops, new paths need, dedicated bus lanes should be maximized, ensure paths are maintained year-round for area residents, consider modal hierarchy when rezoning [revising GLFU] to make transit/bikes/pedestrian modes more attractive than cars, keep improving bike and non-car options, support intersection improvements for ped/bike safety. Support changes in these locations: Owen Conservation Park, Walnut Grove Park, by Stephens School, connector paths from Sheboygan Avenue to Rennebohm Park, Beltline overpass, on-street lanes on High Point, enforce traffic regulations and improve pedestrian and bicycle safety on Old Sauk Rd., Gammon Rd., widening of sidewalks on Regent, Eau Claire, eastwest shared use path connection through high school and middle school property, support intersection improvement at High Point Rd.
Approve of protected bike lanes	(EF) (OC) (EVM18) (EVM5) (HFNA) (CPFG) (CFG)	19	Provide safe biking facilities, want more protected bike lanes with some kind of barrier or additional signage, really like the bike plan, added mileage of protected two-way cycle track on major mixed-use corridors will really help reduce congestion, hopefully the need for a large parking ratio for new development - potentially down to .5 spaces per dwelling unit, youth use lanes, would like map showing protected lanes versus not protected lanes-not the same experience, in favor of all ped/bike proposals in the plan-the west side is fully dominated by cars

6

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Concern on pedestrian/bike safety	(EF) (EIM12) (OC) (AVM4)	29	Converting sidewalk to shared-use path and shared use streets will increase accidents and injuries, add curb extensions/bump-outs at all intersections within the 1.5 mile school walk radius, more lighting and street crossings, rail corridor needs to promote safety first, need speed limits for bikes (electric), use education and training programs for bicyclists/ pedestrians/drivers and enforcement of road rules, increase access and crossing along University Avenue and treat as a heavily traveled street, Cooper path access on Old Sauk needs improvement, Beltline underpasses dangerous for bikes, Segoe bike lane needs more protection than a striped buffer, University @ Capital dangerous - reduce Univ to one lane in each direction, Midvale @ Mineral Point not safe for pedestrians, biking on Midvale feels unsafe & Midvale needs parking and protected bike lanes, High Point-Mineral Point intersection unsafe, need to slow car traffic at Old Middleton/Eau Claire, improve bike safety at Eau Claire and Old Sauk, Segoe is difficult to cross-add bump outs to parking lanes and pedestrian oasis at intersections, Hilltop/Segoe-sidewalk moves pedestrians out of drivers' sight-make sidewalk and crosswalk parallel and reduce turn radius, Eau Claire Ave at Old Middleton Rd - terminus of Blackhawk Path - add RRFB, Midvale Blvd at the SW Path - consider automating RRFB or adding overpass, intersections along Eau Claire and Regent (adjacent to Rennebohm Park), improve ped/bike safety at the West Towne Path and Whitney/Beltline, make north and south Gammon bikeable for all ages and abilities, Hammersley needs to be improved or removed from the bike network due to recent developments and parked cars making the street too narrow for safe bike traffic, as a biker/resident on Midvale Blvd adding an unprotected bike path on Midvale is not ideal - it would be more cost-effective to improve a parallel street, crossing Tokay/Segoe, marked pedestrian crossings within West Towne area streets, entire length of Donofrio Drive desperately needs act
Oppose/Concern about Lake Mendota Drive/Camelot Drive to Marshall Park shared-use path	(EIM12) (AVM4) (OC)	6	Concerned about its impact on private property owners, no to street/concrete, yes to the connection, the City does not provide road or waste services anywhere on this road, these homeowners together pay for road repair/reconstruction/snow removal/etc., it is private property and intended for the private use of the Thorstrand Road property owners, it should be removed from consideration as a public path, already have a bike path going all the way down University avenue that connects over to Allen boulevard
Support Mendota - Marshall Park bike path	(OC)	4	Better access to Marshall Park from this side would be amazing, connection via Camelot and Thorstrand is excellent
Support Sauk Creek Greenway Pedestrian/Bike	(EF) (OC)	14	Need for good pedestrian/bike connector E of Beltline between Mineral Point and Old Sauk Road, include path via Sauk Creek Greenway, path would make area more walkable/less car dependent/increase property values, High Point is not a good alternative, more interested in a connector between High Point & Westfield perpendicular to proposed greenway, would like path to minimize disruption to natural habitats and removal of trees

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Oppose/Concern Sauk Creek Greenway Pedestrian/Bike	(EIM12) (OC) (AVM4) (EVM5) (SCNA) (CFG)	107	Concern bike path will: harm ecology/wildlife, disrupt/ invasion of residents' privacy and backyards, cause safety issues, not preserve trees, have more concrete, not increase connections in area, lighting concerns, increase CO2 Greenhouse Gas Emission impact, create unnecessary costs, increase tax and water bills, won't be properly maintained by the City, destroy trees, will be difficult for MPD to patrol, increase stormwater issues, there are already surrounding bike lanes on High Point Road and Westfield Road that are plowed and lit, children can ride on sidewalks along parallel streets, would rather have more direct straight route, people already have access to the greenway, there are 5600 trees and estimates are that 90 percent of the trees will be cut down, would like to delay decision until engineers design creek restoration, planned bike trail is too wide, older people will unsafe, path should be on the other side of the creek, greenway is not wide enough for a pathN-S or E-W path would require taking out 100s of treesWestfield and High Point can suffice, there is a path shown partially in the nearby High Point Road to Sauk Creek park - this path should be extended to Tree Lane instead on this transit priority network per the Complete Green Streets Guide
Support Wexford Greenway Pedestrian/Bike	(OC)	1	
Oppose Wexford Greenway Pedestrian/Bike	(OC)	2	
Oppose Owen Conservancy Pedestrian/Bike	(OC)	2	
Like BRT but oppose further stops	(LCD) (EF)	2	Maybe not as practical when using for grocery trips or certain locations due to destinations being further away from bus lines and stops, especially during inclement weather
Oppose transit oriented development	(OC)	1	Remove Hill Farms from BRT Overlay, don't want multi-unit next to single-family homes
Support transit oriented development	(EF) (OC)	12	Higher density along future BRT routes, increase housing availability near transit
Support Road Diets	(EF) (OC)	9	Gammon side path is good idea, reduce east portion of Mineral Point Road to one-lane each way + protected/buffered bike lanes, implement temporary measures like paint & barriers eventually leading to permanent measures, road diet for Gammon along Memorial/Gillespie
Oppose Road Diets	(EIM12) (AVM4) (OC)	10	Mineral Point Road, Odana Rd, concerns road diet will: cause gridlock, make it difficult to get in/out of driveways and cross streets, cause accidents with merging of the lanes, will increase use of neighborhood streets, difficulty entering/crossing Mineral Point by neighborhood motorists and pedestrians, putting busy roads onto road diet forces cars onto community and neighborhood streets
Concerns on parking	(EF) (OC) (EVM18)	11	On-street parking in high density and high use areas will not be adequate, increase public use of off-street private and/or city or state-owned parking ramps or lots, additional Madison Yards parking should be underground/not facing Sheboygan, add on-street parking back to Sheboygan, moving more parking underground, developers must add more parking so new residents and business patrons don't park on streets, removal of on-street parking on Whitney Way was bad, don't need to consider parking by Rennebohm park, concerned bike paths will remove necessary parking in area, two sided parking should not be allowed on Beach Street- Emergency vehicles have trouble navigating this area, eliminate parking

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Additional traffic turn signals	(EIM12) (OC)	4	Whitney Way going east on Mineral Point Road, In/out of Memorial High School, stop sign on the Presidential/Tocora Intersection, for Tocora Lane where yield signs are now, Sheboygan/Segoe needs stop light
Concern for proposed street changes/planned streets	(EF) (OC) (AVM4) (EVM5)	66	Rennebohm Park-width of sidewalks-biking may conflict with people trying to walk, street is unnecessary and can be dangerous for park goers and trees, would cut park space and not allow room for community garden, do not need additional parking, street through TruStage is not safe, oppose streets by/through Karen Arms/street through the Monticello Aptswould result in loss of housing, proposed street off Old Sauk is on a steep hill in area that already has traffic issues, remove Laub Lane & keep Berwyn Dr, block off Berwyn Drive access to S. Segoe Road, don't change Berwyn/Laub/Segoe intersection, concern opening Middleton Street up to through traffic, don't make Cable Ave a Stephens drop-off site - it's part of a low stress bike route, don't study St. Dunstan's Dr for two-way traffic, no to connecting Sauk Ridge Trail to Appalachian Way/no Appalachian Way connection to Old Sauk- use as greenspace, too many planned streets in West Towne Mall area, oppose new north-south street along Cooper Lane Bike Path to connect with Appalachian Way, Concern over planned streets East of Whitney Way and North of Tokay Blvd, planned streets in West Towne Mall area - should be used as more park space, don't put N-S street in on TruStage property-not needed, proposed straight streets are boring/not creativeif proposed streets were curve it would add interest/slow traffic, proposed streets adjacent to Old Sauk are not needed, Yosemite was a through street at one time - the reason it was blocked and turned into a cul-du-sac was for traffic and pedestrian safety on Yosemite and Old Sauk Roads - the direct access to Old Sauk encouraged traffic and the slope of the street contributed to high speeds on Yosemite - it was a good idea to block Yosemite then and a terrible idea to return it to the same dangerous conditions now
Expand Connections	(EVM18) (OC)	4	Connect the Highlands to Stonefield and Harvest Hill, no bike path currently from Brule street to Tree Lane - excellent area to install a shared use path, add painted bike/parking lines on High Point Rd., connect the east/west path to the on-street bike path at Regent instead of having to go north to go back down Rosa to continue going east or to access Garner Park
Support street changes/planned streets	(OC)	7	Support additional crossings of the Beltline, support a new street between Sheboygan and Rennebohm Park- proactively addressing the traffic needs when more dense housing is built in this area-these roads will help alleviate traffic congestion without reducing Rennebohm park access, support URP street connections between Science Dr. and S. Whitney Way and Science Dr. and Tokay Blvd., support removing Berwyn Dr at Laub and enhancing ped crossing

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Streets	(EF) (OC)	19	Lower radius on wide/add maximum desirable curving radius for curbs, curved turning radius to reduce speeds coming into neighborhoods, install roundabouts at Gammon/Old Sauk and Gammon/Tree, Old Sauk by Crestwood needs a drop-off zone on the N side, no more speed humps, new street across Tru Stage property should intersect Rosa and not Mineral Point, reduce existing neighborhood street connections to Mineral Point/Midvale/University/etc. (modal filters), narrow all traffic lanes to 9' to reduce speed and make room for sidewalks, construct speed humps on Regent St, monitor speed on Segoe, intersection safety improvements @ Whitney Way & Odana, focus on crosswalks vs sidewalks, signal timing adjustments needed at Midvale-Regent intersection left turns, need signal or roundabout at Old Sauk-Old Middleton intersection, slip lane from mall to southbound Gammon is unsafe and drivers can't see people on path, Mineral Point between Midvale and Glenway should not be considered a neighborhood street due to volume of cars, Medium Residential at northwest corner of Segoe/Mineral Point will result in apartment replacing Covenant church and increasing already congested intersection due to school traffic, additional mitigation is needed on Whitney Way north of Mineral Point Road to decrease vehicle speeds and make the street safer for pedestrians, make Whitney Way and south of Mineral Point Road a car light street in the higher density area to promote pedestrian safety -similar to State Street
More Sidewalks/Pedestrian Routes	(OC) (AVM4) (HFNA)	23	Ped-only corridors close to University and Hilldale, close sidewalk gaps (e.g., Tocora, north side of Old Sauk), Old Sauk needs sidewalk west of Crestwood school, Olympic Dr missing sidewalks, better crossings at Whitney Way with flashers, support people first, pedestrian friendly infrastructure, improve access & crossing Mineral Point @ Gammon and by Memorial High School/High Point/Westfield, West Towne needs to be more ped friendly, sidewalk gaps shown on Rosa & Elder Pl where there are sidewalks, missing sidewalk on Sherwood Rd., crossing Midvale at Ames connection, prevent left turn northbound Midvale at Ames, secret sidewalk that leads to a crossing for school access-add bump out in the parking lanes, add midblock ped crossing with RRFB between Mineral Point and Rosa to improve access to Garner Park, add a stairway where there is a zig zag sidewalk to access bike path at northwest corner of Gammon/Beltline, it would be great to have car-free and car-lite streets in higher density areas to promote pedestrian safety and walking/cycling - similar to State St. downtown, sidewalk on one side at Hilltop just south of Segoe
Lighting	(OC) (AVM4) (CPFG)	5	Support Gammon-Beltline underpass ped lighting, increased street lighting, consider dark lighting, more lighting along: Tree Lane Rd, Garner Park, along bike paths (motion lights), Greenbriar bike path, back of Lussier, support increased lighting on shared use path by Ezekiel Gillespie Middle School, concerned about how much light shines upward from the areas extending northeast from the Gammon/Mineral Point area, if lighting is added please take care to limit uplighting
Support Old Middleton-Craig RR Underpass	(OC)	2	
Transit improvements needed	(OC)	2	Need a N-S transit route on Midvale, park-and-ride needed at West Towne
Bus/Bicycle/Intermodal Infrastructure	(OC) (CFG)	5	Add bike racks, B-cycle stations and bike lockers near BRT & other bus stops

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
High Injury Network	(OC)	6	Intersection of Felton PI and Falles Ct-Zero crashes in 25+ years, HIN should highlight intersection of Midvale and Mineral Point Rd which lies 400' southwest, other similar errors may be corrupting the HIN map, MPD do not file reports for bicyclist and pedestrians hit by cars unless seriously injured, MPD should report all non-life threatening injuries, City should also have a self-reporting feature for calls not responded to by MPD, HIN on Gammon is concerning since it is a route to Memorial/Gillespie Middle, disagree with W of Eau Claire and Sheboygan intersection identified on map, disagree with Gammon Place being shown on this map
Convert the Beltline to a street	(OC)	2	Please add in the adjoining areas on the other side of the Beltline to your plan
Beltline underpass improvements	(OC)	9	Underpass floods frequently and ices over in winter, include more information on changes to crossing of Beltline (Whitney Way) where bikers are expected to cross from SW Commuter path to West Towne Path, Old Sauk underpass bike lane is not in great condition and is not protected, City should take responsibility for reducing noise by reducing car dependence and improving public transportation/walkability/bikeability throughout the city and not just development along West Beltline Highway, crossing at Mineral Point Rd and Beltline is dangerous and should have improved crossing signage, restart abandoned bike path along High Point Road-should connect to the Beltline Path
Oppose the proposed bike/ped crossings of the Beltline	(OC)	1	Underpass/overpass at south of Old Sauk Rd is superfluous, too close to underpass at Old Sauk Rd and will interfere with sound barrier
Support the proposed bike/ped crossings of the Beltline	(OC)	3	Excited to see important connection south of Research Park Blvd and increased access to ball fields and ice arena, excited for extending path south of West Towne Mall
Oppose rezoning Planned Developments (PDs)	(OC)	2	Oppose the review of Planned Development zoning
Miscellaneous	(OC)	(single comments - see details at right)	Give Garner Park vibrancy - HDR (10 stories+) across Mineral Point Road from the park, rail corridors should be studied & repurposed for Metro light rail/passenger rail to complement BRT and reduce car dependence, rather than sidewalks in Glen Oak Hills/Crestwood/Highlands - install traffic calming measures like chokers/chicanes to create Woonerfs, allow 30 story bldgs. at West Towne Mall, ban electric motor vehicles on bike paths-unsafe, I do know that the Beltline will be with us for a very long time but we need plan to convert it to a regular street now
NOTE: sidewalk comments summarized in Health & Safety section			
Neighborhoods and Housing			
Want more ownership opportunities with new development	(EVM18) (OC)	9	Apartment ownership in Madison should be explored, affordable condos, want more owner occupied housing
Want more missing middle housing	(EIM12) (EF) (OC) (AVM4) (EVM5) (CPFG)	13	Include escalator clause in plan, be intentional about adding missing middle, more missing middle housing can be done, concerned about multifamily housing, examine other existing barriers to missing middle, want townhouses adjacent to each other (not duplex style), want missing middle-not large apt buildings, want missing middle-hard to create sense of community when everyone is living in a high rise
Like addressing missing middle in Plan	(EF) (EVM18)	4	Missing middle and small multi-unit buildings along collector streets and larger buildings along arterials, middle housing can greatly increase neighborhood density and tax revenue

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS		
Concerns about developers	(AVM4) (EVM5) (HFNA) (OC)	4	Developers need to pay their fair share-specifically regarding schools, certain developers have their input weighed more than the community, out of town investors will dramatically change things for the worse, we have a severe housing ownership deficit and don't want investors buying up houses		
Rent Assistance / Rental concerns	(CPFG)	2	Want rent assistance and rental resources, not fair management practices in Tree Lane and Wexford, landlords are less likely to rent to families, want better conditions		
Want more diverse housing stock	(OC) (CPFG)	3	Want more family friendly spaces, in multifamily units-people have issues with families (noise, etc.), more affordable options, want diverse housing stockbut in strategic locations to minimize disruption to current/future residents		
Prioritize and ensure affordable housing options within new development	(EIM12) (LCD) (EF) (CPFG) (OC)	9	Plan for affordable rent and ownership, don't lure developers to develop new unaffordable housing, can upgrade existing development to be more sustainable, opportunities for more ownership with new development, incentivize developers to build more affordable housingWhitney Reserve and Madison Yards are expensive		
School comments [staff note: the City does not control school attendance areas or school expansions]	(OC) (AVM4) (HFNA)	7	Don't change school attendance boundaries, school expansion needed @ Hamilton/Van Hise, work with MMSD to avoid overcrowding in schools with new development, funding comes with additional students, reconsider school related actions as the City does not control school expansions, confirm Lincoln Elementary School attendance area		
Miscellaneous	(OC)	(single comments - see details at right)	Prioritize City funding assistance for missing middle - not large apts, encourage expansion of Lussier programming, physical barriers needed along Beltline-or a larger plan to reduce traffic and mitigate noise-suggest adding green space/trees		
Economy and Opportunity					
Oppose/ Concern with boundary of Tax Increment Districts [staff note: being in a TID boundary does not mean land would have to develop/redevelop]	(AVM4) (OC)	4	Concerned about the implications for TID 46 and boundary, impact of parks within boundary, against development of 2 city parks and HF pool due to being in TIF boundary		
Agree with increasing small business funding from TIF	(OC)	1	Especially BIPOC-owned businesses		
Agree with increasing childcare options	(OC) (CPFG)	5			
Taxes	(OC)	2	Study the impact of low-density housing on tax revenues - observe the cost to serve the property vs the tax revenue paid for the property - pair finance with Planning to understand how zoning can impact the City's tax revenues		
Land banking	(OC)	2	Strengthen language beyond "consider land banking"this is important for improving economic opportunities for underrepresented groups, consider land banking for Highlands to convert to small conservation areas for large lots		
Culture and Character					
Preserve character of neighborhood(s)	(EF) (HFNA) (OC)	3	Historic district, do not want to change, opposed to housing in a National Historic District being part of TOD overlay		

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Design elements changes	(EIM12) (LCD) (OC) (AFG)	17	Include requirements for aesthetic architectural design (no ugly housing), greenspaces and trees, implement pre-reviewed building plans to allow for faster approvals/construction, architectural approval for very large developments, remove/reduce restrictions so different looking buildings are approved via UDC, more creative building designs, support for "Commercial Core" design requirements, add stipulations to reduce light pollution and protect trees, add murals to blank building walls that lack architectural variety
Oppose/ Concern UDD #6 boundary changes	(EIM12) (EF) (OC)	11	Oppose boundary further south, boundary should not include Rennebohm Park, the south side of Regent street, or existing streets west of Eau Claire Avenue since the TOD Overlay Zoning in those areas differ from UDD, want specific area, add curb extensions at all intersections and midblock crossings every 500'-need to make sure that when development happens is in conjunction with making the surrounding area safer for peds/children, added boundary area in purple rectangle that includes Rennebohm Park should not include single family homes, it is inappropriate to use an UDD to promulgate changes in zoning, it is irrational to include a residential neighborhood a half mile away from University Ave. to this district while removing houses in an essentially identical residential neighborhood area directly abutting University Ave., UDDs add unnecessary bureaucratic red-tape, we should not be expanding UDDs, but reducing them or replacing them with form-based codes, new development needs to complement existing development
Support UDD #6 boundary changes	(OC)	1	Extend UDD 6 to include all of Hilldale Mall
More things to do	(LCD) (CPFG) (CFG)	3	More social spaces in new development, entertainment venue, community center, Hilldale Male like areas,
Support of Arts/Build on arts infrastructure in West Area	(AFG) (OC) (EF)	5	Indoor gathering spots needed for artists to gather, more communal studio space, makers spaces, pop-up opportunities and events for music and arts, more art resources, support wrapping utility boxes-sidewalk poetry-and other things that include the community, support West Plan inclusion of art spaces in Research Park - thank you for including affordable artist studios, exhibition space, etc., in this plan - it is sorely needed on the west side which can feel like a cultural wasteland - artists need it and it would enrich the general public as well
Pools	(CPFG) (OC)	3	Show pools in facilities, issues with accessibility for local private pools for some, highlight Hill Farm Pool significance to West Area
Local Historic District	(OC)	2	There is not much in the way of historical significance in this area that's worth preserving over potential redevelopments that better meet city and community needs, National Register sounds preferable to Local Historic DistrictsMadison needs to be able to changeconcerned about resistance to changes
Youth Sports Complex/ Facilities	(HFNA) (CPFG)	2	Madison needs a new or renovated sports complex for youth - all cities and towns outside of Madison have updated facilities for youth - specifically baseball and softball fields - there are not enough and low income people don't have an option, previously lots to do for youth - but now staffing shortages and shortage of activities - Lussier is a great resource - but not many other options

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Miscellaneous	(OC) (AFG) (CFG)	(single comments - see details at right)	Support neighborhood-oriented events in underserved areas, support educational plaques for Ho-Chunk, wrapping utility boxes is minor worry - while at same time plan drastically changes longtime and beloved character of Hill Farms Neighborhood, if we're considering the historical significance of the Highlands area land use the Plan does not do enough to consider the 12,000 years of Ho-Chunk land use precedent - the City should follow Vancouver's example of the development at Seńakw as reconciliation to the Ho-Chunk Nation, UDD 3 - overriding the requirements to maintain the character of the neighborhoods seems arbitrary and without any evidence simply offering the judgement of "auto-oriented", preserve viewsheds - Old Sauk view blocked by Hilldale area
Green and Resilient			
Increase/preserve greenspaces and parks	(EIM12) (EF) (EVM5) (AVM4) (HFNA) (OC)	20	Continue to provide green areas for newcomers and dog owners, more density creates need for additional park area -especially around South of Mineral Point Road and around Hilldale, preserve trees in Rennebohm, preserve greenspace in University Research Park as it redevelops, more parks - not enough greenspace, ask for parkland dedication for Rennebohm Park, preserve Indian Hills Park, turn golf courses to parks, preserve Kenosha greenway and add as extension of Park between Garner and Glenn Oaks Park, more trees in parks, plan needs to include new greenspaces given all the new development proposed in the plannon-humans need habitatbirds, mammals, invertebrates, insects, etc., the greenspaces should connect for the benefit of wildlife/bikers/hikers-show on a map how the greenspaces could be connected and let residents comment on the proposal, invite you to make parkland more accessible - Odana Hills Golf Course is not for multiple users and there are really only tiny micro parks in the area around it, work with developers of large apartment buildings to increase tree canopy and green areas, New construction in TOD has no open space requirements and minimal terracesCity will need to provide small pocket parks
Like that West Plan address parks and greenspace needs	(EIM12) (OC)	4	Support buying land for more parks, like Vernon Blvd. greenspace
More tree canopy/nature	(EIM12) (EF) (OC)	18	Add to shrubs/plants/trees, avoid removal of trees with new development/BRT/paths, work with Urban Forestry Task Force and report of 2019, do not use grassy swale approach, work proactively to minimize threats of spongy moths and other infestations for trees, keep mature trees, mandatory native vegetation, ban pesticides, provide programs/ initiatives/ grants/ education/zoning requirements for private property owners, this part of Madison is lower density with lots of trees and greenery-should quadruple the amount of new green space you are calling for, plan proposes too much development. Locations for tree preservation/new trees: preserve trees at Whitney, Sheboygan, along S Kenosha greenway, NE of Whitney Way and Mineral Point Rd, increase tree canopy in Rennebohm Park and University Ave, plant trees on the median strips on Regent, Segoe, Whitney Way, and Mineral Point Rd
Stop cutting mature trees	(OC)	3	Stop cutting mature trees down unless absolutely necessary, don't remove old growth oaks
Support West Plan addressing tree coverage	(OC)	4	Support increasing tree canopy, increase terrace widths, need to water new street trees to ensure success
Want West Plan to better address climate crisis	(EIM12) (OC)	4	Climate change increases rainfall issue

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Concerns about stormwater	(HFNA) (OC)	4	Plan states that stormwater is a significant issue in this areabut Plan calls for more impervious surface, concerned about the impact of higher density on stormwater issues, area south of Mineral Point road for increased stormwater holding to reduce the need for Pheasant Branch Creek to be the outlet valve in a 1% storm- consider decreasing impervious surfaces in commercial areas north and south of Mineral Point
Support West Plan addressing stormwater needs in area	(EF)	4	Support efforts in Old Sauk Road down through East Spyglass Court, need to address flooding
Support West Plan addressing stormwater needs in Sauk Creek Greenway	(EIM12) (OC)	6	Addressing flood issues in Sauk Creek; concerned about erosion due to lake of maintenance
Oppose West Plan addressing stormwater needs in Sauk Creek Greenway	(OC)	2	Do not change the greenway in the name of "stormwater mitigation"
More paths in parks and greenways	(OC)	2	Gravel path from Yellowstone through Owen would increase hiking routes; more natural surface paths in greenways and parks
Support future planned parks	(OC)	2	Walkable areas and the ability to host events would be welcomed, park west of West Towne Mall
Concerns about future planned parks	(OC)	4	Park space B conflicts with brand new development, expand park space A instead, park names should come from local Native/Wisconsin tribes, there are 500 acres of open space within 1.2 mile radius of proposed park off Normandy Lanehow can the City justify spending \$20M to purchase developed parcels and remove them from the tax base, do not expand Rennebohm Park into/toward Karen Arms
Support park improvements	(OC) (CPFG) (CFG)	4	A splash pad at Garner Park sounds like a good idea, expand Lussier playground, improve Wexford playground, mountain bike trails in parks would be fun, add pump track/skills course (like at Leopold) to current Odana Hills East Park's playground area, Walnut Grove tennis courts
Do not build road on northern end of Rennebohm Park [staff note: the public review draft shows a planned street <i>north</i> of Rennebohm Park, not in Rennebohm Park]	(OC)	8	Adds little value, adds cost, takes away open space, increase access by putting in a path instead, the proposed drive on the north side of Rennebohm Park goes far beyond what was proposed in the University Hill Farms Neighborhood Plan - the neighborhood plan only proposed a quiet noncontinuous park side drive to provide additional access to Rennebohm Park and intentionally did not go from Segoe thru to Sheboygan Avenue (please see page 36 of the University Hill Farms Neighborhood Plan), building this street would limit the free movement of people and children to and from the park, additional ped/bike access to the park is goodmore car streets around the park is bad, proposed streets are redundantif these streets are needed developers should construct and maintain
Concern plan does not cover all environmental considerations	(EVM18)	2	Hydrology benefits and pathways of contamination associated with our rich but complex groundwater resources and drinking water and impacts on our recreational uses of the west side of Lake Mendota, has an environmental analysis been done on the impact of all this added density in terms of tree canopy/impervious/habitat/noise/traffic etc.
Build a public pool	(OC)	2	Acknowledging that the Hill Farms Swim Club is an amenity, the City should identify a future priority of adding a public pool in the area

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Repurpose Odana Hills Golf Course	(OC)	8	Restructure as a mix of park/recreational spaces and multi-family housing, include other recreational uses that are more equitable and sustainable than golf, support clubhouse with a year round facility, golf courses often have paths through them and could probably be linked together with little disruption to make a longer hiking path, repurpose for dense housing, sell land for dense housingplug City budget deficit
Rain Gardens	(OC)	2	More rain gardens, rain gardens in large terrace bump outs where parking isn't allowed currently
Miscellaneous	(OC)	(single comments - see details at right)	Support greenway shown to west of West Towne Mall, change from "Oak Park Heights Park" to say "Piper Park", Zook Park - increase connectivity between park, surrounding neighborhood, and Southwest Commuter Path via a paved path, encouraging responsible exterior lighting (staff note: already in lighting code), support Vernon Blvd. reduced street width, Spring Harbor Boat Launch is not accessible enough, Reduce air pollution of internal combustion engines (ICE) vehicles by incentivizing EV & hybrids instead or increasing ICE registration fees
Effective Government			
Want budget and cost information/estimates for proposed changes	(EIM12)	2	
Build awareness on City plans and polices	(EF)	2	
Support expanding Lussier Community Education Center	(EIM12) (LCD)	2	
More libraries	(AVM4) (OC)	3	Develop a strategic plan for the Library's west side footprint, and better align to areas of high growth and the new redesigned transit network
Miscellaneous	(OC) (CPFG)	(single comments - see details at right)	If Rennebohm will be a polling placeadd walls, want more resources-like MOMs resource center, limitations to accessing neighborhood centers due to living location, tackle unbridled spending and deal with 27 million without burdening property taxes, Want less police presence as they sit in certain neighborhoods with no explanation - want more intentional interactions and create more neighborhood connections, more residents - need more police
Health and Safety			
Opposed to sidewalks	(OC) (EF)	11	Old Sauk Road northside sidewalk from Crestwood School to San Juan Trail-loss of trees, along north side of Old Sauk Rd, Harwood Circle North, Walnut Grove Neighborhood, concerns about 10' wide sidewalks impact on trees along edge of Rennebohm Park, sidewalk along north side of Old Sauk will be expensive and require a retaining wall which has a long-term maintenance cost, concern about shared use path along Regent - peds/bikes should not share the same path as its dangerous for people with disabilities, prioritize fixing roads over sidewalks and bike paths, northern section of Hilltop (400~450ish block) would prefer to keep that section sidewalk-free, Dearholt does not want sidewalks- recommend canvasing the street to get a more complete picture of resident preferences
Support West Plan shown sidewalks	(OC)	3	Support closing gaps in the sidewalk network - prioritize routes used by children to get to school and residents to get to bus, and routes with no sidewalks on either side, many sidewalks segments that have been created with developments that make no sense- should extend beyond the limits of the property, sidewalk connectivity is critical for safety and pedestrian access to UW Health and the offices on Science Dr and to Sequoia Commons

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Want to add sidewalks/ pedestrian safety	(EF) (OC)	4	Sidewalks on both side of Capital Avenue, corner of Capital/Taychopera Rd., infill sidewalks (relevant streets are: Hilltop, Hillview, Tocora, Edward, Ames, Agnes-only on one side, Sherwood Rd.)
Support actions listed	(AVM4) (OC)	2	Specifically the partnerships actions, prioritize keeping Tree Ln residents in apartments
Concerns on community health care	(HFNA) (CPFG)	2	private equity investors have reduce service and raise rates, changing community health care and hospitals, mental health resources
Concern over traffic and speed	(AVM4) (HFNA) (OC)	4	Like recommendations, but enforcement would be a challenge, lower speeds on Mineral Point heading east towards Science Drive, cars on Segoe continue exceeding new 25 mph limit plus more traffic with new apt. units on Whitney Way, enforce new 25 mph on Whitney Way as drivers still do 40 mph or more
Install speed cameras that issue tickets	(OC)	2	
Miscellaneous	(OC) (CPFG)	(single comments - see details at right)	No halfway houses within 1 mile of schools; mandatory background checks for rentals, want safe/quiet/low crime neighborhood/community, want to add emergency poles (implemented by/within trails and dark paths), no need to have engagement at Tree Lane housing since housing is being sold
Planning Process/Other			
Engagement concerns/suggestions	(OC)	5	Reddit users drawing outside influencers to flood support for their opinions and they don't even live in the neighborhood in question - please require input to show residency proof, please listen to Hill Farms homeowners (don't feel heard), don't feel safe providing comments in public venues due to neighbors being disrespectful - but provide comments on Konveio, Outreach to stakeholders has been low, survey response rates low - more outreach and time for commenting needed
Support for planning process	(OC)	4	Greatly appreciate the work that you do -watched the March 12 public meeting and am embarrassed by the behavior of my fellow community members, appreciate work being done to think about the needs of the city as a whole - including taking into consideration the needs of those that don't have the capacity to actively participate in these planning processes (e.g., those working multiple jobs, raising children, studying in college, etc.), I've lived on West side for almost 60 years and thank those who put countless hours into this plan, please add in the adjoining areas on the other side of the Beltline to your plan
Issues with plan language	(OC)	2	What is a TID? no wonder people don't understand or trust what the city is saying when they don't explain abbreviations and use jargon - use plain English., have list for acronyms
Homeowner "Veto"	(OC)	6	Homeowners should not have veto power over efficient land use - they are obstructing sustainable growth and affordable housing options
General Plan Support	(OC)	7	"I support this plan"
Map corrections	(OC)	2	1/2 of Science Drive has sidewalks both sides, a small portion of Herrick, near Odell, only has sidewalk on one side

COMMENT SOURCE MULTIPLER AREA/DETAILS

18

Phase 3 Engagement

(HFNA) Hill Farms Neighborhood Association October 18

(CPFG) Community Partners Focus Groups October 18 & November 21

(CFG) Chinese Focus Group November 7

(AFG) Arts Focus Group November 13

(SCNA) Sauk Creek Neighborhood Association November 15

(MHCA) Virtual Public Meeting with Highlands Community Association January 31

(LCD) Lussier Community Dinner March 1

(AVM4) Virtual Public Meeting March 4

(EVM5) Virtual Public Meeting March 5

(EIM12) In-Person Public Meeting March 12

(EVM18) Virtual Public Meeting - Zoning Focused March 18

(EF) Emailed Feedback

(OC) Online draft Map and Action Comments

(CP) Community Partner Comments (The West Area Plan hired Community Partners to focus and highlight feedback from underrepresented communities during this planning process. Partners reached over 275+ residents during this planning process).

Staff has reviewed, categorized, and summarized public feedback received since the Phase 3 Feedback Summary from April 20, 2024 to June 10, 2024 through the activities shown in the legend at the bottom of the document. The legend also includes abbreviations used within the document to identify where the various comments came from.*

West Area Plan - Community Feedback Summary

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Land Use			
Approve of land use changes	(OC)	9	Locations: University Triangle area, Mineral Point Rd and Regent (Area D), Land bounded by Eau Claire- Sheboygan-Old Middleton-Hill Farms Office Building; excited about dense mixed use, like keeping more flexible options for future zoning, more density would be great for area, triangle is currently low-intensity, strip-mall style development, the kind of thing you'd expect in a vestigial ex-urb, Appreciate NMU allowing for some commercial amenities/services-not many in neighborhood, area D, MR is covered by NMU and NMU allows for businesses, I appreciate that not every suggested change is downzoning when the City should really only be upzoning, Area C back to LR, Love the idea of having more dense mixed use in this area (Whitney-University-Old Middleton triangle), really help develop the area around the BRT into a vibrant neighborhood that's accessible to people who can't or don't want to drive, It has the potential to give people without a license a lot more independence and could remove the burden of car ownership from residents
Concern/Oppose land use changes	(EF) (OH20) (LZH3) (OC) (WRM) (T6)	67 (37 signees for northwest block of Whitney Way and Regent via email)	Locations: Pierstorff parcels, Old Sauk, northwest block at Whitney Way and Regent, 6021 Old Middleton, 25 Veblen PI, Wynwood Way, Whitney Way, Wynnwood Way/Whitney Way/Old Middleton corner/triangle; Concerns: not in scale nor proportion to the surround area, detrimental impact on the traffic, design of development in relation to existing development, do not want urban high-density development, mega structures cannot be integrated into surrounding neighborhoods, will add to environmental issues like flooding, northwest block at Whitney Way and Regent (want to remain low-residential land use category-want area to remain quiet, keep tree canopy, limited light pollution, safe trees for pedestrians and cyclists, up to two stories in neighborhood), do not want Old Sauk Road to become an apartment corridor, want Old Sauk to remain a nature-oriented settings with suburban setting with parks, trees, and limited commercial access, want Old Sauk development to be limited to single family detached residences or duplexes, Would rather encourage properties that have greater development potential to develop than recommend these single-family homes for development, ideally would like Whitney Way area to be a smaller, neighborhood-serving commercial uses added as an amenity for area residents, do not put hundreds of families in mostly concrete apartment buildings in commercial districts, Keep residential areas residential, no recognition of how high, how dense, or how fast the city is willing to grow, worse- there is no recognition of how far extant, long-term residents are willing to, and should have to, go in the city's process of accepting a greater and greater and greater population within fixed city limits, focuses on alternatives to high density, rental developments, get rid of any rezoning in or adjacent to neighborhoods, Use existing space for apartment building, change land use designation on Parkwood Hills back to what it was, oppose any zoning changes that would allow higher density in the Hill Farms area near Renne
Oppose land use changes for Stone House development and Old Sauk Road	(EF) (OH20) (LZH3) (OC)	13	Changing land use is not the appropriate response or solution to increase housing options, not in scale nor proportion to the surround area, detrimental impact on the traffic on Old Sauk road, design does not align with housing in vicinity, no setback, no mature trees, monstrous apartment building-ugly and unwanted eyesores, Heavier traffic, higher vehicle density, increased use of neighborhood streets parking, noise pollution, light pollution, irreparable effect on wildlife, higher runoff due to removal of trees and vegetation causing flooding events, increased danger to commuting area bikers are some of the unwanted, critical and imminent negative effects of these potential developments, do not want Old Sauk Road to become an apartment corridor, residents want to live near nature, should promote homeownership not people dependent on rentals, will block light of neighbors, other multi-unit housing complex has brought too many police reports, lack of parking concerns
Oppose land use changes for places of worship	(EF) (LZH3) (OC)	4	Locations: St, Thomas Aquinas; want to preserve greenspace, mature trees and quiet residential setting, work proactively with places of worship about plan recommendations and property potential while seeking opportunities to keep or relocate places of worship in neighborhoods, opposed to rezoning church at Regent and Whitney Way

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
More density/housing	(OH20) (VLZH28) (T6) (OC)	23	Way more density, smaller lot sizes, more upzoning, more condos, apartments, commercial space, changing means the development of Madison, more mixed use development-would like more shops and restaurants to be in easy bike/walk distance on the west side, plan is still not going to accommodate all the people looking to move to Madison, need more proactive rezoning, take whole city and future residents into consideration as much as current residents of the area, Upzone the entire city, End single family zoning and low residential land use designation- was created to be exclusionary and leads to unsustainable land use patterns, West Area is one of the least dense areas of the city that is inside the beltline, If Madison is going to be able to accommodate future population growth, it needs to be able to grow everywhere, Certain wealthy areas should not be preserved in amber, more density near transportation networks, Allowing for smaller lots in Madison would be a way to support infill development and allow families to find housing within their budget range, all LR should be moved into LMR or NMU, LR is severely restrictive for a city in a housing shortage, This plan is looking at decades, not next year, and by restricting what can be developed as we continue to grow will cause severe issues until we can approach the subject again in a decade, LR areas in TOD overlay should be LMR, Add LMR all along Whitney Way, area near Sequoya Commons should density, more Sequoya Commons-like mixed-use developments everywhere in West Area, allow 5-story mixed use and residential like Sequoya Commons all along Whitney Way, more density in the TOD overlay areas- especially along Whitney Way, proactively upzone align the TOD corridor to allow more people to make better use of the transit, Increasing density and allowing for mixed uses would reduce demand for transportation by allowing people to live closer to where they want to go and allow for more walking/biking trips as opposed to car dependent transit
Oppose less housing density and mixed use shown in Plan	(OH20) (VLZH28) (T6) (OC)	27	Locations: Area C-should go back to LMR, Area B- should at least be LMR, northwest block of Whitney Way/ Regent Street, Area E; Disappointment about people wanting less housing density and less mixed use, more residents are coming to Madison regardless- need to increase dense development to accommodate for growing population, will pave over farmland and the suburbs instead if people are not in Madison, More traffic and sprawl, Allowing more multi-family units will reduce the city's maintenance budget, adding more housing is critical for affordability, love Sequoya Commons, Need more housing of ALL KINDS near transit, shopping, and jobs, Small, larger, apartments, owner-occupied, condos, and single-family housing, more upzoning, disappointed that upzoning in existing built areas (Highlands, Faircrest, etc) has been shot down, any changes would be slow and people are overreacting, support multifamily, disappointed that church parcels no longer planned for possible higher-density developments, Disappointed that Highlands parcels not able to be subdivided without doing a PD/conditional use, zoning designation that can allow for more densification and/or variety of uses next to BRT stops as an alternate to land use designation, neighborhood mixed use allows for a lot of different types of housing and for businesses that can people close to home and work, It can promote neighborhoods that have vibrant communities and healthier lives with less car dependence, should not be limiting what can be built here for decades, NMU is a great way to allow this neighborhood to develop naturally rather than rigidly, LMR is better than LR, but NMU would allow the community to thrive and bring about more things like libraries, community pools, and cafes, goes against every goal that exists in the Comp Plan; much of LR should be LMR; plan sets us up for failure w/ costly housing, sprawl, increased traffic, less revenue; plan is regressing through continued reduction of density, Areas should be proactively up zoned to higher density
Approve of updates to northwest corner of Regent and Whitney	(EF) (OC)	3	good balance of feedback from Madisonians (including me) who want more land available for higher density housing and those who expressed concern about excessive impact to neighborhoods
Approve of proactive rezoning	(T6) (OC)	4	Long range upzoning, change is slow, let's get the ball rolling
Oppose proactive rezoning	(OC) (T6)	3	Oppose any proactive rezoning, consideration of any zoning changes should only be made with input and approval of current residents and only when such zoning changes are necessary, rezoning won't achieve what you want
Support/increase shown heights	(OC)	2	West Towne should have height limits over 20 stories, support heights along MPR across from Garner Park
Oppose shown heights	(OH20) (T6) (OC)	6	to restrict building height to three stories in blocks or adjacent to residential homes, There should not be any changes that significantly affect the value of a person's home as it is often times the asset to fund their years in a care facility, Don't like the extra tall apartment building along Mineral Point Road, reduce building heights in plan- especially in TOD areas to not lose more vistas, oppose any increase to heights of new developments, No 10-16 story buildings, Height restrictions are arbitrary and limiting for a plan that is setting the stage for decades. We need housing, If a developer wants to build a dense building-don't make it harder, We should be providing incentives for density, not disincentives by making it more challenging
Approve plans for Odana area	(OH20) (T6)	2	Love proactively planning for Odana area, hope is with the BRT, that area can become a great mixed-use area that will be close and usable for many neighborhoods, hopeful area will be a great housing/commercial/restaurant/community destination, would love to see it have a Vilas/Sequoia Commons for the far west side
Concerns for zoning for Odana area	(OC)	1	All of this employment zoning is end of life strip malls, class C office space, and car dealerships that will shortly decamp to the suburbs like the way they left downtown, If given more flexibility, zoning-wise, the Odana area could actually become a dense, mixed use secondary town center,
Approve of Highlands neighborhood rezone removed from plan recommendations	(OC)	1	
Oppose change to Highlands neighborhood	(OC)	4	This change should be reverted, The Highlands area takes up a lot of space for very little housing, should allow for smaller lots in this area, Smaller lots allows for more affordable homes, If we aren't going to make for smaller lots, we should increase the property taxes on the existing lots as they require significantly more resources per person than smaller lots do, should be upzoned, calling Highlands rural is ridiculous, huge lot size is ridiculous, zoning doesn't protect nature or owner from clear cutting lot
Oppose Highlands neighborhood rezone (Staff note: Proactive rezoning proposal for the Highlands area were removed)	(EF) (OH20)	2	Lose park-like greenspace, only provide limited housing-probably higher-priced, recreational space for the community, 70% of the properties have a new citizen-initiated covenant that preserve TR-R zoning code
Do not want more rental apartments	(EF) (OC)	2	Primary issue is again/still only more rental apartments excluding/precluding multiple forms of non-rental development
Family oriented neighborhoods (single family housing)	(VLZH28) (OH20)	4	Important and critical family oriented, residential neighborhoods are to the good quality of life in Madison and needs to be preserved while addressing the housing needs, It can be done, offensive and unfortunate that residents who believe this are immediately labelled as NIMBY's and old, white entitled people, Every piece of land that becomes available in the City should not be a priority for high density rental housing, There is still a place for residential neighborhoods, Concerns are dismissed as elitist and racist, Our concerns for stormwater management, noise, increased traffic are dismissed, disagree with needing less single-family zoning in area- City says homeowners are wrong, homeowners helped build this city, no multifamily buildings, redevelop commercial areas and parking lots into single-family houses

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Miscellaneous	(T6) (OC)	(single comments - see details at right)	Move cemeteries to special institutions- they are not parks and open spaces, ground floor commercial should not be required, the city should consider consolidating zones, Having this many zones is absurd and makes any of these maps needlessly difficult to comprehend, Ground floor commercial is increasingly sitting vacant, It's a mistake to require this as it may make development of desperately-needed housing prohibitive, make more people-center places to lower costs on infrastructure, allow more businesses and amenities, keep people in the city, and makes things more affordable for everyone
Transportation			
Oppose Sauk Creek Greenway pedestrian/bike path	(EF) (OH20) (T6)	34	Oppose putting any bike trail in greenway, concerns about the large trees, degrade the resource, bike path will serve few bikers, greenway could be opportunity for nature education, fits well with Nature Everywhere program, unnecessary east-west bike path, will add more impermeable surfaces, grade is very steep there and it would require more land for switchbacks as well as a bridge, Connection to Walnut Grove Park is not needed, This should NOT be wheelchair accessible, Too many switchbacks and a bridge over the creek may need to be 15 feet above creek bed to survive a 100 year rain, Too costly! It's a drainage area, not a park, No asphalt, No lights, all bought or built our homes because of Sauk Creek Woods, Change wording back regarding allowing engineer to decide what's feasible, reviewed UW road safety records and drive on High Point Road- there is not justified fear for bikers to bike on road, parks paths are not plowed in winter and negates connectivity-don't have money to maintain or replace the paths, will help combat climate change, helps with mental health, any path should be natural and unpaved, do not think path is feasible, Brule is only ,25 miles from Tree Lane, Tree Lane and Farmington are east-west routes, change to trail that is 5ft wide, will feel like they're walking through someone's backyard-which is not a comfortable feeling, could make more bike friendly but paved trail is not the answer, no lights, opinions of bikers in the neighborhood who don't think the path is needed should have greater weight, no maintenance road
Oppose changes made to Sauk Creek Greenway recommendation (Staff note: Sauk Creek Greenway north-south shared-use path was removed from Plan)	(EF) (OH20) (T6) (OC)	157	Disappointed by the removal of the N-S path in Sauk Creek Greenway and would prefer it be added back in, depend on bike paths to move around city and do not want to use roads and would like to reinstate the Sauk Creek bike path, residents' concerns are due to misinformation, bike path would have improved connectivity for bike access to further locations on the west side, the on-street bike routes are insufficient safe to ride on, especially with kids, kids in Tree Ln, area would have been able to hop on their bikes and safely ride to Alicia Ashman Library, some concern about losing parking near the Parks if bike improvements done on parallel streets, North-south route parallel to Sauk Creek Greenway very much needed, Also like that east-west crossing of greenway still there, need real all ages and abilities option for north-south route, city staff needs to explain not all the trees in the Sauk Creek Greenway are quality trees-need to thin out junk trees (Buck thorn, Box elder, and those preventing struggling trees from having the resources), making transportation network more comfortable for people to walk and bike will make a much bigger environmental impact than small number of trees that will be removed to make those connections, needs to be developed to bike, walk, hike, would open the wonderful urban forest to more people, seems like a waste to cut the shortest path possible through this resource, great juncture for students to access school, third spaces, and other community resource, prioritizing building out infrastructure for lower-carbon travel options over vague priorities like "natural feel" of an are, catering to the upset adjacent property owners who view this land as an extension of their properties, exhausting to live in a city where the benefit of everyone is constantly hamstrung by the greed of the very privileged, would become a beloved amenity for the whole neighborhood, not just the a nice space for a few to look out over, saving trees is a goal but not at the expense of pedestrian/cyclist
Approve of dropping north-south Sauk Creek Greenway connection	(OH20) (T6) (OC)	8	City is talking with us, streets provide for a safe and pleasant bike experience that will connect easily to the other bike routes
Approve of east-west Sauk Creek Greenway connection	(OH20) (T6) (OC)	9	East-west path in the Sauk Greenway is an important and needed improvement, desperately need an east-west connection for pedestrians and cyclists between Tree Lane and Old Sauk Road, kids need a safe way to get to Memorial and Jefferson, these are good options and should stay in the plan, cost estimate, helpful for those who otherwise have to go the long way around, will be very important for east-west cyclists using planned future beltline crossing
Oppose to Middleton Street Connection	(EF) (OH20) (T6) (MSV)	219* (203 on signed Opposition to the Proposed Middleton Street Connection Petition) *Received WVHA Board Opposition via email from Board President	Remove from consideration any proposal to remove Middleton Street barricade at Stricker's Pond and extend the street to connect with the City of Middleton, preserve the character of the neighborhood by stopping the connection, Wisconsin Supreme Court Decision (1991), connection could increase traffic counts from 125 vehicles daily to as many as 3500-4000, Middleton Street would become a direct shortcut into downtown Middleton, traffic increase would impact safety for residents, the sensitive wetland habitat (turtles and other wildlife) of Sticker's Pond, discourage active transportation, may increase vehicle conflicts, property damage, and serious injuries or fatallities due to steep 6% grade traveling West from Gammon Road, road is a local road-not collector, current configuration works for pedestrians and bikers, promote the walking paths around both Stricker's and Tiedeman Ponds, should identify potential locations for adding modal filters, more likely to speed on residential streets like this one in neighborhoods that aren't theirs, Madison is so eager to be upwardly mobile that they are at risk for trading all the features that make it special, Perhaps a better strategy would be to widen Gammon Road or to add bus service between Middleton and West Madison, Buses that serve the Middleton Gammon Road area turn on Old Sauk Rd and there is no connection that goes to the West Town mall or southwest Madison businesses
More protected bike lanes	(OH20) (T6) (OC)	10	Want barriers or curb or separated instead of paint, Paint is not infrastructure, would like to see more physical barrier to protect our cyclists, on street bike routes should be protected to allow for more people of all ages and abilities to use them
Approve Owen Conservancy/Crestwood neighborhood pedestrian/bike connection	(OC)	2	Great addition to allow for more north south shared-use paths
Oppose Owen Conservancy/Crestwood neighborhood pedestrian/bike connection	(OC)	2	Concerns the connection will disrupt wildlife
Approve of road diets	(OC)	2	Would like to see reductions in car travel lanes on Gammon Road and Mineral Point road, would improve pedestrian safety and reduce speeds of car travel
Oppose road diets	(OH20) (OC) (T6)	5	Road diets for the purpose of a bike lane, Take bicycles off major roads and on to side streets, oppose any road diet on MPR

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Oppose Rennebohm planned streets	(OC) (T6)	5	Concerned if changes to a road, it'll change to concrete sidewalks and not be blacktop walking paths, oppose the creation of any "green streets" south of Sheboygan Ave, due to increase traffic, safety issues and proximity to private homes on Buffalo Trail, most effective way to prevent through traffic is to not build a through street, effectively lower the amount of buildable housing here because of this planned street, properties would be better able to connect to the park without a (proposed) street in the way
Rennebohm shared use path	(OC)	3	Converting the sidewalk into a shared path is a good start, but should use Regent to use a bike lane to prevent pedestrians from needing to cross in front of bikers, oppose removal of any trees in Rennebohm Park for expansion of bike path, adding a shared-use path would be great
Support Segoe Road pedestrian/bike improvements	(OH20) (OC)	4	Support for protected bike lanes and ped improvements on Segoe south of Regent, like changes made at intersection -hugely improve pedestrian (especially kids) access to the park from both sides of Segoe Road, new crosswalks should be raised to force cars to slow down
Support Gammon Road pedestrian/bike path	(OH20) (OC)	2	Shared use path should extend further south to the mall
Approve of bike facilities on High Point & Westfield (N-S greenway alternatives)	(OC)	5	Should be protected lanes, would prefer greenway path, but protected on-street facility is good too, excited to see car infrastructure being replaced with bike and pedestrian infrastructure
Oppose on-street bicycle facilities	(OC)	2	Plenty of existing options for bikes along High Point & Westfield, don't reduce parking on High Point Rd
Oppose Old Sauk Road changes	(EF) (OC)	2	Old Sauk is 2 land road with lots of existing traffic, hard to make left turns, should not resemble East Washington corridor, car racing concerns through Parkwood on Blue Ridge and Everglade to get to Mineral Point Road or the beltline, Oppose any and all options to evaluate or reconstruct North side of Old Sauk Rd
Appalachian Way	(OH20) (T6) (OC)	5	22d, "extend Appalachian Way to the east, then south to connect Old Sauk Rd," But Sauk Ridge Trl to Appalachian Way is still in the map, no need to extend Appalachian Way, ability to see on-coming westbound traffic is limited at the western entrance and appears to take out the bike path, the eastern entrance will require entering on a steep slope which will be problematic during the winter months - intersection at hill may have vision issues, oppose connecting any and all streets as discussed in 22d,, Appalachian Way, Old Sauk Rd, and Cooper Lane Bike Path
Yosemite connection	(OH20) (OC)	7	Concerned will have to pay towards re-connecting it, original design of through street was steeply sloped and people were driving so fast downhill they caught air - reconnecting will recreate this safety hazard, disregard previous safety concerns of when Yosemite Dr, ran from Yellowstone to Old Sauk Road and reconnect the roadway to its former condition and locations, Connecting Yosemite Place to Yosemite Trail makes no sense- will only increase traffic cutting through the neighborhood-should look at improving efficiency of existing major throughfares and ways to divert traffic through residential neighborhoods, add modal filter
Approve of Marshall Park connection	(T6) (OC)	3	Would be great to allow more pedestrian/bike access to the park and the businesses along Allen
Approve of Bus Rapid Transit (BRT)	(OC) (T6)	2	BRT is great investment and great use of federal grants
Want changes to bus system	(T6)	2	Bus service between Old Sauk Road and Tree Lane on Gammon Road is missing. BRT should have its own lane for the entire route
Approve of bike paths and bike infrastructure shown in plan	(OH20) (T6) (OC)	31	Like multiuse paths, like east/west bike and pedestrian connections, like bike path on Camelot, yes to protected bike lane on Midvale from University to beltline-but Midvale needs resurfacing to be safe for bikers, like path connecting by Odana Hills Golf Course, like on-street facility on Mineral Point Road and Westfield Road, like shared use path shown on east side of Gammon Road, shared-use path along University Ave, Wexford Pond Greenway, shared use path along Regent street from 5' to 10' because of Regent and Eau Claire being wide with long, continuous medians, that limits peds/cyclists' access to the park, do not need free on-street parking at the expense of safe biking facilities, Bike path map should be corroborated with Google Maps bike routes and Strava bike routes that show what routes people actually are traveling, great change to better utilize public land, wayfinding, approve of east-west route through Gillespie MS & Memorial HS, approve of improvements to MPR & High Point intersection, support protected lane on Regent, improving the
Approve of transportation improvements	(OH20) (T6) (OC)	16	Children's safety going to school, help reduce traffic and allow children to be more independent, pedestrian underpass beneath the railroad between Old Middleton Rd,, needed on WB Odana, there's a very short stretch of two lanes before merging down to one, and bikes are in mixed traffic while cars are negotiating that, "improvements" can otherwise sound like "nice to have's", not "life-saving interventions", sidewalks surround kids safety, Tomahawk/University improvements, bump out improvements are welcomed great for safety, and should be used more throughout the city, removing Laub helps reduce the number of conflict points in the area without limiting mobility
Would like to bike path improvements	(OH20) (T6) (OC)	6	Make Keating a bike boulevard up to Owen, Place a bike/ped activated stop at Midvale, This route allows access to the SW path or less traveled streets in Sunset neighborhood and beyond, Mineral Point Rd, from Midvale is too steep for many bicyclists, Keating is a much more gradual grade, Bike lanes on Mineral Point Road from Segoe to Midvale will never be bike friendly, Utilize South Hill Drive rather than Mineral Point Road, And in the future, Keating Terrace rather than Mineral Point Road, bike lanes on Tokay as proposed need to continue to the SW bike path, Utilize Keating Terrace, need north/south bike and pedestrian connectivity, opposed to adding an unprotected bike path to Midvale Blvd-dangerous without physical barrier-more cost effective to make improvements to side streets than using Midvale, barrier or signage on Tokay Blvd for safe biking, planned bike lanes on Mineral Point between Segoe and Midvale will make things more hazardous for bicyclists-use Keating Terrace instead, extend a bike lane on Tokay to connect with SW bike path
Would like to see more safety improvements	(OH20) (T6) (OC)	17	With the numerous projects and re-designs of streets and crossing for cyclist and pedestrian safety, the city should implement features like curb bump outs, raised crossing, daylighting, etc., for any categories of crossings or street, Safety road calming in front of the schools on Segoe, adjust proposed road on the north side of Rennebohm Park so it does not connect to Segoe Rd- that connection will turn the proposed road into a bypass for Sheboygan Ave, It will be a danger to kids using the park and those who are tending the community gardens, Crossing of Beltline between Old Sauk and Mineral Point need for ped/bike, safer pedestrian crossings especially near schools, need more safe crossings of Gammon Road near West Towne and Memorial High School, add bump outs to protect the children, Colony Drive pedestrian crossing at Gammon Road for Muir Elementary and other improved crossing to reduce need for school bussing, intersection of Tree Lane and Mineral Point Road needs no left turns and no U-turns on Mineral Point Road, intersection of Tokay Blvd and Segoe Road, make the crosswalk on Odana Rd and Charles Ln more visible (blinking light as additions have helped but are not enough), Speed enforcement is not a long term solution-Roads need to be engineered for slower speeds, redesigning the roads to slow cars down, modal filter on Tree Lane to make a not through street and reduce traffic, modal filter on Cable Ave/Beach St, modal filter on planned street from Enterprise Lane, modal filer on Berwyn Drive; Whitney Way-South Hill Dr ped improvements; Tree Lane/MPR/Beltline needs bicycle safety improvements, along University Avenue from Allen Blvd to Heim Ave is difficult for bikers on the shared-use path as drivers turning right do not see bikers, any plans to disallow turning right on red on some intersections near schools - Turning right on red can be very dangerous for pedestrians because drivers' attention is primarily on oncoming traffic to the left, not on any pedestrians who might be crossing to the right

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Want less emphasis on bikes	(OH20) (T6) (OC)	7	Can never be Portland, Make bikers pay their share for all the lanes, special plows, extra work, etc., There isn't even a license fee for them, while the rest of us pay for it, pedestrians are on top of the pyramid in Comprehensive Plan, bike recreation are overtaking common sense, Minimize concrete and asphalt, biking on sidewalks is permissible, failsafe, parallel streets to avoid high volume streets, overkill, pedestrians and disable people first; spending money on making bicycling easier is inequitable
Less Parking	(OC)	5	Reducing on-street parking is a high priority, makes walking and cycling much safer by increasing visibility of pedestrians and reducing the chance that biker will get slammed by an opening door, removing parking mandates can free up a large amount of room to plant trees for canopies, don't need additional parking by Rennebohm
More Parking	(T6)	2	Do not remove parking from residential streets, more parking planned in BRT riders
High Injury Network (HIN)	(OC)	4	Flagged Felton Pl and Falles Ct with 0 injuries, Midvale/Mineral Point Rd intersection is not flagged and has many injuries, Any high injury network should be scoped out to slow down speeds; "HIN map is incorrect"
Beltline crossings	(OC)	4	Pedestrian and cyclist safety is top concern, extending shared use path would be great to allow for crossing the area safely, would be a huge improvement to extend the wide sidewalk/shared-use path under the highway on Gammon Road, Beltline should not cut off people from the city if they aren't in a car, underpass improvements at Struck St, will improve cyclist access
Walkers do not feel safe on multipurpose paths	(OH20)	2	We need safe quiet trails for pedestrians, many walkers are NOT safe on the so-called multipurpose bike trails
Miscellaneous	(OH20) (OC) (T6) (EF)	(single comments - see details at right)	City is intentionally making it difficult for people to drive, minimalist approach that helps people traverse the area rather than creating a major thoroughfare, increasing light along shared use paths by schools will disrupt private homes and wildlife, adding alternate student drop-off/pick-up area on Cable Ave does not work well due to the bike/ped connection between the end of Regent St and start of the path on Inner Dr,-will bring more traffic on this street; Do not replace RR with multiuse path if there is ANY chance it could be used for light rail or Chicago connection, more lightening is great for visibility along the path, giving children the ability to travel independently is one of the best things we can do to build a connected community, we should allow the parking lots to be developed into more useful buildings, Mineral Point Rd between Segoe and Midvale does not meet the criteria for a community connector because the volume of vehicles, city should study alternative uses for the rail corridor-Like revitalizing it to be a Metro light rail corridor for the city since Madison will need rail if it continues to grow and densify like it needs to, oppose limiting affordable housing development near Beltline-SUPPORT for affordable housing projects that include noise mitigation along the Beltline, like walking trails and being able to ride bike, approve of street and roadway improvements like Support Berwyn Dr/Laub Ln realignment, Support using parking lots as park and ride, Like planned streets in West Towne and Odana Road areas, oppose making St, Dunstan's a two way street due to more traffic through residential road, The BRT overlay in Hill Farms as it bisects the neighborhood and will (not if but when) create strife among neighbors especially when someone living on the edge of the BRT overlay is faced with a multi-unit or multi-story complex next to their single-family home, Remove northbound Whitney to Odana free-flow right turn, lack of turn lanes outside the immediate intersection, Like shown traffic
NOTE: sidewalk comments summarized in Health & Safety section			
Neighborhoods and Housing			
Want more missing middle housing	(VLZH28) (OH20) (T6) (OC)	8	Not sure how plan encourage missing middle housing, homes, condos, duplex, townhouse, apt, Whatever people can own to build their generational wealth, build small footprint community, City needs to work on a plan to develop housing options like condos, small residences, etc., that allow people to build equity, downsize, feel ownership in the community, The "missing middle" is still missing despite all the city's high density, rental focus, frustrated that City says it can't be done or is difficult, more missing middle in area well connected by shared use paths, need to update GFLU to be LMR or NMU at the minimum,
Affordable housing	(OH20) (OC)	4	Affordable middle housing is needed and fits into our neighborhoods nicely, Affordable home ownership houses, Small houses on small lots-Veridian will build these, Housing needs to help lower income families actually own a piece of property, Small row houses help, Small affordable residential units, attached are fine, hope TIF funds help allow for reduced median rent development
Support Lussier recommendations	(OC)	2	Community hub, essential resource for many, encourage any expansion of programing
Want livable neighborhoods for families	(OH20)	2	Consideration for retaining and maintaining the quality of our neighborhoods
Reduce barriers to development	(OC)	2	The city should consider implementing standard pre-reviewed building plans to make building easier, The city should work to make it easier for up zoning to occur to allow for more housing throughout the city to address the housing crisis, Consider automatic/dynamic zoning, Work with existing property owners to redevelop underutilized parking lots into mixed use buildings
School comments [staff note: the City does not control school attendance areas or school expansions]	(OC)	1	Midvale and Lincoln should have identical attendance areas as they are paired schools
attendance areas or school expansions] Miscellaneous	(OC) (LZH3) (OH20)	(single comments - see details at right)	Would like to see additional senior housing in the area - for folks to stay in the neighborhood, free up some sf homes, Like encouraging developers to add a wider selection of housing for various incomes and family sizes, and adding noise mitigation to development along the Beltline, See land use changes as promoting apartments but would like to see more plans for owner-occupied housing, Want more play areas for children near and in all neighborhoods, Oppose affordable house since affordable housing just makes developers rich, More housing options like condos and differing apartment styles, Great steps to making our community more diverse and improving the economic opportunities for historically marginalized and under represented groups, especially by making use of underutilized commercial/parking areas, I would like to see the language strengthened around these items, These items are really important and only succeed if a real effort is made, "considering" land banking does not mean much, like language surrounding middle housing but plan does not facilitate this land use, Daycares should be exempt from parking requirements to make retrofits of existing buildings more feasible, support increased child care opportunities
Economy and Opportunity			
TID	(OC)	1	TID should include single-family areas where we expect/encourage redevelopment to occur
Utilities	(OC)	1	Support studying capacities to permit new development
Culture and Character			
Cartaire and Character			

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Urban Design District 6	(EF) (OH20) (OC)	4	Concerns that this will lead to single-family housing converting to multifamily housing more than what is allowed by TOD, boundaries should not extend to part of University Ave,, but just properties along University Ave, UDD plan to cram several hundred more apartments, etc., in the Rennebohm area, like mixed use zoning and greenspace in considerations, deemphasizing parking will make it safer for pedestrians and cyclists, more tree canopy will make trail less hot, trail is noisy, exhaust fumes from University Ave, slow speed on University Ave; support alignment of UDD 6 w/TOD requirements, approve of considerations, approve of less car-oriented areas
Lack of defining views	(OH20) (T6)	3	Most are already gone!, lost the views, don't want to lose more vistas-reduce building heights, Madison Yards blocks views of Capitol
Large public spaces	(OH20) (LCD)	3	Large spaces for public meetings with good acoustics; need "town square" area for performances & exhibitions (like Middleton's Stone Horse Green), more places to sing in groups, inclusive public swimming pools
Historical Resources	(OC)	2	The Grayson Building at 4414 Regent Street has architectural significance, is relatively unaltered since it was constructed, and warrants protection from demolition and alteration, The Old National Bank (formerly Anchor Savings and Loan) at the corner of Midvale Boulevard and Vernon, has architectural significance, is in excellent condition, and should be protected from demolition or alteration,
Approve of Design Guidelines	(OC)	2	Engagement while creating walking paths is often overlooked-glad the plan addresses this, buildings are meant to serve people, not cars, People should not be forced to walk through parked cars to arrive at their destination, outdoor dining option are great and make an area feel vibrant
Approve of culture character actions	(OC)	2	Approve of promotion of community areas and having a sense of place, community events are a great idea,
Miscellaneous	(EF) (OH20) (OC)	(single comments - see details at right)	Public art is great for giving the area a unique feeling, like Plan recommendations to add to West area art venues and infrastructure, Maintain historic designation of Hill Farms since BRT overlay cuts neighborhood, a place of worship is on the map and is labeled as a "community gathering place" when it's decidedly hostile to LGBTQ+ members of the community, Add to Design Guidelines: driveway entrances to a property should not be directly connected to the main road/highway in the vicinity to avoid vehicle/pedestrian conflicts (University Ave, Gammon, Mineral Point, etc)
Green and Resilient			
Approve of greenspace shown in Plan	(OH20)	2	City staff's expertise and understanding of how the way we interact now with the land and water called "Madison" will impact how the children and all future generations will be able to live here
Approve of parks shown in Plan	(OH20) (T6) (OC)	6	Plan for more parks south of Mineral Point Rd, future West Towne/Odana Road parks; include smaller parks as well; Focus on new parks in West Towne Area over Odana area
More trees and greenspace	(OH20) (T6)	3	Like tree-lined streets, need more trees and open space, less concrete
Drainage improvements	(OH20)	2	Drainage improvement is necessary if done with minimal impact to the vegetation, needs to be improved with minimal damage to canopy
Protect trees	(OC)	3	Trees (should be protected) along the east side of Whitney Way across from Garver Park (NE corner of Mineral Pt and Whitney Way) contain a long standing population of cicadas (should be documented) that sing every summer; no trees should ever be cut down
Rennebohm Park	(OH20) (OC)	5	Rennebohm Park is underutilized, Add more amenities to the further reaches of the park, Could be as simple as more picnic tables scattered around the edges, Bicycle education/traffic enforcement for safety purposes, will be good if Rennebohm Park becomes larger, which can include greenway on the right side of the park, blacktop walking paths need better lightening -don't change to concrete; support path connection to Sheboygan, expanding the park would be great
Stormwater concerns	(OC) (T6)	2	Concern about new developments adding to rainfall runoff issues, New developments should take in consideration any runoff issues and no new development should be built unless and until developer and city identity and plan for any issues, like recommendations but more can be done-more places where streets can be narrowed and parking removed so better stormwater management and rain gardens can be put in while also improving pedestrian safety, want minimalist approach to stormwater management
Golf Course uses	(OC)	2	Support replacing clubhouse w/ more community-serving facility, there are potential ways large properties can be used
Approve of greater access to parks/lakes	(OC)	3	Access to Zook Park from SW commuter trail
Miscellaneous	(OC) (T6) (OH20)	(single comments - see details at right)	Recreational biking is a great idea, greenspace north of Vernon Boulevard could be used as stormwater management, connect greenways with a mixed use path, Need to address erosion, Approve of greater access to Zook Park from SW commuter trail, Approve of stormwater improvements, support additional greenspace shown for Vernon Blvd, Build trails through woods along Whitney way/Garner Park, Greater attention to the plight of the non-human animals and the vegetation that they depend on to survive, Study water and sewers in West Towne area and north, artesian wells
Effective Government			
Loans programs to assist needy	(OH20)	2	Madison needs to: have a City loan plan for those who need, provide an avenue for those with reduced incomes to actually own a small house (even an attached house counts), Home ownership builds a family and people-centered community,
Miscellaneous	(WRM) (EF) (OH20)	(single comments - see details at right)	Forum for connecting with smaller developers to see what they would need to support 'by-right' development and removing barriers like requiring variances or special permits, distrust of City, Better signage, West area plan needs to include is a more specific approach to planning that clearly examines the impact of any new housing development on the school systems and the existing infrastructure, schools to not have capacity to absorb new students, lack of an infrastructure including taxes from developers is not sustainable and costs are being passed to single family home owners, Where are the funds coming from? The City is already in debt- no one has an answer
Health and Safety			
Support adding sidewalks to Old Sauk	(OC)	2	Add sidewalks on both sides of Old Sauk Rd along entire length
Approve of sidewalks shown	(OC)	5	Locations: Along Old Sauk - really impactful, especially kids who walk to school; support MPR-Highpoint/Gammon/Grand Canyon improvements
Oppose adding any sidewalks	(OC)	4	Especially on cul-de-sacs and dead ends, but generally anywhere
Approve of safety recommendations	(OC)	2	Pedestrian safety and comfort is a great way to help communities connect
Approve of partnerships to have health and recreation programs	(OC)	2	Nature Everywhere, MSCR, Police Foundation
Additional sidewalks	(OC)	2	More direct connection from Gammon/Beltline intersection down to West Towne parking lot, 4800 block of Sherwood and the 5100 block of Holiday have sidewalks missing on both sides

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS	
Miscellaneous	1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1		Adding more police to an area rarely makes an area safer, The city should instead be investing in more and higher quality social services and housing to build community rather than the most expensive and least effective way of solving problems, armed agents of the state, oppose adding sidewalks to Old Sauk due to lack of room, steep hil limited visibility, high traffic counts, topography, and possible cost of retaining walls	
Planning Process/Other				
Like meetings for citizens	(OH20)	7	Like the charts, maps, and especially the professionals, like open-house format, chance for face to face discussion with City staff, Very well designed and organized meeting! Access to staff and their terrific knowledge is wonderful, Staff are so articulate and uniformly polite and patient, Plenty of storyboards and people who are knowledgeable and can explain them,	
Do not like meeting format	(LZH3) (T6)	2	Do not like writing questions, no dialogue, would like an open mic,	
More information on planning process	(OH20)	2	More on the process of staff recommendations > City commissions > City Council and how a draft recommendation can be removed at the staff level based on community input, want storyboard on process of changing a staff recommendation,	
Miscellaneous	(OH20) (OC) (T6)		Like to see plans evolve over time, Do not support plan, Need to listen to the community- Community is against a paved path and the resulting destruction of trees, Definitions of acronyms on the charts make charts hard to understand, Designs in business are always with cost in mind, There seems to be no thought to that in these design processes, With assessments going up, some owners are afraid they will lose their homes, do not like online map as it is hard to post comments and use	

Phase 4 Engagement

(OH20) In-Person Open House - All topics May 20

(VIZH28) Virtual Land Use/Zoning/Housing Meeting May 28 (VT30) Virtual Transportation Meeting May 30 (LZH3) In-Person Land Use/Zoning/Housing Meeting June 3

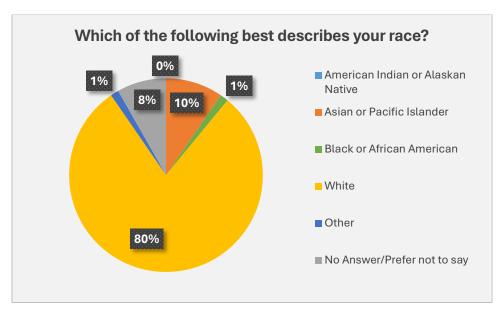
(LCD) Lussier Community Dinner June 7 (WRM) Whitney-Regent Meeting June 10 (MSV) Middleton Street Site Visit June 20

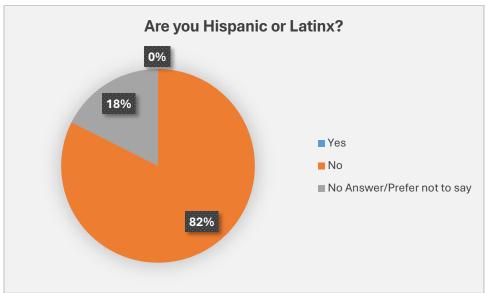
(EF) Emailed Feedback

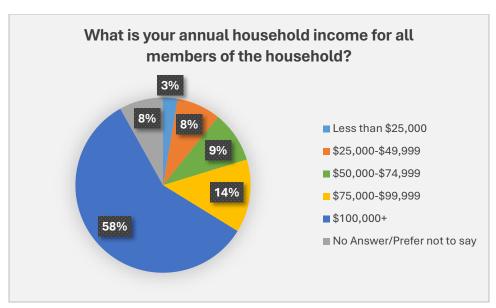
(OC) Online draft Map and Action Comments

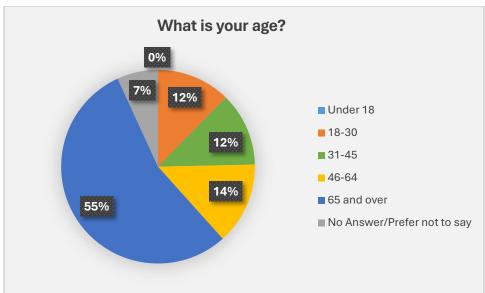
Demographic Summary

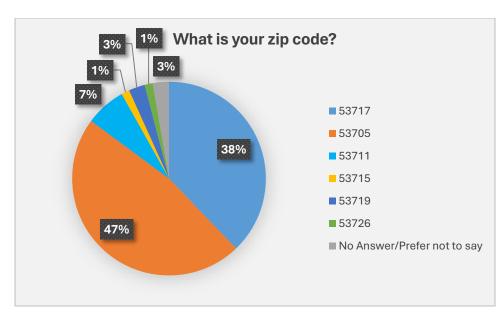
Demographic questions were asked of community survey respondents. Those are summarized here and are generally consistent with the virtual and in-person meetings of May and June.

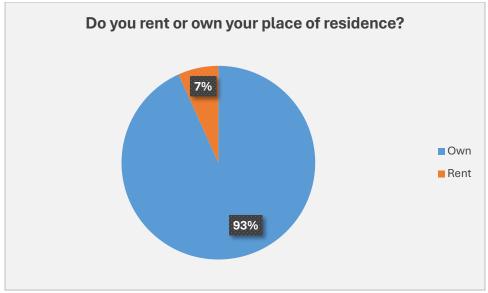












Department of Planning & Community & Economic Development



Planning Division

Bill Fruhling, Interim Director

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To: Mayor, Common Council and (BCCs)

From: Ben Zellers and Linda Horvath, West Area Plan Co-Managers

Re: Adopting the West Area Plan

Date: July 10, 2024

Introduction

Attached is the Final Draft West Area Plan submitted for review and approval. Along with the Northeast Area Plan, these are the first plans prepared under the City's Planning Framework, which established 12 permanent plan geographies covering the entire city and peripheral growth areas. While the Planning Division led plan development, its creation was an interagency-effort with collaboration and contributions from a number of other City agencies including Parks, Engineering, Community Development, Economic Development, Transportation, Traffic Engineering, Streets, Forestry, Water Utility, Public Health, Fire, and Police.

Public Participation

There were many opportunities for the public to participate in the planning process. This included an initial phase exploring the public's thoughts about issues and opportunities, to the more recent phase which focused on final public meetings to review the draft plan. The recent phase was extended to respond to a large volume of public comments and included an additional five public meetings. Throughout the planning process, staff used multiple engagement strategies to reach a broad range of participants. These included traditional public meetings (both in-person and virtual), multi-agency open houses, outdoor plan events, plan pop-ups at community events, focus groups, an arts-focused public meeting, and interactive digital engagement. The Planning Division also contracted with Community Partners to help reach and get feedback from under-represented populations to ensure their voices are reflected in this plan and conducted a business survey in coordination with the Office of Business Resources. Public feedback summaries from throughout the process are available on the project website.

In addition to public engagement, the West Area Plan team met with Boards, Committees, and Commissions likely to see plan referral during the approval process. These mid-process check-ins ensured interim feedback could be considered and incorporated during the planning process instead of the approval process. For links to various meetings and materials please see the <u>project website</u>.

Changes to Draft Plan Based on Public Feedback

Staff made edits to plan drafts in response to public feedback. This included changing future land use back to Low Residential from Low Medium Residential along Merrill Crest near the corner of Whitney Way and Regent Street and removing proactive rezoning there, removing proactive rezoning from the Highlands Neighborhood, changing future land use back to Low Residential from Medium Residential for the Hill Farms Pool site, changing transportation access on Segoe Road by removing parts of Laub Lane and focusing access at Berwyn Drive and adding pedestrian crossing improvements and removing the north-south shared use path from the Sauk Creek Greenway.

City Board, Committee, and Commission Review

The anticipated review and approval schedule is as follows. Any public feedback received during this phase will be forwarded to the BCCs reviewing this plan.

July 16 – Common Council introduction

July 17 - Economic Development Committee (5:00 pm)

July 31 - Transportation Commission (5:00 pm)

August 5 - Landmarks Commission (5:00 pm)

August 14 - Urban Design Commission (4:30 pm); Board of Park Commissioners (6:30 pm)

August 26 - Plan Commission (lead; 5:30 pm)

September 10 - Common Council (6:30 pm)

The West Area Plan is accompanied by both an adopting resolution and an ordinance amending the Comprehensive Plan. The resolution includes archiving previously adopted underlying plans. The Comprehensive Plan amendment (ordinance) is limited to key maps, such as the Generalized Future Land Use and Bicycle Facilities map, among others.

After plan adoption, staff will incorporate any changes made during the approval process. Minor formatting changes, such as moving/resizing graphics or adding supplemental images, may also be made to improve the usability of the document.



File ID: 84328

City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84328

File Type: Resolution Status: Council New

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 07/05/2024

File Name: Approving Landfill No. 3 Contract Final Action:

Title: Approving the Negotiated Agreement Relating to the Dane County Landfill No. 3

and the First Amendment to the Solid Waste Agreement with Dane County.

(District 16)

Notes: ApproveLandfillNo3

Sponsors: Satya V. Rhodes-Conway Effective Date:

Attachments: Negotiated Agreement Relating to Dane County Enactment Number:

Landfill No. 3.pdf, First Amendment to Solid Waste

Agreemernt.pdf

Author: Doran Viste Hearing Date:

Entered by: mglaeser@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Attorney's Office	07/05/2024	Referred for Introduction				
	Action Text:	This Resolution was Referred for Introduction					
	Notes:	Finance Committee (8/26/24), Board of Public Works (7/24/24), Common Council (9/10/24)					

Text of Legislative File 84328

Fiscal Note

The Negotiated Agreement calls for annual payments to the City of \$125,000, starting the year that the facility begins to accept waste (expected to be in 2028). Payments would be prorated in the first year and increase by 2% each year thereafter. The annual payment is a landfill siting fee and compensation payment, and, in addition to impacts caused by the landfill on City services, is intended to compensate the City's for impacts on the golf operations of the adjoining Yahara Hills Golf Course. Payments will be deposited in the general fund. The negotiated agreement also provides future payments from the County for any daily lost rounds of golf attributable to odor complaints or safety concerns at the landfill. Future payments for refunded greens fees as provided under the Agreement shall be deposited in the golf enterprise account.

Title

Approving the Negotiated Agreement Relating to the Dane County Landfill No. 3 and the First Amendment to the Solid Waste Agreement with Dane County. (District 16)

Body

WHEREAS, on August 15, 1973 the County and the City entered into a waste stream agreement in which the City agreed to close its solid waste disposal facilities and use County solid waste disposal facilities; and,

WHEREAS, on January 16, 1984 the County and the City entered into a land sale agreement that allowed the County to construct and open a solid waste disposal facility located at 7102 Maahic Way in the City of Madison, Dane County, Wisconsin, known as Dane County Landfill No. 2, or the Rodefeld Landfill; and,

WHEREAS, it is projected that the Rodefeld Landfill will reach its maximum capacity by 2029; and,

WHEREAS, on February 28, 2023, pursuant to the authorization granted by the Council in RES-22-00319 (Legistar File ID No. 70597) the City sold a portion of the land used for the Yahara Hills Golf Course to the County for the development of a sustainable business park and solid waste disposal facility for which approximately 200 acres was identified for the construction and operation of the Dane County Landfill Site No. 3; and,

WHEREAS, as part of the land sale for the future Landfill Site No. 3, on June 6, 2022 the County and the City entered into a Solid Waste Agreement for 2023-2032 (the "Solid Waste Agreement") to set forth the terms and conditions agreed to by the Parties regarding the City's solid waste stream, tipping fees, daily cover and other City materials, and other issues associated with the County's landfill operations, as well as the to-be-developed County compost site. The term of this Agreement is from January 1, 2023 through December 31, 2032; and,

WHEREAS, the County has applied to the Wisconsin Department of Natural Resources for a license to construct and operate the Dane County Landfill Site No. 3 pursuant to Ch. 289, Subchapter III, Wis. Stats.; and,

WHEREAS, on May 3, 2023, pursuant to the requirements of Wis. Stat. Sec. 289.22(1m), the City received the County's written notification of its intent to develop Landfill Site No. 3 and its intent to establish a negotiating committee to discuss the terms of the landfill development; and,

WHEREAS, pursuant to Wis. Stat. Sec. 289.33(6), on June 20, 2023, the City adopted RES-23-00465, Legistar File ID No. 78136, a siting resolution authorizing the City to participate in negotiations with the County over the development of Landfill Site No. 3 through the Local Negotiating Committee for Dane County Landfill Site No. 3 (the "Local Negotiating Committee"); and,

WHEREAS, on June 26, 2024, the Local Negotiating Committee, made up of three appointees from the City and one each from the Town of Cottage Grove and the Village of McFarland, approved the Negotiated Agreement Relating to Dane County Landfill No. 3, that agreement now being submitted to the Council for approval under Wis. Stat. Sec. 289.33(9). As allowed for by statute, the Negotiated Agreement addresses operational and environmental issues, as well as residential and municipal compensation associated with the proposed landfill; and,

WHEREAS, during negotiations with the County, issues regarding material testing were

discussed, which County and City staff have agreed would be best handled by a separate amendment to the Solid Waste Agreement, which amendment would be approved and executed by the City and the County contemporaneously with the Negotiated Agreement.

NOW THEREFORE BE IT HEREBY RESOLVED, that the Common Council finds that it is in the City's interest to support the development of Dane County Landfill Site No. 3; and,

BE IT FURTHER RESOLVED, that the Common Council approves the Negotiated Agreement Relating to Dane County Landfill No. 3 as submitted to the Common Council by the Local Negotiating Committee, which agreement is attached and incorporated hereto, and authorizes the Mayor and the City Clerk to execute this document on behalf of the City.

BE IT FURTHER RESOLVED, that the Common Council approves the First Amendment to the Solid Waste Agreement with Dane County, which Amendment is attached and incorporated hereto, and authorizes the Mayor and the City Clerk to execute this document on behalf of the City.

BE IT FINALLY RESOLVED, that the City's execution of the Negotiated Agreement is contingent upon the receipt of an executed First Amendment from the County.

NEGOTIATED AGREEMENT RELATING TO DANE COUNTY LANDFILL NO. 3

Between the County of Dane, the City of Madison, the Town of Cottage Grove, and the Village of McFarland

This Negotiated Agreement ("Agreement") relating to the Dane County Landfill No. 3, entered into by and between the County of Dane, a quasi-municipal corporation in the State of Wisconsin (hereinafter referred to as "County"), the City of Madison, a municipal corporation of the State of Wisconsin (hereinafter referred to as "City"), the Town of Cottage Grove, a body corporate and politic of the State of Wisconsin (hereinafter referred to as "Town"), and the Village of McFarland, a municipal corporation of the State of Wisconsin (hereinafter referred to as "Village") is effective as of the date by which all parties have signed hereunder and according to the terms and conditions set forth herein.

RECITALS

WHEREAS, on August 15, 1973 the County and the City entered into a waste stream agreement in which the City agreed to close its solid waste disposal facilities and use County solid waste disposal facilities; and,

WHEREAS, on January 16, 1984 the County and the City entered into a land sale agreement that allowed the County to construct and open a solid waste disposal facility located at 7102 Maahic Way in the City of Madison, Dane County, Wisconsin, known as Dane County Landfill No. 2, or the Rodefeld Landfill; and,

WHEREAS, it is projected that the Rodefeld Landfill will reach its maximum capacity by 2029; and,

WHEREAS, on February 28, 2023, the City sold a portion of the land used for the Yahara Hills Golf Course to the County for the development of a sustainable business park and solid waste disposal facility for which approximately 200 acres was identified for the construction and operation of the Dane County Landfill No. 3; and,

WHEREAS, the County has applied to the Wisconsin Department of Natural Resources for a license to construct and operate the Dane County Landfill No. 3 pursuant to Ch. 289, Subchapter III, Wis. Stats.; and,

WHEREAS, the City of Madison and the Town of Cottage Grove have adopted Siting Resolutions and appointed members to the Local Negotiating Committee for Dane County Landfill No. 3 ("Local Negotiating Committee"); and,

WHEREAS, pursuant to the requirements of Sec. 289.33, Wis. Stats., the Local Negotiating Committee was formed, which includes four members appointed by the City and one member appointed by the Town; and,

WHEREAS, the County, the City and the Town have agreed in writing that, pursuant to Sec. 289.33(7n), Wis. Stats., the Village, a municipality that does not qualify as an affected municipality, may be added as a party to the proceeding; and,

WHEREAS, the Village has enacted a Siting Resolution and appointed a member to the Local Negotiating Committee; and,

WHEREAS, the Local Negotiating Committee having met numerous times approved this Agreement pursuant to Sec. 289.33, Wis. Stats.; and,

WHEREAS, the governing bodies of the County, the City, the Town, and the Village have approved this Agreement.

NOW, THEREFORE, in consideration of the mutual promises, covenants, and agreements hereinafter set forth, the County, the City, Town and the Village enter into this Agreement, subject to the following terms and conditions:

ARTICLE I

DEFINITIONS

For the purposes of this Agreement, the following definitions apply:

<u>Active Site Life</u> means the period during which Solid Waste is transported to or from the Facility or is Disposed of in the Limits of Waste at the Facility by the County or by any other person.

Agreement means this Negotiated Agreement relating to the Dane County Landfill No. 3.

<u>Authorized User</u> means any person, business, or municipality who is authorized by the County to Dispose of Solid Waste at the Facility.

City means the City of Madison, its officers, its officials, its employees, and its agents.

<u>City Engineer</u> means the City of Madison City Engineer, or designee.

<u>City Water Utility General Manager</u> means the General Manager of the City of Madison Water Utility, or designee.

<u>Commercial Solid Waste Vehicle</u> means a vehicle that is used in commerce to transport solid waste with a gross vehicle weight of 26,001 pounds or more.

<u>Compensation</u> means remuneration for potential impacts from operation of the Facility.

<u>County</u> means the County of Dane, its officers, its officials, its employees and its agents, who is the lawfully proposed operator of the Facility.

Course means the Yahara Hills Golf Course and its staff, customers, or operations.

<u>Dane County Code of Ordinances</u> or <u>DCO</u> means the code for which Dane County has authority to prepare and enforce.

<u>Department of Natural Resources</u> or <u>WDNR</u> means the Wisconsin Department of Natural Resources, or its successor agency.

<u>Designated Waste Disposal Area</u> means the location where Authorized Users are instructed to Dispose of Solid Waste.

<u>Director</u> means Director of the Dane County Department of Waste & Renewables.

<u>Discharge</u> means, but is not limited to, spilling, leaking, pumping, pouring, emitting, emptying, or dumping of Solid Waste or Hazardous Waste in the County.

<u>Disposal</u> and <u>Dispose</u> means the Discharge, deposit, injection, dumping, or placing of Solid Waste at the Facility.

<u>Disposal Operations</u> means any activities directly related to the construction or operation of the Facility including but not limited to daily waste covering, maintenance of vehicles and equipment, placement of soil cover, collection of litter and debris, constructing, surveying, environmental monitoring, environmental testing, repairing, maintaining, and placement of final cover on the Facility.

Eligible Property means a residential property eligible for Compensation.

<u>Eligible Vacant Buildable Lot</u> means an undeveloped lot or parcel of record that is zoned for residential use under the applicable zoning ordinance and is eligible for residential development under the applicable municipal comprehensive plan as of the date of this Agreement.

Emergency means an unforeseen circumstance at the Facility or occurring at any other location in the County that jeopardizes the public health, safety and welfare of persons or property in the County.

<u>Facility</u> means the Dane County Landfill No. 3 and any infrastructure or property that is necessary for Disposal Operations.

<u>Feasibility Determination</u> means the decision and the associated conditions issued by the Department of Natural Resources to the County in response to the submittal of a Feasibility Report for the Dane County Landfill Site No. 3.

<u>Fair Market Value</u> means the cash value of an Eligible Property assuming that the Facility does not exist.

<u>Final Closure</u> means the time at which the Facility ceases to accept Solid Waste within the Limits of Waste, and includes all actions required under all applicable statutes, rules and regulations to prepare the Facility for Long Term Care and to make it suitable for other uses.

<u>Hazardous Waste</u> means any Solid Waste identified as a hazardous waste by the Department of Natural Resources, under Sec. 291.05(2), Wis. Stats., or identified as a hazardous waste by regulations adopted by the Department of Natural Resources in Ch. NR 660, Wis. Admin. Code, or its successor chapters.

<u>Landfill Operations Manager</u> means the County employee who is responsible for the daily operations and maintenance of the Facility and managing Disposal Operations.

<u>Limits of Waste</u> means the total area and volume conditionally approved by the Department of Natural Resources in the Feasibility Determination and subsequently modified and approved in the Plan of Operation as the Disposal capacity area for the Disposal of Solid Waste at the Facility. A preliminary map of the Facility and the Limits of Waste is included in Attachment A.

<u>Local Approvals</u> means any local approval as "local approvals" are defined in Sec. 289.33(3)(d), Wis. Stats., or its successor provisions.

<u>Local Committee</u> or <u>Local Negotiating Committee</u> means the Dane County Landfill No. 3 Local Negotiating Committee, organized under Sec. 289.33, Wis. Stats., and consisting of representatives of the City, the Town, and the Village.

<u>Long Term Care</u> means any activities at the Facility, including routine care, maintenance and monitoring at the Facility following the Final Closure of the Facility.

<u>Madison General Ordinance</u> or <u>MGO</u> means the code for which the City of Madison has authority to prepare and enforce.

<u>MMSD</u> means the Madison Metropolitan Sewerage District created under Ch. 200, Wis. Stats., that provides service to municipalities in Dane County along with its treatment works, facilities and operations.

Mitigation means to lessen or moderate the severity of actions or impacts.

<u>Plan of Operation</u> means the WDNR approved operating permit or plan of operation, and the conditions set therein, for the Facility, as provided for in Sec. 289.30, Wis. Stats.

<u>Practicable</u> means those actions or efforts used or found in actual practice or well-established to be feasible, that are reliable and are efficient.

<u>Pre-existing Local Approvals</u> means any pre-existing local approvals as "preexisting local approvals" are defined in Sec. 289.33(3)(fm), Wis. Stats., or its successor provisions.

<u>Reasonable Person</u> means the standard used to convey general community sensibility as to tolerable, acceptable impacts to neighborhood residents without accommodation to individual, subjective reactions of hypersensitive persons.

<u>Solid Waste</u> means solid waste as defined by Sec. 289.01(33), Wis. Stats., and includes any garbage, refuse, sludge from a waste treatment plant, water supply treatment plant, or air pollution control facility, and other discarded or salvageable materials, including solid, liquid, semisolid, or contained gaseous materials resulting from industrial, commercial, mining, and agricultural operations, and from community activities and conditionally approved for Disposal at the Facility by the Department of Natural Resources in the Plan of Operation.

<u>Storage</u> means the authorized holding of Solid Waste at the Facility for a temporary period of time at the end of which period the said Solid Waste is to be then transported offsite, composted, processed, treated or ultimately Disposed of in the Limits of Waste or at any other location pursuant to WDNR approved practice and procedures.

<u>Streets Superintendent</u> means the Streets Superintendent of the Streets Division of the City's Department of Public Works, or designee.

<u>Sustainability Campus</u> means the Dane County Landfill No. 3, adjacent business park development, and associated activities.

<u>Town</u> means, unless the context requires otherwise, the Town of Cottage Grove, its officers, officials, employees and agents.

<u>Unsubstantiated Complaint</u> means a complaint or condition which has been determined by the County and the City, Town, or Village as not likely to have been caused by the Facility or Disposal Operations.

<u>Village</u> means, unless the context requires otherwise, the Village of McFarland, its officers, officials, employees and agents.

Waste Facility Siting Board means the Wisconsin Waste Facility Siting Board, or its successor agency.

ARTICLE II

TRANSPORTATION

A. ACCESS AND HAUL ROUTES

The County shall direct all Commercial Solid Waste Vehicles to use USH 12/18 and CTH AB as the principal haul route to the Facility. Reasonable accommodations shall be made to allow Commercial Solid Waste Vehicles alternative access to the Facility in an Emergency and when significant highway construction occurs on USH 12/18 or CTH AB near the Facility. Siggelkow Road shall not be used as a haul route to or from the Facility. Commercial Solid Waste Vehicles serving properties in close proximity to the Facility may use local roads, including Siggelkow Road, to access the haul routes and the Facility.

B. VEHICLE LITTER CONTROL

The County shall impose a uniform policy regarding securing and covering loads to prevent unreasonable problems involving litter or threats to public safety from windblown debris generated from vehicles transporting Solid Waste to or from the Facility. The County shall require vehicles carrying materials which can leak, fall, be blown, or otherwise be dislodged from the vehicle, to be constructed and loaded so as to prevent their contents from dropping, sifting, leaking or otherwise escaping therefrom and to be covered or appropriately secured in order to enter or leave the Facility while transporting Solid Waste.

The County shall monitor inbound and outbound vehicles and require all Authorized Users to inspect vehicles after unloading to insure that all loose debris which could be blown out of the vehicle is removed. It shall be a violation of the Facility's policy, for which the County can impose a monetary penalty or deny entry, to leave debris in vehicles or to allow for debris to exit the vehicle on Facility roadways or anywhere outside of the Designated Waste Disposal Area. As allowable by Sec. 348.10 (5)(a), Wis. Stats., the County may allow vehicles carrying bulk material such as sand, gravel, dirt or other materials to enter, use, or exit the Facility without a cover.

C. LITTER PICKUP

The County shall inspect for and collect litter generated from vehicles of Authorized Users as necessary on and alongside USH 12/18, CTH AB and any such other roads within $\frac{1}{2}$ mile of the Facility pursuant with Articles III and V of this Agreement.

D. MUD TRACKING

The County shall maintain the paved entrance area to the Facility and adjacent public roads free of mud generated from vehicles exiting the Facility. This shall be done by performing daily inspections for mud, maintaining paved or graveled access roads and gravel tracking pads inside the Facility, and by sweeping or washing down paved roads inside the Facility and adjacent to the entrance.

The County shall maintain ready access to street sweeping equipment. Upon its own initiative or request by the City, the Town, or the Village, the County shall employ this equipment to remove mud from any paved street or road used by Authorized Users within 1/2 mile of the Facility entrance.

ARTICLE III

FACILITY OPERATIONS

A. LOCAL APPROVALS

The County, its officials, its officers, its employees and its agents shall be subject to, and shall comply with all applicable Local Approvals and pre-existing Local Approvals. The Limits of Waste, the Facility in general, and the operations at the Facility shall be subject to all applicable pre-existing City and County ordinances and approvals. In particular, but without limitation, all pre-existing local zoning approvals are applicable to the Facility.

B. SOURCE OF WASTE

No more than 5% of waste Disposed of at the Facility may be waste generated outside of Dane County unless an exception is specifically authorized by the County Board of Supervisors. Waste generated at the Sustainability Campus as a residual of recycling or normal business activities is considered waste that is generated within the County. No action will be taken by the County Board to allow Disposal of more than 5% of Solid Waste from outside the County without first providing 30 days' notice to the City, the Town, and the Village.

C. HOURS OF OPERATION

The County shall only receive Solid Waste at the Facility between 6:00 a.m. and 5:00 p.m., Monday through Friday, and 7:00 a.m. and 2:00 p.m. on Saturdays. The County may operate equipment up to 30 minutes before the hours listed above to facilitate opening the Facility. The Facility may receive Solid Waste from 7:00 a.m. to 5:00 p.m. on Saturdays following either:

- 1. Any legal holiday recognized by the City of Madison or Dane County; or,
- 2. A week within which adverse weather conditions (such as high winds, rain, extreme cold, ice or heavy snow) have prevented local municipalities from collecting waste for at least one (1) day or the County from operating the Facility for an aggregate period of more than five (5) hours in any one day.

Subject to applicable City Ordinances, the County reserves the right to conduct construction activity at the Facility in a manner to achieve an expedited schedule based upon seasonal and weather conditions. The above operating hours are not applicable to construction activity.

The above operating hours are not applicable when it is necessary for the Facility to extend its hours to be able to accept Solid Waste generated by an Emergency.

D. YAHARA GOLF COURSE OPERATIONS

The County is constructing the Facility on land formerly owned and operated by the City as the Course. The Course is operated as an enterprise under the City's Parks Division and the authority of the Parks Superintendent. Under the terms of separate intergovernmental agreements

and leases, as the Facility is constructed and operations commence, the Course will be reduced from 36 holes, to 27 holes, to eventually 18 holes, all of which will operate directly adjacent to the western boundary of the Facility as it is developed and operations commence. The City intends to continue Course operations during the lifetime of the Facility, which, given the outdoor element of the activity and the specialized nature of the Course property, presents distinctive issues to the City and the County that must be addressed. Operational concerns relating the Course are therefore addressed where appropriate.

E. PREVENTION OF NUISANCE CONDITIONS

The County agrees that it shall take whatever precautions are reasonably Practicable to prevent and mitigate nuisance conditions. The County will publicize contact information of appropriate staff and maintain an online system for reporting the conditions and respond to the complaints pursuant to Article V of this Agreement. The County acknowledges that the Facility is subject to the City's noise control ordinance Sec. 24.08, MGO, and shall maintain operations consistent with that ordinance.

F. ODOR ABATEMENT

The County agrees to exercise reasonable, Practicable efforts based on then current technology to control odor from the Facility to mitigate odors offensive to a Reasonable Person. Odor will be controlled by proper landfilling operations: applying daily cover or an alternative daily cover approved by WDNR to all Solid Waste and through implementation of a gas control system. The County agrees to maintain the active gas control system at the Facility for the life of the Facility and as long thereafter as is reasonably determined necessary to control gas migration or odor.

G. DUST ABATEMENT

The County agrees to exercise reasonable, Practicable efforts based on current technology to minimize dust from the Facility operations. Dust is generated during dry conditions, and mainly attributed to vehicular traffic on roadways at the Facility. Dust is also generated from open, disturbed, unvegetated land.

The County agrees to maintain a vegetative cover on all areas at the Facility which are disturbed and not being actively used, to control windblown dust.

A water truck shall be available at the Facility and shall be used for watering access roads, borrow areas, and other areas where dust may be generated whenever conditions are conducive to dust generation.

H. LITTER CONTROL

The County will comply at all times with this Agreement concerning blowing debris and shall operate the Facility in complete compliance with present and future applicable federal and state statutes, administrative code requirements, regulations, and WDNR, including but not limited to, requirements as per the current amended version of Ch. NR 500 and Ch. NR 506, Wis. Admin. Code, or any successor provisions.

- 1. <u>Facility Design.</u> Prior to the acceptance of waste at the Facility, the County shall install a six-foot (6') high fence (i.e., chicken wire, chain-link, or similar material) around the Facility that is sufficient to minimize the escape of materials from the Facility and to restrict access to the Facility by pedestrians and vehicles.
- 2. <u>Operational Controls.</u> The County shall use portable fencing windscreens in the active area when Practicable. The County shall take all reasonable measures to prevent debris from the Facility from being deposited upon properties in the vicinity including the Course, including keeping its fences, windscreens, berms and landscaping free and clear of such debris.

The fencing shall be cleaned of litter and debris as deemed necessary by the County but not less frequent than once per week. Further, if any litter escapes the Facility, the County agrees to pick up the litter on neighboring properties and return it to the Facility for Disposal.

The County agrees to stop accepting waste any day when waste is leaving the site as determined by the Director or their designee. Wind speed shall be monitored on a continuous basis and any wind speeds above 30 miles per hour will be recorded with records being maintained and reviewed by the County daily as part of routine monitoring. Records may be reviewed by the municipalities upon request. The County shall inform all haulers of this rule and provisions shall be made so that the haulers can check with the County regarding Facility closure.

- 3. <u>Inspections.</u> The County shall exercise a reasonable daily regimen to inspect and control blowing debris leaving the Facility on all days when the Facility is in operation. The County shall inspect the adjacent property boundary with the Course, adjacent yards, and the right-of-way along USH 12/18, Millpond Road, CTH AB, and Siggelkow Road and future access road along the southern perimeter of the Facility for litter and remove litter as necessary. The County shall not be required to conduct litter inspections on any Sunday and legal holiday unless a nuisance condition has been reported and requires a response per Article V of this Agreement.
- 4. <u>Course Litter Collection.</u> When required to remove litter on the Course, the County will coordinate access to the Course with Course staff to minimize disruption of Course usage by customers. The County shall exercise due care when accessing the Course for this purpose, and may not use any equipment on the Course for litter collection purposes without the express permission of the City. County shall be responsible for any damage to the Course that occurs from litter collection.

I. VECTOR CONTROLS

The Local Committee considers vectors to include rodents, birds (especially gulls), insects (e.g., flies, mosquitoes, etc.), and other unwanted pests. All of these pests or vectors, except gulls, can be controlled through proper compacting and covering of the waste and grading of the site to eliminate ponded water, and by implementing a proper rodent baiting program at the Facility. The County agrees to exercise as part of its operational duties to perform pest control as needed and to contract if necessary with a professional pest control specialist.

The County shall have a Bird Control Plan for the Facility. Federal Aviation Administration (FAA) Advisory Circulars AC 15/5200-33b, Hazardous Wildlife Attractants On or Near Airports and AC 150/5200-34A, Construction or Establishment of Landfills Near Public Airports shall be referred to as guidance when creating the Bird Control Plan.

J. SCREENING/ LANDSCAPING PLAN

The County shall provide partial screening as Practicable to the Facility. The perimeter berms and plantings shall be concentrated along the periphery of the Facility, adjacent to the right-of-ways, and along the western border with the Course. This screening shall also comply with approved Conditional Use Permit Zoning. These specifications and plans may be amended pursuant to revisions required for City zoning approvals and WDNR requirements.

The County shall work with the City's Parks Division to establish reasonably sufficient screening between the Facility and the Course, which shall include, in addition to fencing and windscreens, the construction of berms along the final property line, and the planting of trees and the installation of other landscaping features along the Facility's boundary with the Course. With the agreement of the City, the screening may be placed on the City's property. The purpose of this screening is to create a buffer between users of the Course and Facility operations and to minimize impacts of the Facility on Course operations. All such screening shall be the County's cost, unless otherwise agreed to by the City and the County.

K. <u>DRAINAGE</u>, <u>STORMWATER UTILITY CHARGES</u>, <u>LEACHATE</u>, <u>AND EROSION</u> CONTROL

- 1. <u>Facility Design</u>. During the planning phase of the Facility, the County shall obtain a stormwater permit from the County Land and Water Resources Department. This shall be done after such plans are prepared as part of the Plan of Operation for the Facility. Plans shall be implemented in accordance with Ch. 14, DCO, and WDNR recommendations and approvals. The County shall not be required to obtain any approvals or permits from the City for erosion control or stormwater at the Facility. Concepts which shall be included in these plans can be described as follows:
- a. Surface water drainage to perimeter ditches and discharge into sedimentation basins is the primary erosion control measure that shall be applied in the borrow excavation areas during Facility development. This may include temporary ditches, diversion berms, and/or sedimentation basins. Any runoff from this area which is not routed to a perimeter drainage ditch for eventual discharge through a sedimentation basin shall be routed through erosion bales, silt fences, or temporary sedimentation basins within the borrow excavation areas.
- b. Surface water which ponds on the liner prior to the placement of Solid Waste shall be tested for conductivity. If conductivity values are indicative of surface water, this water shall be pumped to the perimeter drainage ditch. If values are indicative of leachate, the water shall be handled as leachate. All water which comes in contact with Solid Waste shall be treated as leachate.

- c. Semi-annual maintenance shall include inspection and repair of all drainage ditches and sedimentation basins. Disturbed areas shall be regraded, seeded and fertilized as necessary to maintain efficient flow and operation of all drainage features.
 - 2. <u>Operational Phase.</u> During the operational phase of the Facility, the County shall:
- a. Submit annual site plans to the City Engineer showing the areas of the Facility that are draining to the leachate collection system and those areas draining to the surface stormwater system (which shall be charged as part of the City's Stormwater Utility fees).
- b. Submit pumping volumes and analytical testing results to the City Engineer to allow for determination of sanitary demand charges for transmission and treatment of leachate. Pumping volumes shall be submitted to the City Engineer a minimum of monthly. Analytical testing results prescribed by Sec. 35.02(1)(d), MGO, shall be submitted to the City Engineer a minimum of annually. Copies of analytical testing results prescribed by sewer use ordinance or permits issued by MMSD shall be sent to the City Engineer concurrent with submittal to MMSD.
 - 3. Closure. Upon and as part of Facility Closure, the County shall:
- a. Submit final drainage and erosion control plans to the County Land and Water Resources Department and obtain approval therefrom. This shall be done after such plans are prepared as part of the Plan of Operation for the Facility. Plans shall be implemented in accordance with Ch. 14, DCO, and WDNR recommendations and approvals.
- b. Submit final site plans to the City Engineer documenting the final approved contours and drainage patterns. The stormwater utility fees shall be determined based upon these final site plans.
- c. In perpetuity or until leachate is no longer generated at the Facility, the County shall submit monthly pumping volumes to the City Engineer to allow determination of sanitary demand charges for treatment of leachate. Testing results prescribed by Sec. 35.02(1)(d), MGO, shall be submitted a minimum of annually.
- 4. Off-Site Drainage. County will design and operate the Facility so as not to unreasonably discharge stormwater onto adjoining property including the Course. The County acknowledges that the Course is not intended to be used for stormwater discharge and retention. The County shall perform all work and maintenance necessary to keep the stormwater facilities in good working order and in compliance with its stormwater control permit. The parties acknowledge that despite reasonable efforts by the County some stormwater discharge from the Facility could occur as a result of Force Majeure.

L. LANDFILL OPERATOR TRAINING

The County shall have the WDNR required number of certified "Facility Managers" and "Site Operators" on-site or available pursuant to Ch. NR 524, Wis. Admin. Code. The County

shall act pursuant to Ch. NR 524, Wis. Admin. Code, relating to certification requirements, continuing education and training requirements, and certificate holder responsibilities.

M. FACILITY REVENUE LIMITATIONS

Revenue generated by Facility operations through tipping fees shall be used to support operations of the Dane County Department of Waste & Renewables only. If the County generates revenue from gas generated at the Facility, such revenue shall be used to support Department operations, unless otherwise agreed to in writing by the City.

ARTICLE IV

ENVIRONMENTAL COMPLIANCE AND REPORTING

A. REGULATORY REQUIREMENTS

The County shall comply, at all times, with this Agreement and shall also operate the Facility, at all times, in complete compliance with all applicable federal and state statutes, administrative codes, regulations, rules and requirements. The County shall undertake at its expense environmental controls and monitoring addressing nuisance concerns including "other monitoring" as specified in Sec. NR 507.12, Wis. Admin. Code, pursuant to WDNR requirements for this Facility.

B. REPORTS TO MUNICIPALITIES

During the Active Site Life and extending after Final Closure, during which time the County is required to satisfy any net worth or other financial responsibility standard under any state or federal law, the County shall ensure that electronic copies of the annual Plan of Operation compliance report, WDNR operating permit and major modification to operating permit, and any notice of non-compliance or notice of violations that are provided to or are received from the WDNR or any other state agency or any federal agency associated with the Facility, are available on a webpage or other form of electronic clearinghouse that notifies City, Town, and Village designees when items are uploaded, except when said documents are confidential and not subject to public disclosure under the Wisconsin Public Records law.

C. PRIVATE DRINKING WATER WELL MONITORING

Private drinking water wells designated by WDNR in the Plan of Operation shall be monitored per WDNR monitoring requirements by the County for purposes of determining the quality of the water in such wells. Any property owner listed in Attachment C is eligible to participate in the elective private well monitoring program established in Attachment C at the cost of the County. The costs of sampling and testing as required by this Section shall be borne by the County.

All reports and test results relating to private well monitoring and sampling will be made available to the well owners. The reports shall include a list of current Preventive Action Limits, as defined under Ch. NR 140, Wis. Admin. Code for each parameter that is sampled for and notification of any exceedances. The well monitoring program, required by WDNR in the Plan of Operation Approval Letter, shall be perpetual consistent with the current WDNR standards. The results will be made available to the City Engineer, the City Water Utility General Manager, the Town, and the Village designee upon request. Results will also be made available to individual residents in Compensation groups upon request.

D. ENVIRONMENTAL CONTAMINATION

Although groundwater contamination is highly unlikely, the County shall pay for the reasonable replacement costs of wells serving existing principal use structures upon the properties listed on Attachment C where sampling results exceed safe drinking water standards and the

Facility is found to be cause of the exceedance, as required under Sec. NR 508.04, Wis. Admin. Code. The County shall be responsible to provide emergency potable water for human consumption and provide to users of such contaminated wells an alternative adequate water supply upon twenty-four (24) hour notification to the County of such contamination, until the source of the contamination can be confirmed or the well is replaced. The County agrees to provide water fit for human consumption and bathing and water for use by livestock.

E. ENVIRONMENTAL MONITORING

The County shall comply with all monitoring requirements imposed by WDNR, or required by the Wisconsin Administrative Code and any applicable successor regulations (including specifically monitoring provisions as stated in Ch. NR 507, Wis. Admin. Code), as well as any other state or federal rule or requirement applicable to monitoring for the Facility. The County shall also comply with all environmental monitoring conditions imposed by WDNR's Feasibility Determination, the approved Plan of Operation and specifically the Facility monitoring plan for the site as described in Sec. NR 514.05(8), Wis. Admin. Code, the monitoring report prepared in compliance with Sec. NR 514.06(7), Wis. Admin Code, and any additional closure or Long Term Care plans required by WDNR for the Facility.

ARTICLE V

OPERATIONAL CONCERNS AND ENFORCEMENT SYSTEMS

A. COUNTY CONTACT INFORMATION AND REPORTING SYSTEM

The County shall maintain a website that includes the phone numbers and email addresses for the Department personnel listed in this Agreement. The County shall also maintain and publicize a single e-mail address for receiving complaints and feedback.

The County will establish, maintain, and publicize an online reporting system that the public may use to submit feedback and complaints. The reporting system shall be monitored by appropriate County personnel.

B. CONDITIONS THAT REQUIRE IMMEDIATE ONSITE ATTENTION

For concerns requiring immediate attention of onsite personnel, the City, Town, or Village should contact the Landfill Operations Manager. If unable to reach the Landfill Operations Manager, the Director should be contacted.

C. COUNTY RESPONSE TO CONCERNS AND COMPLAINTS

The County shall be responsible for all management, control, and resolution of operational concerns relating to the Facility. All parties to this Agreement recognize that the Facility has the potential to create conditions that require correction, but that these conditions can largely be corrected or mitigated with appropriate response. As such, adjacent municipalities agree to submit a complaint on the online reporting system and/or notify appropriate personnel, of the need for response to such a condition and allow the County reasonable time to address the issue.

D. PROGRESSIVE COMPLAINT SYSTEM

For conditions that are not promptly addressed by the County under Section C or are severe in nature, there shall be a multi-level system to handle complaints involving operational concerns. This system shall be used to cooperatively resolve complaints without the necessity of recourse to the court system.

The Streets Superintendent shall monitor the County's compliance where operational concerns relating to odor, litter or mud tracking exist, shall document recurring nuisance problems, and shall seek cooperative solutions to problems involving complaints. When the Streets Superintendent communicates in writing with either the County or the complainant, the other party shall receive a copy of this written communication.

1. <u>Individual Complaints (Level I)</u>. A Level I Complaint is defined as a single or isolated condition that is not an environmental or health risk and is able to be corrected with appropriate response from the County, generally within business hours. A list of conditions and required response actions and timelines from the County for Level I Complaints is included in Attachment B. Level I Complaints have the potential to and may be submitted by a single person, a municipality, or another entity by means of the online reporting system, phone, e-mail or

verbally. If a Level I Complaint is submitted to the City, Town, or Village, the complaint shall be submitted to the Department as soon as reasonably practical. Level I Complaints will be tracked and documented by the County.

- 2. <u>Multiple Party, Ongoing or Priority Complaints (Level II)</u>. A Level II Complaint is defined as a condition that impacts an increased number of residents or is an ongoing or unresolved issue which requires prompt attention or additional resources from the County. A complaint will be elevated to Level II under the following circumstances:
- a. Five or more members of the public submit complaints to the County, City, Town or Village of a single operational concern within a 48 hour period.
- b. The Streets Superintendent substantiates the complaint and elevates the issue to a Level II Complaint.
- c. A Level I Complaint has not been resolved within the required timeframe included in Attachment B.

A list of conditions, required response actions, and timelines from the County for Level I and Level II Complaints is included in Attachment B.

For complaints that are submitted directly to the County and rise to Level II Complaints the Director, or their designee, shall also notify the Streets Superintendent of the complaint and the steps the County will take to resolve the complaint and also when the complaint has been resolved. The Village and Town designees shall also be notified if the Level II Complaint involves a resident of their respective communities.

If a complaint is being designated as Level II by the Streets Superintendent, the complaint must submitted in writing to the Department.

Any action taken or written response to a Level II Complaint shall be maintained as a Departmental record and a copy shall be provided to the Street Superintendent. The Streets Superintendent shall retain copies of all documents related to Level II Complaints.

3. <u>Unresolved Complaints (Level III)</u>. A Level III Complaint is defined as a Level II Complaint that is deemed by the Streets Superintendent as unresolved within the required timeframes included in Attachment B. Upon the designation of a Level III Complaint, the Street Superintendent shall make a pre-sanction determination in writing, which shall be issued to the complainant(s), the Director, and to the Town and Village. This pre-sanction notice shall provide a reasonable period of time, appropriate to the circumstances, to correct the problem. This presanction notice shall be based upon reasonable investigation and substantial findings of fact. All relevant evidence assembled by the Street Superintendent shall be made available to the Department. Mandatory corrective action shall be limited to those actions found to be reasonable and Practicable under the circumstances presented in the complaint.

If the Streets Superintendent and Director are unable to develop or agree upon the appropriate corrective action, a third-party consultant shall be selected by the County and Streets Superintendent and hired by the County at County's expense. If the parties cannot agree

on a third party consultant, the complaint may be escalated to Level IV. The third-party consultant shall decide the most Practicable and cost effective action of the presently available alternatives to effectively remedy the concern and determine the timeline for implementation. This remedial action shall be performed at the County's expense. If the County cannot provide necessary remedial action in a timely fashion, then a private contractor shall be retained to perform the work at the County's expense.

4. <u>Violation Notice (Level IV)</u>. If the County fails to take corrective action within a reasonable time reasonable time after receiving a pre-sanction notice, the Street Superintendent may issue a violation notice to the County. If the alleged violation involves property located within the Town or Village, notice shall be given to the appropriate entity.

When the Director disputes a violation, the Street Superintendent shall provide the County with an opportunity to meet and confer regarding the alleged violation. If the alleged violation involves property under Town or Village jurisdiction, a representative of the Town or Village shall have the opportunity to participate. If the parties do not agree as to the existence of a violation, or if a violation is not remedied as required by this Agreement, any municipality may seek any remedy available to it at law or in equity.

E. UNSUBSTANTIATED COMPLAINTS

At any level of the complaint system, there is the potential for a complaint to be determined to be unsubstantiated or caused by non-landfill activities. If at any point the County determines a complaint is unsubstantiated or not a result of landfill activities, the County will document the complaint as unsubstantiated.

For Level II Complaints that the County determines as unsubstantiated, the County will notify the Streets Superintendent and schedule a meeting at the location of the issue to verify or provide documentation of the determination. If the two parties agree that the complaint is unsubstantiated the County will document the complaint as unsubstantiated. If the Streets Superintendent elects not to meet, the complaint will be considered unsubstantiated. If the Streets Superintendent finds the complaint substantiated, the City and County will follow the progressive complaint system pursuant to this Agreement. If the complaint involves a resident from the Village or Town, their respective designees will be notified of the complaint and meeting times and be allowed to participate in the meetings.

F. REIMBURSEMENT FOR CORRECTIVE ACTIONS

If the County has been given adequate time as allowed in Attachment B to resolve a substantiated complaint such as litter or mud and any of the adjacent municipalities finds it necessary to collect litter or clean mud from roadways attributable to the Facility, the County agrees to pay that municipality a fee equal to twice the amount of the cost to correct the issue.

ARTICLE VI

LIMITATIONS AND FINAL USE

A. SITING FUTURE LANDFILLS

Any future expansion of the Limits of Waste beyond that authorized by this Agreement and related WDNR permit or the siting of an additional landfill by the County shall require renegotiation pursuant to Sec. 289.33, Wis. Stats.

B. SITING CLAY BORROW AREAS NEARBY

The County will not utilize any property within Dane County for clay borrow sites without full compliance with the County's Nonmetallic Mining Reclamation Ordinance Ch. 74, DCO, including an approved reclamation plan and permit.

C. <u>HEIGHT LIMITATIONS</u>

The maximum final design elevation of the Facility shall be 1,136 feet above mean sea level (MSL), or as approved by WDNR during the landfill permitting process if WDNR approved elevation is lower. There shall be no vertical expansion above the maximum design elevation without renegotiation of this Agreement. This shall not prevent Dane County from obtaining approval from WDNR for intermediate waste grades that are higher than final waste grades.

D. FINAL USE

The final use of the Limits of Waste and other related areas as described below upon Final Closure shall be as a nature conservancy or recreation area subject to the following terms and conditions:

- 1. The area shall include the Limits of Waste areas, on-site storm water detention facilities and areas required for monitoring, leachate storage and removal, or maintenance of the closed landfill.
- 2. No uses inconsistent with conservancy or recreation zoning shall be allowed on the area, except those required for monitoring, leachate storage and removal, landfill gas extraction and processing, biogas pipeline injection, and screening and maintenance of the closed landfill.
- 3. The County shall, after Final Closure, develop and maintain the Limits of Waste areas to establish the areas as nature conservancy or recreation area, subject to reasonable safety and security precautions. The County shall also maintain, repair and provide Long Term Care of the Limits of Waste at the Facility to preserve the Limits of Waste, insofar as possible, consistent with the requirements of the WDNR, in a natural state as a nature conservancy area, with the maintenance and preservation and replacement of existing vegetation whenever possible. The County reserves the right to limit public access wherever and whenever appropriate.

After Final Closure, areas of the conservancy may be used for permitted uses as approved by applicable zoning and WDNR regulations in compliance with state statutes. No waste Disposal shall be allowed in the conservancy area after closure of the landfill. The areas of the Facility not included within the nature conservancy area, including all existing buildings and structures may be used consistent with City zoning.

ARTICLE VII

COMPENSATION

A. <u>RESIDENTIAL PROPERTY COMPENSATION</u>

- 1. <u>Payment Amounts</u>. For the first year of Compensation, the County shall make an annual payment to eligible residential property owners of the Eligible Properties listed in Attachment D as follows:
 - a. Group A: Annual Payment of \$8,800
 - b. Group B: Annual Payment of \$3,500
 - c. Group C: Annual Payment of \$1,800

In subsequent years, the annual payment to all groups shall be adjusted by the All Urban Consumer- Minneapolis-St. Paul, Minnesota-Wisconsin CPI, or equivalent if unavailable.

- 2. <u>Term.</u> The first prorated payment shall be made based on when Solid Waste acceptance begins at the Facility. If the first year of compensation occurs after 2028, the values shall be adjusted by CPI. Annual payments shall be made thereafter based upon the calendar year until Solid Waste is no longer received at the Facility. In the year of Final Closure the County shall make the full annual payment.
 - 3. General Eligibility. These payments are subject to the following conditions:
- a. These payments only apply to Eligible Properties listed in Attachment D. Commercial property is excluded.
- b. Payments shall be made to the owners of non-owner occupied single-family properties, not the occupants.
- c. Payments apply only to the title owners of property in fee simple or vendees of record of land contracts for sale in fee simple who owned the property prior to the date of Plan of Operation approval.
- d. Compensation payments are nontransferable. Transfer of ownership interests of any of the Eligible Properties shall result in termination of payments for that property.
- e. For residential dwellings built after the date of this Agreement but before the date of approval of the Plan of Operation, the County will score the property based on the potential for impacts from the landfill and make a determination on the eligibility of the property and Compensation levels. The eligibility will be presented to the affected municipalities for approval via majority vote.
 - f. Duplex units are included, but only the owner of the property is compensated.

- g. If an Eligible Property has joint ownership, then a single Compensation payment shall be made to the joint owners.
- h. No property listed in Attachment D shall be eligible for an annual Compensation payment under this Agreement and the Rodefeld Landfill Negotiated Agreement in the same year. If a property qualifies for annual Compensation under both Agreements only the higher amount shall be paid to the property owner.
 - i. Multifamily dwellings (three or more units) are not eligible for Compensation.
- j. Acceptance of Compensation by property owners grants the County rights as well as reasonable access for environmental monitoring and investigations related to this Agreement including debris pickup on owners' properties. Acceptance of Compensation will include signing a form acknowledging these rights and conditions before receipt of first payment (see Attachment E for sample agreement).
- k. The County shall make all payments directly to the eligible property owners. Property owners shall be responsible for providing the County with notice as to any change in address for transmittal of payments.

B. RESIDENTIAL PROPERTY VALUE GUARANTEES

In consideration of the potential adverse impact that the Facility may have on neighboring properties, the County shall protect, in the manner provided for in this Section, the Fair Market Value of Eligible Properties listed in Attachment D against property value loss in the event of sale during the Active Site Life of the Facility and extended as provided for below. Such program shall provide protection and be administered as follows:

1. General Eligibility.

- a. Only owners of record of an Eligible Property, as shown in Attachment D, as of the date of approval of the Plan of Operation or any party obtaining the entire interest in an Eligible Property by reason of the death of a spousal joint tenant or in a divorce action by court decree, shall be eligible for property value guarantees under this Agreement.
- b. Eligibility for property value guarantee under this Section is not transferable, and will only be paid to the title owners of property in fee simple or vendees of record of land contracts for sale in fee simple who owned the property as of the date of Plan of Operation approval or have obtained the entire interest in an Eligible Property per Para. 1 (a).
- c. To be eligible for property value guarantee under this Section, the owner must place the Eligible Property on the market for sale with a Wisconsin licensed real estate broker, except as otherwise provided herein. To initiate this process, the owner shall provide the County with the name of the broker with which such property is listed, the proposed terms of sale, and a request for an appraisal. The sale must be an arm's length transaction, except as provided herein.

- d. The property value guarantee extends to vacant buildable lots within the compensation zones, but the protection only guarantees the value of the land without improvements and not the value of a future home.
- e. The property value guarantee shall not extend to commercial property that may also include a residence.
- f. Maintenance of this obligation shall not terminate for eligible property owners in Compensation Groups A and B.
- g. Maintenance of this obligation shall terminate one (1) year beyond the final date of acceptance of Solid Waste for eligible property owners in Group C. To qualify for this obligation, eligible property owners in Group C must notify the County per Para. 1 (b) above within one (1) year after closure of the Facility. Failure to notify the County of an eligible property owner's intent to sell within the above-stated time periods shall terminate the coverage provided in this Section for that property. If the owner has notified the County in a timely manner, eligible residential property owners shall have a time period based on the Madison Realtors average time to sell residential properties plus an additional thirty (30) days to obtain a written, accepted offer to purchase. If no written, accepted offer to purchase is received within this time period, the owner may elect in writing to require that the County purchase the property. The time period specified in the preceding sentence shall also be applicable to non-arm's length transaction.

2. Determination of Fair Market Value.

- a. Within sixty (60) days of receipt of notice from an eligible property owner as provided in Para. 1(c) above, the County shall, cause the property to be appraised at its Fair Market Value.
- b. The County shall provide a copy of such appraisal to the eligible property owner within ten (10) days of the completion of said appraisal.
- c. The eligible property owner may request a second appraisal to be paid for by the property owner if there is disagreement as to the values stated in the first appraisal.
- d. If the appraisers retained under Para. 2(a) and (c) above do not agree on the Fair Market Value, but the difference in the appraised Fair Market Value is less than ten (10) percent, the Fair Market Value shall be the average of the two appraisals.
- e. If the appraisals retained under Para. 2(a) and (c) above do not agree on the Fair Market Value and the difference between the two appraisals is more than ten (10) percent, a third appraiser shall be selected by the County and the Owner. The third appraiser shall review the existing appraisal reports and provide an appraisal of the Fair Market Value. The two appraisals which are closest to each other in determining Fair Market Value shall be selected and averaged to derive the Fair Market Value that will be binding for these purposes upon the County and owner. The cost for the first and third appraisal (if necessary) shall be paid by the County.
- 3. <u>Obligation to Pay Compensation</u>. An Eligible Property shall continue to be listed for sale until:

- a. The owner sells the property for a cash price equal to or greater than the Fair Market Value, in which case no payment shall be made by the County.
- b. The property is continuously listed for sale with a broker for a time period based on the Madison Realtors average time to sell residential properties, and the highest cash price of any offer to purchase received by the owner is less than the Fair Market Value. The owner shall then notify the County of the offer and the County shall have ten (10) days to elect in writing to purchase the property for a cash price equal to the Fair Market Value. If the County fails to exercise such election, the owner may sell the property to the purchaser identified in the offer to purchase upon the terms provided, and the County shall pay the owner within thirty (30) days of receipt of notice of the closing an amount equal to the difference between the price set forth in the offer to purchase and the Fair Market Value.
- c. If the property is listed for sale with a broker for a time period based on the Madison Realtors average time to sell residential properties plus an additional thirty (30) days, and no written accepted offer to purchase is received, the owner may elect in writing to require that the County purchase the property. Within thirty (30) days of receipt of the owner's election, the County shall purchase the property for a cash amount equal to the Fair Market Value.
- d. If an owner chooses to sell the property to someone other than an arm's length purchaser (e.g., an intra-family sale), the owner shall be entitled to Compensation which is the lesser of either the Fair Market Value less the Current Value or the Fair Market Value less the final sales price at closing.

Where an owner plans to execute a non-arm's length transaction, owner shall notify the County at least thirty (30) days prior to actual conveyance. When an owner elects to sell an Eligible Property without listing the property with a licensed real estate broker, this conveyance shall be presumed not to be an arm's length transaction.

If the owner notifies the County of the intent to execute a non-arm's length transaction, the valuation procedures under Para. 2 above shall apply to determine the property's present marketable value as of the date of said notification ("Current Value").

- e. In the event any offer to purchase a property provides for seller financing, appropriate adjustments shall be made to determine the equivalent present day cash value.
- 4. <u>Alternative Compensation</u>. In lieu of the property value compensation provided in Para. 2 above, an owner may elect to receive, a lump sum payment of \$1,000 from the County, without determining the Fair Market Value or the Current Value of the property. If the owner elects to receive the lump sum payment, the owner shall so notify the County in writing within thirty (30) days of offering the property for sale (either with or without listing the property with a real estate broker). The alternative compensation payment shall be made within twenty (20) days of the owner providing proof of an actual sale to the County. These payments shall only be made upon proof of an actual sale. The County's obligation to protect the Fair Market Value of an Eligible Property ceases upon making the one-time payment of \$1,000 with respect to that property. Furthermore, all compensation payment obligations to this property under Section VII.A. cease upon such payment.

5. <u>Miscellaneous Provisions</u>. Payment of property value guarantees shall be made only once for any individual tax parcel included in Attachment D. Compensation under this Section is only available for the tax parcel upon which the residential property is located if tax parcels are to be sold separately. In the event that a portion of a parcel upon which the residence is located is offered by the owner for sale, the County, at its discretion and to avoid subsequent appraisal costs, may decide to have appraisals made for that portion and simultaneously for the rest of the parcel. If the County chooses to proceed in this manner, it shall make any payment as if both the portion severed and the rest of the parcel had been conveyed at that time.

The Compensation payable under this Section shall apply only to conveyances made by deed or land contract and shall not apply to conveyances of leasehold interests, easements or other conveyances of partial interests by protected parties.

C. COMPENSATION TO COURSE

The County agrees that it will reimburse the City's Golf enterprise for any refunded greens fees that are given due to odor complaints from golfers using the Course, consistent with rain check policy. Also, if, in the Parks Superintendent's determination and after consultation with the County Director, the City has to temporarily close the Course due to unreasonable odor or other hazards directly attributable to Facility operations, the County will reimburse the City's Golf enterprise for lost revenue during such a closure. Lost revenue, for this purpose, shall be determined by the product of the length of the closure and the average daily Course revenue. The daily Course revenue shall be established on a peak-season (from May 15-Sept. 30) and off-peak season (all other days when the Course is open) basis. The average daily Course revenue shall be determined by calculating daily Course revenue over the prior three calendar years (prorated to 18 holes if necessary), specifically including greens fees, concessions, cart rental and driving range revenue, and excluding pro shop and retail sales revenue. Reimbursements under this provision shall not exceed \$50,000 in any calendar year and shall be paid annually to the City's Golf enterprise. If closures under this provision continue after this limit is exceeded, the City may pursue other remedies against the County.

D. MUNICIPAL COMPENSATION

- 1. <u>Payment Amounts.</u> The County agrees to pay annual landfill siting fees of \$20,000 to the Village and \$50,000 to the Town, and an annual landfill siting fee and compensation payment to the City in the amount of \$125,000, which Payments shall commence based on when Solid Waste acceptance begins at the Facility.
- 2. <u>Term.</u> The first payments shall be due to the municipalities within sixty (60) days of the beginning of Solid Waste acceptance at the Facility, and shall be prorated in the first year. Thereafter, payments shall be due no later than June 1 each calendar year, and shall be increased annually by 2% following the first payment. County's obligation to make municipal payments pursuant to this Section shall terminate upon Final Closure.
- 3. <u>General Eligibility and Limitations.</u> No municipality shall be eligible for an annual compensation payment under this Agreement and the Rodefeld Landfill Negotiated Agreement in

the same year. If a municipality qualifies for annual compensation under both Agreements only the higher amount shall be paid to the municipality.

Municipal compensation payments may be funded from landfill revenues, and the County shall not make other payments to the City, Town or the Village relating to landfill impacts, other than payments relating to the Course under Section C or payments relating to specific services provided to the Facility, unless specifically agreed to by a separate agreement between the County, the City, the Town and the Village.

E. CONTINGENCY FUND

The County shall make payments to a self-administered contingency action account as provided in this Section. This fund shall be known as the Contingency Fund. The purpose of the fund is to insure that monies will be immediately available to the County at any time to pay any costs incurred by the County or a local government by reason of maintenance, repair, remediation and reconstruction of the Facility or for any other damages caused or costs incurred by reason of the presence of the Facility and when such costs for any reason are not readily available from any other public fund. Where funds are shown to be available from another public fund specifically for this purpose, those funds shall be used prior to this fund.

The County shall create a perpetual individual account administered by the County to be used exclusively for environmental response and remedial action associated with the Facility. Upon closure of the Facility, the County shall deposit into the account an amount, if any, necessary to bring it to minimum balance at closure of \$100,000. Interest on the account may be used by the County after the fund reaches the amount of \$1,000,000.00. The County shall make annual payments to this account at the rate of \$0.10 per ton of Solid Waste landfilled at the Facility each year.

- 1. <u>Administration of Fund</u>. The Contingency Fund shall be maintained in one or more investment accounts with the County holding the responsibility to achieve reasonable return on these funds and administering the accounts accordingly. Funds shall be released and used solely for the purpose of implementing any actions described in this Section which are determined to be necessary or appropriate by the City and the County, under the recommendations of the City's Streets Division and County Department of Waste & Renewables or similar departments.
- 2. <u>Limits on Use of Fund</u>. The Contingency Funds shall not be used to relieve the County of any of its state-mandated obligations relating to or arising from construction, operation, Final Closure or Long Term Care requirements at the Facility. Further, the fund is intended to provide limited remedial response in order to protect the safety and well-being of local residents. Lack of funds in the contingency fund shall not limit the County's liability for landfill related contamination on or off the site.
- 3. <u>Duration of Fund</u>. The fund shall be perpetual; provided, however, that in the event of the complete removal of the Solid Waste Disposed of at the Facility and decontamination of the site, any remaining money in the fund may be applied to help pay the cost of such removal and decontamination. Monies left over after these payments shall be deposited in the County's Landfill Reserve Account if it still exists or, if it doesn't, any account selected by the County Board.

ARTICLE VIII

CONTRACT PROVISIONS

A. NOTICE TO PARTIES

Any notices required by the terms and conditions of this Agreement not otherwise addressed herein are, at minimum, to contain the address and names of the parties as noted below, are to be sent by first class mail to these parties and are to be considered by each party as written notice when received. It is further understood that the City, the Town, the Village, and the County each shall be responsible to provide to the other parties any appropriate change of address or any appropriate change of name by providing the other parties with written notice of "address change" or "name change". The notices under this Section shall be sent by certified mail to the following:

County of Dane c/o the County Clerk of Dane County 210 Martin Luther King, Jr. Blvd., Room 112 Madison, Wisconsin 53703

City of Madison c/o the City Clerk of the City of Madison 210 Martin Luther Kind, Jr. Blvd., Room 103 Madison, Wisconsin 53703

Town of Cottage Grove c/o the Town Clerk of the Town of Cottage Grove 4058 CTR N Cottage Grove, WI 53527

Village of McFarland c/o the Village Clerk 5915 Milwaukee Street PO Box 110 McFarland, WI 53558

B. TITLES

The titles to articles, sections, subsections and paragraphs used in this Agreement are for informational purposes only, except where it may be necessary to an understanding of the content of the Agreement.

C. GOVERNING LAW

This Agreement and the provisions contained herein shall be construed, enforced and governed, in all respects, in accordance with the laws and the statutes of the State of Wisconsin.

D. SCOPE OF AGREEMENT

The County and the City have entered into several agreements relating to the Facility. This Agreement was formulated pursuant to Secs. 289.33, and 66.0301, Wis. Stats., to address local concerns. No ambiguity as to related agreements is intended by the parties to this Agreement. To the extent this Agreement conflicts with any other agreement to which any of the parties hereto are also the parties, the more restrictive agreement shall govern the Facility until a clarifying amendment hereto is agreed to by the parties.

E. AMENDMENT

This Agreement may be amended only by a mutually stipulated, written agreement between the parties who have executed this Agreement.

F. BINDING EFFECT

This Agreement shall bind the County, the City, the Town, the Village, the Local Landfill Negotiating Committee, their respective legal representatives, their respective legal successors and their respective legal assigns.

G. REASONABLENESS OF APPROVALS

Any approvals required under this Agreement shall not be unreasonably withheld.

H. CONSTRUCTION OF TERMS

No terms or conditions contained in this Agreement shall be construed to create a situation where performance becomes impossible, nor shall any provision herein be construed to create a conflict with any duty or obligation that the County may have under any existing or future statute, rule or regulation of the United States of America, the State of Wisconsin, or any federal or state agency having jurisdiction over the design and operation of the Facility.

I. BREACH

Any waiver by any party to a breach of any term or condition of this Agreement shall not be considered a waiver of any subsequent breach by the party of the same term or any other term or condition of this Agreement.

J. <u>SEVERABILITY</u>

If any provision of this Agreement contravenes or is invalid under any law of the State of Wisconsin or of the United States, the contravention or invalidity shall not invalidate the whole Agreement, but rather this Agreement shall be construed as if it did not contain that particular provision or provisions held to be invalid and the rights and duties of the parties shall be construed and enforced accordingly.

K. FORCE MAJEURE

No party to this Agreement shall be liable for failure to perform any duty or obligation that said party may have under this Agreement where such failure has been occasioned by any act of god, fire, strike, inevitable accident, war, court order or binding determination of a governmental agency.

L. **NONDISCRIMINATION**

In the performance of the services under this agreement, the Parties agree to abide by their own respective affirmative action plans and in doing so agree not to discriminate, in violation of any state or federal law, against any employee or applicant because of race, religion, marital status, age, color, sex, handicap, national origin or ancestry, income level or source of income, arrest record or conviction record, less than honorable discharge, physical appearance, sexual orientation, gender identity, political beliefs, or student status. The parties further agree not to discriminate, in violation of any state or federal law, against any subcontractor or person who offers to subcontract on this Agreement because of race, religion, color, age, disability, sex, sexual orientation, gender identity or national origin.

M. SUPPORT FOR EXPANSION

City, Town, Village, and their respective officers, agents, employees, or designees agree to support the Facility and continued operation of the Facility as currently proposed, and agree not to file a contested case or other litigation regarding such design proposal and agree not to assist any opponent of the proposed Facility.

N. COUNTERPARTS; ELECTRONIC DELIVERY

This Agreement and any document executed in connection herewith may be executed in counterparts, each of which shall be deemed an original, but all of which together shall constitute the same document. Signatures on this Agreement may be exchanged between the parties by facsimile, electronic scanned copy (.pdf) or similar technology and shall be as valid as original; and this Agreement may be converted into electronic format and signed or given effect with one or more electronic signature(s) if the electronic signature(s) meets all requirements of Ch. 137, Wis. Stats., or other applicable Wisconsin or Federal law. Executed copies or counterparts of this Agreement may be delivered by facsimile or email and upon receipt will be deemed original and binding upon the parties hereto, whether or not a hard copy is also delivered. Copies of this Agreement, fully executed, shall be as valid as an original.

O. <u>INDEMNIFICATION</u>

The County shall indemnify, hold harmless, and defend the City, Village, and the Town, their officers, officials, agents, employees, and any duly appointed committee, from and against any and all liability including claims, demands, losses, costs, damages, and expenses of every kind and description by reason of bodily injury sustained by any person or persons (including death at any time resulting therefrom), or damages to property, including loss of use thereof, resulting from acts or omissions of the County which are negligent, unlawful or in breach of this Agreement and

arise out of or in connection with or occur during operation or use of the Facility, whether direct or indirect, anticipated or unanticipated, including but not limited to the design, siting, construction, operation, maintenance, control, repair, remediation, administration, surveillance, monitoring, closure, and Long Term Care of the Facility, and the Disposal, treatment, Storage, processing and removal of Solid Waste at the Facility. This provision is not intended to limit or waive any defenses available to the aforementioned municipalities, including those established by Sec. 893.80, Wis. Stats., or any successor statutory provision. Nor does this provision limit these municipalities potential responsibility as generators of Solid Waste Disposed of at the Facility.

P. SURVIVABILITY

The following provisions shall survive as set forth in terms of the respective provision:

- 1. Article III, Section F regarding maintenance of an active gas control system.
- 2. Article III, Section K(3)(c) regarding monthly leachate volume and testing.
- 3. Article IV, Section B regarding maintenance of a webpage.
- 4. Article IV, Section D regarding residential well protections.
- 5. Article VII, Section B(1)(f) regarding residential property value guarantees.
- 6. Article VII, Section E(3) regarding the Facility's contingency fund.

for Dane County Landfill Site No. 3

IN WITNESS WHEREOF, the Parties hereto have caused this Agreement to be executed by their proper officers on the day and year last written below.

FOR THE LOCAL NEGOTIATING COMMITTEE FOR DANE COUNTY LANDFILL SITE NO. 3

David P. Schmiedicke, Chair
On behalf of the Local Negotiating Committee

FOR THE CITY OF MADISON

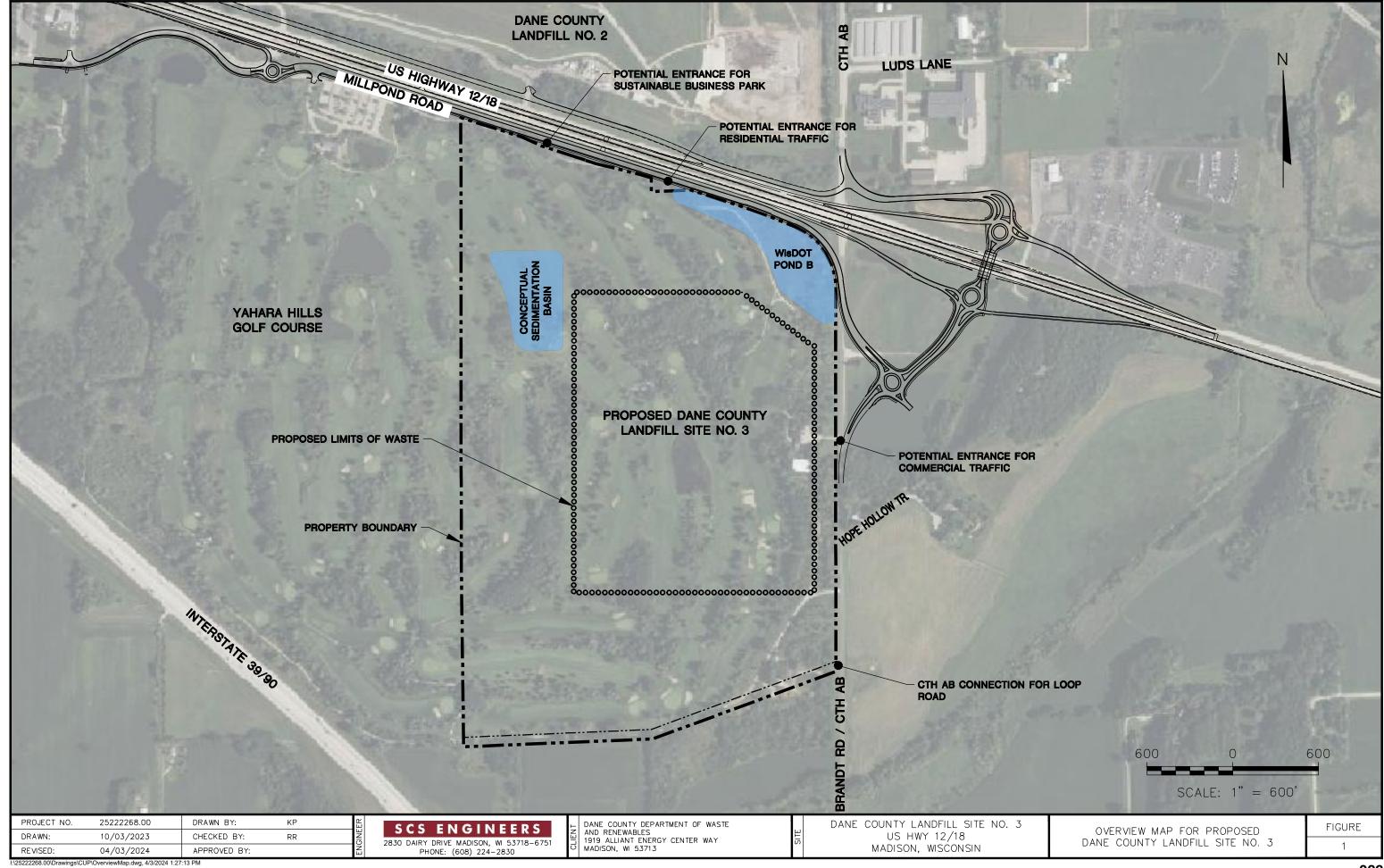
Satya Rhodes-Conway, Mayor	Date	
Maribeth Witzel-Behl, City Clerk Countersigned:	Date	
David P. Schmiedicke, Finance Director	Date	
Approved as to form:		
Michael Haas, City Attorney	Date	
Execution of this Agreement by the City is authorized, ID No, adopted by the, 20	horized by Resolution Enactment No. RES Common Council of the City of Madison	

FOR THE TOWN OF COTTAGE GROVE Kris Hampton, Town Chair Date Kim Banigan, Town Clerk Approved as to form: Christopher T. Nelson, Town Attorney Date Execution of this Agreement by the Town is authorized by _______, adopted by the Town Board of the Town of Cottage Grove on _______, 20___.

FOR THE VILLAGE OF MCFARLAND Carolyn A. Clow, Village President Date Matthew Schuenke, Village Administrator Date Approved as to form: Allen D. Reuter, Village Attorney Date Execution of this Agreement by the Village is authorized by ________, adopted by the Village Board of the Village of McFarland on _______, 20___.

FOR THE COUNTY OF DANE		
Jamie Kuhn, County Executive	Date	
Scott McDonell, County Clerk	Date	
Approved as to form:		
David R. Gault, Deputy Corporation Counsel	Date	
Execution of this Agreement by the County is author the Board of Supervisors of Dane County on	ized by, 20	, adopted by

$\label{eq:attachment} \mbox{ATTACHMENT A}$ $\mbox{MAP OF FACILITY AND LIMITS OF WASTE}$



ATTACHMENT B COMPLAINT RESPONSE

Complaint Response Protocols

CONDITION REPORTED	DANE COUNTY RESPONSE ACTION	LEVEL I RESPONSE TIMELINE	LEVEL II RESPONSE TIMELINE
	Substantiate the report by verifying weather patterns, site conditions and other outside factors.	Within 3 business hours.	Within 2 hours of becoming Level II Complaint.
	If odor likely or probably, Dane County will mobilize to the site of the odor complaint to determine if the odor is ongoing, the likely source of the odor (gas, garbage, other activity, etc.) and what Mitigation steps are necessary.	Within 3 business hours if landfill activities determined to be possible or likely.	Within 4 hours of becoming Level II Complaint.
Odor	If odor determined to be ongoing, Dane County will take action to mitigate the odor by taking one or more of the following steps:		
	a. Adjust landfill gas well collectors to apply more vacuum. Document the changes.	Within 5 business hours of substantiated odor.	Within 5 hours of becoming Level II Complaint.
	b. Increase odor neutralizing agents.	Within 5 business hours of substantiated odor.	If found to be necessary, within 3 business hours of becoming Level II Complaint.
	c. Apply additional soil cover.	Within 8 business hours of substantiated odor.	If found to be necessary, within 3 business hours of becoming Level II Complaint.
Excessive Litter on Roadways	Dane County to pick up litter.	Within 2 business days or 3 calendar days.	Within 1 business day.
Landfill Litter on Golf Course	Dane County to pick up litter.	Within 24 hours.	Immediately if reported during business hours or prior to next business day.
Mud on Roadway	Dane County to sweep up or clean mud from roadways.	Within 3 business hours.	Within 1 business hour.

ATTACHMENT C ELECTIVE PRIVATE WELL MONITORING PROGRAM

Elective Private Well Monitoring Program – Sampling Parameters

Sampling Parameters	Frequency Until Closure	Frequency After Closure
Alkalinity	Annually	Once Every 3 years
Chloride	·	
Field conductivity (at 25°C)		
Field pH		
Field temperature		
Hardness		
VOC scan		

Elective Private Well Monitoring Program – Eligible Properties

7610 FEMRITE DR	3288 SIG CT
7241 FEMRITE DR	3290 SIG CT
3108 HOPE HOLLOW TRL	3363 PIERCE RD
3098 HOPE HOLLOW TRL	3032 SIGGELKOW RD
3104 HOPE HOLLOW TRL	3310 COUNTY HIGHWAY AB
7302 SIGGELKOW RD	3115 SIGGELKOW RD
3646 COUNTY HIGHWAY AB	3125 SIGGELKOW RD
3620 COUNTY HIGHWAY AB	3131 SIGGELKOW RD
3624 COUNTY HIGHWAY AB	3274 COUNTY HIGHWAY AB
3358 SIGGELKOW RD	3111 SIGGELKOW RD
3352 SIGGELKOW RD	3124 LUDS LN
3316 SIGGELKOW RD	3369 PIERCE RD
3280 LEE SOUTH CT	3372 STORCK RD
3181 SIGGELKOW RD	3341 STORCK RD
3187 SIGGELKOW RD	3334 SIGGELKOW RD
3195 SIGGELKOW RD	3330 SIGGELKOW RD
3284 MANSION CIR	3295 LEE SOUTH CT
3276 MANSION CIR	7402 FEMRITE DR
3294 SIG CT	3702 COUNTY HIGHWAY AB
3285 KOWEL CT	3802 GALA WAY
3279 KOWEL CT	7604 FEMRITE DR
3164 LUDS LN	3275 MANSION CIR

ATTACHMENT D

PROPERTY VALUE GUARANTEE AND ANNUAL COMPENSATION ELIGIBLE PROPERTY LIST

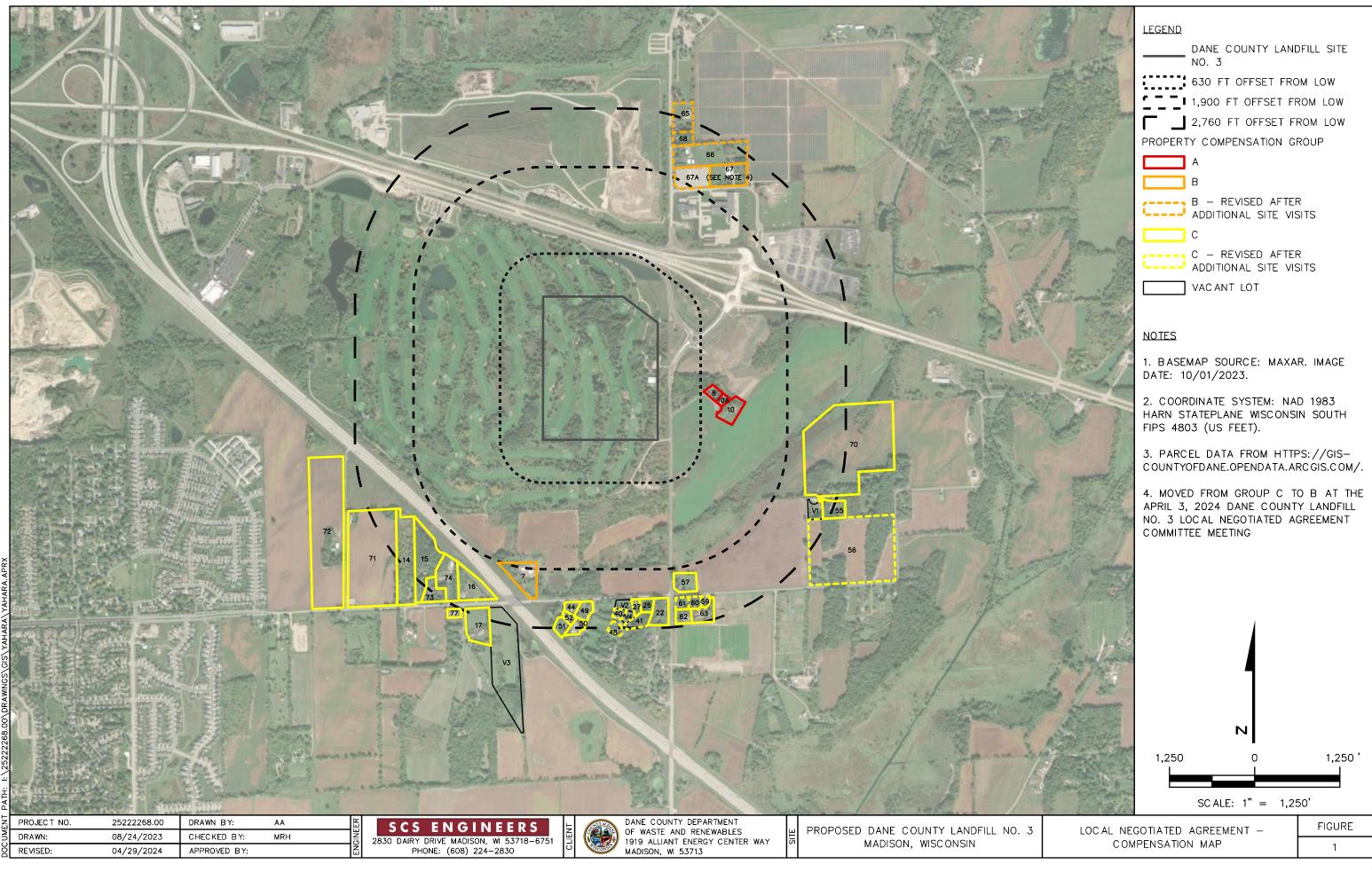
Property Value Guarantee And Annual Compensation - Eligible Property List

Ouarantee Ana Annual Compensation	- Bugiote I roperty List
PHYSICAL ADDRESS	GROUP
3108 HOPE HOLLOW TRL	A
3098 HOPE HOLLOW TRL	A
3104 HOPE HOLLOW TRL	A
7302 SIGGELKOW RD	В
3646 COUNTY HIGHWAY AB	В
3620 COUNTY HIGHWAY AB	В
3124 LUDS LN	В
3164 LUDS LN	В
3624 COUNTY HIGHWAY AB	В
3358 SIGGELKOW RD	С
3352 SIGGELKOW RD	С
3316 SIGGELKOW RD	С
3280 LEE SOUTH CT	С
3181 SIGGELKOW RD	С
3187 SIGGELKOW RD	С
3195 SIGGELKOW RD	С
3284 MANSION CIR	С
3276 MANSION CIR	С
3294 SIG CT	С
3275 MANSION CIR	С
3285 KOWEL CT	С
3279 KOWEL CT	С
3288 SIG CT	С
3290 SIG CT	С
3363 PIERCE RD	С
3032 SIGGELKOW RD	C
3310 COUNTY HIGHWAY AB	С
3115 SIGGELKOW RD	С
3125 SIGGELKOW RD	С
3131 SIGGELKOW RD	С
3274 COUNTY HIGHWAY AB	С
3111 SIGGELKOW RD	С
3369 PIERCE RD	С
3372 STORCK RD	С
3341 STORCK RD	С
3334 SIGGELKOW RD	С
3330 SIGGELKOW RD	С
3295 LEE SOUTH CT	С
	PHYSICAL ADDRESS 3108 HOPE HOLLOW TRL 3098 HOPE HOLLOW TRL 3104 HOPE HOLLOW TRL 7302 SIGGELKOW RD 3646 COUNTY HIGHWAY AB 3620 COUNTY HIGHWAY AB 3124 LUDS LN 3164 LUDS LN 3624 COUNTY HIGHWAY AB 3358 SIGGELKOW RD 3352 SIGGELKOW RD 3316 SIGGELKOW RD 3280 LEE SOUTH CT 3181 SIGGELKOW RD 3187 SIGGELKOW RD 3195 SIGGELKOW RD 3295 SIGGELKOW RD 3296 MANSION CIR 3276 MANSION CIR 3276 MANSION CIR 3294 SIG CT 3275 MANSION CIR 3285 KOWEL CT 3279 KOWEL CT 3288 SIG CT 3290 SIG CT 3290 SIG CT 3290 SIG CT 310 COUNTY HIGHWAY AB 3115 SIGGELKOW RD 3115 SIGGELKOW RD 3117 SIGGELKOW RD 3117 SIGGELKOW RD 3118 SIGGELKOW RD 3119 SIGGELKOW RD 3111 SIGGELKOW RD 3111 SIGGELKOW RD 3111 SIGGELKOW RD 3131 SIGGELKOW RD 3131 SIGGELKOW RD 3131 SIGGELKOW RD 3369 PIERCE RD 3372 STORCK RD 3371 STORCK RD 33341 STORCK RD 33341 STORCK RD

Vacant Buildable Lots Eligible for Property Value Guarantee

MAP ID	PARCEL ID
V1	018/0711-311-9130-2
V2	154/0710-364-0258-1
V3	154/0710-363-8466-1
V4	154/0710-364-0300-1

Note: Map ID Corresponds to Figure in Attachment D.



ATTACHMENT E

SAMPLE AGREEMENT FOR ELIGIBLE, RESIDENTIAL PROPERTY OWNERS

SAMPLE AGREEMENT FOR ELIGIBLE, RESIDENTIAL PROPERTY OWNERS

<u>DANE COUNTY LANDFILL NO. 3 RESIDENTIAL</u> <u>PROPERTY OWNER AGREEMENT</u>

Agreement made this	day of	, 20_	_, between Dane County,
by the Dane County Department of W	Vaste & Renewables, 191	9 Alliant	Energy Center Way,
Madison, Wisconsin, 53713, hereina	fter referred to as "Count	ty," and [C	Owner Name(s)]
hereinafter referred to as "Property O	wner(s)" or "Eligible Pro	perty Ow	ner(s)" own(s) a
residential dwelling on the property lo	ocated at [Property Addre	ess], [Parc	el #], hereinafter referred
to as the "Subject Property" or "Prope	erty" and wishing to be c	ontacted a	t:
	(Name)		
	(Mailing Add	dress)	
	(Mailing Add		ceive Compensation, if
	(Email Addr	ess(es))	
	(Phone Num	ber(s))	

This Agreement arises from negotiations between the County, City of Madison, Town of Cottage Grove, and Village of McFarland now formalized in "Negotiated Agreement Relating to the Dane County Landfill No. 3" hereinafter referred to as the "Negotiated Agreement." The terms and conditions of the Negotiated Agreement impacting Eligible Property Owners may be viewed on the [link to website]. In the event of any conflict between this Agreement and the Negotiated Agreement, the terms of the Negotiated Agreement shall control.

ELIGIBILITY

As established by the Negotiated Agreement, Property Owners are hereby eligible for the following programs or Compensation so long as Property Owner(s) abide(s) by the terms and conditions of this Agreement:

- 1. Property Owner(s) shall be eligible for the elective private well monitoring program pursuant to Article IV, Section C titled "Private Drinking Water Well Monitoring" and Attachment C of the Negotiated Agreement
- 2. Although unlikely, Property Owner(s) shall be eligible for replacement costs of wells due to impacts from the landfill and access to emergency potable water pursuant to Article IV, Section D titled "Environmental Contamination" of the Negotiated Agreement.
- 3. Property Owner(s) shall be eligible for annual compensation payments pursuant to Article VII, Section A, titled "Residential Property Compensation" of the Negotiated Agreement.

4. Property Owner(s) shall be eligible for property value guarantees pursuant to Article VII, Section B titled "Residential Property Value Guarantees" of the Negotiated Agreement.

REQUIREMENTS OF PROPERTY OWNER

- 1. Property Owner(s) hereby declare that they own the subject property. This agreement shall terminate immediately upon transfer of ownership of the above-stated property, with the exception of a transfer of the Property upon death to a spousal joint tenant or in a divorce action by court decree.
- 2. Property Owner(s) hereby grant(s) to County reasonable access rights to the above-stated Subject Property as well as all other contiguous property held in common ownership for the following purposes: to ensure compliance with Department of Natural Resources codes and regulations regarding wells, including inspection of wells and abandonment and replacement of wells as necessary, to conduct environmental monitoring and investigation related to the County Solid Waste Facility; and, to remove windblown debris and litter originating from the landfill in a timely manner.
- 3. Property Owner(s) shall not unreasonably withhold County access to the subject property.
- 4. Property Owner(s) shall immediately notify Dane County Department of Waste & Renewables at waste.renewables@countyofdane.com of any change of address for above stated payee or change of ownership to above-stated subject property.

REQUIREMENTS OF COUNTY

- 1. County shall adhere to the complaint response timelines in Attachment B of Negotiated Agreement pursuant to Article V, Section D titled "Progressive Complaint System" of the Negotiated Agreement.
- 2. County shall exercise the above-stated access rights in a manner not to unreasonably interfere with Property Owner's(s') reasonable use and enjoyment of subject property. Furthermore, County will restore above-referenced, impacted Property to its existing condition prior to any required disturbance as expediently as possible.
- 3. County shall make the above-stated compensation payments to Property Owner(s) named in this agreement annually no later than June 1 for the current compensation year.
- 4. Compensation payments and other correspondence regarding this agreement shall be sent to the mailing address listed in this agreement.

GENERAL

The term of this agreement shall be for so long as Property Owner(s) is (are) entitled to any of the above-referenced compensation from County as set forth in the Negotiated Agreement or amendments thereto.

This agreement is not assignable and all rights provided herein are nontransferable. However, County may contract with third parties to conduct activities specifically described in above-stated provisions according to County access rights to Subject Property and contiguous properties all in common ownership. This agreement shall not inure to the benefit of administrators, assignees, successors, heirs, or executors of either party.

There may be no modification of this agreement, except in writing, executed with the same formalities as this instrument. This instrument including all text incorporated herein by reference contains the entire agreement between the parties hereto.

It is mutually understood and agreed that this contract shall be governed by the laws of the State of Wisconsin, both as to interpretation and performance.

In *witness* whereof, the parties have each executed this agreement at the place and date as signified below.

PROPERTY OWNER(S)	
By:	(Signature) Dated:
By:	(Signature) Dated:
COUNTY OF DANE	
By:	(Signature) Dated:
John Welch, Director	
Attest:	_ (Signature) Dated:
Attest: Dane County Corporation Counsel	_ (Signature) Dated:

FIRST AMENDMENT TO THE SOLID WASTE AGREEMENT For 2023-2032

Between the County of Dane and the City of Madison

THIS AGREEMENT, entered into by and between the County of Dane, a quasi-municipal corporation in the State of Wisconsin (hereinafter referred to as "County"), and the City of Madison, a municipal corporation of the State of Wisconsin (hereinafter referred to as "City"), is effective as of the date by which both parties have signed hereunder and according to the terms and conditions set forth herein.

WITNESSETH:

WHEREAS, on June 6, 2022, the County and the City (hereinafter referred to collectively as the "Parties") entered into a Solid Waste Agreement for 2023-2032 (the "Solid Waste Agreement") to set forth the terms and conditions agreed to by the Parties regarding the City's solid waste stream, tipping fees, daily cover and other City materials, and other issues associated with the County's landfill operations, as well as the to-be-developed County compost site. The term of this Agreement is from January 1, 2023 through December 31, 2032; and,

WHEREAS, the County is proposing to open a new landfill, Dane County Landfill No. 3, and as part of the statutory requirements for doing so, the City adopted a siting resolution and participated in the local negotiating committee process associated with the proposed landfill; and,

WHEREAS, during the local negotiation process, the City and the County identified some issues that both felt were more appropriately addressed within the structure of the Solid Waste Agreement; and,

WHEREAS, under Section 16 of the Solid Waste Agreement, the Agreement may only be amended by the written agreement of the Parties.

NOW, THEREFORE, in consideration of the above recitals and mutual covenants of the parties, the receipt and sufficiency of which is acknowledged by each party for itself, the Parties do agree as follows:

- 1. Section 14.c. of the Solid Waste Agreement is created as follows:
 - "c. Materials Testing and Acceptance. The County shall not impose unreasonable testing requirements stricter than applicable federal or state laws or regulations, or the approved Plan of Operation for the Facility, when determining the suitability of materials for disposal, including materials to be used for cover. At the discretion of the Director of Waste & Renewables, materials may be rejected if they create nuisance conditions such as excessive odors or unwanted pests, create operational issues such as unstable roadways or working surfaces, have an unreasonable level of contamination, or are otherwise unsuitable for disposal at the County's landfill operations."

Page 2 of 3

IN WITNESS WHEREOF, the County and the City, by their respective authorized agents, have caused this Agreement to be executed, effective as of the date by which all parties hereto have affixed their respective signatures.

FOR THE COUNTY OF DANE

Jamie Kuhn, County Executive	Date	
Scott McDonell, County Clerk	Date	
Execution of this Agreement by the County is authorized by		_, adopted by the Board of

FOR THE CITY OF MADISON Satya Rhodes-Conway, Mayor Date Maribeth Witzel-Behl, City Clerk Countersigned: David P. Schmiedicke, Finance Director Approved as to form:

Execution of this Agreement by the City is authorized by Resolution Enactment No. RES - , ID No.

_____, adopted by the Common Council of the City of Madison on ______, 2024.

Date

Michael Haas, City Attorney



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84330

File Type: Resolution Status: Council New

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 07/08/2024

File Name: CSM - 126 Langdon St Final Action:

Title: Approving a Certified Survey Map of property owned by 126 Langdon St, LLC

located at 126 Langdon Street (District 2).

Notes:

File ID: 84330

Sponsors: Planning Division Effective Date:

Attachments: CSM Application.pdf, Project Letter of Intent.pdf, Enactment Number:

Proposed CSM.pdf

Author: Hearing Date:

Entered by: tparks@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Planning Division	07/08/2024	Referred for Introduction				
	Action Text: Notes:	This Resolution was Ref Plan Commission (7/29/24)					

Text of Legislative File 84330

Fiscal Note

No City appropriation is required with the approval of this certified survey map. City costs associated with urban development in this area will be included in future operating and capital budgets subject to Common Council approval.

Title

Approving a Certified Survey Map of property owned by 126 Langdon St, LLC located at 126 Langdon Street (District 2).

Body

WHEREAS a Certified Survey Map of property owned by 126 Langdon St, LLC (dba Steve Brown Apartments) located at 126 Langdon Street, City of Madison, Dane County, Wisconsin has been duly filed for approval by the Plan Commission, its Secretary or their designee, as provided for in Section 16.23(4)(f) of Madison General Ordinances; and

WHEREAS Chapter 236, Wisconsin Statutes requires that the Madison Common Council approve any dedications proposed or required as part of the proposed division of the lands contained on said Certified Survey Map;

NOW THEREFORE BE IT RESOLVED that said Certified Survey Map, bond and subdivision contract, subsequent affidavits of correction, parkland acquisition documents, easement or right-of-way release or procurement documents or any other related document or documents as deemed necessary by the Secretary of the Plan Commission in accordance with the approval of said Certified Survey Map are hereby approved by the Madison Common Council.

BE IT FURTHER RESOLVED that the Mayor and City Clerk of the City of Madison are hereby authorized to sign the above mentioned documents related to this Certified Survey Map.

BE IT FURTHER RESOLVED that all dedications included in this Certified Survey Map or required as a condition of approval of this Certified Survey Map be and are hereby accepted by the City of Madison.

BE IT FURTHER RESOLVED that the Common Council authorizes City staff to request approval from the Capital Area Regional Planning Commission of any minor revisions to adopted environmental corridor boundaries within the Central Urban Service Area relating to this land division, and that the Council recognizes and adopts said revised boundaries.

BE IT FURTHER RESOLVED that the Planning Division is authorized to reflect the recorded Certified Survey Map in the Comprehensive Plan and any applicable neighborhood plans.

SUBDIVISION APPLICATION

** Please read both pages of the application completely and fill in all required fields **

For a digital copy of this form with fillable fields, please visit:

https://www.cityofmadison.com/sites/default/files/city-of-madison/development-services-center/documents/SubdivisionApplication.pdf

If you need an interpreter, translator, materials in alternate formats or other accommodations to access these forms, please call the Planning Division at (608) 266-4635.

Si necesita interprete, traductor, materiales en diferentes formatos, u otro tipo de ayuda para acceder a estos formularios, por favor llame al (608) 266-4635.

Yog tias koj xav tau ib tug neeg txhais lus, tus neeg txhais ntawv, los sis xav tau cov ntaub ntawv ua lwm hom ntawv los sis lwm cov kev pab kom paub txog cov lus qhia no, thov hu rau Koog Npaj (Planning Division) (608) 266-4635.

1. Application Type

City of Madison 6/17/24 11 a.m. Planning Division Madison Municipal Building, Suite 017 215 Martin Luther King, Jr. Blvd. P.O. Box 2985 Madison, WI 53701-2985 (608) 266-4635



NOTICE REGARDING LOBBYING ORDINANCE: If you are seeking approval of a development that has over 40,000 square feet of non-residential space, or a residential development of over 10 dwelling units, or if you are seeking assistance from the City with a value of \$10,000 (including grants, loans, TIF or similar assistance), then you likely are subject to Madison's lobbying ordinance (M.G.O. Sec. 2.40). You are required to register and report your lobbying. Please consult the City Clerk's Office for more information. Failure to comply with the lobbying ordinance may result in fines.

☐ Preliminary Subdivision Plat ☐ Final Subdivision Plat ☑ Land Division/Certified Survey Map (CSM)
If a Plat, Proposed Subdivision Name:
2. Review Fees
 For Preliminary and/or Final Plats, an application fee of \$250, plus \$50 per lot or outlot contained on the plat. For Certified Survey Maps, an application fee of \$250 plus \$200 per lot and outlot contained on the CSM. Make checks payable to "City Treasurer" and mail it to the following address: City of Madison Building Inspection; P.O. Box 2984; Madison, WI 53701-2984. Please include a cover page with the check which includes the project address, brief description of the project, and contact information.
3. Property Owner and Agent Information
Name of Property Owner: 126 Langdon St LLC Representative, if any: Steve Brown A partments c/o Dan Seeley
Street address: 120 W Gorham Street, Madison, WI 53703 City/State/Zip: 120 W Gorham Street, Madison, WI 53703
Telephone: Email: _dseeley@ stevebrownapts.com
Firm Preparing Survey: Vierbicher Associates Contact: Mike Ziehr / Justin Zampardi
Street address: 999 Fourier Drive, Suite 201 City/State/Zip: Madison, WI 53717
Telephone: 608-821-3962 Email: mzie@ vierbicher.com/jzam@ vierbicher.com
Check only ONE – ALL Correspondence on this application should be sent to: ☐ Property Owner, OR ☑ Survey Firm
4. Property Information for Properties Located within Madison City Limits
Parcel Addresses: 126 Langdon Street
Tax Parcel Number(s): 0709-144-1016-6
Zoning District(s) of Proposed Lots: DR2 (Downtown Residential) School District: Madison
Please include a detailed description of the number and use of all proposed lots and outlots in your letter of intent.
4a. Property Information for For Properties Located <i>Outside</i> the Madison City Limits in the City's Extraterritorial Jurisdiction:
Parcel Addresses (note town if located outside City): $\underline{\hspace{1cm}}^{\mathrm{N/A}}$
Date of Approval by Dane County: N/A Date of Approval by Town: N/A
• For an exterritorial request to be scheduled, approval letters from both the Town and Dane County must be submitted.
5. Subdivision Contents and Description. Complete table as it pertains to your request; do not complete gray areas.

Land Use	Lots	Outlots	Acres
Residential	1		0.7071
Retail/Office			
Industrial			

Land Use	Lots	Outlots	Acres
Other (state use):			
Outlots Dedicated to the Public (Parks, Stormwater, etc.)			
Outlots Maintained by a Private Group or Association			
PROJECT TOTALS			

6. Required Submittal Materials

<u>Digital (PDF) copies</u> of all items listed below (if applicable) are required. Applicants are to submit each of these documents as <u>individual PDF files</u> in an e-mail sent to <u>PCapplications@cityofmadison.com</u>. The transmittal shall include the name of the project and applicant. Note that <u>an individual email cannot exceed 20MB</u> and it is the <u>responsibility of the applicant</u> to present files in a manner that can be accepted. Electronic submittals via file hosting services (such as Dropbox) are <u>not</u> allowed. Applicants who are unable to provide the materials electronically should contact the Planning Division at <u>Planning@cityofmadison.com</u> or (608) 266-4635 for assistance.

☐ A Completed Subdivision Application Form (i.e. both sides of this form)

☐ **Map Copies** (prepared by a Registered Land Surveyor):

- For <u>Preliminary Plats</u>, the drawings must be drawn to scale and are required to provide all information as set forth in M.G.O. Sec. 16.23 (7)(a).
- For Final Plats, the drawings must be drawn to scale and drawn to the specifications of §236.20, Wis. Stats..
- For <u>Certified Survey Maps (CSMs)</u>, the drawings shall include all of the information set forth in <u>M.G.O. Secs. 16.23 (7)(a)</u> and (d), including existing site conditions, the nature of the proposed division and any other necessary data. Utility data (field located or from utility maps) may be provided on a separate map submitted with application.

For Plat & CSMs, in addition to the PDF copy, a digital CADD file shall also be submitted in a format compatible with AutoCAD. The digital CADD file(s) shall be referenced to the Dane County Coordinate System and shall contain, at minimum, the list of items stated below, each on a separate layer/level name. The line work shall be void of gaps and overlaps and match the plat, preliminary plat or CSM as submitted: a) Right-of-Way lines (public and private); b) Lot lines; c) Lot numbers; d) Lot/Plat dimensions; e) Street names; f) Easement lines (i.e. all in title and shown on the plat or CSM including wetland & floodplain boundaries.)

□ **Letter of Intent**: One copy of a letter describing the proposed subdivision or land division in detail including, but not limited to:

- The number and type/use of the lots and outlots proposed with this subdivision or land division, including any outlots to be dedicated to the public;
- Existing conditions and uses of the property;
- Phasing schedule for the project, and;
- The names of persons involved (property owner(s), subdivider, surveyor, civil engineer, etc.).
 - * The letter of intent for a subdivision or land division may be the same as the letter of intent submitted with a concurrent Land Use Application for the same property.
 - ** A letter of intent is not required for Subdivision Applications for lot combinations or split duplexes.

□ **Report of Title and Supporting Documents**: One copy of a City of Madison standard 60-year Report of Title obtained from a title insurance company as required in M.G.O. Sec. 16.23 and as satisfactory to the Office of Real Estate Services. Note:

- <u>The Report of Title must have been completed within three (3) months of the submittal date of this application. Title insurance or a title commitment policy are NOT acceptable</u> (i.e. a Preliminary Title Report or a Record Information Certificate).
- The electronic PDF submittal shall include images of the vesting deeds and all documents listed in the Report of Title.
- <u>Do not email these files to the City's Office of Real Estate Services</u>. Send them instead to the email address noted at the top of this page.

For Surveys Outside the Madison City Limits: One copy of the approval letters from the town where the	property is
located <u>and</u> Dane County shall be submitted with your request. The Plan Commission may not consider an	application
within its extraterritorial jurisdiction without prior approval from the town and Dane County.	

	,	,
7.	7. Applicant Declarations:	
	The signer attests that the application has been completed accurately	y and all required materials have been submitted:
	Applicant's Printed Name: Michael J. Ziehr Sig	gnature:
	Date: June 17, 2024 Interest In Property On This Date:	Land Surveyor

EFFECTIVE: JANUARY 2021 PAGE 2 OF 940



Steve Brown Apartments: The Langdon

Steve Brown Apartments is pleased to present the following Conditional Use Permit Application for the redevelopment of the 126 Langdon Street property, formerly the Langdon Apartments. This project is requesting a Conditional Use Permit (CUP) for a student focused housing project that delivers amenities and housing options for the neighborhood while reflecting the guidelines of the adopted plans and Downtown Residential 2 zoning district.

Request: Conditional Use Permit

Multi-Family dwelling larger than 8 units

Certified Survey Map

Project Name: The Langdon

Applicant: **Steve Brown Apartments** Dan Seeley

> 120 West Gorham Street Madison, WI 53703

Owner: 126 Langdon Street LLC.

> 120 West Gorham Street Madison, WI 53703

Design Team:

Architecture: Brownhouse Laurel Brown Shane Fry

202 West Gorham Street

Madison, WI 53703

Engineering/Landscape: Vierbicher Justin Zampardi Sarah Church

999 Fournier Drive Madison, WI 53717

Entitlement: Vandewalle & Associates **Brian Munson**

> 120 East Lakeside Street Madison, WI 53715

120 East Lakeside Street • Madison, Wisconsin 53715 • 608.255.3988 • 608.255.0814 Fax 247 Freshwater Way, Suite 530 • Milwaukee, Wisconsin 53204 • 414.988.8631 www.vandewalle.com

Site Data:

Address: 126 Langdon Street PIN: 0709-1441-016-6

Parcel Size: 30,801 square feet (.71 acres) Existing Zoning: Downtown Residential 2 (DR-2)

Adopted Plan Recommendations:

Comprehensive Plan

The Comprehensive Plan Designates the site as Medium Residential which allows for a range of building sizes and a general density of 20-90 du/acre.

Downtown Plan

The Downtown Plan identifies the site as part of the Langdon Street district and calls for "Predominately Residential Uses" set within the context of predominately student housing. The site is also identified as part of the 5+2 additional building height areas, but the project is not seeking bonus height.

Maximum Building Height Map:



06/14/24 Page 2 of 9

Project Summary:

The applicant is requesting conditional use approval for the construction of a 5 story 79-unit student focused multi-family building, consistent with the current zoning and plan recommendations for the site. The property is the former Langdon Dormitory site, demolished by the owner in 2019, a 7-story dormitory with 375-395 residents when in operation.

Dwelling Unit Mix:

Studios	8
One Bedroom	13
Two Bedroom	58
Total:	79

Parking:

Underbuilding Vehicle Stalls: 35 parking stalls

Bicycle Parking Stalls: 79 (standard stalls)

Usable Open Space:

Required: 2,740 sq. ft.

Supplied: 5,069 sq. ft. (balconies)

2,998 sq. ft. (at grade open space)

Lot Coverage:

Total acreage 30,801 sq. ft. Impervious area 24,000 sq. ft. Lot Coverage 77.9%

Project Schedule: Spring 2025 construction start

Fall 2026 occupancy

06/14/24 Page 3 of 9

Conditional Use Review Standards:

1. The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, or general welfare.

The proposal for multi-family housing is consistent with the historic use of the site (formerly the Langdon Private Dormitory) and the surrounding area. The overall building density and scale are compatible with the adjoining neighborhood which is comprised of a range of building sizes and heights of 2-5+ stories.

2. The City is able to provide municipal services to the property where the conditional use is proposed, given due consideration of the cost of providing those services.

The site is well served with readily available municipal services adjacent to the site.

3. The uses, values and enjoyment of other property in the neighborhood for purposes already established will not be substantially impaired or diminished in any foreseeable manner.

The proposed building is consistent with the previous use of the site and reflects the recommendations of the Comprehensive Plan, Downtown Plan, and current zoning on the site.

4. The establishment of the conditional use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.

The proposal will improve vehicular and fire access to the adjoining properties and will not preclude development or improvement of the surrounding properties.

5. Adequate utilities, access roads, drainage, parking supply, internal circulation improvements, including but not limited to vehicular, pedestrian, bicycle, public transit and other necessary site improvements have been or are being provided.

The proposal will implement all of the necessary site improvements to support the project.

6. Measures, which may include transportation demand management (TDM) and participation in a transportation management association have been or will be taken to provide adequate ingress and egress, including all off-site improvements, so designed as to minimize traffic congestion and to ensure public safety and adequate traffic flow, both on-site and on the public streets.

The project widens the shared access drive with adjoining properties creating better access for vehicles and fire for the site and the surrounding buildings.

06/14/24 Page 4 of 9

7. The conditional use conforms to all applicable regulations of the district in which it is located.

Met

8. When applying the above standards to an application by a community living arrangement, the Plan Commission shall:

Not Applicable

9. When applying the above standards to any new construction of a building or an addition to an existing building the Plan Commission shall find that the project creates an environment of sustained aesthetic desirability compatible with the existing or intended character of the area and the statement of purpose for the zoning district. In order to find that this standard is met, the Plan Commission may require the applicant to submit plans to the Urban Design Commission for comment and recommendation. (Am. by ORD-14-00030, 2-18-14)

The project concept has been discussed and presented to the Alder, City Staff, and Capital Area Neighborhood Association for feedback and refinement to make sure that the project continues the character and scale of the Langdon Neighborhood and underlying zoning.

10. When applying the above standards to an application for a reduction in off-street parking requirements, the Plan Commission shall consider and give decisive weight to all relevant facts, including but not limited to, the availability and accessibility of alternative parking; impact on adjacent residential neighborhoods; existing or potential shared parking arrangements; number of residential parking permits issued for the area; proximity to transit routes and/or bicycle paths and provision of bicycle racks; the proportion of the total parking required that is represented by the requested reduction; the proportion of the total parking required that is decreased by Sec. 28.141. The characteristics of the use, including hours of operation and peak parking demand times design and maintenance of off-street parking that will be provided; and whether the proposed use is now or a small addition to an existing use.

The proposal meets the parking requirements for the Downtown Residential 2 zoning district. This site is well served with pedestrian and bicycle routes and transit service.

11. When applying the above standards to telecommunication facilities, the Plan Commission shall consider the review of the application by a professional engineer required by Sec. 28.143.

Not Applicable.

12. When applying the above standards to an application for height in excess of that allowed in the district, the Plan Commission shall consider recommendations in adopted plans; the impact on surrounding properties, including height, mass,

06/14/24 Page 5 of 9

orientation, shadows and view; architectural quality and amenities; the relationship of the proposed building(s) with adjoining streets, alleys, and public rights of ways; and the public interest in exceeding the district height limits.

Not Applicable.

13. When applying the above standards to lakefront development under Sec. 28.138, the Plan Commission shall consider the height and bulk of principal buildings on the five (5) developed lots or three hundred (300) feet on either side of the lot with the proposed development.

Not applicable.

- 14. When applying the above standards to an application for height in excess of that allowed by Section 28.071(2)(a) Downtown Height Map for a development located within the Additional Height Areas identified in Section 28.071(2)(b), the Plan Commission shall consider the recommendations in adopted plans, and no application for excess height shall be granted by the Plan Commission unless it finds that all of the following conditions are present:
 - a. The excess height is compatible with the existing or planned (if the recommendations in the Downtown Plan call for changes) character of the surrounding area, including but not limited to the scale, mass, rhythm, and setbacks of buildings and relationships to street frontages and public spaces.
 - b. The excess height allows for a demonstrated higher quality building than could be achieved without the additional stories.
 - c. The scale, massing and design of new buildings complement and positively contribute to the setting of any landmark buildings within or adjacent to the projects and create a pleasing visual relationship with them.

 Not applicable.
- 15. When applying the above standards to an application to redevelop a site that was occupied on January 1, 2013 by a building taller than the maximum building height allowed by <u>Section 28.071(2)(a)</u> Downtown Height Map, as provided by <u>Section 28.071(2)(a)</u>1., no application for excess height shall be granted by the Plan Commission unless it finds that all the following additional conditions are also present:

While the site was occupied by a building taller than the maximum building height, the project is not seeking a height exception.

16. When applying the above standards to an application for limited production and processing use, the Plan Commission shall consider the effect of such a use on the surrounding properties, including the effects of odors, noise, vibration, glare, hours of operation, and other potential side effects of a manufacturing process. (Cr. by ORD-15-00124, 11-11-15)

Not Applicable.

06/14/24 Page 6 of 9

Downtown Design Guidelines

Site Design + Building Placement Orientation

Access + Circulation

The site parking and loading is accessed from the shared driveway adjacent to the north side of the building. This driveway is proposed to be widened to create additional circulation room and fire access for this project and all of the adjoining properties. Additional stalls for pickup and deliveries have been added off of the southern shared driveway to create an off-street staging area.

Usable Open Space -Residential Development

The project offers residents both balcony and ground level open space adjacent to the building, consistent with the urban location.

Landscaping

The ground level landscaping offers a well designed landscape setting with urban appropriate plantings.

Lighting

Building lighting is proposed to provide appropriate levels of light while maintaining the character of the neighborhood.

Architecture

Massing

The residential building is designed to fit into the rhythm and context of the Langdon Street district while developing a site with significant grade change. The five-story massing is broken down with articulation and materials that are complimentary to the character of the neighborhood while avoiding faux-historic references

Building Components

The residential building follows the downtown plan recommendations to create a architectural expression and articulation consistent with the pattern of the neighborhood.

Visual Interest

This proposal bridges the need to have a structure that both feels at home near a historic district, but also one that separates itself as a modern building. The building itself uses material selection and massing to break up the façade into smaller pieces. Brick is used at the corners to ground the building and tie it into the surrounding structures. There will be detailing within the masonry, such as herringbone and different depths. At the top of the third story there is a cornice line which matches the adjacent structures. Where the mass continues up to the 5th story, we introduce both

06/14/24 Page 7 of 9

cement board siding and metal panels. The recessed balconies help to give relief to the façade and further enhance the different masses. On the lake side, extensive fenestration is used to highlight those views.

Door and Window Openings

Door and window openings are designed to fit into the surrounding context.

Building Materials

Materials on the primary and secondary facades are designed to be in alignment with the character and materiality of the Langdon district and feature durable high quality materials that are complimentary of the surroundings.

Terminal Views and Highly-Visible Corners
Not applicable

Awnings and Canopies Not applicable.

Signage

Signage for the building will be integrated into the project through future submittals and will be consistent with the signage ordinance.

06/14/24 Page 8 of 9

Legal Description

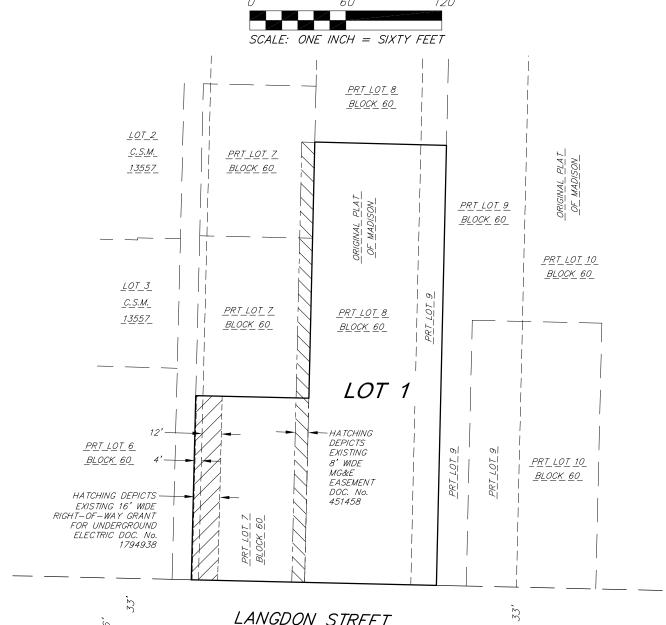
All that part of Lot Eight (8) and the Southwesterly 16 feet of Lot Nine (9), Block Sixty (60), Original Plat of Madison, according to the Pritchette Plat thereof, in the City of Madison, Dane County, Wisconsin, which lies Southeasterly of a line parallel with and 275 feet Northwesterly from the Southeasterly line of said Lots.

ALSO the following described lands,

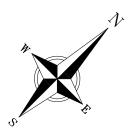
The Southeast 115 feet of Lot Seven (7), and the Northeasterly 4 1/2 feet in width of the Southeast 115 feet of Lot Six (6), Block Sixty (60), Original Plat of Madison, according to the recorded plat thereof, in the City of Madison, Dane County, Wisconsin

06/14/24 Page 9 of 9

PART OF LOTS 6, 7, 8 AND 9, BLOCK 60, ORIGINAL PLAT OF MADISON AS RECORDED IN VOLUME A OF PLATS, ON PAGE 3, AS DOCUMENT NUMBER 102 AND LOCATED IN THE SW 1/4 OF THE SE 1/4 OF SECTION 14, TOWNSHIP 07 NORTH, RANGE 09 EAST, CITY OF MADISON, DANE COUNTY, WISCONSIN



 LANGDON STREET	, ,	33'	 	
	9	33,	 . — —	



EXISTING EASEMENTS

Vierbicher planners | engineers | advisors



FN: 240290 DATE: 06/12/2024 REV:

Drafted By: MZIE

SURVEYED FOR: Steve Brown Apts. 120 W. Gorham Street Madison, WI 53703

C.S.M. No._____

Doc. No.____

Vol.____ Page____

SHEET 5 OF 9

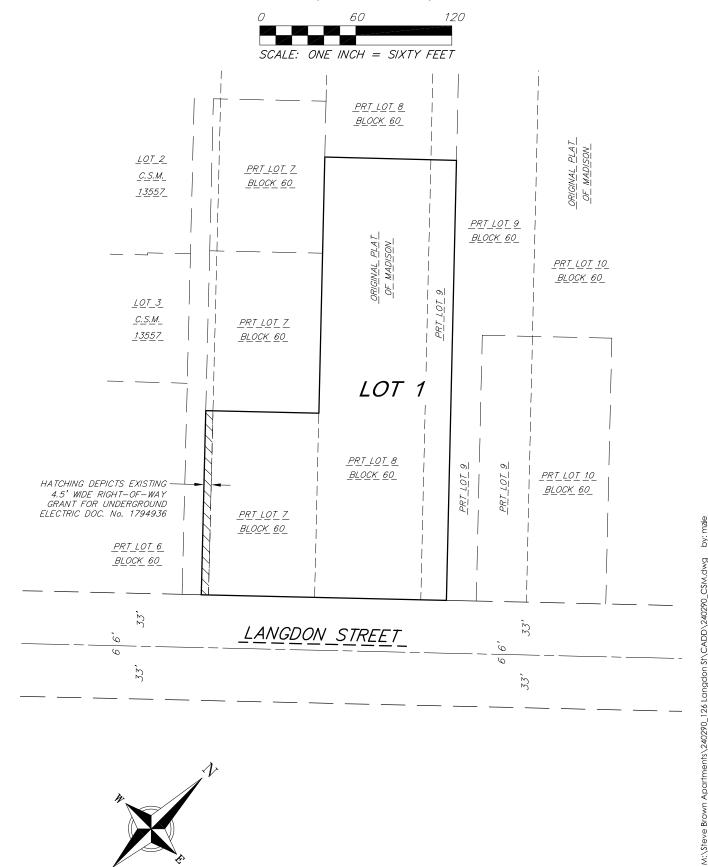
by: mzie

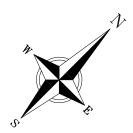
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PART OF LOTS 6, 7, 8 AND 9, BLOCK 60, ORIGINAL PLAT OF MADISON AS RECORDED IN VOLUME A OF PLATS, ON PAGE 3, AS DOCUMENT NUMBER 102 AND LOCATED IN THE SW 1/4 OF THE SE 1/4 OF SECTION 14, TOWNSHIP 07 NORTH, RANGE 09 EAST, CITY OF MADISON, DANE COUNTY, WISCONSIN





EXISTING EASEMENTS

vierbicher planners | engineers | advisors



FN: 240290 DATE: 06/12/2024 REV:

Drafted By: MZIE

SURVEYED FOR: Steve Brown Apts. 120 W. Gorham Street Madison, WI 53703

C.S.M. No. Doc. No. Page

SHEET 6 OF 9

. 2024 - 9:07a

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by: mzie

PART OF LOTS 6, 7, 8 AND 9, BLOCK 60, ORIGINAL PLAT OF MADISON AS RECORDED IN VOLUME A OF PLATS, ON PAGE 3, AS DOCUMENT NUMBER 102 AND LOCATED IN THE SW 1/4 OF THE SE 1/4 OF SECTION 14, TOWNSHIP 07 NORTH, RANGE 09 EAST, CITY OF MADISON, DANE COUNTY, WISCONSIN

LEGAL DESCRIPTION

Part of Lots 6, 7, 8 and 9, Block 60, Original Plat of Madison, as recorded in Volume A of Plats, on Page 3, as Document Number 102 and located in the Southwest Quarter of the Southeast Quarter of Section 14, Township 07 North, Range 09 East, City of Madison, Dane County, Wisconsin, more fully described as follows:

Commencing at the South quarter corner of said Section 14; thence S88*46'41"E, along the south line of the Southeast Quarter of said Section 14, a distance of 557.92 feet; thence N01"13'19"E, 879.60 feet to northwesterly right—of—way line of Langdon Street and the point of beginning of this description; thence N43"35'57"W, 115.06 feet; thence N46"14'44"E, 70.65 feet; thence N43"40'57"W, 160.07 feet; thence N46"24'50"E, 82.37 feet; thence S43"40'57"E, 275.04 feet to the aforementioned northwesterly right—of—way line of said Langdon Street; thence S46"18'04"W, along said northwesterly right—of—way line, 153.18 feet to the point of beginning.

Said description contains 30,802 square feet or 0.7071 acres, more or less.

17 Jun 2024 - 8:23a M:\Steve Brown Apartments\240290_126 Langdon St\CADD\240290_CSM.dwg by: mzie

Vierbicher planners | engineers | advisors



FN: 240290 DATE: 06/12/2024

REV:
Drafted By: MZIE
Chapted By: IZAM

SURVEYED FOR: Steve Brown Apts. 120 W. Gorham Street Madison, WI 53703 C. S. M. No._____

Doc. No.____

Vol.____ Page_____

SHEET 7 OF 9

PART OF LOTS 6, 7, 8 AND 9, BLOCK 60, ORIGINAL PLAT OF MADISON AS RECORDED IN VOLUME A OF PLATS, ON PAGE 3, AS DOCUMENT NUMBER 102 AND LOCATED IN THE SW 1/4 OF THE SE 1/4 OF SECTION 14, TOWNSHIP 07 NORTH, RANGE 09 EAST, CITY OF MADISON, DANE COUNTY, WISCONSIN

caused the land described on this Certified Survey Map to be surveyed, divided, mapped and dedicated as represented on the map hereon. We further certify that this Certified Survey map is required by S236.34 to be submitted to the City of Madison for approval. Witness the hand and seal of said owner this day of, 2024.
126 Langdon Street, LLC a Wisconsin limited liability company
Ву:
State of Wisconsin))ss.
)ss. County of Dane)
Personally came before me this day of, 2024, the above named, to me known to be the persons who executed the foregoing instrument
and acknowledged the same.
My Commission expires: Notary Public, State of Wisconsin
The target a section of the section
CONSENT OF MORTGAGEE
BMO Harris Bank N.A., a banking association duly organized and existing under and by virtue of the laws of the State of Wisconsin, mortgagee of the above described land, does hereby consent to the surveying, dividing, mapping and dedicating of the land described on this Certified Survey Map and does hereby consent to the Owner's Certificate.
IN WITNESS WHEREOF, the said BMO Harris Bank N.A., has caused these presents to be signed by its
Wisconsin, on this its, at Waukesha, with a substitution of the substituti
BMO Harris Bank N.A.
Ву:
State of Wisconsin)
)ss. County of Waukesha)
Personally came before me this day of, 2024,, 2024,, and the above named banking association, to me known to be the
persons who executed the foregoing instrument, and to me known to be such of said banking association, and acknowledged that they executed
the foregoing instrument as such officer as the deed of said banking association, by its authority.
Notary Public, State of Wisconsin
My Commission expires:

VIETDICHET planners | engineers | advisors



FN: 240290 DATE: 06/12/2024 RFV:

REV:
Drafted By: MZIE
Chacked By: IZAA

SURVEYED FOR: Steve Brown Apts. 120 W. Gorham Street Madison, WI 53703

C.S.M. No._____

Doc. No._____

Vol.____ Page_____

SHEET 8 OF 9

Enactment Number day of the acceptance of those lands City of Madison for public use.	, File ID Number , 20, and that	adonted on the	
,	realcated and rights conveyed by	said enactment further provided for said certified survey map to the	or
Dated this day of _	, 20		
Maribeth Witzel-Behl, City Clerk	City of Madison		
	COMMISSION CERTIFICATE		
	Secretary of the City of Madison		
By: Matthew Wachter, Secretary of the Plan Coi	Date: pmission		
certify to the best of my know divided and mapped the lands one (1) is a correct represento surveyed and the division of th orovided. I further certify that	sional Land Surveyor, S—2401, do edge and belief, that I have surv described herein and that the ma tion of the exterior boundaries of at land in accordance with the in this Certified Survey Map is in fu of the Wisconsin State Statutes,	veyed, ap on sheet f the land nformation , Chapter Ordinance	
A—Ė7 of the Wisconsin Adminis of the City of Madison in surve	ing, dividing and mapping the sc	arrio.	
A—Ë7 of the Wisconsin Adminis of the City of Madison in surve Vierbicher Associates, Inc. By: Michael J. Ziehr		arrie.	
A—È7 of the Wisconsin Adminis of the City of Madison in surve Vierbicher Associates, Inc. By: Michael J. Ziehr Date: <u>JUNE 12, 2024</u> Signed:	ving, dividing and mapping the so	arric.	
A—Ë7 of the Wisconsin Adminis of the City of Madison in surve Vierbicher Associates, Inc. By: Michael J. Ziehr Date: <u>JUNE 12, 2024</u>	ving, dividing and mapping the so	anc.	
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City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84337

File ID:84337File Type:ResolutionStatus:Council New

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 07/08/2024

Final Action:

File Name: Delete the classification of Transit Safety Coordinator

in CG44, R05. Create the classifications of Transit Safety Coordinator 1 in CG44, R05, and Transit Safety Coordinator 2 in CG44, R07. Recreate position #4850 of Transit Safety Coordinator as

Transit

Title: Delete the classification of Transit Safety Coordinator in CG44, R05. Create the

classifications of Transit Safety Coordinator 1 in CG44, R05, and Transit Safety

Coordinator 2 in CG44, R07. Recreate position #4850 of Transit Safety

Coordinator as Transit Safety Coordinator 2 and reallocate the employee, J. Maki,

to the new classification.

Notes:

Sponsors: Director of Human Resources Effective Date:

Attachments: PB memo Transit Safety Coordinator 6.27.24.pdf,

Transit Safety Coordinator 1 Class Spec 2024 DRAFT.pdf, Transit Safety Coordinator 2 Class Spec

2024 DRAFT.pdf

Author: Ann Schroeder, HR Analyst

Hearing Date:

Enactment Number:

Entered by: kklafka@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Human Resource	os 07/08/2024	Referred for Introduction				
	Department Action Text:	This Resolution was Ref		ction			
	Notes:	Finance Committee (7/22/2	4), Personnel Boar	d (7/17/24), Common Council (8/	/6/24)		

Text of Legislative File 84337

Fiscal Note

The proposed reclassification results in an annualized cost increase of \$5,000 to \$9,000. Any additional costs in 2024 will be absorbed within Metro Transit's existing operating budget authority.

Title

Delete the classification of Transit Safety Coordinator in CG44, R05. Create the classifications of Transit Safety Coordinator 1 in CG44, R05, and Transit Safety Coordinator 2 in CG44, R07. Recreate position #4850 of Transit Safety Coordinator as Transit Safety Coordinator 2 and reallocate the employee, J. Maki, to the new classification.

Body

Resolution to delete the classification of Transit Safety Coordinator in CG44, R05 and to create the classifications of Transit Safety Coordinator 1 in CG44, R05 and Transit Safety Coordinator 2 in CG44, R07; further resolve to recreate position #4850 of Transit Safety Coordinator as Transit Safety Coordinator 2 and to reallocate the current employee, J. Maki, to the classification of Transit Safety Coordinator 2 in the Transit Utility operating budget thereof.

TO: Personnel Board

FROM: Ann Schroeder, Metro Transit

DATE: June 27, 2024

SUBJECT: Transit Safety Coordinator

At the request of Transit General Manager Justin Stuehrenberg and Transit Chief Operating Officer Ayodeji Arojo, I have studied the 1.0 FTE position #4850 of Transit Safety Coordinator, CG44/R05. After reviewing the updated position description and interviewing the supervisor and employee, I recommend the following:

- Delete the classification of Transit Safety Coordinator in CG44/R05
- Create the classification of Transit Safety Coordinator 1 in CG44/R05
- Create the classification of Transit Safety Coordinator 2 in CG44/R07
- Recreate position #4850 of Transit Safety Coordinator as Transit Safety Coordinator 2
- Reallocate employee J. Maki to the position in the classification in CG44/R07

The position of Transit Safety Coordinator was created in the Transit Utility's 2021 budget in response to requirements by the Federal Transit Administration (FTA). The original class specification reads:

This is responsible professional, administrative and technical work coordinating an overall centralized and uniform approach to varied safety and security programming throughout Metro Transit. This position is responsible for the development, management, and administration of policies and procedures necessary to safeguard employees, customers, facilities, equipment and vehicles of Metro Transit.

Examples of duties and responsibilities include:

Serve as the clearinghouse and technical expert on applicable Federal, State, and local health and safety laws, rules, and regulations, and coordinate associated compliance and reporting activities. Develop, implement, and evaluate Metro's safety and security plans for compliance with federal, state and location regulations through the agency.

The Classification Change Worksheet highlighted the following items as current duties of the position:

- Update and review Metro's FTA mandated Agency Safety Plan
- Develop and deliver training on safety related topics for various Metro units
- Oversee new security related capital improvement projects (purchase of cameras)
- Evaluate job task analysis and evaluate job risk/safety

The tasks the position is doing align with the original class specification:

Prepare plans and reports as needed for federal and state regulatory compliance... Coordinate or conduct safety and security related training and

drills agency wide.... Serve as technical resource to **evaluate the purchase of safety and security equipment** for consideration in budgeting processes.... Monitor and identify trends in **worker incidents, risk analysis**, and corrective actions. [emphasis added]

While the tasks involved in the classification remain largely the same, the level of independence and expertise with which they are done has increased significantly. If the current employee were to leave, it would be difficult to replace them at this current level. The agency would need to rely heavily on other staff to step in until a new hire became sufficiently knowledgeable about the transit industry and extensive FTA rules and regulations to be proficient in the position.

The Hearings and Accommodation Specialist 1 and 2 series are in CG18/R05 and CG18/R07 respectively. The class specification for Hearings and Accommodation Specialist 2 reads:

This work involves the consideration of CDA decisions...as in accordance with the law, HUD regulations, and CDA policies. The work involves the interpretation of applicable laws and regulations, the rending of written decisions and citing of statutes and regulations in support of decisions....

This series is structured to provide advancement from Hearings and Accommodations Specialist 1 to Hearings and Accommodation Specialist 2, as a function of the employee's career development....

The Hearings and Accommodation Specialist 2 is expected to independently perform the full range of duties and responsibilities....

The Hearings and Accommodations Specialist 1 and 2 compensation group and ranges are similar to Metro Transit's CG44/R05 (Transit Safety Coordinator) and CG44/R07. In comparison to that career progression series, originally both the Transit Service Manager and a Transit Operations Manager did some of this work, and the class specification stated that the employee would work "under the direction of the Transit Services Manager...." That is appropriate for the entry level professional work of the position. Currently, the position is the primary contact for safety and security for the agency, interprets and implements FTA rules and regulations, and works quite independently. This is similar to an objective level professional position such as the Hearings and Accommodations Specialist 2.

In addition, the position has taken on some new tasks not previously in the classification specification, and demonstrates increased autonomy, independent decision making, and leadership responsibilities. It represents Metro on the Transit Mutual Insurance Board of Directors and votes without input from other Metro staff; chairs committees rather than just coordinating them; leads writing grants for safety and security funding rather than just lending technical information; and is developing and implementing initiatives like the first Metro Transit Roadeo the agency has had in many years as a part of a multi-pronged and creative approach to promoting safety culture. These are key responsibilities in keeping with an objective rather than entry level position.

In contrast, while the Transit Safety Coordinator focuses specifically on the Transit Utility, the Safety Coordinator 1 and 2 classifications (CG 18/R08 and CG18/R10) for the City of Madison have a much broader scope, administering a centralized City program that involves coordinating with staff in 30+ City agencies in the application of program-related laws and regulations. The

Safety Coordinator also has significant duties related to the City's Workers' Compensation program and serves as backup to the Risk Manager (CG18/R15).

Creating classifications of Transit Safety Coordinator 1 and Transit Safety Coordinator 2 as part of a career series progression that would be expected to happen in approximately two years based on greater independence, leadership, and additional duties, allows for employee career development while decreasing the amount of needed oversight, direction, and completion of duties required by managers.

For the reasons outlined in this memo, I recommend that the classification of Transit Safety Coordinator be deleted from CG 44/R05 in the salary schedule; a new classification of Transit Safety Coordinator 1 be created in CG 44/R05; a new classification of Transit Safety Coordinator 2 be created in CG44/R07; position #4850 of Transit Safety Coordinator be recreated as Transit Safety Coordinator 2 in CG44/R07; and the current employee J. Maki be reallocated to the new position in the Metro Transit operating budget.

Editor's Note:

Effective Date: December 24, 2023

Compensation	2024 Annual	2024 Annual	2024 Annual
Group/Range	Minimum	Maximum	Maximum
	(Step 1)	(Step 5)	(+12% longevity)
18/05	\$ 65,679	\$ 76,149	\$ 85,287
44/05	\$ 65,658	\$ 76,125	\$ 85,260
18/07	\$ 70,091	\$ 83,030	\$ 92,994
44/07	\$ 70,068	\$ 83,003	\$ 92,964

cc: Justin Stuehrenberg, Transit General Manager
Ayodeji Arojo, Transit Chief Operating Officer
Allisa Brown, Transit HR Manager
Tom Kimble, Transit Finance Manager
Erin Hillson, Human Resources Director
Emaan Abdel-Halim, Human Resources Services Manager
Kurt Rose, Employee and Labor Relations Manager
Tameaka Bryant, Labor Relations Specialist
Justin Maki, Transit Safety Coordinator

TRANSIT SAFETY COORDINATOR 1

CLASS DESCRIPTION

General Responsibilities:

This is responsible professional, administrative and technical work coordinating an overall centralized and uniform approach to varied safety and security programming throughout Metro Transit. This position is responsible for the development, management, and administration of policies and procedures necessary to safeguard employees, customers, facilities, equipment and vehicles of Metro Transit. The work involves providing consultation to managers in identifying and resolving safety problems, implementing related City programs, and coordinating the sharing of information relative to safety and security matters. Under the direction of the Transit Services Manager, the employee must exercise judgment and discretion in coordinating the functions and compliance of Metro's risk and safety programs in compliance with federal regulations.

Within the Transit Utility, this series is structured to provide advancement from Transit Safety Coordinator 1 to Transit Safety Coordinator 2 as a function of the employee's career development. Movement to Transit Safety Coordinator 2 will generally take at least 2 years and is dependent on the employee taking on additional duties and responsibilities with a higher degree of judgment, independence, and discretion.

Examples of Duties and Responsibilities:

Serve as the clearinghouse and technical expert on applicable Federal, State, and local health and safety laws, rules, and regulations, and coordinate associated compliance and reporting activities. Develop, implement, and evaluate Metro's safety and security plans for compliance with federal, state, and local regulations throughout the agency.

Provide technical assistance and coordination to management and to Metro's safety committees in the areas of safety and security.

Develop a management system to evaluate plan implementation for compliance and efficacy. Monitor and identify trends in worker incidents, risk analysis, and corrective actions. Report on employee safety related discipline. Analyze and report on vehicle and facilities insurance claims. Monitor effectiveness of individual safety programs.

Maintain records and statistics to monitor safety and security incidents. Monitor and report trends and significant incidents related to safety and security. Summarize and analyze results, develop recommended solutions to address identified hazards. Work with management to establish a plan of action and on-going hazard management systems to eliminate or moderate the identified hazards. Develop written reports on the results, follow-up on the actions taken by the unit relative to the plan, and prepare final reports as necessary.

Prepare plans and reports as needed for federal and state regulatory compliance, public oversight bodies, and internal work groups. Coordinate and update existing plans across federal safety, local emergency preparedness, and property insurer requirements. Maintain and update plans as necessary for compliance, mitigations, and efficacy.

- 1 -

960

Administer safety and security policies in coordination with City Risk Management personnel. Perform routine on-site observations for program adherence. Identify and work to eliminate hazards, and encourage safe working habits and environments. Identify needs and provision of on-site personal protective equipment for various programs.

Coordinate the implementation of Metro Transit's safety goals, rules, and/or programs. Deliver plans to applicable to units within the agency for implementation by unit managers. Work with managers to support safety and security initiatives. Develop, implement, and maintain standard operating procedures related to safety and security in coordination with unit managers and city staff as appropriate.

Monitor activities and provide technical assistance on the development of safety and security manuals and procedures for tasks, equipment, facilities, certifications, and emergency response.

Prepare necessary communications to managers, employees, safety committees, etc. to provide information on safety and security issues.

Staff and/or support regular safety and security programming during agency unit meetings. Staff and coordinate safety and security related committees and work groups. Actively administer safety related programming as defined by the collective bargaining agreement.

Participate in training on safety and security policies and procedures. Serve as main resource for Metro's safety training needs and trouble shoot issues. Coordinate or conduct safety and security related training and drills agency wide. Coordinate emergency responder training on Metro equipment.

Liaison with police department and school district regarding dangerous and illegal situations on the buses and/or transfer points. Train on Emergency Operations Center procedures to support operations staffing.

Support staff in responding to customer and employee complaints, questions, and concerns regarding safety and security incidents. Coordinate with the Transit Operations Chief or designee on appropriate administrative and emergency actions when necessary and/or refer for resolution.

Monitor the scheduling of security patrols at facilities. Learn safety and security monitoring systems throughout Metro facilities. Manage Transit Exclusion policy.

Serve as a technical resource to evaluate the purchase of safety and security equipment for consideration in budgeting processes.

Coordinate annual safety and employee recognition events.

Perform related work as required.

QUALIFICATIONS

Knowledge, Skills and Abilities:

Working knowledge of safety program administration principles, practices, equipment, and resources. Working knowledge of Federal, State and local laws, code, regulations relating to safety. Working knowledge of and ability to use computer software applicable to the duties of the position. Ability to learn and understand specific transit safety regulations. Ability to operate related testing equipment. Ability to identify unsafe working conditions, operating practices, or safety training needs and to develop effective recommendations and strategies for their resolution. Ability to create extensive written plans and reports and analyze data regarding safety statistics and other related information. Ability to evaluate plans, polices, trainings, and systems for compliance and effectiveness ability to analyze raw data for reporting. Ability to establish effective working relationships with co-workers, other department/city staff, and the public. Ability to work effectively with multicultural populations. Ability to communicate effectively both verbally, in writing, and at public presentations. Ability to effectively investigate workplace safety concerns. Ability to recommend purchases of safety related equipment. Ability to conduct research into safety-related matters in order to develop effective recommendations. Ability to support recommendations with appropriate data. Ability to develop effective workplace safety policies and procedures. Ability to maintain adequate attendance.

Training and Experience:

Generally, positions in this classification will require:

Two (2) years of responsible occupational safety experience, which included involvement in safety program development, plan writing, implementation, and monitoring; including hazard identification and resolution. Such experience will normally have been gained after graduation from an Associate's degree program in Occupational Safety or closely related field. A Bachelor's Degree in Occupational Safety or a closely related field may be substituted for one (1) year of work experience. Other combinations of training and/or experience which can be demonstrated to result in the possession of the knowledge, skills, and abilities necessary to perform the duties of the position will also be considered.

Specific training and experience requirements will be established at the time of recruitment.

Special Requirements:

Ability to meet the transportation needs of the position.

Transit Safety Institute Certification within three (3) years.

Physical Requirements:

The employee will be expected to physically visit Metro worksites throughout the City in order to conduct safety studies. This may involve walking over various terrain, including hilly, muddy, and snowy terrain. Work may be performed in all weather conditions and may involve dangerous chemicals, heavy machinery, and other hazardous conditions. The incumbent must possess the physical strength, ability and acuity necessary to conduct safety studies and inspections.

Department/Division	Comp. Group	Range
Metro	44	5

Approved:		
	Erin Hillson	Date
	Human Resources Director	



- 4 **- 963**

TRANSIT SAFETY COORDINATOR 2

CLASS DESCRIPTION

General Responsibilities:

This is responsible professional, administrative and technical work coordinating an overall centralized and uniform approach to varied safety and security programming throughout Metro Transit. This position is responsible for the development, management, and administration of policies and procedures necessary to safeguard employees, customers, facilities, equipment and vehicles of Metro Transit. The work involves providing consultation to managers in identifying and resolving safety problems, implementing related City programs, and coordinating the sharing of information relative to safety and security matters. Under the direction of the Transit Services Manager, the employee must exercise judgment and discretion in coordinating the functions and compliance of Metro's risk and safety programs in compliance with federal regulations.

Within the Transit Utility, this series is structured to provide advancement from Transit Safety Coordinator 1 to Transit Safety Coordinator 2 as a function of the employee's career development. Movement to Transit Safety Coordinator 2 will generally take at least 2 years and is dependent on the employee taking on additional duties and responsibilities with a higher degree of judgment, independence, and discretion.

Examples of Duties and Responsibilities:

Serve as the clearinghouse, technical expert, and leader on applicable Federal, State, and local health and safety laws, rules, and regulations, and coordinate associated compliance and reporting activities. Develop, implement, update, and evaluate Metro's safety and security plans for compliance with federal, state, and local regulations throughout the agency.

Provide technical assistance and coordination to management and lead Metro's safety committees in the areas of safety and security.

Develop a management system to evaluate plan implementation for compliance and efficacy. Monitor and identify trends in worker incidents, risk analysis, and corrective actions. Report on employee safety related discipline. Analyze and report on vehicle and facilities insurance claims. Monitor effectiveness of individual safety programs and recommend updates and changes.

Maintain records and statistics to monitor safety and security incidents. Monitor and report trends and significant incidents related to safety and security. Summarize and analyze results, develop recommended solutions to address identified hazards. Work with management to establish a plan of action and on-going hazard management systems to eliminate or moderate the identified hazards. Develop written reports on the results, follow-up on the actions taken by the unit relative to the plan, and prepare final reports as necessary.

Prepare plans and reports as needed for federal and state regulatory compliance, public oversight bodies, and internal work groups. Present reports as needed. Coordinate and update existing plans across federal safety, local emergency preparedness, and property insurer requirements. Maintain and update plans as necessary for compliance, mitigations, and efficacy.

- 1 - **964**

Administer safety and security policies in coordination with City Risk Management personnel. Perform routine on-site observations for program adherence. Identify and work to eliminate hazards, and encourage safe working habits and environments. Identify needs and provision of on-site personal protective equipment for various programs.

Lead the implementation of Metro Transit's safety goals, rules, and/or programs. Deliver plans to applicable to units within the agency for implementation by unit managers. Work with managers to support safety and security initiatives. Develop, implement, and maintain standard operating procedures related to safety and security in coordination with unit managers and city staff as appropriate.

Monitor activities and lead or provide technical assistance on the development of safety and security manuals and procedures for tasks, equipment, facilities, certifications, and emergency response.

Prepare necessary communications to managers, employees, safety committees, etc. to provide information on safety and security issues.

Staff and lead regular safety and security programming during agency unit meetings. Staff and lead safety and security related committees and work groups. Actively administer safety related programming as defined by the collective bargaining agreement.

Participate in training on safety and security policies and procedures. Lead Metro's safety training needs and trouble shoot issues. Develop, coordinate and/or conduct safety and security related training and drills agency wide. Coordinate emergency responder training on Metro equipment.

Liaison with police department and school district regarding dangerous and illegal situations on the buses and/or transfer points. Train on Emergency Operations Center procedures to support operations staffing.

Support staff in responding to customer and employee complaints, questions, and concerns regarding safety and security incidents. Coordinate with the Transit Operations Chief or designee on appropriate administrative and emergency actions when necessary and/or refer for resolution.

Monitor the scheduling of security patrols at facilities. Learn safety and security monitoring systems throughout Metro facilities. Manage Transit Exclusion policy.

Serve as a technical resource to evaluate the purchase of safety and security equipment for consideration in budgeting processes.

Coordinate annual safety and employee recognition events. Represent Metro Transit on the Transit Mutual Insurance Board.

Perform related work as required.

QUALIFICATIONS

Knowledge, Skills and Abilities:

Thorough knowledge of safety program administration principles, practices, equipment, and resources. Thorough knowledge of Federal, State and local laws, code, regulations relating to safety. Thorough knowledge of and ability to use computer software applicable to the duties of the position. Working knowledge of specific transit safety regulations. Ability to operate related testing equipment. Ability to identify unsafe working conditions, operating practices, or safety training needs and to develop effective recommendations and strategies for their resolution. Ability to create extensive written plans and reports and analyze data regarding safety statistics and other related information. Ability to evaluate plans, polices, trainings, and systems for compliance and effectiveness ability to analyze raw data for reporting. Ability to establish effective working relationships with co-workers, other department/city staff, and the public. Ability to work effectively with multicultural populations. Ability to communicate effectively both verbally, in writing, and at public presentations. Ability to effectively investigate workplace safety concerns. Ability to recommend purchases of safety related equipment. Ability to conduct research into safety-related matters in order to develop effective recommendations. Ability to support recommendations with appropriate data. Ability to develop effective workplace safety policies and procedures. Ability to lead groups and present reports and technical material. Ability to maintain adequate attendance.

Training and Experience:

Generally, positions in this classification will require:

Four (4) years of responsible occupational safety experience, which included involvement in safety program development, plan writing, implementation, and monitoring; including hazard identification and resolution, with at least two years of that experience being in a transit specific setting, such as that which would be gained as a Transit Safety Coordinator 1. Such experience will normally have been gained after graduation from an Associate's degree program in Occupational Safety or closely related field. A Bachelor's Degree in Occupational Safety or a closely related field may be substituted for one (1) years of work experience. Other combinations of training and/or experience which can be demonstrated to result in the possession of the knowledge, skills, and abilities necessary to perform the duties of the position will also be considered.

Specific training and experience requirements will be established at the time of recruitment.

Special Requirements:

Ability to meet the transportation needs of the position.

Transit Safety Institute Certification within one (1) year.

Physical Requirements:

The employee will be expected to physically visit Metro worksites throughout the City in order to conduct safety studies. This may involve walking over various terrain, including hilly, muddy, and snowy terrain. Work may be performed in all weather conditions and may involve dangerous chemicals, heavy machinery, and other hazardous conditions. The incumbent must possess the physical strength, ability and acuity necessary to conduct safety studies and inspections.

- 3 -

Department/Division	Comp. Group	Range
Metro	44	7

Approved:		
	Erin Hillson	Date
	Human Resources Director	



- 4 **- 967**



File ID: 84339

City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84339

File Type: Resolution Status: Council New

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 07/08/2024

Final Action:

File Name: Reallocate the employee of position #3938 from a

Civil Technician 2 (CG16, R14) to the position of Engineering Program Specialist 1 (CG16, R17).

Title: Reallocate the employee of position #3938 from a Civil Technician 2 (CG16, R14)

to the position of Engineering Program Specialist 1 (CG16, R17).

Notes:

Sponsors: Director of Human Resources Effective Date:

Attachments: PB Memo TE Civil Tech 2.pdf, PD TS.pdf Enactment Number:

Author: Julie Trimbell, HR Analyst Hearing Date:

Entered by: kklafka@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Human Resource Department Action Text: Notes:	This Resolution was Ref		uction ard (7/17/24), Common Council (8/6	5/24)		

Text of Legislative File 84339

Fiscal Note

The proposed resolution reallocates an employee underfilled at a Civil Technician 2 to the position's budgeted level of Engineering Program Specialist 1. The annualized cost increase from the change is \$6,000 to \$10,000. Any additional costs in 2024 will be accommodated within Traffic Engineering's existing operating budget authority.

Title

Reallocate the employee of position #3938 from a Civil Technician 2 (CG16, R14) to the position of Engineering Program Specialist 1 (CG16, R17).

Body

Resolution reallocating the employee of position #3938, T. Stella, from a Civil Technician 2 (CG16, R14) to the budgeted position of Engineering Program Specialist 1 (CG16, R17) within

the Traffic Engineering Operating Budget, thereof.

TO: Personnel Board

FROM: Julie Trimbell, Human Resources

DATE: June 21, 2024

SUBJECT: Civil Technician 2 – Traffic Engineering (TE) Division

The Human Resources Department received a request from City Traffic Engineer Yang Tao and Computer Mapping/GIS Coordinator Eric Halvorson on April 4, 2024 to study the 1.0 FTE position #3938 of Engineering Program Specialist 1 (CG 16, Range 17), currently underfilled as a Civil Technician 2 (CG16, Range 14) by Tim Stella. Upon reviewing the updated position description, the current class specification and other related class specifications, and conducting interviews with both Tim and Eric, I recommend Tim be reallocated as an Engineering Program Specialist 1 in CG16, Range 17.

Tim was hired by the City in 2017 as a Civil Technician 1 and progressed up the career ladder to a Civil Technician 2 in 2020. The Civil Technician 2 class specification describes:

...technical support work performed in a professional engineering and public works construction environment. The work involves performing field and office tasks such as: technical drafting of plans, maps, drawings and layouts; developing and/or using computer applications; making routine engineering computations; performing construction inspection activities; and performing other related field and office work. The work is performed under the direct supervision of higher level technical or professional staff. [emphasis added]

Previously the position's main responsibility (approximately 75% of workload) was site plan reviews. Approximately two years ago, the responsibilities changed to include coordination of the Pavement Marking Program and the Parking Lot and Commercial Access Program (approximately 85% of workload). The Pavement Marking Program responsibilities include annually evaluating pavement markings throughout the city, planning and prioritizing remarking, determining appropriate marking type, coordinating with TE shop staff and external contractors, developing work plans in AutoCAD, obtaining bids, developing the budget schedule and monitoring the budget, inspecting and evaluating work performed, and coordinating billings.

The Parking Lot and Commercial Access Program involves conducting and coordinating the review of site plans, commercial driveway access and parking lots submitted by private developers. The work includes reviewing plans, adding comments for minor modifications, coordinating and problem solving with a Planning Engineer, other City agencies, architectural firms and civil engineering companies to ensure projects align with ordinances and TE policies and best practices, collecting fees and deposits, verification and final sign off on all site plan review projects for TE, and serving as back up to the Traffic Engineer 2 on the Development Assistance Team to share comments and address developer questions. This program was previously coordinated by an

Engineering Program Specialist who retired. Tim is coordinating both programs and requires minimal supervision in performing related tasks unless unique situations arise.

The coordination of such programs aligns with the tasks performed within the Engineering Program Specialist series. The Engineering Program Specialist 1 describes:

...responsible program coordination, project, and specialized field work in a professional engineering environment. The work involves direct responsibility for specialized programs, projects, and activities relating to the support of engineering staff and projects. Positions at this level perform technical and quasi-professional functions such as conducting field surveys and evaluations, preparing plans and specifications, and coordinating regulated facilities/activities (e.g., underground storage tanks, waste oil collection, landfill monitoring, marking program, crash program, etc.). Under the limited supervision of a professional engineer or manager positions at this level function independently in area(s) of responsibility. The work may involve leading other staff on a temporary or permanent basis. A Program Specialist 1 level may have in-depth responsibility for at least one program in its entirety, with general responsibility for other programs. [emphasis added]

Whereas the Engineering Program Specialist 2 describes:

...responsible program coordination/administration and project work in a professional engineering environment. The work involves direct responsibility for a wide range of specialized programs, projects, and activities relating to the support of engineering staff and projects. Positions at this level perform technical and quasi-professional functions such as plan preparation and/or review, preparation of on-going and periodic studies and technical reports, communication with contractors and the public, policy and plan interpretation, and field inspection of projects and program activities. Under the general supervision of a professional engineer or manager, positions at this level function independently, serve as experts in specialized areas, and recommend policy and/or procedure changes as appropriate. Positions at this level also coordinate responses and represent the department to the Public and other departments, and work directly with multiple sections within a department. The work may involve leading other staff on a temporary or permanent basis. [emphasis added]

An Engineering Program Specialist 2 is responsible for coordinating a wide range of programs and serving as an expert in those specialized areas, as well as the following:

- Recommend system, policy, and/or procedural changes to resolve the issue or problem.
 Communicate with affected and/or interested parties to explain the process and the results. Tim's involvement in recommending such changes is minimal at this time.
- Lead on completion of research and special studies formulating into a response that
 addresses more complex issues requested by professional staff for department needs.
 Make assignments, research historical and current data files from all pertinent sources, and
 arrange data collection as necessary. Compile and analyze resultant information and
 prepare reports and recommendations or contribute to the work being done by other
 professional staff. Lead on program changes and specialized on data recordkeeping and
 best practices. Tim does conduct manual traffic data studies in the spring and fall, but does

- not serve as a lead on studies, research data, formulate responses to complex issues, or prepare reports and recommendations related to the data.
- Assist the Traffic Engineers with complaints and requests for information for Traffic
 Engineering facilities, neighborhoods and general Public. Handle complaints, questions and
 problems from all sources; investigate problems, conduct or oversee the conducting of
 detailed field and records studies to determine the actual nature and severity of the
 problem(s). Tim responds to the public on occasion, but does not address much in terms of
 complaints or conduct studies to determine the nature and severity of problems. Tim does
 coordinate and problem solve with others and addresses developer questions at the
 Development Assistance Team meetings, but in a backup capacity to the Traffic Engineer 3.

The program coordination responsibilities in this position are clearly beyond the scope of a Civil Technician 2. Tim is managing two programs, but at this time he is not performing some of the higher level areas of the Engineering Program Specialist 2 classification such as: recommending and leading system, policy and procedural changes; leading research and special studies and responding to complex issues; preparing reports and recommendations; or handling complaints, questions and problems, investigating problems, and conducting or overseeing the conducting of detailed field and records studies to determine the actual nature and severity of the problem(s).

Based on the current responsibilities assigned to this position, it is best classified as an Engineering Program Specialist 1 and therefore I recommend reallocating the employee as an Engineering Program Specialist 1 in CG16, Range 17, within the Traffic Engineering Division budget.

The necessary resolution to implement this recommendation has been drafted.

Editor's Note:

Effective Date: 1/21/2024

Compensation	2024 Annual	2024 Annual	2024 Annual
Group/Range	Minimum	Maximum	Maximum
	(Step 1)	(Step 5)	+12% longevity
16/14	\$63,997	\$72,036	\$80,681
16/17	\$69,197	\$79,847	\$89,428

cc: Tom Lynch – Director of Transportation
Yang Tao - City Traffic Engineer
Eric Halvorson - Computer Mapping/GIS Coordinator
Richard Marx, Local 6000 Representative

CITY OF MADISON POSITION DESCRIPTION

1. Name of Employee (or "vacant"):

Tim Stella

Work Phone: 266-6525

2. Class Title (i.e. payroll title):

Civil Tech 2

3. Working Title (if any):

N/A

4. Name & Class of First-Line Supervisor:

Eric Halvorson

Work Phone: 266-6527

5. Department, Division & Section:

Transportation, Traffic Engineering

6. Work Address:

215 Martin Luther King Jr Blvd Suite 109

7. Hours/Week: 38.75

Start time: 7:00 a.m. End time: 3:30 p.m.

8. Date of hire in this position:

6-19-17

9. From approximately what date has employee performed the work currently assigned:

July 2022

10. Position Summary:

This is basic technical support work performed in a professional engineering and public works mapping and construction environment. The work involves performing field and office tasks such as computer drafting of maps, drawings and layouts, updates to the City's computerized record maps such as traffic signs, signals, condition diagrams and crash maps, including updates to the associated database tables, assisting in construction inspection and documentation, making field measurements, and performing other related tasks. The work is performed under the direct supervision of higher-level technical or professional staff and is reviewed in progress and upon completion. Perform more complex and/or comprehensive technical support functions. Provide leadership and guidance to others in the division.

11. Functions and Worker Activities: (Do NOT include duties done on an "Out-of-Class" basis.)

40% A. Pavement Marking Program

- Under limited supervision of the Assistant City Traffic Engineer and Traffic Engineering Operations Manager, administer, oversee and coordinate the citywide epoxy marking program. This includes the semi-permanent pavement marking program (included street, bike lane and pedestrian cross-walk markings).
- 2. Evaluate existing markings to determine needs remarking of using semi-permanent marking and other markings.
- 3. Determine a project and budget schedule for the year and coordinate with the Engineer Division, Traffic Engineering staff and TE-shop.
- 4. Design the pavement marking layout; prepare necessary resolutions, contracts and specifications; prepare plans and present project to the BWP and CC; meet with contractor to develop work schedule.
- Coordinate the development of street epoxy marking plans. Design the marking needed for streets to be constructed and maintenance during the year. Develop a plan using City, State and Federal standards and prepare drawings. Review work as it is performed and determines if additional marking is needed.
- 6. Conduct or coordinate final inspection of the contractors' epoxy pavement and marking works in the city. These include coordinating and assisting contractors on both in house questions and in the field layout. Assist contractors when needed by phone or in person. Work outside of the City's normal working hours and make decision and determine the best appropriate way for installation and/or marking.
- 7. Coordinate pavement & markings billings; verify and make changes if needed; approval final quantities and create LPO; submit to Administrator for making payments.

45% B. Parking Lot, Site Plan Review and Driveway Permits

- 1. Under limited supervision of the Assistant City Traffic Engineer and TE Development Specialist, assist with administering the parking lot and access program.
- 2. Conduct and coordinate the review of site plans and commercial driveway access and parking lots.
- 3. Coordinate and problem solve with other city departments, architectural firms and civil engineering companies in reviewing private development sites to ensure each project aligns with all City ordinances, policy's and modern Traffic Engineering best practices to achieve the highest standards for safety and sustainability.

4.

- 5. and and achieve highest standards for the development to have a highly sustainable future.
- 5% C. Provide mapping and graphic products for City Traffic staff, other governmental agencies and the general public
 - 1. Print maps of Traffic assets from CAD or GIS programs
 - 2. Create poster board prints for public meetings
 - 3. Print condition diagrams for public as requested
 - 4. Other tasks as assigned
- 5% D. Assist in providing technical support and training to City Traffic staff relating to CAD hardware/software and GIS mapping hardware/software
 - 1. Assist users with Arc products
 - 2. Assist users with MicroStation
 - 3. Print condition diagrams for public as requested
 - 4. Other tasks as assigned
- 5% E. Perform field related duties as necessary in support of mapping and record conversion duties and other related work as assigned
 - 1. Inventory existing signs for construction projects
 - 2. Perform site distance studies
 - 3. Conduct traffic count studies
 - 4. Collect field data as requested
 - 5. Other tasks as assigned

12. Primary knowledge, skills and abilities required:

> Working knowledge of the methods and techniques for inspecting public works construction projects. Knowledge of drafting techniques, materials and tools, including basic familiarity with computer assisted drafting (CAD). Knowledge of standard engineering nomenclature and mathematics including geometry and trigonometry. Knowledge of surveying techniques and equipment. Ability to make neat and legible technical drawings. Ability to perform mathematical computations quickly and accurately. Ability to read detailed maps, plans and specifications. Ability to follow instructions and established procedures. Ability to establish and maintain effective working relationships with internal staff and with persons in other agencies, contractors and the public. Ability to perform fieldwork including observations, measurements, data collections, survey crew work, and construction inspection activities. Ability to maintain adequate attendance.

13. Special tools and equipment required:

Major mapping and GIS systems including plotters, printers, scanners, CD recorders, etc. will be provided

14. Required licenses and/or registration:

Possession of or the ability to obtain a valid Wisconsin driver's license

15. Physical requirements:

> The ability to stand for long periods of time, access and walk around an active construction site, withstand adverse weather extremes (cold, hot, rain, or sun), tolerate loud noises, dust and pollen. Ability to operate a CAD workstation with hand and eye coordination using a mouse and keyboard for inputting data.

16. Supervision received (level and type):

Traffic Engineering Development Specialist

17. Leadership Responsibilities:

This po	sition:		has no leadership responsib	ory activities (Supervisory Analysis Form attached). ibility. p (please provide detail under Function Statement).
Employ	ee Ackr	owledg	ment:	
	I have	oeen pr	form and believe that it accur ovided with this description of ts (see attached).	urately describes my position. of my assignment by my supervisor.
EMPLOY	ΈE			DATE

18.

19.	Supe	rvisor Statement:
		I have prepared this form and believe that it accurately describes this position. I have reviewed this form, as prepared by the employee, and believe that it accurately describes this position.
		I have reviewed this form, as prepared by the employee, and find that it differs from my assessment of the position. I have discussed these concerns with the employee and provided them with my written comments (which are attached).
		I do <u>not</u> believe that the document should be used as the official description of this position (i.e., for purposes of official decisions).
		Other comments (see attached).
	SUPER	RVISOR DATE

Instructions and additional forms are available from the Human Resources Dept., Room 261, Madison Municipal Bldg., calling 266-4615 or visiting cityofmadison.com/employeenet/policies-procedures/position-descriptions.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84340

File ID: 84340 File Type: Resolution Status: Council New

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 07/08/2024

Final Action:

File Name: Awarding Public Works Contract 9502, Warner Park

Community Recreation Center Expansion (12th AD) and amending the 2024 Adopted Capital Budgets for Parks Division and Engineering-Facilities Divisions to

complete the project.

Title: Awarding Public Works Contract 9502, Warner Park Community Recreation Center Expansion (District 12) and amending the 2024 Adopted Capital Budgets for Parks Division and Engineering-Facilities Divisions to complete the project

(District 12)

Notes: Amy Scanlon

Sponsors: Amani Latimer Burris And Charles Myadze Effective Date:

Attachments: 9502 BidOpeningTab.pdf, 9502 award.pdf Enactment Number:

Author: Jim Wolfe, City Engineer Hearing Date:

Entered by: hfleegel@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	1 Engineering Division 07/08/2024		Referred for Introduction				
	Action Text:	This Resolution was Re	ferred for Introdu	ction			
	Notes:	Finance Committee (7/22/2	4), Board of Public	Works (7/24/24), Common Cou	ncil (8/6/24)		

Text of Legislative File 84340

Fiscal Note

The proposed resolution authorizes the award of Public Works Contract 9502 Warner Park Community Recreation Center Expansion (including alternates 1 and 2) and amends the 2024 Adopted Capital Budgets for Parks Division and Engineering-Facilities Divisions. The budget amendment transfers existing GO Borrowing authorization from other Parks Division projects to the Warner Park Community Recreation Center Expansion project (Munis 17196).

To complete the project the resolution authorizes \$1,354,300 in transfers from the following MUNIS projects to the Parks Division Warner Park Community Recreation Center Expansion

project (Munis 17196).

Project Number	Agency Budget	<u>Item</u> <u>An</u>	<u>nount</u>
13937	Parks Division	Country Grove Park Shelter	\$600,000
14729	Engineering Division	Forest Hill Cemetery Office	\$500,000
12860	Parks Division	2020 WPCRC Improvements	\$37,100
12799	Parks Division	Warner Park Beach House	\$6,300
14014	Parks Division	Warner Park Ped Bridge	\$13,000
14595	Parks Division	Warner Park Overflow Lot	\$60,600
14603	Parks Division	2023 WPCRC Exercise Equipm	ent \$17,300
14011	Parks Division	Tenney Clean Beach System	\$120,000

Total Transfers \$1,354,300

Title

Awarding Public Works Contract 9502, Warner Park Community Recreation Center Expansion (District 12) and amending the 2024 Adopted Capital Budgets for Parks Division and Engineering-Facilities Divisions to complete the project (District 12)

Body

WHEREAS, the Common Council has approved the development of an expansion of the Warner Park Community Recreation Center and has authorized the use of approximately \$9,000,000 of combined City and Federal resources to support the design and construction of that facility; and,

WHEREAS, after the project team charged with leading the City's effort completed a robust series of design phases, all of which included a professionally developed construction cost estimate, the Council, on April 16, 2024, approved plans and specifications and authorized the Board of Public Works to advertise and receive bids for Public Works contract 9502, Warner Park Community Recreation Center Expansion (RES - 24-00255) which were to include a base construction bid and, separately two Alternate components; and,

WHEREAS, Alternate 1 involves installing a clean photovoltaic system for the building, Alternate 2 involves expanding the building's geothermal field to support the heating load for the entire facility; and,

WHEREAS, the A/E consultant team completed construction documents for Public Works bidding and received a final construction cost estimate of \$6,264,184 (plus the standard 8% construction contingency of \$501,135); and,

WHEREAS, after advertising the project from 05/17/2024 to 06/27/2024, five competitive general contractor base bids were received with an apparent low base bid of \$8,457,300 (plus an 8% construction contingency of \$676,584); and,

WHEREAS, Alternate 1 would add \$278,700 (plus an 8% contingency of \$22,296) to the base bid, and would be funded by Engineering Energy Improvements [Munis 15347]; Alternate 2 would add \$333,900 (plus an 8% contingency of \$26,712) to the base bid and would be funded by the project budget [Munis 17196]; and,

WHEREAS, the total bid, including only the added cost associated with Alternate 2, plus an 8% contingency of \$703,296 comes to \$8,791,200, an amount approximately \$1,250,000 over the project's maximum construction budget; and,

WHEREAS, the remaining \$104,300 in transfers will be utilized for percent for art, staff

construction administration time, furniture/fixtures/equipment, and remaining miscellaneous project costs; and

WHEREAS, Engineering staff have concluded that rebidding the Warner Park Community Recreation Center Expansion project would likely not result in significantly lower costs, but would delay completion of the project and generally have a negative impact on bidders who responded; and,

WHEREAS, the Country Grove Shelter project is underway and has been awarded under authorized budget, and several Warner Park-related projects are completed and have remaining unused funding due to the costs being under initial budget estimates; and,

WHEREAS, the Forest Hill Cemetery Office Improvement project and the Tenney Clean Beach System project will be deprioritized, and funding identified for these projects will be made available to the Warner Park Community Recreation Center Expansion project; and,

NOW,THEREFORE, BE IT RESOLVED, that the Common Council accepts the following bids for miscellaneous improvements and authorizes the Mayor and City Clerk to enter into a contract with the low bidder for the Warner Park Community Recreation Center Expansion project, as determined by the base bids contained herein, and proceed with a contract for work identified as part of the base bid and additive alternates #1 (solar PV) and #2 (Heat Recovery System), subject to the Contractor's compliance with Section 39.02 of the Madison General Ordinances concerning compliance with the Affirmative Action provisions and subject to the Contractor's compliance with Section 33.07 of the Madison General Ordinances regarding Best Value Contracting; and,

BE IT FURTHER RESOLVED, that the Parks Division 2024 Adopted Capital Budget be amended by \$1,354,300 through the transfer of existing budget authority from park projects in the Engineering - Facility Improvements and Parks Division Budgets 2024 Capital Budgets to the Warner Park Community Recreation Center Expansion project (Munis 17196), and to allocate \$300,996 of 2024 Energy Improvement funds (Munis 15347) to the project; and,

BE IT STILL FURTHER RESOLVED, that the Common Council directs staff to continue to review the project for grant opportunities such as Inflation Reduction Act (IRA) direct payments and Focus on Energy incentives that can offset other borrowing sources; and,

BE IT FINALLY RESOLVED, that the funds be encumbered to cover the costs of the projects contained herein.

WARNER PARK COMMUNITY RECREATION CENTER EXPANSION CONTRACT NO. 9502 DATE: 6/27/24

PREQUALIFICATION: 425

		BASE	ALTERNATE	PREQUAL
CONTRACTORS	TOTAL BID	BID	TOTAL	STATUS
Engineering Estimate &				
Construction Budget Dollar Value	\$6,265,000.00			
Vogel Bros. Building Co.	\$9,069,900.00	\$8,457,300.00	\$612,600.00	OK
Riley Construction Company, Inc.	\$9,095,630.00	\$8,550,000.00	\$545,630.00	ок
Tri-North Builders, Inc.	\$9,292,710.00	\$8,723,578.00	\$569,132.00	OK
Joe Daniels Construction Co., Inc.	\$9,400,990.00	\$8,859,690.00	\$541,300.00	OK
J. H. Findorff & Son Inc.	\$9,485,597.00	\$8,885,380.00	\$600,217.00	OK

CONTRACT NO. 9502 Warner Park Community Recreation Center Expansion Vogel Bros. Building Co.

CONTRACT AWARD MAX CONTINGENCY (8%)	\$ 9,069,900.00 \$ 725,592.00
17196-51-140: 53310 (90924)	8,791,200.00
ACCT. CONTINGENCY 8%	703,296.00
ACCT. SUBTOTAL	9,494,496.00
15347-401-140: 54210 (90924)	278,700.00
ACCT. CONTINGENCY 8%	22,296.00
ACCT. SUBTOTAL	300,996.00
GRAND TOTAL INCL. CONTINGENCY	\$ 9,795,492.00



File ID: 84351

City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84351

File Type: Resolution Status: Council New

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 07/09/2024

Final Action:

File Name: Submitting the appointment of Eric M. Knepp for

confirmation of a five-year term as Parks

Superintendent.

Title: Submitting the appointment of Eric M. Knepp for confirmation of a five-year term

as Parks Superintendent.

Notes:

Sponsors: Satya V. Rhodes-Conway Effective Date:

Attachments: Knepp Contract - Final.pdf Enactment Number:

Author: Erin Hillson, Director of Human Resources Hearing Date:

Entered by: kklafka@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Human Resource Department Action Text: Notes:	This Resolution was Ref					

Text of Legislative File 84351

Fiscal Note

As stated in the Employment Agreement between the City of Madison and Eric M. Knepp, the Park's Superintendent's initial year's salary will be based on a total annualized rate of \$174,020 effective August 6, 2024. The Parks Superintendent position is approved and funded in the 2024 Adopted Operating Budget and is included in the 2025 Parks Division Operating Budget request. Annual salary adjustments may be made during the term of the agreement at the Mayor's discretion, subject to Common Council approval.

Title

Submitting the appointment of Eric M. Knepp for confirmation of a five-year term as Parks Superintendent.

Body

WHEREAS, the Mayor has appointed Eric M. Knepp to the position of Parks Superintendent and has recommended to the Common Council that their appointment be confirmed; and,

WHEREAS, the parties have reached an accord on the terms and conditions of an Employment Agreement between the City and Eric M. Knepp;

NOW THEREFORE, be resolved that Eric M. Knepp's appointment to the position of Parks Superintendent is hereby confirmed, and the Mayor and the City Clerk's Office are authorized to execute an Employment Agreement between the City and Eric M. Knepp.

EMPLOYMENT AGREEMENT BETWEEN THE CITY OF MADISON AND ERIC M. KNEPP

This Agreement made this August 6, 2024, by and between the City of Madison, a municipal corporation of Dane County, Wisconsin (hereafter, the "City") and Eric M. Knepp, a natural person (hereafter, the "Superintendent").

WITNESSETH;

WHEREAS, the City desires to hire the Superintendent as an employee of the City of Madison to perform the services described herein on its sole behalf as the Parks Superintendent, and

WHEREAS, the Superintendent possesses the necessary knowledge, skill, abilities and experience to perform such services and is willing to perform such services as the Parks Superintendent, and

WHEREAS, the Superintendent has been duly selected and has been confirmed for appointment to the position of Parks Superintendent by the Common Council of the City of Madison on August 6, and

WHEREAS, the Common Council of the City has authorized the execution of the Agreement by Resolution No. RES_____.

NOW, THEREFORE, in consideration of the mutual covenants, terms, and agreements contained in this document, the receipt and sufficiency of which is mutually acknowledged, the parties agree as follows:

I. PARKS SUPERINTENDENT HIRED

Eric Knepp is hired as a non-civil service employee of the City, holding the position of Parks Superintendent pursuant to the terms, conditions and provisions of this Agreement. The Superintendent shall have and exercise full authority and discretion as a Division Head within the City's organizational structure and act as Appointing Authority for employees of the Parks in accordance with all appropriate City Ordinances and Mayor's Administrative Procedure Memoranda.

II. FUNCTIONS, DUTIES AND RESPONSIBILITIES OF THE PARKS SUPERINTENDENT

A. <u>General Responsibilities</u>:

The principal function of the Parks Superintendent shall be the management of the Parks Division.

This is responsible managerial, professional and administrative work in planning, organizing and directing the programs, operations, services and staff of the Parks Division. The Parks Division encompasses: Parks and Athletics Maintenance, State Street Mall Maintenance, Aquatics, Street Use and Event Planning, Parks Planning, Conservation, Construction, Olbrich Botanical Gardens, and the Forest Hill Cemetery. The Parks Superintendent directs and manages related programs and facilities including diverse urban park areas, the Goodman Pool, Warner Park Community Recreation Center, Breese Stevens Stadium, Warner Park Baseball Stadium, City golf courses, beaches, boat launch areas, recreation programs, volunteer services, use agreements and the utilization of all other parks and parks facilities. Under the policy direction of the Mayor, the Parks Superintendent must exercise considerable judgment and discretion in meeting program objectives.

B. <u>Examples of Duties and Responsibilities</u>:

This position is responsible for planning, organizing, directing and controlling the programs, services and activities of the Parks Division, both directly and through subordinate supervisors. Duties and responsibilities include:

- Manage, plan, organize and direct the programs, operations, services and staff of the Parks Division. Develop program objectives and priorities in accordance with overall City and Parks commission policies relating to development, utilization and maintenance of City parks and facilities. Manage related special events (e.g., Shake the Lake, Movie Night Series, Ride the Drive). Develop, manage and maintain urban park space.
- Oversee development and administration of diverse community-based recreational programming, some of which includes significant revenue considerations (e.g., the Goodman Pool, the City's golf courses, Warner Park Community Recreation Center, Olbrich Botanical Gardens, concession services and parks use fees and agreements). Collaborate with various community organizations to activate and program public spaces and parks for the betterment of the community. Maintain situational awareness of changing community interests in public space allocation and effectively implement changes to programs, policies, and land use of City parks and open spaces.
- Direct the fiscal management of the Parks Division, including overseeing the preparation, submission and administration of the agency's annual capital and operating budgets. Coordinate overall financial planning for the Parks Division, incorporating governmental and private funding in

concert with the Parks Foundation. Direct payroll, purchasing and related financial operations of the division. Oversee the development and administration of all fees and charges for enterprise operations (pool, golf courses, cemetery, concessions, Warner Park Community Recreation Center, Olbrich Botanical Gardens, recreation services, reservable shelters and buildings, park and athletic use and use agreements.). Oversee the annual special charges process for the Division, including the Mall Maintenance Special Charge districts. Oversee the development of policies and procedures for the effective implementation of Park Impact Fees and Land Dedication standards.

- Oversee and provide organizational direction with the Parks Management Team. Plan, direct and administer human resources programs for the division, both directly and through lower-level supervisors. Hire, train, supervise, evaluate and discipline staff.
- Oversee and monitor implementation of systematic maintenance programs of Parks Division lands, facilities and equipment to reduce operating costs and prolong property life and value. Oversee design, planning and construction of facilities and parks. Create and establish policies and procedures for building, facility and land operation and use that enhance environmental sustainability. Create and establish policies and procedures for managing multiple national and local landmark sites. Oversee and monitor the implementation of a systematic urban forestry program to promote a healthy, resilient, and sustainable urban forest.
- Serve as Executive Secretary to the Parks Commission. Manage and support related subcommittees, including the Parks Foundation Board, Warner Park Community Recreation Center, Golf, Habitat Stewardship, Facilities Programs and Fees, Long Range Planning, and Olbrich Botanical Society. Recommend and implement park policies and procedures. Collaborate with and provide guidance and project coordination to the Parks Foundation for fund raising of City-Sponsored projects.
- Plan and direct ongoing public relations and volunteer programs. Initiate Parks Division projects involving cooperative efforts between public and private organizations. Represent diverse Parks interests, programs, services and events in the media and with community groups. Effectively communicate the mission, goals and objectives of the Parks Division through public presentations and publications.

- Implement recommendations of the existing Park and Open Space Plan, and oversee updates and revisions to the plan. Work with local planning authorities to assess and integrate City park and related land use with larger regional plans. Communicate and coordinate the implementation of long-range plans in relation to Parks Division responsibilities and programs. Establish Division workplans to prioritize Division resources being allocated to address strategic goals of the Parks Division, Parks Commission, and City.
- Maintain situational awareness of the active political environment Citywide. Monitor state of practice and legislative changes in the park and recreation field, including those related to impact fees. Coordinate and collaborate with Alders, the Mayor's Office, and other City agencies on Parks related matters. Participate as a member of the Public Works Management Team and Mayor's Department and Division Head Management Team.
- Demonstrate a commitment to the City's racial equity and social justice initiatives (RESJI). Participate in and help lead city-wide and agency efforts toward implementing RESJI principles.
- Instill a culture of continuous learning and a commitment to ongoing initiatives involving performance excellence systems. Demonstrate and promote organizational values, in everyday work, to further the mission and vision of the City of Madison.
- Perform related work as required.
- C. The Superintendent agrees to perform such functions and duties at a professional level of competence and efficiency. The Superintendent shall abide by all requirements of the laws of the State of Wisconsin, and of the ordinances, resolutions, regulations, rules and practices of the City which exist at the time of execution of this Agreement or which may, hereafter, be enacted or amended by the State of Wisconsin or the City in the exercise of their lawful authority. In the event a provision of this Agreement conflicts with any City ordinance, resolution, regulation, rule or policy, the provision of the Agreement shall control, except that nothing herein shall be interpreted as modifying the obligations or terms Madison General Ordinance §3.35 (the Ethics Code).
- D. The Superintendent shall devote full time to the duties and responsibilities provided herein and shall engage in no pursuit that interferes with them. The Mayor, however, may approve the Superintendent's reasonable time away from the regular duties and responsibilities provided such time is approved

in advance and taken as vacation leave or absence without pay. Further, the Mayor may authorize other limited outside professional activities on City time provided that they are determined to be of benefit to the City and the Superintendent is not compensated for such activities. Nothing herein limits the Superintendent from performing outside services for compensation provided such outside services have been approved by the Mayor, are not done on City time, and otherwise comply with City ordinances and rules.

- E. The standard City workweek is 38.75 hours. However, the Superintendent shall have reasonable flexibility from this standard to accommodate additional time expended outside regular working hours required by attendance at meetings and the like. Such flexibility is not intended to provide or be used as additional vacation or other paid leave.
- F. The Superintendent shall have no right to make contracts or commitments for or on behalf of the City except as preauthorized by statute, ordinance or express written consent of the City.
- G. The Superintendent shall continue to reside within the City of Madison for the duration of this contract. As a condition of accepting this contract, the Superintendent agrees to waive any right to challenge this residency requirement, by court action or otherwise.

III. COMPENSATION AND BENEFITS

- A. The Superintendent's salary shall be based on an annualized rate of \$174,020 and shall be paid in approximately equal biweekly payments according to regular City payroll practices. Annual salary adjustments during the term of this agreement may be made at the Mayor's discretion, subject to approval of the Common Council, as provided in the City's established managerial pay plan. The Superintendent shall not be entitled to receive any additional overtime compensation, compensatory time off, or bonuses.
- B. The Superintendent shall, in addition to the compensation provided in Paragraph A above, and except as otherwise set forth in the Agreement, be entitled to the following benefits:
 - The Superintendent shall receive the same benefits as all other non-represented professional employees in Compensation Group 18 as may be provided and/or modified by the Madison General Ordinances, Resolution of the Common Council, Administrative Procedure Memoranda or other official City action throughout the duration of this agreement subject to paragraph II. (G) above.
 - 2. The Superintendent shall be entitled to twenty-seven (27) days of

vacation in each year of this Agreement. Credited but unused vacation in excess of ten (10) days may be carried forward to the succeeding year with the approval of the Human Resources Superintendent. Except as otherwise provided, the Superintendent shall be paid in full for credited but unused vacation existing at the expiration of this Agreement or upon the Superintendent's retirement, when qualified for receipt of Wisconsin Retirement Fund benefits. Beginning in 2024 the Superintendent accrues a balance of more than five weeks from the preceding year(s), the Superintendent may elect to convert up to twenty (20) days of their annual vacation to an amount of cash equivalent, calculated on their regular earnings. The Superintendent shall apply for such conversion option in accordance with City procedures, and such amount shall be paid in a manner determined by the City. Upon effectiveness of this agreement, the Superintendent shall receive a one-time cashout of accrued unused vacation totaling 387.50 hours.

- 3. Sick Leave: If the Superintendent leaves the position before the end of the contract period, the Superintendent shall be entitled to payment in full (100%) of any earned but unused sick leave accumulated during each of the fully completed contract period(s). The Superintendent shall be entitled to one-half (50%) of any earned but unused sick leave accumulated to the day the Superintendent terminated City employment during the contract period. If the City terminates the Superintendent's contract before the end of the contract period or the Superintendent leaves the position at the end of the contract period or the Superintendent retires and qualifies for WRS benefits, they shall be entitled to payment in full (100%) of any sick leave the Superintendent would have earned through the end of that year.
- 4. The Superintendent shall be eligible to participate at City expense in professional seminars, conferences, workshops and related meetings consistent with the role as Superintendent and in accordance with applicable Administrative Procedure Memoranda.
- 5. The Superintendent shall be reimbursed for relevant professional association and/or licensure dues.
- 6. The Superintendent shall be eligible for smart phone with data plan reimbursement up to seventy-five (75) dollars per month for City usage.
- IV. TERM: RENEWAL OPPORTUNITY; NON-RENEWAL

- A. This Agreement shall take effect on August 6, 2024, and shall expire on August 5, 2029, unless terminated sooner as provided herein. All salary and benefit changes shall apply the pay period following August 6, 2024.
- B. The Mayor, in their sole discretion, may offer renewal of this Agreement to the Superintendent. The Mayor shall notify the Superintendent of the intent to renew the Agreement at least ninety (90) calendar days before the expiration of this Agreement. Failure to so notify the Superintendent shall extend the term of this Agreement by the time of the delay in actual notification (but in no event for more than ninety (90) days) without change in the Superintendent's anniversary date, and shall not act as a full renewal of the Agreement. Renewal of the agreement and of its provisions shall be subject to the approval of the Common Council. In the event the Common Council does not renew this Agreement, this Agreement will remain in effect for ninety (90) days following the non-renewal action by the Common Council or five (5) years from the date of this Agreement, whichever is later.
- D. The Mayor, in their sole discretion, may elect not to offer renewal of this Agreement to the Superintendent. In such event, the Mayor shall notify the Superintendent of the intent not to renew the contract at least ninety (90) calendar days before the expiration of this Agreement. Failure to so notify shall extend the term of this Agreement by the time of the delay in actual notification (but in no event for more than ninety (90) days) and shall not act as a renewal of the Agreement. At the expiration of the Agreement, the parties' rights, duties, responsibilities and obligations shall end. However, the Superintendent will, at the sole discretion of the Mayor, be eligible to take a voluntary demotion into any vacant or newly created position for which the Superintendent is qualified.
- E. In the event of non-renewal of this Agreement, under either Paragraph D above, the Mayor may, in their sole discretion, terminate this Agreement at any earlier date within ninety (90) days of the expiration of this Agreement, as determined by the Mayor. The early termination is to be accomplished by (a) notifying the Superintendent of the date of early termination, and (b) committing to buy out the balance of this Agreement by paying the Superintendent the balance due under this Agreement in a lump sum, including salary and leave benefits (vacation, floating holiday, paid leave, sick leave) earned or to be earned through the original term of this Agreement, together with payment of the City's share of any health insurance premiums or the provision for such payment through the original term of this Agreement. The buy-out may be for the full period left on this Agreement, or any portion of the final ninety (90) days thereof. If this Agreement is terminated early through the provisions of this buy-out clause. the Superintendent's employment with the City ends as of the date of early termination.

V. PERSONNEL ACTIONS

The Superintendent is subject to the Mayor's supervision and is, during the term of this Agreement, subject to the Mayor's authority to impose discipline on or to discharge the Superintendent as is provided in Sec. 9 of the City of Madison Personnel Rules, or as may be renumbered or amended hereafter. The Superintendent shall be entitled to the procedural appeal and provisions contained in such subsection or as may be provided other non-represented employees at the time of imposition of suspension or discharge.

VI. CITY OBLIGATIONS AND RIGHTS

The City shall provide staff, equipment, supplies and space that it deems reasonable, in its sole discretion, for the conduct of the work of the Superintendent. The City retains the sole right to determine the organizational structure and overall functioning of the Parks.

VII. REOPENING THE AGREEMENT

Either party may request that the Agreement be reopened for renegotiation if or when the Superintendent's duties or responsibilities change significantly. A "significant" change in the Superintendent's duties is defined as that degree of change in duties and responsibilities that would qualify a civil service position for reclassification pursuant to standard City personnel practices.

Factors which may be considered include the addition or deletion of duties, changes in Department services or the addition or deletion of programs. If there is no agreement, the original Agreement shall control and shall not be reopened. Agreement changes, if any, and any resulting reclassification of the position shall not be deemed the creation of a new position so as to require competition.

VIII. LIABILITY PROTECTION

The City shall defend and indemnify the Superintendent against and for any and all demands, claims, suits, actions and legal proceedings brought against them in their official capacity or personally for acts performed within the scope of their employment to the extent and only to the extent authorized by the Wisconsin Statutes in effect at the time of the act complained of and as may be provided by any City insurance coverage for employees at such time.

IX. STATEMENT OF ECONOMIC INTERESTS

Pursuant to Madison General Ordinance §3.35 (the Ethics Code), the Superintendent shall file a Statement of Economic Interests with the City Clerk within 14 days of their appointment. Each person required to file a Statement of

Economic Interests shall annually file with the Clerk an updated Statement no later than April 30 of each year.

X. DOCUMENTS AND MATERIALS PROPERTY OF THE CITY

All of the documents, materials, files, reports, data and the like which the Superintendent prepares or receives while this Agreement is in effect are the sole property of the City of Madison. The Superintendent will not publish any such materials or use them for any research or publication without attribution to the City other than as work performed pursuant to the terms of this Agreement.

XI. APPEARANCE BEFORE ANY CITY ENTITY FOLLOWING SEPARATION FROM EMPLOYMENT

The Superintendent shall be subject to the provisions of Madison General Ordinance §3.35 (the Ethics Code).

XII. TERMINATION OF AGREEMENT

- A. The Superintendent may elect to terminate this Agreement before the expiration of the contract period. If the Superintendent provides less than forty-five (45) calendar days' notice in writing to the Mayor, the Superintendent forfeits all rights to the cash equivalent of any of the benefits enumerated in Section III. B. of the Agreement. If the Superintendent provides forty-five (45) calendar days' notice, or greater, in writing to the Mayor, the benefits enumerated in Section III. B. of the Agreement will be paid according to the terms of the Superintendent leaving during the contract period. These forfeiture provisions do not apply if the Superintendent retires from this position and qualifies for benefits under the Wisconsin Retirement System.
- B. The Superintendent's discharge (as provided for in section 9 of the City of Madison Personnel Rules) during the term of this Agreement shall be deemed a breach of material provision of the Agreement. In the event of a discharge or other breach of a material provision of the Agreement by the Superintendent, the Superintendent shall forfeit all compensation and benefits from the date of notification of the breach by the City. This action shall not impact the receipt of benefits earned during the total period of employment. In the event of an alleged breach of a material provision of this Agreement by either party, the concerned party shall notify the other party in writing within thirty (30) working days, which shall be followed by a meeting of the parties to resolve the alleged breach. In the event the issue is not resolved, the Superintendent or the City may pursue contract remedies.
- C. The City retains the right, in its sole discretion, to abolish the position of

Parks Superintendent or to reorganize as it deems in the best interest of the City. In the event the City abolishes the position of Parks Superintendent or reorganizes the Department to the extent that the position of Parks Superintendent is no longer required, this Agreement shall terminate and all rights, duties and obligations of the parties shall mutually end without recourse ninety (90) calendar days after final approval of such abolishment of position or reorganization by the Common Council, except as provided in Madison General Ordinance §3.35 (the Ethics Code). In such case, all benefits provided in renewal or non-renewal of the agreement apply.

XIII. NO ASSIGNMENT OR SUBCONTRACT

The Superintendent shall not assign or subcontract any interest or obligation under this Agreement.

XIV. AMENDMENT

This Agreement shall be amended only by written Addendum to Agreement of the parties approved and authorized for execution in the same fashion as this original Agreement.

XV. NO WAIVER

No failure to exercise and no delay in exercising any right, power or remedy on either party's part shall operate as a waiver thereof, nor shall any single or partial exercise of any right, power or remedy preclude any other or further exercise thereof, or the exercise of any other right, power, or remedy.

XVI. ENTIRE AGREEMENT

No agreements, oral or written, express or implied, have been made by either party hereto, except as expressly provided herein. All prior agreements and negotiations are superseded hereby. This Agreement and any duly executed addenda or amendments thereto constitute the entire Agreement between the parties hereto.

XVII. SEVERABILITY

In the event any provisions of this Agreement are determined by any court of law to be unconstitutional, illegal, or unenforceable, it is the intention of the parties that all other provisions of this Agreement shall remain in full force and effect.

XVIII. GOVERNING INTENT AND LAW

This Agreement shall be interpreted in the first instance in accordance with the spirit and intent of the Substitute Report of the Human Resources Committee

Report approved by the Common Council on August 2, 1988 and shall be controlled, construed and enforced in accordance with the laws of the State of Wisconsin.

IN WITNESS WHEREOF, the parties have executed this Agreement to be effective as of the day and year contained herein.

CITY OF MADISON A Municipal Corporation	
Witness	Satya Rhodes-Conway, Mayor
Witness	Maribeth L. Witzel-Behl, City Clerk
Witness	Eric M. Knepp, Parks Superintendent
APPROVED:	APPROVED AS TO FORM:
David P. Schmiedicke Finance Director	Michael Haas, City Attorney



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84358

File ID: 84358 File Type: Resolution Status: Council New

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 07/09/2024

File Name: 11540 - Owl Creek PSA assignment to KBJ Develop Final Action:

LLC

Title: Authorizing the assignment of an existing Purchase and Sale Agreement and previously allotted Affordable Housing Fund loan to KBJ Develop LLC, pertaining to a permanently affordable owner-occupied housing development project in the

Owl Creek neighborhood. (District 16)

Notes:

Sponsors: Jael Currie Effective Date:

Attachments: Enactment Number:

Author:Andy Miller, Real Estate SpecialistHearing Date:Entered by:cklawiter@cityofmadison.comPublished Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	1 Economic Development 07/09/2024 Division		Referred for Introduction				
	Action Text:	This Resolution was Ref	erred for Introdu	ction			
Notes: Finance Committee (7/22/24), Common Council (8/6/24)				cil (8/6/24)			

Text of Legislative File 84358

Fiscal Note

Fiscal note pending.

Title

Authorizing the assignment of an existing Purchase and Sale Agreement and previously allotted Affordable Housing Fund loan to KBJ Develop LLC, pertaining to a permanently affordable owner-occupied housing development project in the Owl Creek neighborhood. (District 16)

Body

WHEREAS, on December 6, 2022, the City of Madison ("City") Common Council authorized the allocation of up to \$600,000 from the City's Affordable Housing Fund ("Funding") to Kaba-Baal LLC ("Buyer") to support the development of four permanently affordable owner-occupied

homes in the Owl Creek neighborhood ("Project"), as further detailed in File ID No. 74456, Resolution Enactment No. RES-22-00824; and

WHEREAS, to further support the Project, on July 11, 2023, the City authorized a Purchase and Sale Agreement ("PSA") containing the terms and conditions of the sale of one vacant City-owned residential lot to the Buyer, as detailed in File ID No. 78332, Resolution Enactment No. RES-23-00489; and

WHEREAS, in the time since the above resolutions were authorized, the Buyer has created a new limited liability company, KBJ Develop LLC ("KBJ"), under which they desire to develop the Project; and

WHEREAS, to accommodate this request and allow the Project to proceed, City Common Council approval is necessary to authorize the assignment of the PSA to KBJ, as well as to authorize KBJ to receive the Funding.

NOW, THEREFORE, BE IT RESOLVED, that the City's Common Council hereby authorizes the City's execution of an amendment to the PSA for the purpose of assigning the Buyer's interests in the PSA to KBJ Develop LLC, a Wisconsin limited liability company; and

BE IT FURTHER RESOLVED, that the City's Common Council hereby authorizes the City to provide the Funding to KBJ Develop LLC, and authorizes the Mayor and City Clerk to execute, deliver, publish, file and record such other documents, instruments, notices and records, and take such other actions as shall be deemed necessary or desirable to accomplish the purpose of this resolution, on a form and in a manner that has been approved by the City Attorney.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84359

File ID: 84359 File Type: Resolution Status: Council New

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 07/09/2024

File Name: Naming Justin Festge-Russell as Madison's 2024-25 Final Action:

Youth Poet Laureate.

Title: Naming Justin Festge-Russell as Madison's 2024-25 Youth Poet Laureate.

Notes:

Sponsors: Marsha A. Rummel Effective Date:

Attachments: Enactment Number:

Author: Karin Wolf Hearing Date:

Entered by: kwolf@cityofmadison.com Published Date:

History of Legislative File

 Ver- Acting Body:
 Date:
 Action:
 Sent To:
 Due Date:
 Return
 Result:

 sion:
 Date:

 Department of Planning and Community and 07/09/2024 Referred for Introduction

Economic Development

Action Text: This Resolution was Referred for Introduction

Notes: Common Council (8/6/24)

Text of Legislative File 84359

Fiscal Note

Fiscal note pending.

Title

Naming Justin Festge-Russell as Madison's 2024-25 Youth Poet Laureate.

Body

WHEREAS, poetry promotes dialogue and understanding between generations; and

WHEREAS, poetry promotes civic responsibility, civic engagement, empathy, understanding, and mutual connection; and

WHEREAS, on December 13, 2021, the Madison Common Council passed Resolution No. 68286, RES-21-00343, establishing a Youth Poet Laureate Program; and

WHEREAS, on May 7, 2024, the Madison Arts Commission issued a call for the 2024-25 Madison Youth Poet Laureate with a deadline of June 7, 2024; and

WHEREAS, at their June 11, 2024 meeting, the Madison Arts Commission, in consultation with Madison Poet Laureate Steven Dawson, met to vet Youth Poet Laureate applications; and

WHEREAS, after careful consideration the Madison Arts Commission recommended that Justin Festge-Russell, Usin, Crystal Huang be named Finalists and Jusitn Festge-Russell serve as the 2024-25 Madison Youth Poet Laureate and Crystal Huang as the alternate.

NOW, THEREFORE, BE IT RESOLVED, that the City of Madison names Justing Festge-Russell to serve as Madison's 2024-25 Youth Poet Laureate, with Crystal Huang as alternate.

BE IT FURTHER RESOLVED, that Justin Festge-Russell will be invited to read a poem at the September 10, 2024 Common Council meeting.

BE IT FINALLY RESOLVED, that Justin Festge-Russell shall be known as the "Madison Youth Poet Laureate" from September 10, 2024 through August 31, 2025.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84360

			riie Nullibe	91. 04300					
	File ID:	84360	File Type: Res	solution	Status:	Council Ne Business	W		
	Version:	1	Reference:		Controlling Body:	COMMON COUNCIL			
					File Created Date :	07/09/2024	ļ		
_	File Name:	Authorizing General Obliq 2024	gation Promissory N	otes	Final Action:				
	Title: BY TITLE ONLY: Authorizing the Issuance and Establishing the Parameters for the Sale of Not to Exceed \$[] General Obligation Promissory Notes, of the City of Madison, Wisconsin, and Directing the Final Approval of the Terms Thereof.								
	Notes:	GenObligationPromNotes	s2024						
	Sponsors:	Satya V. Rhodes-Conwa	у		Effective Date:				
Δ	Attachments:			Enactment Number: Hearing Date: Published Date:					
	Author:	Matthew Robles							
	Entered by:	mglaeser@cityofmadisor	n.com						
isto	ory of Legisl	ative File							
/er- ion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:		
1	Attorney's Offi	ice 07/09/2024	Referred for						
	Action Text Notes			24)					
Te	ext of Legisla	ative File 84360							
	iscal Note iscal note pe	nding.							
B to W	Exceed \$[_	LY: Authorizing the Issu] General Obl d Directing the Final Ap	igation Promissory	Notes, of the C		Not			



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84373

File ID: 84373 File Type: Resolution Status: Council New

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 07/09/2024

File Name: Final Action:

Title: Authorizing City of Madison, on behalf of the Greater Madison MPO, to submit a grant application to the United States Department of Transportation for up to \$900,000 for the Greater Madison MPO Regional Safe Streets- Community Safety Enhancement Partnership project, amending the 2025 adopted operating budget, to accept grant funding, if awarded, and authorizing the Mayor and Clerk to sign the resulting grant agreement.

Notes:

Sponsors: Derek Field And Barbara Harrington-McKinney Effective Date:

Attachments: Enactment Number:

Author: Colleen Hoesly, Transportation Planner, Greater Madison N

Hearing Date:

Entered by: lheiser-ertel@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Department of Pl and Community a Economic Develo	and	Referred for Introduction				
	Action Text: This Resolution was Re		erred for Introduc	ction			
	Notes: Finance Committee (7/22/2		4), Common Counc	il (8/6/24)			

Text of Legislative File 84373

Fiscal Note

Fiscal note pending.

Title

Authorizing City of Madison, on behalf of the Greater Madison MPO, to submit a grant application to the United States Department of Transportation for up to \$900,000 for the Greater Madison MPO Regional Safe Streets- Community Safety Enhancement Partnership project, amending the 2025 adopted operating budget, to accept grant funding, if awarded, and authorizing the Mayor and Clerk to sign the resulting grant agreement.

Body

WHEREAS, the City of Madison Planning Division provides staffing for the Greater Madison

MPO (Metropolitan Planning Organization) and the city serves as the MPO's fiscal and administrative agent; and

WHEREAS, the Greater Madison MPO (Metropolitan Planning Organization) is the designated MPO for the Madison Metropolitan Area with responsibilities to perform metropolitan transportation planning and programming activities; and

WHEREAS, the Greater Madison MPO is interested in obtaining a Safe Streets and Roads for All Planning Grant from the USDOT for the purposes of assisting local agencies within the MPO's planning area to develop supplemental safety planning studies to support the MPO's Regional Safety Action Plan; and

WHEREAS, the grant will provide funding for the following roadway safety planning activities:

- 1) Up to \$300,000 to the City of Fitchburg to develop a Vision Zero Plan,
- 2) Up to \$150,000 to the City of Verona to develop a Safe Routes to School Plan,
- 3) Up to \$150,000 to the Village of Cottage Grove to develop an Active Transportation Plan.
- 4) Up to \$150,000 to the Village of Shorewood Hills to develop a Safety Action Plan,
- 5) Up to \$150,000 to Dane County to develop a Highway Safety Plan; and

WHEREAS, The Greater Madison MPO would be the designated recipient, and each participating local agency would be subrecipients, required to provide the 20% local match for their included project, and provide resolutions of support demonstrating financial commitment; and

WHEREAS, according to the fiscal note, the City and participating subrecipients have or will have the ability to meet the financial obligations of the cost-sharing requirements, and the Greater Madison MPO as the lead applicant and/or participating subrecipients have the resources and capacity to complete the required tasks associated with the grant; and WHEREAS, the grant application form includes legal terms, obligations, and/or commitments from the City, and/or requires a signature from a person who is authorized to bind the City to such things; and

WHEREAS, the Greater Madison MPO staff can attest to the validity and veracity of the statements and representations contained in the grant application; and

WHEREAS, if awarded, the granting agency may require the City to sign a contract, grant agreement, and/or other documents binding the City to certain obligations.

NOW, THEREFORE, BE IT RESOLVED, the City of Madison, as fiscal and administrative agent for the Greater Madison MPO, is authorized, on behalf of the City, to apply for the USDOT Safe Streets and Roads for All grant and sign all application materials, after any legal terms within the application are reviewed by the City Attorney; and

BE IT FURTHER RESOLVED, that, if awarded, the City is authorized to enter into an agreement with GRANTOR for the grant, and the Mayor and City Clerk are authorized to sign the agreement and related paperwork, in a format approved by the City Attorney; and BE IT FINALLY RESOLVED, that Greater Madison MPO staff are authorized to sign related paperwork or forms that are not part of the grant agreement or that are provided after the Mayor and Clerk sign the initial grant agreement, if required, after any legal terms included in those forms are reviewed by the City Attorney.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84374

File ID:84374File Type:ResolutionStatus:Council New

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 07/09/2024

File Name: Final Action:

Title: Authorizing a noncompetitive contract amendment with Electric Citizen LLC to

rebuild the Madison-area Out-of-School Time (MOST) website on the new City

website platform.

Notes:

Sponsors: Satya V. Rhodes-Conway And Yannette Figueroa Effective Date:

Cole

Attachments: Electric Citizen Non-Compete 07.2024.pdf Enactment Number:

Author: Hearing Date:

Entered by: alythjohan@cityofmadison.com Published Date:

History of Legislative File

Ver-	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return	Result:
sion:						Date:	

1 Department of Information 07/09/2024 Referred for Technology Introduction

Action Text: This Resolution was Referred for Introduction

Notes: Finance Committee (7/22/24), Common Council (8/6/24)

Text of Legislative File 84374

Fiscal Note

Fiscal note pending.

Title

Authorizing a noncompetitive contract amendment with Electric Citizen LLC to rebuild the Madison-area Out-of-School Time (MOST) website on the new City website platform.

Body

Background:

MOST is a citywide collaboration involving the City of Madison, the Madison Metropolitan School District, Dane County, United Way, the Madison Community Foundation and about 40 different youth serving community organizations that seeks to help close Madison's opportunity gap by ensuring that all children and youth in Madison have access to comprehensive, high-quality out-of-school time programs that contribute to positive youth development;

educational achievement; and career, college, and community readiness.

The MOST Program Finder is a web-based search tool created to enable families and youth to more easily find quality programs that are available before and after school, or during summer and holiday breaks for Madison-area children and youth. The tool currently operates on old website software and the system needs to be rebuilt. When re-built, the finder will include over 100 out-of-school time programs in the search tool.

Action:

WHEREAS, Electric Citizen LLC was selected via RFP # 10020-0-2021 and worked with the Information Technology (IT) Department to rebuild the City website on the new website platform; and

WHEREAS, the current project exceeded the funding required to migrate the Madison-area Out-of-School Time (MOST) website requiring Council authorization to execute a contract amendment in the amount of \$160,000 to complete this work; and

WHEREAS, the current website platform will go end-of-life in November of 2024; and

WHEREAS, under sec. 4.26(4)(b), of the Madison General Ordinances if the aggregate amount of the fee for services will be \$50,000 or more and the contract was not subject to a competitive bidding process, the contract shall meet one of the other requirements of sec. 4.26(4)(a) and be approved by the Common Council; and

WHEREAS, for the reasons stated in the attached Noncompetitive Selection Request Form, the proposed contract with Electric Citizen LLC meets the exception to the bidding process in section 4.26(4)(a)7. which states "A particular consultant has provided services to the City on a similar or continuing project in the recent past, and it would be economical to the City on the basis of time and money to retain the same consultant"; and

NOW THEREFORE BE IT RESOLVED that the Mayor and City Clerk are authorized to execute a services contract amendment not to exceed \$160,000.00 with Electric Citizen LLC for the above-described services.



CITY OF MADISON • FINANCE DEPARTMENT • PURCHASING SERVICES

Non-Competitive Selection Request

Date:	07/09/2024					
Requisition Number:	22000186 (8 characters)					
Requestor Name:	michelle gibson					
Requestor Phone Number:	608-261-9672					
Requestor Email:	mcgibson@cityofmadison.com					
Fund:	1100 GENERAL V					
Agency:	17 INFORMATION TECHNOLOGY					
Major:	53*** Supplies/Goods					
	541** Utilities					
	542** Building/Facility Maintenance/Repair					
	☐ 543** Software/Equipment Maintenance/Repair					
	544** Public Works Maintenance/Repair					
	☐ 545** Training/HR-Related Services					
	✓ 546** Consulting/Professional Services					
	548** Grants/Loans/Insurance/Other Services					
Total Purchase Amount:	\$155,680.00					
Vendor Name:	Electric Citizen LLC					
Product/Service Description:	Professional Services for Drupal Upgrade Project (City Website)					
	\$50,000 and UNDER This form will be sent to the Purchasing Supervisor for review.					
	OVER \$50,000 Complete this form and draft a resolution using the sample resolutions provided by the City Attorney to your Budget Analyst. Your resolution will not be added to the Finance Committee agenda without this form.					
Check the box(es) for the except	ion criteria you feel are applicable:					
Public exigency (eme processes.	rgency) will not permit the delay incident to advertising or other competitive					
·	The services or goods required are available from only one person or firm (i.e., true sole source).					
3. The services are for p	 The services are for professional services to be provided by attorneys. 					
4. The services are to be	. The services are to be rendered by a university, college, or other educational institution.					
	. No acceptable bids have been received after formal advertising.					
	plished by law or professional code.					
<u></u>						

✓	7.	A particular consultant has provided services to the City on a similar or continuing project in the recent past, and it would be economical to the City on the basis of time and money to retain the same consultant.
	8.	Otherwise authorized by law, rule, resolution, or regulation. Explain:
	-	rocurement is being paid with Federal or State grant funds, the vendor was identified by name in the roved Grant Application. (OPTIONAL)
REAS	SON	FOR REQUEST
Provid servic uniqu detail	de de e can e per the u	MPETITIVE SELECTION PROCESS CANNOT BE USED: tailed explanation below. For a true sole source, provide all information to explain why this product or a only be purchased from this vendor. For one-of-a-kind items not sold through distributors, explain the formance features of the product requested that are not available from any other product. For services, unique qualifications this vendor possesses, or other reason(s) that meet the criteria selected above. ecific, measurable factors and qualifications.
Tech	nolo	Citizen LLC was selected via RFP # 10020-0-2021 and worked with the Information gy (IT) Department to rebuild the City website on the new website platform; current website platform will go end-of-life in November 2024.
rece	nt pa	ultant has provided services to the City on a similar or continuing project in the ast, and it would be economical to the City on the basis of time and money to retain consultant
CON	IME	NTS REGARDING PURCHASES OVER \$50,000
		izen was selected through an RFP process in 2021, and has been paid \$289,864.50 for this his contract is the only one between the City and this vendor.
Date:	07/	/11/2024
Date:	07/	/11/2024 Submit



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84375

File ID:84375File Type:ResolutionStatus:Council New

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 07/09/2024

File Name: Final Action:

Title: Authorizing the Mayor and City Clerk to enter into a 3-year, competitively selected

Microsoft Enterprise Enrollment Agreement with Microsoft Corporation, and a 3-year, competitively selected services agreement with CDW Government, LLC, a

reseller of Microsoft and other software.

Notes:

Sponsors: Yannette Figueroa Cole And Satya V. Effective Date:

Rhodes-Conway

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: alythjohan@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Department of Information	07/09/2024	Referred for		_		

Technology Introduction

Action Text: This Resolution was Referred for Introduction

Notes: Finance Committee (7/22/24), Common Council (8/6/24)

Text of Legislative File 84375

Fiscal Note

Fiscal note pending.

Title

Authorizing the Mayor and City Clerk to enter into a 3-year, competitively selected Microsoft Enterprise Enrollment Agreement with Microsoft Corporation, and a 3-year, competitively selected services agreement with CDW Government, LLC, a reseller of Microsoft and other software.

Body

WHEREAS, the City has used Microsoft for Windows licenses and desktop, server and business tools for many years and traditionally purchases licenses and support through software resellers that have been selected by the State of Wisconsin through a competitive

selection process administered by the State; and,

WHEREAS, under MGO 4.26, the City's competitive purchasing requirements are satisfied by making purchases from a vendor that has been selected by the State; and,

WHEREAS, CDW Government, LLC (CDW-G) is one such vendor selected by the State as a reseller of Microsoft products and services and the state's contract includes a process for local government to participate; and,

WHEREAS, Information Technology Department continues to leverage the City's investment in the cloud based M365 infrastructure to advance the availability and the adoption of collaboration tools (email, instant messaging, collaboration tools, video, and voice conferencing) across the enterprise; and,

WHEREAS, this requires Enrollment Agreements for Microsoft subscriptions, through a contract with CDW-G, as well as signing a new agreement directly with Microsoft Corporation; and,

WHEREAS, APM 1-1 requires Common Council approval before the City can indemnify another party. The current Microsoft Universal Licensing Terms for Only Services includes the following clause:

"Customer agrees to defend, indemnify and hold Microsoft harmless from and against all damages, costs and attorneys' fees in connection with any claims arising from a High-Risk Use associated with the Online Services, including any claims based in strict liability or that Microsoft was negligent in designing or providing the Online Service(s) to Customer. The foregoing indemnification obligation is in addition to any defense obligation set forth in Customer's licensing agreement and is not subject to any limitation of, or exclusion from, liability contained in such agreements."

The Common Council authorizes the City to enter into an agreement with Microsoft that includes the foregoing clause or one substantially similar to it. Upon review the Risk Manager has approved this clause as it presents minimal risk to the City; and WHEREAS, the City routinely purchases approximately \$2,500,000 per year in software products and services from CDW-G as a State authorized reseller, approximately \$1,850,000 in Microsoft 365 licenses; and

WHEREAS, the City's current reseller agreement with CDW-G will expire in September and must be renewed to maintain these critical software licenses and services; and

WHEREAS, under MGO 4.26(3)(e), service contracts of more than one year that average more than \$100,000 per year in cost require Common Council approval and signature by the Mayor and City Clerk; and,

NOW, THEREFORE, BE IT RESOLVED, that the Mayor and City Clerk are authorized to sign a reseller service agreement with CDW Government, LLC for 3 years, for the purchase of software and software maintenance or subscriptions as needed; and

BE IT FURTHER RESOLVED, the Mayor and City Clerk are authorized to sign a 3-year State and Local Volume Pricing Microsoft Enterprise Enrollment agreement with Microsoft Corporation for the Microsoft 365 subscription, and the Finance Director or designee is authorized to sign Amendments, Mid-Term Product Selection forms, etc. with Microsoft if

needed to add software products during the term of the three-year agreement.

BE IT FINALLY RESOLVED, that the Finance Director or designee is authorized to sign future Microsoft Enrollment Agreements and reseller service agreements with CDW Government for the lifetime of the State contract with CDW Government pending availability of funds in the budget and in a form approved by the City Attorney;



City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84377

File ID:84377File Type:ResolutionStatus:Council New

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 07/09/2024

File Name: Adopting the West Area Plan Final Action:

Title: Adopting the West Area Plan, adopting an amendment to the Comprehensive

Plan and archiving underlying plans.

Notes:

Sponsors: John P. Guequierre Effective Date:

Attachments: West Area Plan Introduction Draft, West Plan Phase Enactment Number:

1 Feedback Summary, West Plan Phase 2 Feedback Summary, West Plan Phase 3 Feedback Summary, West Plan Phase 4 Feedback Summary, Ordinance 84383-Adopt the West Area Plan, West Area

Adopting Memo.pdf

Author: Linda Horvath and Ben Zellers, Planning Division

nning Division Hearing Date:

Entered by: Ihorvath@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Department of P and Community Economic Devel	and	Referred for Introduction				
	Action Text:	This Resolution was Referred for Introduction					
Notes: Plan Commission (8/26/24), Economic Development Committee (7/17/24), Transportation Commission Landmarks Commission (8/5/24), Board of Parks Commissioners (8/14/24), Urban Design Commission Common Council (9/10/24)				, ,,			

Text of Legislative File 84377

Fiscal Note

Fiscal note pending.

Title

Adopting the West Area Plan, adopting an amendment to the Comprehensive Plan and archiving underlying plans.

Body

WHEREAS, on August 2, 2022 the City of Madison Common Council authorized the Planning Division's shift to a planning framework with twelve (12) Area Plans that cover the entire city

and are updated every ten (10) years (RES-22-00577); and

WHEREAS, the City of Madison Common Council authorized the Planning Division to prepare the West Area Plan on January 3, 2023 (RES-23-00021); and

WHEREAS, consistent with RES-22-00577, archiving of adopted plans within the West Area Plan boundary was considered as part of the development of the West Area Plan and the adopted plans were reviewed to identify the recommendations from each adopted plan that should be incorporated into the West Area Plan before being archived; and

WHEREAS, the planning framework states that the Generalized Future Land Use Map developed through each area plan process will directly update the Comprehensive Plan's Generalized Future Land Use Map; and

WHEREAS, the City of Madison Common Council adopted a public participation plan for the West Area Plan on July 25, 2023 (RES-23-00516), as required by Wis. Stat. § 66.1001(4) to amend the Comprehensive Plan, which described how the City intended to make the process accessible to all; and

WHEREAS, the public participation process included engagement through a project website, interactive mapping exercise, virtual meetings, in-person meetings, emailed project updates to interested parties, distribution of informational materials, focus groups, neighborhood association visits, social media posts, traditional media, and board/committee/commission progress updates throughout the process; and

WHEREAS, the public participation process also included contracting with several Community Partners and coordinating with the City's Neighborhood Navigators to engage traditionally underrepresented residents through focus groups and participation in community events and activities; and

WHEREAS, Plan progress updates were provided to several City boards/ committees/ commissions during the planning process; and

WHEREAS, the West Area Plan's actions are based on: citywide policies including the Comprehensive Plan, recommendations in underlying plans, and feedback received from the community; and

WHEREAS, Plan actions focus on guiding changes to the physical aspects of the West Area that occur incrementally over decades, such as development initiated by the private sector and infrastructure improvements typically carried out by the public sector; and

WHEREAS, the City of Madison has held at least one public hearing on the Comprehensive Plan amendment in compliance with the requirements of Section 66.1001(4)(d) of the Wisconsin Statutes.

NOW, THEREFORE BE IT RESOLVED that the City of Madison Common Council adopts the West Area Plan.

BE IT FURTHER RESOLVED that the portions of the Comprehensive Plan's Generalized Future Land Use Map and Bicycle Facilities Map that overlaps the West Area is hereby updated to reflect the West Area Plan.

BE IT FURTHER RESOLVED that the Spring Harbor Neighborhood Plan, the University Hill Farms Neighborhood Plan, the Odana Area Plan, and the portions of the Southwest Neighborhood Plan, the Midvale Heights/Westmorland Joint Neighborhood Plan, and the Hoyt Park Area Neighborhood Plan that are within the West Area Plan boundary will be archived, which means they will no longer be used by the City for project review or planning purposes.

BE IT FURTHER RESOLVED that relevant maps and text in the Comprehensive Plan shall be updated to address the adoption of the West Area Plan and archiving of the above-listed plans.

BE IT FURTHER RESOLVED that, as recommended by the Plan Commission, the City of Madison Common Council adopts Ordinance File ID 84383, which will constitute its amendment of the City of Madison Comprehensive Plan under Section 66.1001 of the Wisconsin Statutes to update the Generalized Future Land Use Map, Bicycle Facilities Map, and maps and text that reference underlying archived plans.

BE IT FINALLY RESOLVED that staff is directed to implement the actions contained in the West Area Plan.

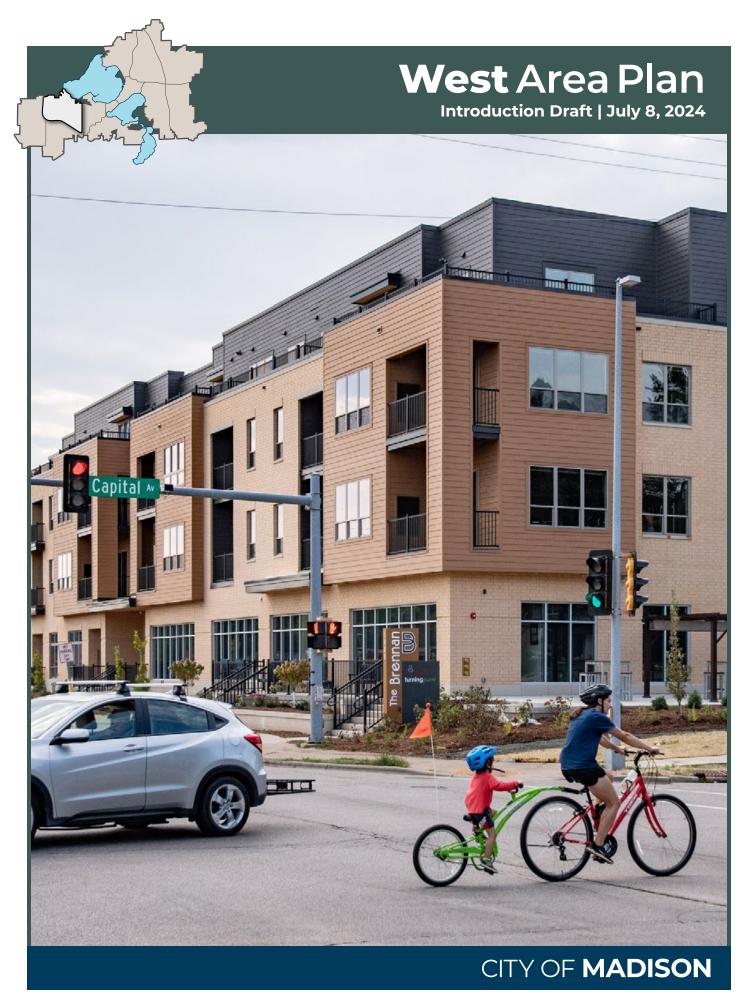


Table of Contents

Acknowledgements1
Introduction2
Land Use 10
Transportation
Neighborhoods and Housing30
Economy and Opportunity34
Culture and Character39
Green and Resilient46
Effective Government54
Health and Safety54
Appendix

Adopted by the Common Council on (Date)

Resolution RES24XXX Legistar file # XXXXX

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Introduction



Planning Framework

The West Area Plan is one of the initial two area plans prepared following the City's Planning Framework adopted by the Common Council on August 2, 2022. The framework covers a consistent set of topics and provides clear, high-level plan actions for specific physical improvements. The framework established 12 distinct geographies, each of which will be updated approximately every 10 years. Many actions in this Plan can be implemented over the next decade, but some actions are included that either have an uncertain timeline or may take longer than a decade to implement.

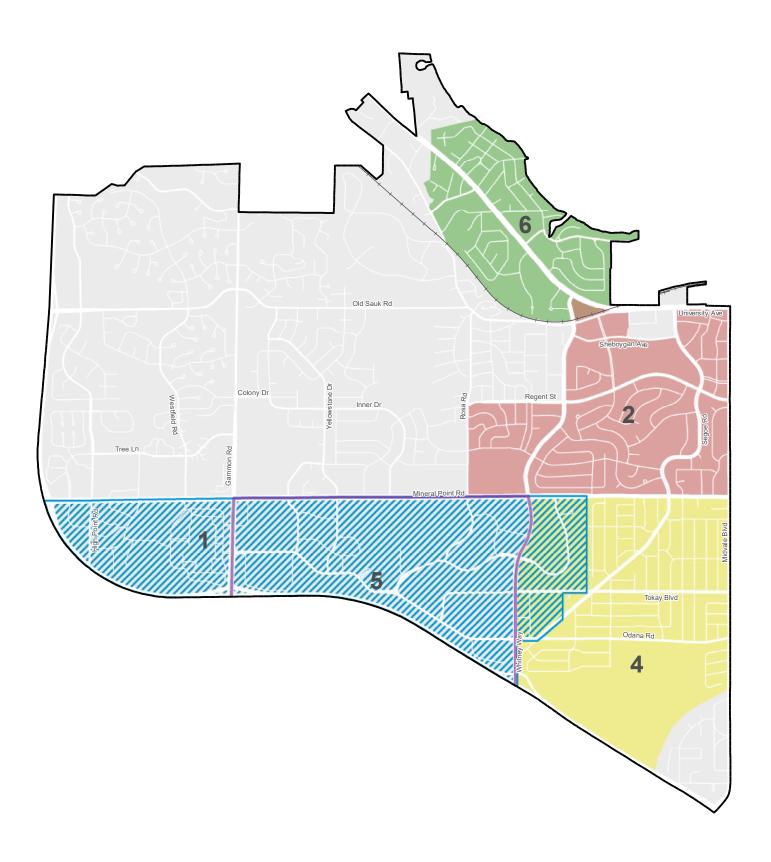
Previously Adopted Underlying Plans

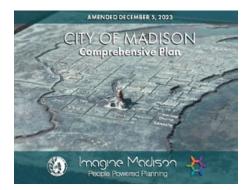
While this Plan is a new effort under the Planning Framework, it is not the first plan to be prepared within the area. The Common Council adopted five plans to guide City and neighborhood decision-making in portions of the West Area since 2006. The five plans have been important for their neighborhoods over the past 15-plus years. However, the Planning Framework provides a more eguitable approach to creating plans. Under the previous approach certain areas of the city received greater attention while others never had a plan beyond the citywide Comprehensive Plan. Providing full coverage of a growing city and regular updates to reflect changing conditions required a new approach to planning. This approach also creates greater clarity under a single Area Plan, rather than underlying plans with different and sometimes inconsistent recommendations for a particular geography.

Part of the West Area Plan process involved reviewing previously adopted plans with a focus on potential physical or regulatory changes specific to the West Area, such as land use, zoning, building height, transportation, parks and open space, public utilities, and stormwater infrastructure. Recommendations that simply repeated existing Citywide policies, have already been implemented, are inconsistent with current City policy, or were to be implemented by non-City entities were not incorporated in this Plan. The underlying plans were then archived: they will no longer guide development review, future City projects, policies, budgets, or work plans. Going forward, the West Area Plan will guide decisions. Archived plans for the West Area are:

- 1. Odana Area Plan (2021)
- 2. University Hill Farms Neighborhood Plan (2016)
- 3. Hoyt Park Area Neighborhood Plan (2014)
- 4. Midvale Heights/Westmorland Joint Neighborhood Plan (2009)
- 5. Southwest Neighborhood Plan (2008)
- 6. Spring Harbor Neighborhood Plan (2006)

The portion of the Hoyt Park Area Neighborhood Plan that underlies the West Area is not shown on the map since it was replaced by the University Hill Farms





Neighborhood Plan. Only the portions of the Midvale Heights/Westmorland Joint Neighborhood Plan and the Southwest Neighborhood Plan that underly the West Area Plan boundary are considered archived.

Relationship between the West Area Plan and the Comprehensive Plan

The Comprehensive Plan includes citywide policies and priorities for investment based on feedback received through an extensive community engagement effort. Recommendations are grouped by the seven elements illustrated below. The Comprehensive Plan also includes policies that guide future growth in a way that supports citywide policies.

The West Area Plan applies policies from the Comprehensive Plan, and other citywide initiatives, into specific recommendations for the West Area geography based on a closer evaluation of the area and additional community engagement.

While Area Plans help guide future growth, they don't automatically create or require development. Instead, these Plans guide the City's review process when development is proposed. Proposals that generally follow plan recommendations are likely to be approved. Those that don't face greater scrutiny, and approval is less likely. Plans also provide a guide for future City projects and programs, and future City budgets will determine the timing of those actions City budgets will determine the timing of those actions.

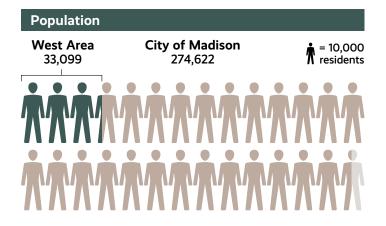


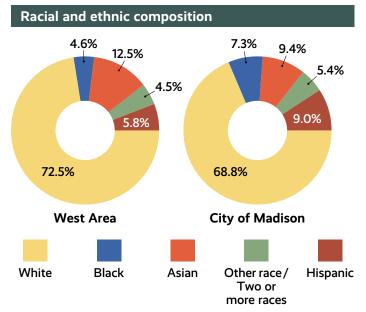
The West Area

The City of Madison is located within the ancestral homelands of the Ho-Chunk people, who have lived in this area for time immemorial. The West area is over 5,500 acres (8.7 square miles), bounded by the West Beltline Highway to the west and south, Midvale Boulevard to the east and the Village of Shorewood Hills, Lake Mendota, and the City of Middleton to the north. The area features several of Madison's largest commercial and employment centers, including Hilldale Shopping Center, West Towne Mall, University Research Park, and the Odana Road Corridor. The planning area is also crisscrossed or bordered by some of the busiest roads and transit corridors in the region, including University Avenue, Mineral Point Road, Whitney Way, Gammon Road, and the West Beltline Highway.

People are at the heart and soul of a community, and there are just over 30,000 residents in the West Area. People of color make up 27.5% of the population. Residents of Asian backgrounds comprise 12.5% of the population, many of whom live in the Sheboygan Avenue, Segoe Road, and Oakbridge areas. Residents identifying as Black are 4.6% of the population, many living in the Tree Lane and Wexford Ridge areas (2020 Census). Marsh The West Area has a strong social network of community-building groups, agencies, and insti-Old Sauk Rd Owen Sheboygan Ave Conservation Rennebohm Park Colony Dr Regent St Rd Rd Garnei Tokay Blvd **West Area Plan** Odana Rd **Planning Area** Odana Hills Area Plan Boundary tutions that offer programming, services, entertainment, and other resources. There are 27 registered neighborhood groups that vary in structure, mission, and agenda (see the City's Neighborhoods Map). Some host occasional social events while others organize year-round activities, community improvement projects, engagement in city projects, review of development proposals, and more. The Alicia Ashman Public Library is located within the West Area and Sequoya Commons Public Library is just outside it. There are a number of community-based organizations, schools, and places of worship in the Area

that also build community. The West Area has four public elementary schools





Income and Household Economics

6

	West Area	City of Madison
Poverty rate (families)	7.0%	5.9%
Median income	\$82,401	\$70,466
Homeownership (Tenure)	52.7%	44.5%
Cost-burdened households – owners	25.5%	19.8%
Severely cost-burdened households – owners	10.2%	6.8%
Cost-burdened households – renters	39.1%	46.5%
Severely cost-burdened households – renters	21.2%	25.5%

(and portions of six school attendance areas), two middle schools, and one high school in the Madison Metropolitan School District. There are also approximately 20 places of worship. In addition to spiritual support and religious affiliation, some offer community gathering space, pre-school classes, day care, social activities, blood drives, food pantries, health screenings, community gardens, and more.

Development History and Character

The earliest annexation of land to the City in the West Area occurred in 1947, when part of today's Summit Woods Neighborhood and the Odana Hills Golf Course were added to the city, followed by what would later become Hilldale and Rennebohm Park. The city continued to grow to the south and west through the 1950s, 1960s, and 1970s. Other than portions of University Research Park, which is a former UW Madison farm that began to develop in the late 1980s, the West Area was largely built out by the mid-1990s.

The area contains a variety of post-World War II development styles in a relatively compact area. University Hill Farms, to the southwest of Midvale Boulevard and University Avenue, was the first major development in the area, and was constructed in the 1950s and 1960s. Development continued to the south and west, with the gridded streets of the Midvale Heights neighborhood and the curvilinear streets of Faircrest and Parkwood Hills. The West Towne Mall regional shopping center was constructed at what was the edge of the city in the early 1970s, serving both new residents and drawing retail out of downtown. Development of the Walnut Grove, Sauk Creek, and Wexford Village neighborhoods, which include cul-de-sacs and development on private roads, followed in the late 1970s through the late 1980s. Other than a few scattered undeveloped properties, the only major piece of undeveloped land remaining in the West Area is a 35-acre parcel of land owned by TruStage (formerly CUNA) located south of Mineral Point Road between University Research Park and Oakwood Village.

The redevelopment of the parking lots surrounding Hilldale Mall in the mid-2000s kicked off a number of significant redevelopment projects that have increased the density of the West Area. Major redevelopment projects over the last 15 years include University Crossing at

Whitney Way and University Avenue, redevelopment of State-owned land as the Madison Yards development between Sheboygan Avenue and University Avenue, the completion of several major apartment buildings in the "University Park" project at the former Westgate Mall, and several major apartment/mixed-



use buildings in the Grand Canyon Drive/Yellowstone Drive area north of Odana Road. With Bus Rapid Transit (BRT) coming to the West Area in late 2024, the demand for further residential and mixed-use redevelopment is expected to continue.

Planning Process and Public Engagement

The West Area Plan process officially began with Common Council authorization on January 3, 2023. There were five planning phases, beginning with background data gathering and review of underlying sub-area plans. There were numerous opportunities for public participation. City staff hosted virtual and in-person public meetings in winter, spring and summer 2023, and invited people to share feedback through an online Interactive Map and Community Survey. City staff also conducted a business survey to learn about issues and priorities of business owners. In spring 2023 staff engaged with area teens through UW-Madison's PEOPLE Program class on urban planning at Vel Phillips Memorial High School. Youth engagement continued in the summer with the Youth Action internship program at Lussier Community Education Center.

Community Partners who have engagement expertise and trusted relationships with Black, Indigenous and People of Color communities were hired through a "Call for Commu-

nity Partners" to ensure the voices of people who are traditionally underrepresented in city processes were heard. Community Partners included the African Center for Community Development, Claire Baker, Francis Medrano, and Sedgwick Smith. They conducted a variety of outreach and education activities from

Hilldale Mall



The Madison Yards project redeveloped a former Wisconsin DOT parking lot into apartments, offices, and a grocery store.



New affordable housing replaced the former Westgate Mall at Whitney Way and Tokay Boulevard.



August 2023 public meeting at Rennebohm Park



May 2024 West Area Plan open house

May through November 2023, including helping city staff plan and facilitate focus groups, and providing education and outreach at festivals, fairs, and other activities in the community.

The final phase of the planning process ran from March 2024 through early June 2024 and included four virtual public meetings and four in person public meetings. The draft Plan was also posted to the <u>project webpage</u> for online review and feedback, and plan information was available at Alicia Ashman Library, Sequoya Library, and the Lussier Community Education Center. Themes that emerged from public feedback for each of the Plan's topic areas are included within the introductions of each chapter.

While adopted citywide plans (like the Comprehensive Plan) and adopted underlying plans served as the starting point for West Area Plan actions, public feedback provided additional insights into what people value and what they feel are the most important assets and opportunities. This helped identify themes and develop actions that are relevant to the community, while also being grounded in the realities of what is feasible for the City to implement and what can be implemented by community partners.

In some cases, conflicting feedback had to be balanced when drafting actions. For example, some residents felt that more housing is needed, while others felt enough development has already occurred in the West Area.

A final draft Plan was developed for review and approval by City Boards, Committees, Commissions, and the Common Council after the final phase of public engagement. The adopted Plan will be implemented over time by City agencies and other governmental entities, community organizations, and other stakeholders. Policy makers are responsible for the allocation of resources for the entire City, so funding for the West Area Plan's City-led actions will

Planning Process and Public Engagement



8 City of Madisor 1021

be weighed against other projects citywide. Securing funding from outside sources, leveraging other available funding, or combining projects will help in implementing some of the actions in this Plan.

Plan Format

The Plan is structured around the Comprehensive Plan's seven elements, with each element generally having four sections:

- 1. An introduction, covering background information and current conditions
- 2. What We Heard, including major themes in feedback and comments
- 3. Actions, which focus on City-led items
- 4. Partnerships, which focus on items led by entities other than the City but may have some City involvement or contributions

While public feedback themes were generally used to inform actions and partnerships, not all summarized themes have corresponding actions or partnerships. Some feedback themes contradicted each other. Some themes conflicted with adopted City policy, such as the Comprehensive Plan. Lastly, some themes addressed topics the City has little control over. Actions in each chapter are listed by number, but do not have a priority order.

Comprehensive Plan Goals

This Plan aims to advance the following goals from the Comprehensive Plan:

- Madison will be comprised of compact, interconnected neighborhoods anchored by a network of mixed-use activity centers.
- Madison will have a safe, efficient, and affordable regional transportation system that offers a variety of choices among transportation modes.

The planning is comprised of employment, retail, institutional/governmental, parks and open space, and residential uses, with a limited amount of vacant land. Residential uses cover approximately 44% of the Area. Most of this residential land is zoned for single-family residential, with only 1% zoned for 2 to 4 dwelling units and 5% zoned for multi-family housing (greater than four units per building). There are approximately 16,200 dwelling units in the planning area, 42% of which are single-family dwellings (7% higher than the city as a whole), 1% are two-family dwellings (4% lower than the city), and 57% are multi-family dwelling units (3% lower than the city).

Commercial uses make up approximately 15% of the area's land use, while Parks and Open Space make up approximately 12%. The vast majority of the commercial uses are clustered in and around Hilldale Shopping Center, at Whitney Way and the Beltline, along Odana Road, and in the West Towne Mall area. Employment uses are clustered in University Research Park and areas to the west along Odana Road. The combination of shopping and employment nodes make the West Area a destination for the region, attracting shoppers and employees from Dane County and beyond.

Land Use	Acres	% Of Total
Residential	2,447	44%
Commercial	814	15%
Institutional/Governmental	296	5%
Industrial	2	~0%
Park and Open Space	648	12%
Agriculture/Vacant/Under Construction	102	2%
Utilities, Transportation	1,269	23%
Total	5,578	100%

Source: Capital Area Regional Planning Commission (CARPC) 2020 Land Use

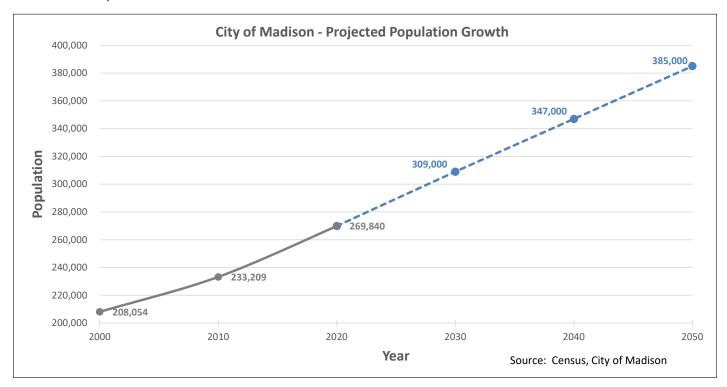
Future Growth

According to the Comprehensive Plan, the city is projected to add 115,000 new residents and 50,000 jobs between 2020 and 2050. While this growth will be spread across the entire city, a portion will occur within the West Area. The Comprehensive Plan emphasizes accommodating projected growth through redevelopment of underutilized sites, particularly on transit corridors. Such corridors and Activity Centers are identified as "Growth Priority Areas" in the Comprehensive Plan and include corridors like Mineral Point Road and University Avenue, along with Activity Centers like Hilldale and West Towne Mall. Accommodating growth through redevelopment makes use of existing infrastructure, reduces dependency on automobile travel, and preserves farmland, among other benefits.

10 City of Madisor 1023

Much of the redevelopment within the West Area to date has occurred along transit corridors, a trend that this Plan anticipates will continue. The Generalized Future Land Use (GFLU) Map in the Plan recommends the most intensive planned land uses along BRT lines, including Mineral Point Road and Sheboygan Avenue. There are many underutilized properties and large surface parking lots along the BRT line recommended for increased development intensity. See the Land Use Vision map for overall future land use themes.

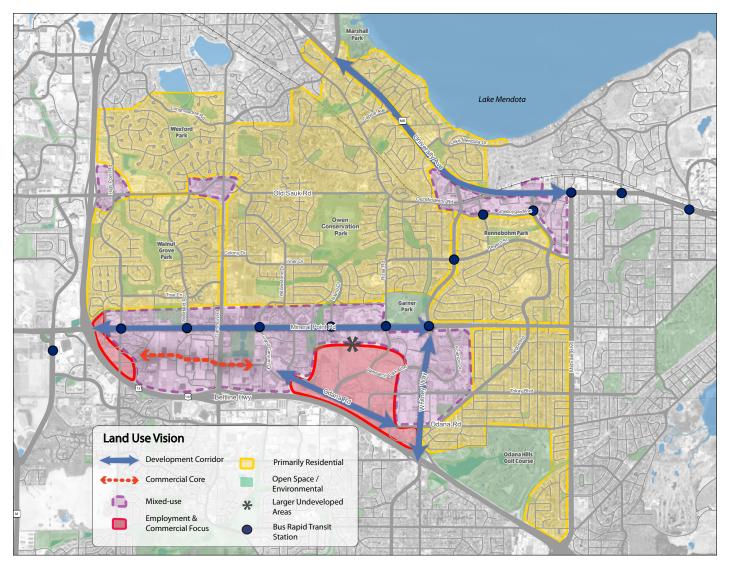
In addition to allowing more intensity along major corridors, places of worship, which have traditionally been mapped as "Special Institutional" or "Low Residential" in past plans, have been shifted to land use categories that allow mixed-use or multi-family residential development. If the institutions – many of which are along major corridors – should ever choose to pursue additional development on a portion of their property, consolidate, relocate, or close, more intense redevelopment can be considered.



Recommended Future Land Use and Growth Areas

The GFLU Map establishes a framework for how the West Area can grow and evolve over time. The map identifies broad categories of development such as residential, commercial, or mixed-use. The future land use categories guide future growth and establish the basis for how the City reviews development proposals. The land use mapping and actions in this Plan embody several of the Comprehensive Plan's citywide strategies, including:

- Concentrating the highest intensity development on major transit corridors and at activity centers;
- Facilitating compact growth to reduce development of farmland;
- Increasing the amount of available housing;
- Supporting the development of a wider mix of housing types, sizes, and costs.



While the GFLU Map establishes a future vision, there isn't a defined timeline for change. Many factors impact development, making it hard to predict where and when it might occur. The overall economy, market demand, available properties, financing, and developer priorities all influence the timing and location of development proposals.

The larger areas that meet the Comprehensive Plan's strategy of concentrating the highest intensity development on major transit corridors and at Activity Centers are shown as mixed-use on the Land Use Vision map, and include West Towne Mall, University Research Park, and the Hilldale area. While these areas have seen varying intensities of redevelopment already, significant portions remain as low-density uses and/or surface parking lots. Major "Development Corridors" shown on the Land Use Vision map are University Avenue, Mineral Point Road, Odana Road, and Whitney Way from Mineral Point to the Beltline. These mixed-use areas and corridors will see the majority of redevelopment in the West Area. However, there are other smaller properties and areas that can be appropriate for redevelopment, consistent with the planned land uses on the GFLU map.

The land use categories on the GFLU map are consistent with the Comprehensive Plan categories and are intended to guide decisions on zoning and

discussions and decisions on zoning and redevelopment proposals should be focused on whether the proposed changes are consistent with the GFLU Map, the actions in this Plan, and the goals, strategies, and actions in the Comprehensive Plan. Regent St Tokay Blvd **West Area Plan Generalized Future Land Use Bus Rapid Transit (BRT) Station** High Residential (HR) **Commercial Core** Neighborhood Mixed Use (NMU) Planned Streets Community Mixed Use (CMU) **Employment or Residential Only Mixed Use Area** Regional Mixed Use (RMU) **Generalized Future Land Use** General Commercial (GC) Low Residential (LR) Employment (E) Low-Medium Residential (LMR) Parks and Open Space (P) Medium Residential (MR) Special Institutional (SI)

redevelopment proposals. However, they are not zoning designations. Future

West Area Plan 1026

Implementation and Development Regulation Maximum Building Heights

The Comprehensive Plan provides height guidance for most land uses. A select few land use categories don't have recommended height ranges or allow Area Plans to recommend heights that may differ from those ranges. Mapping maximum building heights for these categories is intended to increase predictibility and simplify the development review process by communicating recommended height limits for these categories. The maximum building height recommendations consider the intensity of development for each land use category, adjacent land uses, desired urban form, and proximity to amenities like transit and parks. Refer to the Comprehensive Plan for guidance on areas without identified maximum heights on this map.

Additionally, some maximum building heights in this Plan are taller than the Morey Airport maximum height map in the northwestern portion of the West Area. If a project seeks to exceed Morey height limits a variance must be secured from the City of Middleton.

Commercial Core

The GFLU Map identifies several mixed-use categories focused on major corridors. These areas are planned for a mixture of residential, commercial, and em-

Land use categories

Images below illustrate types of development commonly found in each category. More information on each of the land use categories can be found in the Comprehensive Plan.

*In select conditions, Low Residential may allow up to 30 DU/ac and three stories. In select conditions, Low Medium Residential may allow up to 70 DU/ac and four stories.



Low Residential (LR)











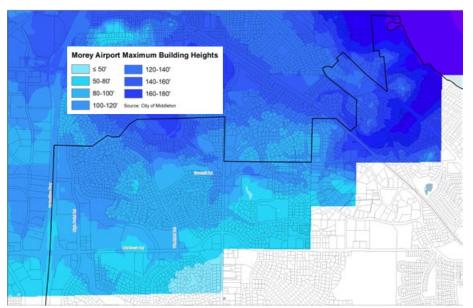








14 City of Madisor 1027



Morey Airport height regulations may impact development in the northwest part of the West Area

ployment uses to achieve an active and vibrant place. While a mixture of uses is envisionsed, mixed-use buildings with retail or other commercial uses on the ground floor are generally not required. Within these areas, commercial uses are seen as appropriate as part of a broader use mix, but not every location can support ground floor commercial businesses. Visibility and enough residents and customers nearby are needed to support the businesses that provide goods and services. Future buildings along Commercial Core frontages, however, should have ground floor commercial uses (retail, restaurant, service, or office uses). These areas appear capable of supporting commercial spaces and are located at important nodes and along major corridors.

Employment or Residential Only Mixed-Use

A few areas on the GFLU Map are designated as "Employment or Residential Only Mixed Use Area." These areas should not include retail nodes, instead focusing on residential and/or employment land uses.

Aligning Future Land Use and Zoning

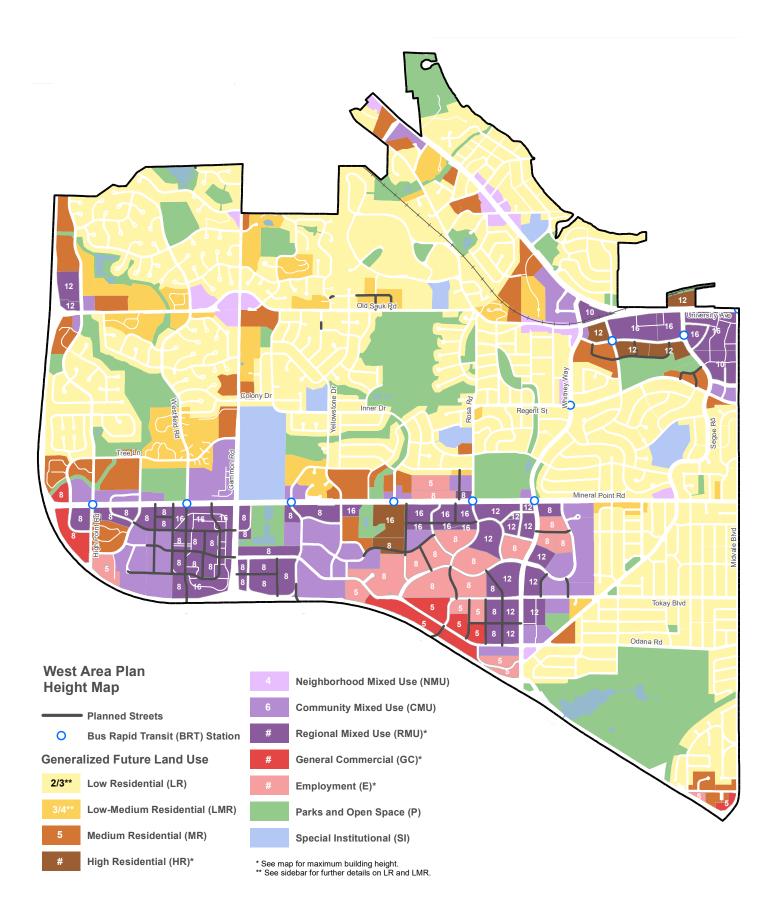
Zoning plays a key role in the implementation of adopted plans. Several recent modifications to the zoning code facilitate higher-density and compact forms of development across the city, including in the West Area. These modifications included increasing the number of dwelling units allowed in mixed-use buildings without conditional use approval and creation of a Transit Oriented Development (TOD) overlay zoning district. TOD encourages development of new housing units along the City's primary transit routes through increases in allowed density and elimination of minimum parking requirements. The TOD overlay also includes increased requirements to improve the design of new development. Portions of the West Area are located within the TOD Overlay District.

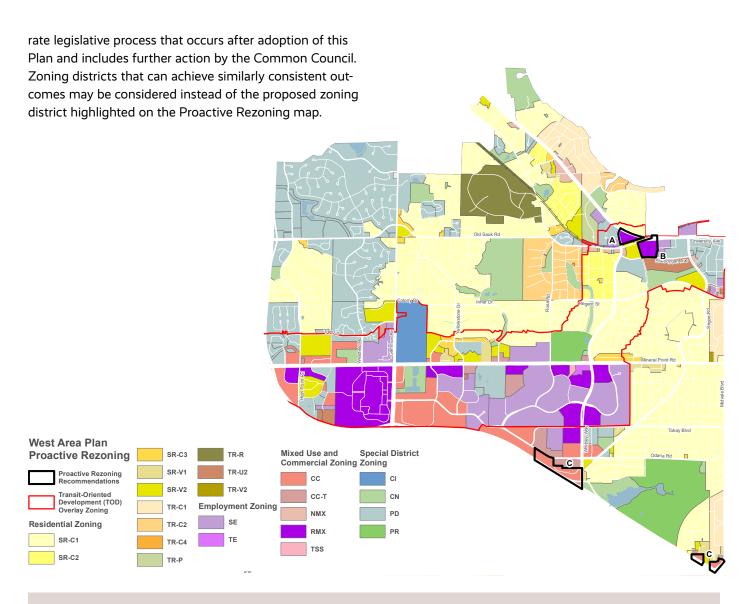
This Plan recommends several properties for proactive rezoning. Proactive rezoning is a City-initiated process that rezones property to a district consistent with this Plan's GFLU Map. Implementation of proactive rezoning is a sepa-

What We Heard

- More mixed-use development and high density at appropriate locations.
- 2. Avoid over-developing and losing neighborhood character.
- Ground floor development at the corner of Regent/Whitney Way and along Midvale Boulevard should include active uses.
- Desire not to include retail in areas of low-density development, while others would lke retail that they can walk to.
- 5. Create more walkable spaces.
 Some areas that can benefit
 from improved walkability are
 West Towne Mall area, parks,
 the BRT route near employment and residential areas, and
 the Madison Yards area.
- More options for low density housing that isn't single-family are desired, such as Missing Middle housing and smaller 2–3 story apartments.
- A balance between single-family and multi-family development is needed.
- Some skepticism about changing planned land use from Low Residential (LR) to other residential or mixed-use categories that would allow for more intense future development.
- A mix of feedback between some people wanting more housing and businesses while others who don't want more construction.
- 10. Appreciation for the mix of uses within the West Area, with retail, employment, restaurants in proximity to, or part of, neighborhoods.

West Area Plan 17028





Planned Development Zoning

Many areas in the city have a site-specific zoning called Planned Development (PD). PDs are intended for unique buildings or development projects that can't be approved using conventional zoning districts. These developments don't need to follow other zoning requirements and establish their own unique and detailed zoning code addressing uses, building placement, height, landscaping, parking, etc.

PDs were heavily used prior to the adoption of the current zoning code in 2013. The previous code, adopted in 1966, was so burdensome, complex, and out-of-date that PD zoning was routinely used for relatively typical projects, including single-family homes and small apartment buildings. Over the years, the City approved over 1,500 PDs, and while they simplified the approval process, they create long-term challenges for residents, property owners, and the City. Zoning

codes are amended over time to address new issues or priorities that emerge, but PD files are rarely updated and reflect the time when they were adopted. Zoning code updates allowing accessory dwelling units and the keeping of chickens, or those requiring electric vehicle charging or bird-safe glass, do not apply to these PDs. The zoning text for PDs are only available in person (not online, like the rest of the code), and review and approval for minor changes like the addition of a shed can take weeks instead of minutes. The current zoning code is much more adept at handling these now-typical developments that were previously approved as PDs. Transitioning PDs approved under the old code to conventional zoning districts will improve transparency and usability for residents and property owners while improving the ability of the City to respond quickly to inquiries and routine applications like fences, sheds, and additions.

West Area Plan 1030

The West Area also has many properties with Planned Development (PD) zoning. Each PD has a unique set of regulations which is cumbersome for both residents and the City. This Plan recommends developments zoned PD – the bulk of which are along or west of Gammon Road – be reviewed for potential transition into conventional zoning districts to simplify zoning applications for residents and simplify administration for staff.

Actions

- 1. Update the Comprehensive Plan Generalized Future Land Use Map to be consistent with the Future Land Use map in this Plan.
- 2. Rezone properties identified in the Proactive Rezoning map to implement the goals of this Plan and encourage development consistent with the actions in this Plan:

Site	GFLU	Existing Zoning	Potential Zoning	Notes
Α	RMU	NMX	RMX	Create consistency with GFLU Map; allow for more intense development in close proximity to transit.
В	RMU	SE	RMX	Create consistency with GFLU Map; allow for more intense development in close proximity to transit.
С	GC/E	CC-T	CC	Change to discourage residential along Beltline Highway.

- 3. Review Planned Development (PD) zoning covering development along and/or west of Gammon Road for a potential transition to conventional zoning to simplify resident applications to the City for minor projects and simplify staff administration.
- 4. Implement maximum building height recommendations shown on the Building Height Map, with a mechanism for allowing bonus stories for developments that achieve specific affordability or sustainability parameters, similar to downtown height bonuses in City ordinances (Madison General Ordinances 28.071(2)(a)2).
- 5. Create a new commercial and employment zoning district with limited allowable residential development for areas along the West Beltline Highway and similar areas elsewhere in the City.
- 6. Require ground floor commercial uses in the locations shown as Commercial Core on the GFLU Map.

18 City of Madisor 1031

Transportation



The West Area's transportation system is anchored by the West Beltline Highway, Old Sauk Road, Mineral Point Road, Gammon Road, University Avenue, Whitney Way, and Midvale Boulevard. Transit service is anchored by the City's first BRT route, which runs on Mineral Point Road, Whitney Way, Sheboygan Avenue, Segoe Road, and University Avenue. Local transit routes run along Old Sauk Road, Tokay Boulevard, and Odana Road, among other major streets. The bicycle network has significantly improved in recent years, with on-street facilities added to Whitney Way, Odana Road, and a portion of Tokay Boulevard to supplement existing routes on Segoe Road and Midvale Boulevard from the Beltline to Mineral Point Road. Shared-use paths are part of the bicycle network as well, though the path network has gaps. People report a lack of north-south bike connectivity in the West Area. The sidewalk network is generally robust, with some exceptions in neighborhoods like Spring Harbor, Glen Oak Hills, and Crestwood.

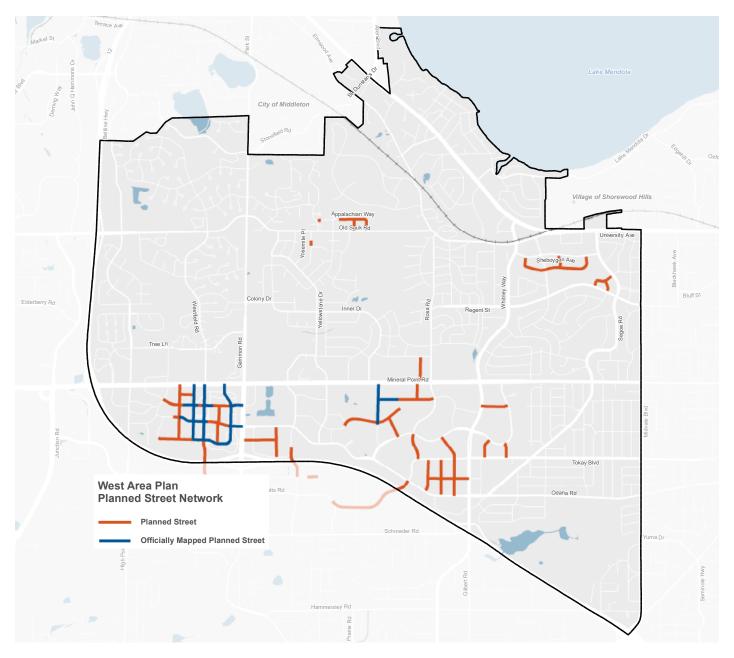
Street Network

The street network is made up of major roads with limited connections to neighborhoods and commercial areas. Many area neighborhoods have local street networks that are internally focused and feature winding, circuitous routes, or cul-de-sac and dead-end configurations. This concentrates traffic on fewer streets, and increases travel time for all modes of transportation. An exception is the Midvale Heights Neighborhood, which has a grid street system and multiple connections to major roads. Commercial and employment areas like West Towne Mall and University Research Park were designed around cars. That is reflected in a street network with large blocks, private drives, few through streets, and limited connections to major streets.

What We Heard

- 1. Better road, bicycle and pedestrian connectivity is needed.
- 2. With all transportation improvements, address the needs of people of all ages, backgrounds and abilities.
- Safer, more efficient and enjoyable pedestrian and bicycle transportation, especially for students travelling to and from area schools and after-school programs.
- 4. Safer and more direct northsouth pedestrian and bike connections that also feed into east-west connections.
- Appreciation for how walkable and bikeable neighborhoods are within the area, with access to transit and destinations (restaurants, shops, services).
- 6. Better walkability within neighborhoods.
- Safer, more direct path connections between neighborhoods and commuter paths that take people to employment and shopping areas within and outside the West Area.
- 8. No shared-use path in the Sauk Creek Greenway, while others would like a shared-use path there.
- Appreciation for the existing transit accessibility to the UW-Madison campus and downtown, while also desiring further transit service frequency and additional routes.
- 10. Safer and more comfortable bicycle facilities like protected lanes with barriers.

West Area Plan 1032



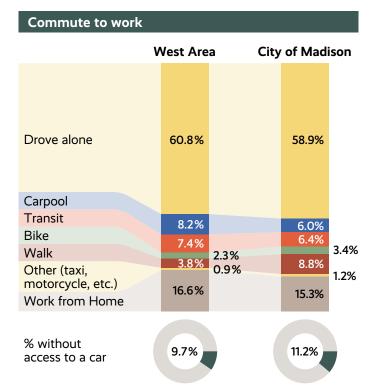
Changes to the street network are recommended to create walkable, pedestrian-oriented places, better connect neighborhoods, and distribute vehicular traffic over multiple routes. The largest concentration of planned streets is within the West Towne Mall area. These streets are needed to support that area's future growth and should be built as redevelopment occurs. Other street connections, such as Appalachian Way and Yosemite Place, would better link existing neighborhoods.

Transit

Transit service in the West Area is robust, with Metro Transit's east-west BRT route replacing Route A. BRT Route A provides 15-minute service starting at Junction Road, travelling along Mineral Point Road, Whitney Way, Sheboygan Avenue and University Avenue, through campus and downtown, and along East Washington Avenue to East Towne Mall (see the City's BRT Map). It serves high ridership areas, including West Towne Mall, surrounding commercial areas, Vel Phillips Memorial High School, Ezekial Gillespie Middle School, Sheboygan

Avenue apartments, and the Hilldale area. The BRT system includes dedicated bus lanes along much of its length, passenger stations, and large electric buses.

Metro Transit implemented its Transit Network Redesign in 2023 (see the Metro Transit Route Map). The new system reduces the number of routes and stops and increases bus frequency to improve reliability and efficiency. In addition to Rapid Route A, the West Area is served by routes D, E, F, J, H, R, 28, and 38. Mineral Point Road and some portions of Whitney Way and University Avenue have 15-minute bus service on weekdays and Saturday. Tokay Boulevard, portions of Whitney Way, Old Sauk Road, University Avenue, and Odana Road have 30-minute weekday/Saturday service. North Gammon Road and a portion of Old Sauk Road has 60-80-minute service. The Transit Network Redesign has led to mixed experiences for transit riders with some preferring it over the old system, while others find it difficult and sometimes unsafe to access stops.



Bike and Pedestrian Network

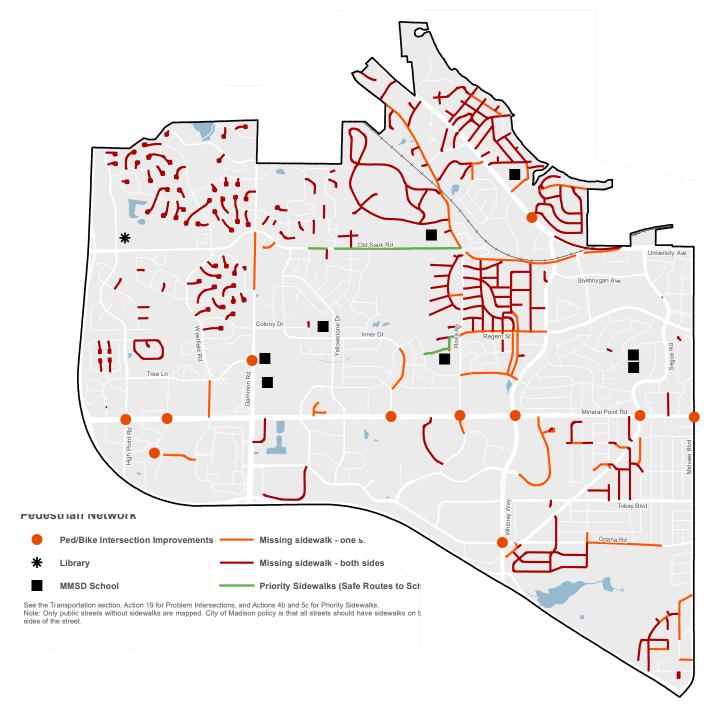
About 75% of the West area's street network includes sidewalks on one or both sides of the street.

However, a number of neighborhoods were constructed without sidewalks, including Spring Harbor, Glen Oak Hills, Crestwood, Highlands, Wexford Village, Sauk Creek, and Walnut Grove. It is City policy to add sidewalks as streets are reconstructed, so over time these areas will become safer, more enjoyable places to walk. The City's Complete Green Streets Guide will guide reconstruction, including how the space within the right-of-way will be used for the street, terrace, sidewalk or other pedestrian/bicycle infrastructure, in context with the surroundings.

Some major streets in the West Area include bike lanes. While there are several shared-use paths in the West Area, such as the University Avenue Path, Blackhawk Path, Southwest Path, and West Towne Path, portions of the area lack connectivity for bikes, especially north to south. Further, bike lanes on heavily travelled streets and in multi-lane intersections feel unsafe for many bicyclists and the motorists that drive alongside them. There are also large greenspaces, like the Walnut Grove Greenway, Sauk Creek Greenway, and Owen Conservation Park, that limit connectivity as they span multiple blocks and do not include through-paths accessible to people of all ages and abilities.

The City established an initial "All Ages and Abilities Bike Network" map in the Complete Green Streets Guide. The Network identifies streets and paths that are intended to provide a complete network of low-stress bikeways between neighborhoods, key destinations in the city, and to adjacent municipalities. Some streets or paths may need to be built or improved to provide low-stress connections, but the map serves as the long-range plan for a full network. The initial All Ages and Abilities Bike Network map is planned to be updated in the coming years. Information in this Plan on recommended bicycle facilities will inform changes to the All Ages and Abilities map.

West Area Plan 2-1034



Other Transportation Initiatives

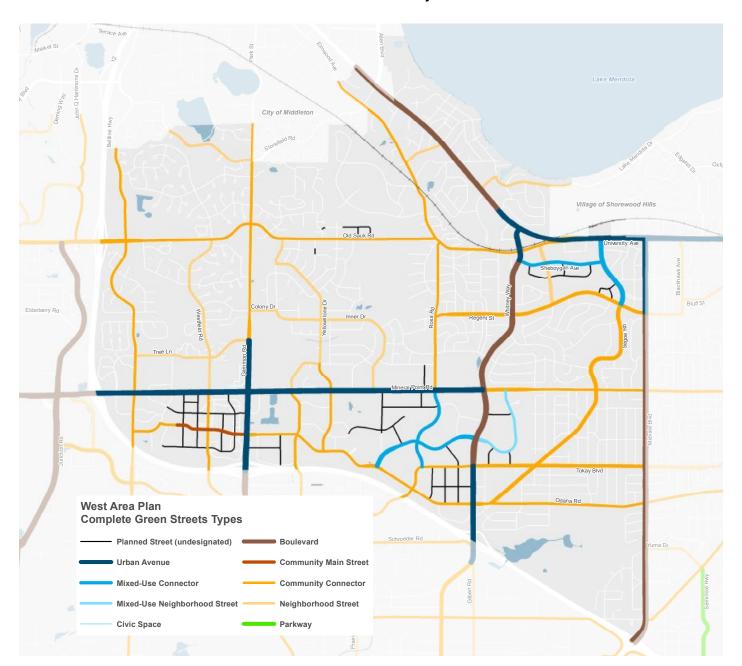
The High Injury Network Map shows street segments with elevated levels of serious injuries and fatalities. Areas where people tend to walk, bike and board transit are of particular concern because collisions between motor vehicles and people result in more serious injuries. This includes the street segments shown near Hilldale Shopping Center, Sheboygan Avenue, the commercial area along South Whitney Way between Tokay Boulevard and the West Beltline Highway, Gammon Road near Vel Phillips Memorial High School and Ezekiel Gillespie Middle School, and the Midvale Boulevard/Southwest Path intersection.

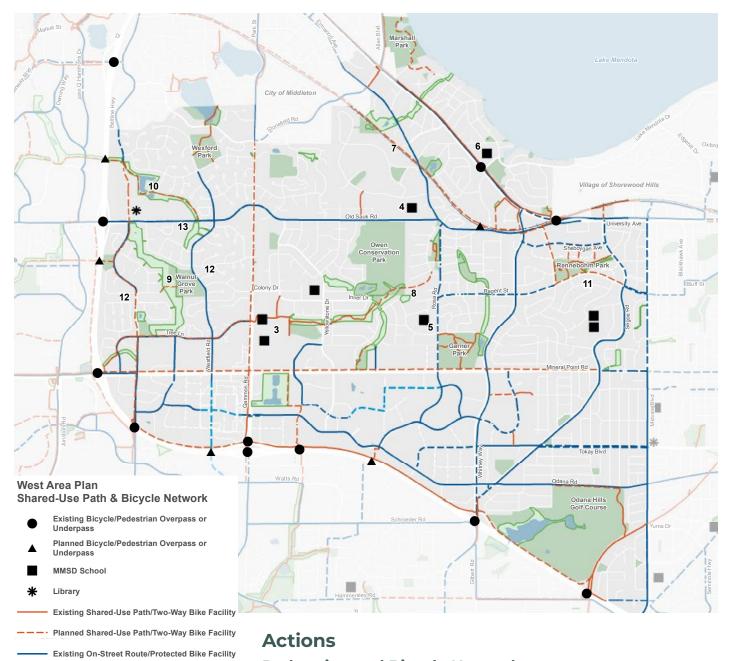
The City's <u>Vision Zero campaign</u> and <u>Safe Streets Madison program</u> focus on eliminating traffic deaths and serious injuries on city streets by improving public infrastructure. The West Area Plan's transportation actions complement

these initiatives. Vision Zero's main goal is eliminating traffic fatalities throughout the city by 2035. This includes redesigning high injury and fatality street segments to slow vehicle speeds and make intersections safer for people walking, biking, and driving. Safe Streets Madison emphasizes traffic safety measures such as speed humps, mini traffic circles, and pedestrian refuge islands. Further initiatives that will be complementary include the upcoming All Ages and Abilities and Pedestrian Network Plans. In all of these initiatives, there is an emphasis on improvements in areas where people with low incomes and people of color live since they face disproportionate impacts from the historic focus on automobile infrastructure.



Bike lanes and a pedestrian refuge island on Westfield Road at Colony Drive





Pedestrian and Bicycle Network

Planned On-Street Route/Protected Bike Facility

Planned On-Street Route/Protected Bike Facility

(planned roads)

Greenway

City of Madison Park

Note: See text the Transportation actions for numbered

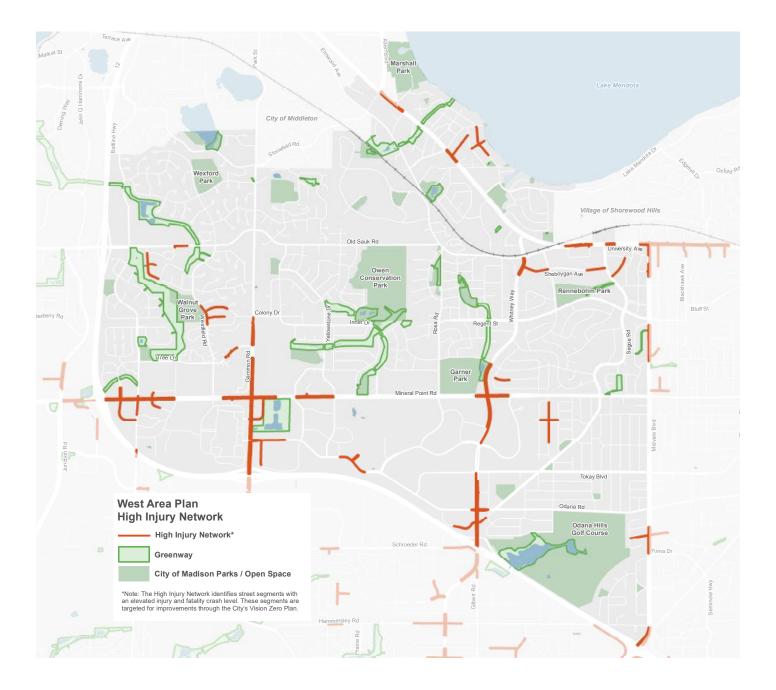
- Close gaps in the sidewalk network shown on the Sidewalk Network Map through the Safe Streets Madison Program, as streets are reconstructed, and as priority sidewalk additions can be constructed through state and federal grants (see priority sidewalk actions 4 b. and 5 c. below)
- 2. Implement the pedestrian and bicycle improvements shown on the Shared-Use Path and Bicycle Network Map. Further description is included below with the actions corresponding to the numbers on the map.
- 3. Vel Phillips Memorial High School and Ezekiel Gillespie Middle School
 - a. Study the addition of a left turn arrow from northbound N. Gammon Rd. into Tree Ln. to the traffic signal at the Tree-Gammon intersection.
 - b. Add a shared-use path along the east side of N. Gammon Rd. from Mineral Point Rd. to the City of Middleton. Starting at Tree Lane or Colony Drive, narrow Gammon Road to one lane of traffic in each direction with a center turn lane to reduce speeding, increase safety, and allow for construction of the shared-use path.

a center turn lane to reduce speeding, increase safety, and allow for construction of the shared-use path.

- c. Consider narrowing travel lanes and other design changes on N. Gammon Rd. between Mineral Point Rd. and Tree Ln. to calm traffic and enhance safe connections to schools.
- d. Coordinate with the Madison Metropolitan School District (MMSD) on an improved east-west shareduse path connection through the high school and middle school property.
- e. Work with the Wisconsin Department of Transportation (WisDOT) to clarify the lane markings and signage at the Beltline Hwy. ramps at Gammon Rd. to improve pedestrian and bicyclist safety and comfort.



Gammon Road at Sawmill Road and Ponwood Circle



West Area Plan 1038



Southwest Path at Midvale Boulevard

- f. When WisDOT pursues replacement of the Beltline Hwy. bridge at S. Gammon Rd., work with them to extend the wide sidewalk/shared-use path under the highway.
- g. Work with WisDOT to add lighting under the S. Gammon Rd. overpass.
- h. Improve pedestrian and bike access and safety at the Mineral Point Rd. intersections with S. High Point Rd., S. Gammon Rd., and Grand Canyon Dr.

4. Crestwood Elementary School

- a. Review MMSD's school travel plan and flow of people walking, biking and driving to the school to improve safety at the school.
- b. Evaluate options to add or enhance ped/bike facilities on Old Sauk Rd where they are missing, through street reconstruction and the Safe Streets Madison Program.

5. Stephens Elementary School

- a. In coordination with MMSD, mark bike lanes on N. Rosa Rd. while adding bump outs for school busses and installing more durable terrace paving for students getting on and off busses.
- b. Consider adding an alternate student drop-off/pick-up area at the back of the school on Cable Ave.
- c. Add a priority sidewalk on the remainder of the north side of Cable Ave., the west side of Beach St. and the north and south sides of Anchorage Ave. between Beach St. and Rosa Rd., along with crosswalks at the Cable Ave. and Beach St. intersection, as part of the Safe Streets Madison Program.

6. Spring Harbor Middle School

- a. Replace the pedestrian underpass beneath the railroad between Old Middleton Rd. and Craig Ave. and add lighting and a new pathway extending from Craig Ave.
- 7. If the rail corridor becomes inactive, work with WisDOT to study the feasibility of converting the corridor to a shared-use path that parallels University Ave., like the Southwest Path.
- 8. Construct a shared-use path from Island Dr. through the southeast corner of Owen Conservation Park to Bordner Dr. in coordination with the Crestwood Neighborhood Association.
- 9. During the Sauk Creek Greenway Corridor Plan, use detailed engineering data to consider the impacts and benefits of adding an All Ages and Abilities shared-use path to make an east-west connection across the greenway to Walnut Grove Park. Any All Ages and Abilities path should be designed using environmentally sensitive best practices and minimizing impacts to trees and other vegetation, wildlife habitat, and adjacent properties.
- 10. When the Wexford Pond Greenway comes up for reconstruction, use environmentally sensitive best practices to design a shared-use path through the greenway. Where possible, the path should follow the existing maintenance path to provide continued access for greenway management, and it should provide a safe, accessible, and enjoyable route for bicyclists, pedestrians, people in wheelchairs, and other users, while minimizing impacts to wildlife habitat, vegetation, trees and adjacent properties.

City of Madisor 1039 26

- 11. Convert existing five-foot wide sidewalks to 10' wide shared-use paths along Regent St. from N. Segoe Rd. to Eau Claire Ave. and along Eau Claire Ave. from Old Middleton Rd. to Regent St. while maintaining or widening existing terraces and minimizing tree impacts. This would facilitate bicycle connections without removing on-street parking that is needed to serve Rennebohm Park. Shared-use path implementation should occur in concert with additional dedication of parkland for Rennebohm Park.
- 12. Reduce on-street parking on High Point Rd. and Westfield Rd. between Old Sauk Rd. and Mineral Point Rd. to allow for construction of all ages and abilities bike facilities.
- 13. Extend the one lane in each direction configuration of Old Sauk Road from Westfield Road to High Point Road while adding a center turn lane and buffering the existing bicycle lanes.
- 14. Integrate pedestrian and bicycle infrastructure like benches, pedestrian-oriented lighting, and bike racks into terrace areas adjacent to high-traffic pedestrian and bicycle routes, especially in and around the "Commercial Core" areas shown on the GFLU Map.
- Integrate facilities like bike racks, bike lockers and BCycle stations in street reconstruction and development projects along BRT corridors and at BRT stations.
- 16. Implement bicycle wayfinding.
- 17. Work with the City of Middleton to:
 - a. Extend a shared-use path from Camelot Drive through Marshall Park to connect with Middleton Beach Road. Note: the line shown on the Shared-Use Path & Bicycle Network map represents the connection but may not be the final route.
 - b. Ensure cross-border bicycle connectivity as both communities continue to improve bicycle facilities, including lanes on John Q. Hammons Dr., paths or lanes on N. High Point Rd., and a possible path along N. Gammon Rd.
- 18. Work with WisDOT to improve pedestrian and bicycle connectivity across the Beltline by:
 - Integrating new Beltline crossings shown on the Shared-Use Path and Bicycle Network Map into any future Beltline construction/reconstruction to enhance connectivity for pedestrians and bicyclists.
 - b. Improving conditions for transit, bicyclists, and pedestrians at Beltline interchanges.
 - c. Improving conditions of the Beltline Hwy. underpass at Struck St. and making needed improvements for safety and comfort of pedestrians and bicyclists such as upgrades to pavement, drainage, and lighting.
- Improve pedestrian and bicyclist safety at the major intersections identified on the Pedestrian Network map.



Old Sauk Road at Farmington Way



The Struck Street underpass of the Beltline is an important connection for pedestrians and bicyclists, but is in need of upgrades.



Existing Conditions

20. Improve pedestrian safety along Segoe Rd. by by removing Laub Ln. from Berwyn Dr. to S. Segoe Rd. (see diagram below) and by reducing pedestrian exposure and crossing distance with bump-outs, pedestrian refuge islands, and continental crosswalk markings, especially where Segoe Rd. crosses Wedgewood-Manor Cross, Tokay Blvd., and Tocora-Hilltop.



Proposed Intersection Improvements

Road Connectivity and Traffic Circulation

- 21. Update the Complete Green Streets Guide to reflect the Complete Green Streets Types Map in this Plan.
- 22. Preserve the opportunity to connect Middleton St. in the future.
- 23. Improve road connectivity with the following street segment changes (also see the Planned Streets Map):
 - a. Study the transformation of St. Dunstan's Dr. to a two-way street from Allen Blvd. to Old Middleton Rd. if the St. Dunstan's Episcopal Church site is proposed for redevelopment.
 - b. Connect Yosemite Place to Yosemite Trail to improve connectivity south of Old Sauk Rd.
 - c. Contingent on redevelopment proposed in the area, extend Appalachian Way to the east, then south to connect to Old Sauk Rd. Add a new north-south street alongside the Cooper Lane Bike Path to connect with Appalachian Way extended.
 - d. If properties north of Rennebohm Park redevelop, reserve property to create a narrow, pedestrian- and bicyclist friendly street to the north of the park to break up a large block, provide the framework to orient buildings toward the public park, and create alternate access to the properties fronting Sheboygan Ave. The street should have the minimum allowable speed limit and be designed to minimize non-local through traffic.

City of Madiso 1041 28



Partnerships

A. Work with WisDOT to:

- i. Add new Beltline crossings shown on the Planned Streets Map and the Shared-Use Path to enhance connectivity for cars, pedestrians, bicyclists, and transit, shorten trips, and provide alternative north-south routes to Beltline interchanges at S. Gammon Rd. and S. Whitney Way.
- ii. Contribute Beltline planning to ensure that City interests are represented as part of WisDOT processes.
- B. Explore public-private partnerships with area commercial property owners to create a Park and Ride system that makes better use of underutilized parking lots on the weekdays.
- C. Work with the Madison Metropolitan School District to increase lighting along the north-south shared-use path that runs along the east side of the Ezekial Gillespie Middle School property and connects to Inner Dr.
- D. Work with the MMSD and school principals to address pedestrian, bicycle, and traffic safety issues around schools when street repaving, reconstruction, or other changes to the right-of-way are being considered. Strategies may include signage, increasing crosswalk visibility, modifying traffic patterns, increasing nearby sidewalk connectivity, and implementing safe All Ages and Abilities bicycle connections.

Planned Street (conceptual location; if properties redevelop)

Potential Park Expansion (conceptual location; if properties redevelop)

Planned Shared-Use Path (conceptual location; if properties redevelop)

West Area Plan 2 1042

Neighborhoods and Housing

Comprehensive Plan Goals

This Plan intends to advance the following goals from the Comprehensive Plan:

- Madison will be a safe and welcoming city of strong and complete neighborhoods that meet the needs of all residents.
- Madison will have a full range of quality and affordable housing opportunities throughout the city.

This Plan seeks to strengthen neighborhoods in the West Area and provide opportunities for new housing as the Area continues to evolve. Madison's strong job market and high quality of life has led to consistent population growth. However, a limited housing supply leads to limited housing options and high housing costs. With Madison anticipated to add about 115,000 new residents between 2020 and 2050 - or about 3,800 new residents per year - housing supply issues may continue despite ongoing efforts to improve housing availability.

The Comprehensive Plan directs a majority of future growth to occur as infill and redevelopment. Madison is gradually losing the ability to expand outwards and extending City services for development at the edge of the city is more costly. For these and other reasons, the City's ability to accommodate growth through redevelopment is increasingly important. The West Area provides an opportunity to build a significant amount of new housing in close proximity to jobs, shopping, services and transit. There is also an opportunity to diversify the West Area's housing stock, which includes an abundance of areas comprised solely or almost entirely of single-family homes. Additionally, large parts of the West Area are low-density commercial land uses. Much of the redevelop-

> ment in the West Area will occur on these auto-oriented commercial sites and underutilized parking areas, which does not displace current residents.

> As current residents age, young residents move to Mad-

ison in large numbers (an increase of approximately 14,000 more 20- to 40-year-olds in the past ten years), and racial and ethnic diversity increases, it is important to have housing options to accommodate all stages of life and living arrangements. While household size has been declining for decades in both Madison and Wisconsin and the number of families with children has remained flat compared to overall household growth, a mix of 2-, 3-, and 4-bedroom units for families should be thoughtfully included, especially near schools and at affordable prices.

% of housing by construction date

	West Area	City of Madison
Since 2000	11.5%	25.2%
1980-1999	29.8%	22.3%
1960-1979	39.8%	24.8%
1940-1959	15.3%	13.8%
Before 1940	3.6%	13.9%



The Monticello Apartments on Sheboygan Avenue

Affordability

The City's most effective way to increase the supply of new income- and rent-restricted affordable rental housing is through the financial assistance programs offered by the Community and Economic Development Divisions (CDD and EDD). The Affordable Housing Fund-Tax Credit (AHF-TC) and Tax Increment Financing (TIF) Programs are

the City's primary tools for impacting affordable rental housing development. The AHF-TC is especially impactful when used in cooperation with the Wisconsin Housing and Economic Development Authority's Low-Income Housing Tax Credits. CDD also provides financial support to smaller scale, non-tax credit affordable rental developments like housing cooperatives. New lower cost or

subsidized housing should be in areas that already have a high level of resources and amenities, including transit. Areas along University Avenue, Sheboygan Avenue, Whitney Way, and Mineral Point Road are particularly well-positioned for this type of development. However, it is often more cost-efficient to preserve existing naturally affordable older housing stock than to build new low-cost housing. While naturally-occurring affordability necessarily requires a "hands-off" stance from the City, maintenance is obviously required and it may be lost altogether as prices continue to increase if additional housing is not built to meet demand. State statutes and court decisions do not allow the City to enact rent control or require new developments include affordable housing.

Mix of Housing Types

In addition to larger multifamily buildings constructed in activity centers and corridors identified in the Comprehensive Plan's Growth Priority Areas Map, lower-impact residential development can occur in existing lower-density residential areas through the addition of smaller buildings often referred to as the "Missing Middle." These building types are designed to fit with predominantly single-family residential areas. On the recommended GFLU Map Low-Medium Residential areas are primarily intended to accommodate Missing Middle. Missing Middle housing types can also be added to Medium Residential areas and in Low Residential areas located near activity centers or along transit corridors.

In addition to the different physical characteristics of housing, such as size of units and number of units in a building, it is also important for a variety of tenancy options be available. The most common tenancy arrangements are fee-simple ownership (often for single-family residences) and rental apartments. Co-housing, condominiums, housing cooperatives, and community land trusts can, however, provide further housing choice to residents.

Complete Neighborhoods

Awareness and access to existing community programs and services, especially for youth and seniors, is important for neighborhood health. Several community facilities provide services along the southern portion of the area such as Madison School and Community Recreation (MSCR) - Odana, Madinah Community Center, and Huckleberry and Persimmon. Except for the Alicia Ashman Library and Lussier Community Education Center, there are large geographic areas without support in the rest of the West Area. Residents of the area desire more community facilities where people can obtain programing and support including childcare, youth activities, job training, senior care, managing living expenses, and other family needs.

Schools are also catalysts for establishing strong neighborhoods, as children's activities, daily social interactions, and community projects create connections. The West Area has a strong network of schools, with four public elementary schools (and portions of six attendance areas), two middle schools, and one high school in the Madison Metropolitan School District.

Finally, access to healthy food is another important consideration in healthy neighborhoods. Four full-service grocery stores are located within the West Area, with several more just outside the planning boundary. There are also several smaller convenience stores and ethnic grocers in and around the Area. Seasonal farmers markets occur at University Research Park and Hilldale Mall.

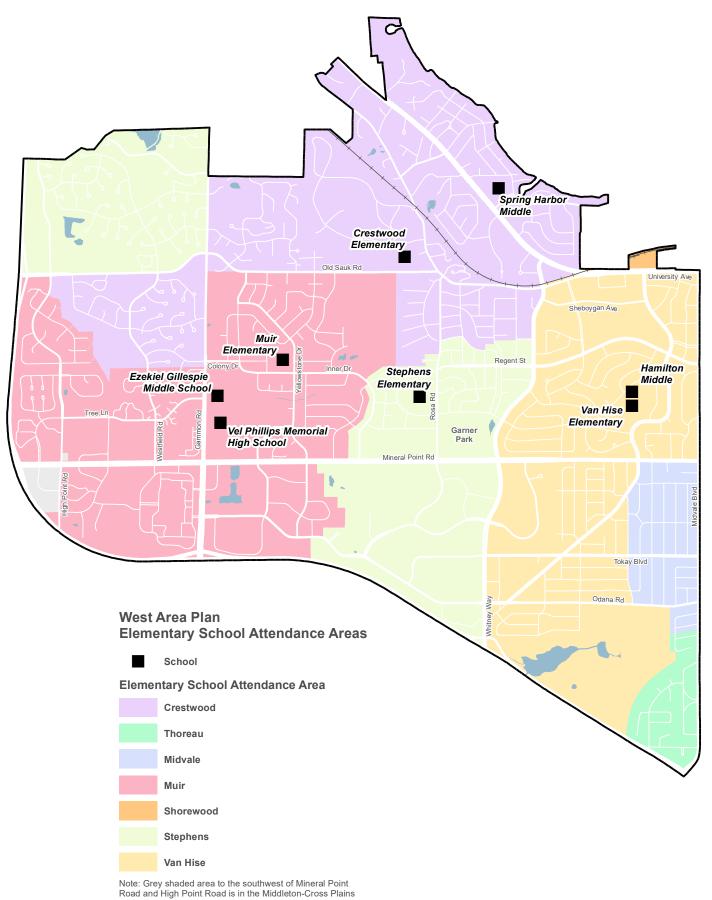
Missing Middle housing is a range of multi-unit or clustered housing types scaled between single-family detached houses and larger apartment buildings, such as rowhouses, small multifamily buildings, tiny homes, bungalow courts, courtyard apartment buildings, accessory dwelling units, and live-work buildings.

Complete neighborhoods are neighborhoods that include a range of housing types and costs, neighborhood-serving businesses, stores, and services; schools, and places of worship. Neighborhood components are ideally accessible by foot, bicycle, or transit through a network of well-connected streets and blocks, usable public spaces, and a system of connected parks, paths, and greenways.

Affordable housing is housing for which the occupants are paying no more than 30 percent of gross household income for housing costs, including utilities. Households with housing costs exceeding 30% of income are considered housing cost-burdened. The City of Madison's direct and development subsidy programs are primarily designed to reduce housing cost burdens for renter households with incomes at or below 60% county median income (CMI) and for owner-occupied households at or below 80% CMI.

The City of Madison Community
Development Division's Housing
Development & Financing Program provides loans to for-profit and non-profit housing developers for the construction and rehabilitation of new income and rent-restricted affordable rental and owner-occupied housing.

West Area Plan 3-1044



Road and High Point Road is in the Middleton-Cross Plains Area School District.

Actions

- Work with developers to encourage a wider mix of apartment unit sizes, including smaller units for younger and older households, as well as units large enough to support larger families (three or more bedrooms) in proximity to schools.
- Encourage and incentivize development of smaller-scale "Missing Middle" housing types in Low-Medium Residential (LMR) areas on the GFLU Map, especially in areas near schools, parks, and transit service, through zoning ordinance changes.
- For new housing partially funded by the City (through land banking, the Affordable Housing Fund, TIF) work with partners to distribute housing at costs affordable to a variety of households throughout the area, with a focus on areas along the BRT corridor.
- Development along the Beltline Highway should include a variety of noise mitigation improvements. Limit or entirely restrict City financial support for affordable housing in these locations, unless significant noise mitigation measures are included.
- 5. Increase access to information regarding First Time Home Buyer Assistance programs for rental and utility payment support, the City's property tax assistance program for seniors, and the City's low-cost mortgage and rehabilitation loans, and historic preservation tax credit programs for existing residents and those looking to move to the West Area.
- If the large residential parcels on Sheboygan Avenue or bordering Rennebohm Park redevelop, new residential development should connect to the park and surrounding amenities and include community spaces serving residents.

Partnerships

A. Increase awareness and access to existing community programs and services, especially for youth and seniors, and explore expanding them in such locations as the community rooms of new housing developments, Lussier Community Education Center, and through co-location of community-based organizations.

What We Heard

- The need for housing from public feedback and recommendations from underlying plans generally fell into the following five categories of desired housing types:
 - Affordable housing, including affordable homeownership opportunities.
 - A variety of tenancy structures in addition to just fee-simple ownership and rental.
 - c. A wider mix of unit sizes, including those large enough to support families with children.
 - d. Housing for all stages of life.
 - e. Smaller-scale multi-family buildings, including building types commonly referred to as the Missing Middle.
- The desire for more/expanded community facilities such as community centers, libraries, and daycare opportunities.
- Appreciation of quiet neighborhoods/streets that are still close to shops and restaurants.

Economy and Opportunity

Comprehensive Plan Goals

This Plan aims to advance the following goals from the Comprehensive Plan:

- Madison will have a growing, diversified economy that offers opportunity for businesses and residents to prosper.
- Madison will have equitable education and advancement opportunities that meet the needs of each resident.

The West Area plays an outsized employment role within the city and region. Of the Area's approximately 30,000 residents, approximately 15,000 are in the workforce. The Area hosted approximately 23,600 jobs in 2020, up by more than 3,000 over the previous decade (Census On The Map 2021). Job sectors where the Area has a higher proportion than the City as a whole include retail trade, finance and insurance, and professional/scientific/technical services. The area has fewer than its fair share of jobs in the educational services sector. Employees working in the West Area generally reflect citywide ratios as far as educational attainment, race, and ethnicity.

Major employment nodes include the approximately 4,200 jobs within University Research Park, which is a hub for biotechnology companies in Wisconsin. Many Research Park jobs are high-paying. The West Area includes two of the region's most successful retail areas: West Towne Mall and Hilldale Mall, both of which attract shoppers from around southcentral Wisconsin. There are numerous health care clinics, along with a concentration of dentistry practices at Old Sauk and Gammon Roads. Financial services, led by TruStage (formerly CUNA) along Mineral Point Road, are a significant employer as well. The State



TruStage recently invested in a new training building along Mineral Point Road, but the company's overall employment in the area is decreasing, resulting in demolition of one of their office buildings.

of Wisconsin continues to consolidate multiple state agencies into the new Hill Farms State Office Building, including WisDOT's headquarters. Additionally, the State of Wisconsin Investment Board has moved from downtown to a new office building in the Hilldale area.

Office Development

The COVID-19 pandemic significantly increased remote work, generally reducing office occupancy. This shift has heavily affected older office

space. Older ("Class C") office space in the Yellowstone-Grand Canyon Drive area is receiving increased interest from developers looking to bring multifamily housing and mixed-use development to an area that, until recently, has not had any residences west of Yellowstone Drive. Other areas with small, relatively old office buildings, like Midvale Boulevard south of Hilldale and the Odana Road-Medical Circle area, are also seeing redevelopment interest.

Pre- and post-COVID 19, the West Area has experienced a relatively strong market for new office space, led by the Hill Farms State Office Building, along with new lab space constructed by University Research Park and Exact Sciences' new headquarters in University Research Park. Navitus Health Solutions also anchors a large office building at the corner of Gammon and Mineral Point Roads that was completed in 2018.



The State of Wisconsin Investment Board moved from its downtown office to a new location within the Madison Yards development close to Hilldale Mall.

While TruStage has been a long-time employment anchor in the area and completed a major new training center in 2022, it has been reducing its workforce. It has demolished a major office building at the corner of Rosa and Mineral Point Roads.

Service and Retail Sector

The changing retail landscape, which was accelerated by COVID, is impacting the West Towne area more significantly than the Hilldale area, with small strip malls along the Odana Road corridor attracting developer interest for multifamily residential and mixed-use projects. A long-term trend of big box retail being adaptively reused for entertainment, fitness, and services started pre-COVID with Dave and Busters taking over a portion of a former Sears store and Urban Air Adventure Park taking over a



A former drive-through bank on Odana Road was redeveloped into the Urbana mixed-use project.

Jobs within area, by major sector

	West Area	,	of Madi	son
Agriculture, Mining, Utilities, Construction (NAICS #11-23)	3.7%	0.6%	3.8%	
Manufacturing, Transportation,		1.5 70	6.3%	
Warehousing (31-33, 48-49) Wholesale Trade (42)	16.0%		2.3%	
Retail Trade (44-45)			8.4%	
Information (51)		2.0%	2.7%	
Finance, Insurance, Real Estate (52-53)	13.3%		7.5%	
			8.7%	
Professional, Scientific, and Technical Services (54)	14.5%		6.5%	
Management of Companies, Administration & Support (55-56)	4.1%		14.6%	
Educational Services (61)	4.2%			
Health Care and Social Assistance (62)	19.3%		19.3%	
Accommodation and Food Services (72)	7.6%		5.4%	
Arts, Entertainment, Other Services (71, 81)	5.2%		4.6%	
Public Administration (92)	8.2%		9.9%	

% with 4+ years of college

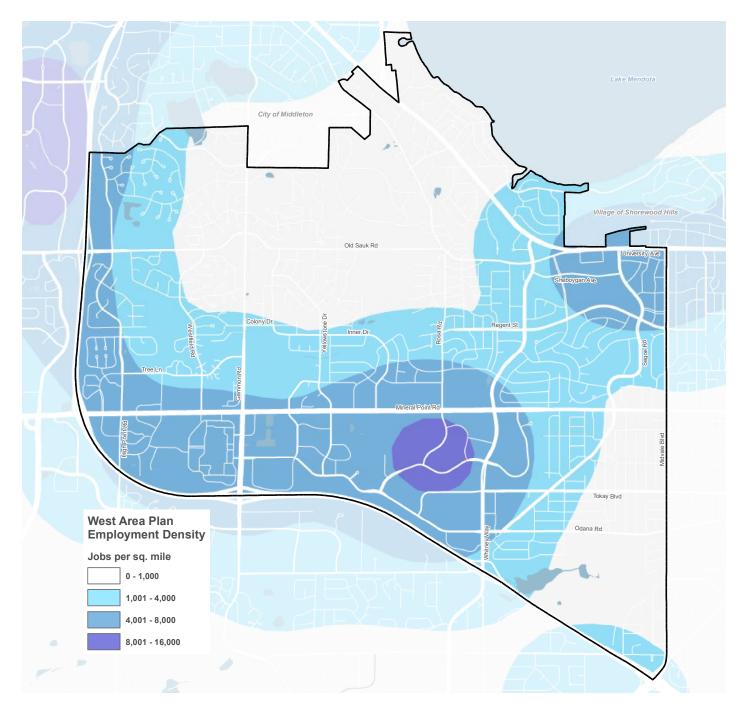
63.4%

54.9%

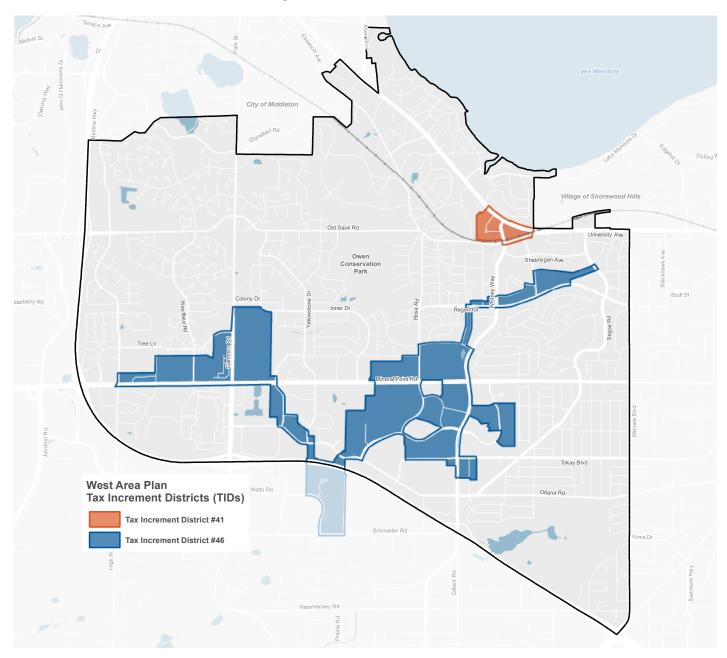


Two small office buildings along Midvale Boulevard were demolished to construct the Manchester on Midvale apartment building.

former Toys R Us store. The trend is continuing in the West Towne area, with Crunch Fitness moving into a former Shopko and MSCR filling a high-turnover space along the Beltline. T.J. Maxx moving across Whitney Way opened the former Westgate Mall property for redevelopment with almost 500 units of multifamily housing. Similar consolidation elsewhere in the West Area can both help fill vacant retail space and open land to address the city's ongoing need for housing. While these changes all illustrate the evolving retail landscape, they also demonstrate the overall adaptability, health, and desirability of the West Area, as many vacant or underutilized spaces are adaptively reused or redeveloped relatively quickly.



The restaurant sector is generally strong within the West Area, with a concentration of franchise locations in and around West Towne Mall. Local restaurants are more common to the east, along Odana Road and Whitney Way. Hilldale Mall hosts a mixture of local restaurants and higher-end national chains.



Actions

- Analyse land banking within the concentration of commercial development south of Mineral Point Road as a potential method for reusing existing buildings for affordable local business space (potentially as part of an incubator) and for redeveloping surplus surface parking into affordable housing.
- 2. Monitor the performance of TIDs #41 and #46 to determine whether amendments are financially feasible to fund affordable housing, non-assessable infrastructure (bicycle network improvements, pedestrian safety improvements, new streets, stormwater management improvements, etc.),

What We Heard

- Maintain some affordable space for small businesses as it changes and grows through increased redevelopment.
- Add to business diversity in areas currently lacking in services and restaurants through implementing additional mixed-use development.
- Finding staff can be a challenge in some sectors, with the lack of affordable housing a contributing factor.
- 4. The area has easy access to downtown and is convenient to shopping, dining, and many employment areas.
- 5. An appreciation of the current small businesses in the area and a desire for more local businesses, especially within walking distance of residents.

- creation of a business incubator, land banking, small business assistance, and other economic development initiatives in and around these TIDs.
- 3. As part of TID #41 and #46 evaluation, determine whether a commercial rent assistance or guarantee program to assist small and emerging businesses is financially feasible, particularly to allow BIPOC-owned businesses afford space in areas where rent increases may occur. Gap assistance could be used to help businesses displaced by redevelopment afford ground-floor space in new mixed-use buildings.
- 4. Form a staff team to review the possibility of a new TID in the West Towne area/Mineral Point Road corridor to support redevelopment, stormwater management improvements, and other non-assessable infrastructure that is consistent with City plans.
- 5. Increase affordable childcare options, taking advantage of existing commercial spaces where non-profit childcare providers could locate, ideally along transit corridors and close to employment areas.

Partnerships

- A. Explore the development of a business incubator, with lower cost space, shared resources and expertise needed to help grow local businesses.
 - i. Partner with business, economic development, and community organizations, such as Latino, Black, Hmong, and Madison Chambers of Commerce, Wisconsin Women Business Initiative Corporation, and established incubators, to ensure an incubator meets the needs of BI-POC-owned businesses and fosters opportunities for long-term success.
 - Consider integrating an incubator with land banking, using renovated commercial space to grow local businesses and retain affordable options.
 - iii. Incorporate networking, mentorship, and collaboration opportunities as part of business incubator planning.
 - iv. Integrate youth mentorship/training into incubator space, potentially in partnership with nearby Vel Phillips Memorial High School.
 - v. Explore creating a program similar to the Public Market's "Market Ready" as part of incubator planning to help new businesses and small businesses prepare for incubator occupancy.
 - vi. Work with local commercial brokers and property owners to connect small businesses to available space as they "graduate" from an incubator.
- B. As business areas transition to a more connected, pedestrian-friendly environment, assist the creation of a neighborhood business association or BID (business improvement district) to maintain amenities and facilitate events.
- C. Continue building relationships with and supporting economic development partners, especially those with ties to historically underrepresented communities. For example, the Madison Black Chamber of Commerce, Latino Chamber of Commerce of Dane County, and the Hmong Wisconsin Chamber of Commerce have vibrant memberships, and the City should collaborate with them, and other similar business and economic development groups, to help them grow opportunities for entrepreneurs to locate in the West Area.

Culture and Character

The Comprehensive Plan encourages vibrant and inviting places, preserving **Comprehensive Plan** historic and special places, creating safe and affirming community spaces, pub-Goals lic art, and providing opportunities to learn, create, collaborate, and enjoy the arts. This Plan seeks to lend further detail to those goals and strategies, realiz-This Plan aims to advance the ing that neighborhoods across Madison vary not only by size, but also through following goals from the Compretheir culture and character. The character of the area can be found in the activhensive Plan: ities at parks, community gardens, architecture of the built environment, pools, · Madison will be a vibrant and the work of community-based organizations, and other aspects of what makes creative city that values and a neighborhood a neighborhood. Building on and maintaining a sense of place builds upon its cultural and reflective of businesses, history, and connection between residents and their historic assets. community creates strong and resilient communities. This Plan looks to balance Madison will have a unique growth and change with historic and cultural character and strong sense of resources to preserve the Area's history place in its neighborhoods and while making it welcoming for current the city as a whole. and new residents. Sheboygan Ave Rennebohm Park **West Area Plan Community Gathering Places** Library/Community Center Place of Worship Recreation/Entertainment

The cultural assets of the Area significantly contribute to neighborhoods – these assets can come in the form of religious institutions, neighborhood institutions, schools, businesses, and other gathering places, both public and private. Preschool of the Arts, Madison Ballet, and the Camera Company are just some examples of the cultural assets in the planning area. Parks oftentimes host cultural events as well. For example, the Capitol City Band holds an annual summer concert series in the Rennebohm Park shelter that brings together residents of different cultures and ages, and Garner Park hosts the annual Opera in the Park event. More community events could be undertaken by establishing Neighborhood Improvement Districts to fund physical and placemaking activities within the area.



The vacant former Copps grocery store is in Urban Design District 3.



Madison Yards is within Urban Design District 6.

Urban Design Districts

Urban design is also an important contributor to culture and character. The city's Urban Design Districts contribute to the aesthetics of the built environment in addition some aspects of urban design regulated in the zoning code. The city's Urban Design Districts generally ensure development contributes to creating a high quality, well-designed built environment within certain areas of the city. There are eight Urban Design Districts that are primarily located along the city's major transportation corridors. Each Urban Design District (UDD) is unique

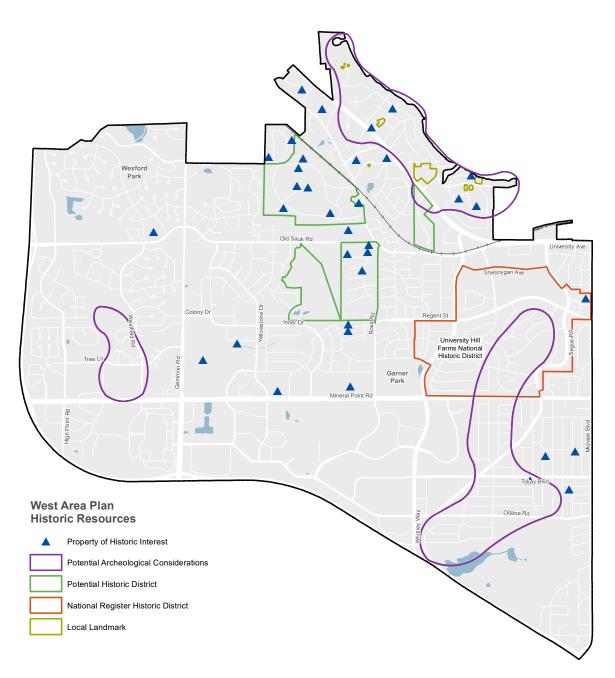
and establishes requirements and guidelines for new development, buildings, remodeling, or additions to existing buildings. Some districts are more prescriptive than others. While the requirements and guidelines are different in each district, generally they address building design (height, setbacks, and stepbacks), design and quality of exterior materials and architectural detailing, lighting, signage, the design of private open space, landscaping, and screening.

There are two UDDs in the West Area: UDD 3 and UDD 6. UDD 3 is southwest of the Tokay Boulevard and Whitney Way intersection and is characterized by commercial and employment development. UDD 6 is located along University Avenue from Middleton to University Bay Drive and is characterized by a wide variety of older strip commercial and newer mixed-use development, with some single-family residential included in the boundary in the western half of the district. This plan outlines actions related to the review/re-evaluation of UDD 3 and UDD 6 boundaries and the guidelines and requirements enumerated in the districts. While outlined in this plan UDD-related actions are intended to be part of a separate, citywide UDD review process.

Historic Resources

The West Area encompasses many sites and resources rich in history and culture. Residents expressed appreciation of these sites, the history of the West Area, scenic views from local vantage points, and the desire for more public art and art opportunities. There are several designated City of Madison landmarks or properties listed in the National Register of Historic Places as shown on the Historic Resources Map. Additionally, this Plan lists properties of potential architectural or historic significance and outlines the process if a property owner wishes to pursue designation (see the Historic Resources Review in the Appendix for further details). There are also archaeological sites in the West

Area, several of which are human burial sites. These sites may require additional review and consultation with the Ho-Chunk Nation and Office of the State Archaeologist. The archaeological sites could also be eligible for local or Federal historic designation. Properties listed in the National Register are potentially eligible for preservation tax credits for work on buildings, and burial sites are eligible for a property tax exemption. As part of this planning process, staff conducted a viewshed analysis of vantage points of the Capitol from publicly accessible locations within the Area. However, views from the Area were limited by existing development, street alignments, tree canopy, and topography.



What We Heard

- 1. Preserve the history and character of neighborhoods while looking to the future.
- Residents like and want to see more cultural diversity within the community.
- 3. More inclusive community events, spaces, and resources.
- 4. Expand arts infrastructure, opportunities, and spaces on the westside.
- 5. Integrate more public art throughout the area.
- 6. Preserve viewsheds of greenspaces, lakes, and the Capitol.

Actions

- Review UDD 3 (MGO section 33.24(10)) to determine whether it should be revised or repealed. Many requirements for UDD 3 development are adequately addressed by the current zoning code, which was adopted approximately 25 years after UDD 3. Other ordinance language governing UDD 3, such as the Statement of Purpose that says the requirements were established, in part, to "assure that future development will complement the existing development in the district," contradict the actions in this Plan, which expressly seeks to transition the area away from its current auto-oriented form.
- 2. Changes to UDD 6 (MGO section 33.02(13)) should be analyzed as part of a larger citywide UDD review to:
 - a. Revise the boundaries to not include single-family development within the district boundary and add existing multifamily development to the south of Sheboygan Avenue and existing commercial development between Hilldale Mall, Midvale Blvd., Regent St., and Sheboygan Ave. to the district boundary.
 - b. Align regulations with the TOD overlay zoning district.
 - c. Explore whether UDD 6 should be split into subareas to account for the widely varied characteristics and surroundings along the three-plus mile long corridor.
 - d. Consider establishing minimum and maximum building heights in some or all of UDD 6 (note there is already a two-story minimum height in the portions of the UDD that are within the TOD overlay zoning district).
 - e. Integrate design standards into the "Building Design" requirements to ensure buildings fronting shared-use paths address the path with architectural elements and building access at the same design level as the front of the building.
 - f. Add additional building and site design elements to the "Building Design" requirements to require ground floor residential units facing public right-of-way have individual unit entrances from the sidewalk.
 - g. Add an element to the "Building Design" guidelines to provide direct pedestrian connection(s) from buildings to adjacent public parks and/or shared-use paths.
- Integrate public art into upcoming public construction projects, including Sidewalk Poetry, Utility Box Art Wraps, and public art at BRT stations, along gateway corridors, and along bike paths. For large-scale construction projects, explore hosting workshops with the community to identify locations for public art.
- Implement the "Art Through City Building" concepts as described in the <u>City's Public Art Framework</u>, <u>Cultural Plan</u>, and other adopted policies to make the West Area more welcoming to people of every race, ethnicity, gender, age, and ability.

Partnerships

- A. Work with the property owners and managers of the apartments on Sheboygan Avenue to identify opportunities to partner on placemaking opportunities to enliven the area and engage the community.
- B. Work with neighborhoods eligible for creation of a local historic district and/ or a national register historic district to establish district(s). National Register or local historic district nominations should be initiated by the neighborhood, with technical assistance provided by the City. Note that National

Register districts allow residents to apply for tax credits, whereas local historic districts establish design requirements that shape future reinvestment and development in these neighborhoods (see the Historic Resources Review in the Appendix for more information).

- C. The property at 6405 Mineral Point Road hosts the historic Otto & Louisa Toepfer Farm house, which is now part of Otto's Restaurant & Bar. Should changes or development be proposed for this site, the property owner is encouraged to adaptively reuse the historic building.
- D. Encourage the creation of neighborhood-oriented events in underserved areas, such as block parties, food cart nights, or summer concert series, to bring residents together more frequently. Such events should focus promotion on underserved communities.
- E. Work with area property owners and nonprofits to add space for artist(s)-in-residence, affordable artist studios, exhibitions, performances, and rehearsals.
- F. Work with University Research Park and UW-Madison Arts staff to explore possibilities for locating arts space within or close to the Research Park. Space could take the form of studios, partnerships with businesses, an artist residency, or other formats.

Design Guidelines

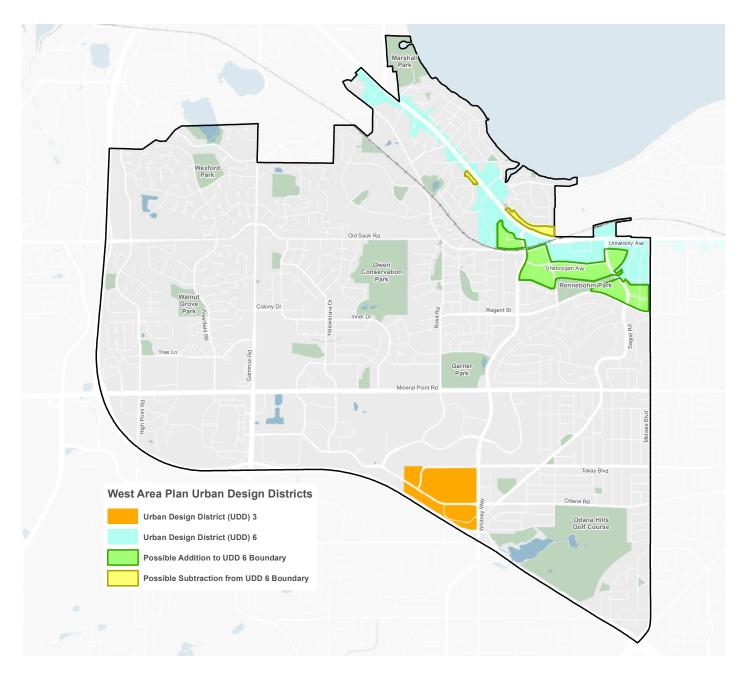
The West Area contains several prominent retail and employment destinations, like the West Towne Mall area, Hilldale area, and University Research Park, along with major multifamily developments, like Sheboygan Avenue. Throughout the Area redevelopment and development within mixed-use, employment, medium residential, and high residential land uses on the GFLU Map should reference the following design guidelines to create an engaging public realm with development that is more transit, pedestrian, and bicycle friendly than the traditional suburban style of development prevalent along some corridors within the Area.

Building Height, Stepbacks, and Setbacks

- 1. Buildings fronting "Commercial Core" areas, as shown on the GFLU Map, should be set back the minimum distance required by zoning to create a consistent, engaging, and walkable street frontage.
- Buildings not fronting Commercial Core areas should generally be minimally set back to maintain walkability and create easy pedestrian connections to the street and nearby transit. Substantial variations in setbacks between adjoining parcels should be avoided.
- 3. Buildings over five floors set back less than 10 feet should step back a minimum of 15 feet from the minimum setback line above the fifth floor along rights-of-way that are under 110' wide (for reference, Mineral Point Road and Whitney Way are over 110' wide, while Odana Road and Old Sauk Road are less than 110' wide).

Building Materials and Articulation

- 4. Material changes should not be made within the same plane without a programmatic change or a minimum notable relief.
- 5. Buildings should be designed so that uses, entrances, and design elements provide amenities to enhance the visual and pedestrian character of the street.
- 6. Blank building walls that lack architectural detail, variety, and windows along street-facing facades should be avoided.



- 7. Modulation to create porches, balconies, roof terraces, entrances, and other active uses are encouraged for residential units/buildings.
- 8. Porches and balconies should be well integrated within the design and provide usable open space for residential tenants.

Site Layout & Building Orientation

- 9. New buildings should be oriented on sites to maximize pedestrian, transit, and bicyclist use while shielding parking from the public realm.
- 10. All ground floor commercial uses shall have usable entrances facing a public street that are open to the public during businesses' regular hours.
- 11. Where possible, parking should be placed under buildings (below grade), in parking structures, or behind buildings. Parking should be shielded from streets by landscaping, walls, or fences, and should not be placed between a building and the street. Above-ground structured parking should be shielded from public streets by liner buildings.

- 12. All ground floor residential units facing a street shall have their own street entrance with a front porch/stoop.
- 13. Building entrances should be designed as the focal points of the front facade, and should utilize overhangs, porches, stoops or other elements to add a pedestrian rhythm to the street façade.
- 14. "Commercial Core" areas on the GFLU Map should have active ground floor commercial uses (retail, restaurant, service, or office uses) and are encouraged to have outdoor patios, dining, awnings, and display windows.
- 15. Where possible, locate the tallest mass of a building to minimize shading of the sidewalk/street (However, other guidelines should not be compromised to meet this element).

Green and Resilient

Comprehensive Plan Goals

This Plan aims to advance the following goals from the Comprehensive Plan:

- Madison will be a leader in stewardship of our land, air, and water resources.
- Madison will have a model park and open space system that preserves our significant natural features and offers spaces for recreation and bringing residents together.

The West Area's variety of parks and greenspace provide residents excellent opportunities to participate in a wide range of active and passive outdoor activities, from basketball to birdwatching. Several parks in the area attract residents from around the city and the region, like Odana Hills Golf Course, Owen Conservation Park, and Garner Park, with its Opera in the Park event.

This chapter also covers elements of a green and resilient city aside from parks, such as tree canopy cover and stormwater management. While other sustainability measures, like renewable energy and energy efficient buildings, also have a major role to play in a green and resilient city, they are more effectively addressed on a citywide basis through the Comprehensive Plan and Sustainability Plan. This Plan focuses on elements specific to the West Area.

Parks and Greenways

Overall, the Area has an excellent system of parks, with a total of 540 acres in 36 parks. There is approximately 18 acres of parkland per 1,000 residents. 78% of households are within a quarter mile of a park. However, there is an area south of Mineral Point Road lacking park access that is emerging as a new neighborhood with residential units replacing former retail and office develop-

> ment. This area around Grand Canyon Drive and Yellowstone Drive requires new park space to serve the growing number of residents that do not have access to nearby park space.

While the variety of amenities in the Area's parks are

appreciated by residents, continued investments will need to be made to keep up with the growing population of the area and the changing preferences of residents. For example, the City's first dedicated pickleball courts were added to Garner Park in 2016 to meet the exploding popularity of the sport, and a playground in Rennebohm Park was replaced with an inclusive playground in 2023. Community park amenities like those found in Garner and Rennebohm parks can attract residents from across the West Side. While mini and neighborhood parks tend to focus on serving the immediately surrounding neighborhoods, they can also sometimes feature unique recreational opportunities which can draw from a large area. Investments and changing amenities will be guided by Parks Development Plans (PDPs) for mini and neighborhood parks and master plans for community parks. The Parks Division completed West Area PDPs for all mini and neighborhood parks alongside the West Area Plan, and master plans for community parks are expected to follow over the next decade.

The West Area has about 207 acres of greenways that are maintained by the Engineering Division for stormwater

Types of Parks

A mini park is a small park (generally < 5 acres) with limited recreational activities that serves the immediate surrounding area.

A **neighborhood park** is generally 5+ acres and serves as the recreational focus of the neighborhood, with more recreational amenities than a mini park.

A community park is usually 20+ acres that is designed to serve/attract residents from beyond the surrounding neighborhood.

A conservation park is primarily managed to preserve Madison's native landscapes, plants, and animal populations for the careful use and enjoyment of visitors.

Type of Park	# of Parks	Acres in West Area
Mini Parks	14	28
Neighborhood Parks	8	65
Community Parks	5	140
Conservation Parks	3	118
Golf Course (Odana Hills)	1	171
Open Space/Not Classified	4	18
Total	36	540

management. In some limited cases parks and greenways overlap. While the main purpose of greenways are for stormwater management, they also provide significant wildlife habitat and tree canopy in some locations. Engineering has recently completed a series of watershed studies that cover the West Area to help prioritize greenway and stormwater management projects to address runoff from more intense storms due to climate change.

Trees and Canopy Coverage

Overall, the Area has about 31% canopy coverage, with most coverage to the north of Mineral Point Road and east of University Research Park. West Area parks and greenways – especially conservation parks like Owen – play a strong role in the area's tree canopy, but ultimately City terrace trees and canopy on private property play a larger role. The City does not regulate trees



Rennebohm Park playground



on single-family or duplex lots but does **Garner Park shelter** require landscaping plans for multifamily and commercial development. West Area Plan Parks and Open Space Odana Rd City of Madison Ponds/Greenways**** Conservation Park Mid-Block Passage Neighborhood or Mini Park** General Future Park Area*** City Golf Course Community Park* MMSD Schools * See details for numbered parks in the Parks section.

** Includes land designated as "Open Space." Follow adopted Parks Development Plans for Neighborhood and Mini Parks.

*** Acquire minimum of 5 acres in overlay Area A (out of 13 acres shown). Acquire minimum of ~10 acres in overlay Area B (out of 45 acres shown), in either one or two contiguous areas.

**** Land managed by City Engineering.

Current landscaping requirements for commercial development are more stringent than when most of the West Area originally developed. The requirements should result in more canopy on private property over the long term as properties, some of which have substantial surface parking lots, are redeveloped. Maintaining terrace trees is a citywide effort, but over the long term there will be an opportunity to add more terrace trees in the Area as properties are redeveloped and the planned street network is implemented. This will break up large blocks and expanses of surface parking.



Capital Avenue street end at Lake Mendota

Stormwater Management and Flood Reduction

Trees, parking, and greenspace all have an impact on stormwater management, which continues to be a concern within the Area. The City's stormwater management ordinance was revised in 2020 to require better stormwater management on redeveloping properties. Still, the Area contains some of the more challenging watersheds in the city for stormwater management, such as a portion of the Willow Creek watershed, which drains stormwater from a low point at University Avenue and Midvale Boulevard that would not drain if not for two large storm sewer pipes, one of which was completed in 2023. While significant upgrades in stormwater management have been made, more investment in the system is needed. The watershed studies completed in the Area and across the city will help prioritize the most effective solutions for

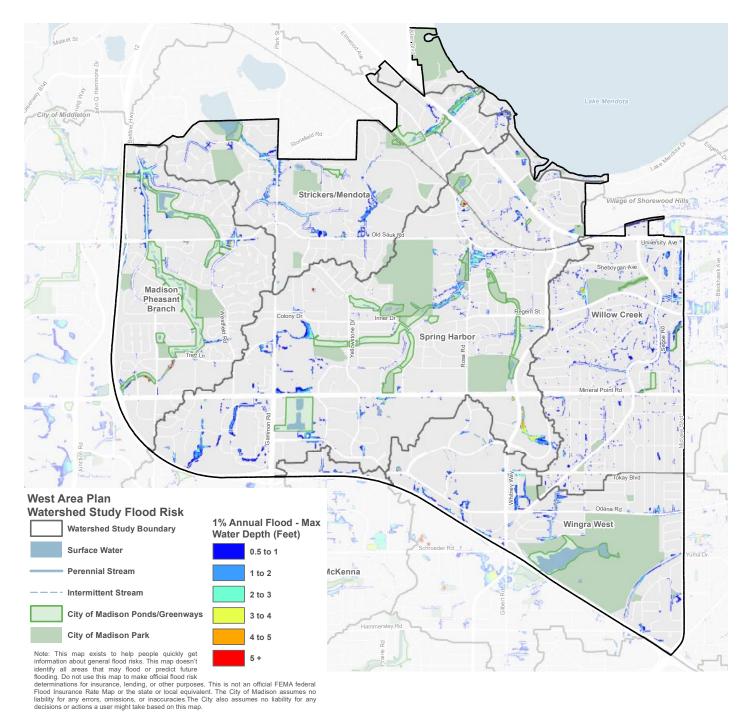
the City to implement in greenways and for storm sewers. With so much of the area developed as single-family housing, homeowners can also help improve water quality and decrease flooding in small events by implementing green infrastructure projects like rain gardens.

Water Quality

The City's 2020 update to its stormwater management ordinance, combined with recommendations to improve stormwater management in completed watershed plans, will help improve surface water quality. Compared to older parts of the city and areas with more of an industrial history, the West Area has relatively few sources of groundwater contamination, according to the Wisconsin Department of Natural Resources' Remediation and Redevelopment Database. The highest number of cases are associated with gas station and auto repair businesses that have been remediated and closed. An ongoing groundwater quality challenge is the chloride contamination from road salt that is impacting Well 14, along University Avenue. The Water Utility has modified the well to draw more water from the lower aquifer to reduce chloride. Additionally, the Streets Division has reduced winter salt use citywide. Annual use has dropped from the 12,000-16,000 ton range prior to the winter of 2014-15 to a 6,000-10,000 ton range since that time. While the reduction will help water quality in the long term, groundwater can see delayed results from changes to street salt use. If chloride concentration in Well 14 does not drop, a new well may need to be considered. The Water Utility currently owns land close to the Whitney Way-Mineral Point Road intersection that could be used to address the issue.

Urban Agriculture

The Area is largely developed. The largest undeveloped property is along Mineral Point Road between University Research Park and Oakwood Village. It is privately owned by TruStage, and is likely to be developed in the future. Overall, the Area lacks a substantial amount of space for urban agriculture. However, opportunities exist for smaller-scale community gardens on private land, such as the gardens at Madison Christian Community along Old Sauk Road, and through community group partnerships with the the City's Parks Division, as is in place for the community gardens at Rennebohm Park. Community gardens are allowed under the parks and recreation zoning district and all commercial, mixed-use, residential, and employment zoning districts.



What We Heard

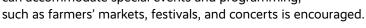
- Appreciation for the variety of parks, natural areas, greenways, and other greenspaces throughout the area, and the trails, recreational activities, and wildlife within those spaces.
- A strong tree canopy on private lots and public greenspaces, with a desire to retain and/or improve canopy coverage, especially south of Mineral Point Road.
- A mix of desires for adding more biking/mountain biking facilities in parks and greenspaces vs. a desire to keep those spaces bike-free.
- A desire for more facilities like drinking fountains and restrooms in parks and different/ additional recreational facilities/ amenities like more pickleball courts.
- A general goal to "sustain and maintain" current parks while also looking for opportunities to add more park space as additional housing is constructed.
- The importance of proper stormwater management, reducing pesticide use, and reducing salt use to maintain or improve water quality.

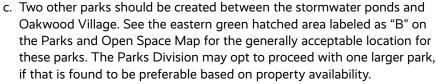
Actions

Parks

- 1. Garner Park:
 - Explore additional trails in Garner Park as part of any future improvements for the park.
- 2. Rennebohm Park:
 - Require dedication of additional parkland as part of any redevelopment of land adjoining the park that increases the number of residential dwelling units.
 - b. Consider the following elements as part of future upgrades to the park: addition of a water play feature, improvements to the shelter, additional seating near the shelter/tennis courts/paths, and path lighting.
 - c. Provide pedestrian and bicycle access to the park from Sheboygan Avenue, potentially as part of additional north-south and east-west streets (see planned future streets in the Land Use and Transportation chapter).
- 3. Marshall Park: Increase access to the park from the south by constructing a pedestrian/bicycle connection to the park from Lake Mendota Drive/ Camelot Drive. Note: the line shown on the Shared-Use Path & Bicycle Network map represents the connection but may not be the final route.
- 4. Implement changes in Odana Hills Golf Course and adjacent park land to:
 - a. Reinvest in the Course in a manner that is aligned with the Task Force on Municipal Golf's recommendations to provide a sustainable, high-quality 18-hole course that is environmentally and financially sustainable and inclusive of year-round diverse golf park programming.
 - b. Work to integrate stormwater management into the Course to protect neighboring properties, enhance the natural environment, and build off of volunteer-driven efforts to promote a more sustainable landscape.
 - c. Replace the existing clubhouse with a year-round, fully accessible facility that serves a broader public purpose beyond golf.
 - d. Take a holistic approach to managing Odana Hills Park (west), Odana Hills Park (east), and Odana Hills Golf Course to expand the variety of recreational opportunities available throughout the combined properties.
- 5. Zook Park: Increase connectivity between the park, surrounding neighborhood, and the Southwest Commuter Path via a paved path.
- 6. Initiate Park Master Plans within the next 10 years for community parks in the planning area (Garner, Marshall, Rennebohm, Walnut Grove, Wexford).
- 7. Implement <u>adopted Park Development Plans</u> for neighborhood and mini parks over the next 15 years.
- 8. Improve public lake access at lake access courts and parks north of Lake Mendota Drive.
 - a. Construct an ADA-compliant accessible path at the most appropriate location to increase inclusive lake access.
 - b. Add signage (in addition to the existing standard green street signs) to indicate public lake access.
 - c. Work with the Ho-Chunk Nation to place educational plaques at courts and/or along Lake Mendota Drive to describe and illustrate the history of the Ho-Chunk Nation in the area.
- 9. Create a small neighborhood park with a variety of outdoor recreation opportunities south and west of University Avenue and west of Spring Harbor

- Drive to serve this part of the West Area, which is underserved by existing parks.
- Look for opportunities to install recreational biking facilities in the Area, such as skill tracks, pump tracks, and single track biking facilities, in alignment with Madison Bicycle Adventure Trail (MadBAT) goals.
- 11. Create three new parks that are a minimum of five acres each to serve future residents in redeveloping commercial areas. The General Future Park Area overlay shown on the map designates the general area within which a smaller portion of the land will be sited for a park or open space in the future. The large general overlay is necessary because at this point it is not known where the park and open space will be sited.
 - a. One park should be as centrally located as possible amongst any future redevelopment of West Towne Mall and the big box stores to the west of the mall. While the block containing the Von Maur site would be ideal if the mall were redeveloped all at once, Von Maur is the most recent investment in the area and therefore likely to remain over the mid- or longterm. Alternative locations are shown within the overall western "general future park area" labeled as "A" on the Parks and Open Space Map. This park may need to be two smaller parcels connected by a green corridor.
 - An urban feel, like Lisa Link Peace Park or McPike Park, should be considered for the western park (A on the map), due to its location amidst more intensive planned mixed-use development and the possibility of serving residents, patrons of nearby businesses, and visitors to the area. A design that can accommodate special events and programming,





- d. Park space should be acquired through purchase of property and through parkland dedication which is required of new residential development. Parkland dedication may be required of projects when they are proposed adjacent to dedicated parkland in either area A or area B on the map.
- e. The eastern park(s) (B on the map) may be developed with a focus on serving the local neighborhood and would ideally include park space adjacent to the major stormwater facility south of Mineral Point Road and west of Gammon Place.
- f. Future designs and amenities for new park land will be determined via park development planning processes undertaken by the Parks Division. Such a planning process will proactively engage a diverse group of residents, including students from nearby Ezekiel Gillespie and Vel Phillips Memorial schools. The park planning process will consider amenities and park names that are welcoming to BIPOC residents.



Marshall Park



Garner Park Pickleball courts

Trees and Canopy Coverage

- 12. Enhance the tree canopy on public right-of-way. The biggest opportunity for increasing street tree canopy on existing streets is within University Research Park.
- 13. As recommended in the Complete Green Streets Guide, terrace widths on new streets planned for the area should be wide enough to accommodate healthy canopy trees and/ or bioswales, typically requiring a minimum of 10 feet. Expansion of existing substandard terraces (generally less than eight feet) should be accommodated through additional dedication of right-ofway whenever possible.
- 14. Work with Oakwood Village to explore the prospects of an official, publicly accessible "green corridor" connecting Nautilus Point Park through the Oakwood campus to planned City park space in the Grand Canyon Dr. Yellowstone Dr. area and to the stormwater ponds to the west.
- 15. Evaluate stormwater utility parcels for plantings and ecological restoration consistent with the Stormwater Utility Vegetation Management Plan once adopted.

Stormwater Management and Flood Reduction

- 16. Implement recommendations included in the City's watershed studies to reduce flooding and create a more resilient stormwater management system. Pursue federal funding for stormwater management improvements in watersheds that face the most severe challenges, such as Willow Creek.
- 17. Focus City funding for public and private green infrastructure projects on watersheds that face the most severe challenges, such as Willow Creek and Spring Harbor, or in areas near the lakeshores or have concerns with water quality. Pair funding with increased publicity efforts surrounding City initiatives to boost green infrastructure on private property.
- 18. Implement innovative stormwater management techniques as part of future right-of-way design for both new street construction planned for the West Towne area if/when any redevelopment occurs, other new planned streets in University Research Park, and reconstruction of existing streets.
- 19. Projects in the four planned blocks directly south of the entrance to Sunset Memory Gardens should explore placing stormwater management measures mid-block, as a contiguous feature with accompanying mid-block pedestrian connections (see the "Mid-Block Passage" labeled "C" on the Parks and Open Space Map).
- 20. Consider integration of stormwater management into future park space for large storms as a natural feature in a way that does not compromise the amenities and use of the park outside of large storm events.
- 21. Work with the City of Middleton on cross-border stormwater management issues, especially regarding the Pheasant Branch watershed and Stricker Pond.
- 22. Work with the Village of Shorewood Hills on improving stormwater management in the Willow Creek watershed to reduce incidences of flooding along University Avenue and Midvale Boulevard in the face of increasing annual rainfall.

Sustainability & Urban Agriculture

23. Remove the north side of Vernon Boulevard in favor of additional greenspace for a community garden or civic space.

Partnerships

- A. While the City has a significant role to increase the West Area's canopy coverage from its existing 31% to the 40% goal established in the <u>Urban Forestry Task Force's report</u> through plantings on public property, the increased coverage will not be achieved without action on private property. The City should work with major property owners in the West Area, such as MMSD and University Research Park, to increase canopy on non-City land.
- B. Explore partnership opportunities with community groups to create additional community gardens, especially in proximity to concentrations of multifamily housing.

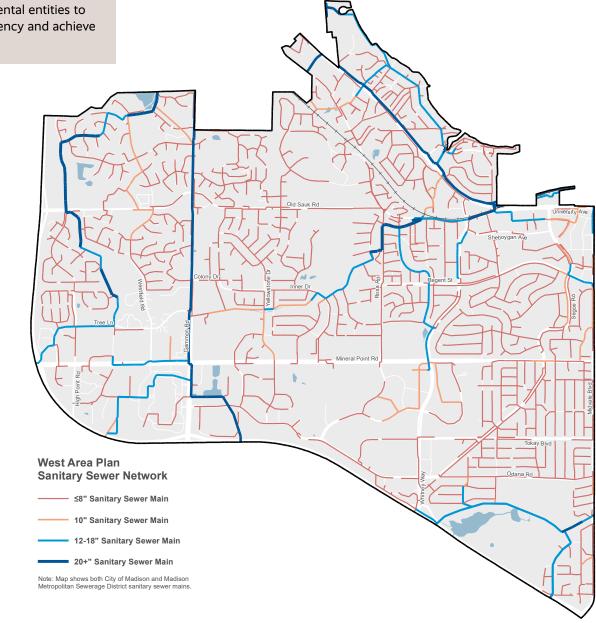
Effective Government

Comprehensive Plan Goals

This Plan aims to advance the following goals from the Comprehensive Plan

- · Madison will have efficient and reliable public utilities, facilities, and services that support all residents.
- Madison will collaborate with other governmental and non-governmental entities to improve efficiency and achieve shared goals.

The West Area has been largely developed since the mid-1990s. With that has come the full complement of City services, such as police and fire protection, utilities, waste management, snow removal, and libraries. The City will continue to provide a full range of services to all neighborhoods, while optimizing where community facilities are located in the Area and surrounding areas. Optimization is needed to balance provision of services with the long-term costs to operate facilities and the growing and changing population of the West Side. For example, the the Police Department's West District was split when the Midtown Police Station was completed in 2018 to better cover West Madison due to rapid growth on the far West Side. A public works campus is also taking shape to the west of the Beltline to improve service delivery on the West Side as land continues to develop west of the Beltline.



54

Redevelopment of retail and office uses into residential results in changing demands for City services and infrastructure. For example, City sanitary sewer mains may need upsizing as redevelopment transitions low-density commercial and office space to mixed-use development or multifamily residential development. Upsizing of sanitary sewer mains is most likely to be needed when properties are served by a main of eight inches in diameter - the smallest mains in the Area. Additional adjustments to services will likely be needed in the future as the West Area, and the city, continue to grow.

Maximizing the efficient provision of facilities and services can be achieved through careful coordination among City agencies and with other municipalities. Examples include some residents on the north side of the Area visiting the Middleton Library and the City of Madison Fire Department providing service to the Village of Shorewood Hills. Such cooperation can help avoid costly duplication of services. A further important focus is to provide customers, especially underrepresented populations, with the tools to effectively gain access to Madison's services and resources.

What We Heard

- Residents expressed a desire to continue engaging with the City as details and designs are developed to implement plan actions.
- 2. Communication around plan implementation is needed.
- 3. Access to schools, libraries, and other amenities is appreciated.

Actions

- Review sanitary sewer capacity needs related to anticipated development/ redevelopment and pair capacity upgrades with road construction projects where possible.
- 2. Study water utility service area #8 (West Towne area and north of West Towne) to determine whether a new well is needed to serve the area.

Partnerships

- A. Work with MMSD and the Lussier Community Education Center to facilitate expansion of Lussier to provide additional community space and services to area residents and students.
- B. Explore opportunities to secure/reserve space to serve as a community center, neighborhood center, or meeting space for future residents of a new Odana neighborhood, potentially in conjunction with, or close to, new park space.



Lussier Community and Education Center

Health and Safety

Comprehensive Plan Goals

This Plan aims to advance the following goals from the Comprehensive Plan

- Madison will be a place where all residents have equitable health outcomes.
- Madison will be a place where residents and visitors are safe at home and feel welcome in the community.

Madison strives to be a place where all residents have equitable health outcomes, residents and visitors are safe, and all feel welcome in the community. To support these goals, the Comprehensive Plan encourages more community partnerships, better access to mental and physical health services, healthy and safe living environments, and public health approaches to violence prevention.

According to the <u>Neighborhood Indicators Project 2022 data</u>, the West Area is relatively safe compared to the city as a whole. Despite having 11% of the City's population and a significant amount of commercial development, only 9% of property related offenses (which includes thefts, burglary, stolen vehicles and such other offenses) were reported. The Area also only had 6% of the City's crimes against people and 5% crimes against society. Crimes against society include narcotics, liquor law, gambling, weapons and prostitution.

The Area also had only 11% of citywide crashes, 12% of emergency medical services calls, and 9% calls for Fire service. With regard to infant health, several measures indicate the Area is doing very well compared to the rest of the city. Only 11% of births are pre-term and only 10% received less than adequate prenatal care. The Area also had very few property maintenance and zoning violations, with only 5% of reported citywide violations.

The following actions support health and safety for residents, with a focus on physical changes. This includes building off of strategies and actions the City is already engaged with through the All Ages and Abilities Bike Network Plan, Vision Zero Program, Violence Prevention Plan, and Community Health Improvement Plan. Implementation of these initiatives will help improve health and safety in the Area through things like neighborhood friendly infrastructure and street design, building safe public spaces by providing buffers and green spaces that separate public areas from transportation corridors, providing protected on-street facilities for vulnerable users in street design, and focusing on All Ages and Abilities facilities when possible while encouraging human centered transportation design such as slow streets and shared streets, and closing gaps in the pedestrian and bicycle network. Health and Safety also covers equitable outcomes for residents, and this includes continued citywide efforts that will support Area residents such as expanding resources to build and maintain a strong, connected fiber network and radio communication system for existing and future City operations.

Actions

- Monitor driving behavior and speed and enforce traffic regulations on streets along school routes including N. Gammon Rd., Tree Ln., Old Sauk Rd., Rosa Rd., Segoe Rd., and other areas where people walk frequently and feel endangered and/or are experiencing higher levels of serious traffic related injuries and fatalities (see the High Injury Network map).
- 2. Conduct Vision Zero speed analyses on Indian Hills, High Point Rd., Tree Ln., Whitney Way., Mineral Point Rd., Old Sauk Rd., Rosa Rd. and Segoe Rd. and explore potential traffic calming improvements such as road diets and speed bumps; pair these actions with speed enforcement.
- 3. Increase opportunities for West District Police and community members to engage and build trust with residents of color and people living with lower incomes including the Tree Ln., Oakbridge and Wexford Ridge areas. Activities could include Coffee with a Cop, and Police participation with youth recreation programs, block parties and cultural events and activities.
- 4. Include the Tree Ln. and Wexford Ridge neighborhood as a potential new Neighborhood Resource Team area to improve equitable outcomes by focusing City resources and building stronger community connections for the growing population of people of color and those living with lower incomes.
- 5. Encourage new buildings within 200 feet of the Beltline to use materials and insulation that keep interior noise levels below 52 decibels as per state code TRANS 405. Design sites to shield outdoor spaces from noise above 67 decibels. Developments near the Beltline should follow setback and noise regulations outlined in Wisconsin statutes (TRANS 233, TRANS 405), and MGO Section 16.23.

Partnerships

- A. Work with MSCR and Lussier Community Education Center (LCEC) to improve access to health and recreation programs at MSCR Odana, LCEC, schools, and other locations such as walking/running groups, yoga, meditation, and healthy cooking and nutrition classes.
- B. Work with resident leaders to continue to monitor environmental factors that may contribute to, or protect against violence (e.g. dimly lit areas, abandoned buildings, increased green space, etc.) and use violence prevention resources such as the Madison Dane County Violence Prevention: A Roadmap to Reducing Violence Report to address related issues.
- C. Work with the National Interscholastic Cycling Association and other organizations that offer recreational activities outside of schools, to welcome, encourage, and support participation by middle and high school students of color.
- D. Expand options in the Area where residents without air conditioning can go to get relief from the heat by partnering with LCEC, area businesses, centers of worship, and others to establish them as City-recognized cooling sites.
- E. Partner with Pepartnership Inc. to establish a personal essentials pantry in the Tree Ln. and Wexford Neighborhood area.
- F. Create partnerships between neighborhood groups, YMCA, MSCR and area schools to establish youth recreation programs at Haen Family Park and nearby school open space for activities like flag football, basketball and soccer.
- G. Work with residents, neighborhood groups, Madison Arts and non-profit organizations to activate West Area parks and other greenspaces in Oakbridge, Tree Ln., and Wexford Ridge areas for people of all ages with musical performances, community meals, arts activities and recreation.

What We Heard

- There is speeding and unsafe driving on some area roads, which makes walking and biking unpleasant, especially for kids walking, biking and skateboarding to school and people with limited mobility.
- 2. Low lighting along school routes is a concern.
- 3. Pockets of gun violence and stolen cars exist in the area.
- 4. There is a lack of information sharing and communication between the City and community.
- 5. Activities for teenagers are lacking.
- People of color and those living with lower incomes can tend to feel distrustful of Police due to negative experiences.

Appendix

Historic Resources Review57	7
Plan Implementation Summary62	2
Glossary73	3

Historic Resources Review

Staff completed a "windshield survey" (observing properties while driving through the area) on November 16, 2022 to assess historic resources in the West Area. Several properties with potential architectural significance were noted during the survey. Staff conducted additional research using files from the Wisconsin Historical Society, City of Madison preservation and building permit files, historic phone directories, and US Census records to supplement the windshield survey.

There are several known archaeological sites throughout the planning area, with most relating to early Native American history. These sites are either mound locations, which are culturally significant human burial sites, or historic village locations. Some of these sites have been disturbed by subsequent development, but further ground-disturbing work may require additional review and consultation with the Ho-Chunk Nation and the Office of the State Archaeologist. While there is a concentration of archaeological sites along the shore of Lake Mendota, there are other sites throughout the planning area, as Madison has been the site of long-term human occupation. There is a Human Burial Site Tax Exemption process for properties with known human burial sites that makes the undevelopable portion of a property tax exempt. Owners of such property should contact the Wisconsin Office of the State Archaeologist and the Madison Assessor's Office.

The area also contains six designated City of Madison landmarks: three for history, one for architecture and history, and two for archaeology. The West Area also contains the University Hill Farms National Register Historic District. Being listed on the National Register is an honorary designation that comes with Federal financial incentives to encourage adaptive reuse of historic buildings. The City's Preservation Planner identified 35 individual properties that warranted additional investigations to explore either National Register or local historic designations. There are also four neighborhoods that warrant additional investigations for possible National Register or local historic district designation: Crestwood, Blackhawk Park, Highlands, and Odana Hills.

Crestwood was developed in the 1930s and contains an excellent collection of Midcentury architecture with houses designed by Frank Lloyd Wright, Marshall Erdman, and Norman Kandl. It is also the location of the Carson and Beatrice Gulley House. Carson Gulley was a noted African American chef and TV personality. His story is detailed in the City's <u>Underrepresented Communities Historic Resources Survey</u>.

Blackhawk Park has previously had an intensive survey and the Wisconsin State Historic Preservation Office has determined it to be eligible to the National Register of Historic Places. This area is currently known as the Trillium Homes Condominium Association but was originally developed by the Harnischfeger Corporation with simple Minimal Traditional style, post-World War II detached housing.

The Highlands has a diverse collection of architect-designed homes from a variety of time periods. Given the number of properties called out as possibly being architecturally significant, it is likely worthwhile to investigate a possible National Register historic district designation.

Odana Hills features a grouping of early Midcentury Modern residential architecture that may warrant additional investigations into possibly becoming a National Register Historic District.

There are several individual properties and some neighborhoods that could benefit from having their story officially documented through the National Register process. As it is an honorary process, it does not place additional regulations on private property owners but does provide additional considerations for government-funded or permitted projects for properties and opens the possibility for preservation tax credits to help property owners adapt their properties for new and ongoing uses.

Historic Preservation does not prevent development or changes of use, but it does shape how redevelopment happens. It is not a tool for exclusionary zoning. Properties with possible historic value are opportunities for adaptive reuse of places of architectural or historic significance so that these places could be sensitively integrated into the ongoing growth of Madison. Sharing Madison's unique culture and character through historic preservation, representing the history of Madison's diversity, and building reuse as an environmental sustainability strategy are priorities in of the Madison Preservation Plan.

Designated Landmarks

Name and Address	Area of Significance	Notes
Thorstrand Estate/Mary North House 1 Thorstrand Rd	History	Daughter of Magnus & Annie Swenson. One of two houses on family estate, both designed by Law & Law
Thorstrand Estate/ Magnus Swenson House 2 Thorstrand Rd	History	Founder of UW College of Agriculture, advances in sugar production
John R. Commons House 1645 Norman Way	Architecture & History	House designed by Cora Tuttle. John Commons was a UW economist, significant contributions to labor history and social legislation
Hickory Hill House 1721 Hickory Dr	History	Built in 1842 by John Hicks. Home of noted conservationist, Walter Scott, for 40 years with significant tree plantings on the property during his tenure
Merrill Springs Mound Group II	Archaeology	Listed in the National Register for its significance in Native American history
Spring Harbor Mound Group	Archaeology	Listed in the National Register for its significance in Native American history

National Register

Name and Address	Area of Significance	Notes
University Hill Farms	Architecture & History	Listed in the National Register for its significance in Urban Planning and its Midcentury architecture.

Properties of Historic Interest

Name and Address	Area of Significance	Notes
Crestwood Neighborhood	Architecture & History	Early neighborhood association, collection of International and Midcentury architecture, should explore for possible National Register of Historic Places designation
Thomas & Eva Laufer House 5710 Arbor Vitae Pl	Architecture	International style, designed by Marshall Erdman
Mabel & Rolfe B. Sawtelle House 5705 Arbor Vitae Pl	Architecture	International style
John S. Bordner House 5746 Bittersweet Pl	Architecture	International style, designed by Norman Kandl
Beth Whitaker & Mary Schlaefer House 5818 Anchorage Ave	Architecture	Wrightean style, designed by Marshall Erdman
Eugene & Mary Van Tamelen House 5817 Anchorage Ave	Architecture	Usonian style. Frank Lloyd Wright architect, Marshall Erdman contractor
St. Dunstan's Church 6205 University Dr	Architecture	Midcentury Modern, Graven, Kenney & Iverson architects, Wengler addition
Eric Vogelman House 6109 Quetico Dr	Architecture	Vogelman's house and architect for Midcentury/Wrightean structure
Louise O. Kloepper & Mary A. Lescohier House 1621 Capital Ave	Architecture & History	Midcentury Modern/Wrightean structure, William Westley Peters architect. Designed for Mary Lescohier in recognition for her support of Monona Terrace in the 1950s
Dale Heights Presbyterian Church 5501 University Ave	Architecture	Midcentury Modern church, William Kaeser architect
Blackhawk Park Historic District 1 Craig-147 Craig Ave	Architecture & History	Determined eligible to the National Register as a historic district for its development history and architecture
Erdman and Associates Office and Shop 5117 University Ave	Architecture & History	Usonian style. William Kaeser architect, Marshall Erdman builder. Offices and shop of Marshall Erdman's architecture firm
Warner Farmhouse/Merrill Springs Farm 5010 Risser Rd	History	House constructed in 1858 for Rev. Louis Jenkins. Property named for longtime caretaker, Alf Merrill
Sunflower House/James Dresser House 5126 Tomahawk Tr	Architecture	Expressionist style house built by architect James Dresser as his residence.
Merrill Spring Park & Cistern 5102 Spring Ct	Engineering & Landscape Architecture	Sandstone cistern constructed by the Wisconsin Emergency Relief Administration (WERA) in 1934
Harold & Lorraine White House 1705 Hickory Dr	Architecture	Tudor Revival style Sears & Roebuck kit house with excellent historic integrity
David Robert Cheney House 6214 N Highlands Ave	Architecture	Wrightean house designed by engineer David Cheney using wood reclaimed from an earlier bridge project.
CUNA Mutual Insurance Building 5910 Mineral Point Rd	Architecture	International style office building, William Kaeser architect
Marbella Apartment Complex 6302 Mineral Point Rd	Architecture	New Formalist apartment building constructed in 1972 as a luxury apartment project. James Browing architect, Keith Parr landscape architect.

Name and Address	Area of Significance	Notes
James Madison Memorial High School 201 S Gammon Rd	Architecture	Midcentury Modern educational building. Grave, Kenney & Iverson architects
Orley W. Jensen House 6605 Gettysburg Dr	Architecture	Shed style residential structure
Owen Park 310 Jetty Dr	History & Archaeology	The caretakers house and rootcellar remain on the property and there is the potential for archaeological information in the former location of the house and barn. This property was associated with Professor Edward Owen, experimental agricultural uses in the City, and private development of the area with the intention of it becoming a park.
Highlands Mendota Beach School 5930 Old Sauk Rd	Architecture	Midcentury school, William Kaeser architect
George Kaeser Residence 5725 Old Middleton Rd	Architecture	William Kaeser designed this Wrightean residence for his brother George Kaeser
William Grubb Farmhouse/ W.H. Lighty House 6010 S Highlands Ave	History	Farmhouse constructed ca. 1860 for William Grubb. Property was later purchased by W.H. Lighty who developed correspondence courses for the UW Extension Office, beginning the education program for the Extension service.
Ronald & Louise Mattox Residence 1115 Willow Ln	Architecture	French Provincial style residence designed by architect Jerome Cerny
Professor Edward & Laura Kemers Residence 1102 Willow Ln	Architecture	Arts & Crafts style house designed by architect Frank Riley
Susan King & Robert Horowitz House 6217 N Highlands Ave	Architecture	Postmodern residence designed by architect Hardy Holzman Pfeiffer
Charles W. Curtis House 1406 W Skyline Dr	Architecture	Usonian house constructed in 1959 by architect Marshall Erdman
Keith Yelinek House 1 Larch Cir	Architecture	Shed style residential structure constructed by Wil-Mac Builders
Gertrude Herman House 1425 Skyline Dr	Architecture	Wrightean house designed by Dohm Construction
Highlands Neighborhood	Architecture	Collection of significant architectural residences should be explored for National Register eligibility
Madison Christian Church 7118 Old Sauk Rd	Architecture	Exaggerated Modern religious architecture, designed by Strang Partners
Carson & Beatrice Gulley House 5701 Cedar Pl	History	Associated with the life of noted African-American chef and television personality, Carson Gulley
Roger E. McMullin House 4509 Ames St	Architecture	Usonian house constructed in 1955 by architect Roger McMullin as his residence
Vic H. Peterson House 509 Piper Dr	Architecture	Usonian house designed by builder V.H. Peterson in 1953
Muriel & Dr. Royal Rotter House 614 Odell St	Architecture	Wrightean house designed by builder Herman E. Postweiler in 1955. Explore significance of houses at 610 and 618 Odel as possibly same builder with different architecture than rest of neighborhood. Perhaps a mini district
Oswald B Anderson House 602 Hilltop Dr	Architecture	New England Colonial Revival house built in 1958 by builder F.J. Vivian

Name and Address	Area of Significance	Notes
John Lundeen House 5701 Lake Mendota Dr	Architecture	Organic architecture, H. Fritz architect, house was sited and constructed so as to protect the existing trees on the property, which required a variance when it was constructed in 1969.
Odana Hills Neighborhood	Architecture	Collection of early Midcentury residential buildings. Further investigation of possible National Register Historic District is recommended.
Anchor Bank Building 302 N Midvale Blvd	Architecture	Anchor Savings & Loan was designed by Graven, Kenney, & Iverson. Constructed in 1966 and expanded in 1979, it is an excellent example of Contemporary Architecture.

Plan Implementation Summary

	Land Use Actions	Agencies
1	Update the Comprehensive Plan Generalized Future Land Use Map to be consistent with the Future Land Use map in this Plan.	Planning
2	Rezone properties identified in the Proactive Rezoning map to implement the goals of this Plan and encourage development consistent with the actions in this Plan. Ensure all impacted property owners are aware of potential zoning changes.	Planning, Zoning
3	Review Planned Development (PD) zoning covering development along and/or west of Gammon Road for a potential transition to conventional zoning to simplify resident applications to the City for minor projects and simplify staff administration.	Planning, Zoning
4	Implement maximum building height recommendations shown on the Generalized Future Land Use Map via zoning, with a mechanism for allowing bonus stories for developments that achieve specific affordability or sustainability parameters, similar to downtown height bonuses in City ordinances (Madison General Ordinances 28.071(2)(a)2).	Planning
5	Create a new commercial and employment zoning district with limited allowable residential development for areas along the West Beltline Highway and similar areas elsewhere in the City.	Planning, Zoning
6	Require ground floor commercial uses in the locations shown as Commercial Core on the Future Land Use Map.	Planning
	Transportation Actions	Agencies
1	Close gaps in the sidewalk network shown on the Sidewalk Network Map through the Safe Streets Madison Program, as streets are reconstructed, and as priority sidewalk additions can be constructed through state and federal grants (see priority sidewalk actions 4 b. and 5 c. below)	Engineering, Traffic Engineering
2	Implement the pedestrian and bicycle improvements shown on the Shared-Use Path and Bicycle Network Map. Further description is included below with the actions corresponding to the numbers on the map.	Engineering, Traffic Engineering
3	Vel Phillips Memorial High School and Ezekiel Gillespie Middle School	
a.	Study the addition of a left turn arrow from northbound N. Gammon Rd. into Tree Ln. to the traffic signal at the Tree-Gammon intersection.	Traffic Engineering
b.	Add a shared-use path along the east side of N. Gammon Rd. from Mineral Point Rd. to the City of Middleton. Starting at Tree Lane or Colony Drive, narrow Gammon Road to one lane of traffic in each direction with a center turn lane to reduce speeding, increase safety, and allow for construction of the shared-use path.	Engineering, Traffic Engineering
C.	Consider narrowing travel lanes and other design changes on N. Gammon Rd. between Mineral Point Rd. and Tree Ln. to calm traffic and enhance safe connections to schools.	Traffic Engineering
d.	Coordinate with the Madison Metropolitan School District (MMSD) on an improved eastwest shared-use path connection through the high school and middle school property.	MMSD, Engineering
e.	Work with the Wisconsin Department of Transportation (WisDOT) to clarify the lane markings and signage at the W. Beltline Hwy. ramps at Gammon Rd. to improve pedestrian and bicyclist safety and comfort.	Traffic Engineering
f.	When WisDOT pursues replacement of the W. Beltline Hwy. bridge at S. Gammon Rd., work with them to extend the wide sidewalk/shared-use path under the highway.	Traffic Engineering
g.	Work with WisDOT to add lighting under the S. Gammon Rd. overpass.	Engineering, Traffic Engineering

a. Review MMSD's school travel plan and flow of people walking, biking and driving to the school to improve safety at the school. b. Evaluate options to add or enhance ped/bike facilities on Old Sauk Rd. where they are missing, through street reconstruction and the Safe Streets Madison Program. 5 Stephens Elementary School a. In coordination with MMSD, mark blike lanes on N. Rosa Rd. while adding bump outs for school busses and installing more durable terrace paving for students getting on and off busses. Consider adding an alternate student drop-off/pick-up area at the back of the school on Cable Ave. C. Add a priority sidewalk on the remainder of the north side of Cable Ave., the west side of Beach St. and the north and south sides of Anchorage Ave. between Beach St. and Rosa Rd., along with crosswalks at the Cable Ave. and Beach St. intersection, as part of the Safe Streets Madison Program. 5 Spring Harbor Middle School a. Replace the pedestrian underpass beneath the railroad between Old Middleton Rd. and Craig Ave. and add lighting and a new pathway extending from Craig Ave. 7 If the rail corridor becomes inactive, work with WisDOT to study the feasibility of converting the corridor to a shared-use path that parallels University Ave., like the Southwest Path. 8 Construct a shared-use path from Island Dr. through the southeast corner of Owen Conservation Park to Bordner Dr. in coordination with the Crestwood Neighborhood Association. 9 During the Sauk Creek Greenway Corridor Plan, use detailed engineering data to consider the impacts and benefits of adding an All Ages and Abilities spath should follow the existing maintenance path to provide continued access had benefits of adding an All Ages and Abilities path should follow the existing maintenance path to provide continued access had benefits of reconstruction, use environmentally sensitive best practices to design a shared-use path through the greenway. Where possible, the path should follow the existing maintenance path to provide continued acc	4	Crestwood Elementary School	
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	13	Road to High Point Road while adding a center turn lane and buffering the existing bicycle	•

14	Integrate pedestrian and bicycle infrastructure like benches, pedestrian-oriented lighting, and bike racks into terrace areas adjacent to high-traffic pedestrian and bicycle routes, especially in and around the "Commercial Core" areas shown on the GFLU Map.	Engineering, Traffic Engineering
15	Integrate facilities like bike racks, bike lockers and BCycle stations in street reconstruction and development projects along BRT corridors and at BRT stations.	Engineering, Traffic Engineering
16	Implement bicycle wayfinding.	Engineering, Traffic Engineering
17	Work with the City of Middleton to:	
a.	Extend a shared-use path from Camelot Drive through Marshall Park to connect with Middleton Beach Road. Note: the line shown on the Shared-Use Path & Bicycle Network map represents the connection but may not be the final route.	Engineering, Parks
b.	Ensure cross-border bicycle connectivity as both communities continue to improve bicycle facilities, including lanes on John Q. Hammons Dr., paths or lanes on N. High Point Rd., and a possible path along N. Gammon Rd.	Traffic Engineering
18	Work with WisDOT to improve pedestrian and bicycle connectivity across the Beltline by:	
a.	Integrating new Beltline crossings shown on the Shared-Use Path and Bicycle Network Map into any future Beltline construction/reconstruction to enhance connectivity for pedestrians and bicyclists.	Engineering, Traffic Engineering
b.	Improving conditions for transit, bicyclists, and pedestrians at Beltline interchanges.	Engineering, Traffic Engineering
C.	Improving conditions of the W. Beltline Hwy. underpass at Struck St. and making needed improvements for safety and comfort of pedestrians and bicyclists such as upgrades to pavement, drainage, and lighting.	Engineering, Traffic Engineering
19	Improve pedestrian and bicyclist safety at the major intersections identified on the Pedestrian Network map.	Engineering, Traffic Engineering
20	Improve pedestrian safety along Segoe Rd. by by removing Laub Ln. from Berwyn Dr. to S. Segoe Rd. and by reducing pedestrian exposure and crossing distance with bump-outs, pedestrian refuge islands, and continental crosswalk markings, especially where Segoe Rd. crosses Wedgewood-Manor Cross, Tokay Blvd., and Tocora-Hilltop.	Engineering, Traffic Engineering
21	Update the Complete Green Streets Guide to reflect the Complete Green Streets Types Map in this Plan.	Engineering, Traffic Engineering
22	Preserve the opportunity to connect Middleton Street in the future.	Engineering
23	Improve road connectivity with the following street segment changes (also see the Planned Streets Map):	Engineering, Traffic Engineering
a.	Study the transformation of St. Dunstan's Drive to a two-way street from Allen Blvd. to Old Middleton Rd. if the St. Dunstan's Episcopal Church site is proposed for redevelopment.	Engineering, Traffic Engineering
b.	Connect Yosemite Place to Yosemite Trail to improve connectivity south of Old Sauk Rd.	Engineering, Traffic Engineering
C.	Contingent on redevelopment proposed in the area, extend Appalachian Way to the east, then south to connect to Old Sauk Road. Add a new north-south street alongside the Cooper Lane Bike Path to connect with Appalachian Way extended.	Engineering, Traffic Engineering
d.	If properties north of Rennebohm Park redevelop, reserve property to create a narrow, pedestrian- and bicyclist friendly street to the north of the park to break up a large block, provide the framework to orient buildings toward the public park, and create alternate access to the properties fronting Sheboygan Ave. The street should have the minimum allowable speed limit and be designed to minimize non-local through traffic.	Engineering, Traffic Engineering

e.	If properties north of Rennebohm Park redevelop reserve property to create a narrow, pedestrian- and bicyclist friendly street to the north of the park to break up a large block, provide the framework to orient buildings toward the public park, and create alternate access to the properties fronting Sheboygan Ave. The street should have the minimum allowable speed limit and be designed to minimize non-local through traffic.	Engineering, Traffic Engineering, Parks
	Transportation Partnerships	Agencies
Α	Work with WisDOT to:	
i.	Add new Beltline crossings shown on the Planned Streets Map and the Shared-Use Path to enhance connectivity for cars, pedestrians, bicyclists, and transit, shorten trips, and provide alternative north-south routes to Beltline interchanges at S. Gammon Rd. and S. Whitney Way.	Engineering, Traffic Engineering
ii.	Contribute Beltline planning to ensure that City interests are represented as part of Wis- DOT processes.	Engineering, Traffic Engineering
В	Explore public-private partnerships with area commercial property owners to create a Park and Ride system that makes better use of underutilized parking lots on the weekdays.	Engineering, Traffic Engineering
С	Work with the Madison Metropolitan School District to increase lighting along the north-south shared-use path that runs along the east side of the Ezekial Gillespie Middle School property and connects to Inner Dr.	Madison Area MPO, private property owners
D	Work with the MMSD and school principals to address pedestrian, bicycle, and traffic safety issues around schools when street repaving, reconstruction, or other changes to the right-of-way are being considered. Strategies may include signage, increasing crosswalk visibility, modifying traffic patterns, increasing nearby sidewalk connectivity, and implementing safe All Ages and Abilities bicycle connections.	Traffic Engineering, Engineering, MMSD
	menting sale All Ages and Abilities bicycle connections.	
	Neighborhoods and Housing Actions	Agencies
1		Agencies Community Development, Economic Development
1	Neighborhoods and Housing Actions Work with developers to encourage a wider mix of apartment unit sizes, including smaller units for younger and older households, as well as units large enough to support larger	Community Development, Economic
	Neighborhoods and Housing Actions Work with developers to encourage a wider mix of apartment unit sizes, including smaller units for younger and older households, as well as units large enough to support larger families (three or more bedrooms) in proximity to schools. Encourage and incentivize development of smaller-scale "Missing Middle" housing types in Low-Medium Residential (LMR) areas on the GFLU Map, especially in areas near	Community Development, Economic Development Community Development,
2	Neighborhoods and Housing Actions Work with developers to encourage a wider mix of apartment unit sizes, including smaller units for younger and older households, as well as units large enough to support larger families (three or more bedrooms) in proximity to schools. Encourage and incentivize development of smaller-scale "Missing Middle" housing types in Low-Medium Residential (LMR) areas on the GFLU Map, especially in areas near schools, parks, and transit service, through zoning ordinance changes. For new housing partially funded by the City (through land banking, the Affordable Housing Fund, TIF) work with partners to distribute housing at costs affordable to a variety of	Community Development, Economic Development Community Development, Planning Community
3	Neighborhoods and Housing Actions Work with developers to encourage a wider mix of apartment unit sizes, including smaller units for younger and older households, as well as units large enough to support larger families (three or more bedrooms) in proximity to schools. Encourage and incentivize development of smaller-scale "Missing Middle" housing types in Low-Medium Residential (LMR) areas on the GFLU Map, especially in areas near schools, parks, and transit service, through zoning ordinance changes. For new housing partially funded by the City (through land banking, the Affordable Housing Fund, TIF) work with partners to distribute housing at costs affordable to a variety of households throughout the area, with a focus on areas along the BRT corridor. Development along the Beltline Highway should include a variety of noise mitigation improvements. Limit or entirely restrict City financial support for affordable housing in these	Community Development, Economic Development Community Development, Planning Community Development Community Community Community Community
3	Neighborhoods and Housing Actions Work with developers to encourage a wider mix of apartment unit sizes, including smaller units for younger and older households, as well as units large enough to support larger families (three or more bedrooms) in proximity to schools. Encourage and incentivize development of smaller-scale "Missing Middle" housing types in Low-Medium Residential (LMR) areas on the GFLU Map, especially in areas near schools, parks, and transit service, through zoning ordinance changes. For new housing partially funded by the City (through land banking, the Affordable Housing Fund, TIF) work with partners to distribute housing at costs affordable to a variety of households throughout the area, with a focus on areas along the BRT corridor. Development along the Beltline Highway should include a variety of noise mitigation improvements. Limit or entirely restrict City financial support for affordable housing in these locations, unless significant noise mitigation measures are included. Increase access to information regarding First Time Home Buyer Assistance programs for rental and utility payment support, the City's property tax assistance program for seniors, and the City's low-cost mortgage and rehabilitation loans, and historic preservation tax	Community Development, Economic Development Community Development, Planning Community Development Community Development Community Community Community Community Community Community Community

Increase awareness and access to existing community programs and services, especially for youth and seniors, and explore expanding them in such locations as the community rooms of new housing developments, Lussier Community Education Center, and through co-location of community-based organizations. **Economy and Opportunity Actions Agencies** 1 Analyse land banking within the concentration of commercial development south of Min-Economic eral Point Road as a potential method for reusing existing buildings for affordable local Development, business space (potentially as part of an incubator) and for redeveloping surplus surface **Planning** parking into affordable housing. 2 Monitor the performance of TIDs #41 and #46 to determine whether amendments are Economic financially feasible to fund affordable housing, non-assessable infrastructure (bicycle net-Development work improvements, pedestrian safety improvements, new streets, stormwater management improvements, etc.), creation of a business incubator, land banking, small business assistance, and other economic development initiatives in and around these TIDs. 3 As part of TID #41 and #46 evaluation, determine whether a commercial rent assistance Economic or guarantee program to assist small and emerging businesses is financially feasible, par-Development ticularly to allow BIPOC-owned businesses afford space in areas where rent increases may occur. Gap assistance could be used to help businesses displaced by redevelopment afford ground-floor space in new mixed-use buildings. 4 Form a staff team to review the possibility of a new TID in the West Towne area/Mineral Economic Point Road corridor to support redevelopment, stormwater management improvements, Development and other non-assessable infrastructure that is consistent with City plans. 5 Increase affordable childcare options, taking advantage of existing commercial spaces Economic where non-profit childcare providers could locate, ideally along transit corridors and close Development to employment areas. **Economy and Opportunity Partnerships Agencies** Α Explore the development of a business incubator, with lower cost space, shared resources and expertise needed to help grow local businesses. a. Partner with business, economic development, and community organizations, such as Latino, Black, Hmong, and Madison Chambers of Commerce, Wisconsin Women Business Initiative Corporation, and established incubators, to ensure an incubator meets the needs of BIPOC-owned businesses and fosters opportunities for long-term success. Consider integrating an incubator with land banking, using renovated commercial space to grow local businesses and retain affordable options. Incorporate networking, mentorship, and collaboration opportunities as part of business incubator planning. Integrate youth mentorship/training into incubator space, potentially in partnership with d. nearby Vel Phillips Memorial High School. Explore creating a program similar to the Public Market's "Market Ready" as part of incubator planning to help new businesses and small businesses prepare for incubator occupancy. Work with local commercial brokers and property owners to connect small businesses to

66 City of Madisor 1081

As business areas transition to a more connected, pedestrian-friendly environment, assist the creation of a neighborhood business association or BID (business improvement dis-

available space as they "graduate" from an incubator.

trict) to maintain amenities and facilitate events.

В

С	Continue building relationships with and supporting economic development partners, especially those with ties to historically underrepresented communities. For example, the Madison Black Chamber of Commerce, Latino Chamber of Commerce of Dane County, and the Hmong Wisconsin Chamber of Commerce have vibrant memberships, and the City should collaborate with them, and other similar business and economic development groups, to help them grow opportunities for entrepreneurs to locate in the West Area.	Economic Development with Madison Black Chamber of Commerce, Latino Chamber of Commerce of Dane County and the Hmong Wisconsin Chamber of Commerce
	Culture and Character Actions	Agencies
1	Review UDD 3 (MGO section 33.24(10)) to determine whether it should be revised or repealed. Many requirements for UDD 3 development are adequately addressed by the current zoning code, which was adopted approximately 25 years after UDD 3. Other ordinance language governing UDD 3, such as the Statement of Purpose that says the requirements were established, in part, to "assure that future development will complement the existing development in the district," contradict the actions in this Plan, which expressly seeks to transition the area away from its current auto-oriented form.	Planning
2	Changes to UDD 6 (MGO section 33.02(13)) should be analyzed as part of a larger citywide UDD review to:	
a.	Revise the boundaries to not include single-family development within the district boundary and add existing multifamily development to the south of Sheboygan Avenue and existing commercial development between Hilldale Mall, Midvale Blvd., Regent St., and Sheboygan Ave. to the district boundary.	Planning
b.	Align regulations with the TOD overlay zoning district.	Planning, Zoning
c.	Explore whether UDD 6 should be split into subareas to account for the widely varied characteristics and surroundings along the three-plus mile long corridor.	Planning
d.	Consider establishing minimum and maximum building heights in some or all of UDD 6 (note there is already a two-story minimum height in the portions of the UDD that are within the TOD overlay zoning district).	Planning
e.	Integrate design standards into the "Building Design" requirements to ensure buildings fronting shared-use paths address the path with architectural elements and building access at the same design level as the front of the building.	Planning
f.	Add additional building and site design elements to the "Building Design" requirements to require ground floor residential units facing public right-of-way have individual unit entrances from the sidewalk.	Planning
g.	Add an element to the "Building Design" guidelines to provide direct pedestrian connection(s) from buildings to adjacent public parks and/or shared-use paths.	Planning
3	Integrate public art into upcoming public construction projects, including Sidewalk Poetry, Utility Box Art Wraps, and public art at BRT stations, along gateway corridors, and along bike paths. For large-scale construction projects, explore hosting workshops with the community to identify locations for public art.	Planning
4	Implement the "Art Through City Building" concepts as described in the <u>City's Public Art Framework</u> , <u>Cultural Plan</u> , and other adopted policies to make the West Area more welcoming to people of every race, ethnicity, gender, age, and ability.	Planning

West Area Plan 67 1082

	Culture and Character Partnerships	Agencies
A	Work with the property owners and managers of the apartments on Sheboygan Avenue to identify opportunities to partner on placemaking opportunities to enliven the area and engage the community.	
В	Work with neighborhoods eligible for creation of a local historic district and/or a national register historic district to establish district(s). National Register or local historic district nominations should be initiated by the neighborhood, with technical assistance provided by the City. Note that National Register districts allow residents to apply for tax credits, whereas local historic districts establish design requirements that shape future reinvestment and development in these neighborhoods (see the Historic Resources Review in the Appendix for more information).	
С	The property at 6405 Mineral Point Road hosts the historic Otto & Louisa Toepfer Farm house, which is now part of Otto's Restaurant & Bar. Should changes or development be proposed for this site, the property owner is encouraged to adaptively reuse the historic building.	
D	Encourage the creation of neighborhood-oriented events in underserved areas, such as block parties, food cart nights, or summer concert series, to bring residents together more frequently. Such events should focus promotion on underserved communities.	
Е	Work with area property owners and nonprofits to add space for artist(s)-in-residence, affordable artist studios, exhibitions, performances, and rehearsals.	
F	Work with University Research Park and UW-Madison Arts staff to explore possibilities for locating arts space within or close to the Research Park. Space could take the form of studios, partnerships with businesses, an artist residency, or other formats.	
	Green and Resilient Actions	Agencies
1	Garner Park:	
a.	Explore additional trails in Garner Park as part of any future improvements for the park.	Parks, Engineering
2	Rennebohm Park:	
a.	Require dedication of additional parkland as part of any redevelopment of land adjoining the park that increases the number of residential dwelling units.	Planning, Parks
b.	Consider the following elements as part of future upgrades to the park: addition of a water play feature, improvements to the shelter, additional seating near the shelter/tennis courts/paths, and path lighting.	Traffic Engineering, Parks
C.	Provide pedestrian and bicycle access to the park from Sheboygan Avenue, potentially as part of additional north-south and east-west streets (see planned future streets in the Land Use and Transportation chapter).	Parks, Engineering, Traffic Engineering
3	Marshall Park: Increase access to the park from the south by constructing a pedestrian/bicycle connection to the park from Lake Mendota Drive/Camelot Drive. Note: the line shown on the Shared-Use Path & Bicycle Network map represents the connection but may not be the final route.	
4	Implement changes in Odana Hills Golf Course and adjacent park land to:	Parks, Engineering
a.	Reinvest in the Course in a manner that is aligned with the Task Force on Municipal Golf's recommendations to provide a sustainable, high-quality 18-hole course that is environmentally and financially sustainable and inclusive of year-round diverse golf park programming.	
b.	Work to integrate stormwater management into the Course to protect neighboring properties, enhance the natural environment, and build off of volunteer-driven efforts to promote a more sustainable landscape.	

City of Madisor 1083

C.	Replace the existing clubhouse with a year-round, fully accessible facility that serves a broader public purpose beyond golf.	
d.	Take a holistic approach to managing Odana Hills Park (west), Odana Hills Park (east), and Odana Hills Golf Course to expand the variety of recreational opportunities available throughout the combined properties.	
5	Zook Park: Increase connectivity between the park, surrounding neighborhood, and the Southwest Commuter Path via a paved path.	Engineering, Parks
6	Initiate Park Master Plans within the next 10 years for community parks in the planning area (Garner, Marshall, Rennebohm, Walnut Grove, Wexford).	Parks
7	Implement <u>adopted Park Development Plans</u> for neighborhood and mini parks over the next 15 years.	Parks
8	Improve public lake access at lake access courts and parks north of Lake Mendota Drive.	Parks
a.	Construct an ADA-compliant accessible path at the most appropriate location to increase inclusive lake access.	
b.	Add signage (in addition to the existing standard green street signs) to indicate public lake access.	
C.	Work with the Ho-Chunk Nation to place educational plaques at courts and/or along Lake Mendota Drive to describe and illustrate the history of the Ho-Chunk Nation in the area.	
9	Create a small neighborhood park with a variety of outdoor recreation opportunities south and west of University Avenue and west of Spring Harbor Drive to serve this part of the West Area, which is underserved by existing parks.	Parks
10	Look for opportunities to install recreational biking facilities in the Area, such as skill tracks, pump tracks, and single track biking facilities, in alignment with Madison Bicycle Adventure Trail (MadBAT) goals.	Parks
11	Create three new parks that are a minimum of five acres each to serve future residents in redeveloping commercial areas. The General Future Park Area overlay shown on the map designates the general area within which a smaller portion of the land will be sited for a park or open space in the future. The large general overlay is necessary because at this point it is not known where the park and open space will be sited.	Parks
a.	One park should be as centrally located as possible amongst any future redevelopment of West Towne Mall and the big box stores to the west of the mall. While the block containing the Von Maur site would be ideal if the mall were redeveloped all at once, Von Maur is the most recent investment in the area and therefore likely to remain over the mid- or long-term. Alternative locations are shown within the overall western "general future park area" labeled as "A" on the Parks and Open Space Map. This park may need to be two smaller parcels connected by a green corridor.	Parks
b.	An urban feel, like Lisa Link Peace Park or McPike Park, should be considered for the western park (A on the map), due to its location amidst more intensive planned mixed-use development and the possibility of serving residents, patrons of nearby businesses, and visitors to the area. A design that can accommodate special events and programming, such as farmers' markets, festivals, and concerts is encouraged.	Parks
C.	Two other parks should be created between the stormwater ponds and Oakwood Village. See the eastern green hatched area labeled as "B" on the Parks and Open Space Map for the generally acceptable location for these parks. The Parks Division may opt to proceed with one larger park, if that is found to be preferable based on property availability.	Parks
d.	Park space should be acquired through purchase of property and through parkland dedication which is required of new residential development. Parkland dedication may be required of projects when they are proposed adjacent to dedicated parkland in either area A or area B on the map.	Parks

West Area Plan 6 1084

e.	The eastern park(s) (B on the map) may be developed with a focus on serving the local neighborhood and would ideally include park space adjacent to the major stormwater facility south of Mineral Point Road and west of Gammon Place.	Parks
f.	Future designs and amenities for new park land will be determined via park development planning processes undertaken by the Parks Division. Such a planning process will proactively engage a diverse group of residents, including students from nearby Ezekiel Gillespie and Vel Phillips Memorial schools. The park planning process will consider amenities and park names that are welcoming to BIPOC residents.	Parks
12	Enhance the tree canopy on public right-of-way. The biggest opportunity for increasing street tree canopy on existing streets is within University Research Park.	Engineering, Traffic Engineering
13	As recommended in the Complete Green Streets Guide, terrace widths on new streets planned for the area should be wide enough to accommodate healthy canopy trees and/ or bioswales, typically requiring a minimum of 10 feet. Expansion of existing substandard terraces (generally less than eight feet) should be accommodated through additional dedication of right-of-way whenever possible.	Traffic Engineering, Engineering
14	Work with Oakwood Village to explore the prospects of an official, publicly accessible "green corridor" connecting Nautilus Point Park through the Oakwood campus to planned City park space in the Grand Canyon Dr. – Yellowstone Dr. area and to the stormwater ponds to the west.	Engineering
15	Evaluate stormwater utility parcels for plantings and ecological restoration consistent with the Stormwater Utility Vegetation Management Plan once adopted.	Engineering
16	Implement recommendations included in the City's watershed studies to reduce flooding and create a more resilient stormwater management system. Pursue federal funding for stormwater management improvements in watersheds that face the most severe challenges, such as Willow Creek.	Engineering
17	Focus City funding for public and private green infrastructure projects on watersheds that face the most severe challenges, such as Willow Creek and Spring Harbor, or in areas near the lakeshores or have concerns with water quality. Pair funding with increased publicity efforts surrounding City initiatives to boost green infrastructure on private property.	Engineering
18	Implement innovative stormwater management techniques as part of future right-of-way design for both new street construction planned for the West Towne area if/when any redevelopment occurs, other new planned streets in University Research Park, and reconstruction of existing streets.	Engineering
19	Projects in the four planned blocks directly south of the entrance to Sunset Memory Gardens should explore placing stormwater management measures mid-block, as a contiguous feature with accompanying mid-block pedestrian connections (see the "Mid-Block Passage" labeled "C" on the Parks and Open Space Map).	Engineering
20	Consider integration of stormwater management into future park space for large storms as a natural feature in a way that does not compromise the amenities and use of the park outside of large storm events.	Engineering
21	Work with the City of Middleton on cross-border stormwater management issues, especially regarding the Pheasant Branch watershed and Stricker Pond.	Engineering
22	Work with the Village of Shorewood Hills on improving stormwater management in the Willow Creek watershed to reduce incidences of flooding along University Avenue and Midvale Boulevard in the face of increasing annual rainfall.	Engineering
23	Remove the north side of Vernon Boulevard in favor of additional greenspace for a community garden or civic space.	Engineering

70 City of Madisor 1085

	Green and Resilient Partnerships	Agencies
Α	While the City has a significant role to increase the West Area's canopy coverage from its existing 31% to the 40% goal established in the Urban Forestry Task Force's report through plantings on public property, the increased coverage will not be achieved without action on private property. The City should work with major property owners in the West Area, such as MMSD and University Research Park, to increase canopy on non-City land.	
В	Explore partnership opportunities with community groups to create additional community gardens, especially in proximity to concentrations of multifamily housing.	
	Effective Government Actions	Agencies
1	Review sanitary sewer capacity needs related to anticipated development/redevelopment and pair capacity upgrades with road construction projects where possible.	Engineering
2	Study water utility service area #8 (West Towne area and north of West Towne) to determine whether a new well is needed to serve the area.	Water Utility
	Effective Government Partnerships	Agencies
Α	Work with MMSD and the Lussier Community Education Center to facilitate expansion of Lussier to provide additional community space and services to area residents and students.	MMSD, Lussier Community Education Center
В	Explore opportunities to secure/reserve space to serve as a community center, neighborhood center, or meeting space for future residents of a new Odana neighborhood, potentially in conjunction with, or close to, new park space.	Community Development, Planning
	Health and Safety Actions	Agencies
1	Monitor driving behavior and speed and enforce traffic regulations on streets along school routes including N. Gammon Rd., Tree Ln., Old Sauk Rd., Rosa Rd., Segoe Rd., and other areas where people walk frequently and feel endangered and/or are experiencing higher levels of serious traffic related injuries and fatalities (see the High Injury Network map).	West District Police
2	Conduct Vision Zero speed analyses on Indian Hills, High Point Rd., Tree Ln., Whitney Way., Mineral Point Rd., Old Sauk Rd., Rosa Rd. and Segoe Rd. and explore potential traffic calming improvements such as road diets and speed bumps; pair these actions with speed enforcement.	West District Police, Traffic Engineering
3	Increase opportunities for West District Police and community members to engage and build trust with residents of color and people living with lower incomes including the Tree Ln., Oakbridge and Wexford Ridge areas. Activities could include Coffee with a Cop, and Police participation with youth recreation programs, block parties and cultural events and activities.	West District Police
4	Include the Tree Ln. and Wexford Ridge neighborhood as a potential new Neighborhood Resource Team area to improve equitable outcomes by focusing City resources and building stronger community connections for the growing population of people of color and those living with lower incomes.	Department of Civil Rights
5	Encourage new buildings within 200 feet of the Beltline to use materials and insulation that keep interior noise levels below 52 decibels as per state code TRANS 405. Design sites to shield outdoor spaces from noise above 67 decibels. Developments near the Beltline should follow setback and noise regulations outlined in Wisconsin statutes (TRANS 233, TRANS 405), and MGO Section 16.23.	Planning

West Area Plan 1086

Health and Safety Partnerships Agencies Α Work with MSCR and Lussier Community Education Center (LCEC) to improve access to health and recreation programs at MSCR Odana, LCEC, schools, and other locations such as walking/running groups, yoga, meditation, and healthy cooking and nutrition classes. В Work with resident leaders to continue to monitor environmental factors that may contribute to, or protect against violence (e.g. dimly lit areas, abandoned buildings, increased green space, etc.) and use violence prevention resources such as the Madison Dane County Violence Prevention: A Roadmap to Reducing Violence Report to address related issues. C Work with the National Interscholastic Cycling Association and other organizations that offer recreational activities outside of schools, to welcome, encourage, and support participation by middle and high school students of color. D Expand options in the Area where residents without air conditioning can go to get relief from the heat by partnering with LCEC, area businesses, centers of worship, and others to establish them as City-recognized cooling sites. Ε Partner with Pepartnership Inc. to establish a personal essentials pantry in the Tree Ln. and Wexford Neighborhood area. F Create partnerships between neighborhood groups, YMCA, MSCR and area schools to establish youth recreation programs at Haen Family Park and nearby school open space for activities like flag football, basketball and soccer. G Work with residents, neighborhood groups, Madison Arts and non-profit organizations to activate West Area parks and other greenspaces in Oakbridge, Tree Ln., and Wexford Ridge areas for people of all ages with musical performances, community meals, arts

activities and recreation.

72 City of Madisor 1087

Glossary

West Area Plan

Accessory dwelling unit: A second dwelling unit contained within a single-family dwelling or within a detached building located on the same lot as a single-family dwelling. This definition includes accessory buildings constructed in connection with a private garage or a private garage converted into a dwelling unit.

Activity Center: An intensively developed area that is the visual and/or functional center of a neighborhood(s) or a district. Activity centers are typically comprised of a mix of land uses developed at a higher intensity than the surrounding area including residential, commercial, employment, civic, institutional, and parks and open space uses.

Affordable housing: housing for which the occupant(s) are paying no more than 30 percent of gross household income for housing costs, including utilities. Households with costs exceeding 30% of income are considered housing cost-burdened. The City of Madison's owner-occupant and rental development subsidy programs are primarily designed to reduce housing cost burdens for renter households with incomes at or below 60% of Dane County's median income (CMI) and for owner-occupied households at or below 80% CMI.

Affordable Housing Fund: A City of Madison program to provide loans and grants to for-profit and non-profit housing developers for the construction of new affordable rental housing.

Berm: A linear mound generally built to screen views, define areas, or direct stormwater.

Bicycle share (B-Cycle): A kiosk-based bike rental system offering hourly, daily, or subscription-based usage.

Big box retail: A physically large retail store with at least 75,000 square feet of floor area and a regional sales market. They are usually part of a national or regional chain of stores. Some examples include Walmart, Target, Best Buy, and Home Depot. (Source: APA: A Planners Dictionary)

BIPOC: An umbrella term for people of color, which stands for Black, Indigenous, and people of color. The term acknowledges that not all people of color face equal levels of injustice, recognizing that Black and Indigenous people are severely impacted by systemic racial injustices. (Source: Merriam-Webster)

Bump out: An extension of a raised curb into a roadway, typically a parking lane, to create additional terrace or sidewalk space. This can be used to shorten pedestrian crossing distances or create additional space for street trees while encouraging lower vehicular speeds.

Bus Rapid Transit (BRT): is a frequent, faster, and more reliable bus system that uses larger buses to transport more riders. BRT's features include frequent all-day service and direct routes with fewer stops. It utilizes special traffic signals to help buses get through intersections faster, dedicated bus lanes, and stations with off-board fare payment kiosks.

Capital Area Regional Planning Commission (CARPC): One of nine commissions in Wisconsin established to coordinate planning and development among area municipalities. CARPC develops and promotes regional plans, provides objective information and professional planning services, and focuses local attention on issues of regional importance. CARPC carries out land use planning and areawide water quality management planning for the greater Madison region. State statutes charge it with the duty of preparing and adopting a master plan for the physical development of the region. The Department of Natural Resources contracts with the Commission to maintain a continuing areawide water quality management planning process to manage, protect, and enhance the water resources of the region. (Source: CARPC)

Commercial Core: Street frontages where future development proposals are required to include ground floor commercial spaces.

Community Development Block Grant (CDBG): A federal program which provides annual grants to states and cities for affordable housing, anti-poverty, infrastructure, and planning activities that benefit low- to moderate-income persons.

Community garden: An area of land or space managed and maintained by a group of individuals to grow and harvest food crops and/or non-food, ornamental crops, such as flowers, for personal or group use, consumption or donation.

Community land trust: nonprofit organizations whose primary objective is the creation of homes that remain permanently affordable, providing successful homeownership opportunities for generations of lower income families. Under the com-

7-1088

munity land trust (CLT) model, homeowners purchase their house, but not the land (this lowers the purchase price). The land is leased by the homeowner from the land trust. When homeowners sell, 75% of the appreciated value stays with the house, so it's more affordable for the next buyer. The CLT homeowner also gets to keep the equity they invested into the home plus 25% of the increase in value. (Sources: Madison Area Community Land Trust and Grounded Solutions Network)

Commercial Ownership Assistance program: The City's Commercial Ownership Assistance Program (COA) is focused on helping business owners expand their enterprises by transitioning from renting space to owning commercial property for their business. Priority is given to applicants who are people of color, immigrants, women, the disabled, veterans and any other underrepresented groups.

Complete neighborhood: are neighborhoods that include a range of housing types and costs, neighborhood-serving businesses, stores, services, schools, and places of worship. These elements are ideally accessible by foot, bicycle, or transit through a network of well-connected streets and blocks, usable public spaces, and a system of connected parks, paths, and greenways.

Continental crosswalk: A crosswalk with a more visible type of striping (painting), consisting of thick white stripes parallel to the direction of vehicular travel.

Cost-burdened: a household is cost-burdened when they are paying too much for their home (when total housing costs exceed 30% of their gross monthly income).

Drumlin: A linear or oval-shaped hill created by the streamlined movement of glacial ice sheets across rock debris. (Source: Britannica)

Easement: A legal tool that grants one party the right to use property that another party owns and possesses. (Sources: Investopedia, Merriam-Webster Dictionary)

Exclusionary zoning: A term applied to zoning standards, districts, or policies that seek to prevent people of certain races, ethnicities, or income levels from buying homes or living in specific areas or neighborhoods. This could include extensive use of exclusively single-family districts, large minimum lot or open space sizes, and narrow occupancy (household size) rules. (Source: Planetizen)

Foreign Trade Zone (FTZ): a specific area within the United States, located in or near a port of entry, where certain types of merchandise can be imported without going through formal customs entry procedures or paying import duties. FTZs were designed to encourage international trade and U.S. employment. (Sources: Dane Country Regional Airport and Port of Seattle)

Frontage road: A street adjacent to a freeway, expressway, or arterial street separated therefrom by a dividing strip and providing access to abutting properties. (Source: APA: A Planners Dictionary)

Grade-separated intersection: an intersection where one roadway or lane travels above or below other roadways by separating where they cross each other with an overpass or underpass.

Greenway: Linear corridors of land and water and the natural, cultural, and recreational resources they link together. (Source: Massachusetts Office of Energy and Environmental Affairs)

Healthy Retail Access Program: A program created by Madison's Food Policy Council that provides funds for healthy retail projects that aim to improve access to affordable, healthy, and culturally appropriate food and retail within underserved areas.

Historic district: A significant concentration, linkage, or continuity of sites, buildings, structures, or objects united historically or aesthetically by plan or physical development. A local, state, or the federal government can officially recognize districts. (Source: U.S. National Park Service)

Housing cooperative: A residence that is collectively owned and controlled by its members: the people who live in the housing. The buildings typically have private bedrooms but shared spaces, such as common kitchens and recreation areas. Members often share responsibility for cooking, daily chores, and property maintenance.

Impervious surface: Any hard-surfaced area that does not readily absorb or retain water, including but not limited to building roofs, parking and driveway areas, graveled areas, sidewalks, and paved recreation areas. (Source: APA: A Planners Dictionary)

Income- and rent-restricted housing: a type of housing where a specific number of units in a building are reserved for low-income households. A qualifying household's income level must be at or below a specific level for that household's size, such as 60% of the county median income. A limit is also set on the monthly rent to ensure that housing is more affordable for those households, often due to some form of subsidy. These restrictions are typically enforced through a Land Use Restriction Agreement.

Incubator: An area, commercial space, and/or building designated for the cultivation and enhancement of new or future businesses.

Infill development: Development of vacant or underused lots that are surrounded by developed areas.

Land Banking: A City program used to acquire land and buildings that could be used for future economic development, affordable housing projects, and other City uses. The goal is to acquire strategic properties for future purposes that might include: assisting displaced businesses, reducing blight, stabilizing housing markets, improving the quality of life of residents and neighborhoods, and preserving land for City purposes.

Makerspace: A term used to describe a place where people gather to share resources and knowledge, work on projects, network, and build. This could include artist studios, small-scale fabrication spaces, workshops, commercial kitchens, or similar spaces.

Market-rate housing: Housing that does not have any restrictions on rent or household income.

Missing Middle Housing: A range of housing types scaled between single-family detached houses and larger apartment buildings. Housing types that are considered as part of the missing middle include duplexes, triplexes, four-units, row-houses, live-work buildings, accessory dwelling units (ADUs), clustered small homes like bungalow courts, and some small apartment buildings. The scale of missing middle housing is compatible with most existing single-family residential areas.

Mixed-use: A building or groups of buildings designed to encourage a diversity of compatible land uses, which include a mixture of two or more of the following uses: residential, office, retail, recreational, light industrial, and other miscellaneous uses. (Source: City of Beaverton, OR)

Neighborhood Development Plan (NDP): A plan prepared for largely undeveloped land on the city's edge. NDPs are adopted as supplements of the Comprehensive Plan and include recommendations for land use, transportation, parks and open space, and utilities.

Neighborhood Plan: A plan prepared for an already-developed area of the city that includes recommendations for land use, urban design, transportation, parks, placemaking, and other improvements/investments/changes to a given area. Neighborhood plans can encompass more than one neighborhood and are generally adopted as supplements to the Comprehensive Plan.

Neighborhood Resource Teams (NRTs): A citywide effort to coordinate and improve the delivery of City services to Madison's neighborhoods. NRTs provide a regular forum for City employees to meet, discuss, and support each other's efforts in delivering excellent City services. NRT membership can include alders, City staff, and non-City staff participants.

Official Map: A formal public record used to indicate where a government is likely to require right-of-way, easements, or land for future roads, drainageways, utilities, or recreation facilities. Within officially mapped areas, a property owner maintains control and use of their property but building permits cannot be issued. Official Mapping is established in Wisconsin State statute 62.23(6). (Source: UW Extension)

Operating costs: Expenses associated with the maintenance and administration of a business or government on a day-to-day basis, such as salaries. (Source: Investopedia)

Per- and polyfluoroalkyl substances (PFAS): A group of chemicals used to make products that resist heat, oil, stains, grease, and water. They are long lasting chemicals, which break down very slowly over time and exposure to them may be linked to harmful health effects. (Sources: US Environmental Protection Agency and Centers for Disease Control and Prevention)

Placemaking: Creation of an environment that fosters community, stimulates interaction, encourages entrepreneurship, generates innovation, and nurtures humanity. (Source: Project for Public Spaces)

West Area Plan

Proactive rezoning: Proactive rezoning is when the City, rather than a property owner or developer, proposes to rezone land so that it is consistent with plan recommendations. When the City proposes to proactively rezone areas during a planning process, it informs property owners about the recommendation and whether it could affect them.

Property Tax Assistance for Seniors Program: A City program that pays all or a portion of property taxes for qualified homeowners who are over the age of 65 and own a single-family residence within the City of Madison.

Rectangular rapid flashing beacons (RRFBs): A flashing signal activated by pedestrians that alerts drivers to yield to crossing pedestrians.

Redevelopment: Construction of a new building where a building already exists.

Rental Rehab Loan Program: City of Madison program which offers financial assistance, including low-interest loans, for Madison property owners to renovate and improve rental housing.

SafeGrowth: A philosophy built on the belief that healthy and functioning small neighborhoods provide the safest way to build cities in the 21st Century. It is based on the premise that crime is best tackled within small neighborhoods by harnessing the creative energy of functioning neighborhood groups, by employing the latest crime prevention methods, and by adopting an annual SafeGrowth® Plan to address crime and fear. (Source: SafeGrowth.org)

SEED Program: A City of Madison program administered by the Madison Food Policy Council that provides grants to improve the local food system and make food more accessible to Madison residents.

Sense of Place: The characteristics of a location that make it readily recognizable as being unique and different from its surroundings and that provides a feeling of belonging to or being identified with that particular place. (Source: Scottsdale, AZ)

Setbacks: A building design where there are fewer stories closer to the lot line (for example, near sidewalks and adjacent properties) than the rest of the building.

Shared-use path: a path or lane shared by pedestrian, bicycle, and other non-motorized users.

Smaller-scale housing: Also referred to as missing middle housing, it is a range of smaller multi-unit or clustered housing types compatible in scale with single-family homes. (Source: Opticos Design, Inc.)

Stepbacks: A building design element that is typically applied to the upper-story of a development to establish compatibility with surrounding development and maintain a pedestrian-oriented scale. A stepback requires that any portion of a building above a certain height is further pushed-in towards the center of the property.

Stormwater: Untreated runoff from rainfall and snowmelt. It flows across impervious surfaces (such as streets), through fields, and over construction sites, crossing municipal boundaries and can carry contaminants to lakes and streams. (Source: Dane County Office of Lakes & Watersheds)

Tabletop crossing or intersection: An intersection where the roadway ramps up to the sidewalk level to create greater visibility for pedestrians, improve usability for individuals who are mobility impaired, and slow vehicles down. Also known as a raised crossing or intersection.

Tax Increment Financing (TIF): A governmental finance tool to provide funds to construct public infrastructure, promote development opportunities, and expand the tax base.

Terrace: The space between the sidewalk and the curb along a street.

Through movement: within an intersection, the through movement refers to vehicles or users going straight and not turning onto the cross street.

Transit-Oriented Development (TOD): Compact, walkable, mixed-use development that is centered on quality public transit. It typically includes a mix of housing, office, retail, neighborhood amenities, and other uses within walking distance of a transit station. TOD reduces the need for driving by creating compact, vibrant, walkable neighborhoods with convenient access to activities and destinations connected by transit.

Tree canopy: The part of a city or area that is shaded by trees; The layer of leaves, branches, and stems of trees that obscure the ground when viewed from above. (Source: Center for Watershed Protection)

Underrepresented groups: Groups of people with a common race, ethnicity, immigration status, age, income level, gender identity, or sexual orientation who have not typically participated in City decision-making processes corresponding with the proportion of the population they comprise. These groups have often experienced discrimination or marginalization based on their identity.

Urban agriculture: The production of food for personal consumption, market sale, donation, or educational purposes within cities and suburbs.

Urban form: the patterns of building height and development intensity as well as the structural elements that define an area physically, such as natural features, transportation corridors, open space, public facilities, and other elements. (Source: City of Los Angeles)

Water quality: The condition of water, including its chemical, physical, and biological characteristics with respect to its expected use, for example, drinking, swimming, or fishing. (Source: Florida Brooks National Marine Sanctuary, Key West, Florida)

Watershed: Watersheds are an area of land that drain to the same location (the outlet).

Wayfinding: Wayfinding refers to information systems that guide people through a physical environment and enhance their understanding and experience of the space. (Source: The Society for Experiential Graphic Design)

Zoning Code: An ordinance that regulates land use, lot size, building placement, building height, and other aspects of the development of land.

January through June 2023 - Community Feedback Summary

COMMENT	FEEDBACK SOURCE	MULTIPLER*	AREA/DETAILS
LAND USE - Assets			
Lots of places to shop and eat	(KOF), (KOM), (CS)	17	Hilldale
Like the lack of congestion	(KOF)	3	
Like the views and vistas	(KOF), (KOM), (SHNA)	3	
Support infill redevelopment	(CS)	2	
LAND USE - Recommendations			
Want lower-density/lower-rise buildings	(KOF), (CS)	44	Odana, against multi-family housing, limit multi-story apartments that crowd sidewalks and limit views, avoid over development, want single family homes, keep under 4 stories, want less low-income housing, too many condos/apartments
Want more mixed-use	(KOF), (KOM), (CS)	28	rezone old commercial into mixed use, more development in Odana, want more food options in commercial spaces, fit economy within housing stock, combine small scale rentals into single family areas to prevent large scale rental from becoming run-down, more like Hilldale, use TIF funding, small business on ground floor of apartments
Want higher density	(KOF), (KOM), (CS)	21	against parking lots, less single family, more density along major roads, near BRT and activity centers
City should be beautiful	(CS)	7	·
Convert old commercial to new development	(CS)	7	West Towne
Prioritize people not cars	(KOF), (CS)	4	
Need corridor plans for major roads	(SHNA), (CS)	3	Transportation corridors
Preserve farmland	(CS)	2	
Transition to structured parking	(CS)	2	away from single family areas
TRANSPORTATION - Assets			
Like walkability	(KOF), (KOM), (CS)	32	around city, on quiet trails, to schools, shaded
Like existing bike connections	(KOF), (KOM), (CS)	14	Odana, around parks, bike trails accessible to housing
Like the bus service	(KOF), (CS)	9	BRT
Like the lowered speed limits; want more lowered	(KOF), (CS)	7	lower on Tree Lane and Westfield Rd, South Highlands Ave, Old Middleton
TRANSPORTATION - Recommendations			
Expand bus service	(KOF), (CS)	42	reach areas not along Route A, make closer stops to Tokay instead of ending on Odana, in low-income areas, redesign makes it too far to walk

Expand bike pathways	(KOF), (KOM), (CS), (OH)	36	Old Middleton Rd at University Ave ramp, to downtown, north-south connections, connecting to bus stops/BRT, to West Towne Mall, Southwest Commuter Path to Hill Farms, access to Beltline Path (Craig Ave to Huegel Elementary), prioritize in high traffic areas, Whitney Way, Want Segoe to be predominately biking road (with separated lanes), trails from near west to far west, fill in gaps, connect existing paths, all age pathways,
Safety for bikers and pedestrians	(KOF), (KOM), (CS)	34	buffers between cars and bikes, buffers between bikes and people, reduce bike barriers, more off road paths, pedestrian safety on sidewalks (from bikers, skate boards, scooters), bikers don't stop at stop signs
Need improved safety at intersections and crosswalks for bikes & pedestrians	(KOF), (KOM), (CS)	24	Indiana Hills, High Point Road, Mineral Point, Whitney Way and Segoe
Expand walkability	(KOF), (CS)	21	West Towne Mall, to parks, BRT near employment and residential, Hill Farms
Reduce traffic	(KOF), (KOM), (CS)	14	Reduce speeds and lanes on arterial roads; put pedestrians, bikes, and transit first
Decrease car lanes	(CS)	7	
Sidewalk improvement and infill	(KOF), (CS)	6	High Point Road
Concern over lack of parking	(KOM), (CS)	5	
Street Improvements	(KOM), (CS)	6	Spring Harbor St, Old Sauk Rd, South Highlands Ave (potholes)
Overboard on speed limit restriction with minimal enforcement	(CS)	3	
Car Intersection improvements	(CS)	2	Left hand turn at Old Sauk and High Point, need wide enough lanes for safety
Sidewalks comes with downsides	(CS)	1	Cut mature trees, little car traffic can manage bikers and walkers without dedicated lanes
Decrease stoplights and pedestrian crossings	(CS)	1	Routes to downtown, particularly University Ave
Increase Metro access for people with disabilities	(CS)	1	
NEIGHBORHOODS & HOUSING - Assets			
Good diversity of current housing stock	(KOF), (CS)	6	keep the single family homes
Like the amount of affordable housing	(KOF), (CS)	4	
Like mix of residential and commercial sites	(CS)	1	good balance
Like ample amount of lot size	(CS)	1	
Like the Sequoya Commons	(KOF)	1	
NEIGHBORHOODS & HOUSING - Reccomendations			
Want more affordability	(KOF), (CS)	63	address property taxes, along bus lines, want it to be evenly distributed across city, not just concentrated in certain areas, for seniors, outside of Section 8 program
Want more diverse housing stock	(KOF), (CS)	17	ADUs, Townhouses and condos, missing middle, multi-family, senior options, single family

Need to increase housing development	(KOF), (KOM), (CS)	16	Along transit corridors, Hilldale, Odana, redevelopment of mall, convert non-active businesses/churches to more housing (Whitney Way, Copps), a must in the right areas, TOD is too broad of a brush, incentivize development in areas that can better accommodate it vs. an incremental increase (SF to MF), on vacant or underdeveloped commercial parcels
Want more interesting building facades	(KOF), (CS)	12	commercial and residential
Against multi-family buildings	(KOF), (CS)	6	Crime, not enough green spaces, too much multi-residential, limit public housing, want single family homes predominantly
Don't want buildings set right up to the sidewalk	(KOF), (KOM), (SHNA), (CS)	4	
More subsidized housing that is well managed	(CS)	2	Need low income housing
Less subsidized housing	(CS)	2	crime concerns
Air B&Bs taking up too much space	(KOF)	1	
Redevelop existing offices and apartments	(KOM)	1	
Increase updates to apartment buildings	(CS)	1	encourage apartment owners to update to heat pumps & higher building insulation & window standards for existing buildings
GREEN & RESILIENT - Recommendations			
Sustainability in all aspects	(KOM), (CS)	30	incentivize private owners of massive paved parking lots to install underground holding tanks, permeable pavement & rain gardens, improve recycling, city wide composting, include environmental learning standards in school, fight climate change
Plant more trees	(CS)	26	education on importance of trees (value, cooling, carbon capture)
Improve water quality and natural areas/habitats	(CS)	21	protect natural growth, improve lake conditions, leaf pick up, clean and safe drinking water
Preserve animal habitats	(CS)	2	
Address climate change and storm water runoff	(KOF), (KOM), (CS), (OH)	18	Hill farms, use natural storm water management, flooding issues on Kenosha, Research Park, better storm water collection
Reduce pollution	(CS)	10	chemicals in parks and on lawns, no gas lawn equipment, reduce salt use, PFAS
Renewable energy	(CS)	9	
Want less light pollution	(KOM), (CS)	8	concern over lighting in parks that will disturb natural areas, encourage Dark Sky outdoor lighting
Streets should be an ecosystem	(KOF), (CS), (SHNA)	6	maintain tree cover, replant trees that are removed, native flowers in medians, encourage native plantings in people's yards
Bring gardens closer to residents through schools, backyards, and parks	(KOF), (CS)	2	
Add nature barriers	(KOM), (CS)	3	between housing and beltline, between public and private properties
Existing housing stock is old and energy inefficient	(KOF)	1	
Increase Resiliency	(CS)	1	better infrastructure now to protect the community
PARKS & GREENSPACE - Assets			

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Like the current parks/greenspaces	(KOF), (KOM), (CS)	105	Spring Harbor, open spaces, Walnut Grove park, Sauk Creek, Kenosha Parkway, Glen oaks, Bordner park, dog park, greenways, Owen Conservancy, Garner,
Like the amount of trees	(KOF), (KOM), (CS)	38	
Like being close to the lake, ponds, parks, and greenways	(KOF), (CS)	9	Kettle pond- connection to nature, like access to parks, Access to university and lakeshore preserve
Nature programing	(KOM)	2	ice rinks in winter, activities in Rennebohm Park
PARKS & GREENSPACE - Reccomendations			
Increase/preserve greenspaces and parks in dense areas	(KOF), (KOM), (CS)	79	maintain current parks, create pocket parks, willing to up-zone to get more parks, more trees at Garner Park, new park in the area of Odana Rd., make Garner Park a regional Central Park, more no mow areas
Concern about Sauk Creek Greenway reconstruction (bike path and tree removal)	(CS)	69	want to keep trees, would be a hazard to pedestrians (kids, elderly, dogs), would harm wildlife, concerned about home valuation, don't want bike path, want Greenway to be quiet place to explore and think bike paths should be closer to roads (Farmington and Sauk Creek Drive), acknowledge that storm water needs to be addressed, new bike route near Walnut Grove ahead of, during, and after construction
Park amenity improvements and maintenance	(KOF), (KOM), (CS)	68	more accessible bathrooms, more shade, more drinking water with bottle fillers (new and replacements), more seating, seating in shade, add nature playgrounds, pickle ball, improved tennis court, mobility accommodations for older adults in parks and trails, dog parks, maximize open space, intergenerational playgrounds, permanent infrastructure in Bordner Park and Glen Canyon Park, ice rinks, volleyball courts, partner with neighborhood associations and residents to take care of parks, kayak/canoe launch at Spring harbor, Skyline park playground (dangerous chipped areas), recycle bins
Against tree removal	(KOF), (CS)	30	Kenosha greenway
Environmental stewardship	(KOF), (KOM), (CS)	25	Newberry Park & Research Park; add native vegetation & access to conservancy areas, Reduce invasive species (Sauk Creek Greenway), Address overgrown area and stagnant water on Julia St, preservation of Sauk Creek woods, Protect existing habitats and wildlife, concerned about Garner woods, clear wild parsnip near waterways, reduce chemical treatments in parks
Want more connected and intentional greenways	(KOF), (KOM), (CS)	15	Connect parks through bike trails, make Odana Hills golf course more multi-functional, more multi-functional walking paths through greenways, more mountain bike trails through greenways and along paths
Don't divide parks with bike trails	(KOF), (CS)	15	Walnut Grove
Want Sauk Creek greenway reconstruction	(CS)	6	want trees to be replanted near walking path
Need parking at Walnut Grove	(KOF)	1	

ECONOMY & OPPORTUNITY - Assets			
Proximity to downtown amenities	(CS)	23	
Like the quality of facilities	(KOF), (KOM), (CS)	12	schools, libraries, Urgent care, BRT, garden club
Locations of businesses	(KOF), (CS)	11	Hilldale, convenient to the bus, grocery stores and Target
ECONOMY & OPPORTUNITY - Recommendations			
More restaurants, coffee shops, clinics	(CS)	20	want more unique development: rooftops, More development around transit, add green spaces around businesses. build more businesses/cafes into neighborhoods
Fill in vacant commercial spaces with small scale businesses and community resources	(KOF), (KOM), (CS)	16	need more childcare, more restaurants and coffee shops, less CBD, liquor stores, and chiropractors - want more community supported services, more shared spaces
Food access	(KOF), (KOM), (CS)	9	more farmers markets, walkable food access in Sequoya Commons, restaurants, cafes, and food trucks near BRT stops, incentivize grocery stores and healthy food options to expand to keep up with growing population, more restaurants and cafes, grocery stores within biking distance
Support marginalized and local business owners/workers	(CS)	6	support marginalized entrepreneurs through cutting red tape and offering support of city navigators, MBE, WBE, DBE, hire handicapped
More schools	(CS)	5	
Concern over commercial displacement	(KOM), (CS)	4	support business owners, support small businesses, find location for small offices
Internet	(KOF), (CS)	3	Make work from home easier, municipal internet or improve private coverage
More employment	(CS)	3	near new/dense housing, one of few places to add employment at scale
HEALTH & SAFTEY - Assets			
Quiet	(CS)	23	
Safe and low-crime	(KOF), (KOM), (CS)	16	
ike that much of the area is close to medical services	(CS)	1	
HEALTH & SAFTEY - Recommendations			
Speeding and vehicle accidents	(CS)	42	reduce speeding: Whitney and Mineral Point, Odana, Old Sauk Road, stop DUIs
Reduce crime	(CS)	33	against guns, want more cops with body cams, lighted sidewalks, pathways, and parking, support police and first responders, less crime on pathways/bike trails in Tree Lane Apt and Wexford Ridge area, concern over bike paths near residential homes
Community services	(KOM), (CS)	16	alternatives to police, will need more community services if new families are brought in, address "panhandlers" at intersections, Good Neighbor Project, mental health crisis, social workers, help homeless, more community leaders

		1	
More police enforcement	(CS)	7	Tree Lane Housing, Walgreens and Kwik Trip, need officers building rapport in neighborhoods - driving squad cars feels isolating
Enforcement of building codes	(CS)	1	Fire safety
Walkability promotes health & safety	(CS)	1	
CULTURE & CHARACTER - Assets			
Good people in community	(KOF), (KOM), (CS)	33	
Like the charming character/suburban feel	(KOF), (CS)	15	
Like diversity of people	(CS)	7	
CULTURE & CHARACTER - Recommendations			
Want more local/community involvement	(CS)	18	maintaining parks, health and safety, in development decisions, in creation of Sauk Creek Plans
Maintain character of neighborhood	(KOF), (CS)	6	need to preserve history while looking to future
Want more support	(CS)	7	childcare, living expenses, job training, elder care, community run commercial facilities, to meet family needs
Want more diversity in neighborhood	(CS)	6	
Want more cultural facilities and art	(KOF), (CS)	5	libraries, schools
City is more focused on development than quality of life	(KOF), (CS)	4	promote local developers who care about the community
Want more DEI in planning practices	(CS)	3	
Want attention on place making	(CS)	1	
Want more youth activities	(CS)	11	

Phase 1 Engagement Feedback

(KOF) Virtual Meetings February 6, 12pm & February 13, 6pm

(KOM) In- Person Meeting March 9

(SHNA) Spring Harbor Neighborhood Association Meeting April 27

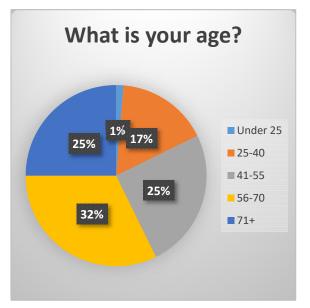
(OH) Open House- May 10 Written Comments

(OCM) Online Commenting Map Comments

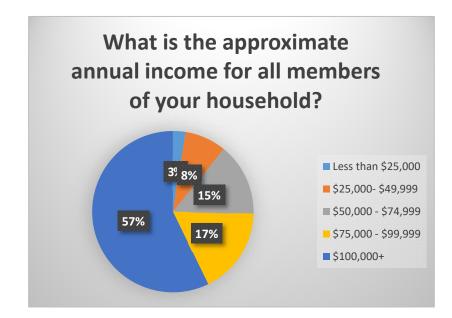
(CS) Community Survey Comments

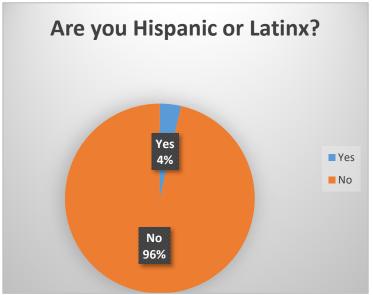
*Note: The multiplier refers to the number of time the comment was shared.

Demographic questions were asked of community survey respondents. Those are summarized here, and are generally consistent with the virtual and in-person meetings of February and March.









6/30/2023

Phase 2: mid-July 2023-August 2023. Staff has reviewed, categorized, and summarized feedback received thus far from various methods of public engagement (see the key at the end of the summary for a full description of the feedback source abbreviations). The "multiplier" column indicates how many times similar comments were received.

West Area Plan - Community Feedback Summary

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Land Use and Transportation: Compact Land Efficient Transportation			
Expand bike pathways/protected lanes)	(LYA), (EVM), (LVM), (RPM), (CS), (CP)	92	Connecting Madison bike infrastructure to Middleton on Gammon Rd, Midvale, behind Odana golf course, connect good neighborhood path to old Middleton, both sides of Bluff St, N-S connection to Odana and southwest path, Hamersley, Van Hise, Hamilton
Expand walkability	(LYA), (EVM), (RPM), (CS), (CP)	53	
Want more density	(EVM), (RPM), (CS)	52	Along transit lines, convert more LR to LMR, reduce parking, more LMR, MR, and NMU instead of LR
Safety for bikers and pedestrians	(EVM), (RPM), (CS),(CP)	48	Bikers need to obey traffic laws, keep cars out of bike/turning lanes, dedicated bike lane on Hamersley, reduce on-street parking, prefer protected bike lanes to shared lanes, Nakoma curb cuts are dangerous, more all ages and abilities, more lighting at S Gammon Rd, High Point Rd, Beltline and adjacent areas
Expand bus service	(CS), (CP)	40	New service doesn't meet some needs, should be expanded to low-income/high density areas, expand east-west connections, concerns about number of bus stops in area
Want more mixed-use	(RPM), (CS)	35	Activate ground floors, corner of regent/Whitney Way, along Midvale, Odana, want walkable spaces, don't put in areas of low development density
Speed limit reduction and enforcement	(EVM), (RPM), (CS)	32	Road diets and speed bumps, Tree Lane speeds, 20 is plenty, Whitney Way, Rosa Rd., Mineral Point Road, Segoe, Old Sauk

No Sauk Creek Bicycle Route	(EVM), (RPM), (CS)	27	Don't want lights or concrete path, distribution of water management and natural habitat, preserve trees, afraid city won't maintain, steep incline at start of path would make dangerous, Tree Lane/High Point intersection dangerous, prioritize floodplain, creek, trees, and climate over maintenance or bike trails, dangerous to pedestrians walking in area
Intersection and crosswalk improvements	(EVM), (LVM), (RPM), (CS), (CP)	26	Midvale Ave/University Ave, University Ave West of Whitney way, Genomic/Mineral Pt, new cross streets along Mineral Pt, traffic signal at Regent and Segoe, difficult to cross Segoe, Odana west of Whitney Way, Old Middleton/ N Whitney Way, Karen Arms/Regent, Segoe and Hilltop, Whitney Way/Gilbert, South of Mineral Point Rd along Gammon Rd
Maximize low-density	(RPM), (CS)	19	More options for low density housing that isn't single family, balance of single and multi-family, pool and churches should go to LR or LMR not MR, more 2-3 story apartments
Prioritize people over cars	(EVM), (RPM), (CS)	18	
Sidewalk improvements	(LYA), (EVM), (LVM), (RPM), (CS), (CP)	13	Summit Woods, Odana, Hilltop, bury power lines for trees, increase snow removal in winter near bus stops, widen sidewalks, along Memorial
Reduce traffic	(CS)	12	traffic calming on Whitney Way and at Beltline interchanges
Street Improvements	(LYA), (RPM), (CP)	12	Road repairs needed on Old Sauk, keep traffic moving
More B-cycle stations	(LYA), (RPM)	11	
Support High Point Road Bike Connections	(LVM), (RPM)	9	Want one lane each way, prefer protected or off road (5), prefer shared lane (2)
Parking concerns	(RPM), (CS)	7	on-street parking: unsafe for bikers, blocking curbs and hydrants, if city decreases parking lanes, need to work with apartments to direct cars elsewhere
Support Sauk Creek Bicycle Route	(LVM), (RPM), (CS)	6	On street bicycle facilities will only be so safe, need alternative, restructure but support trees
Support Westfield Bicycle Route	(RPM), (CS)	6	Extend through mall and across beltline, want protected lanes and removal of parking
Concern over planned streets	(EVM)	1	
Expand bicycle wayfinding	(CS)	1	

No Westfield Bike Lanes	(RPM)	1	Don't want on-street parking removed
NEIGHBORHOODS & HOUSING: Complete Neighborhoods Housing			
Want more affordability	(RPM), (CS), (CP)	49	Affordability only in certain areas will segregate people
Want more diverse housing stock	(EVM), (CS)	25	condos, duplexes, rent to own, co-op, ADU, diverse facades, missing middle
More housing development	(EVM), (CS)	20	
Require setbacks and natural buffers	(RPM)	3	Additional setbacks for residential units, need plant buffer between high and low zones
ECONOMY & OPPORTUNITY: Growing Economy Equitable Education and Advancement			
More businesses like retail and restaurants	(LYA), (CS)	35	
Support marginalized and small business owners/workers	(RPM), (CS)	16	Food trucks
More accessible options for employment for adults and youth	(LYA), (CP)	10	More youth internships, jobs, opportunities
CULTURE & CHARACTER: Cultural Vibrance Unique Character			
Inclusive community events and spaces	(LYA), (CP)	10	Expand Lussier Community Education Center as it is important to community and offers a lot of series to community and needs more space to better serve community
Youth facilities and activities	(LYA), (CP)	10	
Bring food trucks to parks	(LYA)	10	
Aquariums	(LYA)	10	
Maintain community and welcoming culture	(EVM), (RPM), (CS)	6	Midvale Heights,
Want more diversity in neighborhood	(CS)	5	
Community Resources	(CP)	1	Lack of resources and information dissemination in west area
GREEN & RESILIENCE: Natural Resources Parks and Recreation			
Increase/preserve greenspaces and parks in dense areas	(LYA), (RPM), (CS)	62	Add more playgrounds
More tree canopy cover	(LYA), (EVM), (RPM), (CS)	51	Bury power lines to make room, want city to provide trees for free, more trees needed around west towne mall
Address climate crisis	(RPM), (CS)	33	
		25	Add shelters to parks that don't have them already, more parks for dogs, public bathroom upgrades, more amenities at
Park amenity improvements and maintenance	(EVM), (CS), (CP)	23	Garner Park for families and children
Park amenity improvements and maintenance Improve water quality and natural areas/habitats	(CS)	20	[· · · · · · · · · · · · · · · · · · ·

Stormwater Management	(RPM), (CS)	15	natural methods, concerned about flood control from new
Reduce pollution	(CS)	12	development (Brennan, harbor view, UW clinic) PFAS, road salt, air quality
Energy efficiency	(CS)	12	homes, electric vehicle charging, help residence install solar panels, tax programs for businesses
Cleaner beaches	(LYA)	10	
More dog friendly trails and spaces	(LYA)	10	
Native plantings in parks and greenspaces	(CS)	9	
Want more connected and intentional parks and greenways	(EVM), (LVM), (CS)	8	Like advocated for in 1961 plan, only purpose is not stormwater management
Sustainable development	(CS)	7	Green architecture, LEED qualities
Urban Agriculture	(LVM), (CS)	3	Community garden, edible trees and vegetation in parks,
EFFECTIVE GOVERNMENT: Efficient Services Regional Cooperation Community Facilities			
Improved police enforcement	(CS)	17	Regular and community policing, body cams
Reduce crime	(CS)	13	stolen cars, gun violence
Expand Lussier Community Center	(LYA)	15	
More fire stations	(LYA)	10	
Teenage clubs for sports (iceskating and rollerblading), games, and entertainment	(LYA)	10	
Need homeless shelter; animal shelters	(LYA)	10	
Increase lighting	(LYA)	10	Between Memorial HS and residential south of Beltline (underpass)
Diverse Participation	(EVM), (CS)	3	Survey/public meeting participants are self selected and majorly represent older, wealthy people- Need more representation
Incorporate underlying plans	(EVM)	1	
More libraries	(RPM)	1	In high densities and for transit dependent populations (Sheboygan and BRT lines)

Phase 2 Engagement

(LYA) Youth Action Interns - July 18

(EVM) Virtual Meeting July 18 (evening)

(LVM) Virtual Meeting July 26 (noon)

(RPM) In- Person Meeting/Open House August 2 (evening)

(CS) Community Survey Comments

(CP) Community Partner Comments (The West Area Plan hired Community Partners to focus and highlight feedback from underrepresented communities during this planning process. Partners reached over 275+

Phase 3: August 2023-April 19, 2024

Staff has reviewed, categorized, and summarized public feedback received from August 2023 to April 19, 2024 through the activities shown in the legend at the bottom of the document. The legend also includes abbreviations used within the document, to identify where the various comments came from.*

Public comments received during Board, Committee and Commission meetings and third-party gatherings like neighborhood association meetings, friends groups, and other activities are not included. Board, Committee and Commission meeting recordings are available via the West Area Plan project webpage.

The "multiplier" column approximates how many times similar comments were received. There were over 1000+ online comments received. In certain cases, individuals may have submitted the same or similar comment multiple times. If the comment is not within the scope of what the West Plan covers, staff shared the comments with the appropriate city agency.

*The details provided below are only from comments directly received by the City. There have been external community engagement efforts done like the Highlands Community Survey of property owner households and a Sauk Creek Greenway survey regarding the proposed shared-use path and the results of those surveys are not included below.

West Area Plan - Community Feedback Summary

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Land Use			
Support increased density/housing density	(EIM12) (LCD) (EF) (OC) (EVM18)	64	Like redeveloping commercial areas to increase housing density, remove barriers to building multifamily buildings, this just forces property owners to get special approval for rezoning and variances, which only deters them from constructing the desperately needed housing, too much LR land use shown & not enough mixed-use, require more condos/townhomes, there's no excuse for so much of area to be exclusively single family, allow mixed use, there's no way that this little amount of suggested rezoning by the planning department would ever need meet the growth needs of the area, consider rezoning areas along arterials to include mid density housing and mixed use, more efficient use of resources and less environmental impact, bring more diversity to neighborhoods, allow duplex, triplex, and rowhouses in single-family areas, stop protecting low density SF, Plan does not go far enough to increase MF housing, increase density near shared use path to provide destinations and reason to use paths, promote walkability and bikeability, suggested locations for density: near Garner Park/Segoe corridor (LMR)/Hilldale/Gammon Rd/Old Sauk Rd/Odana Rd/Tokay Blvd/West Towne area (where folks won't complain about it being in their backyard)/University/Midvale
Oppose higher density	(EIM12) (EF) (OC) (EVM18) (HFNA)	19	Do not want higher density of residents, multi-family units impacts safety of current residents and changes character of neighborhood, no development that impacts schools, higher density within built up parts of city puts too much pressure on transportation infrastructure and drives down property values, light pollution, traffic, sewer, water, noise, parking, don't need more 10+ story buildings, people want single-family houses, areas already congested, concerns development will eliminate tree canopy, locations with concerns: Old Sauk, University Ave, Whitney Way, Old Middleton Road

5/1/2024 5/1/2024

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Old Sauk Rd.	(OC) (EVM5)	4	Agree with all actions that allow missing middle housing, remove escalator clause for LR, LMR and MR designations in Old Sauk Rd. area, add a "House-like development pattern" map note (similar to the Isthmus) for LMR on Old Sauk Rd farm property, don't build an apt. complex on Old Sauk Road, the development information mtg indicated the development will require residents to pay for onsite parking-this will push residents to park on nearby residential streets
Oppose West Plan Height Map	(OC)	2	Keep residential areas lower height and in-style with the residential neighborhood. Location: south side of Sheboygan Ave
Support West Plan Height Map	(EF) (OC)	9	Locations: West Towne Mall (increase heights), Western edge of Eau Claire Ave (2-5 stories), Buffalo Trl. (2 stories to protect single-family housing), SW corner of Mineral Point Road and Rosa Rd, Red Cross site
Support Hill Farms pool rezone [staff note: a change to GFLU, not zoning, is shown in public review draft]	(OC)	1	Homeowner in Hill Farms-fully support changes in zoning to increase housing density. Madison needs more housing and there's plenty of space in this part of town.
Oppose Hill Farms Pool rezone [staff note: a change to GFLU, not zoning is shown in public review draft]	(EIM12) (EF) (OC)	19	Take up rezoning at the time of an actual proposal, it is not close to transit or major streets, far from the nearest multifamily development (across a wide empty marsh), there are SF homes here not apartments, need more public pools, should be designated as park/green space
Approve expanding mixed use in residential areas	(LCD) (EF) (EVM18) (EVM5)	17	Want closer amenities, create walkable neighborhoods, integrate residential and commercial, allow heterogeneity along arterial streets, will bring better quality of life, likely improve many homeowners' property values, great way to reduce carbon emissions, want small apartment buildings/mixed use like Sequoya Commons sprinkled all throughout the neighborhood, stop protecting low-density single-family development
Oppose expanding mixed use in residential areas	(EIM12) (EF) (OC)	8	Concern the change will: encourage the loss of single family homes and churches, result in less options for homeownership, encourage developers to buy out current homes, increase traffic, overcrowd schools, less parking on the street, more noise, more light pollution, bring more problems, impact the quality of life of residents in area and health of the neighborhood, cause possible stormwater issues, lose privacy with buildings overlooking yards/homes, bring down property values, negatively affect those in lower social economic status ability to own home, plans should not focus on intensive mixed use development but affordable owner occupied housing for community service professionals, should focus using underutilized commercial spaces for additional housing density, maintain single family residential neighborhoods
Support changing single-family areas	(OC)	5	Areas around Hilldale ripe for upzoning - there's no excuse for so much of that area to be exclusively single family, Odana and Tokay need to be upzoned from single family to mid density and mixed use, there's way too much detached single family housing along major roads/arterials, consider rezoning areas along arterials to include mid density housing and mixed use, LR = selfish

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Oppose changing single-family areas	(EF) (HFNA) (OC) (CFG)	15	Preserve and encourage single family housing and ownership, do not want higher height development next to single family homes, rental units do not contribute to an investment to the neighborhood community, rental occupants are less likely to have and raise families with children, single family houses are more affordable for families, don't want high density in areas, use vacant or underdeveloped commercial or existing mixed use space that could accommodate med-high density residential instead; Locations: NMU near Hickory Dr. and University Ave., properties between Wynnwood Way and Whitney Way, Old Sauk Rd (stop escalator clause), generally oppose changes from SF to NMU or higher density residential
Concern about proposed GFLU changes	(EF) (EIM12) (OC)	26	Concerns: how change will impact current property owners, trees/wildlife, creating more access points causing more traffic congestion, lower property values, create more problems, will create urban high density/overdevelopment, change character of neighborhood, will not encourage missing middle development. Locations with concerns: Low-Medium Residential for 600 block of Dearholt Rd- want LR with SR-C3 zoning potentially, NW Block at Whitney and Regent-LMR instead, Dearholt should remain LR, Hillcrest should not be MR, N Gammon (200 Block), Wynwood Way, Whitney Way, LR to LMR along northside of Old Sauk Rd., MR between Old Sauk Rd. and Gammon should remain LMR, MR along Rosa Rd and South Hill Drive - should not developed beyond R-1, MR on Gammon Rd and Colony Dr is overdevelopment and should be LR, Beltline and Hammersley, limit population density along Old Sauk between Gammon and Old Middleton due to 2 lane street, Crestwood Elementary - steep hill, keep MR along western edge of Eau Claire Ave between Sheboygan and Regent and be sensitive to redevelopment to protect single family homes along Buffalo Trl., MR at Segoe Rd and Mineral Point Rd
Support proposed GFLU changes	(EF) (OC) (EVM18)	10	Good to have more medium/high density residential, need to allow small stores in neighborhoods so they are walkable - too much driving is required to get basics/groceries in many developments, LMR for Old Sauk Rd but oppose using the escalator clause to increase density, expand NMU area at Old Sauk and Gammon
Increase proposed GFLU changes	(OC)	10	GFLU changes are small-support more, too much low residential, low residential should be eliminated entirely to allow higher densities, add more LMR along streets like Whitney Way, Mineral Point, Regent, etc., would like to see more mixed use within residential areas focused on social life like shops and cafes within low/medium residential areas, allow up to 5 stories and mixed use along Whitney Way, allow 4 stories and mixed use at Midvale intersections (like Sequoya Commons), allow 7 stories at south end of Midvale, more intensity along Whitney Way - area is currently monotonous

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Opposed/concern - proposed zoning changes	(EIM12) (OC) (AVM4)	26	Want development to not change character of neighborhood, parks should not be rezoned unless to specially promote their use as community space in neighborhoods, residents near any proposed rezoning should have say in advance of a developer proposing a project, 4 stories at Whitney/Regent is too tall, rather than proactively rezoning Area A - seems like land use change is better, rezone of Area E seems not to affect much - make sure it doesn't negatively affect YMCA and affordable childcare, opposed to blanket rezoning rules - city should be zoned to retain beautiful character this city has now, don't understand how some lots are selected and some are not, cuts homeowners out of zoning decisions, Locations of concerns: NW Block at Whitney and Regent (Single Family)/NW Block at Whitney and Wynnwood Way/Sheboygan Ave, Rezone Area D (parking concerns)/Rezone Area A
Support proposed zoning changes	(EIM12) (EF) (OC) (EVM18) (HFNA)	37	Regional Mixed Use shown in Proactive Rezone Map areas B and C, zoning plans emphasize densifying housing and service with focus on providing affordable and missing middle housing, help reduce climate warming emissions and environmental sustainability, help reach equity and inclusion goals by providing affordable housing of the City, help not push low-income people to outskirts of the city to find housing, rezoning to prepare for future growth is a good idea - better now than when the need is critical, Madison desperately needs it, hope zoning changes can help people afford to live and stay in Madison and support local businesses, support proactive rezoning at Regent & Whitney, support B, C, E as long as existing businesses do not end up paying high lease costs
Increase proposed zoning changes	(EF) (OC) (EVM18) (HFNA)	34	We should eliminate single family zoning like other cities, automatically approve upzones and transfers from single use to mixed use zoning, landowners should be able to densify by right, loosen zoning along throughfares to address ingrained racism and climate change, will allow more amenities, SR-C1 zoning mandates car use - we need more mixed-use neighborhoods for walkability, doesn't go far enough to allow for willing redevelopment, critical need now, entire city needs to be upzoned to allow more homes for people, support hierarchical zoning allowing housing in all zones, allow duplex, triplex, and rowhouses in single-family areas, all residential zoning should be increased by one level similar to TOD, single-family zoning is too restrictive, add more multi use zoning to allow for offices, restaurants, grocery stores, etc. to be near more people to allow for walkability and bikeability. Locations advocated for increases: upzoning generally along major bus and transit corridors, Midvale Heights to support more missing middle and mixed-use, Whitney Way corridor, area around Rennebohm, increase RMX along University Ave, upzone all SR-C1/SR-C2/SR-C3 to SR-V1 to support Missing Middle housing, golf courses to allow for living spaces, upzone Segoe Rd to allow rowhouses, Old Sauk area, upzone low density residential areas in SF zoning along Whitney Way from Regents to Mineral Point Rd., especially near intersections, single-family area next to higher density areas of Hill Farms and Hilldale, all of Whitney Way should be MR and mixed-use, residential area along northside of Odana Rd. between Dearholt and Wedgewood could become SR-C3 since it's near a grocery store

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Support rezoning or future land use change for churches [staff note: the initial public review draft had only proposed proactive rezoning for two sites; others were proposed for future land use map changes]	(EIM12) (EF) (EVM18) (AVM4) (HFNA)	10	If churches choose to leave, area should be Low-Medium residential to allow for townhomes or condos for homeownership, churches are near BRT, could hold off change until BRT has been running for some years, could help extend their stay because being rezoned allows them to take advantage of new financing, like mixed use recommendation to maintain community supporting area/third spaces, ensure that development is pedestrian-oriented and includes viable neighborhood businesses, to encourage this area to develop as a complete neighborhood (not just additional housing)
Oppose rezoning or future land use change for churches [staff note: the initial public review draft had only proposed proactive rezoning for two sites; others were proposed for future land use map changes]	(EIM12) (EF) (HFNA) (OC)	26	Concerns: churches will sell to developers and lead to no churches in the neighborhood, what will repurposing of churches will look like, how the change will impact the church financially or influence churches to move or stay, encourage loss of single family homes, will contribute to lack of opportunity for homeownership, parking, churches should be Low-Medium residential, take up rezoning at the time of an actual proposal, churches are community serving spaces, change in future land use unnecessary because the Comprehensive Plan already addresses it, single family or duplex homes with backyards generate a real feeling of long term residency and community and apartment buildings do not
Oppose rezone of Highlands	(EF) (OC) (MHCA)	30	Concerns: shrink minimum lot sizes, loss of historic layout and design of neighborhood - O.C. Simmonds design, impact tree canopy of neighborhood, alter building requirements, loss of park-like setting for residents and community members who enjoy walking through the neighborhood will change character of Highlands neighborhood/one of Madison's premier residential neighborhood, will not add any meaningful quantity of new homes for Madison, will lead to more housing, more pressure on existing low volume roads and need for larger roads with curb and gutter, rezoning Highlands will add high end housing that isn't needed, people who live there paid a hefty price for their neighborhood of spacious lots
Support rezone of Highlands	(OC)	3	Small change to zoning would not significantly impact the neighborhood, the existing zoning type simply should not exist in an urban area, still private residential property that cannot be enjoyed by the community, can't guarantee that these lots actually preserve tree cover, SR-C1 is restricted to 2-stories and does not have any goals concerning the preservation of trees
Clarify land use map	(OC)	2	Include escalator clause on all GFLU maps, on map it looks like you have expanded the pool area to the homes next door and changed the zoning

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Expand bike/protected shared-use paths	(EIM12) (EF) (OC) (EVM18) (HFNA)	32	Painted lines with no physical barrier are not as safe, generally support bicycle paths that are not shared with a street or have a physical barrier from traffic, use quieter streets for shared use routes rather than heavily traveled streets, Old Sauk Road needs a fully protected bike lane, bike path on Segoe south of Regent is unprotected and drivers will go into bike lane at high speeds, no SE/NW bike route at all south of Regent/Old Sauk and is major gap in bike network. Add bike path: behind Queen of Peace Church for West/East connection, connectors and bus/BRT routes, to connect path behind Memorial High Schools to other paths, along railroad line, to connect Boundary Park to Skyline/Highlands neighborhoods, in Greenway connecting Old Middleton to Camelot, to complete West Towne area path west of Gammon, to complete bike lane network through URP
Support Gammon Rd. shared-use path	(OC)	6	Avoid this area currently because of poor biking facilities, path under Beltline should be improved as noted in plan text, slip lanes are unsafe and cars are always in cross walk, it should be a protected lane
Concern about bike lanes	(EIM12) (EF) (OC)	4	Bike lane at Glenway ends in traffic, sidewalk conversion on N. Segoe Road and Old Middleton does not facilitate bicycle connections and will decrease parkland, bike lanes on Odana/Midvale intersection disappear, shared use path on Regent St. is of minimal benefit since it does not extend to Whitney Way and removes trees along Regent St.
Oppose bike lanes/bike infrastructure	(EIM12) (OC) (EVM18)	9	Bikes are moving road hazards, winters mean bike infrastructure not needed, protected lanes are not welcoming - they are unsafe and provide a false sense of security, adding so many bike paths across Madison for small subset of population that bike, destroying nature for paving. Locations of concerns: Mineral Point Road, path along Regent by Rennebohm Park (not needed and will cut down trees where traffic is low), Eau Claire Road near Rennebohm Park, already a path along University Avenue
Approve of West Plan addressing pedestrian, bike, and transit/bus needs	(EIM12) (EF) (OC) (AVM4) (EVM5)	32	Like more safe bike routes, improved bus lines and bus stops, new paths need, dedicated bus lanes should be maximized, ensure paths are maintained year-round for area residents, consider modal hierarchy when rezoning [revising GLFU] to make transit/bikes/pedestrian modes more attractive than cars, keep improving bike and non-car options, support intersection improvements for ped/bike safety. Support changes in these locations: Owen Conservation Park, Walnut Grove Park, by Stephens School, connector paths from Sheboygan Avenue to Rennebohm Park, Beltline overpass, on-street lanes on High Point, enforce traffic regulations and improve pedestrian and bicycle safety on Old Sauk Rd., Gammon Rd., widening of sidewalks on Regent, Eau Claire, eastwest shared use path connection through high school and middle school property, support intersection improvement at High Point Rd.
Approve of protected bike lanes	(EF) (OC) (EVM18) (EVM5) (HFNA) (CPFG) (CFG)	19	Provide safe biking facilities, want more protected bike lanes with some kind of barrier or additional signage, really like the bike plan, added mileage of protected two-way cycle track on major mixed-use corridors will really help reduce congestion, hopefully the need for a large parking ratio for new development - potentially down to .5 spaces per dwelling unit, youth use lanes, would like map showing protected lanes versus not protected lanes-not the same experience, in favor of all ped/bike proposals in the plan-the west side is fully dominated by cars

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Concern on pedestrian/bike safety	(EF) (EIM12) (OC) (AVM4)	29	Converting sidewalk to shared-use path and shared use streets will increase accidents and injuries, add curb extensions/bump-outs at all intersections within the 1.5 mile school walk radius, more lighting and street crossings, rail corridor needs to promote safety first, need speed limits for bikes (electric), use education and training programs for bicyclists/ pedestrians/drivers and enforcement of road rules, increase access and crossing along University Avenue and treat as a heavily traveled street, Cooper path access on Old Sauk needs improvement, Beltline underpasses dangerous for bikes, Segoe bike lane needs more protection than a striped buffer, University @ Capital dangerous - reduce Univ to one lane in each direction, Midvale @ Mineral Point not safe for pedestrians, biking on Midvale feels unsafe & Midvale needs parking and protected bike lanes, High Point-Mineral Point intersection unsafe, need to slow car traffic at Old Middleton/Eau Claire, improve bike safety at Eau Claire and Old Sauk, Segoe is difficult to cross-add bump outs to parking lanes and pedestrian oasis at intersections, Hilltop/Segoe-sidewalk moves pedestrians out of drivers' sight-make sidewalk and crosswalk parallel and reduce turn radius, Eau Claire Ave at Old Middleton Rd - terminus of Blackhawk Path - add RRFB, Midvale Blvd at the SW Path - consider automating RRFB or adding overpass, intersection of Midvale/Odana has smaller accidents frequently, add curb extensions/bump-outs at all intersections along Eau Claire and Regent (adjacent to Rennebohm Park), improve ped/bike safety at the West Towne Path and Whitney/Beltline, make north and south Gammon bikeable for all ages and abilities, Hammersley needs to be improved or removed from the bike network due to recent developments and parked cars making the street too narrow for safe bike traffic, as a biker/resident on Midvale Blvd adding an unprotected bike path on Midvale is not ideal - it would be more cost-effective to improve a parallel street, crossing Tokay/Segoe, marked pedes
Oppose/Concern about Lake Mendota Drive/Camelot Drive to Marshall Park shared-use path	(EIM12) (AVM4) (OC)	6	Concerned about its impact on private property owners, no to street/concrete, yes to the connection, the City does not provide road or waste services anywhere on this road, these homeowners together pay for road repair/reconstruction/snow removal/etc., it is private property and intended for the private use of the Thorstrand Road property owners, it should be removed from consideration as a public path, already have a bike path going all the way down University avenue that connects over to Allen boulevard
Support Mendota - Marshall Park bike path	(OC)	4	Better access to Marshall Park from this side would be amazing, connection via Camelot and Thorstrand is excellent
Support Sauk Creek Greenway Pedestrian/Bike	(EF) (OC)	14	Need for good pedestrian/bike connector E of Beltline between Mineral Point and Old Sauk Road, include path via Sauk Creek Greenway, path would make area more walkable/less car dependent/increase property values, High Point is not a good alternative, more interested in a connector between High Point & Westfield perpendicular to proposed greenway, would like path to minimize disruption to natural habitats and removal of trees

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Oppose/Concern Sauk Creek Greenway Pedestrian/Bike	(EIM12) (OC) (AVM4) (EVM5) (SCNA) (CFG)	107	Concern bike path will: harm ecology/wildlife, disrupt/ invasion of residents' privacy and backyards, cause safety issues, not preserve trees, have more concrete, not increase connections in area, lighting concerns, increase CO2 Greenhouse Gas Emission impact, create unnecessary costs, increase tax and water bills, won't be properly maintained by the City, destroy trees, will be difficult for MPD to patrol, increase stormwater issues, there are already surrounding bike lanes on High Point Road and Westfield Road that are plowed and lit, children can ride on sidewalks along parallel streets, would rather have more direct straight route, people already have access to the greenway, there are 5600 trees and estimates are that 90 percent of the trees will be cut down, would like to delay decision until engineers design creek restoration, planned bike trail is too wide, older people will unsafe, path should be on the other side of the creek, greenway is not wide enough for a path—N-S or E-W path would require taking out 100s of trees—Westfield and High Point can suffice, there is a path shown partially in the nearby High Point Road to Sauk Creek park this path should be extended to Tree Lane instead on this transit priority network per the Complete Green Streets Guide
Support Wexford Greenway Pedestrian/Bike	(OC)	1	
Oppose Wexford Greenway Pedestrian/Bike	(OC)	2	
Oppose Owen Conservancy Pedestrian/Bike	(OC)	2	
Like BRT but oppose further stops	(LCD) (EF)	2	Maybe not as practical when using for grocery trips or certain locations due to destinations being further away from bus lines and stops, especially during inclement weather
Oppose transit oriented development	(OC)	1	Remove Hill Farms from BRT Overlay, don't want multi-unit next to single-family homes
Support transit oriented development	(EF) (OC)	12	Higher density along future BRT routes, increase housing availability near transit
Support Road Diets	(EF) (OC)	9	Gammon side path is good idea, reduce east portion of Mineral Point Road to one-lane each way + protected/buffered bike lanes, implement temporary measures like paint & barriers eventually leading to permanent measures, road diet for Gammon along Memorial/Gillespie
Oppose Road Diets	(EIM12) (AVM4) (OC)	10	Mineral Point Road, Odana Rd, concerns road diet will: cause gridlock, make it difficult to get in/out of driveways and cross streets, cause accidents with merging of the lanes, will increase use of neighborhood streets, difficulty entering/crossing Mineral Point by neighborhood motorists and pedestrians, putting busy roads onto road diet forces cars onto community and neighborhood streets
Concerns on parking	(EF) (OC) (EVM18)	11	On-street parking in high density and high use areas will not be adequate, increase public use of off-street private and/or city or state-owned parking ramps or lots, additional Madison Yards parking should be underground/not facing Sheboygan, add on-street parking back to Sheboygan, moving more parking underground, developers must add more parking so new residents and business patrons don't park on streets, removal of on-street parking on Whitney Way was bad, don't need to consider parking by Rennebohm park, concerned bike paths will remove necessary parking in area, two sided parking should not be allowed on Beach Street- Emergency vehicles have trouble navigating this area, eliminate parking

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Additional traffic turn signals	(EIM12) (OC)	4	Whitney Way going east on Mineral Point Road, In/out of Memorial High School, stop sign on the Presidential/Tocora Intersection, for Tocora Lane where yield signs are now, Sheboygan/Segoe needs stop light
Concern for proposed street changes/planned streets	(EF) (OC) (AVM4) (EVM5)	66	Rennebohm Park-width of sidewalks-biking may conflict with people trying to walk, street is unnecessary and can be dangerous for park goers and trees, would cut park space and not allow room for community garden, do not need additional parking, street through TruStage is not safe, oppose streets by/through Karen Arms/street through the Monticello Aptswould result in loss of housing, proposed street off Old Sauk is on a steep hill in area that already has traffic issues, remove Laub Lane & keep Berwyn Dr, block off Berwyn Drive access to S. Segoe Road, don't change Berwyn/Laub/Segoe intersection, concern opening Middleton Street up to through traffic, don't make Cable Ave a Stephens drop-off site - it's part of a low stress bike route, don't study St. Dunstan's Dr for two-way traffic, no to connecting Sauk Ridge Trail to Appalachian Way/no Appalachian Way connection to Old Sauk- use as greenspace, too many planned streets in West Towne Mall area, oppose new north-south street along Cooper Lane Bike Path to connect with Appalachian Way, Concern over planned streets East of Whitney Way and North of Tokay Blvd, planned streets in West Towne Mall area - should be used as more park space, don't put N-S street in on TruStage property-not needed, proposed straight streets are boring/not creativeif proposed streets were curve it would add interest/slow traffic, proposed streets adjacent to Old Sauk are not needed, Yosemite was a through street at one time - the reason it was blocked and turned into a cul-du-sac was for traffic and pedestrian safety on Yosemite and Old Sauk Roads - the direct access to Old Sauk encouraged traffic and the slope of the street contributed to high speeds on Yosemite - it was a good idea to block Yosemite then and a terrible idea to return it to the same dangerous conditions now
Expand Connections	(EVM18) (OC)	4	Connect the Highlands to Stonefield and Harvest Hill, no bike path currently from Brule street to Tree Lane - excellent area to install a shared use path, add painted bike/parking lines on High Point Rd., connect the east/west path to the on-street bike path at Regent instead of having to go north to go back down Rosa to continue going east or to access Garner Park
Support street changes/planned streets	(OC)	7	Support additional crossings of the Beltline, support a new street between Sheboygan and Rennebohm Park- proactively addressing the traffic needs when more dense housing is built in this area-these roads will help alleviate traffic congestion without reducing Rennebohm park access, support URP street connections between Science Dr. and S. Whitney Way and Science Dr. and Tokay Blvd., support removing Berwyn Dr at Laub and enhancing ped crossing

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Streets	(EF) (OC)	19	Lower radius on wide/add maximum desirable curving radius for curbs, curved turning radius to reduce speeds coming into neighborhoods, install roundabouts at Gammon/Old Sauk and Gammon/Tree, Old Sauk by Crestwood needs a drop-off zone on the N side, no more speed humps, new street across Tru Stage property should intersect Rosa and not Mineral Point, reduce existing neighborhood street connections to Mineral Point/Midvale/University/etc. (modal filters), narrow all traffic lanes to 9' to reduce speed and make room for sidewalks, construct speed humps on Regent St, monitor speed on Segoe, intersection safety improvements @ Whitney Way & Odana, focus on crosswalks vs sidewalks, signal timing adjustments needed at Midvale-Regent intersection left turns, need signal or roundabout at Old Sauk-Old Middleton intersection, slip lane from mall to southbound Gammon is unsafe and drivers can't see people on path, Mineral Point between Midvale and Glenway should not be considered a neighborhood street due to volume of cars, Medium Residential at northwest corner of Segoe/Mineral Point will result in apartment replacing Covenant church and increasing already congested intersection due to school traffic, additional mitigation is needed on Whitney Way north of Mineral Point Road to decrease vehicle speeds and make the street safer for pedestrians, make Whitney Way and south of Mineral Point Road a car light street in the higher density area to promote pedestrian safety -similar to State Street
More Sidewalks/Pedestrian Routes	(OC) (AVM4) (HFNA)	23	Ped-only corridors close to University and Hilldale, close sidewalk gaps (e.g., Tocora, north side of Old Sauk), Old Sauk needs sidewalk west of Crestwood school, Olympic Dr missing sidewalks, better crossings at Whitney Way with flashers, support people first, pedestrian friendly infrastructure, improve access & crossing Mineral Point @ Gammon and by Memorial High School/High Point/Westfield, West Towne needs to be more ped friendly, sidewalk gaps shown on Rosa & Elder Pl where there are sidewalks, missing sidewalk on Sherwood Rd., crossing Midvale at Ames connection, prevent left turn northbound Midvale at Ames, secret sidewalk that leads to a crossing for school access-add bump out in the parking lanes, add midblock ped crossing with RRFB between Mineral Point and Rosa to improve access to Garner Park, add a stairway where there is a zig zag sidewalk to access bike path at northwest corner of Gammon/Beltline, it would be great to have car-free and car-lite streets in higher density areas to promote pedestrian safety and walking/cycling - similar to State St. downtown, sidewalk on one side at Hilltop just south of Segoe
Lighting	(OC) (AVM4) (CPFG)	5	Support Gammon-Beltline underpass ped lighting, increased street lighting, consider dark lighting, more lighting along: Tree Lane Rd, Garner Park, along bike paths (motion lights), Greenbriar bike path, back of Lussier, support increased lighting on shared use path by Ezekiel Gillespie Middle School, concerned about how much light shines upward from the areas extending northeast from the Gammon/Mineral Point area, if lighting is added please take care to limit uplighting
Support Old Middleton-Craig RR Underpass	(OC)	2	
Transit improvements needed	(OC)	2	Need a N-S transit route on Midvale, park-and-ride needed at West Towne
Bus/Bicycle/Intermodal Infrastructure	(OC) (CFG)	5	Add bike racks, B-cycle stations and bike lockers near BRT & other bus stops

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
High Injury Network	(OC)	6	Intersection of Felton PI and Falles Ct-Zero crashes in 25+ years, HIN should highlight intersection of Midvale and Mineral Point Rd which lies 400' southwest, other similar errors may be corrupting the HIN map, MPD do not file reports for bicyclist and pedestrians hit by cars unless seriously injured, MPD should report all non-life threatening injuries, City should also have a self-reporting feature for calls not responded to by MPD, HIN on Gammon is concerning since it is a route to Memorial/Gillespie Middle, disagree with W of Eau Claire and Sheboygan intersection identified on map, disagree with Gammon Place being shown on this map
Convert the Beltline to a street	(OC)	2	Please add in the adjoining areas on the other side of the Beltline to your plan
Beltline underpass improvements	(OC)	9	Underpass floods frequently and ices over in winter, include more information on changes to crossing of Beltline (Whitney Way) where bikers are expected to cross from SW Commuter path to West Towne Path, Old Sauk underpass bike lane is not in great condition and is not protected, City should take responsibility for reducing noise by reducing car dependence and improving public transportation/walkability/bikeability throughout the city and not just development along West Beltline Highway, crossing at Mineral Point Rd and Beltline is dangerous and should have improved crossing signage, restart abandoned bike path along High Point Road-should connect to the Beltline Path
Oppose the proposed bike/ped crossings of the Beltline	(OC)	1	Underpass/overpass at south of Old Sauk Rd is superfluous, too close to underpass at Old Sauk Rd and will interfere with sound barrier
Support the proposed bike/ped crossings of the Beltline	(OC)	3	Excited to see important connection south of Research Park Blvd and increased access to ball fields and ice arena, excited for extending path south of West Towne Mall
Oppose rezoning Planned Developments (PDs)	(OC)	2	Oppose the review of Planned Development zoning
Miscellaneous	(OC)	(single comments - see details at right)	Give Garner Park vibrancy - HDR (10 stories+) across Mineral Point Road from the park, rail corridors should be studied & repurposed for Metro light rail/passenger rail to complement BRT and reduce car dependence, rather than sidewalks in Glen Oak Hills/Crestwood/Highlands - install traffic calming measures like chokers/chicanes to create Woonerfs, allow 30 story bldgs. at West Towne Mall, ban electric motor vehicles on bike paths-unsafe, I do know that the Beltline will be with us for a very long time but we need plan to convert it to a regular street now
NOTE: sidewalk comments summarized in Health & Safety section			
Neighborhoods and Housing			
Want more ownership opportunities with new development	(EVM18) (OC)	9	Apartment ownership in Madison should be explored, affordable condos, want more owner occupied housing
Want more missing middle housing	(EIM12) (EF) (OC) (AVM4) (EVM5) (CPFG)	13	Include escalator clause in plan, be intentional about adding missing middle, more missing middle housing can be done, concerned about multifamily housing, examine other existing barriers to missing middle, want townhouses adjacent to each other (not duplex style), want missing middle-not large apt buildings, want missing middle-hard to create sense of community when everyone is living in a high rise
Like addressing missing middle in Plan	(EF) (EVM18)	4	Missing middle and small multi-unit buildings along collector streets and larger buildings along arterials, middle housing can greatly increase neighborhood density and tax revenue

11 5/1/2024

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Concerns about developers	(AVM4) (EVM5) (HFNA) (OC)	4	Developers need to pay their fair share-specifically regarding schools, certain developers have their input weighed more than the community, out of town investors will dramatically change things for the worse, we have a severe housing ownership deficit and don't want investors buying up houses
Rent Assistance / Rental concerns	(CPFG)	2	Want rent assistance and rental resources, not fair management practices in Tree Lane and Wexford, landlords are less likely to rent to families, want better conditions
Want more diverse housing stock	(OC) (CPFG)	3	Want more family friendly spaces, in multifamily units-people have issues with families (noise, etc.), more affordable options, want diverse housing stockbut in strategic locations to minimize disruption to current/future residents
Prioritize and ensure affordable housing options within new development	(EIM12) (LCD) (EF) (CPFG) (OC)	9	Plan for affordable rent and ownership, don't lure developers to develop new unaffordable housing, can upgrade existing development to be more sustainable, opportunities for more ownership with new development, incentivize developers to build more affordable housingWhitney Reserve and Madison Yards are expensive
School comments [staff note: the City does not control school attendance areas or school expansions]	(OC) (AVM4) (HFNA)	7	Don't change school attendance boundaries, school expansion needed @ Hamilton/Van Hise, work with MMSD to avoid overcrowding in schools with new development, funding comes with additional students, reconsider school related actions as the City does not control school expansions, confirm Lincoln Elementary School attendance area
Miscellaneous	(OC)	(single comments - see details at right)	Prioritize City funding assistance for missing middle - not large apts, encourage expansion of Lussier programming, physical barriers needed along Beltline-or a larger plan to reduce traffic and mitigate noise-suggest adding green space/trees
Economy and Opportunity			
Oppose/ Concern with boundary of Tax Increment Districts [staff note: being in a TID boundary does not mean land would have to develop/redevelop]	(AVM4) (OC)	4	Concerned about the implications for TID 46 and boundary, impact of parks within boundary, against development of 2 city parks and HF pool due to being in TIF boundary
Agree with increasing small business funding from TIF	(OC)	1	Especially BIPOC-owned businesses
Agree with increasing childcare options	(OC) (CPFG)	5	
Taxes	(OC)	2	Study the impact of low-density housing on tax revenues - observe the cost to serve the property vs the tax revenue paid for the property - pair finance with Planning to understand how zoning can impact the City's tax revenues
Land banking	(OC)	2	Strengthen language beyond "consider land banking"this is important for improving economic opportunities for underrepresented groups, consider land banking for Highlands to convert to small conservation areas for large lots
Culture and Character			
Preserve character of neighborhood(s)	(EF) (HFNA) (OC)	3	Historic district, do not want to change, opposed to housing in a National Historic District being part of TOD overlay

12 5/1/2024

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Design elements changes	(EIM12) (LCD) (OC) (AFG)	17	Include requirements for aesthetic architectural design (no ugly housing), greenspaces and trees, implement pre-reviewed building plans to allow for faster approvals/construction, architectural approval for very large developments, remove/reduce restrictions so different looking buildings are approved via UDC, more creative building designs, support for "Commercial Core" design requirements, add stipulations to reduce light pollution and protect trees, add murals to blank building walls that lack architectural variety
Oppose/ Concern UDD #6 boundary changes	(EIM12) (EF) (OC)	11	Oppose boundary further south, boundary should not include Rennebohm Park, the south side of Regent street, or existing streets west of Eau Claire Avenue since the TOD Overlay Zoning in those areas differ from UDD, want specific area, add curb extensions at all intersections and midblock crossings every 500'-need to make sure that when development happens is in conjunction with making the surrounding area safer for peds/children, added boundary area in purple rectangle that includes Rennebohm Park should not include single family homes, it is inappropriate to use an UDD to promulgate changes in zoning, it is irrational to include a residential neighborhood a half mile away from University Ave. to this district while removing houses in an essentially identical residential neighborhood area directly abutting University Ave., UDDs add unnecessary bureaucratic red-tape, we should not be expanding UDDs, but reducing them or replacing them with form-based codes, new development needs to complement existing development
Support UDD #6 boundary changes	(OC)	1	Extend UDD 6 to include all of Hilldale Mall
More things to do	(LCD) (CPFG) (CFG)	3	More social spaces in new development, entertainment venue, community center, Hilldale Male like areas,
Support of Arts/Build on arts infrastructure in West Area	(AFG) (OC) (EF)	5	Indoor gathering spots needed for artists to gather, more communal studio space, makers spaces, pop-up opportunities and events for music and arts, more art resources, support wrapping utility boxes-sidewalk poetry-and other things that include the community, support West Plan inclusion of art spaces in Research Park - thank you for including affordable artist studios, exhibition space, etc., in this plan - it is sorely needed on the west side which can feel like a cultural wasteland - artists need it and it would enrich the general public as well
Pools	(CPFG) (OC)	3	Show pools in facilities, issues with accessibility for local private pools for some, highlight Hill Farm Pool significance to West Area
Local Historic District	(OC)	2	There is not much in the way of historical significance in this area that's worth preserving over potential redevelopments that better meet city and community needs, National Register sounds preferable to Local Historic DistrictsMadison needs to be able to changeconcerned about resistance to changes
Youth Sports Complex/ Facilities	(HFNA) (CPFG)	2	Madison needs a new or renovated sports complex for youth - all cities and towns outside of Madison have updated facilities for youth - specifically baseball and softball fields - there are not enough and low income people don't have an option, previously lots to do for youth - but now staffing shortages and shortage of activities - Lussier is a great resource - but not many other options

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Miscellaneous	(OC) (AFG) (CFG)	(single comments - see details at right)	Support neighborhood-oriented events in underserved areas, support educational plaques for Ho-Chunk, wrapping utility boxes is minor worry - while at same time plan drastically changes longtime and beloved character of Hill Farms Neighborhood, if we're considering the historical significance of the Highlands area land use the Plan does not do enough to consider the 12,000 years of Ho-Chunk land use precedent - the City should follow Vancouver's example of the development at Seńakw as reconciliation to the Ho-Chunk Nation, UDD 3 - overriding the requirements to maintain the character of the neighborhoods seems arbitrary and without any evidence simply offering the judgement of "auto-oriented", preserve viewsheds - Old Sauk view blocked by Hilldale area
Green and Resilient			
Increase/preserve greenspaces and parks	(EIM12) (EF) (EVM5) (AVM4) (HFNA) (OC)	20	Continue to provide green areas for newcomers and dog owners, more density creates need for additional park area -especially around South of Mineral Point Road and around Hilldale, preserve trees in Rennebohm, preserve greenspace in University Research Park as it redevelops, more parks - not enough greenspace, ask for parkland dedication for Rennebohm Park, preserve Indian Hills Park, turn golf courses to parks, preserve Kenosha greenway and add as extension of Park between Garner and Glenn Oaks Park, more trees in parks, plan needs to include new greenspaces given all the new development proposed in the plannon-humans need habitatbirds, mammals, invertebrates, insects, etc., the greenspaces should connect for the benefit of wildlife/bikers/hikers-show on a map how the greenspaces could be connected and let residents comment on the proposal, invite you to make parkland more accessible - Odana Hills Golf Course is not for multiple users and there are really only tiny micro parks in the area around it, work with developers of large apartment buildings to increase tree canopy and green areas, New construction in TOD has no open space requirements and minimal terracesCity will need to provide small pocket parks
Like that West Plan address parks and greenspace needs	(EIM12) (OC)	4	Support buying land for more parks, like Vernon Blvd. greenspace
More tree canopy/nature	(EIM12) (EF) (OC)	18	Add to shrubs/plants/trees, avoid removal of trees with new development/BRT/paths, work with Urban Forestry Task Force and report of 2019, do not use grassy swale approach, work proactively to minimize threats of spongy moths and other infestations for trees, keep mature trees, mandatory native vegetation, ban pesticides, provide programs/ initiatives/ grants/ education/zoning requirements for private property owners, this part of Madison is lower density with lots of trees and greenery-should quadruple the amount of new green space you are calling for, plan proposes too much development. Locations for tree preservation/new trees: preserve trees at Whitney, Sheboygan, along S Kenosha greenway, NE of Whitney Way and Mineral Point Rd, increase tree canopy in Rennebohm Park and University Ave, plant trees on the median strips on Regent, Segoe, Whitney Way, and Mineral Point Rd
Stop cutting mature trees	(OC)	3	Stop cutting mature trees down unless absolutely necessary, don't remove old growth oaks
Support West Plan addressing tree coverage	(OC)	4	Support increasing tree canopy, increase terrace widths, need to water new street trees to ensure success
Want West Plan to better address climate crisis	(EIM12) (OC)	4	Climate change increases rainfall issue

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Concerns about stormwater	(HFNA) (OC)	4	Plan states that stormwater is a significant issue in this areabut Plan calls for more impervious surface, concerned about the impact of higher density on stormwater issues, area south of Mineral Point road for increased stormwater holding to reduce the need for Pheasant Branch Creek to be the outlet valve in a 1% storm- consider decreasing impervious surfaces in commercial areas north and south of Mineral Point
Support West Plan addressing stormwater needs in area	(EF)	4	Support efforts in Old Sauk Road down through East Spyglass Court, need to address flooding
Support West Plan addressing stormwater needs in Sauk Creek Greenway	(EIM12) (OC)	6	Addressing flood issues in Sauk Creek; concerned about erosion due to lake of maintenance
Oppose West Plan addressing stormwater needs in Sauk Creek Greenway	(OC)	2	Do not change the greenway in the name of "stormwater mitigation"
More paths in parks and greenways	(OC)	2	Gravel path from Yellowstone through Owen would increase hiking routes; more natural surface paths in greenways and parks
Support future planned parks	(OC)	2	Walkable areas and the ability to host events would be welcomed, park west of West Towne Mall
Concerns about future planned parks	(OC)	4	Park space B conflicts with brand new development, expand park space A instead, park names should come from local Native/Wisconsin tribes, there are 500 acres of open space within 1.2 mile radius of proposed park off Normandy Lanehow can the City justify spending \$20M to purchase developed parcels and remove them from the tax base, do not expand Rennebohm Park into/toward Karen Arms
Support park improvements	(OC) (CPFG) (CFG)	4	A splash pad at Garner Park sounds like a good idea, expand Lussier playground, improve Wexford playground, mountain bike trails in parks would be fun, add pump track/skills course (like at Leopold) to current Odana Hills East Park's playground area, Walnut Grove tennis courts
Do not build road on northern end of Rennebohm Park [staff note: the public review draft shows a planned street <i>north</i> of Rennebohm Park, not in Rennebohm Park]	(OC)	8	Adds little value, adds cost, takes away open space, increase access by putting in a path instead, the proposed drive on the north side of Rennebohm Park goes far beyond what was proposed in the University Hill Farms Neighborhood Plan - the neighborhood plan only proposed a quiet noncontinuous park side drive to provide additional access to Rennebohm Park and intentionally did not go from Segoe thru to Sheboygan Avenue (please see page 36 of the University Hill Farms Neighborhood Plan), building this street would limit the free movement of people and children to and from the park, additional ped/bike access to the park is goodmore car streets around the park is bad, proposed streets are redundantif these streets are needed developers should construct and maintain
Concern plan does not cover all environmental considerations	(EVM18)	2	Hydrology benefits and pathways of contamination associated with our rich but complex groundwater resources and drinking water and impacts on our recreational uses of the west side of Lake Mendota, has an environmental analysis been done on the impact of all this added density in terms of tree canopy/impervious/habitat/noise/traffic etc.
Build a public pool	(OC)	2	Acknowledging that the Hill Farms Swim Club is an amenity, the City should identify a future priority of adding a public pool in the area

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Repurpose Odana Hills Golf Course	(OC)	8	Restructure as a mix of park/recreational spaces and multi-family housing, include other recreational uses that are more equitable and sustainable than golf, support clubhouse with a year round facility, golf courses often have paths through them and could probably be linked together with little disruption to make a longer hiking path, repurpose for dense housing, sell land for dense housingplug City budget deficit
Rain Gardens	(OC)	2	More rain gardens, rain gardens in large terrace bump outs where parking isn't allowed currently
Miscellaneous	(OC)	(single comments - see details at right)	Support greenway shown to west of West Towne Mall, change from "Oak Park Heights Park" to say "Piper Park", Zook Park - increase connectivity between park, surrounding neighborhood, and Southwest Commuter Path via a paved path, encouraging responsible exterior lighting (staff note: already in lighting code), support Vernon Blvd. reduced street width, Spring Harbor Boat Launch is not accessible enough, Reduce air pollution of internal combustion engines (ICE) vehicles by incentivizing EV & hybrids instead or increasing ICE registration fees
Effective Government			
Want budget and cost information/estimates for proposed changes	(EIM12)	2	
Build awareness on City plans and polices	(EF)	2	
Support expanding Lussier Community Education Center	(EIM12) (LCD)	2	
More libraries	(AVM4) (OC)	3	Develop a strategic plan for the Library's west side footprint, and better align to areas of high growth and the new redesigned transit network
Miscellaneous	(OC) (CPFG)	(single comments - see details at right)	If Rennebohm will be a polling placeadd walls, want more resources-like MOMs resource center, limitations to accessing neighborhood centers due to living location, tackle unbridled spending and deal with 27 million without burdening property taxes, Want less police presence as they sit in certain neighborhoods with no explanation - want more intentional interactions and create more neighborhood connections, more residents - need more police
Health and Safety			
Opposed to sidewalks	(OC) (EF)	11	Old Sauk Road northside sidewalk from Crestwood School to San Juan Trail-loss of trees, along north side of Old Sauk Rd, Harwood Circle North, Walnut Grove Neighborhood, concerns about 10' wide sidewalks impact on trees along edge of Rennebohm Park, sidewalk along north side of Old Sauk will be expensive and require a retaining wall which has a long-term maintenance cost, concern about shared use path along Regent - peds/bikes should not share the same path as its dangerous for people with disabilities, prioritize fixing roads over sidewalks and bike paths, northern section of Hilltop (400~450ish block) would prefer to keep that section sidewalk-free, Dearholt does not want sidewalks- recommend canvasing the street to get a more complete picture of resident preferences
Support West Plan shown sidewalks	(OC)	3	Support closing gaps in the sidewalk network - prioritize routes used by children to get to school and residents to get to bus, and routes with no sidewalks on either side, many sidewalks segments that have been created with developments that make no sense- should extend beyond the limits of the property, sidewalk connectivity is critical for safety and pedestrian access to UW Health and the offices on Science Dr and to Sequoia Commons

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Want to add sidewalks/ pedestrian safety	(EF) (OC)	4	Sidewalks on both side of Capital Avenue, corner of Capital/Taychopera Rd., infill sidewalks (relevant streets are: Hilltop, Hillview, Tocora, Edward, Ames, Agnes-only on one side, Sherwood Rd.)
Support actions listed	(AVM4) (OC)	2	Specifically the partnerships actions, prioritize keeping Tree Ln residents in apartments
Concerns on community health care	(HFNA) (CPFG)	2	private equity investors have reduce service and raise rates, changing community health care and hospitals, mental health resources
Concern over traffic and speed	(AVM4) (HFNA) (OC)	4	Like recommendations, but enforcement would be a challenge, lower speeds on Mineral Point heading east towards Science Drive, cars on Segoe continue exceeding new 25 mph limit plus more traffic with new apt. units on Whitney Way, enforce new 25 mph on Whitney Way as drivers still do 40 mph or more
Install speed cameras that issue tickets	(OC)	2	
Miscellaneous	(OC) (CPFG)	(single comments - see details at right)	No halfway houses within 1 mile of schools; mandatory background checks for rentals, want safe/quiet/low crime neighborhood/community, want to add emergency poles (implemented by/within trails and dark paths), no need to have engagement at Tree Lane housing since housing is being sold
Planning Process/Other			
Engagement concerns/suggestions	(OC)	5	Reddit users drawing outside influencers to flood support for their opinions and they don't even live in the neighborhood in question - please require input to show residency proof, please listen to Hill Farms homeowners (don't feel heard), don't feel safe providing comments in public venues due to neighbors being disrespectful - but provide comments on Konveio, Outreach to stakeholders has been low, survey response rates low - more outreach and time for commenting needed
Support for planning process	(OC)	4	Greatly appreciate the work that you do -watched the March 12 public meeting and am embarrassed by the behavior of my fellow community members, appreciate work being done to think about the needs of the city as a whole - including taking into consideration the needs of those that don't have the capacity to actively participate in these planning processes (e.g., those working multiple jobs, raising children, studying in college, etc.), I've lived on West side for almost 60 years and thank those who put countless hours into this plan, please add in the adjoining areas on the other side of the Beltline to your plan
Issues with plan language	(OC)	2	What is a TID? no wonder people don't understand or trust what the city is saying when they don't explain abbreviations and use jargon - use plain English., have list for acronyms
Homeowner "Veto"	(OC)	6	Homeowners should not have veto power over efficient land use - they are obstructing sustainable growth and affordable housing options
General Plan Support	(OC)	7	"I support this plan"
Map corrections	(OC)	2	1/2 of Science Drive has sidewalks both sides, a small portion of Herrick, near Odell, only has sidewalk on one side

COMMENT SOURCE MULTIPLER AREA/DETAILS

Phase 3 Engagement

(HFNA) Hill Farms Neighborhood Association October 18

(CPFG) Community Partners Focus Groups October 18 & November 21

(CFG) Chinese Focus Group November 7

(AFG) Arts Focus Group November 13

(SCNA) Sauk Creek Neighborhood Association November 15

(MHCA) Virtual Public Meeting with Highlands Community Association January 31

(LCD) Lussier Community Dinner March 1

(AVM4) Virtual Public Meeting March 4

(EVM5) Virtual Public Meeting March 5

(EIM12) In-Person Public Meeting March 12

(EVM18) Virtual Public Meeting - Zoning Focused March 18

(EF) Emailed Feedback

(OC) Online draft Map and Action Comments

(CP) Community Partner Comments (The West Area Plan hired Community Partners to focus and highlight feedback from underrepresented communities during this planning process. Partners reached over 275+ residents during this planning process).

1122

Staff has reviewed, categorized, and summarized public feedback received since the Phase 3 Feedback Summary from April 20, 2024 to June 10, 2024 through the activities shown in the legend at the bottom of the document. The legend also includes abbreviations used within the document to identify where the various comments came from.*

West Area Plan - Community Feedback Summary

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Land Use			
Approve of land use changes	(OC)	9	Locations: University Triangle area, Mineral Point Rd and Regent (Area D), Land bounded by Eau Claire- Sheboygan-Old Middleton-Hill Farms Office Building; excited about dense mixed use, like keeping more flexible options for future zoning, more density would be great for area, triangle is currently low-intensity, strip-mall style development, the kind of thing you'd expect in a vestigial ex-urb, Appreciate NMU allowing for some commercial amenities/services-not many in neighborhood, area D, MR is covered by NMU and NMU allows for businesses, I appreciate that not every suggested change is downzoning when the City should really only be upzoning, Area C back to LR, Love the idea of having more dense mixed use in this area (Whitney-University-Old Middleton triangle), really help develop the area around the BRT into a vibrant neighborhood that's accessible to people who can't or don't want to drive, It has the potential to give people without a license a lot more independence and could remove the burden of car ownership from residents
Concern/Oppose land use changes	(EF) (OH20) (LZH3) (OC) (WRM) (T6)	67 (37 signees for northwest block of Whitney Way and Regent via email)	Locations: Pierstorff parcels, Old Sauk, northwest block at Whitney Way and Regent, 6021 Old Middleton, 25 Veblen PI, Wynwood Way, Whitney Way, Wynnwood Way/Whitney Way/Old Middleton corner/triangle; Concerns: not in scale nor proportion to the surround area, detrimental impact on the traffic, design of development in relation to existing development, do not want urban high-density development, mega structures cannot be integrated into surrounding neighborhoods, will add to environmental issues like flooding, northwest block at Whitney Way and Regent (want to remain low-residential land use category-want area to remain quiet, keep tree canopy, limited light pollution, safe trees for pedestrians and cyclists, up to two stories in neighborhood), do not want Old Sauk Road to become an apartment corridor, want Old Sauk to remain a nature-oriented settings with suburban setting with parks, trees, and limited commercial access, want Old Sauk development to be limited to single family detached residences or duplexes, Would rather encourage properties that have greater development potential to develop than recommend these single-family homes for development, ideally would like Whitney Way area to be a smaller, neighborhood-serving commercial uses added as an amenity for area residents, do not put hundreds of families in mostly concrete apartment buildings in commercial districts, Keep residential areas residential, no recognition of how high, how dense, or how fast the city is willing to grow, worse- there is no recognition of how far extant, long-term residents are willing to, and should have to, go in the city's process of accepting a greater and greater and greater population within fixed city limits, focuses on alternatives to high density, rental developments, get rid of any rezoning in or adjacent to neighborhoods, Use existing space for apartment building, change land use designation on Parkwood Hills back to what it was, oppose any zoning changes that would allow higher density in the Hill Farms area near Renne
Oppose land use changes for Stone House development and Old Sauk Road	(EF) (OH20) (LZH3) (OC)	13	Changing land use is not the appropriate response or solution to increase housing options, not in scale nor proportion to the surround area, detrimental impact on the traffic on Old Sauk road, design does not align with housing in vicinity, no setback, no mature trees, monstrous apartment building-ugly and unwanted eyesores, Heavier traffic, higher vehicle density, increased use of neighborhood streets parking, noise pollution, light pollution, irreparable effect on wildlife, higher runoff due to removal of trees and vegetation causing flooding events, increased danger to commuting area bikers are some of the unwanted, critical and imminent negative effects of these potential developments, do not want Old Sauk Road to become an apartment corridor, residents want to live near nature, should promote homeownership not people dependent on rentals, will block light of neighbors, other multi-unit housing complex has brought too many police reports, lack of parking concerns
Oppose land use changes for places of worship	(EF) (LZH3) (OC)	4	Locations: St, Thomas Aquinas; want to preserve greenspace, mature trees and quiet residential setting, work proactively with places of worship about plan recommendations and property potential while seeking opportunities to keep or relocate places of worship in neighborhoods, opposed to rezoning church at Regent and Whitney Way

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
More density/housing	(OH20) (VLZH28) (T6) (OC)	23	Way more density, smaller lot sizes, more upzoning, more condos, apartments, commercial space, changing means the development of Madison, more mixed use development-would like more shops and restaurants to be in easy bike/walk distance on the west side, plan is still not going to accommodate all the people looking to move to Madison, need more proactive rezoning, take whole city and future residents into consideration as much as current residents of the area, Upzone the entire city, End single family zoning and low residential land use designation- was created to be exclusionary and leads to unsustainable land use patterns, West Area is one of the least dense areas of the city that is inside the beltline, If Madison is going to be able to accommodate future population growth, it needs to be able to grow everywhere, Certain wealthy areas should not be preserved in amber, more density near transportation networks, Allowing for smaller lots in Madison would be a way to support infill development and allow families to find housing within their budget range, all LR should be moved into LMR or NMU, LR is severely restrictive for a city in a housing shortage, This plan is looking at decades, not next year, and by restricting what can be developed as we continue to grow will cause severe issues until we can approach the subject again in a decade, LR areas in TOD overlay should be LMR, Add LMR all along Whitney Way, area near Sequoya Commons should density, more Sequoya Commons-like mixed-use developments everywhere in West Area, allow 5-story mixed use and residential like Sequoya Commons all along Whitney Way, more density in the TOD overlay areas- especially along Whitney Way, proactively upzone align the TOD corridor to allow more people to make better use of the transit, Increasing density and allowing for mixed uses would reduce demand for transportation by allowing people to live closer to where they want to go and allow for more walking/biking trips as opposed to car dependent transit
Oppose less housing density and mixed use shown in Plan	(OH20) (VLZH28) (T6) (OC)	27	Locations: Area C-should go back to LMR, Area B- should at least be LMR, northwest block of Whitney Way/ Regent Street, Area E; Disappointment about people wanting less housing density and less mixed use, more residents are coming to Madison regardless- need to increase dense development to accommodate for growing population, will pave over farmland and the suburbs instead if people are not in Madison, More traffic and sprawl, Allowing more multi-family units will reduce the city's maintenance budget, adding more housing is critical for affordability, love Sequoya Commons, Need more housing of ALL KINDS near transit, shopping, and jobs, Small, larger, apartments, owner-occupied, condos, and single-family housing, more upzoning, disappointed that upzoning in existing built areas (Highlands, Faircrest, etc) has been shot down, any changes would be slow and people are overreacting, support multifamily, disappointed that church parcels no longer planned for possible higher-density developments, Disappointed that Highlands parcels not able to be subdivided without doing a PD/conditional use, zoning designation that can allow for more densification and/or variety of uses next to BRT stops as an alternate to land use designation, neighborhood mixed use allows for a lot of different types of housing and for businesses that can people close to home and work, It can promote neighborhoods that have vibrant communities and healthier lives with less car dependence, should not be limiting what can be built here for decades, NMU is a great way to allow this neighborhood to develop naturally rather than rigidly, LMR is better than LR, but NMU would allow the community to thrive and bring about more things like libraries, community pools, and cafes, goes against every goal that exists in the Comp Plan; much of LR should be LMR; plan sets us up for failure w/ costly housing, sprawl, increased traffic, less revenue; plan is regressing through continued reduction of density, Areas should be proactively up zoned to higher density
Approve of updates to northwest corner of Regent and Whitney	(EF) (OC)	3	good balance of feedback from Madisonians (including me) who want more land available for higher density housing and those who expressed concern about excessive impact to neighborhoods
Approve of proactive rezoning	(T6) (OC)	4	Long range upzoning, change is slow, let's get the ball rolling
Oppose proactive rezoning	(OC) (T6)	3	Oppose any proactive rezoning, consideration of any zoning changes should only be made with input and approval of current residents and only when such zoning changes are necessary, rezoning won't achieve what you want
Support/increase shown heights	(OC)	2	West Towne should have height limits over 20 stories, support heights along MPR across from Garner Park
Oppose shown heights	(OH20) (T6) (OC)	6	to restrict building height to three stories in blocks or adjacent to residential homes, There should not be any changes that significantly affect the value of a person's home as it is often times the asset to fund their years in a care facility, Don't like the extra tall apartment building along Mineral Point Road, reduce building heights in plan- especially in TOD areas to not lose more vistas, oppose any increase to heights of new developments, No 10-16 story buildings, Height restrictions are arbitrary and limiting for a plan that is setting the stage for decades. We need housing, If a developer wants to build a dense building-don't make it harder, We should be providing incentives for density, not disincentives by making it more challenging
Approve plans for Odana area	(OH20) (T6)	2	Love proactively planning for Odana area, hope is with the BRT, that area can become a great mixed-use area that will be close and usable for many neighborhoods, hopeful area will be a great housing/commercial/restaurant/community destination, would love to see it have a Vilas/Sequoia Commons for the far west side
Concerns for zoning for Odana area	(OC)	1	All of this employment zoning is end of life strip malls, class C office space, and car dealerships that will shortly decamp to the suburbs like the way they left downtown, If given more flexibility, zoning-wise, the Odana area could actually become a dense, mixed use secondary town center,
Approve of Highlands neighborhood rezone removed from plan recommendations	(OC)	1	
Oppose change to Highlands neighborhood	(OC)	4	This change should be reverted, The Highlands area takes up a lot of space for very little housing, should allow for smaller lots in this area, Smaller lots allows for more affordable homes, If we aren't going to make for smaller lots, we should increase the property taxes on the existing lots as they require significantly more resources per person than smaller lots do, should be upzoned, calling Highlands rural is ridiculous, huge lot size is ridiculous, zoning doesn't protect nature or owner from clear cutting lot
Oppose Highlands neighborhood rezone (Staff note: Proactive rezoning proposal for the Highlands area were removed)	(EF) (OH20)	2	Lose park-like greenspace, only provide limited housing-probably higher-priced, recreational space for the community, 70% of the properties have a new citizen-initiated covenant that preserve TR-R zoning code
Do not want more rental apartments	(EF) (OC)	2	Primary issue is again/still only more rental apartments excluding/precluding multiple forms of non-rental development
Family oriented neighborhoods (single family housing)	(VLZH28) (OH20)	4	Important and critical family oriented, residential neighborhoods are to the good quality of life in Madison and needs to be preserved while addressing the housing needs, It can be done, offensive and unfortunate that residents who believe this are immediately labelled as NIMBY's and old, white entitled people, Every piece of land that becomes available in the City should not be a priority for high density rental housing, There is still a place for residential neighborhoods, Concerns are dismissed as elitist and racist, Our concerns for stormwater management, noise, increased traffic are dismissed, disagree with needing less single-family zoning in area- City says homeowners are wrong, homeowners helped build this city, no multifamily buildings, redevelop commercial areas and parking lots into single-family houses

7/10/2024

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Miscellaneous	(T6) (OC)	(single comments - see details at right)	Move cemeteries to special institutions- they are not parks and open spaces, ground floor commercial should not be required, the city should consider consolidating zones, Having this many zones is absurd and makes any of these maps needlessly difficult to comprehend, Ground floor commercial is increasingly sitting vacant, It's a mistake to require this as it may make development of desperately-needed housing prohibitive, make more people-center places to lower costs on infrastructure, allow more businesses and amenities, keep people in the city, and makes things more affordable for everyone
Transportation			
Oppose Sauk Creek Greenway pedestrian/bike path	(EF) (OH20) (T6)	34	Oppose putting any bike trail in greenway, concerns about the large trees, degrade the resource, bike path will serve few bikers, greenway could be opportunity for nature education, fits well with Nature Everywhere program, unnecessary east-west bike path, will add more impermeable surfaces, grade is very steep there and it would require more land for switchbacks as well as a bridge, Connection to Walnut Grove Park is not needed, This should NOT be wheelchair accessible, Too many switchbacks and a bridge over the creek may need to be 15 feet above creek bed to survive a 100 year rain, Too costly! It's a drainage area, not a park, No asphalt, No lights, all bought or built our homes because of Sauk Creek Woods, Change wording back regarding allowing engineer to decide what's feasible, reviewed UW road safety records and drive on High Point Road- there is not justified fear for bikers to bike on road, parks paths are not plowed in winter and negates connectivity-don't have money to maintain or replace the paths, will help combat climate change, helps with mental health, any path should be natural and unpaved, do not think path is feasible, Paule is only ,25 miles from Tree Lane, Tree Lane and Farmington are east-west routes, change to trail that is 5ft wide, will feel like they're walking through someone's backyard-which is not a comfortable feeling, could make more bike friendly but paved trail is not the answer, no lights, opinions of bikers in the neighborhood who don't think the path is needed should have greater weight, no maintenance road
Oppose changes made to Sauk Creek Greenway recommendation (Staff note: Sauk Creek Greenway north-south shared-use path was removed from Plan)	(EF) (OH20) (T6) (OC)	157	Disappointed by the removal of the N-S path in Sauk Creek Greenway and would prefer it be added back in, depend on bike paths to move around city and do not want to use roads and would like to reinstate the Sauk Creek bike path, residents' concerns are due to misinformation, bike path would have improved connectivity for bike access to further locations on the west side, the on-street bike routes are insufficient safe to ride on, especially with kids, kids in Tree Ln, area would have been able to hop on their bikes and safely ride to Alicia Ashman Library, some concern about losing parking near the Parks if bike improvements done on parallel streets, North-south route parallel to Sauk Creek Greenway very much needed, Also like that east-west crossing of greenway still there, need real all ages and abilities option for north-south route, city staff needs to explain not all the trees in the Sauk Creek Greenway are quality trees need to thin out junk trees (Buck thorn, Box elder, and those preventing struggling trees from having the resources), making transportation network more comfortable for people to walk and bike will make a much bigger environmental impact than small number of trees that will be removed to make those connections, needs to be developed to bike, walk, hike, would open the wonderful urban forest to more people, seems like a waste to cut the shortest path possible through this resource, great juncture for students to access school, third spaces, and other community resource, prioritizing building out infrastructure for lower-carbon travel options over vague priorities like "natural feel" of an are, catering to the upset adjacent property owners who view this land as an extension of their properties, exhausting to live in a city where the benefit of everyone is constantly hamstrung by the greed of the very privileged, would become a beloved amenity for the whole neighborhood, not just the a nice space for a few to look out over, saving trees is a goal but not at the expense of pedestrian/cyclist
Approve of dropping north-south Sauk Creek Greenway connection	(OH20) (T6) (OC)	8	City is talking with us, streets provide for a safe and pleasant bike experience that will connect easily to the other bike routes
Approve of east-west Sauk Creek Greenway connection	(OH20) (T6) (OC)	9	East-west path in the Sauk Greenway is an important and needed improvement, desperately need an east-west connection for pedestrians and cyclists between Tree Lane and Old Sauk Road, kids need a safe way to get to Memorial and Jefferson, these are good options and should stay in the plan, cost estimate, helpful for those who otherwise have to go the long way around, will be very important for east-west cyclists using planned future beltline crossing
Oppose to Middleton Street Connection	(EF) (OH20) (T6) (MSV)	219* (203 on signed Opposition to the Proposed Middleton Street Connection Petition) *Received WVHA Board Opposition via email from Board President	Remove from consideration any proposal to remove Middleton Street barricade at Stricker's Pond and extend the street to connect with the City of Middleton, preserve the character of the neighborhood by stopping the connection, Wisconsin Supreme Court Decision (1991), connection could increase traffic counts from 125 vehicles daily to as many as 3500-4000, Middleton Street would become a direct shortcut into downtown Middleton, traffic increase would impact safety for residents, the sensitive wetland habitat (turtles and other wildlife) of Sticker's Pond, discourage active transportation, may increase vehicle conflicts, property damage, and serious injuries or fatalities due to steep 6% grade traveling West from Gammon Road, road is a local road-not collector, current configuration works for pedestrians and bikers, promote the walking paths around both Stricker's and Tiedeman Ponds, should identify potential locations for adding modal filters, more likely to speed on residential streets like this one in neighborhoods that aren't theirs, Madison is so eager to be upwardly mobile that they are at risk for trading all the features that make it special, Perhaps a better strategy would be to widen Gammon Road or to add bus service between Middleton and West Madison, Buses that serve the Middleton Gammon Road area turn on Old Sauk Rd and there is no connection that goes to the West Town mall or southwest Madison businesses
More protected bike lanes	(OH20) (T6) (OC)	10	Want barriers or curb or separated instead of paint, Paint is not infrastructure, would like to see more physical barrier to protect our cyclists, on street bike routes should be protected to allow for more people of all ages and abilities to use them
Approve Owen Conservancy/Crestwood neighborhood pedestrian/bike connection	(OC)	2	Great addition to allow for more north south shared-use paths
Oppose Owen Conservancy/Crestwood neighborhood pedestrian/bike connection	(OC)	2	Concerns the connection will disrupt wildlife
Approve of road diets	(OC)	5	Would like to see reductions in car travel lanes on Gammon Road and Mineral Point road, would improve pedestrian safety and reduce speeds of car travel
Oppose road diets	(OH20) (OC) (T6)	1	Road diets for the purpose of a bike lane, Take bicycles off major roads and on to side streets, oppose any road diet on MPR Will be very convenient for people to come to the park

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Oppose Rennebohm planned streets	(OC) (T6)	5	Concerned if changes to a road, it'll change to concrete sidewalks and not be blacktop walking paths, oppose the creation of any "green streets" south of Sheboygan Ave, due to increase traffic, safety issues and proximity to private homes on Buffalo Trail, most effective way to prevent through traffic is to not build a through street, effectively lower the amount of buildable housing here because of this planned street, properties would be better able to connect to the park without a (proposed) street in the way
Rennebohm shared use path	(OC)	3	Converting the sidewalk into a shared path is a good start, but should use Regent to use a bike lane to prevent pedestrians from needing to cross in front of bikers, oppose removal of any trees in Rennebohm Park for expansion of bike path, adding a shared-use path would be great
Support Segoe Road pedestrian/bike improvements	(OH20) (OC)	4	Support for protected bike lanes and ped improvements on Segoe south of Regent, like changes made at intersection -hugely improve pedestrian (especially kids) access to the park from both sides of Segoe Road, new crosswalks should be raised to force cars to slow down
Support Gammon Road pedestrian/bike path Approve of bike facilities on High Point & Westfield (N-S greenway alternatives)	(OH20) (OC) (OC)	5	Shared use path should extend further south to the mall Should be protected lanes, would prefer greenway path, but protected on-street facility is good too, excited to see car infrastructure being replaced with bike and pedestrian infrastructure
Oppose on-street bicycle facilities	(OC)	2	Plenty of existing options for bikes along High Point & Westfield, don't reduce parking on High Point Rd
Oppose Old Sauk Road changes	(EF) (OC)	2	Old Sauk is 2 land road with lots of existing traffic, hard to make left turns, should not resemble East Washington corridor, car racing concerns through Parkwood on Blue Ridge and Everglade to get to Mineral Point Road or the beltline, Oppose any and all options to evaluate or reconstruct North side of Old Sauk Rd
Appalachian Way	(OH20) (T6) (OC)	5	22d, "extend Appalachian Way to the east, then south to connect Old Sauk Rd," But Sauk Ridge Trl to Appalachian Way is still in the map, no need to extend Appalachian Way, ability to see on-coming westbound traffic is limited at the western entrance and appears to take out the bike path, the eastern entrance will require entering on a steep slope which will be problematic during the winter months - intersection at hill may have vision issues, oppose connecting any and all streets as discussed in 22d,, Appalachian Way, Old Sauk Rd, and Cooper Lane Bike Path
Yosemite connection	(OH20) (OC)	7	Concerned will have to pay towards re-connecting it, original design of through street was steeply sloped and people were driving so fast downhill they caught air - reconnecting will recreate this safety hazard, disregard previous safety concerns of when Yosemite Dr, ran from Yellowstone to Old Sauk Road and reconnect the roadway to its former condition and locations, Connecting Yosemite Place to Yosemite Trail makes no sense- will only increase traffic cutting through the neighborhood-should look at improving efficiency of existing major throughfares and ways to divert traffic through residential neighborhoods, add modal filter
Approve of Marshall Park connection	(T6) (OC)	3	Would be great to allow more pedestrian/bike access to the park and the businesses along Allen
Approve of Bus Rapid Transit (BRT)	(OC) (T6)	2	BRT is great investment and great use of federal grants
Want changes to bus system	(T6)	2	Bus service between Old Sauk Road and Tree Lane on Gammon Road is missing, BRT should have its own lane for the entire route
Approve of bike paths and bike infrastructure shown in plan	(OH20) (T6) (OC)	31	Like multiuse paths, like east/west bike and pedestrian connections, like bike path on Camelot, yes to protected bike lane on Midvale from University to beltline-but Midvale needs resurfacing to be safe for bikers, like path connecting by Odana Hills Golf Course, like on-street facility on Mineral Point Road and Westfield Road, like shared use path shown on east side of Gammon Road, shared-use path along University Ave, Wexford Pond Greenway, shared use path along Regent street from 5' to 10' because of Regent and Eau Claire being wide with long, continuous medians, that limits peds/cyclists' access to the park, do not need free on-street parking at the expense of safe biking facilities, Bike path map should be corroborated with Google Maps bike routes and Strava bike routes that show what routes people actually are traveling, great change to better utilize public land, wayfinding, approve of east-west route through Gillespie MS & Memorial HS, approve of improvements to MPR & High Point intersection, support protected lane on Regent, improving the
Approve of transportation improvements	(OH20) (T6) (OC)	16	Children's safety going to school, help reduce traffic and allow children to be more independent, pedestrian underpass beneath the railroad between Old Middleton Rd,, needed on WB Odana, there's a very short stretch of two lanes before merging down to one, and bikes are in mixed traffic while cars are negotiating that, "improvements" can otherwise sound like "nice to have's", not "life-saving interventions", sidewalks surround kids safety, Tomahawk/University improvements, bump out improvements are welcomed great for safety, and should be used more throughout the city, removing Laub helps reduce the number of conflict points in the area without limiting mobility
Would like to bike path improvements	(OH20) (T6) (OC)	6	Make Keating a bike boulevard up to Owen, Place a bike/ped activated stop at Midvale, This route allows access to the SW path or less traveled streets in Sunset neighborhood and beyond, Mineral Point Rd, from Midvale is too steep for many bicyclists, Keating is a much more gradual grade, Bike lanes on Mineral Point Road from Segoe to Midvale will never be bike friendly, Utilize South Hill Drive rather than Mineral Point Road, And in the future, Keating Terrace rather than Mineral Point Road, bike lanes on Tokay as proposed need to continue to the SW bike path, Utilize Keating Terrace, need north/south bike and pedestrian connectivity, opposed to adding an unprotected bike path to Midvale Blvd-dangerous without physical barrier-more cost effective to make improvements to side streets than using Midvale, barrier or signage on Tokay Blvd for safe biking, planned bike lanes on Mineral Point between Segoe and Midvale will make things more hazardous for bicyclists-use Keating Terrace instead, extend a bike lane on Tokay to connect with SW bike path
Would like to see more safety improvements	(OH20) (T6) (OC)	17	With the numerous projects and re-designs of streets and crossing for cyclist and pedestrian safety, the city should implement features like curb bump outs, raised crossing, daylighting, etc., for any categories of crossings or street, Safety road calming in front of the schools on Segoe, adjust proposed road on the north side of Rennebohm Park so it does not connect to Segoe Rd- that connection will turn the proposed road into a bypass for Sheboygan Ave, It will be a danger to kids using the park and those who are tending the community gardens, Crossing of Beltline between Old Sauk and Mineral Point need for ped/bike, safer pedestrian crossings especially near schools, need more safe crossings of Gammon Road near West Towne and Memorial High School, add bump outs to protect the children, Colony Drive pedestrian crossing at Gammon Road for Muir Elementary and other improved crossing to reduce need for school bussing, intersection of Tree Lane and Mineral Point Road needs no left turns and no U-turns on Mineral Point Road, intersection of Tokay Blvd and Segoe Road, make the crosswalk on Odana Rd and Charles Ln more visible (blinking light as additions have helped but are not enough), Speed enforcement is not a long term solution-Roads need to be engineered for slower speeds, redesigning the roads to slow cars down, modal filter on Tree Lane to make a not through street and reduce traffic, modal filter on Cable Ave/Beach St, modal filter on planned street from Enterprise Lane, modal filer on Berwyn Drive; Whitney Way-South Hill Dr ped improvements; Tree Lane/MPR/Beltline needs bicycle safety improvements, along University Avenue from Allen Blvd to Heim Ave is difficult for bikers on the shared-use path as drivers turning right do not see bikers, any plans to disallow turning right on red on some intersections near schools - Turning right on red can be very dangerous for pedestrians because drivers' attention is primarily on oncoming traffic to the left, not on any pedestrians who might be crossing to the right

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Want less emphasis on bikes	(OH20) (T6) (OC)	7	Can never be Portland, Make bikers pay their share for all the lanes, special plows, extra work, etc., There isn't even a license fee for them, while the rest of us pay for it, pedestrians are on top of the pyramid in Comprehensive Plan, bike recreation are overtaking common sense, Minimize concrete and asphalt, biking on sidewalks is permissible, failsafe, parallel streets to avoid high volume streets, overkill, pedestrians and disable people first; spending money on making bicycling easier is inequitable
Less Parking	(OC)	5	Reducing on-street parking is a high priority, makes walking and cycling much safer by increasing visibility of pedestrians and reducing the chance that biker will get slammed by an opening door, removing parking mandates can free up a large amount of room to plant trees for canopies, don't need additional parking by Rennebohm
More Parking	(T6)	2	Do not remove parking from residential streets, more parking planned in BRT riders
High Injury Network (HIN)	(OC)	4	Flagged Felton Pl and Falles Ct with 0 injuries, Midvale/Mineral Point Rd intersection is not flagged and has many injuries, Any high injury network should be scoped out to slow down speeds; "HIN map is incorrect"
Beltline crossings	(OC)	4	Pedestrian and cyclist safety is top concern, extending shared use path would be great to allow for crossing the area safely, would be a huge improvement to extend the wide sidewalk/shared-use path under the highway on Gammon Road, Beltline should not cut off people from the city if they aren't in a car, underpass improvements at Struck St, will improve cyclist access
Walkers do not feel safe on multipurpose paths	(OH20)	2	We need safe quiet trails for pedestrians, many walkers are NOT safe on the so-called multipurpose bike trails
Miscellaneous	(OH20) (OC) (T6) (EF)	(single comments - see details at right)	City is intentionally making it difficult for people to drive, minimalist approach that helps people traverse the area rather than creating a major thoroughfare, increasing light along shared use paths by schools will disrupt private homes and wildlife, adding alternate student drop-off/pick-up area on Cable Ave does not work well due to the bike/ped connection between the end of Regent St and start of the path on Inner Dr,-will bring more traffic on this street; Do not replace RR with multiuse path if there is ANY chance it could be used for light rail or Chicago connection, more lightening is great for visibility along the path, giving children the ability to travel independently is one of the best things we can do to build a connected community, we should allow the parking lots to be developed into more useful buildings, Mineral Point Rd between Segoe and Midvale does not meet the criteria for a community connector because the volume of vehicles, city should study alternative uses for the rail corridor-Like revitalizing it to be a Metro light rail corridor for the city since Madison will need rail if it continues to grow and densify like it needs to, oppose limiting affordable housing development near Beltline- SUPPORT for affordable housing projects that include noise mitigation along the Beltline, like walking trails and being able to ride bike, approve of street and roadway improvements like Support Berwyn Dr/Laub Ln realignment, Support using parking lots as park and ride, Like planned streets in West Towne and Odana Road areas, oppose making St, Dunstan's a two way street due to more traffic through residential road, The BRT overlay in Hill Farms as it bisects the neighborhood and will (not if but when) create strife among neighbors especially when someone living on the edge of the BRT overlay is faced with a multi-unit or multi-story complex next to their single-family home, Remove northbound Whitney to Odana free-flow right turn, lack of turn lanes outside the immediate intersection, Like shown traffi
NOTE: sidewalk comments summarized in Health & Safety section	(OTIZO) (OC) (TO) (ET)		
Neighborhoods and Housing			
Want more missing middle housing	(VLZH28) (OH20) (T6) (OC)	8	Not sure how plan encourage missing middle housing, homes, condos, duplex, townhouse, apt, Whatever people can own to build their generational wealth, build small footprint community, City needs to work on a plan to develop housing options like condos, small residences, etc., that allow people to build equity, downsize, feel ownership in the community, The "missing middle" is still missing despite all the city's high density, rental focus, frustrated that City says it can't be done or is difficult, more missing middle in area well connected by shared use paths, need to update GFLU to be LMR or NMU at the minimum,
Affordable housing	(OH20) (OC)	4	Affordable middle housing is needed and fits into our neighborhoods nicely, Affordable home ownership houses, Small houses on small lots-Veridian will build these, Housing needs to help lower income families actually own a piece of property, Small row houses help, Small affordable residential units, attached are fine, hope TIF funds help allow for reduced median rent development
Support Lussier recommendations	(OC)	2	Community hub, essential resource for many, encourage any expansion of programing
Want livable neighborhoods for families	(OH20)	2	Consideration for retaining and maintaining the quality of our neighborhoods
Reduce barriers to development	(OC)	2	The city should consider implementing standard pre-reviewed building plans to make building easier, The city should work to make it easier for up zoning to occur to allow for more housing throughout the city to address the housing crisis, Consider automatic/dynamic zoning, Work with existing property owners to redevelop underutilized parking lots into mixed use buildings
School comments [staff note: the City does not control school attendance areas or school expansions]	(OC)	1	Midvale and Lincoln should have identical attendance areas as they are paired schools
Miscellaneous	(OC) (LZH3) (OH20)	(single comments - see details at right)	Would like to see additional senior housing in the area - for folks to stay in the neighborhood, free up some sf homes, Like encouraging developers to add a wider selection of housing for various incomes and family sizes, and adding noise mitigation to development along the Beltline, See land use changes as promoting apartments but would like to see more plans for owner-occupied housing, Want more play areas for children near and in all neighborhoods, Oppose affordable house since affordable housing just makes developers rich, More housing options like condos and differing apartment styles, Great steps to making our community more diverse and improving the economic opportunities for historically marginalized and under represented groups, especially by making use of underutilized commercial/parking areas, I would like to see the language strengthened around these items, These items are really important and only succeed if a real effort is made, "considering" land banking does not mean much, like language surrounding middle housing but plan does not facilitate this land use, Daycares should be exempt from parking requirements to make retrofits of existing buildings more feasible, support increased child care opportunities
Economy and Opportunity			
TID	(OC)	1	TID should include single-family areas where we expect/encourage redevelopment to occur
Utilities	(OC)	1	Support studying capacities to permit new development
Culture and Character			

7/10/2024

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Urban Design District 6	(EF) (OH20) (OC)	4	Concerns that this will lead to single-family housing converting to multifamily housing more than what is allowed by TOD, boundaries should not extend to part of University Ave,, but just properties along University Ave, UDD plan to cram several hundred more apartments, etc., in the Rennebohm area, like mixed use zoning and greenspace in considerations, deemphasizing parking will make it safer for pedestrians and cyclists, more tree canopy will make trail less hot, trail is noisy, exhaust fumes from University Ave, slow speed on University Ave; support alignment of UDD 6 w/TOD requirements, approve of considerations, approve of less car-oriented areas
Lack of defining views	(OH20) (T6)	3	Most are already gone!, lost the views, don't want to lose more vistas-reduce building heights, Madison Yards blocks views of Capitol
Large public spaces	(OH20) (LCD)	3	Large spaces for public meetings with good acoustics; need "town square" area for performances & exhibitions (like Middleton's Stone Horse Green), more places to sing in groups, inclusive public swimming pools
Historical Resources	(OC)	2	The Grayson Building at 4414 Regent Street has architectural significance, is relatively unaltered since it was constructed, and warrants protection from demolition and alteration, The Old National Bank (formerly Anchor Savings and Loan) at the corner of Midvale Boulevard and Vernon, has architectural significance, is in excellent condition, and should be protected from demolition or alteration,
Approve of Design Guidelines	(OC)	2	Engagement while creating walking paths is often overlooked-glad the plan addresses this, buildings are meant to serve people, not cars, People should not be forced to walk through parked cars to arrive at their destination, outdoor dining option are great and make an area feel vibrant
Approve of culture character actions	(OC)	2	Approve of promotion of community areas and having a sense of place, community events are a great idea,
Miscellaneous	(EF) (OH20) (OC)	(single comments - see details at right)	Public art is great for giving the area a unique feeling, like Plan recommendations to add to West area art venues and infrastructure, Maintain historic designation of Hill Farms since BRT overlay cuts neighborhood, a place of worship is on the map and is labeled as a "community gathering place" when it's decidedly hostile to LGBTQ+ members of the community, Add to Design Guidelines: driveway entrances to a property should not be directly connected to the main road/highway in the vicinity to avoid vehicle/pedestrian conflicts (University Ave, Gammon, Mineral Point, etc)
Green and Resilient			
Approve of greenspace shown in Plan	(OH20)	2	City staff's expertise and understanding of how the way we interact now with the land and water called "Madison" will impact how the children and all future generations will be able to live here
Approve of parks shown in Plan	(OH20) (T6) (OC)	6	Plan for more parks south of Mineral Point Rd, future West Towne/Odana Road parks; include smaller parks as well; Focus on new parks in West Towne Area over Odana area
More trees and greenspace	(OH20) (T6)	3	Like tree-lined streets, need more trees and open space, less concrete
Drainage improvements	(OH20)	2	Drainage improvement is necessary if done with minimal impact to the vegetation, needs to be improved with minimal damage to canopy
Protect trees	(OC)	3	Trees (should be protected) along the east side of Whitney Way across from Garver Park (NE corner of Mineral Pt and Whitney Way) contain a long standing population of cicadas (should be documented) that sing every summer; no trees should ever be cut down
Rennebohm Park	(OH20) (OC)	5	Rennebohm Park is underutilized, Add more amenities to the further reaches of the park, Could be as simple as more picnic tables scattered around the edges, Bicycle education/traffic enforcement for safety purposes, will be good if Rennebohm Park becomes larger, which can include greenway on the right side of the park, blacktop walking paths need better lightening -don't change to concrete; support path connection to Sheboygan, expanding the park would be great
Stormwater concerns	(OC) (T6)	2	Concern about new developments adding to rainfall runoff issues, New developments should take in consideration any runoff issues and no new development should be built unless and until developer and city identity and plan for any issues, like recommendations but more can be done-more places where streets can be narrowed and parking removed so better stormwater management and rain gardens can be put in while also improving pedestrian safety, want minimalist approach to stormwater management
Golf Course uses	(OC)	2	Support replacing clubhouse w/ more community-serving facility, there are potential ways large properties can be used
Approve of greater access to parks/lakes	(OC)	3	Access to Zook Park from SW commuter trail
Miscellaneous	(OC) (T6) (OH20)	(single comments - see details at right)	Recreational biking is a great idea, greenspace north of Vernon Boulevard could be used as stormwater management, connect greenways with a mixed use path, Need to address erosion, Approve of greater access to Zook Park from SW commuter trail, Approve of stormwater improvements, support additional greenspace shown for Vernon Blvd, Build trails through woods along Whitney way/Garner Park, Greater attention to the plight of the non-human animals and the vegetation that they depend on to survive, Study water and sewers in West Towne area and north, artesian wells
Effective Government			
Loans programs to assist needy	(OH20)	2	Madison needs to: have a City loan plan for those who need, provide an avenue for those with reduced incomes to actually own a small house (even an attached house counts), Home ownership builds a family and people-centered community,
Miscellaneous	(WRM) (EF) (OH20)	(single comments - see details at right)	Forum for connecting with smaller developers to see what they would need to support 'by-right' development and removing barriers like requiring variances or special permits, distrust of City, Better signage, West area plan needs to include is a more specific approach to planning that clearly examines the impact of any new housing development on the school systems and the existing infrastructure, schools to not have capacity to absorb new students, lack of an infrastructure including taxes from developers is not sustainable and costs are being passed to single family home owners, Where are the funds coming from? The City is already in debt- no one has an answer
Health and Safety			
Support adding sidewalks to Old Sauk	(OC)	2	Add sidewalks on both sides of Old Sauk Rd along entire length
Approve of sidewalks shown	(OC)	5	Locations: Along Old Sauk - really impactful, especially kids who walk to school; support MPR-Highpoint/Gammon/Grand Canyon improvements
Oppose adding any sidewalks	(OC)	4	Especially on cul-de-sacs and dead ends, but generally anywhere
Approve of safety recommendations	(OC)	2	Pedestrian safety and comfort is a great way to help communities connect
Approve of partnerships to have health and recreation programs	(OC)	2	Nature Everywhere, MSCR, Police Foundation
Additional sidewalks	(OC)	2	More direct connection from Gammon/Beltline intersection down to West Towne parking lot, 4800 block of Sherwood and the 5100 block of Holiday have sidewalks missing on both sides

7/10/2024

COMMENT	SOURCE	MULTIPLER	AREA/DETAILS
Miscellaneous	(OC)		Adding more police to an area rarely makes an area safer, The city should instead be investing in more and higher quality social services and housing to build community resiliency, rather than the most expensive and least effective way of solving problems, armed agents of the state, oppose adding sidewalks to Old Sauk due to lack of room, steep hill with limited visibility, high traffic counts, topography, and possible cost of retaining walls
Planning Process/Other			
Like meetings for citizens	(OH20)	7	Like the charts, maps, and especially the professionals, like open-house format, chance for face to face discussion with City staff, Very well designed and organized meeting! Access to staff and their terrific knowledge is wonderful, Staff are so articulate and uniformly polite and patient, Plenty of storyboards and people who are knowledgeable and can explain them,
Do not like meeting format	(LZH3) (T6)	2	Do not like writing questions, no dialogue, would like an open mic,
More information on planning process	(OH20)	2	More on the process of staff recommendations > City commissions > City Council and how a draft recommendation can be removed at the staff level based on community input, want storyboard on process of changing a staff recommendation,
Miscellaneous	(OH20) (OC) (T6)		Like to see plans evolve over time, Do not support plan, Need to listen to the community- Community is against a paved path and the resulting destruction of trees, Definitions of acronyms on the charts make charts hard to understand, Designs in business are always with cost in mind, There seems to be no thought to that in these design processes, With assessments going up, some owners are afraid they will lose their homes, do not like online map as it is hard to post comments and use

Phase 4 Engagement

(OH20) In-Person Open House - All topics May 20

(VLZH28) Virtual Land Use/Zoning/Housing Meeting May 28 (VT30) Virtual Transportation Meeting May 30

(LZH3) In-Person Land Use/Zoning/Housing Meeting June 3

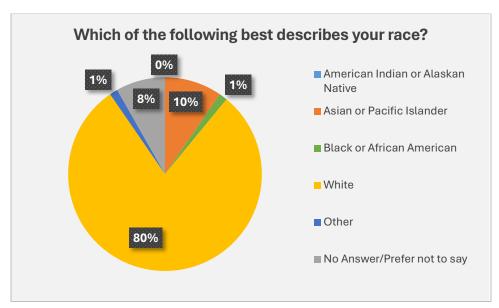
(LCD) Lussier Community Dinner June 7 (WRM) Whitney-Regent Meeting June 10 (MSV) Middleton Street Site Visit June 20

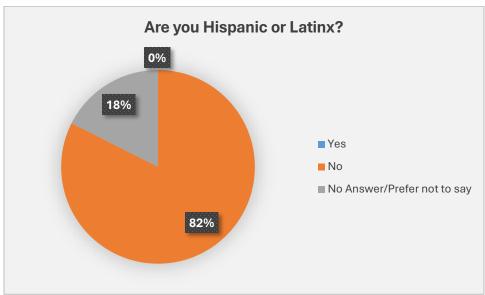
(EF) Emailed Feedback

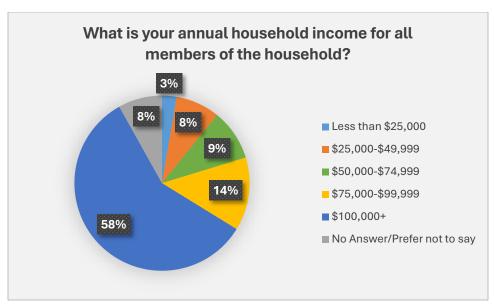
(OC) Online draft Map and Action Comments

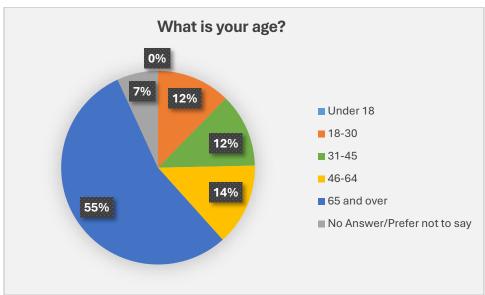
Demographic Summary

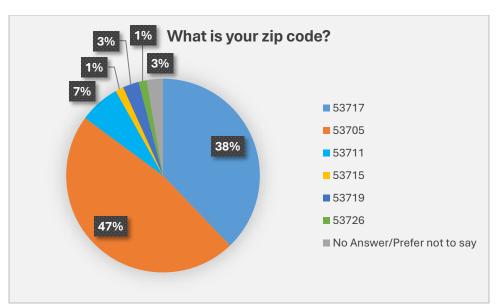
Demographic questions were asked of community survey respondents. Those are summarized here and are generally consistent with the virtual and in-person meetings of May and June.













Department of Planning & Community & Economic Development



Planning Division

Bill Fruhling, Interim Director

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To: Mayor, Common Council and (BCCs)

From: Ben Zellers and Linda Horvath, West Area Plan Co-Managers

Re: Adopting the West Area Plan

Date: July 10, 2024

Introduction

Attached is the Final Draft West Area Plan submitted for review and approval. Along with the Northeast Area Plan, these are the first plans prepared under the City's Planning Framework, which established 12 permanent plan geographies covering the entire city and peripheral growth areas. While the Planning Division led plan development, its creation was an interagency-effort with collaboration and contributions from a number of other City agencies including Parks, Engineering, Community Development, Economic Development, Transportation, Traffic Engineering, Streets, Forestry, Water Utility, Public Health, Fire, and Police.

Public Participation

There were many opportunities for the public to participate in the planning process. This included an initial phase exploring the public's thoughts about issues and opportunities, to the more recent phase which focused on final public meetings to review the draft plan. The recent phase was extended to respond to a large volume of public comments and included an additional five public meetings. Throughout the planning process, staff used multiple engagement strategies to reach a broad range of participants. These included traditional public meetings (both in-person and virtual), multi-agency open houses, outdoor plan events, plan pop-ups at community events, focus groups, an arts-focused public meeting, and interactive digital engagement. The Planning Division also contracted with Community Partners to help reach and get feedback from under-represented populations to ensure their voices are reflected in this plan and conducted a business survey in coordination with the Office of Business Resources. Public feedback summaries from throughout the process are available on the project website.

In addition to public engagement, the West Area Plan team met with Boards, Committees, and Commissions likely to see plan referral during the approval process. These mid-process check-ins ensured interim feedback could be considered and incorporated during the planning process instead of the approval process. For links to various meetings and materials please see the <u>project website</u>.

Changes to Draft Plan Based on Public Feedback

Staff made edits to plan drafts in response to public feedback. This included changing future land use back to Low Residential from Low Medium Residential along Merrill Crest near the corner of Whitney Way and Regent Street and removing proactive rezoning there, removing proactive rezoning from the Highlands Neighborhood, changing future land use back to Low Residential from Medium Residential for the Hill Farms Pool site, changing transportation access on Segoe Road by removing parts of Laub Lane and focusing access at Berwyn Drive and adding pedestrian crossing improvements and removing the north-south shared use path from the Sauk Creek Greenway.

City Board, Committee, and Commission Review

The anticipated review and approval schedule is as follows. Any public feedback received during this phase will be forwarded to the BCCs reviewing this plan.

July 16 – Common Council introduction

July 17 - Economic Development Committee (5:00 pm)

July 31 - Transportation Commission (5:00 pm)

August 5 - Landmarks Commission (5:00 pm)

August 14 - Urban Design Commission (4:30 pm); Board of Park Commissioners (6:30 pm)

August 26 - Plan Commission (lead; 5:30 pm)

September 10 - Common Council (6:30 pm)

The West Area Plan is accompanied by both an adopting resolution and an ordinance amending the Comprehensive Plan. The resolution includes archiving previously adopted underlying plans. The Comprehensive Plan amendment (ordinance) is limited to key maps, such as the Generalized Future Land Use and Bicycle Facilities map, among others.

After plan adoption, staff will incorporate any changes made during the approval process. Minor formatting changes, such as moving/resizing graphics or adding supplemental images, may also be made to improve the usability of the document.



City of Madison

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Master

File Number: 84378

File ID: 84378 File Type: Resolution Status: Council New

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 07/10/2024

File Name: Final Action:

Title: BY TITLE ONLY: Mid Year Appropriation Resolution

Notes: Introduced BY TITLE ONLY on 7/16/24

Sponsors: Satya V. Rhodes-Conway Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: mmcclain@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:
1	Finance Department	07/10/2024	Referred for Introduction				

Action Text: This Resolution was Referred for Introduction

Notes: Finance Committee (7/22/24), Common Council (8/6/24)

Text of Legislative File 84378

Fiscal Note

Fiscal note pending.

Title

BY TITLE ONLY: Mid Year Appropriation Resolution



File ID: 84379

City of Madison

City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84379

File Type: Resolution Status: Council New

Business

Version: 1 Reference: Controlling Body: COMMON

COUNCIL

File Created Date: 07/10/2024

Enactment Number:

File Name: Adopting the Northeast Area Plan Final Action:

Title: Adopting the Northeast Area Plan, adopting an amendment to the

Comprehensive Plan and archiving underlying plans.

Notes:

Sponsors: Derek Field And Sabrina V. Madison Effective Date:

Attachments: NEAP Adopting Cover Letter, Northeast Area Plan

Introduction Draft, Ordinance 84382-Adopt Northeast

Area Plan

Author: Angela Puerta and Dan McAuliffe, Planning Division Hearing Date:

Entered by: apuerta@cityofmadison.com Published Date:

History of Legislative File

Ver- sion:	Acting Body:	Date:	Action:	Sent To:	Due Date:	Return Date:	Result:	
1	Department of P and Community Economic Devel	and	4 Referred for Introduction					
	Action Text:	This Resolution was Referred for Introduction						
	Notes:	Plan Commission (8/26/24), Economic Development Committee (7/17/24), Transportation Commission (7/31/24), Community Development Block Grant Committee (8/1/24), Landmarks Commission (8/5/24), Board of Parks Commissioners (8/14/24), Urban Design Commission (8/14/24), Common Council (9/10/24)						

Text of Legislative File 84379

Fiscal Note

Fiscal note pending.

Title

Adopting the Northeast Area Plan, adopting an amendment to the Comprehensive Plan and archiving underlying plans.

Body

WHEREAS, on August 2, 2022 the City of Madison Common Council authorized the Planning Division's shift to a planning framework with twelve (12) Area Plans that cover the entire city and are updated every ten (10) years (RES-22-00577); and

WHEREAS, the City of Madison Common Council authorized the Planning Division to prepare

the Northeast Area Plan on January 17, 2023 (RES-23-00068); and

WHEREAS, the City of Madison Common Council adopted a public participation plan for the Northeast Area Plan on September 19, 2023 (RES-23-00589), as required by Wis. Stat. § 66.1001(4) to amend the Comprehensive Plan, which described how the City intended to make the process accessible to all; and

WHEREAS, the public participation process included more traditional engagement through a project website, interactive mapping, virtual meetings, in-person meetings, emailed project updates to interested parties, distribution of informational materials, focus groups, neighborhood association visits, social media posts, traditional media; and

WHEREAS, the public participation process also included contracting with several Community Partners and coordinating with the City's Neighborhood Navigators to engage traditionally underrepresented residents through focus groups and participation in community events and activities; and

WHEREAS, Plan progress updates were provided to several City boards, committees, and commission during the process; and

WHEREAS, the Community Development Block Grant (CDBG) Committee designated the Hawthorne-Truax area (portions of Census Tracts 25.0, 26.01 and 26.02), Sandburg area (portion of Census Tract 26.02), and Burke Heights area (portion of Census Tracts 26.01 and 26.03) as CDBG eligible neighborhoods, with a higher proportion of low- and moderate- income residents, for the purposes of providing planning assistance and to facilitate deeper public engagement and guide future neighborhood investments; and

WHEREAS, the Wisconsin Department of Transportation (WisDOT) is studying proposed changes to the Highway 51 and Interstate 39/90/94 corridors which includes the Northeast Area, and the Northeast Area Plan actions related to these WisDOT studies reflect the community's desire to connect neighborhoods safely and comfortably and not be solely focused on moving vehicles; and

WHEREAS, consistent with RES-22-00577, archiving of adopted plans within the Northeast Area Plan boundary was considered as part of the development of the Northeast Area Plan, and the adopted plans were reviewed to identify the recommendations from each adopted plan that should be incorporated into the Northeast Area Plan before being archived; and

WHEREAS, the planning framework states that the Generalized Future Land Use Map developed through each area plan process will directly update the Comprehensive Plan's Generalized Future Land Use Map; and

WHEREAS, the Northeast Area Plan's actions are based on: citywide policies including the Comprehensive Plan, recommendations in underlying plans, and feedback received from the community, and

WHEREAS, Plan actions focus on guiding changes to the physical aspects of the Northeast Area that occur incrementally over decades, such as development initiated by the private sector and infrastructure improvements typically carried out by the public sector; and

WHEREAS, the Community Action Strategy includes actions for physical improvements and capacity building in the Hawthorne-Truax, Sandburg, and Burke Heights areas; and

WHEREAS, the City of Madison has held at least one public hearing on the Comprehensive Plan amendment in compliance with the requirements of Section 66.1001(4)(d) of the Wisconsin Statutes.

NOW, THEREFORE BE IT RESOLVED that the City of Madison Plan Commission recommends adoption of the Northeast Area Plan.

BE IT FURTHER RESOLVED that, the East Towne-Burke Heights Neighborhood Development Plan (initially adopted in 1987), Hanson Road Neighborhood Development Plan (2000) Carpenter-Ridgeway-Hawthorne-Truax Neighborhood Plan (2001), Ridgewood East Central Development Plan (2002), Greater East Towne Area Plan (2021) and Hawthorne-Truax Neighborhood Plan (2023) are archived which means they will no longer be used by the City for project review or planning purposes.

BE IT FURTHER RESOLVED that relevant maps and text in the Comprehensive Plan shall be updated to address the adoption of the Northeast Area Plan and archiving of the above-listed plans.

BE IT FURTHER RESOLVED that, as recorded in its official minutes, the City of Madison Plan Commission recommends the City of Madison Common Council adopt Ordinance File ID 84382, which will constitute its amendment of the City of Madison Comprehensive Plan under Section 66.1001 of the Wisconsin Statutes to update the Generalized Future Land Use Map and Bicycle Facilities map to reflect the Northeast Area Plan.

BE IT FINALLY RESOLVED that staff is directed to implement the actions contained in the Plan, including Community Development Block Grant-funded projects in the Community Action Strategy.

Department of Planning & Community & Economic Development



Planning Division

Bill Fruhling, Interim Director

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To: Mayor, Common Council and (BCCs)

From: Dan McAuliffe and Angela Puerta, Northeast Area Plan project managers

Re: Adopting the Northeast Area Plan

Date: July 8, 2024

Attached is the Northeast Area Plan draft being considered for approval. Along with the West Area Plan, these are the first plans developed under the City's Planning Framework which established 12 plan geographies covering the entire city. While plan development was led by the Planning Division, its creation was an interagency-effort with collaboration and contributions from many City agencies.

Public Engagement

There were many opportunities for the public to participate in the planning process. From initial phases that sought to understand the public's thoughts about issues and opportunities, to the most recent phase which focused on review of draft plan recommendations, multiple engagement strategies were used to reach a broad range of participants. These included traditional public meetings (both in-person and virtual), multi-agency open houses, outdoor plan events, neighborhood walking tours, plan pop-ups at community events, an art charrette and interactive digital engagement. The Planning Division also contracted with Community Navigators to help reach and get feedback from under-represented populations to ensure their voices are reflected in this plan. A business survey was also conducted in collaboration with the City's Office of Business Resources. Summaries of feedback received throughout the process are available at the following links (Feedback through May 2024; Feedback on Public Review Draft).

Public comments and feedback led directly to several Plan recommendations. These include addressing the "fragmented" bike network, new and expanded parks in the Northeast Area, and land use changes to encourage housing development. The planned homeless shelter on Bartillon Drive was often mentioned by participants, and plan recommendations reflects concerns with a lighting audit and the creation of a Neighborhood Resource Team.

In addition to public engagement, the Northeast Area Plan team met with several City Boards, Commissions and Committees (BCCs) which the plan would likely be referred to during the approval process. These mid-process check-ins ensured initial BCC feedback could be considered and incorporated during the planning process instead of the approval process.

Public Review Draft

The Northeast Area Plan Public Review Draft was published on May 20, 2024 with three public engagement events in the following weeks. Those events, as well as an online engagement platform, allowed the public to give feedback and ask questions about the draft document. The Public Review Draft was preceded by the Recommendation Framework, released in December 2023, which outlined an initial draft of the major recommendations of the Plan.

Approval Schedule

The anticipated review and approval schedule is as follows. Any public feedback received during this phase will be forwarded to the BCCs reviewing this plan.

July 16 – Common Council introduction

July 17 – Economic Development Committee

July 31 – Transportation Commission

August 1 – Community Development Block Grant Committee

August 5 - Landmarks Commission

August 14 – Urban Design Commission (4:30 pm); Board of Park Commissioners (6:30 pm)

August 26 – Plan Commission (lead)

September 10 - Common Council

The Northeast Area Plan draft is accompanied by both an adopting resolution and an ordinance amending the Comprehensive Plan. The resolution includes archiving previously adopted underlying plans. The Comprehensive Plan amendment (ordinance) is limited to key maps, such as the Generalized Future Land Use and Bicycle Facilities map, among others.

After plan adoption, staff will incorporate any changes made during the approval process. Minor formatting changes, such as moving/resizing graphics or adding supplemental images, may also be made to improve the usability of the document.

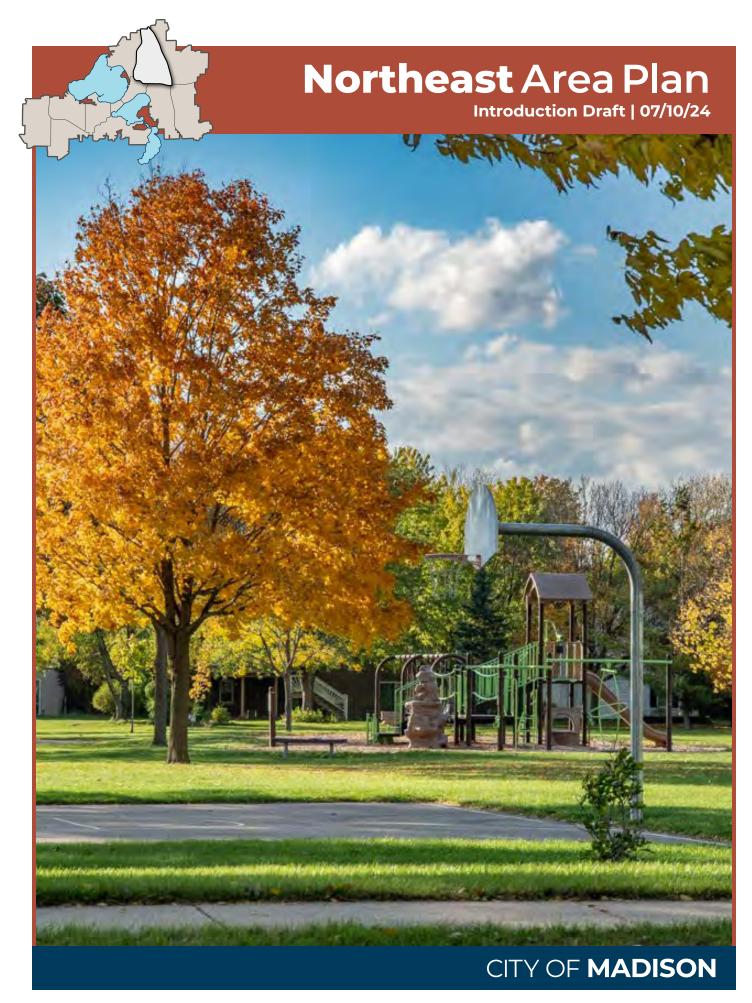


Table of Contents

Acknowledgments
Introduction
Land Use
Transportation
Neighborhoods and Housing41
Economy and Opportunity45
Culture and Character
Green and Resilient58
Effective Government65
Health and Safety
Community Action Strategy74
Appendix

Adopted by the Common Council on (Date)

Resolution RES24XXXLegistar file # XXXXX

Acknowledgments

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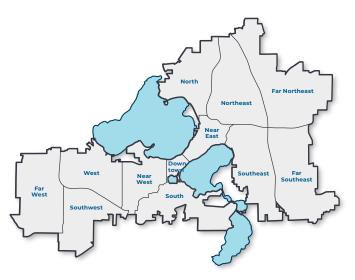
Public Health Madison and Dane County

Justin Svingen Melinda Knuth

Community Navigators

African Center for Community Development Mathias Lemos LLC Harambee Village

Introduction



3

Planning Framework

The Northeast Area Plan is one of the first plans following the City's new Planning Framework. The Planning Framework established 12 plan geographies to be the basis for future area plans. These 12 plans will be established and updated on a ten-year cycle, ensuring all areas of Madison have a current plan. This more equitable approach to creating plans avoids past situations where certain areas received greater attention while others were only addressed in the citywide Comprehensive Plan. The Planning Framework creates greater clarity by having a single area plan rather than overlapping plans written at different times with different recommendations.

Previously adopted underlying plans

Previously adopted underlying plans served critical roles in guiding decisions around new development, infrastructure, and budgeting. Adopted by the Common Council, these guided millions of dollars in investment after extensive public outreach efforts. Previously adopted plans significantly overlapping with the Northeast Area will be archived and will no longer guide City decisions. Going forward, the Northeast Area Plan will guide decisions. Archived plans for the Northeast Area:

- 1. Hawthorne-Truax Neighborhood Plan (2023)
- 2. Greater East Towne Area Plan (2022)
- 3. Hanson Road Neighborhood Development Plan (2000; amendments through 2021)
- 4. Ridgewood East Central Development Plan (2002)
- 5. Carpenter-Hawthorne-Ridgeway-Sycamore-Truax Neighborhood Plan (2001)
- 6. East Towne-Burke Heights Neighborhood Development Plan (1987)

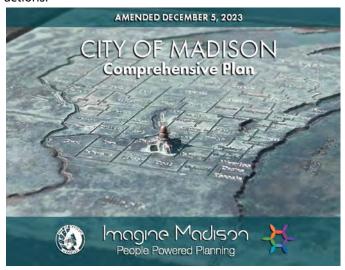
Recommendations in these underlying plans were reviewed and considered as part of this Plan's development. Recommendations were incorporated if they are consistent with the Comprehensive Plan or current City policies, and generally addressed physical or regulatory changes specific to the Northeast Area, including land use, zoning, building height, transportation, parks and open space, public utilities, and stormwater infrastructure. Recommendations already implemented or that repeat existing citywide policy were not incorporated in this Plan.

Relationship between The Northeast Area Plan and the Comprehensive Plan

The Comprehensive Plan includes citywide policies and priorities for investment based on feedback received through an extensive community engagement effort. Recommendations are grouped by the seven elements illustrated below. The Comprehensive Plan also includes policies that guide future growth in a way that supports citywide policies.

The Northeast Area Plan applies policies from the Comprehensive Plan, and other citywide initiatives, into specific recommendations for the Northeast Area geography based on a closer evaluation of the area and additional community engagement.

While Area Plans help guide future growth, they don't automatically create or require development. Instead, these Plans guide the City's review process when development is proposed. Proposals that generally follow plan recommendations are likely to be approved. Those that don't, face greater scrutiny, and approval is less likely. Plans also provide a guide for future City projects and programs, and future City budgets will determine the timing of those actions.











The Northeast Area

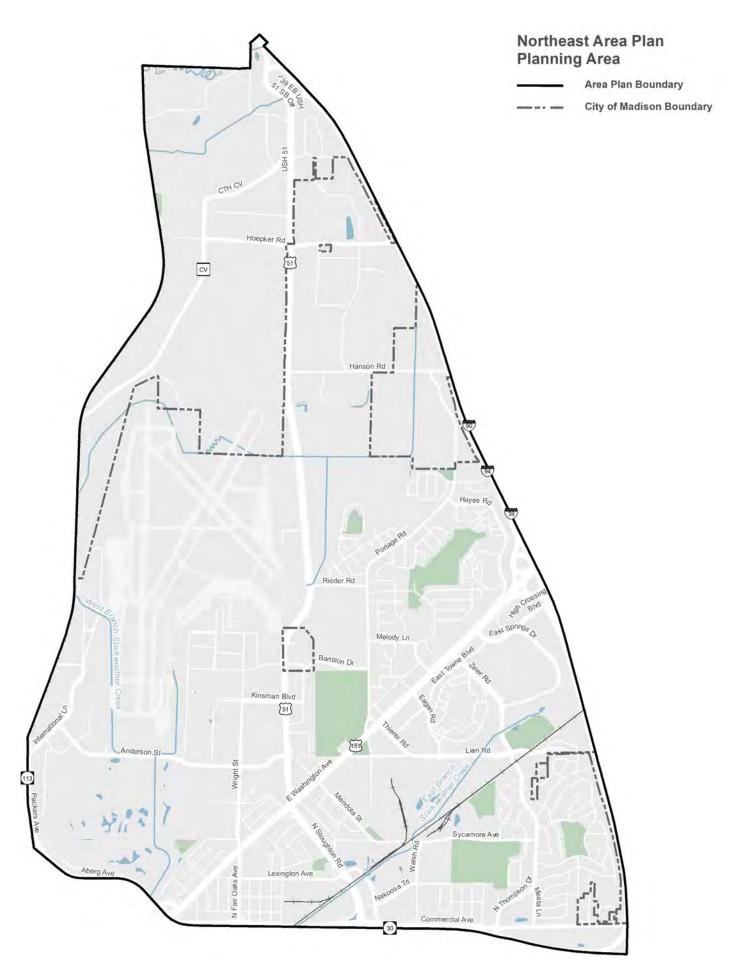
The Northeast Area is generally bound by Interstate 39/90/94 to the east, Highway 30 and Aberg Avenue to the south, and Packers Avenue and the Canadian Pacific rail corridor to the west. The 7,608-acre (12-square mile) area includes major destinations such as the Dane County Regional Airport, Madison College - Truax Campus, and East Towne Mall. It's also defined by natural features including Starkweather Creek, extensive wetlands, and a wooded drumlin at the center of the Sandburg neighborhood.

East Washington Avenue is the primary transportation corridor in the Northeast Area, and a major part of the City's transit network. The east-west Bus Rapid Transit (BRT) line will open in late 2024, providing residents improved travel times and more frequent trips. Highway 51 (Stoughton Road) is also a major route through the Northeast Area, although it has fewer connections to neighborhoods and businesses along its path.

Nearly 15,000 residents call the Northeast Area home. It's one of the most diverse areas of Madison, with more than 40% of residents being persons of color. Resident incomes and college degree rates are lower than citywide averages. The Northeast Area is one of the city's most affordable places to live, with housing costs about 25% lower than the citywide averages. There are two neighborhood elementary schools, Hawthorne and Sandburg, and a future City library: the Imagination Center at Reindahl Park. The area is a major employment hub with 21,000 employees centered around the airport and by East Towne Mall (Source: OnTheMap 2021).

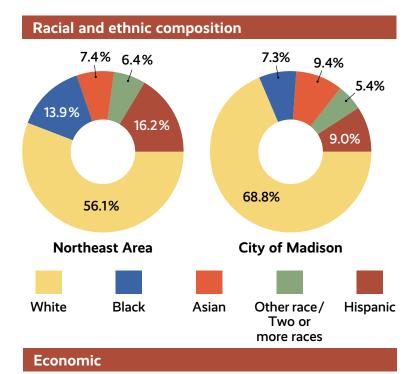
Growth projections in the Comprehensive Plan estimate Madison will add 115,000 residents between 2020 and 2050. As development activity continues to move east along East Washington Avenue from the Downtown area, the Northeast Area could see transformational change on many underutilized sites. This growth will add more than just new residents. It will bring needed new housing that will help slow housing cost increases. It will support existing and future local businesses and create walkable neighborhood destinations. It will add new park and recreational opportunities throughout the Northeast Area.





Demographics

Population City of Madison **Northeast Area** = 10,000 274,622 residents 14,649



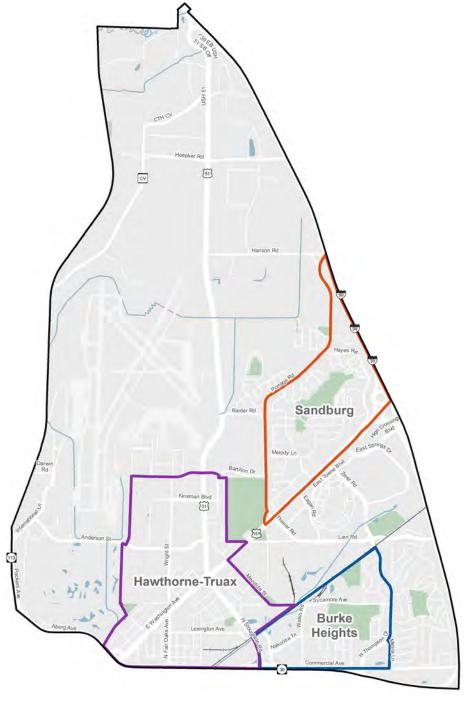
Northeast Area City of Madison

Poverty rate (families)	6.4%	5.9%
Median income	\$59,486	\$70,466
Homeownership (Tenure)	42.4%	44.5%
Cost-burdened households – owners	22.7%	19.8%
Severely cost-burdened households – owners	11.5%	6.8%
Cost-burdened households – renters	36.6%	46.5%
Severely cost-burdened households – renters	14.8%	25.5%

Source: Neighborhood Indicators Project, Census/American Community Survey

Community Action Strategy Areas

The Northeast Area contains three Community Action Strategy areas. Greater numbers of low- to moderate-income households live in these areas, making them eligible for federal Community Development Block Grant funding. The City is directing \$200,000 to Sandburg, \$200,000 to Burke Heights, and \$250,000 to the Hawthorne-Truax area for small scale capital improvements. Additional public engagement and community capacity-building assistance were directed to these areas during the planning process. Given the capacity-building focus, the Community Action Strategy recommendations are not exclusively physical or policy changes, or that are outside City control. These recommendations may rely on community partners for implementation.





Burke Heights



Sandburg



Hawthorne-Truax









Planning Process and Public Engagement

The planning process began in early 2023 and lasted through adoption in summer 2024. It followed a multi-phase approach, with the first meetings focusing on outreach and engagement to ensure community awareness of the process and public feedback. The next series of meetings captured community values and incorporated them into initial recommendations for review, in addition to relevant recommendations from previously adopted underlying plans. The final steps of the process consisted of drafting the plan document, public review of the draft Plan and the formal City approval process. While the public participation strategies evolved over the course of the process, each phase included in-person and virtual events to gather input and facilitate conversations with residents and other stakeholders. Interactive commenting maps and review drafts allowed participants to add their thoughts and see other peoples' comments in a broader community discussion. Neighborhood walks, Park Connects events, and multi-topic open houses were all held to bring more people and viewpoints into the discussion.

The City also hired Community Navigators to help reach a wider range of residents and business owners. Selected from area non-profits with deep connections to northeast Madison, Community Navigators conducted interviews, had informal conversations, promoted the community meetings, and made new connections with people in the Northeast Area.

Primary themes from public engagement is incorporated in each Plan chapter. However, a recurring theme expressed by participants was frustration that the Northeast area was being "dumped on" with negative additions and not seeing positive investments from the public or private sector. Negatives includes noise impacts from the airport, PFAS in Well 15, the homeless encampment at Reindahl Park and the temporary and permanent homeless shelters. While there is excitement for the planned Imagination Center at Reindahl Park, there is frustration that its development is taking more time than originally expected.

Planning Process

Data Gathering & Review

- Interagency staff mtgs
- Public mtgs
 (in-person/virtual)
- Online interactive commenting map
- Regular email notifications and website updates (applies to all phases)

Phase One

Issues & Opportunities

- Community surveys (in-person/virtual)
- 4 public mtgs (in-person/virtual)
- Multiagency Open House
- Review by Board, Commissions & Committees
- Interagency Staff MtgsSandburg Elementary PTA mtg

Phase Two

Draft Plan Concepts

- Pop-up engagement at community events such as Make Music Madison, Westchester Gardens Block Party & others
- Public outreach by 3
 Community Navigators
 (in-person)
- Connect Events at Sycamore & Reindahl Park
- Burke Heights & Sandburg Walk & Talk
- Community Dinner & Alder Forum

Phase Three

Draft Plan Recommendations

- Business Postcard Survey
- Art WorkshopJoint Open House with
- Joint Open House with WisDOT
 Plan Commission
- Board of Parks
 Commissioners
- Transportation Commission

Phase Four

Final Draft Plan Review

- Public Meetings (in-person/virtual)
- (in-person/virtual)Landmarks Commission
- Urban Design
 Commission
- CDBG BoardPublic feedback on Draft Plan
- Joint Open House with WisDOT

Phase Five

MAR-JUN

Adoption & Implementation

- Boards,
 Commissions &
 Committees
- Implementation of Plan

recommendations
Phase Six

JUL+

JAN - MAR

APR - JUN

JUL - OCT

NOV - FEB

2024

2023 202

Plan Format

The Plan is structured around the Comprehensive Plan's seven elements, with each element generally having four sections:

- 1. An introduction, covering background information and current conditions
- 2. What we heard, including major themes in feedback and comments
- 3. Actions, which focus on City-led items
- 4. Partnerships, which focus items led by entities other than the City but may have some City involvement or contributions

While public feedback themes were generally used to inform actions and partnerships, not all summarized themes have corresponding actions or partnerships. Some feedback themes contradicted each other. Some themes conflicted with adopted City policy, such as the Comprehensive Plan. Lastly, some themes addressed topics the City has little control over. Actions in each chapter are listed by number, but do not have a priority order.



What We Heard

- 1. Have walkable neighborhoods, with a mix of housing types and nearby destinations like restaurants, cafes, and local businesses.
- 2. Development should be focused along transit corridors and activity centers.
- 3. Support new development and more housing options.
- 4. Neighborhoods are currently disconnected by highways, railroads, the airport, and natural features like wetlands.



Commercial Avenue and Pulley Drive



FedEx at Hoepker Drive and HWY 51



East Towne Mall area

Comprehensive Plan Goals:

This Plan aims to advance the following goals:

- Madison will be comprised of compact, interconnected neighborhoods anchored by a network of mixed-use activity centers.
- Madison will have a safe, efficient, and affordable regional transportation system that offers a variety of choices among transportation modes.

Land Use

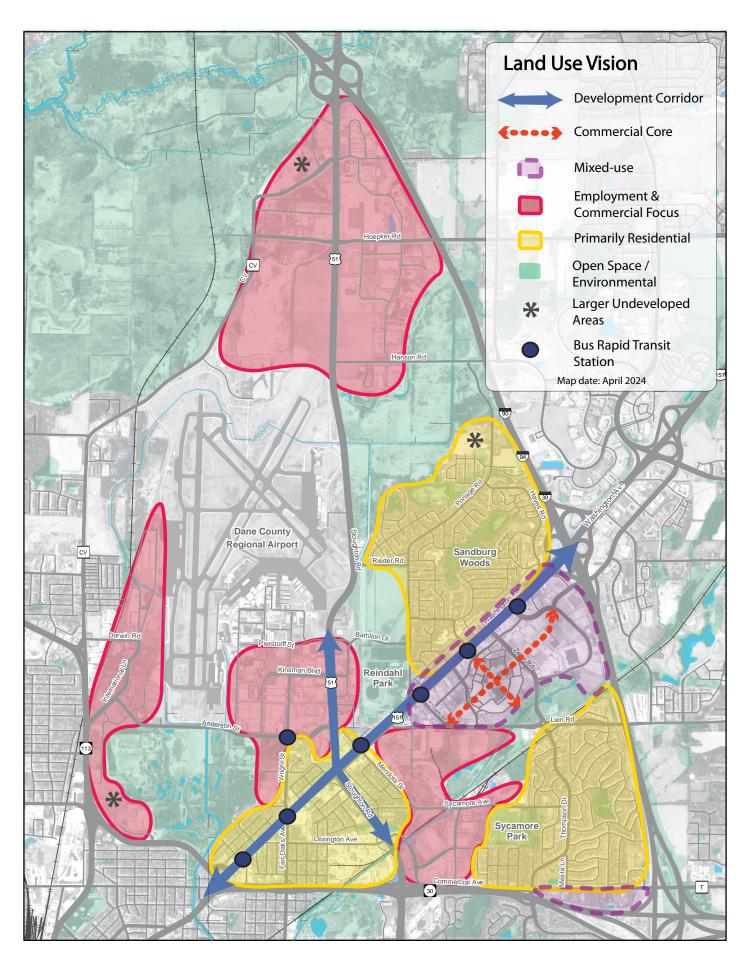
The Northeast Area has a variety of land uses within its nearly 8,000 acres. Parks and open spaces are present throughout the area, and include large wetlands along Starkweather Creek, wooded drumlins, park spaces, and Bridges Golf Course. Approximately 6,900 housing units are predominately located in three separate areas, Carpenter Ridgeway/Hawthorne/Mayfair, Sandburg, and Burke Heights (Thompson Drive). Commercial land, including retail at East Towne and office spaces near the airport, accounts for 11% of the Plan area, with industrial and institutional (educational) land largely centered on Highway 51, occupying 7% of the area.

Land Use	Acres	% Of Total
Residential	946	12%
Commercial	846	11%
Institutional/Governmental	283	4%
Industrial	271	4%
Park and Open Space	1,947	26%
Agriculture/Vacant/Under Construction	644	8%
Mineral Extraction	124	2%
Airport	1,316	17%
Utilities, Transportation	1,231	16%
Total	7,608	100%

Source: Capital Area Regional Planning Commission (CARPC) 2020 Land Use

There is little undeveloped land in the Northeast Area. Most vacant sites are industrial properties near Highway 51 and Hoepker Road. There are few other undeveloped areas, including parcels on Portage Road just north of the Greater Sandburg Neighborhood and on County CV just west of Highway 51.

Much of the recent development within the Northeast area was for industrial uses in the same Highway 51 and Hoepker Road area. In other areas, redevelopment is beginning to occur along the East Washington corridor, including the former Bimbo Bakery site and the Capital Auto Credit property. The East Towne Mall area is also beginning to see investment and redevelopment. Several vacant buildings are being repurposed for new uses, and new hotels, residential, and mixed-use buildings are being built.

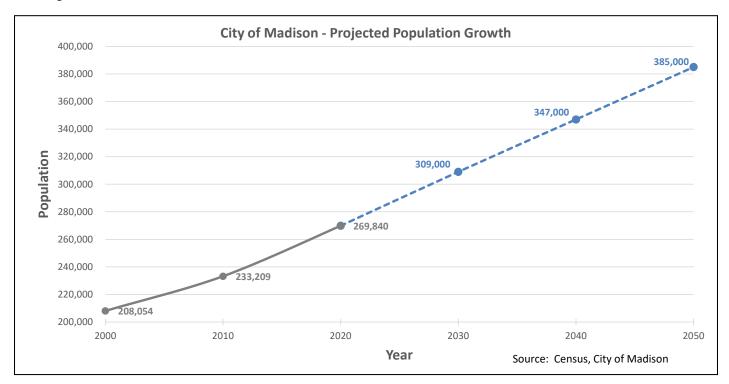




Rise Madison housing development under construction on the former **Gardner Bakery site at East** Washington and Fair Oaks Avenues

Future Growth

According to the Comprehensive Plan, Madison is projected to add 115,000 new residents and 50,000 jobs between 2020 and 20501. This projection is largely based on past growth trends. While this growth will be spread across the entire city, a portion will occur within Northeast Area. The Comprehensive Plan emphasizes accommodating growth through redevelopment of underutilized sites, particularly on transit corridors. Redevelopment generally uses existing infrastructure and can increases housing availability on transit corridors while reducing automobile dependency. Given the lack of undeveloped property in the Northeast Area, redevelopment will likely be the primary way that future growth occurs.

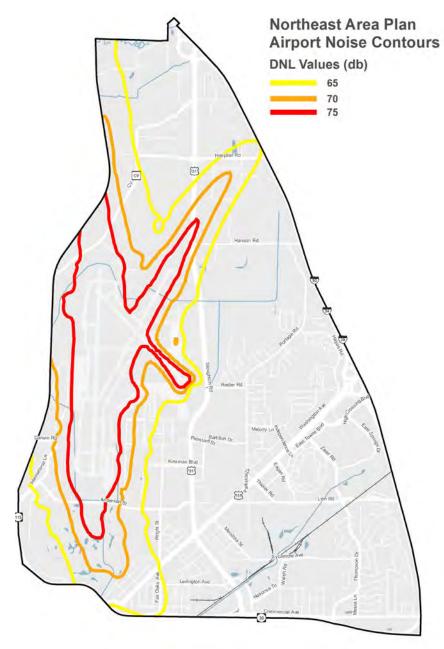




East Washington Avenue construction of BRT station

The Comprehensive Plan identifies **Growth Priority Areas**, which are locations best suited to accommodate growth consistent with the Comprehensive Plan's goals. Within the Northeast Area, the East Washington Avenue corridor, East Towne area, and the industrial area along Highway 51 are identified as Growth Priority Areas where development and redevelopment should be directed and encouraged. These areas have larger concentrations of underutilized properties and are well-served by transit and utilities, making them attractive for redevelopment and to future residents, visitors, and employers. The area around East Towne Mall is defined by expansive parking lots, nearby big box stores, and a street network designed around cars and not people. On East Washington Avenue, there are several strip commercial buildings likely to redevelop in the coming years. Collectively, these Growth Priority Areas could transition to new, well-connected residential, commercial, and employment areas that better meet the needs of the City and its residents.

¹ Source: City of Madison, Greater Madison MPO



Day Night Average Sound Level (DNL) is a noise metric used to reflect a person's cumulative exposure to sound over a 24-hour period and is the Federal Aviation Administration's required noise metric for the assessment of aircraft noise.

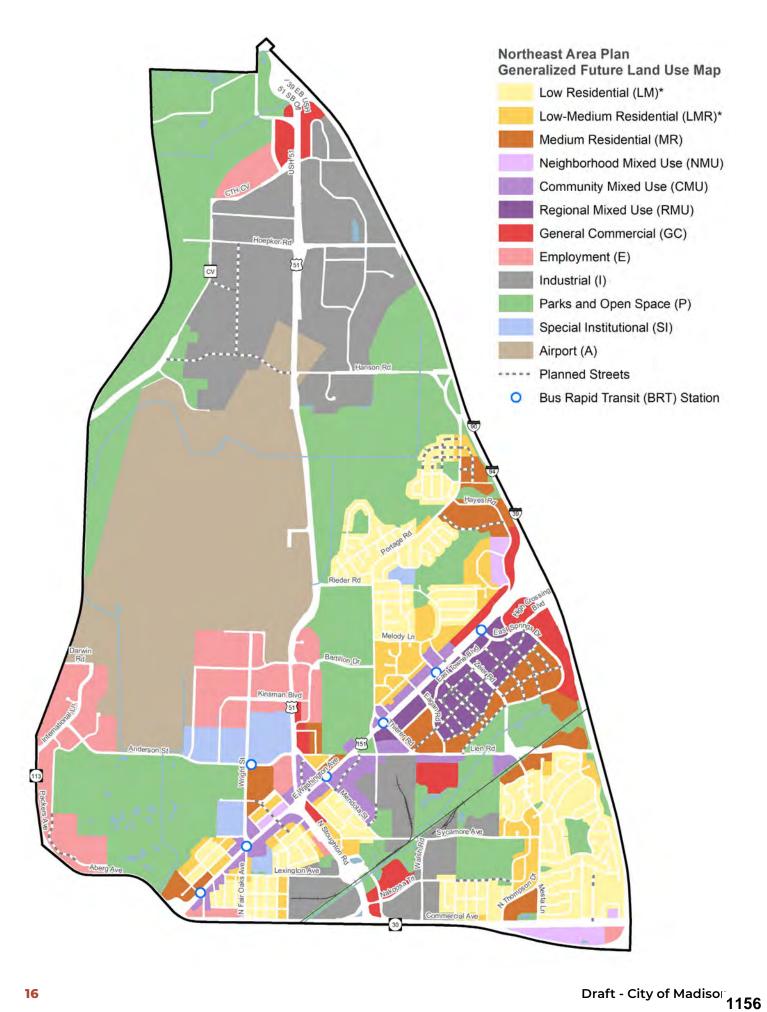


Airport Noise

With F-35 fighter jets replacing F-16s at the Dane County Regional Airport (DCRA), noise exposure in neighborhoods surrounding the airport became a focus of community conversation and concern. Airport sound modeling, which includes both military and commercial flights, indicates the Carpenter Ridgeway area would likely see the greatest impacts, with average sound exposures beyond what is considered compatible for typical residential construction. In 2022, the Common Council established a committee of alders to evaluate alternative approaches to land use and growth in the noise impacted area. Ultimately, those alders found the elevated noise exposure did not merit a prohibition on new residential construction in this area. They did recommend the City encourage sound insulating construction techniques (State law prohibits the City from requiring sound insulating construction).

Dane County Regional Airport (DCRA) is currently developing its noise compatibility program, a federal program that funds strategies to minimize and mitigate noise exposure surrounding airports. Recommended strategies may shift the sound impacts north and away from higher concentrations of residential south of the airport. This may resolve noise compatibility concerns without changes in construction or noise insulation.

As part of the noise compatibility program, DCRA is recommending an expansion of the Airport Affected Area, a legal boundary that allows the DCRA to veto rezonings approved by the City up to three miles away from the airport. While the City can override a veto with a 2/3 majority vote of the Common Council, this creates uncertainty in the development process and could result in fewer housing units getting built.



Recommended Future Land Use and Growth Areas

The Generalized Future Land Use (GFLU) Map establishes a framework for how the Northeast Area can grow and evolve over time. The map identifies broad categories of development such as residential, commercial, or industrial uses, as well as recommended building size. The future land use categories guide future growth and establish the basis for how the City reviews development proposals. The land use recommendations in this Plan embody several of the Comprehensive Plan's citywide strategies, including:

- Concentrating the highest intensity development on major transit corridors and at activity centers
- Facilitating compact growth to reduce development of farmland
- Increasing the amount of available housing
- Supporting the development of a wider mix of housing types, sizes, and costs.

While the GFLU Map establishes a future vision, there isn't a defined timeline for change. Many factors impact development, making it hard to predict where and when it might occur. The overall economy, market demand, available properties and financing, and developer priorities all influence the timing and location of development proposals.



Low Residential (LR) *







Low-Medium Residential (LMR) *







Images below illustrate types of development commonly found in each category. More information on each of the land use categories can be found in the Comprehensive Plan.

*In select conditions, Low Residential may allow development up to 30 dwelling units/acre (DU/ac) and three stories. In select conditions, Low-Medium Residential may allow development up to 70 DU/ac and four stories.















The East Towne area in its current form (above) and a vision for the area at build-out (bottom). Existing building footprints shown in white.



Redevelopment concepts on East Washington Avenue near Highway 51 (above) and Highway 30 (below).



Within the Northeast Area, the most visible change could be at or around East Towne Mall. This Plan incorporates recommendations from the Greater East Towne Area Plan, which envisioned the area transitioning from a regional mall to a vibrant urban neighborhood. A compact, walkable network of streets would support a mixture of uses, including commercial, employment, and residential, with building scales from three to ten stories. Taller, more commercial-oriented buildings are located closer to Bus Rapid Transit (BRT) stations on East Washington Avenue, and the area transitions to a mixture of residential uses towards Starkweather Creek. Areas along the Interstate corridor are recommended to remain as commercial or transition to employment (office, lab, medical, etc.), as road noise makes this location less appropriate for residential. Several new park spaces are recommended to support new residents, including a large green corridor along Starkweather Creek.

Beyond East Towne Mall, the East Washington Avenue corridor has sites that could redevelop in the foreseeable future. Many of these were addressed by the Hawthorne-Truax Neighborhood Plan, which planned for sites to transition from single-story commercial to more intensive mixed-use development.

Along with redevelopment planned for East Washington Avenue and other major sites, several areas are appropriate for "Missing Middle" housing. Missing Middle housing is a range of housing types between single-family detached houses and smaller apartment buildings, such as rowhouses and small multi-family buildings. Low-Medium Residential (LMR) areas on the GFLU Map are intended to accommodate Missing Middle housing. Identifying lower scale residential areas ensures the city will have a mix of housing types, and not just larger residential buildings in Medium Residential (MR) and High Residential (HR) areas.

Implementation and Development Regulation Maximum Building Heights

The Comprehensive Plan provides height guidance for most land uses. A select

few land use categories don't have recommended height ranges or allow Area Plans to recommend heights that may differ from those ranges. Mapping maximum building heights for these categories is intended to increase predictability and simplify the development review process by communicating recommended height limits for these categories. The maximum building height recommendations consider the intensity of development for each land use category, adjacent land uses, desired urban form, and proximity to BRT. The map below illustrates the recommended maximum height in stories. Refer to the Comprehensive Plan for guidance on areas without identified maximum heights on this map. Northeast Area Plan **Building Height Map** Low Residential (1-2) Low-Medium Residential (1-3) Medium Residential (2-5) Neighborhood Mixed Use (2-4) Community Mixed Use (2-6) Regional Mixed Use (2-12)* General Commercial Employment Industrial Parks and Open Space Special Institutional Airport *Land use category where Area Plans may allow height beyond Comprehensive Plan range. White labels are heights limited by Airport Height Limits. Black labels are not limited by Airport Height Limits. 6 4 Low Residential may allow new construction up to 30 DU/ac and three 6 stories in select conditions, generally along arterial streets or where these 4 types of buildings are already present or planned within an adopted subarea plan as part of a pattern of mixed residential development. Low-Medium Residential may allow new construction up to 70 DU/ac and four stories in select conditions. Factors to be considered include relationships between proposed buildings and their surroundings, natural features, lot and block characteristics, and access to urban services, transit, arterial streets, parks, and amenities.

The entire Northeast Area is within <u>Dane County Regional Airport's Height</u> <u>Limitation Zone</u>, which establishes a maximum elevation (feet above sea level) that buildings can't exceed. This, along with ground level elevation, establishes the legal maximum height for every site in the Northeast Area. In several areas, maximum building heights (displayed in stories) are limited by the airport height restrictions and not other considerations.

Commercial Core

The GFLU Map identifies several mixed-use categories, largely focused on the East Washington Avenue corridor and the East Towne Mall area. These areas are planned for a mixture of residential, commercial, and employment uses within the larger area to achieve an active and vibrant place. While a mixture of uses is envisioned, mixed-use buildings with retail or other commercial uses on the ground floor are generally not required. Within these areas, commercial uses are seen as appropriate as part of a broader use mix, but not every location can support ground floor commercial businesses.

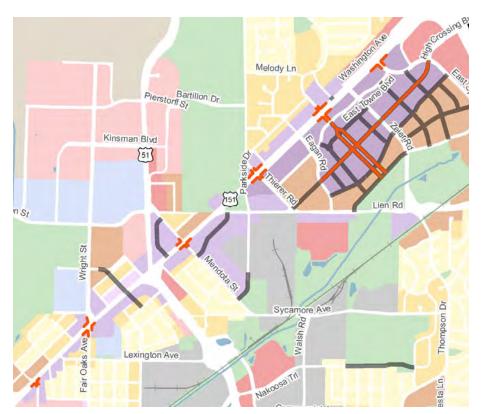
Visibility and enough residents and customers nearby are needed to support the businesses that provide those goods and services.

Future buildings along Commercial Core frontages should have ground floor commercial uses (retail, restaurant, service, or office uses). These areas appear capable of supporting additional commercial spaces and are located at important locations.



Ground floor commercial spaces create active spaces and are required on **Commercial Core frontages**





Planned Streets Commercial Core

Aligning Future Land Use and Zoning

While the GFLU Map establishes a vision and future development guidance, zoning regulates what uses are allowed and what can be built now. Because of this difference, sometimes zoning and land use recommendations point in different directions. As part of this Plan, recommended future land uses and current zoning were evaluated to identify potential conflicts.

This Plan recommends several properties be considered for proactive rezoning, a City-initiated process that changes a property's zoning to a district more consistent with the future land use recommended in this Plan. Certain proactive rezonings may simplify approval processes for future development proposals that are consistent with Plan recommendations. Other are intended to avoid potential land use conflicts that could arise from the current zoning districts. Proactive rezoning is a separate legislative process that occurs after the adoption of this Plan and requires further review by the Common Council.

While proactive rezoning addresses a property's zoning, the evaluation also considered overlay districts. In one instance, a recommended land use change altered the context that was the basis of the Transit Oriented Development (TOD) overlay district boundary. The Target site on Lien Road site is nearly a half mile from a BRT station and on the TOD district boundary. It has no street frontage and is bordered by industrial uses and wetlands. As a result of these factors and the changed land use recommendation, this Plan recommends revising the TOD overlay district boundary to remove these parcels.

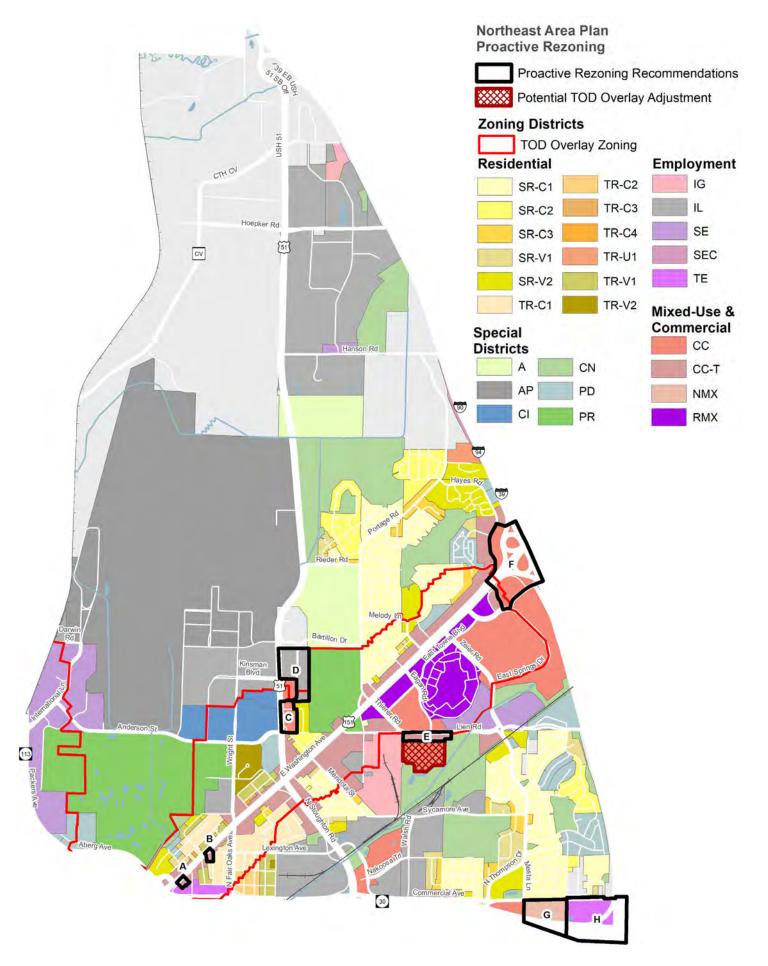
Other potential zoning changes were identified that could improve transparency and usability of the zoning code, but are citywide issues generally beyond the scope of Area Plans. These potential changes center on Planned Development zoning districts and City parks. There are 22 Planned Development zoning districts in the Northeast Area, all adopted under an older version of the City's Zoning Code. These could easily transition to conventional zoning districts that provide greater clarity and transparency while reducing the time it takes for City staff to respond to inquiries and issue routine permits. These transitions would not increase or decrease the allowable use of a property; instead, potential new zoning districts would best match the existing buildings and uses already in place.

The zoning for many parks doesn't reflect their use, potentially creating confusion during park planning processes. This confusion could be avoided by changing the zoning to match the park type established by the Parks Division and the Board of Park Commissioners.

Planned Development Zoning

Many areas in the city have a site-specific zoning called Planned Development (PD). PDs are intended for unique buildings or development projects that can't be approved using conventional zoning districts. These developments don't need to follow other zoning requirements and establish their own unique and detailed zoning code addressing uses, building placement, height, landscaping, parking, etc.

PDs were heavily used prior to the adoption of the current zoning code in 2013. The previous code, adopted in 1966, was so burdensome, complex, and out-of-date that PD zoning was routinely used for relatively typical projects, including single-family homes and small apartment buildings. Over the years, the City approved over 1,500 PDs, and while they simplified the approval process, they create long-term challenges for residents, property owners, and the City. Zoning codes are amended over time to address new issues or priorities that emerge, but PD files are rarely updated and reflect the time when they were adopted. Zoning code updates allowing accessory dwelling units and the keeping of chickens, or those requiring electric vehicle charging or bird-safe glass, do not apply to these PDs. The zoning text for PDs are only available in person (not online, like the rest of the code), and review and approval for minor changes like the addition of a shed can take weeks instead of minutes. The current zoning code is much more adept at handling these now-typical developments that were previously approved as PDs. Transitioning PDs approved under the old code to conventional zoning districts will improve transparency and usability for residents and property owners while improving the ability of the City to respond quickly to inquiries and routine applications like fences, sheds, and additions.



Site	GFLU	Existing Zoning	Recommended Zoning	Notes
Α	CMU	TE	CC-T	Create consistency with GFLU and adjacent zoning.
В	CMU	TR-C1	CC-T	Create consistency with GFLU and adjacent zoning.
С	GC	CC-T	CC	Change to discourage residential in less appropriate area.
D	Е	CC	IL	Change to prevent residential in less appropriate area.
Е	CMU	CC	CC-T	Create consistency with GFLU.
F	GC	CC-T	CC	Change to discourage residential in inappropriate area.
G	NMU	SR-C1, A	NMX	Create consistency with GFLU.
Н	NMU, E	SR-C1	TE	Create consistency with GFLU.

Actions

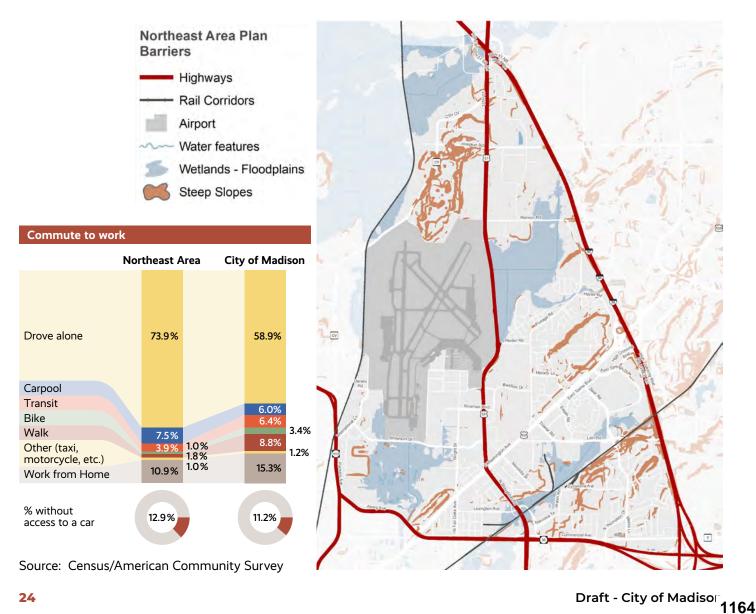
- 1. Update the Comprehensive Plan Generalized Future Land Use Map to be consistent with the Future Land Use map in this Plan.
- 2. Implement maximum building height recommendations shown on the Building Height Map with a mechanism for allowing bonus stories for developments that achieve specific affordability or sustainability parameters, similar to downtown height bonuses in City ordinances (Madison General Ordinances 28.071(2)(a)2).
- 3. Require ground floor commercial space or other active uses in the locations shown on the Commercial Core Areas Map.
- 4. Initiate a follow-up process to consider zoning changes shown on the Proactive Rezoning Map. Ensure all impacted property owners are aware of potential zoning changes.
- 5. Explore transitioning Planned Developments approved prior to 2013 to a matching conventional zoning district to improve transparency, usability, and consistency of regulations while reducing administrative burdens and inquiry response time.

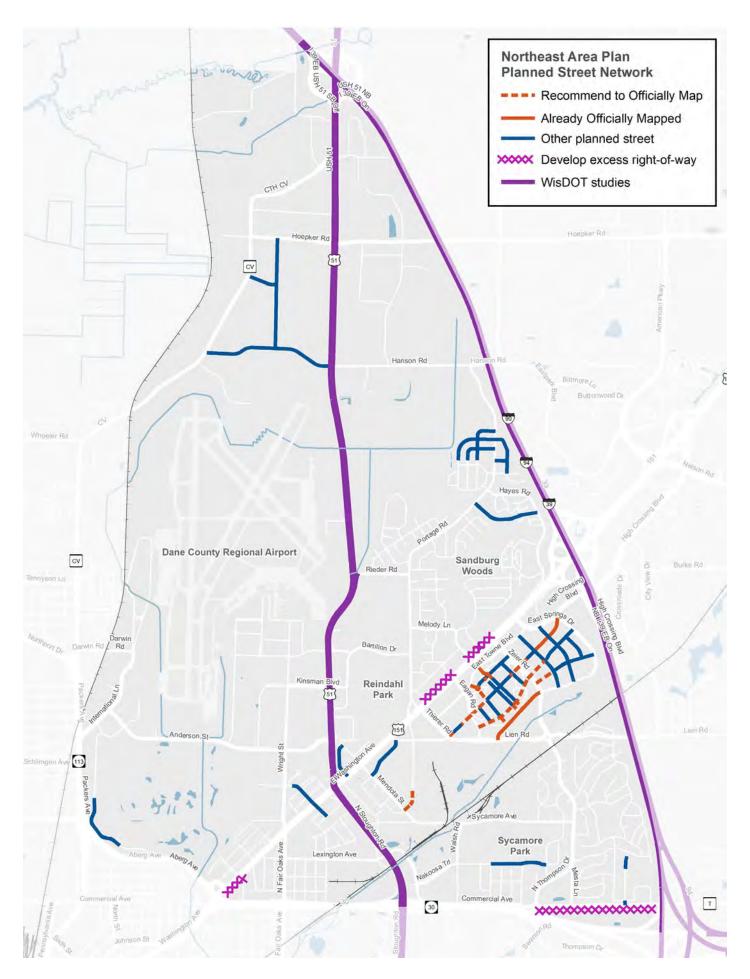
Transportation

What We Heard

- 1. Improve pedestrian safety and comfort along major roads and heavily-traveled neighborhood streets and crossings.
- 2. Speeding in residential areas creates safety concerns.
- 3. Increase bicycle network connectivity to places in the area like schools and to the city as a whole.

The Northeast Area's transportation network includes streets, transit, bicycle routes, and sidewalks. The primary transportation corridors are East Washington Avenue and Highway 51, with Highway 30 and Interstate 39/90/94 on its perimeter. Physical barriers that limit connections and all forms of transportation are prevalent in the Northeast Area. The airport is the largest barrier, spanning three miles between Hoepker Road and Anderson Street, separating Madison's north and northeast sides. Major roadways, such as the Interstate, Highway 30, and Highway 51 have few crossings which limit pedestrian and bicycle options and funnel traffic to those intersections. Two rail lines further limit connections, as it's difficult to obtain approval for new railroad crossings. Large natural barriers also limit connectivity in certain areas, including the Starkweather Creek wetlands south of East Towne Mall. Steep slopes present additional difficulty connecting areas, particularly north of East Washington Avenue.





Street Network

The local street networks within individual neighborhoods are generally complete. However, there are limited connections between neighborhoods due to the natural and physical barriers already discussed. Older neighborhoods such as Carpenter Ridgeway and Hawthorne have a simple, grid-like pattern, while newer neighborhoods built between the 1960s and 1990s have a more meandering but relatively connected street network. The East Towne Mall and surrounding big box retail areas were designed for car access and the street network reflects that. For example, one block has a 1.7-mile perimeter and is 26 times the size of blocks in more walkable areas of the city.

Potential extension of Nakoosa Trail



This Plan recommends changes to the street network to create walkable pedestrian-oriented places, better connect neighborhoods, and balance vehicular traffic over multiple routes. The largest concentration of planned streets is within the East Towne Mall area. These streets are needed to support that area's future growth and should be built as redevelopment occurs. Other street connections, such as the proposed extension of Nakoosa Trail, would better link existing neighborhoods while increasing visibility of, access to, and utilization of Sycamore Park.

The Street Network Map also identifies the highest priority future streets that could be added to the City's Official Map. The Official Map reserves space for future public use, like streets or parks, by preventing the issuance of building permits in that location. The Official Map is amended through a separate legislative process that would occur after the adoption of this Plan and requires further review by the Common Council.





In addition to new street connections, there are instances where land currently used for roads or right-of-way could serve better purposes, potentially being incorporated into the redevelopment of neighboring properties. Two segments recommended for removal are frontage roads on East Washington Avenue, one at Rethke Terrace and one between Hagan Drive and Zeier Road. Removing these frontage roads would create additional space for new development while bringing new buildings closer to East Washington Avenue, resulting in a better pedestrian environment and improved visual character of the area. The Commercial Avenue right-of-way east of Thompson Drive is the third area that could be partially transitioned over time. The 250-foot right-of-way was the original alignment of Highway 30 before it was shifted south. While Commercial Avenue is an important street and would be maintained, much of its rightof-way could be repurposed for more productive uses.

Illustrations of potential development on East Washington Avenue frontage Roads. Independence Lane existing conditions (top) and with the frontage road removed (bottom).



Fair Oaks Avenue (existing condition)



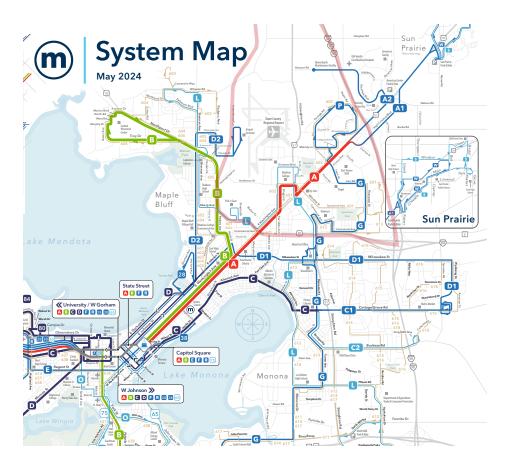
Potential redesign of Fair Oaks Avenue (long-term)

Street design

Residents often discussed speeding as an issue in the Northeast Area. Older street designs contribute to this issue with wide lane widths and underutilized parking lanes, creating a wide open feel for drivers. Changing the road character through improvements such as bump outs or larger reconstructions can repurpose road space for other uses like street trees, rain gardens or protected bike facilities. Illustration (left) shows a potential transformation of Fair Oaks Avenue. Wide tree terraces and a shared use path could be added in the space that was formerly excess roadway and parking lanes.

Transit

In 2023, Metro Transit started a new route network, which was redesigned to increase access to employment, improve bus frequency, and decrease travel times. The network was designed around the new Bus Rapid Transit (BRT) system. BRT, including Route A on East Washington Avenue, includes dedicated bus lanes, new passenger stations, traffic signal priority, and enhanced fare systems to significantly improve travel times on that route. Planned BRT Route B serves the western edge of the Northeast Area, following the Packers Avenue corridor. Several other local routes serve the Northeast Area: Route P goes through the Sandburg neighborhood; Route G links the Burke Heights neighborhood with East Towne Mall and the South Transfer Point; and Route L follows Highway 51 and Aberg Avenue while connecting to Madison's northern and southeastern neighborhoods.



Missing sidewalks near Hawthorne **Elementary School**



Pedestrian crossing of Highway 51 at **East Washington Avenue**



Rectangular rapid flashing beacon (RRFB) in Madison



Cycle track in Charlotte, North Carolina

A two-way protected bike lane (cycle track) could be added to Portage Road to address the fragmented nature of more comfortable bike facilities

Bike and Pedestrian Network

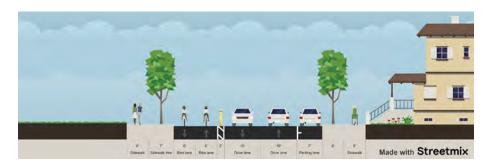
Sidewalks and paths are essential public infrastructure, providing dedicated space for pedestrians to safely and comfortably move about. Within the Northeast Area, 48% of streets (by length) have sidewalks on both sides, consistent with the City's Complete Green Streets Guide. Most missing sidewalks are located within the Carpenter Ridgeway and Hawthorne neighborhoods, which were originally built when that land was in the Town of Burke and later annexed into the City of Madison. Streets with missing sidewalks surrounding Hawthorne Elementary School are particularly concerning, as children must walk on roads during the morning rush hour. Other notable missing sidewalks link residential areas to commercial areas, including Lexington Avenue and blocks of East Washington Avenue near East Towne Mall. Most streets outside the City but within the Northeast Area don't have sidewalks currently but will generally have pedestrian facilities added when they are reconstructed.

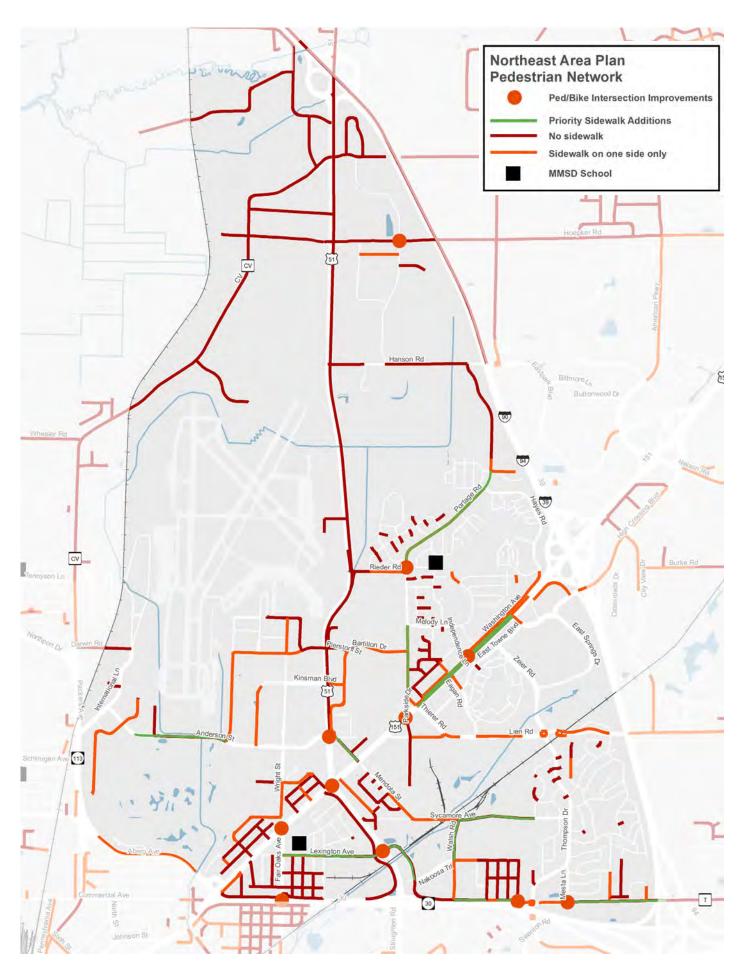
In addition to missing sidewalks, the pedestrian network is impacted by difficult crossings of busier streets. Several intersections on East Washington Avenue as well as Highway 51 were identified by residents as needing improvements to allow safer and more comfortable pedestrian crossings.

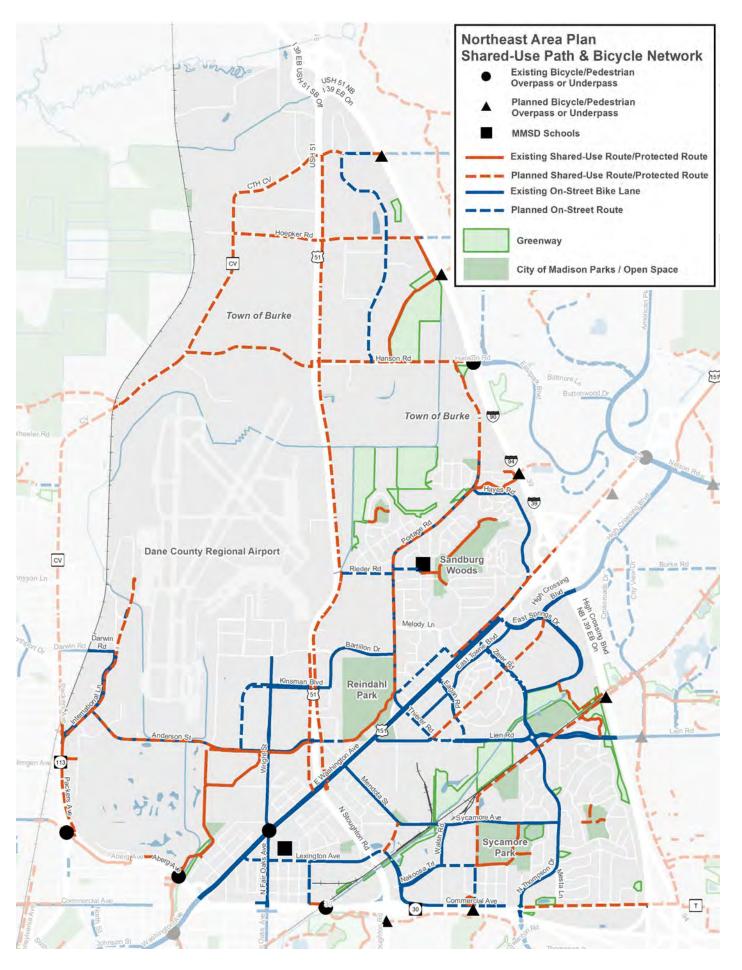
The bicycle network is inconsistent and not well-connected through most of the Northeast Area. Heavily used shared-use paths, like the Starkweather Creek Path, frequently transition to less comfortable on-street routes. Some cyclists don't feel safe using on-street bike lanes, especially on busier streets, and as a result don't use the larger bike network. Residents frequently discussed the challenges of crossing Highway 51, and this barrier separates many Northeast Area neighborhoods from the more robust bike network further to the west.

The Pedestrian Network Map identifies streets that do not have sidewalks on one or both sides. As part of the Hawthorne-Truax Neighborhood Plan, residents expressed broad support for adding sidewalks, particularly near Hawthorne Elementary School. The Pedestrian Network Map also shows Priority Sidewalk Additions, which due to their importance within the pedestrian network and higher traffic volumes, should be considered for construction with or without a street reconstruction project. This Plan identifies specific intersections for safety improvements that may include adding crosswalks, pedestrian islands, tabletop crossings, rapid flashing beacons (RRFBs), and bump outs.

The Bike Network Map shows recommended changes that, once constructed, will provide more consistent and comfortable bike routes. These include enhanced crossings of Highway 51 at Anderson Street and Lexington Avenue and routes that better link eastern neighborhoods. The recommended bicycle network consists of both on-street and shared-use (off street) routes.







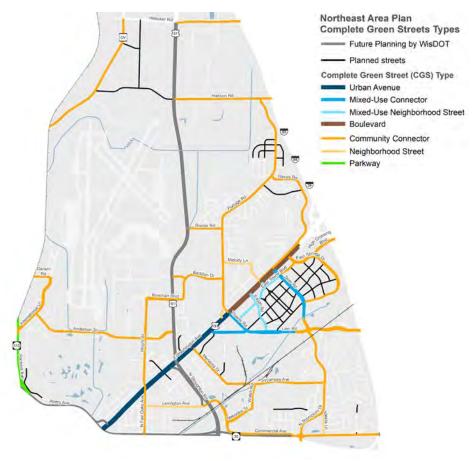
Other Transportation Initiatives

The City has several major transportation initiatives that influenced and are supported by Northeast Area Plan recommendations. The City's Vision Zero Campaign is a data-driven strategy intended to eliminate deaths and injuries on all roadways, bicycle facilities, and sidewalks by 2035. To achieve this goal, physical improvements are made at locations with a high number of traffic-related injuries. The High Injury Network Map uses city-specific crash data to determine where to focus resources to eliminate serious and fatal crashes. Because collisions between motor vehicles and people result in more serious injuries, areas where people tend to walk, bike, and board transit are of particular concern. This includes many of the intersections of East Washington Avenue, Highway 51 and Anderson Street, Portage Road and Rieder Road, and Commercial Avenue and Mesta Lane. Vision Zero is reinforced by the City's Safe Streets Madison Program, which focuses on traffic safety measures (such as speed humps, mini traffic circles, pedestrian islands) and improving connectivity by closing gaps in pedestrian and bicycle networks.

The City established an initial "All Ages and Abilities Bike Network" map in the 2022 Complete Green Streets Guide.

The network identifies streets and paths that are intended to provide a complete network of low-stress bikeways between neighborhoods, key destinations in the city, and to adjacent municipalities. The streets or paths may need to be built or improved to provide a low-stress connection, but the map serves as the long-range plan to reach the goal of a full network. The initial All Ages and Abilities Bike Network map is planned to be updated in the coming years and this Plan's recommended bicycle facilities will inform potential route changes.





Northeast Area Plan - Draft



Stoughton Road and Highway 30 interchange

WisDOT Highway 51 Study

The Wisconsin Department of Transportation (WisDOT) is studying Highway 51 from Interstate 39/90/94 to Highway 30 for potential changes intended to improve safety, mobility, and reduce traffic congestion. While WisDOT's focus is transportation, the City has broader interests regarding potential benefits and impacts from proposed improvements on neighborhoods and businesses, and bicycle and pedestrian connections along its length. The most important intersections within the Northeast Area are East Washington Avenue, Anderson Street, Lexington Avenue, and the Highway 30 interchange. WisDOT has proposed relatively minor changes for the remaining intersections. All intersection designs being evaluated are on WisDOT's project webpage.

East Washington Avenue Intersection:

Two intersection designs are being considered. One is a conventional intersection, similar to the current design but with an additional through lane in each direction and an additional southbound left turn lane. It would require the acquisition of the Schoepp Motors and Subway properties on the northern corners of East Washington and several driveway closures. The second alternative is a jug handle intersection, a design similar to the one at Mineral Point and Junction Roads. This alternative requires significantly more acquisition and demolition on the southwest quadrant of the intersection, needed to accommodate the ramp providing all turns from and to the lanes of southbound Highway 51.

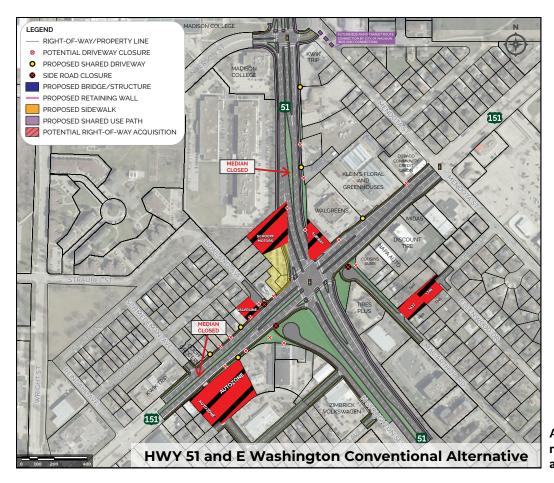
Each design has certain advantages. The conventional intersection requires the least amount of demolition and better supports existing uses and future redevelopment. The jug handle intersection would impact a much larger area, but would result in shorter crossing distances for bikes and pedestrians on East Washington Avenue. In both designs, WisDOT proposes closing the intersections of Rowland Avenue, MacArthur Road, and the frontage road south of East Washington Avenue. Both designs extend Schmedeman Avenue through the Autozone property and Hover Drive to MacArthur Road to provide local access. The City opposes the closures of Rowland Avenue and MacArthur Road.

Lexington Avenue Intersection:

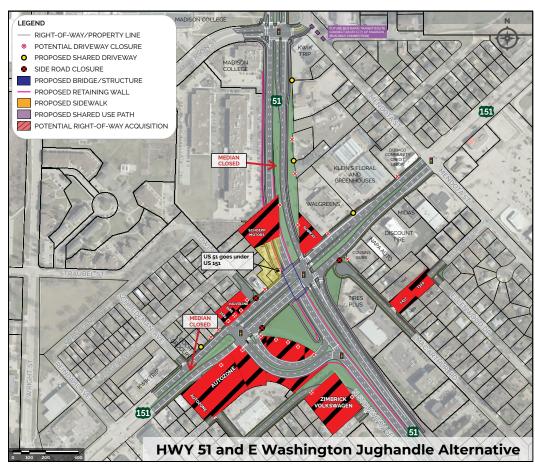
Two intersection designs are being considered. The first is very similar to the current design, but adds pedestrian crossings and shifts the frontage road intersections further west into the County Materials property. The other is a threelegged intersection where Highway 51 passes over Lexington Avenue and turns on and off Highway 51 occur at a new T intersection further north. This design creates a dramatically more comfortable crossing of Highway 51 for bikes and pedestrians, addressing a frequently mentioned issue heard during public engagement events. The grade separation also better supports future Amtrak passenger rail. The City recommends the three-legged intersection.

Highway 30

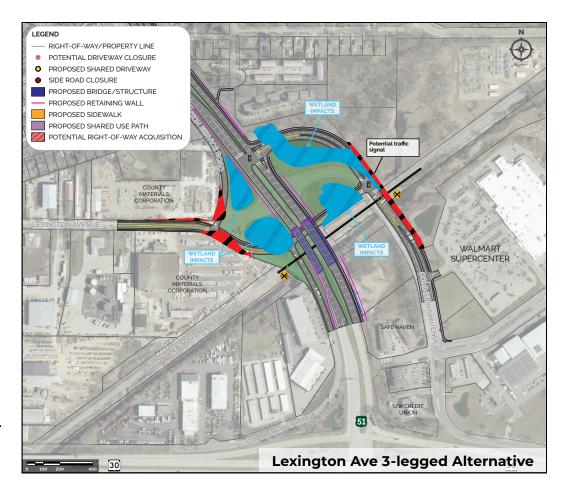
Two options are being considered for the Highway 30 interchange: a conventional interchange similar to the current design and a diverging diamond interchange. The City recommends the conventional interchange as crossings are much simpler for bikes and pedestrians.



At-grade intersection, no bridges required for this alternative



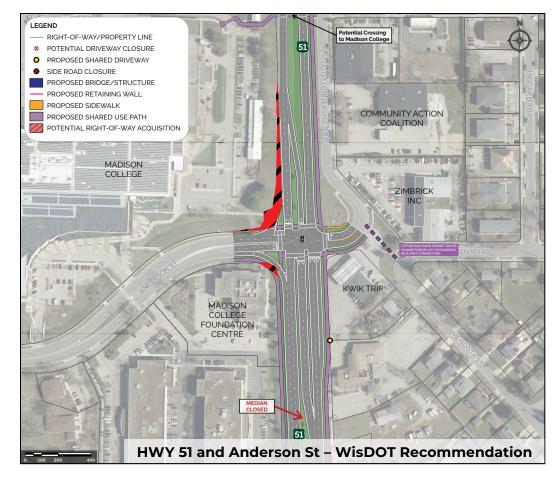
Grade separated intersection, bridges required for southbound U.S. 51 movement in this alternative



Grade separated intersection, bridges required for this alternative. Railroad of intersection is being investigated for potential high-speed rail.



At-grade intersection, no new bridges required for this alternative. Railroad south of intersection if being investigated for potential high-speed rail.



Anderson Street

WisDOT is recommending an intersection very similar to what is there now. Anderson Street is a critical bike and pedestrian crossing, linking the Sandburg neighborhood with Carpenter Ridgeway and shared-use paths connecting to central Madison. Signal timing and phases should ensure comfortable crossing. This intersection will also serve the future BRT crossing and link to Mendota Street.

Bike and Pedestrian Paths:

WisDOT's plans generally reflect the City's planned shared-use path on the east side of Highway 51 south of Kinsman Boulevard. North of Kinsman, there is no sidewalk or shared-use path, preventing safe use of this stretch by those not in motorized vehicles. The City recommends WisDOT include this shared-use path in its plans. If this is not possible, WisDOT should include and fund an alternative path or on-street protected bike lane on a parallel route such as Portage Road.

WisDOT Interstate Study

WisDOT is also studying Interstate 39/90/94 between the Beltline and Wisconsin Dells. WisDOT indicates the study is intended to address existing and future traffic demands, safety issues, and aging and outdated infrastructure.

Interstate

Throughout the Madison area, including the segment that is adjacent to the Northeast Area, WisDOT is proposing to add one lane to the Interstate in each direction. The segment of the Interstate north of Highway 151 would be expanded from 6 lanes to 8 lanes. The portion of the Interstate south of Highway 151 would be expanded from 8 lanes to 10 lanes. To the south of Highway 151 there would also be multi-lane on-off ramps running parallel to the 10 lanes. These "collector-distributor lanes" reduce potential conflict points where drivers can merge between lanes, but they take up a lot of space.

The proposed additional lane in each direction and collector-distributor lanes would result in the Interstate having 17 total lanes in some locations. This increase in the number of lanes would bring traffic closer to noise-sensitive uses along the Interstate such as homes and parks. In some locations, WisDOT would need to acquire private property for this expansion. Additionally, while adding lanes to a highway reduces traffic congestion initially, it incentivizes people to drive more and drive further distances. Over time, as more people drive more and drive further, the highway just becomes congested again.

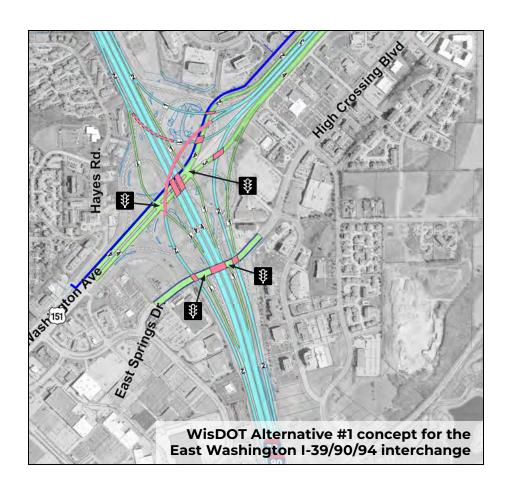
The City has provided comments to WisDOT on the proposed Interstate expansion. The City is suggesting, at a maximum, the addition of a "flex" lane in each direction, which is only open during heavier traffic times. Madison also suggests removal of the separate collector-distributor lanes.

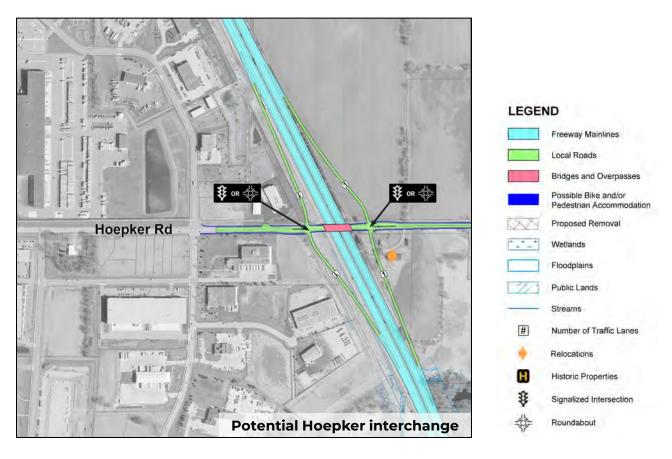
Existing Interchanges

The Interstate project would also reconfigure each interchange. The four existing interchanges adjacent to the Northeast Area are being considered for the following:

- Highway 51 interchange: generally maintain the existing configuration. A new north-south shared-use path through the interchange would be added.
- Highway 151 (East Washington Avenue) interchange: a significant reconfiguration and expansion.
- High Crossing Boulevard partial interchange: generally maintain the existing configuration.
- Badger Interchange (where I-94 meets I-39/90): a significant reconfiguration that would make the interchange taller, but the footprint slightly less expansive.

The Highway 151 (East Washington Avenue) interchange would be significantly different. WisDOT and the City's preferred configuration is a conventional diamond interchange with traffic signals along East Washington Avenue. This configuration does not have any "free flow" on and off ramps that currently exist. Free flow ramps contribute to higher traffic speeds on East Washington Avenue. Reconfiguration of the interchange will help transition the eastern end of East Washington Avenue from a higher speed highway to a City street. Lower speeds will be safer for pedestrians and bicyclists traveling to destinations in





this area, including the BRT stations. WisDOT has also incorporated a shareduse path along the north side of the proposed interchange to connect the east and west sides of the Interstate.

Potential Hoepker Interchange

A potential new interchange is being considered at Hoepker Road. An interchange at this location would improve access to regional medical facilities located in the American Center such as UW Health's East Madison Hospital and Eastpark Medical Center. It would also benefit employers such as American Family Insurance and Alliant Energy while potentially facilitating further development on underutilized sites in the American Center.

An interchange would also serve planned residential development to the northeast of a potential interchange. The City has received residential development proposals and DeForest Area School District has purchased a property for a future school site. This area will experience increased airport noise based on the Airport's latest sound estimates.

Beyond these two areas, future City of Madison development in the larger interchange area is somewhat limited by existing development, the Airport and Airport height restrictions, the City's future municipal boundary with Sun Prairie, and environmental areas associated with Cherokee Marsh, Token Creek, and Starkweather Creek.

WisDOT generally supports a Hoepker interchange because it would reduce traffic on Highway 51 north of the Airport. It would also reduce traffic traveling through the Interstate/Highway 151 interchange. The City of Sun Prairie supports a Hoepker interchange as it would provide a connection between the Interstate and west side of the community, including the Prairie Lakes commercial area and adjacent residential areas.

Hoepker Road to the east of the Interstate, and Portage Road in this area, are currently two-lane, rural roads. These roads would need improvements to accommodate increased traffic from an interchange. Preliminary WisDOT traffic projections for 2050 indicate an interchange would increase future Hoepker Road traffic levels east of the Interstate by about 5,000 vehicles per day. Projected 2050 traffic levels on Portage Road, between Hoepker Road and Hanson Road/Eastpark Boulevard, would increase by about 2,000 vehicles per day.

WisDOT may require local municipalities to pay for a portion of the interchange's cost. Madison's interchange assessment policy requires area property owners to pay for a portion of the cost based on how much traffic their land use generates. Several Dane County municipalities have funded new highway interchanges through tax increment financing, where the tax revenue generated from increased development covers the cost.

Prioritized Non-Motor Vehicle Crossings

To increase pedestrian and bicycle connectivity across the Interstate, the City has emphasized and prioritized crossings that should be added as part of this Interstate project. The highest priority crossings within the Northeast Area are along the rail line north of Lien Road, along East Washington Avenue/Highway 151, and between Hayes Road/DiLoreto Drive and Eastpark Boulevard. Potential pedestrian and bicycle crossings near Anderson and Hoepker Roads are slightly lower priorities. Like interchanges, WisDOT may require the City to pay for a portion of the cost.

The City also requests that each street crossing of the Interstate include sidewalk and bicycle accommodations on both sides of the street. Currently, the Hanson Road crossing is only the crossing that has full accommodations.

Sound Barriers

Over time, sound walls have been constructed along portions of the Interstate and Beltline as part of expansions of these highways. WisDOT is evaluating areas along the Interstate that do not have sound barriers, but have higher numbers of residential units and other noise-sensitive land uses such as parks, to determine whether they qualify. In areas that qualify, property owners and residents vote to determine whether a sound barrier is ultimately constructed for that segment of the Interstate. Sound barriers are typically up to 24 feet tall and reduce sound for areas within a few hundred feet of the wall.

Actions

Road Connectivity and Traffic Circulation

- 1. Officially Map future public streets as shown on the Street Network Map and reserve sufficient public right-of-way to implement the planned street network.
- 2. Consider short and long-term improvements to Fair Oaks Avenue, Portage Road, Thompson Drive and other wide neighborhood streets that reduce unneeded street width to slow vehicular traffic, allow for tree plantings, improve bike, and pedestrian experience and reduce impervious surface. Short-term improvements could include bump-outs to shorten crossing distances and to allow for tree plantings, while long-term reconstructions could reflect Complete Green Street typologies.
- Remove the street segments as shown on the Street Network Map and transition underutilized or unutilized roads or right-of-way into developed areas that have buildings that are close to the street and easily accessible to pedestrians.

Pedestrian and Bicycle Network

- 4. Add sidewalks and other connections shown on the Pedestrian Network Map to improve connectivity and safety. Seek grants or other funding sources to add Priority Connections on high traffic corridors, near schools, or other primary pedestrian routes.
- 5. Add pedestrian and bicycle improvements at the intersections shown on the Pedestrian Network map to improve safety. Highest priority crossings are those on Highway 51 and East Washington Avenue.
- 6. Implement the bicycle network connections shown on the Bicycle Network Map to improve connectivity and safety.
- 7. Update the Complete Green Streets Guide's Street Type Map to incorporate changes to the street types to ensure that streets are designed according to the existing and planned surrounding context.

Partnerships

- A. Work with WisDOT to ensure changes to the Highway 51 corridor aligns with the City's vision for land use and transportation as discussed in this section:
 - i. At East Washington Avenue, the City opposes the closures of Rowland Avenue and MacArthur Road.
 - ii. At Lexington and Commercial Avenues, the City recommends the threelegged intersection.
 - iii. At Highway 30, the City recommends the conventional interchange.
 - iv. At Anderson Street, ensure the intersection design supports comfortable and safe bike and pedestrian crossings, as well as BRT access to Mendota Street.
 - v. The City recommends WisDOT include the shared-use path on the east side of Highway 51 from Kinsman Boulevard to Anderson Road. If this is not possible, WisDOT should include and fund an alternative path or onstreet protected bike lane on a parallel route such as Portage Road.
 - vi. The design should support existing and future redevelopment by limiting right-of-way expansion. If additional right-of-way is needed, the City should work with the WisDOT to acquire surplus parcel remnants and facilitate redevelopment through a request for proposal process.
- B. Work with WisDOT to ensure changes to the Interstate/East Washington Avenue intersection aligns with the City's vision for land use and transportation.
 - Reduce speeds on East Washington Avenue. Free flow ramps and lanes from the Highway 151 freeway to East Washington Avenue and the lack of signals at the interchange contribute to speeds that are not appropriate for a city street. Madison's Vision Zero program recently reduced the speed limit on this section of East Washington Avenue to a more appropriate 35 mph. Alternatives that extend an urban East Washington Avenue with signals will help reduce speeds.
 - ii. Improve connectivity for all modes of travel, including to The American Center, the High Crossing area, and across East Washington Avenue near Hayes Road. The existing interchange design isolates each quadrant, making connections difficult without a car. It is very difficult to access The American Center without using a freeway and it is almost impossible by bike or as a pedestrian. Providing an urban street connection to The American Center helps connect it to the whole of Madison.
 - iii. Reduce noise impacts to surrounding properties by minimizing elevated ramps. Generally, ramps that are not elevated have less potential to transmit highway noise.
 - iv. Transition the character of East Washington Avenue to an urban street to enhance development opportunities. This includes adding sidewalks, street trees, improved street crossings, allowing driveway access, and reducing travel lanes between Stoughton Road and Interstate 39/90/94.
 - v. Changes should enhance connectivity for Bus Rapid Transit (BRT) Service.

Neighborhoods and Housing

Comprehensive Plan Goals

This Plan aims to advance the following goals:

- Madison will be a safe and welcoming city of strong and complete neighborhoods that meet the needs of all residents.
- Madison will have a full range of quality and affordable housing opportunities throughout the city.

Madison is often recognized as a desirable place to live and experiences steady population growth. Some of the main factors driving people to move to Madison include the strong economy and job market, educational opportunities, cultural amenities, and overall high quality life. Since 2010, new housing development has not kept up with demand and has resulted in Madison having one of the lowest residential vacancy rates in the country. The persistent housing shortage increases the cost of housing and makes finding a place to live in Madison difficult. The current housing shortage is likely to continue despite ongoing efforts to increase housing availability through new construction.

The Comprehensive Plan recommends a majority of future growth to occur as infill and redevelopment. Madison is gradually losing the ability to expand outward. In addition, extending City services for development at the edge of the city is more costly. For these reasons, the City's ability to accommodate growth through redevelopment is increasingly important. Future growth should be focused along activity centers and corridors identified in the Comprehensive Plan. See the <u>Growth Priority Areas map</u>. One such location is the East Towne Mall area, which is roughly 300 acres of parking and single-story commercial buildings. This activity center alone could accommodate approximately 12,000 new housing units over time if it were redeveloped following Plan recommendations. Redevelopment on underutilized parking areas or commercial sites also provides a benefit of not displacing residents.

Affordability

The need for housing that is affordable to low- and moderate-income house-holds was one of the most common issues identified by stakeholders during the planning process. The Northeast Area is relatively affordable compared to many parts of Madison. Of the twelve Area Plan geographies, the Northeast Area has the lowest average value for owner-occupied houses and rental houses. The median rent was \$1,013 per month compared to Madison's median rent of \$1,189².

The City does not currently have the ability to regulate rents or require new developments to include affordable housing. Because of this, the City's most impactful way to increase the supply of new income- and rent-restricted affordable rental housing is through financial assistance programs offered by the Community Development Division (CDD) and Economic Development

- 1 Madison Neighborhood Indicators Project 2022
- 2 Source: American Community Survey 2021 5-year estimate

What We Heard

- 1. Increase the supply of affordable housing.
- Have a mix of housing types and sizes, including housing units large enough to support large families.
- Have complete neighborhoods with easy access to amenities such as parks, schools, and stores.
- 4. Improve the quality of existing rental housing.
- 5. Ensure sufficient support for unhoused residents.
- Community concerns about spillover effects from the current Zeier homeless shelter and the planned shelter at Bartillon Drive.

Affordable housing is housing for which occupants are paying no more than 30 percent of gross household income for housing costs, including utilities. Households with costs exceeding 30% of income are considered housing cost-burdened. The City's owner-occupant and rental development subsidy programs are primarily designed to reduce housing cost burdens for renter households with incomes at or below 60% of Dane County's median income (CMI) and for owner-occupied households at or below 80% of CMI.

% of housing by construction date Northeast Area City of Madison Since 2000 25.2% 1980-1999 22.3% 1960-1979 35.6% 24.8% 1940-1959 11.5% 13.8% Before 1940 4.8% 13.9%

Source: Census/American Community Survey

Division (EDD). The Affordable Housing Fund-Tax Credit (AHF-TC) and Tax Increment Financing (TIF) programs work in cooperation with the Wisconsin Housing and Economic Development Authority's Low-Income Housing Tax Credits as the City's primary tools for increasing the supply of affordable rental housing. The Affordable Housing Fund prioritizes areas well-served by transit as it can reduce household transportation costs. See the AHF-TC Map. Within the Northeast Area, the East Washington Avenue BRT corridor is generally identified as a preferred location for development of affordable housing. An exception is west of Fair Oaks Avenue, which has

limited eligibility resulting from airport noise impacts. CDD also provides financial support to smaller-scale, non-tax credit affordable rental developments, including housing cooperatives, and to affordable homeownership opportunities.

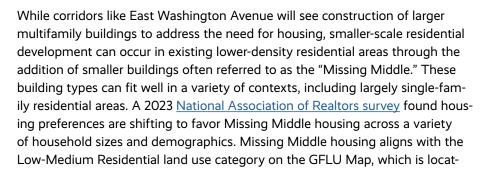
Stakeholders also discussed expanding support and resources for Madison residents who are unhoused. Some stakeholders raised concerns about the impact of the previous encampment of unsheltered individuals at Reindahl Park during the COVID-19 pandemic. During the pandemic, many people avoided congregate shelters, leading to an increase in unsheltered homelessness. After closing the temporary permissible encampments in 2021, the City implemented alternative options, including a temporary campground at Dairy Drive, a short-term hotel program, and an improved congregate shelter. In addition, the temporary shelter at Zeier Road is providing time to plan and construct a permanent shelter on Bartillon Drive. There is not a simple solution to end the complex issue of homelessness and housing instability. Instead, there are multiple ways of housing people to meet their needs and to work toward permanent housing opportunities.



Existing Missing Middle homes in the Grandview Commons neighborhood

Mix of Housing Types

As current residents age, new residents move to Madison in large numbers, and racial and ethnic diversity continue to increase, it is important to have housing options to accommodate all stages of life and living arrangements. In addition to increasing the supply of new affordable rental housing, redevelopment and infill should include market rate apartments and homeownership opportunities, including condominiums. The Northeast Area's split between rental and owner occupancy is similar to Madison overall, but the demand for owner-occupied housing remains high. The City's CDD also provides financial support for owner-occupied development, including co-housing, condominiums, and community land trusts.





Rendering of the future homeless shelter at Bartillon Drive

ed at select areas along Portage Road, Thompson Drive, and areas near East Washington Avenue.

Complete Neighborhoods

Building housing alone does not create complete neighborhoods. Elements like public parks, schools, gathering spaces, local businesses, and access to healthy food are also needed. The Northeast Area has four full-service grocery stores and several smaller convenience stores, specialty stores and ethnic grocers. However, the location of those stores can create barriers for residents without cars, especially those living in the Sandburg area. For more information, see the City's Food Access Improvement Areas map.

Schools are also catalysts for establishing strong neighborhoods, as children's activities, daily social interactions, and community projects create community connections. The Northeast Area is primarily within the Madison Metropolitan School District (MMSD). This includes the Hawthorne and Sandburg Elementary attendance areas, which area affiliated with Sherman and O'Keeffe Middle

cv Woods Dane County Regional Airport £151}

The City of Madison Community
Development Division's Housing
Development and Financing Programs provide loans to for-profit
and non-profit housing developers
for the construction and rehabilitation of income- and rent-restricted
affordable rental and owner-occupied housing.

Complete neighborhoods are neighborhoods that include a range of housing types and costs, neighborhood-serving businesses, stores, services, schools, and places of worship. These elements are ideally accessible by foot, bicycle, or transit through a network of well-connected streets and blocks, usable public spaces, and a system of connected parks, paths, and greenways.

Northeast Area Plan Elementary School Attendance Areas

Elementary Attendance Area

Hawthorne Elementary

Kennedy Elementary

Sandburg Elementary

Schenk Elementary

Windsor Elementary

School

Schools and East High School. The Schenk Elementary and Kennedy Elementary attendance areas are affiliated with Whitehorse Middle School and LaFollette High School.

The northern half of the Northeast Area is in DeForest Area School District's (DASD) Windsor Elementary attendance area. The area east of Zeier Road falls within the Sun Prairie Area School District's (SPASD) Meadow View Elementary attendance area. Both DASD and SPASD own potential elementary school sites just outside the Northeast Area that would likely serve Northeast Area residents. DASD owns a site near the intersection of Portage Road and Hoepker Road. SPASD owns a site at the intersection of Felland Road and Autumn Lake Parkway.

Actions

- 1. Incentivize affordable housing, particularly on the East Washington Avenue and BRT corridors, through the Affordable Housing Fund, Land Banking initiative, and Tax Increment Financing.
- 2. For developments receiving City funding, incentivize a wider mix of rental unit sizes, including those large enough to support larger and multigenerational families (3-4+ bedrooms).
- 3. Consider designating a neighborhood in the Northeast Area as a targeted area for the Rental Rehab Loan Program to further incentivize improvements to aging rental housing properties in lower-income communities.
- 4. Invest in food retail through funding and technical assistance to store operators through the Healthy Retail Access Program and SEED Grants, particularly neighborhoods north of East Washington Avenue which are identified as Food Access Improvement Areas (see Food Access Improvement Areas map).

Partnerships

- A. Establish a community advisory board with representatives from the homeless shelter at Bartillon Drive, local residents, City and County staff, businesses, and community organizations to provide ongoing feedback and guidance for shelter operations and the shelter's impact on the neighborhood.
- B. Create a dedicated communication channel such as an email address or a hotline for residents/businesses to report issues or concerns related to operations at the homeless shelter at Bartillon Drive.
- C. Pursue additional partnerships with Dane County Human Services and nonprofits to ensure renters in the Northeast Area have access to support needed to thrive, including onsite support services. This may include exploring an increase in funding for Dane County's Joining Forces for Families program to expand services available to the Northeast Area.

Economy and Opportunity

Comprehensive Plan Goals

This Plan aims to advance the following goals:

- Madison will have a growing, diversified economy that offers opportunity for businesses and residents to prosper.
- Madison will have equitable education and advancement opportunities that meet the needs of each resident.

The Northeast Area is a pivotal employment hub, supporting 1,000 businesses and other entities with approximately 20,000 jobs¹. Over 30 entities have more than 100 employees. While employment growth in the Northeast Area is uncertain, the Wisconsin Department of Workforce Development projects an

11.2% increase in employment in and around Dane County between 2020 and 2030.

Despite its economic significance, some areas within the planning area face significant economic challenges. Over 25% of Sandburg and approximately 20% of Burke Heights and Mayfair Park residents have incomes below federal poverty thresholds², emphasizing the need for targeted economic intervention. Burke Heights has one of the lowest median incomes in the city, at slightly over \$39,000³.

Industrial Development

Industrial land accounts for 14% of the Northeast Area. The Center for Industry and Commerce and the Interstate Commerce Park, both industrial hubs, are situated on the east side of Highway 51. The area northwest of Hoepker Road and Highway 51 is within the Town of Burke and currently lacks access to water and sewer services. According to the Town of Burke Intergovernmental

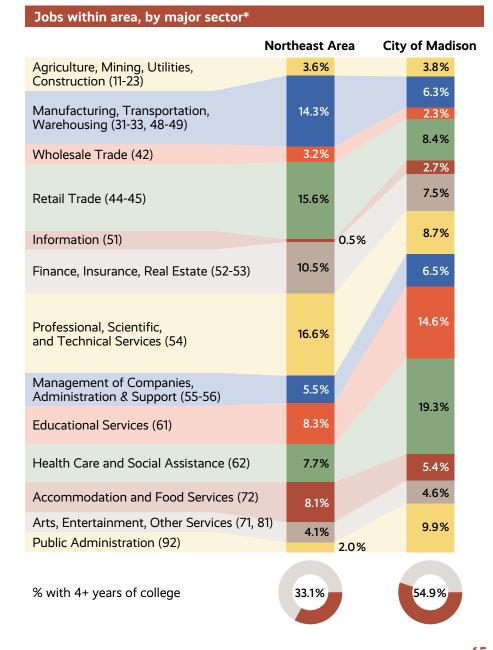
1 Source: OnTheMap 2021 data

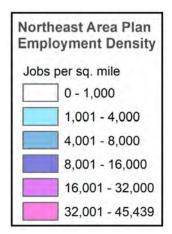
2 Source: Census/American Community

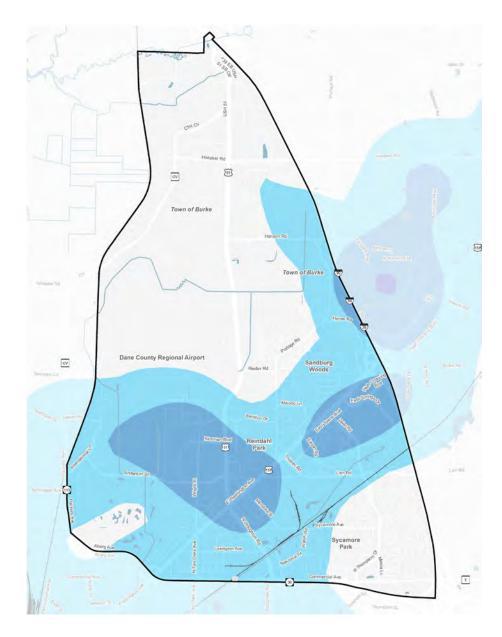
3 Source: Neighborhood Indicators Project

What we Heard

- Promote local business growth, emphasizing the significance of fostering local entrepreneurship and neighborhood-serving activities.
- 2. Improve access to essential services like grocery stores and childcare to support families.







Agreement, the Village of DeForest is responsible for providing water service to the area, but the area will become part of Madison in 2036. This arrangement complicates the development process and requires coordination with multiple municipalities for utilities and approvals. See the Effective Government chapter for more information about intergovernmental cooperation.

Service and Retail Sector

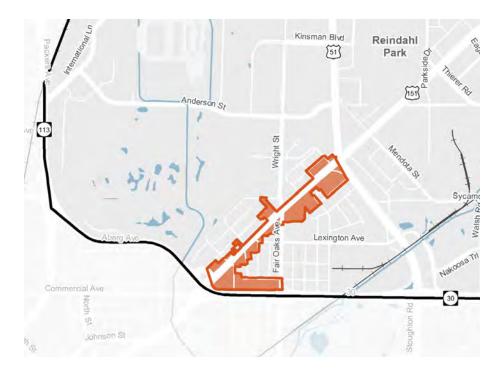
Between 2016 and 2021, service and retail employment in the Northeast Area declined by over 1,400 jobs⁴. The decline is likely due to a number of factors, such as online shopping, retail growth in neighboring Sun Prairie, and lasting impacts from the COVID-19 pandemic. Several large vacant spaces are in and around the East Towne area, including the former Boston Store and Sears stores. While these spaces could be repurposed, they could also be redeveloped into large amounts of housing, and new residents would support existing and future businesses.

⁴ Source: OnTheMap 2021 data

Local Business Survey

As part of the planning process, the City's Office of Business Resources conducted a business survey to understand perspectives within the area. With the assistance of the Northeast Area Community Navigators, responses from a total of 30 businesses indicated the following:

- **Customer Base and Transportation:** Most customers come from outside the planning area by driving or taking transit.
- Parking as a Vital Resource: With most customers driving, businesses heavily depend on parking facilities for their operations.
- Workforce Challenges: Over 55% of respondents discussed the difficulty of finding and hiring qualified employees. The workforce composition reveals that only 25% are full-time employees, with 13% being seasonal. Some respondents also mentioned a shortage of employees in the trades, such as carpentry and construction.
- Location Appeal and Concerns: Some business owners appreciate the location, particularly due to ample parking space. Others discussed airport noise as a notable drawback, negatively impacting the overall business environment.
- Community Improvement Suggestions: Several businesses identified areas of improvement, emphasizing the need for enhanced bus service and bike paths, which can help with hiring and retaining employees.
- Challenges Linked to Homelessness and Crime: Some indicated the rise in homelessness and crime rates in the area is adversely affecting their businesses.
- Lower incomes in the Area: Some respondents suggested that the limited discretionary income could potentially impact local businesses. This implies that businesses might benefit from a diverse mix of income levels in the area.



Northeast Area Plan Tax Increment District (TID) Tax Increment District #52

Actions

- 1. Establish a Greater East Towne Area Implementation Team to assist its redevelopment while also addressing vacant business spaces and parking lots.
- 2. Promote awareness and continue implementation of small business development programs funded through Tax Increment District (TID) #52 and other future area TIDs. These programs include the Facade Grant Program, Building Improvement Grant Program, Small Cap TIF Program, and Commercial Ownership Assistance Program.
- 3. Utilize Land Banking program to buy underutilized properties to facilitate redevelopment that includes employers.
- 4. Concentrate City resources on enhancing the business area along the International Lane corridor south the airport. Optimize available commercial spaces to their full potential to attract investment and foster economic growth.
- 5. Actively promote the Foreign Trade Zone adjacent to Dane County Regional Airport to attract businesses that offer stable employment opportunities. Efforts should focus on attracting more businesses to create employment opportunities for local residents.

Partnerships

- A. Address the childcare needs of working families by collaborating with local childcare providers and community organizations to enhance access to quality childcare services.
 - Broaden the eligibility of Madison College's childcare facility and bring in providers such as Reach Dane and others to commercial spaces of new mixed-use buildings such as the former Bimbo bakery site.
 - ii. Assist with creation of childcare cooperatives and in-home childcare and encourage employers in the area to provide on-site childcare.
- B. Explore creating a program similar to the Madison Public Market's "Market Ready" program to help new businesses locate within commercial space in the Plan area:
 - i. Explore gap assistance to help businesses displaced by redevelopment afford ground-floor space in new mixed-use buildings.
 - ii. Conduct an equity-based market study of the area to further explore strategies for strengthening the economy, with an emphasis on ensuring that underrepresented populations are included in redevelopment, ownership, entrepreneurship, and employment opportunities.
 - iii. Incorporate networking, mentorship, and collaboration opportunities as part of business planning.
 - iv. Work with local commercial brokers and property owners to connect small businesses to available space as they "graduate" from the program.
- C. Support creation of an East Washington Avenue Business Association to connect business owners with each other and with the City to facilitate economic growth and development.
- D. Explore creating a business or neighborhood improvement district along East Washington Avenue to help make public spaces more welcoming to residents, maintain amenities, facilitate events, and promote the area to customers.

Culture and Character

Comprehensive Plan Goals

This Plan aims to advance the following goals:

- Madison will be a vibrant and creative city that values and builds upon its cultural and historic assets.
- Madison will have a unique character and strong sense of place in its neighborhoods and the city as a whole.

Through an examination of community activities, architectural elements, and the impact of local organizations, this Plan aims to provide recommendations that contribute to the culture and character of the Northeast Area. It emphasizes the vision of a central urban plaza that celebrates diverse cultures within the existing East Towne Mall area, proposes changes to Urban Design District (UDD) 5, assigns new potential design guidelines to three additional sub-areas, and explores opportunities for inclusive art and entertainment to enhance vibrancy.

The Greater East Towne Area Plan outlined a vision for the transformation of areas around and including the mall, setting the stage for dynamic growth and revitalization. This Plan recommends expanding the geographical boundaries of UDD 5 and revising UDD 5's recommendations to achieve a vibrant and inclusive urban environment.

The city's population is increasingly diverse. Enhancing the distinctive qualities that define the Northeast Area will ensure it remains inclusive and welcoming to individuals of every race, ethnicity, gender, age, and ability. The Comprehensive Plan advocates for dynamic and inviting spaces, preservation of historic sites, the creation of secure and affirming community spaces, the integration of public art, and the facilitation of opportunities for learning, creativity, collaboration, and arts enjoyment.

The Northeast Area has several of Madison's eastern gateways, with two pivotal intersections: East Washington Avenue at Interstate 39/90/94, and Highway 51 and Interstate 39/90/94. These intersections are the initial impression of the city, underscoring the importance of recommendations that enhance the area's distinct character. There is significant potential to enhance the Northeast Area's walkability and vibrancy, primarily within and around the Highway 51 corridor, East Washington Avenue, Commercial Avenue, and East Towne Mall.

What we Heard

- Create welcoming and safe neighborhoods in the Northeast area by making the streets look nice, adding community art, using signs in different languages, and asking residents for their ideas in the planning process.
- 2. Design a culturally-affirming central green and open space that accommodates diverse public activities and events.
- 3. Explore opportunities for the development of inclusive art concepts and cultural entertainment to enhance vibrancy throughout the area.

Northeast Area Plan - Draft 1189



Amund Reindahl's barn, located in Reindahl park

Historical Resources

There are no City-designated landmarks or properties listed in the National Register of Historic Places in the Northeast Area. However, staff has identified several sites with architectural or historic interest which may be eligible for designation (see appendix: Historical Resource Survey). There are also several previously identified archaeological sites, several of which are human burial sites. These sites may require additional review and consultation with the Ho-Chunk Nation and Office of the State Archaeologist. The archaeological sites could also be eligible for local or federal historic designation. Properties listed in the National Register are potentially eligible for preservation tax credits for work on buildings, and burial sites are eligible for a property tax exemption.

Actions

- 1. Revise and update Urban Design District (UDD) 5 to require new development to be pedestrian-oriented, including comfortable spaces and visually appealing elements along East Washington Avenue. See proposed UDD 5 revisions at the end of this chapter.
- 2. Support a variety of cultural and entertainment venues as the area redevelops. This should include spaces that vary in size and are well suited for a variety of visual and performing arts to make them easily accessible to area residents.
- 3. Add art and placemaking elements through expanded use of the utility box wrap program.
- 4. Provide residents with information and assistance about how to create and host festivals or community events within Reindahl and Sycamore Parks.
- 5. Add benches along Starkweather Creek.
- 6. If East Towne Mall redevelops, add a prominent art piece within the planned urban plaza and connect the new neighborhood with Sycamore Park through arts programs such as Art in Public Places and Poetry in Sidewalks.
- 7. Inform property owners who can officially document sites identified during historic resource surveys (see Historic Resources Map) with historical value through the National Register process.





Utility box wrap at Regent St

Former Poet Laureate, Angela Trudell Vasquez is kneeling in front of her poem, "The Other America Tour...," at the sidewalk along Atwood Ave.

Partnerships

- A. Provide a placemaking best practices guide to businesses and organizations in the area to provide an alternative use of vacant sites and underutilized parking lots.
- B. Collaborate with local businesses and community organizations to beautify the area by addressing unsightly businesses and wasted space like parking lots, areas with excessive amounts of litter, or buildings that are poorly maintained. This may involve current City initiatives like the Facade Improvement Program, Neighborhood Grant Program, and Arts grants.
- C. Improve the Northeast Area's primary gateways at Interstate 39/90/04 and Highway 51 and Interstate 39/90/94 and East Washington Avenue. Enhancements may include:
 - i. Facilitate the construction of higher-density buildings at these key corners to enhance the appearance and create a sense of enclosure. This approach aims to reduce vehicle speed, signaling to drivers that they have transitioned from a highway to city streets.
 - ii. Where possible, introduce visually appealing landscaping and streetscaping elements to establish a more inviting atmosphere at these entrances.
 - iii. Incorporate public art installations or sculptures to infuse the gateways with cultural and artistic elements. This aims to cultivate a distinctive identity and foster community pride.
- D. Invite residents of all backgrounds to participate in a community-based process to design public gathering spaces and streetscapes within or around the existing East Towne Mall area and Commercial Core so this commercial area redevelops into more of a neighborhood-serving place, where people will be comfortable expressing their culture and feel that they belong.
- E. Consider providing art mini-grants and how-to guides to local businesses for permanent exhibition materials, such as art hanging systems and lighting, to generate more opportunities for local artists to share their work with the public.
- F. Work with Dane County and residents to identify community-oriented interim uses for the greenspace in the Wright Street triangle, such as picnic areas, temporary outdoor fitness equipment, and public art installations.
- G. Enhance the aesthetics of vacant facades by installing murals that resonate with the community, such as the telecommunications building at Rowland Avenue and East Washington Avenue.
- H. Explore opportunities for the temporary use of vacant or transitional spaces to support emerging artists while adding to the vibrancy of the area. This may include pop-up galleries, artist-in residencies, and murals.
- I. Provide spaces where people are comfortable expressing their culture and feel that they are part of a community. This applies to streets, sidewalks, paths, transit stops, and other public amenities as well as private development. It may include physical amenities such as multilingual signage, permanent or temporary art, poetry in sidewalks, and murals.



Madison Mural at a parking lot at Monroe St and Commonwealth Ave.

Urban Design District 5

Urban design is also an important contributor to culture and character. Madison's Urban Design Districts (UDDs) contribute to the aesthetics of the built environment in addition to some aspects of urban design regulated in the zoning code. The City's UDDs generally ensure development contributes to creating a high quality, well-designed built environment within certain areas of the city. There are eight UDDs that are primarily located along the city's major transportation corridors. Each Urban Design District is unique and establishes mandatory requirements and advisory guidelines for new development, buildings, remodeling, or additions to existing buildings. Some districts are more detailed than others. While the requirements and guidelines are different in each district, generally they address building design (height, setbacks, and stepbacks), design and quality of exterior materials and architectural detailing, lighting, signage, the design of private open space, landscaping, and screening.

Portions of UDD 5 are within the Northeast Planning Area. It currently runs along East Washington Avenue, generally from Interstate I-90/94/39 to Sixth Street. This Plan recommends revisions to UDD 5 which are anticipated to be further studied and refined as part of a separate, citywide UDD review process. This Plan recommends expanding the Urban Design District and establishing different recommendations for various sub-areas.

1. Revise and update Urban Design District (UDD) 5 to require that development is pedestrian-oriented, including comfortable spaces and visually appealing elements along East Washington Avenue, as well as other public streets and views. As noted in this chapter, revisions to the Urban Design District should create design standards and guidelines that recognize unique planning areas, including the recommended design sub-areas noted in this chapter.



- 2. Extend UDD 5 to the Interstate and explore expanding it to include key sites south of East Washington Avenue, the East Towne Mall area, and Highway 51.
- Incorporate Transit-Oriented Development (TOD) overlay district features to UDD 5 requirements and guidelines, such as minimum building heights of two stories.
- 4. Separate UDD 5 by character into three distinct sub-areas to better guide development along the East Washington Avenue corridor, as illustrated in the Proposed Urban Design District (UDD) 5 Changes Map.
- 5. While the western portion of UDD 5 is outside the Northeast Area, given its distinct character, this area should also be considered as an individual sub-area.

The Urban Design District Changes Map recommends various boundary adjustments to UDD 5 to reflect current property lines and include areas expected to see a high level of construction activity.

UDD 5 Design Guidelines - Recommended Updates

- Update the list of approved building materials and design guidelines to reflect the current standards of the City, particularly focusing on the Urban Design Commission's "purpose and intent" to encourage and promote high-quality design in new buildings, including guidance that:
 - a. Buildings should be designed so uses, entrances, and design elements provide amenities to enhance the visual and pedestrian character of the street.
 - b. Street facing facades that lack architectural detail, variety, and windows are discouraged.
 - c. Use changes in building plane to create porches, balconies, roof terraces, entrances, and other active uses are encouraged for residential units/buildings.
 - d. Larger buildings should incorporate facade modulation and changes in plane.
 - e. Porches and balconies should be well-integrated and provide usable open space for tenants.
 - f. If material changes are proposed, they should generally occur at inside corners or be delineated by a specific transitional detail.
 - g. Encourage developers to incorporate artists into the design team to integrate works of art into their projects.
- 2. Site layout and building orientation guidelines at Commercial Core areas are encouraged to have outdoor patios, outdoor dining, awnings, and display windows.



The Madison Youth Arts building on East Mifflin Street exemplifies effective use of facade articulation, offering diverse design elements that enhance visual appeal and create an inviting atmosphere for pedestrians.



Example of Missing Middle housing



Pedestrian and bike friendly sidewalk with green buffer in Saint Paul, Minnesota.

Potential UDD 5 Sub-Areas

UDD 5. Sub-Area 1 Design-Related Considerations: East Washington Avenue Corridor – Aberg Avenue to Thierer Road

This sub-area is characterized by neighborhood retail and lower-scale development. Development in this area should support neighborhood-serving commercial uses to create complete neighborhoods and provide walkable amenities to residents in the area.

- 3. Support "Missing Middle" residential development such as townhomes, and preserve naturally-occurring affordable housing.
- 4. Proximity to institutional and educational uses should be considered in the development of housing and connections to open spaces.

UDD 5, Sub-Area 2 Design-Related Considerations: East Washington Avenue Corridor – Thierer Road to Interstate

This sub-area is characterized by its regional connectivity. Development in this area is recommended for high-intensity, mixed-use development that serves and supports the larger community and region.

- 5. Support Community Mixed-Use development to create complete neighborhoods and provide walkable amenities to residents in the area.
- 6. Given its location at a major highway interchange, sub-area 2 will remain a gateway to Madison. As such, guidelines and requirements should promote a higher level of design aesthetic (high-quality materials, incorporate visual interest, articulation, and modulation in building design, etc.) and encourage appropriate transitions to less intensive development areas.

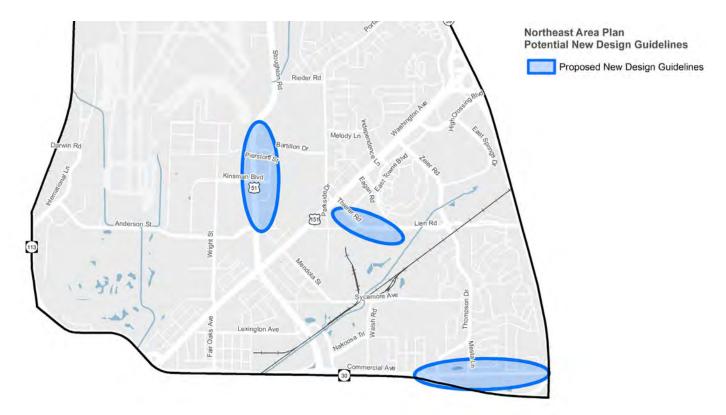
UDD 5, Sub-Area 3 Design-Related Considerations: Greater East Towne Area

This sub-area is intended to create a more multi-modal, transit-oriented development pattern that includes new high-density uses accessed from and oriented to a new local public street network. The area should include a high degree of pedestrian amenities intended to support successful mixed-use redevelopment.

- 7. Development in or near the existing East Towne Mall should consider creating an urban plaza and pedestrian features through and to open spaces when locating building entrances, enhanced sidewalk connections, and pedestrian-orientated uses.
- 8. Construct earth berms along Interstate 39/90/94 or plant dense vegetation such as trees, shrubs, and bushes that serve as both a visual and sound barrier. The irregular surface helps to deflect and absorb sound waves.
- 9. Whenever possible, integrate noise reduction features into the architectural design of residential buildings facing the Interstate and Highway 30. This may include the strategic placement of windows, use of double-glazed glass, and soundproofing materials in walls and roofs.

Additional Design Guidelines

As shown in the Potential New Design Guidelines Map, three areas were identified as future destinations that could benefit from better building and public space design. They also aim to make it easier to get around, add more green-



ery, and include features that make spaces feel welcoming and comfortable for people.

These new guidelines are intended to create more attractive and engaging public areas through building redevelopments and street enhancements. The following guidelines will inform proposed infrastructure projects and proposed private developments.

Highway 51 Proposed Design Guidelines

• Incorporate pedestrian-friendly elements such as well-designed lighting fixtures and wider sidewalks to enhance the pedestrian experience.



Note: This image does not intend to illustrate changes of the intersection of Kinsman Blvd and Stoughton Rd. These suggested changes within private land will only occur if property owners decide to redevelop.

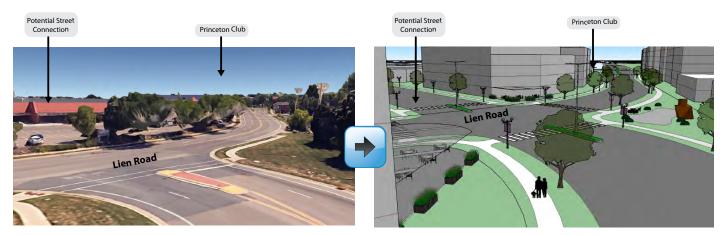
- Relocate utilities underground to eliminate visual clutter, enhance the streetscape, and contribute to a cleaner, more modern aesthetic. This includes transitioning electrical poles from overhead to underground.
- Install clear and well-designed wayfinding signage to guide pedestrians, cyclists, and drivers efficiently. This can include directional signs, informational kiosks, and maps at key points.



- Incorporate an art piece and/or water feature at the intersection of Lien Road and Thierer Road. The artwork should fit with the surroundings, avoiding obstruction of visibility. The chosen piece should serve as a reflection of the cultural and natural character of the area, enhancing the overall aesthetic appeal and local identity.
- Encourage outdoor dining facing the corridor to increase foot traffic for first floor businesses.
- Provide incentives to property owners to replace wire fences with natural barriers like trees, bushes, or water features.
- Build a more visible pedestrian crossing at the Lien Road and Thierer Road intersection, as well as a mid-block pedestrian crossing between Lien Road and Eagan Road.



Bell Street Park, in Seattle, features a well-defined pedestrian crossing that includes greenery and inviting lighting for pedestrians and bicyclists.



Note: These suggested changes within private land will only occur if property owners decide to redevelop.

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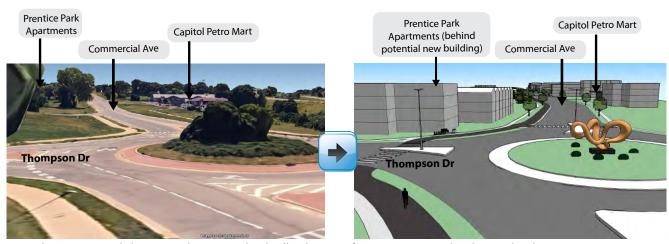
Commercial Avenue Proposed Design Guidelines

- Maximize the use of existing right-of-way to provide bike/pedestrian amenities for adjacent commercial and residential sites.
- Buildings should have storefronts and entrances that are welcoming to pedestrians, such as attractive landscape, inviting signage, well-maintained facades, or outdoor seating areas along Commercial Avenue.
- Implement universal design principles to make the street accessible to people of all abilities, such as detectable warning pads and acoustic pedestrian crossing signals.
- Build pedestrian crossings at Mesta Lane and Eagle Crest Drive for better connectivity to the Ridgewood Neighborhood.
- Consider integrating public art and installations along Commercial Avenue to add cultural and visual interest.
- Implement effective street lighting to enhance visibility and safety, especially during evening hours.

Potential Commercial Avenue Reconfiguration



Note: Redevelopment of right-of-way could also occur to the south or both sides of Commercial Avenue.



Note: These suggested changes within private land will only occur if property owners decide to redevelop.

Green and Resilient

What We Heard

- 1. Improve accessibility to and the experience within parks, open spaces, and natural areas.
- 2. Ensure future growth happens with sustainability as a consideration by appropriately managing stormwater, protecting native species, woodlands, and wetlands, and minimizing the carbon footprint of new buildings and their users.
- 3. Acknowledge climate change and prepare for more frequent and intense weather events, including both precipitation and heat.
- 4. Address past environmental contamination with solutions that protect the health of Madison residents.

Park Expansions

Specific Park Expansion Areas identifies locations were future park space is expected. **General Park Expansion Areas** show where future park space is desired, but a precise location can't be determined due to land availability, lot patterns or other factors.

Comprehensive Plan Goals

This Plan aims to advance the following goals:

- Madison will be a leader in stewardship of our land, air, and water resources.
- Madison will have a model park and open space system that preserves our significant natural features and offers spaces for recreation and bringing residents together.

Natural areas, open spaces, and parks are major features in the Northeast Area and were frequently mentioned as what makes the area special. Residents also mentioned the need to grow sustainably in the coming years, including protecting wetlands, preserving and adding trees, and avoiding growth near sensitive areas. This chapter addresses the intersection between the built environment and the natural environment, including parks and open spaces, sustainability, ecology, and stormwater management.

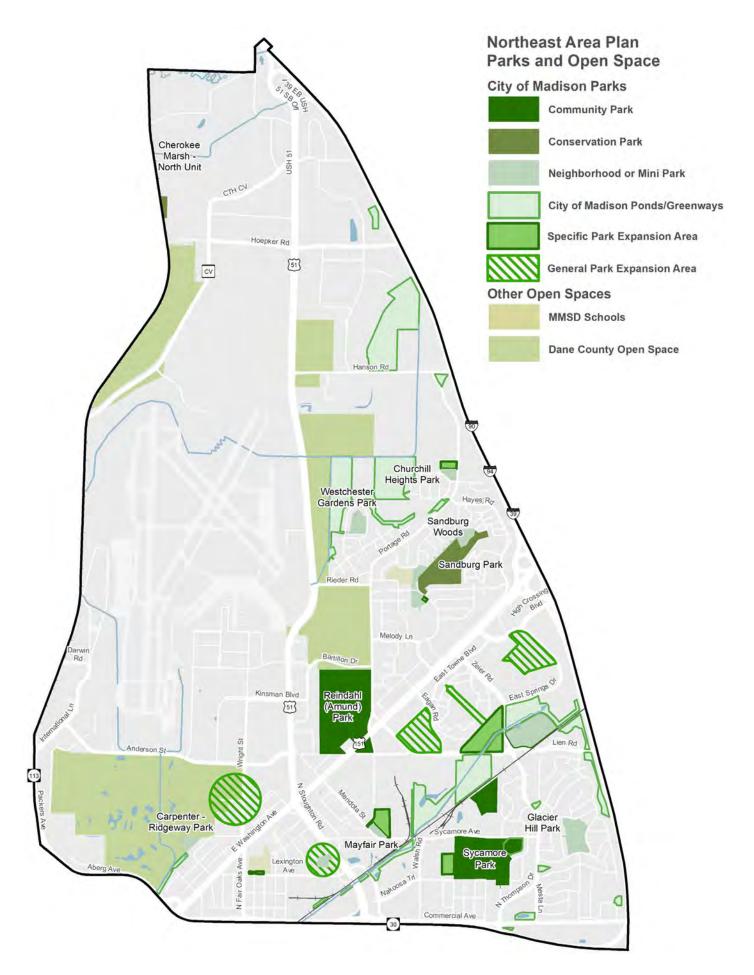
Parks and Greenways

The Northeast Area's variety of parks provide opportunities for a wide range of active and passive outdoor activities, from basketball to birdwatching. The area has 10 parks covering 246 acres that serve its nearly 15,000 residents (17 acres per 1,000 residents), and 82% of households in the Northeast Area are within a quarter mile of a park. However, there is an area north of East Washington Avenue in the Truax Neighborhood lacking in park access. New park space in this area is desired, but much of the vacant land in the area is controlled by the Dane County Regional Airport, which leaves limited options for City park acquisition. Along with addressing park-deficient areas, new park spaces will be needed to serve the recreational needs of the area's future population.

While the variety of amenities in the area's parks are appreciated by residents, continued investments will need to be made to keep pace with the growing population of the area and the changing recreational preferences of residents. For example, Reindahl Park includes the City's only full-size Cricket field and hosts the most soccer fields on the east side. These community park amenities attract residents from across the east side of Madison and beyond. While mini and neighborhood parks tend to focus on serving surrounding neighborhoods, they can also feature unique recreational opportunities that draw from a larger area. Future changes to the City's parks, such as new playgrounds, shelters, and paths are guided by Park Development Plans (for mini and neighborhood parks) and Master Plans (for community parks), which are updated approximately every 10 years.

Reindahl Park

Future changes at Reindahl Park are guided by the 2023 Reindahl Park Master Plan. The Master Plan includes the Madison Public Library's Imagination Center, which will include library and community facilities and an indoor, year-round, reservable park shelter. The Imagination Center is scheduled for



Types of Parks

A mini park is a small park (generally < 5 acres) with limited recreational amenities that serves the immediate surrounding area.

A **neighborhood park** is generally 5+ acres and serves as the recreational focus of the neighborhood, with more recreational amenities than a mini park.

A community park is usually 20+ acres that is designed to serve/attract residents from beyond the surrounding neighborhood.

A conservation park is primarily managed to preserve Madison's native landscapes, plants, and animal populations for the careful use and enjoyment of visitors.

Type of Park	# of Parks	Acres in Northeast Area
Mini Parks	3	9
Neighborhood Parks	4	41
Community Parks	2	162
Conservation Parks	1	34
Total	10	246



Reindahl Park



Sycamore Park

construction in 2025. Other elements of the Master Plan, including a new playground, expanded community gardens, and athletic facility upgrades will be implemented over the next 10-15 years as funding and resources allow.

Sycamore Park

The 2023 Sycamore Park Master Plan recommends new trails, a bicycle pump track, and improved connections to the wooded section of the park north of Sycamore Avenue. Other planned improvements include dog park enhancements, a sun shelter, and a restroom building. The adopted Master Plan does not include the recommended extension of Nakoosa Trail through the park to Jana Lane. However, the extension was reviewed and is supported by the Parks Division.

Sandburg Woods Conservation Park

This conservation park features hilly trails through mature forest on the north side of a drumlin. The variety of ecosystems within this conservation park are protected and managed in accordance with the Parks Division's Land Management Plan.

Neighborhood and Mini Parks

Alongside the Northeast Area Plan, the Parks Division completed Park Development Plans for each neighborhood and mini park in the Northeast Area. These address park-specific needs for new amenities and the potential relocation of existing park amenities. All parks will be considered for improvements to make park features more accessible for individuals with physical limitations, improve wildlife habitat, and incorporate recommendations from the 2020 Parks Division Madison Bicycle Adventure Trail Network (Mad-BAT) plan. For more information, refer to individual Park Development Plans.

- Brigham Park
- Carpenter Ridgeway Park
- Churchill Heights Park
- Glacier Hill Park
- Mayfair Park
- Sandburg Park (Note: Sandburg Park is next to but does not include Sandburg Woods Conservation Park spaces)
- Westchester Gardens Park

Future Park Spaces

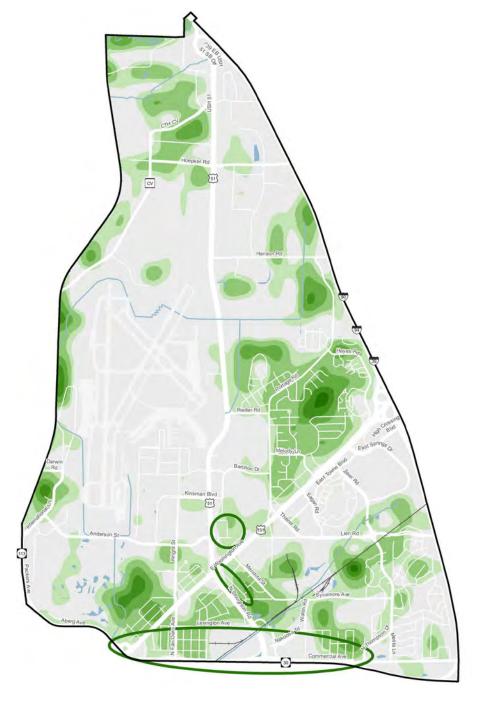
Several new parks are planned in the Northeast Area, particularly around the East Towne Mall area. As these future park spaces are acquired, the park development or master planning processes should be community-based and engage a diverse group of community members to establish spaces that are welcoming and culturally relevant for all city residents. These new parks should incorporate spaces and features that accommodate special events and programing like concerts, festivals, and farmers markets. Refer to Culture and Character actions and partnerships for additional information.

Trees and Canopy Coverage

The Northeast Area's 11% tree canopy coverage is one of the lower coverage rates among the City's 12 plan areas, and about half the level of Madison as a whole. This lack of coverage is due to the airport, large commercial areas, areas within parks such as athletic fields and reclaimed landfills, and wetland areas where trees don't grow. Regardless, it is a priority to add trees to both public and private land. There are gaps in street tree coverage, mostly in the neighborhoods around Fair Oaks Avenue, but also on streets which abut Highway 30. Trees in those locations could establish a buffer between the highway and nearby residential areas. Redevelopment of large commercial properties could lead to increased tree plantings and total canopy coverage, since new development regulations include higher standards for the number of trees that must be provided on a site.



Adequate tree coverage along West Washington Avenue







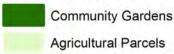
Example of a Rain Garden

Stormwater Management and Flood Reduction

Stormwater management and flooding saw increased attention from many community members after a 2018 rain event where 12 inches of rain fell over a 24-hour period in portions of Madison. While the worst impacts occurred on the west side, the intersection of Highway 51 and Lexington Avenue in the Northeast area was impassable for several hours. This area continues to flood on occasion. The Engineering Division's Starkweather Creek Watershed Study covers nearly all of the Northeast Area and will recommend improvements to minimize flooding from more frequent, large rain events.



Northeast Area Plan **Urban Agriculture**



Water Quality

Part of growing sustainably is addressing contamination from past developments and practices. Redevelopment projects often include environmental cleanups that address contamination from past developments like gas stations, laundromats, or industrial uses. Redevelopment is often the best way to clean or remediate sites, since it creates financial motivation to do so. The highest profile contamination issue in the Northeast Area is Per- and polyfluoroalkyl substances (commonly referred to as PFAS) related to past use of firefighting foam at Dane County Regional Airport. That contamination led to the shutdown of Madison Water Utility's Well 15, which along with Well 29, provides water for the Northeast Area. Well 15 remains offline while a PFAS treatment system is under development. Improvements at the well facility, including installation of PFAS treatment equipment, began in 2024. Madison Water Utility will provide regular updates on construction progress, water testing, and operational status on the Well 15 project website.



Madison Water Utility's Well 15

Urban Agriculture

The Northeast Area has several different types of urban agriculture. Community garden plots are the smallest scale and are located in Reindahl Park and at the Community Development Authority's Truax Housing property. While on public land, the plots are managed by Rooted (Community Groundworks). The Parks Division often receives requests for additional community garden spaces, and these are forwarded to Rooted. Once Rooted determines there is enough demand for new garden spaces, it works with the Parks Division to identify appropriate locations. Once approved, these locations are then added to Rooted's lease with the City. Approximately eight acres of private market gardens (small scale commercial agriculture) are located west of County Highway CV at Hoepker Road. There are several large properties in the Northeast Area planted with row crops. Some are planted as an interim use with eventual development planned, while others are unlikely to develop. Dane County Regional Airport owns approximately 150 acres of land within unbuildable Runway Protection Zones that are currently rented for agriculture. While these are currently planted with row crops, it's possible they could transition to other types of agriculture, such as Community Supported Agriculture (CSA) farms or similar.



Parks and Open Space

- 1. Parkland Acquisition and Expansion: Improve access to, visibility of, and usability of City park spaces through new park development and property acquisition as shown on the Parks and Open Space Map.
- 2. Park Specific Improvements: The following recommendations address issues discussed during the Northeast Area Plan process and were not previously addressed in Parks master planning processes:
 - a. Sandburg Park
 - i. Add a path connection to Sandburg Woods from vacant property on Forest Run Road when it develops.
 - ii. Better define edges between park land and private properties on the north side of Sandburg Park.



Community garden at Leopold Park



Row crops planted in the Runway Protection Zone





Sycamore Dog Park

64

iii. If apartment communities on Portage and Hayes Roads redevelop, establish a new street which abuts the park to improve visibility, access, and uses.

b. Sycamore Park

- i. Extend Nakoosa Trail to Jana Lane, as a phased project, to provide greater visibility, access, and use of the southern portion of the park. Extending Nakoosa Trail to the east of Pulley Street will require minor park configuration changes that should be addressed as part of a future master planning process.
- ii. In the interim, consider changes to the shared-use path intersection with Nakoosa Trail to address reports of unauthorized motor vehicle use on the path.
- 3. Improve access to publicly-owned natural areas to provide passive recreation opportunities:
 - Provide public access to the wetland area north and west of Portage Road when adjacent vacant property develops.
 - b. Establish public access to the wetland area along the rail corridor south of Lien Road as part of the planned Hiestand Path.
 - c. Seek public access along Starkweather Creek between Highway 30 and Commercial Avenue via easement or property acquisition.

Trees and Canopy Coverage

- 4. Proactively evaluate identified areas on the Urban Forestry Map for potential street tree planting sites, with a particular emphasis on the Highway 30/Commercial Avenue frontage.
- Evaluate City stormwater parcels for plantings and ecological restoration consistent with the Stormwater Utility Vegetation Management Plan once adopted.

Stormwater Management and Flood Reduction:

6. As part of the Starkweather Creek Watershed Study, develop stormwater management and watershed protection recommendations. Investigate solutions to areas with chronic flooding such as at Lexington Avenue and Highway 51 and in the "triangle" area between the East Washington Avenue Frontage Road and the Commercial Avenue Service Road.

Green and Resilient Partnerships

- A. Explore opportunities with Dane County to increase use of the County-owned greenspace south of Anderson Street and west of Wright Street since this area is park-deficient.
- B. Work with neighborhood groups, cultural organizations, school classes, and other stakeholders to improve and expand the walking trails that connect Carpenter-Ridgeway Park to Starkweather Creek.
- C. Pursue tree planting options for unneeded highway right-of-way as part of Wisconsin Department of Transportation studies.

Draft - City of Madisor 1204

Effective Government

Comprehensive Plan Goals:

This Plan aims to advance the following goals:

- Madison will have efficient and reliable public utilities, facilities, and services that support all residents.
- Madison will collaborate with other governmental and nongovernmental entities to improve efficiency and achieve shared goals.

The Northeast Area's population increased by approximately 900 residents between 2010 and 2020. Much of the planning area, including areas in the Town of Burke, have been largely developed. Most of the area has the full complement of City services, such as police and fire, utilities, waste management, and snow removal. Town of Burke properties are served by a combination of Town of Burke, City of Madison, and Village of DeForest services. Madison provides fire and emergency medical services to the Town as well as water and sanitary sewer to parts of the Town.

The City will continue to provide a full range of services to neighborhoods (including former Town properties attaching to the City of Madison), while optimizing where community facilities are located to balance provision of services with the long-term operating costs and the growing and changing population of the Northeast Area. Select areas may receive water and sewer service from the Village of Deforest, as specified in the Town of Burke, Village of DeForest, City of Sun Prairie and City of Madison Cooperative Plan. However, new connections in the DeForest service area are not guaranteed after properties attach to the City of Madison. See the cooperative plan for more information.

The Reindahl Imagination Center and homeless shelter at Bartillon Drive are the latest City facilities to be planned in the Northeast Area. Additional adjustments to services will likely be needed in the future as the Northeast Area and the city continue to grow.

A further important focus on the provision of services is to provide individuals and groups, especially underrepresented populations, with the tools to effectively gain access to Madison's services and resources. The Northeast Area is well-situated to provide satellite services to residents that would otherwise need to travel to City offices located downtown. Situated between downtown and the edge of the city, the Northeast area could host City services that would otherwise require a visit downtown. Bus Rapid Transit (BRT) service through the area could provide improved access to such a satellite service facility.

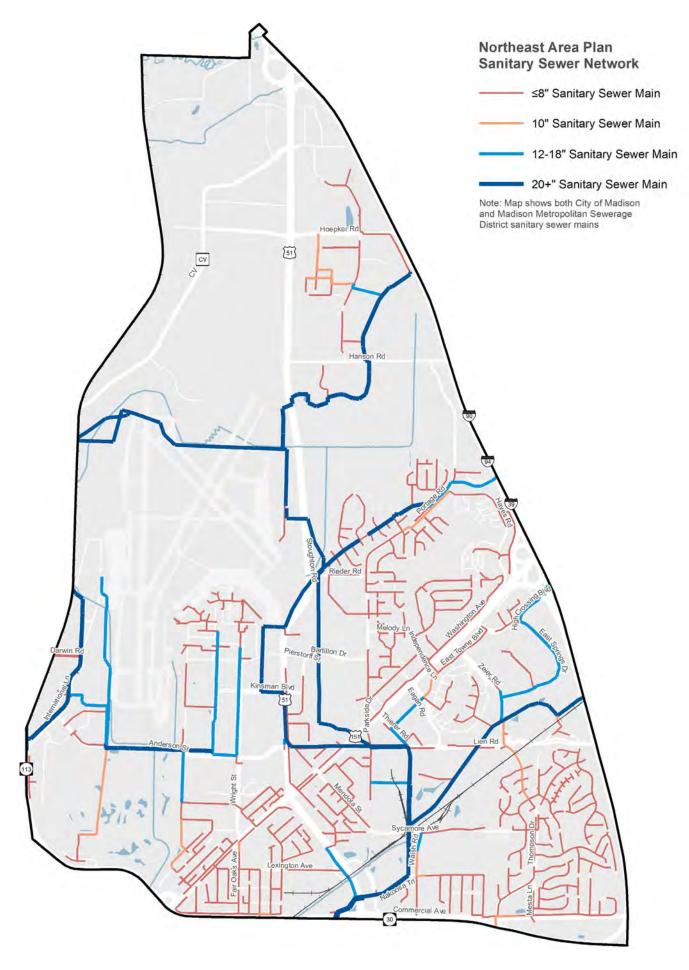
Redevelopment of lower density uses into higher density uses results in changing demands for City services and infrastructure. Adding residential uses into previously low-density or commercial areas requires a different set of services such as parks and pedestrian and bicycle facilities. Additionally, residential uses require a comparatively higher amount of water, which could result in the need to upsize water mains and sanitary sewer mains as redevelopment transitions

What We Heard

- Residents perceive public investments have lagged in the Northeast Area compared to rest of the city.
- 2. Have a smooth transition of municipal services when the final attachment of the Town of Burke occurs in 2036.
- 3. Improve maintenance of City infrastructure within the planning area.



City of Madison recycling service



low-density commercial and office space to mixed-use development or multi-family residential development (see Map: Sanitary Sewer Network). Upsizing of sanitary sewer mains is most likely needed for properties served by eight-inch diameter mains, which are generally the City's smallest diameter mains.

Maximizing the efficiency of facilities and services can best be achieved through careful coordination among City agencies and potentially with other municipalities, specifically the City of Sun Prairie, Village of DeForest, and Town of Burke.

[51] cv Town of Burke Dane County Regional Airport Bartillon De Reindahl [51]

Town of Burke, Village of DeForest, City of Sun Prairie, City of Madison Cooperative Plan

- Town of Burke dissolves 2036, properties attach to DeForest, Sun Prairie, and Madison. See Intergovernmental Boundary Agreements Map for lands attaching to Madison.
- Town properties can attach to Madison if requested by the property owner and approved by the City.
- Specific Town properties are protected from early attachment until 2036 to maintain tax base for the Town unless approved by the Town. See Intergovernmental Agreement Map.
- Madison will provide the full range of City services to Town properties attaching to the City except for properties located near Highway 51 and Interstate 39/90/94. DeForest may provide water and sanitary sewer to these properties per the agreement.
- There is a restriction on the number of housing units that can be built each year within the portion of the DeForest Area School District that is attaching to Madison.

Northeast Area Plan Madison-Burke-Sun Prairie-DeForest Cooperative Plan

Town of Burke Protected Areas

Town of Burke Parcels Attaching to Madison (Final Attachment 2036)

Actions

- 1. Explore satellite facilities for government services to accommodate customer schedules and needs, potentially at the Reindahl Imagination Center.
- 2. Conduct a review of community service and facility needs after the Reindahl Imagination Center is built and new housing is present in the planning area. If gaps are discovered, the Community Development Division should explore a service provider facility (for example, The Village on Park) where providers can rent space and offer: employment services, business development and computer skills training, preventive health services, food and essentials pantry, and fitness and recreation.
- 3. Develop a comprehensive infrastructure plan in advance of Town of Burke properties attaching in 2036. The plan should identify infrastructure needed to support continued industrial development in area north of the airport and capitalize on the City's successful partnerships with the Interstate Commerce Park area and the Center for Industry and Commerce Owners Association.

Partnerships

- A. Work with the applicable school districts to address pedestrian, bicycle, and traffic safety issues around schools when street repaving, reconstruction, or other changes to the right-of-way are being considered. Improvements may include things like signage, increasing crosswalk visibility, modifying traffic patterns, increasing nearby sidewalk connectivity, and implementing safe all-ages-and-abilities bicycle connections.
- B. Pursue changes to the residential building permit restriction in the Burke Cooperative Plan.
- C. Pursue an agreement with Village of DeForest to use existing DeForest sanitary and water infrastructure to serve properties northeast of Hoepker Road and Highway 51.
- D. Work with Federal Government, Air National Guard, Federal Aviation Administration (FAA), and Dane County Regional Airport to secure soundproofing funding for residential buildings and other noise-sensitive uses affected by higher levels of airport noise.

Health and Safety

Comprehensive Plan Goals:

This Plan intends to advance the following goals:

- Madison will be a place where all residents have equitable health outcomes.
- Madison will be a place where residents and visitors are safe at home and feel welcome in the community.

The Comprehensive Plan promotes access to comprehensive mental and physical health services, healthy and secure living environments, and the implementation of public health strategies to prevent violence.

The recommendations in this chapter aim to enhance safety and mitigate noise and air quality issues in the Northeast Area, ultimately improving residents' quality of life and fostering a more vibrant and sustainable community. To implement these recommendations, it is essential that the City, specifically Public Health Madison & Dane County and the Madison Police Department, continue to engage with the local community and other stakeholders to ensure a collaborative and effective approach.

Health

According to the national <u>City Health Dashboard</u>, health conditions in the Northeast area in 2020 were comparatively poorer than the city as a whole. Individuals experience obesity at a higher rate than the citywide average. Northeast Area residents had lower levels of physical activity compared to the city as a whole. Levels of particulate matter (air pollution) in the northern portion of the planning area are some of the highest in the city.

Safety

Safety concerns were frequently expressed by Northeast Area residents. According to the <u>Neighborhood Indicators Project</u> 2022 Edition, the Northeast Area had the highest number of verified criminal offenses in Madison. Madison Police Department records indicate a spike in calls for service and verified criminal offenses within the Northeast Area in 2022, with most calls being for thefts, disturbances, drug incidents, investigations, and burglaries. The increase in calls for service and criminal offenses underscore the need for comprehensive community engagement and targeted strategies to address these issues.

Actions

- Establish one or two <u>Neighborhood Resource Teams</u> (NRTs) to better connect City and community resources to areas experiencing challenges. NRTs also increase opportunities for stakeholders to provide feedback with City and community officials to proactively address safety concerns and other neighborhood issues. See "Potential NRT Area" map.
- 2. Incorporate SafeGrowth strategies that empower neighborhood residents involving them with city government agencies in developing solutions to safety concerns that have been identified.

What We Heard

- 1. Address drug-dealing.
- Add lighting on Reindahl Park bike path, along Portage Road, and other locations to deter crime and increase the sense of safety.
- 3. Address safety concerns on Hayes Road near existing hotels where suspicious behaviors have been reported (frequent short visits, abnormal trash, or debris and unusual odors).
- Ensure adequate services are provided for unhoused and lower-income residents to avoid unintended neighborhood impacts.
- 5. Mitigate issues related to highway and airport noise.
- 6. Improve air quality.

Northeast Area Plan - Draft





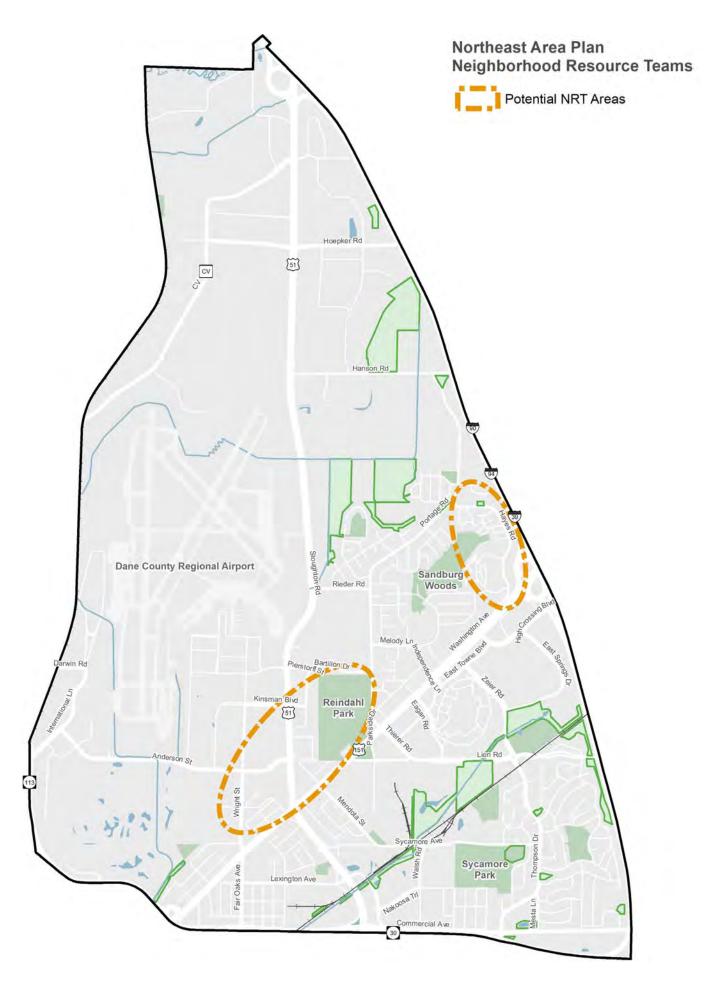
One goal of the City's Vision Zero program is to ensure a safe commute for school children.

72

- Conduct a comprehensive lighting audit in the East Washington Avenue, Mendota Street, Sycamore bike path, and Sandburg Woods bike path areas. Install additional lighting where needed to improve safety and deter theft and drug related offenses.
- 4. Selectively remove trees and brush to increase visibility along businesses abutting Starkweather Creek and Sycamore Park.
- 5. Require sound mitigation (sound insulating materials and construction techniques) on new developments receiving City funding that are located within the Dane County Regional Airport's 65 db DNL noise contour.
- 6. Encourage new buildings within 200 feet of Interstate 39/90/94 and Highway 30 to use materials and insulation that keep interior noise levels below 52 decibels as per state code TRANS 405. Design sites to shield outdoor spaces from noise above 67 decibels. Developments near the highway should follow setback and noise regulations outlined in Wisconsin statutes (TRANS 233, TRANS 405), and MGO Section 16.23.
- 7. Increase pedestrian safety for children traveling to Sandburg Elementary School, with a focus on East Washington Avenue..
- 8. Conduct Vision Zero Speed Analyses while actively enforcing traffic regulations along school routes. Particular emphasis should be placed on streets such as East Washington Avenue, East Towne Boulevard, Packers Avenue, and other residential areas seeing frequent pedestrian activity and experiencing elevated risks of serious traffic-related injuries and fatalities.

Partnerships

- A. Work with WisDOT to explore and implement noise mitigation projects along Highway 30, Highway 51, and Interstate 39/90/94 frontages.
- B. Examine traffic management strategies along Lexington Avenue to slow vehicle speeds in close proximity to Hawthorne Elementary School. Prioritize key intersections such as Powers-East Washington-Lexington Avenues, North Fair Oaks and Lexington Avenues, and Mayfair and Lexington Avenues.
- C. Broaden the availability of cooling options in the Northeast Area for residents without access to air conditioning. Establish partnerships with the East Madison Community Center, local businesses, places of worship, and other community entities to designate them as officially recognized cooling sites.
- D. Collaborate with resident leaders to continue to assess environmental factors that may contribute to or protect against violence, such as dimly lit areas, abandoned buildings, and vacant parking lots.



Community Action Strategy

Introduction

The City receives federal funding to support community development initiatives primarily benefiting low- to moderate-income residents. A portion of this funding is used to connect with residents in these areas to plan for and implement projects and activities that enhance the quality of life in their neighborhoods. These areas are defined by the U.S. Department of Housing and Urban Development and are eligible to receive federal funding to implement recommendations through the Community Development Block Grant (CDBG) program.

The Northeast Area Plan includes three eligible areas: Sandburg, Burke Heights, and Hawthorne-Truax. The Hawthorne-Truax Community Action Strategy (CAS) recommendations were developed through the 2023 Hawthorne-Truax Neighborhood Plan and are incorporated into this Plan.

While Area Plans focus on guiding physical changes over a large area, CAS recommendations focus on specific neighborhoods and include additional topics such as:

- Capacity Building: Enhancing the ability of individuals, organizations, and communities to effectively advocate for their needs and implement relevant projects and activities, and also providing information to community members about public services, programs, and projects that could affect them.
- Community and Stakeholder Relationships: Building relationships between the City and community that are open, understanding, and cooperative helps ensure a more inclusive and responsive approach to plan implementation. Such relationships are built over time and can be powerful catalysts for carrying out community-based recommendations for things like business improvement districts, neighborhood-oriented destinations, and community gatherings.
- Small Scale Physical Improvements: Modest enhancements to specific sites such as street infrastructure, green spaces, public facilities, art installations, traffic calming, community clean-up initiatives, or other community programming. These improvements aim to enhance local aesthetics, functionality, and community engagement.

CAS Project Funds

The City's Community Development Block Grant (CDBG) Committee budgeted a total of \$400,000 in federal CDBG funds to facilitate the implementation of eligible projects in the Sandburg and Burke Heights neighborhoods. The CDBG Committee previously budgeted \$250,000 for the Hawthorne-Truax area.



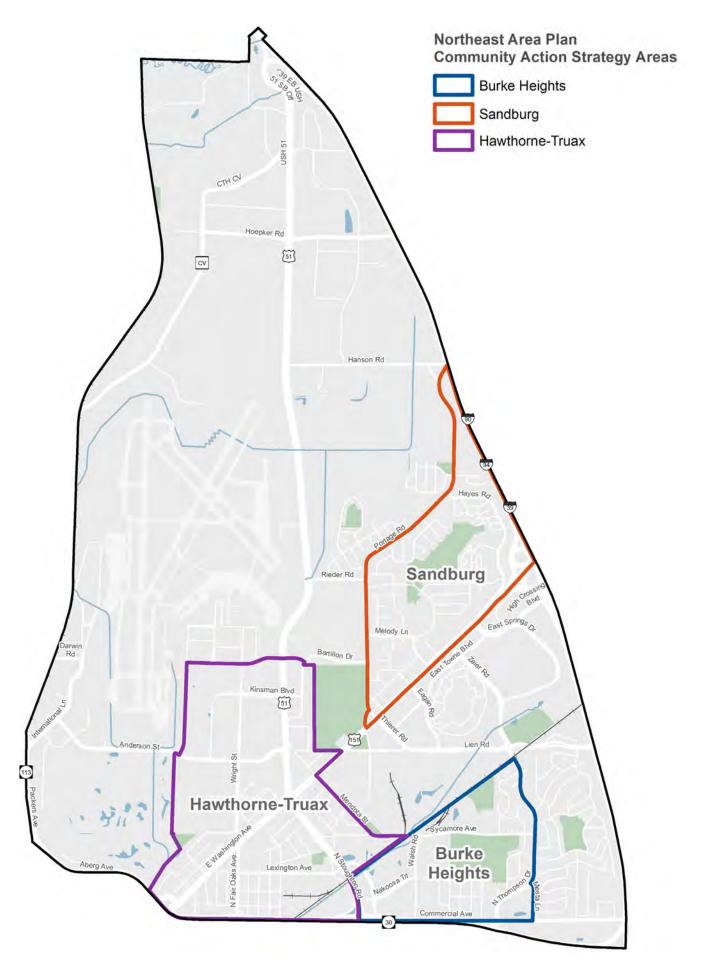
Capacity building example - City resources workshop at Senior Center



Community relationships example -Clean-up neighborhood activity with the youth



Small physical improvements example - sidewalk repair





Sandburg CAS Recommendations: Capacity Building

- 1. Create a cultural inclusion plan for developers that would provide guidance on promoting a diverse mix of tenants.
- 2. Increase promotion of City residential assistance programs, including rental and utility payment support, the property tax assistance program for seniors, and low-cost mortgage and rehabilitation loans.
- 3. Strengthen Public Health's "Reduce Drug Harm and Violence" initiative to expand service to the area for mental and physical health, domestic violence, and violence prevention and intervention.

Community and Stakeholder Relationships

- 4. Develop marketing strategies to draw activity and employment to the business areas along East Washington Avenue.
- 5. Continue building relationships with economic development partners, especially those with ties to Black, Indigenous, and people of color (BIPOC) communities, including the Madison Black Chamber of Commerce, Latino Chamber of Commerce of Dane County, and the Hmong Wisconsin Chamber of Commerce.
- 6. Identify and grow opportunities for entrepreneurs to locate along East Washington Avenue and the Greater East Towne Mall Area.
- 7. Work with local commercial brokers and property owners to connect small businesses to available space as they "graduate" from an incubator.
- 8. Conduct an equity-based market study of the area to further explore strategies for strengthening the economy, with an emphasis on ensuring that people who have been traditionally excluded are included in redevelopment and employment opportunities.
- 9. Establish relationships between the Sandburg Neighborhood Association and Dane County's Joining Forces for Families and Public Health's Violence Prevention Initiatives to help residents connect to services.

Small Scale Physical Improvements

- 10. Integrate pedestrian and bicycle infrastructure like benches, pedestrian-oriented lighting, and bike racks into terrace areas, especially in and around East Washington Avenue.
- 11. Encourage expansion of bicycle share (B-Cycle) facilities to the area.
- 12. Support temporary use of vacant spaces to assist emerging artists while adding to the vibrancy of the area. This may include artist-in-residency spaces, pop-up galleries, and public art.



B-Cycle station

CAS Project Expenditures

The following table includes a list of potential projects prioritized by residents with locations and cost estimates. The City will work on implementing the projects in the order shown, and in some cases may encounter obstacles that require moving to the next project in the list. The intention is to fully implement these projects over time using either CDBG funding alone, CDBG funding in combination with the City budget and/or other funds, or entirely through other means.

Priority	Project	Location	Cost Estimate
1	Lighting and safety	 Lighting Bike and walking paths (Sandburg Woods) Safety: traffic calming and safer crossings along: Portage Road between East Washington Avenue and Reider Road Independence Lane Anniversary Lane Melody Lane and Bartillion Drive Dwight Drive (has 6 speed humps but needs sidewalk) Mayfair Park North Thompson Drive bump outs, continental crosswalks, islands 	 Rectangular rapid flashing beacons (RRFB) at Dwight Drive \$10,000 Dwight Drive curb bump out at Reindahl path \$25,000 Melody Lane and/or Independence Lane speed humps \$10,000 each Pedestrian island at Portage Road and Melody Lane \$10,000 Maximum speed signs \$2,000 Mayfair Park RRFB and curb ramp \$20,000
2	Public gathering spaces	Sandburg ParkBenches in Sandburg Woods	 Art in alcoves along path \$30,000+ (depending on length) Benches \$4,000 each
3	Shelters and pavilions in parks	Sandburg Park	Depending on size
4	Permanent games in parks	Sandburg ParkWestchester Gardens Park	 Sandburg – Additional play equipment \$15,000 Sandburg – Nature play area \$50,000 Westchester – Community patio with shelter \$150,000
5	Community garden	 Sandburg – community coordination with Rooted required 	Water supply and shed

Northeast Area Plan - Draft 1215

Burke Heights





Parks Alive program

Burke Heights CAS Recommendations: Capacity Building

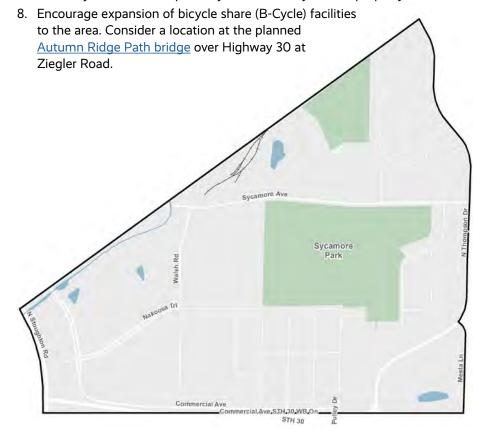
- 1. Include Sycamore Park as a primary location for community programming to help build relationships in the community (e.i., Parks Alive).
- 2. Increase promotion of City residential assistance programs, including rental and utility payment support, the property tax assistance program for seniors, and low-cost mortgage and rehabilitation loans.
- 3. Create a cultural inclusion plan for developers that would provide guidance on promoting a diverse mix of tenants.

Community and Stakeholder Relationships

- 4. Establish relationships between the Burke Heights Neighborhood Association and Dane County's Joining Forces for Families and Public Health's Violence Preventions Initiatives to help residents connect to services.
- 5. Connect the Burke Heights Neighborhood Association and residents with the Parks Division, Madison Public Library, and Mobile Madison to help residents host neighborhood events at Sycamore Park.

Small Scale Physical Improvements

- 6. Coordinate community clean-up events along primary neighborhood streets like Commercial Avenue and Sycamore Avenue.
- 7. Support the establishment of neighborhood planting areas to be maintained by volunteers on privately-owned or City-owned property.



CAS Project Expenditures

The following table includes a list of potential projects prioritized by residents with locations and cost estimates. The City will work on implementing the projects in the order shown, and in some cases may encounter obstacles that require moving to the next project in the list. The intention is to fully implement these projects over time using either CDBG funding alone, CDBG funding in combination with the City budget and/or other funds, or entirely through other means.

Priority	Project	Location	Cost Estimate
1	Lighting and safety	 Sycamore Park Neighborhoods surrounding Sycamore Park Speed limit signs along Sycamore Avenue and Mendota Street 	
2	Community garden	 Sycamore Park – Community coordination with Water supply and s Fruit trees 	
3	Bicycle share (B-cycle) station	Along East Washington Avenue\$1,800 per dockRepair stations \$3,000	
4	Murals/art installations	• Interactive art	• \$30 per sq ft
5	Asphalt art along paths	Sycamore Park dog park path	• \$25 per sq ft

Northeast Area Plan - Draft 1217

Hawthorne-Truax CAS Recommendations: Capacity Building

- 1. Support the establishment, capacity-building, and diversification of groups working to strengthen neighborhood cohesion and connections by sharing tools, tips, and other resources that help these groups become more inclusive and welcoming to the area's diverse residents.
- 2. Share guidance, information and resources with local groups and neighborhood associations to build capacity, support emerging groups, and empower diverse voices. Provide technical support and resources to assist residents in re-activating the Hawthorne Neighborhood Association.
- 3. Increase promotion of City residential assistance programs including rental and utility payment support, the property tax assistance program for seniors, and low-cost mortgage and rehabilitation loans.
- 4. Strengthen Public Health's "Reduce Drug Harm and Violence" initiative to expand service to the plan area for mental and physical health, domestic violence, and violence prevention and intervention.



Community and Stakeholder Relationships

- Increase awareness and access to existing community programs and services and explore expanding them in locations such as the community rooms of new housing developments, Hawthorne Elementary School, the Reindahl Imagination Center, and through co-location of community-based organizations.
- Support Joining Forces for Families and East Madison Community Center in their efforts to address Truax resident needs like affordable childcare, healthcare, and nutritious food.
- 7. Work together with the community and utilize City support and resources such as Parks Alive, Carts in Parks, and Madison Arts Commission funding and festival planning assistance to establish neighborhood events and activities in parks and open space, Truax apartment greenspace, and other gathering places. Feature food, music, crafts, artwork, and cultural performances that celebrate the diverse backgrounds of residents.
- 8. Coordinate with area financial institutions to explore creating financial literacy programming for residents including adults, youth, and school age kids.
- Partner with East Madison Community Center, Project Babies, RISE Wisconsin, the Lullaby program through the Overture Center, and other area organizations to provide support for pregnant people, babies, and young children.
- 10. Work with community partners to support and expand Hawthorne Community School. Consider establishing Open Schoolhouse evenings that offer community programs and resources.
- 11. Expand the Hawthorne Elementary Walking School Bus to serve students from the Carpenter-Ridgeway and Hawthorne neighborhoods.
- 12. Create additional teen activities and programing, similar to the Goodman Center Teen Loft, at the East Madison Community Center or in other community spaces.
- 13. Explore hosting community service and health fairs at Access Community Health Center, East Madison Community Center, Hawthorne Elementary School, and other neighborhood locations.
- 14. Support organizations such as New Bridge Madison and Goodman Community Center to bring programming and services for older adults to the area, including community events such as neighborhood concerts.

Small Scale Physical Improvements Recommendations

- 15. Explore possibilities for adding murals to blank building facades, such as the telecommunications building at Rowland Avenue and East Washington Avenue.
- 16. Work with Dane County to identify community-oriented interim uses for the greenspace in the Wright Street triangle.
- 17. Add one or more canoe and kayak launches along the west branch of the Starkweather Creek.
- 18. Create urban arts districts along Lexington, and Mayfair Avenues to support the establishment of artist workshops, music studios, and makerspaces.



Wright Street Green Open Space



Downtown LA Arts District

CAS Project Expenditures

The following table includes a list of community-driven projects as prioritized by residents with potential locations and cost estimates. City staff will work on implementing the projects in the order shown, and in some cases may encounter stumbling blocks that require moving to the next project in the list. The intention is to fully implement all of these projects over time using either CDBG funding alone, CDBG funding in combination with the City budget and/or other funds, or entirely through other means.

Priority	Project	Location	Cost Estimate
1	Youth recreation facilities	 Highest priority need is near Truax apartments, though location must be identified 	Basketball court: \$100,000Bike/skate facility: \$400,000
2	Lighting and other enhancements in tunnels and underpasses	 Tunnel under East Washington Avenue at Wright Street/Fair Oaks Avenue and the Marsh View Path under Highway 30 	East Washington tunnel: \$15,000Marsh View Path: \$100,000
3	Hawthorne Elementary School playground enhancements	Walking path around playgroundAccessible playground equipmentMural	 Walking path: \$50,000 Accessible playground equipment: \$125,000 Mural: \$100,000
4	Community gathering spaces	 Potentially include the Wright Street triangle and parklets on Fair Oak Avenue 	Wright Street triangle: \$25,000Fair Oaks parklets: \$25,000
5	Expand access to childcare services	 Renovation of childcare space at East Madison Community Center 	 TBD, possibly in coordination with future CDA Truax redevelopment phase

Appendix

Historic Resource Survey	84
Action Matrix	88
Glossarv	.94

Northeast Area Plan - Draft 1221

Historic Resource Survey

Methodology

To identify potential historic resources in the Northeast Area, staff completed a visual survey of properties in May 2023 and noted properties of potential architectural significance. Staff then completed additional research using files from the Wisconsin Historical Society, City of Madison preservation and building permit files, Sanborn maps, archival phone directories, and US Census records.

Findings

The area contains no City-designated landmarks and no properties listed in the National Register of Historic Places. Of the buildings noted for architectural or historic interest, that information is included as a note for that property in case property owners are interested in pursuing a future historic designation. Many of these are buildings that were places of worship and schools, which often serve as the location for significant architecture in a community, in addition to being the location of community gatherings and activities.

Staff did not complete an assessment of the buildings at Truax Field Air National Guard Base as the potential for historic designation or preservation is best handled as part of a federal process through the National Historic Preservation Act.

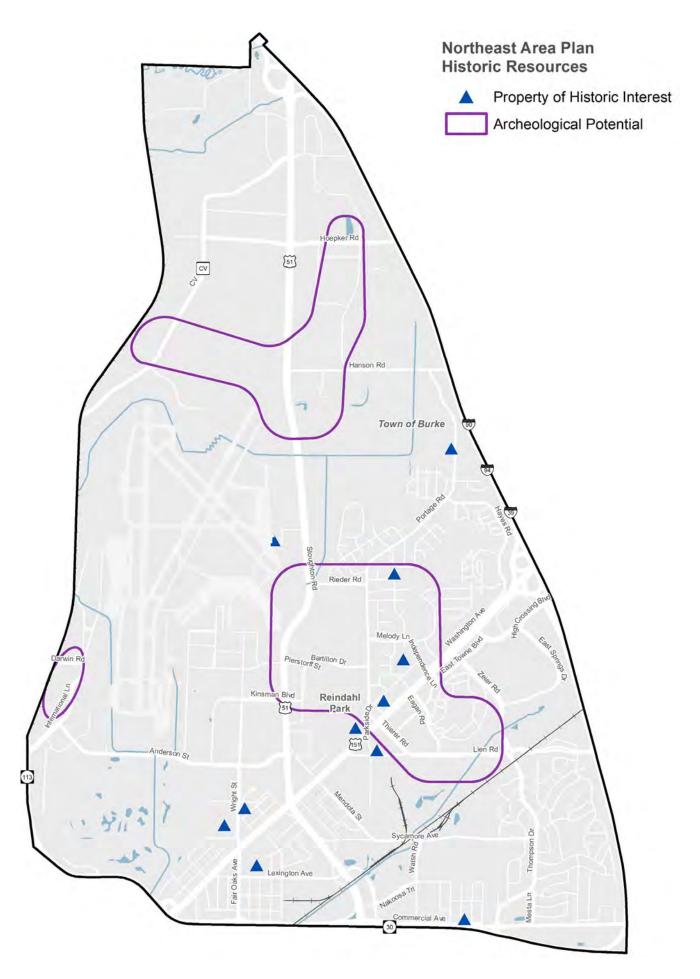
As Madison is the site of long-term human occupation, there are also several archaeological sites within the Northeast Area, several of which are known human burial sites. Most of these are associated with the Ho-Chunk people and any work that impacts these properties should be done in consultation with the Tribe during the earliest stages of the planning process. Any of the locations that are known human burial sites will need to secure a Request to Disturb from the Wisconsin Historical Society before engaging in ground-disturbing activity.

Recommendations

There are several individual properties that could benefit from having their story officially documented through the National Register process. As it is an honorary process, it does not place additional regulations on private property owners but does provide additional considerations for government undertakings that may impact their properties and opens the possibility for preservation tax credits to help property owners adapt their properties for new and ongoing uses. The preservation tax credit program is very popular, as there is both a State and Federal program.

For properties with known human burial sites on them, there is the Human Burial Site Tax Exemption process that makes the undevelopable portion of a property exempt from taxes. For that work, property owners should contact the Wisconsin Office of the State Archaeologist.

Historic preservation does not prevent development or changes of use, but it does shape how redevelopment happens. It is not a tool for exclusionary zoning. Properties with possible historic value are opportunities for adaptive reuse of places of architectural or historic significance so that these places could be sensitively integrated into the ongoing growth of Madison. Sharing Madison's unique culture and character through historic preservation, representing the history of Madison's diversity, and building reuse as an environmental sustainability strategy are priorities in of the Madison Preservation Plan.



Properties of Historic Interest

86

Name and Address	Area of Sig- nificance	Notes
Motel Royal 705 Redland Dr	History & Architecture	Constructed in 1950, this motorcourt style motel has changed uses to apartments, but still retains a great deal of historic integrity. This property was surveyed by WisDOT in 2014 and 2018
Islamic Center of East Madison (historic: Parkside Presbyterian)	Architecture	Constructed in 1958, this MidCentury religious building was designed by noted architect W.V. Kaeser
4002 Lien Rd		
Amund Reindahl Barn 3900 E Washington Ave	History & Architecture	The barn is all that remains of Amund Reindahl's farm, which he purchased in 1901. Reindahl bought the farm in 1901, and much of the land is now a City park.
H.P. Hall House	History 0	• •
4117 Dwight Dr	History & Architecture	The Greek Revival house was constructed in the 1850s as the home of H.P. Hall, who had a large farm in the area. The building has several additions off the rear of the building and now serves as a nursing home.
H.P. Hall Bird Effigy	Archaeology	Once the site of a large bird effigy mound on the farm of H.P. Hall, years of farming and subsequent development have largely destroyed remains of the mound, but there is still the potential for human burials remaining. Any ground-disturbing activity requires archaeological monitoring and a Request to Disturb from the Office of the State Archaeologist.
O'Connell Memorial US Army Reserve Center 1439 Wright St	History & Architecture	The two buildings associated with the USARC were constructed in 1961 and feature a training and administration building towards the front of the property and a multi-bay vehicle maintenance shop at the rear of the property. The buildings have had few significant changes to them over time and are indicative of post-WWII military infrastructure in smaller cities like Madison.
Wisconsin National Guard Headquarters Armory 1420 Wright St	History & Architecture	This Late Modern Building was constructed in 1956 and was home to the Wisconsin National Guard Headquarters Armory until 1994. This building is indicative of post-WWII military infrastructure in smaller cities like Madison. The building has had very few significant changes over time and is able to convey its historic associations.
Madison Municipal Airport Hangar	History & Architecture	Constructed by the Works Progress Administration in the Rustic style, this airport hangar was once paired with a municipal airport terminal building.
3400 N Stoughton Rd		

Draft - City of Madisor 1224

Name and Address	Area of Sig- nificance	Notes
Lucinda & Samuel Messer- schmidt House	History & Architecture	Constructed in 1930, this English Revival house is unusual for the Madison area in that it is clad in fieldstone. The house was designed by Samuel Massacraphmidt, who lived at this house with his
5487 Portage Rd		signed by Samuel Messerschmidt, who lived at this house with his wife Lucinda. In 2000, the building was relocated to this site from where it had been a part of a farmstead at 5723 STH 51.
Hoepker Scatter	Archaeology	Previous investigations have yielded information about early Anglo settlement of this site
Truax Air Park Mound	Archaeology	The mound site is protected with a conservation easement held by the Madison Trust for Historic Preservation. This is a Native Ameri- can mound and burial site.
Sandburg Elementary School 4114 Donald Dr	Architecture	The MidCentury Modern school building was designed by Weiler, Strang, McMullin and Associates and constructed in 1968
East Side Evangelical Lutheran Church	Architecture	The MidCentury Modern church was designed by J. Flad & Associates and constructed by Vogel Brothers in 1963
2310 Independence Ln		
Hawthorne School	Architecture	The MidCentury Modern school was designed by Law, Law, Potter & Nystrom and constructed by Findorf Construction in 1958
3344 Concord Ave		

Action Matrix

88

	Land Use Actions	Agencies
1	Update the Comprehensive Plan Generalized Future Land Use Map to be consistent with the Future Land Use map in this Plan.	Planning
2	Implement maximum building height recommendations shown on the Building Height Map with a mechanism for allowing bonus stories for developments that achieve specific affordability or sustainability parameters, similar to downtown height bonuses in City ordinances (Madison General Ordinances 28.071(2)(a)2).	Planning, Zoning
3	Require ground floor commercial space or other active uses in the locations shown on the Commercial Core Areas Map.	Planning
4	Initiate a follow-up process to consider zoning changes shown on the Proactive Rezoning Map. Ensure all impacted property owners are aware of potential zoning changes.	Planning, Zoning
5	Explore transitioning Planned Developments approved prior to 2013 to a matching conventional zoning district to improve transparency, usability, and consistency of regulations while reducing administrative burdens and inquiry response time.	Planning, Zoning
	Transportation Actions	Agencies
1	Officially Map future public streets as shown on the Street Network Map and reserve sufficient public right-of-way to implement the planned street network.	Planning, Engineering
2	Consider short and long-term improvements to Fair Oaks Avenue, Portage Road, Thompson Drive and other wide neighborhood streets that reduce unneeded street width to slow vehicular traffic, allow for tree plantings, improve bike, and pedestrian experience and reduce impervious surface. Short-term improvements could include bump-outs to shorten crossing distances and to allow for tree plantings, while long-term reconstructions could reflect Complete Green Street typologies.	Engineering, Traffic Engineering
3	Remove the street segments as shown on the Street Network Map and transition underutilized or unutilized roads or right-of-way into developed areas that have buildings that are close to the street and easily accessible to pedestrians.	Engineering, Traffic Engineering
4	Add sidewalks and other connections shown on the Pedestrian Network Map to improve connectivity and safety. Seek grants or other funding sources to add Priority Connections on high traffic corridors, near schools, or other primary pedestrian routes.	Engineering, Traffic Engineering
5	Add pedestrian and bicycle improvements at the intersections shown on the Pedestrian Network map to improve safety. Highest priority crossings are those on Highway 51 and East Washington Avenue.	Engineering, Traffic Engineering
6	Implement the bicycle network connections shown on the Bicycle Network Map to improve connectivity and safety.	Engineering, Traffic Engineering
7	Update the Complete Green Streets Guide's Street Type Map to incorporate changes to the street types to ensure that streets are designed according to the existing and planned surrounding context.	Transportation, Traffic Engineering
	Neighborhoods and Housing Actions	Agencies
1	Incentivize affordable housing, particularly on the East Washington Avenue and BRT corridors, through the Affordable Housing Fund, Land Banking initiative, and Tax Increment Financing.	Community Development, Economic Development
2	For developments receiving City funding, incentivize a wider mix of rental unit sizes, including those large enough to support larger and multigenerational families (3-4+ bedrooms).	Community Develop- ment, Planning

Draft - City of Madisor 1226

3	Consider designating a neighborhood in the Northeast Area as a targeted area for the Rental Rehab Loan Program to further incentivize improvements to aging rental housing properties in lower-income communities.	Community Development
4	Invest in food retail through funding and technical assistance to store operators through the Healthy Retail Access Program and SEED Grants, particularly neighborhoods north of East Washington Avenue which are identified as Food Access Improvement Areas (see Food Access Improvement Areas map).	Economic Development
	Economy and Opportunity Actions	Agencies
1	Establish a Greater East Towne Area Implementation Team to assist its redevelopment while also addressing vacant business spaces and parking lots.	Economic Develop- ment, Planning
2	Promote awareness and continue implementation of small business development programs funded through Tax Increment District (TID) #52 and other future area TIDs. These programs include the Facade Grant Program, Building Improvement Grant Program, Small Cap TIF Program, and Commercial Ownership Assistance Program.	Economic Development
3	Utilize Land Banking program to buy underutilized properties to facilitate redevelopment that includes employers.	Economic Development
4	Concentrate City resources on enhancing the business area along the International Lane corridor south the airport. Optimize available commercial spaces to their full potential to attract investment and foster economic growth.	Economic Development
5	Actively promote the Foreign Trade Zone adjacent to Dane County Regional Airport to attract businesses that offer stable employment opportunities. Efforts should focus on attracting more businesses to create employment opportunities for local residents.	Economic Development
	Culture and Character Actions	Agencies
1	Povice and undate Urban Design District (UDD) 5 to require new development to be	Dlanning
1	Revise and update Urban Design District (UDD) 5 to require new development to be pedestrian-oriented, including comfortable spaces and visually appealing elements along East Washington Avenue. See proposed UDD 5 revisions at the end of this chapter.	Planning
2	pedestrian-oriented, including comfortable spaces and visually appealing elements along	Planning, Economic Development
	pedestrian-oriented, including comfortable spaces and visually appealing elements along East Washington Avenue. See proposed UDD 5 revisions at the end of this chapter. Support a variety of cultural and entertainment venues as the area redevelops. This should include spaces that vary in size and are well suited for a variety of visual and per-	Planning, Economic
2	pedestrian-oriented, including comfortable spaces and visually appealing elements along East Washington Avenue. See proposed UDD 5 revisions at the end of this chapter. Support a variety of cultural and entertainment venues as the area redevelops. This should include spaces that vary in size and are well suited for a variety of visual and performing arts to make them easily accessible to area residents. Add art and placemaking elements through expanded use of the utility box wrap pro-	Planning, Economic Development
2	pedestrian-oriented, including comfortable spaces and visually appealing elements along East Washington Avenue. See proposed UDD 5 revisions at the end of this chapter. Support a variety of cultural and entertainment venues as the area redevelops. This should include spaces that vary in size and are well suited for a variety of visual and performing arts to make them easily accessible to area residents. Add art and placemaking elements through expanded use of the utility box wrap program. Provide residents with information and assistance about how to create and host festivals	Planning, Economic Development Planning
3	pedestrian-oriented, including comfortable spaces and visually appealing elements along East Washington Avenue. See proposed UDD 5 revisions at the end of this chapter. Support a variety of cultural and entertainment venues as the area redevelops. This should include spaces that vary in size and are well suited for a variety of visual and performing arts to make them easily accessible to area residents. Add art and placemaking elements through expanded use of the utility box wrap program. Provide residents with information and assistance about how to create and host festivals or community events within Reindahl and Sycamore Parks.	Planning, Economic Development Planning Planning, Parks
2 3 4 5	pedestrian-oriented, including comfortable spaces and visually appealing elements along East Washington Avenue. See proposed UDD 5 revisions at the end of this chapter. Support a variety of cultural and entertainment venues as the area redevelops. This should include spaces that vary in size and are well suited for a variety of visual and performing arts to make them easily accessible to area residents. Add art and placemaking elements through expanded use of the utility box wrap program. Provide residents with information and assistance about how to create and host festivals or community events within Reindahl and Sycamore Parks. Add benches along Starkweather Creek. If East Towne Mall redevelops, add a prominent art piece within the planned urban plaza and connect the new neighborhood with Sycamore Park through arts programs such as	Planning, Economic Development Planning Planning, Parks Parks, Engineering Planning, Economic
2 3 4 5 6	East Washington Avenue. See proposed UDD 5 revisions at the end of this chapter. Support a variety of cultural and entertainment venues as the area redevelops. This should include spaces that vary in size and are well suited for a variety of visual and performing arts to make them easily accessible to area residents. Add art and placemaking elements through expanded use of the utility box wrap program. Provide residents with information and assistance about how to create and host festivals or community events within Reindahl and Sycamore Parks. Add benches along Starkweather Creek. If East Towne Mall redevelops, add a prominent art piece within the planned urban plaza and connect the new neighborhood with Sycamore Park through arts programs such as Art in Public Places and Poetry in Sidewalks. Inform property owners who can officially document sites identified during historic resource surveys (see Historic Resources Map) with historical value through the National	Planning, Economic Development Planning Planning, Parks Parks, Engineering Planning, Economic Development
2 3 4 5 6	East Washington Avenue. See proposed UDD 5 revisions at the end of this chapter. Support a variety of cultural and entertainment venues as the area redevelops. This should include spaces that vary in size and are well suited for a variety of visual and performing arts to make them easily accessible to area residents. Add art and placemaking elements through expanded use of the utility box wrap program. Provide residents with information and assistance about how to create and host festivals or community events within Reindahl and Sycamore Parks. Add benches along Starkweather Creek. If East Towne Mall redevelops, add a prominent art piece within the planned urban plaza and connect the new neighborhood with Sycamore Park through arts programs such as Art in Public Places and Poetry in Sidewalks. Inform property owners who can officially document sites identified during historic resource surveys (see Historic Resources Map) with historical value through the National Register process.	Planning, Economic Development Planning Planning, Parks Parks, Engineering Planning, Economic Development Planning

2	Sandburg Park	
a. :	-	Doules Engineering
1.	Add a path connection to Sandburg Woods from vacant property on Forest Run Road when it develops.	Parks, Engineering
ii.	Better define edges between park land and private properties on the north side of Sandburg Park.	Parks, Engineering
iii.	If apartment communities on Portage and Hayes Roads redevelop, establish a new street which abuts the park to improve visibility, access, and uses.	Traffic Engineering, Engineering
b.	Sycamore Park	
i.	Extend Nakoosa Trail to Jana Lane, as a phased project, to provide greater visibility, access, and use of the southern portion of the park. Extending Nakoosa Trail to the east of Pulley Street will require minor park configuration changes that should be addressed as part of a future master planning process.	Traffic Engineering, Parks, Engineering
ii.	In the interim, consider changes to the shared-use path intersection with Nakoosa Trail to address reports of unauthorized motor vehicle use on the path.	Traffic Engineering, Parks, Engineering
3	Improve access to publicly-owned natural areas to provide passive recreation opportunities:	Parks, Engineering
a.	Provide public access to the wetland area north and west of Portage Road when adjacent vacant property develops.	Parks, Engineering
b.	Establish public access to the wetland area along the rail corridor south of Lien Road as part of the planned Hiestand Path.	Parks, Engineering
C.	Seek public access along Starkweather Creek between Highway 30 and Commercial Avenue via easement or property acquisition.	Parks, Engineering
4	Proactively evaluate identified areas on the Urban Forestry Map for potential street tree planting sites, with a particular emphasis on the Highway 30/Commercial Avenue frontage.	Streets/Forestry, Engineering
5	Evaluate City stormwater parcels for plantings and ecological restoration consistent with the Stormwater Utility Vegetation Management Plan once adopted.	Engineering
6	As part of the Starkweather Creek Watershed Study, develop stormwater management and watershed protection recommendations. Investigate solutions to areas with chronic flooding such as at Lexington Avenue and Highway 51 and in the "triangle" area between the East Washington Avenue Frontage Road and the Commercial Avenue Service Road.	Engineering
	Effective Government Actions	Agencies
1	Explore satellite facilities for government services to accommodate customer schedules and needs, potentially at the Reindahl Imagination Center.	Engineering, Library, Parks
2	Conduct a review of community service and facility needs after the Reindahl Imagination Center is built and new housing is present in the planning area. If gaps are discovered, the Community Development Division should explore a service provider facility (for example, The Village on Park) where providers can rent space and offer: employment services, business development and computer skills training, preventive health services, food and essentials pantry, and fitness and recreation.	Community Development, Economic Development
3	Develop a comprehensive infrastructure plan in advance of Town of Burke properties attaching in 2036. The plan should identify infrastructure needed to support continued industrial development in area north of the airport and capitalize on the City's successful partnerships with the Interstate Commerce Park area and the Center for Industry and Commerce Owners Association.	Planning, Engineering

Draft - City of Madisor 1228

90

	Health and Safety Actions	Agencies
1	Establish one or two Neighborhood Resource Teams (NRTs) to better connect City and community resources to areas experiencing challenges. NRTs also increase opportunities for stakeholders to provide feedback with City and community officials to proactively address safety concerns and other neighborhood issues. See "Potential NRT Area" map.	Civil Rights
2	Incorporate SafeGrowth strategies that empower neighborhood residents involving them with city government agencies in developing solutions to safety concerns that have been identified.	Police, Public Health
3	Conduct a comprehensive lighting audit in the East Washington Avenue, Mendota Street, Sycamore bike path, and Sandburg Woods bike path areas. Install additional lighting where needed to improve safety and deter theft and drug related offenses.	Engineering, Police
4	Selectively remove trees and brush to increase visibility along businesses abutting Starkweather Creek and Sycamore Park.	Engineering
5	Require sound mitigation (sound insulating materials and construction techniques) on new developments receiving City funding that are located within the Dane County Regional Airport's 65 db DNL noise contour.	Planning, Community Development
6	Encourage new buildings within 200 feet of Interstate 39/90/94 and Highway 30 to use materials and insulation that keep interior noise levels below 52 decibels as per state code TRANS 405. Design sites to shield outdoor spaces from noise above 67 decibels. Developments near the highway should follow setback and noise regulations outlined in Wisconsin statutes (TRANS 233, TRANS 405), and MGO Section 16.23.	Planning, Zoning
7	Increase pedestrian safety for children traveling to Sandburg Elementary School, with a focus on East Washington Avenue	Traffic Engineering
8	Conduct Vision Zero Speed Analyses while actively enforcing traffic regulations along school routes. Particular emphasis should be placed on streets such as East Washington Avenue, East Towne Boulevard, Packers Avenue, and other residential areas seeing frequent pedestrian activity and experiencing elevated risks of serious traffic-related injuries and fatalities.	Traffic Engineering, Transportation
	Sandburg CAS Recommendations:	Agencies
1	Create a cultural inclusion plan for developers that would provide guidance on promoting a diverse mix of tenants.	Community Develop- ment, Planning
2	Increase promotion of City residential assistance programs, including rental and utility payment support, the property tax assistance program for seniors, and low-cost mortgage and rehabilitation loans.	Community Development
3	Strengthen Public Health's "Reduce Drug Harm and Violence" initiative to expand service to the area for mental and physical health, domestic violence, and violence prevention and intervention.	Public Health
4	Develop marketing strategies to draw activity and employment to the business areas along East Washington Avenue.	Economic Development
5	Continue building relationships with economic development partners, especially those with ties to Black, Indigenous, and people of color (BIPOC) communities, including the Madison Black Chamber of Commerce, Latino Chamber of Commerce of Dane County, and the Hmong Wisconsin Chamber of Commerce.	Economic Development
6	Identify and grow opportunities for entrepreneurs to locate along East Washington Avenue and the Greater East Towne Mall Area.	Economic Development
7	Work with local commercial brokers and property owners to connect small businesses to available space as they "graduate" from an incubator.	Economic Development

Northeast Area Plan - Draft 1229

8	Conduct an equity-based market study of the area to further explore strategies for strengthening the economy, with an emphasis on ensuring that people who have been traditionally excluded are included in redevelopment and employment opportunities.	Economic Development
9	Establish relationships between the Sandburg Neighborhood Association and Dane County's Joining Forces for Families and Public Health's Violence Prevention Initiatives to help residents connect to services.	Joining Forces for Families, Public Health, NRT
10	Integrate pedestrian and bicycle infrastructure like benches, pedestrian-oriented lighting, and bike racks into terrace areas, especially in and around East Washington Avenue.	Engineering
11	Encourage expansion of bicycle share (B-Cycle) facilities to the area.	Traffic Engineering
12	Support temporary use of vacant spaces to assist emerging artists while adding to the vibrancy of the area. This may include artist-in-residency spaces, pop-up galleries, and public art.	Planning
	Burke Heights CAS Recommendations:	Agencies
1	Include Sycamore Park in the City's Parks Alive program.	Parks, NRT
2	Increase promotion of City residential assistance programs, including rental and utility payment support, the property tax assistance program for seniors, and low-cost mortgage and rehabilitation loans.	Community Development
3	Create a cultural inclusion plan for developers that would provide guidance on promoting a diverse mix of tenants.	Planning
4	Establish relationships between the Burke Heights Neighborhood Association and Dane County's Joining Forces for Families and Public Health's Violence Preventions Initiatives to help residents connect to services.	Joining Forces for Families, Public Health, NRT
5	Connect the Burke Heights Neighborhood Association and residents with the Parks Division, Madison Public Library, and Mobile Madison to help residents host neighborhood events at Sycamore Park.	Planning, Parks
6	Coordinate community clean-up events along primary neighborhood streets like Commercial Avenue and Sycamore Avenue.	Burke Heights Neighborhood Association
7	Support the establishment of neighborhood planting areas to be maintained by volunteers on privately-owned or City-owned property.	Parks, Engineering, Burke Heights Neigh- borhood Association
8	Encourage expansion of bicycle share (B-Cycle) facilities to the area. Consider a location at the planned Autumn Ridge Path bridge over Highway 30 at Ziegler Road.	Traffic Engineering
	Hawthorne-Truax CAS Recommendations:	Agencies
1	Support the establishment, capacity-building, and diversification of groups working to strengthen neighborhood cohesion and connections by sharing tools, tips, and other resources that help these groups become more inclusive and welcoming to the area's diverse residents.	Planning
2	Share guidance, information and resources with local groups and neighborhood associations to build capacity, support emerging groups, and empower diverse voices. Provide technical support and resources to assist residents in re-activating the Hawthorne Neighborhood Association.	Planning
3	Increase promotion of City residential assistance programs including rental and utility payment support, the property tax assistance program for seniors, and low-cost mortgage and rehabilitation loans.	Community Development
4	Strengthen Public Health's "Reduce Drug Harm and Violence" initiative to expand service to the plan area for mental and physical health, domestic violence, and violence prevention and intervention.	Public Health

Draft - City of Madisor 1230

92

5	Increase awareness and access to existing community programs and services and explore expanding them in locations such as the community rooms of new housing developments, Hawthorne Elementary School, the Reindahl Imagination Center, and through co-location of community-based organizations.	Hawthorne Neighbor- hood Association
6	Support Joining Forces for Families and East Madison Community Center in their efforts to address Truax resident needs like affordable childcare, healthcare, and nutritious food.	Community Development
7	Work together with the community and utilize City support and resources such as Parks Alive, Carts in Parks, and Madison Arts Commission funding and festival planning assistance to establish neighborhood events and activities in parks and open space, Truax apartment greenspace, and other gathering places. Feature food, music, crafts, artwork, and cultural performances that celebrate the diverse backgrounds of residents.	Hawthorne Neighbor- hood Association
8	Coordinate with area financial institutions to explore creating financial literacy programming for residents including adults, youth, and school age kids.	Economic Development
9	Partner with East Madison Community Center, Project Babies, RISE Wisconsin, the Lullaby program through the Overture Center, and other area organizations to provide support for pregnant people, babies, and young children.	Hawthorne Neighbor- hood Association
10	Work with community partners to support and expand Hawthorne Community School. Consider establishing Open Schoolhouse evenings that offer community programs and resources.	Hawthorne Neighbor- hood Association
11	Expand the Hawthorne Elementary Walking School Bus to serve students from the Carpenter-Ridgeway and Hawthorne neighborhoods.	MMSD
12	Create additional teen activities and programing, similar to the Goodman Center Teen Loft, at the East Madison Community Center or in other community spaces.	East Madison Community Center
13	Explore hosting community service and health fairs at Access Community Health Center, East Madison Community Center, Hawthorne Elementary School, and other neighborhood locations.	Hawthorne Neighbor- hood Association
14	Support organizations such as New Bridge Madison and Goodman Community Center to bring programming and services for older adults to the area, including community events such as neighborhood concerts.	Community Development
15	Explore possibilities for adding murals to blank building facades, such as the telecommunications building at Rowland Avenue and East Washington Avenue.	Planning
16	Work with Dane County to identify community-oriented interim uses for the greenspace in the Wright Street triangle.	Planning, Parks
17	Add one or more canoe and kayak launches along the west branch of the Starkweather Creek.	Engineering, Parks
18	Create urban arts districts along Lexington, and Mayfair Avenues to support the establishment of artist workshops, music studios, and makerspaces.	Planning

Glossary

Accessory dwelling unit: A second dwelling unit contained within a single-family dwelling or within a detached building located on the same lot as a single-family dwelling. This definition includes accessory buildings constructed in connection with a private garage or a private garage converted into a dwelling unit.

Activity Center: An intensively developed area that is the visual and/or functional center of a neighborhood(s) or a district. Activity centers are typically comprised of a mix of land uses developed at a higher intensity than the surrounding area including residential, commercial, employment, civic, institutional, and parks and open space uses.

Affordable housing: Housing for which the occupant(s) are paying no more than 30 percent of gross household income for housing costs, including utilities. Households with costs exceeding 30% of income are considered housing cost-burdened. The City of Madison's owner-occupant and rental development subsidy programs are primarily designed to reduce housing cost burdens for renter households with incomes at or below 60% of Dane County's median income (CMI) and for owner-occupied households at or below 80% CMI.

Affordable Housing Fund: A City of Madison program to provide loans and grants to for-profit and non-profit housing developers for the construction of new affordable rental housing.

Berm: A linear mound generally built to screen views, define areas, or direct stormwater.

Bicycle share (B-Cycle): A kiosk-based bike rental system offering hourly, daily, or subscription-based usage.

Big box retail: A physically large retail store with at least 75,000 square feet of floor area and a regional sales market. They are usually part of a national or regional chain of stores. Some examples include Walmart, Target, Best Buy, and Home Depot. (Source: APA: A Planners Dictionary)

BIPOC: An umbrella term for people of color, which stands for Black, Indigenous, and people of color. The term acknowledges that not all people of color face equal levels of injustice, recognizing that Black and Indigenous people are severely impacted by systemic racial injustices. (Source: Merriam-Webster)

Bump out: An extension of a raised curb into a roadway, typically a parking lane, to create additional terrace or sidewalk space. This can be used to shorten pedestrian crossing distances or create additional space for street trees while encouraging lower vehicular speeds.

Bus Rapid Transit (BRT): is a frequent, faster, and more reliable bus system that uses larger buses to transport more riders. BRT's features include frequent all-day service and direct routes with fewer stops. It utilizes special traffic signals to help buses get through intersections faster, dedicated bus lanes, and stations with off-board fare payment kiosks.

Capital Area Regional Planning Commission (CARPC): One of nine commissions in Wisconsin established to coordinate planning and development among area municipalities. CARPC develops and promotes regional plans, provides objective information and professional planning services, and focuses local attention on issues of regional importance. CARPC carries out land use planning and areawide water quality management planning for the greater Madison region. State statutes charge it with the duty of preparing and adopting a master plan for the physical development of the region. The Department of Natural Resources contracts with the Commission to maintain a continuing areawide water quality management planning process to manage, protect, and enhance the water resources of the region. (Source: CARPC)

Commercial Core: Street frontages where future development proposals are required to include ground floor commercial spaces.

Community Development Block Grant (CDBG): A federal program which provides annual grants to states and cities for affordable housing, anti-poverty, infrastructure, and planning activities that benefit low- to moderate-income persons.

Community garden: An area of land or space managed and maintained by a group of individuals to grow and harvest food crops and/or non-food, ornamental crops, such as flowers, for personal or group use, consumption or donation.

Community land trust: nonprofit organizations whose primary objective is the creation of homes that remain permanently affordable, providing successful homeownership opportunities for generations of lower income families. Under the community land trust (CLT) model, homeowners purchase their house, but not the land (this lowers the purchase price). The land is leased by the homeowner from the land trust. When homeowners sell, 75% of the appreciated value stays with the house, so it's more affordable for the next buyer. The CLT homeowner also gets to keep the equity they invested into the home plus 25% of the increase in value. (Sources: Madison Area Community Land Trust and Grounded Solutions Network)

Commercial Ownership Assistance program: The City's Commercial Ownership Assistance Program (COA) is focused on helping business owners expand their enterprises by transitioning from renting space to owning commercial property for their business. Priority is given to applicants who are people of color, immigrants, women, the disabled, veterans and any other underrepresented groups.

Complete neighborhood: are neighborhoods that include a range of housing types and costs, neighborhood-serving businesses, stores, services, schools, and places of worship. These elements are ideally accessible by foot, bicycle, or transit through a network of well-connected streets and blocks, usable public spaces, and a system of connected parks, paths, and greenways.

Continental crosswalk: A crosswalk with a more visible type of striping (painting), consisting of thick white stripes parallel to the direction of vehicular travel.

Cost-burdened: a household is cost-burdened when they are paying too much for their home (when total housing costs exceed 30% of their gross monthly income).

Drumlin: A linear or oval-shaped hill created by the streamlined movement of glacial ice sheets across rock debris. (Source: Britannica)

Easement: A legal tool that grants one party the right to use property that another party owns and possesses. (Sources: Investopedia, Merriam-Webster Dictionary)

Exclusionary zoning: A term applied to zoning standards, districts, or policies that seek to prevent people of certain races, ethnicities, or income levels from buying homes or living in specific areas or neighborhoods. This could include extensive use of exclusively single-family districts, large minimum lot or open space sizes, and narrow occupancy (household size) rules. (Source: Planetizen)

Facade modulation: Stepping back or extending forward a portion a building's facade. This can be done with vertical divisions using different textures or materials or adding features such as dormers, stepped roofs, arcades, awnings, and window bays.

Foreign Trade Zone (FTZ): a specific area within the United States, located in or near a port of entry, where certain types of merchandise can be imported without going through formal customs entry procedures or paying import duties. FTZs were designed to encourage international trade and U.S. employment. (Sources: Dane Country Regional Airport and Port of Seattle)

Frontage road: A street adjacent to a freeway, expressway, or arterial street separated therefrom by a dividing strip and providing access to abutting properties. (Source: APA: A Planners Dictionary)

Grade-separated intersection: an intersection where one roadway or lane travels above or below other roadways by separating where they cross each other with an overpass or underpass.

Greenway: Linear corridors of land and water and the natural, cultural, and recreational resources they link together. (Source: Massachusetts Office of Energy and Environmental Affairs)

Healthy Retail Access Program: A program created by Madison's Food Policy Council that provides funds for healthy retail projects that aim to improve access to affordable, healthy, and culturally appropriate food and retail within underserved areas.

Historic district: A significant concentration, linkage, or continuity of sites, buildings, structures, or objects united historically or aesthetically by plan or physical development. A local, state, or the federal government can officially recognize districts. (Source: U.S. National Park Service)

Housing cooperative: A residence that is collectively owned and controlled by its members: the people who live in the housing. The buildings typically have private bedrooms but shared spaces, such as common kitchens and recreation areas. Members often share responsibility for cooking, daily chores, and property maintenance.

Impervious surface: Any hard-surfaced area that does not readily absorb or retain water, including but not limited to building roofs, parking and driveway areas, graveled areas, sidewalks, and paved recreation areas. (Source: APA: A Planners Dictionary)

Income- and rent-restricted housing: a type of housing where a specific number of units in a building are reserved for low-income households. A qualifying household's income level must be at or below a specific level for that household's

size, such as 60% of the county median income. A limit is also set on the monthly rent to ensure that housing is more affordable for those households, often due to some form of subsidy. These restrictions are typically enforced through a Land Use Restriction Agreement.

Incubator: An area, commercial space, and/or building designated for the cultivation and enhancement of new or future businesses.

Infill development: Development of vacant or underused lots that are surrounded by developed areas.

Land Banking: A City program used to acquire land and buildings that could be used for future economic development, affordable housing projects, and other City uses. The goal is to acquire strategic properties for future purposes that might include: assisting displaced businesses, reducing blight, stabilizing housing markets, improving the quality of life of residents and neighborhoods, and preserving land for City purposes.

Makerspace: A term used to describe a place where people gather to share resources and knowledge, work on projects, network, and build. This could include artist studios, small-scale fabrication spaces, workshops, commercial kitchens, or similar spaces.

Market-rate housing: Housing that does not have any restrictions on rent or household income.

Missing Middle Housing: A range of housing types scaled between single-family detached houses and larger apartment buildings. Housing types that are considered as part of the missing middle include duplexes, triplexes, four-units, rowhouses, live-work buildings, accessory dwelling units (ADUs), clustered small homes like bungalow courts, and some small apartment buildings. The scale of missing middle housing is compatible with most existing single-family residential areas.

Mixed-use: A building or groups of buildings designed to encourage a diversity of compatible land uses, which include a mixture of two or more of the following uses: residential, office, retail, recreational, light industrial, and other miscellaneous uses. (Source: City of Beaverton, OR)

Neighborhood Development Plan (NDP): A plan prepared for largely undeveloped land on the city's edge. NDPs are adopted as supplements of the Comprehensive Plan and include recommendations for land use, transportation, parks and open space, and utilities.

Neighborhood Plan: A plan prepared for an already-developed area of the city that includes recommendations for land use, urban design, transportation, parks, placemaking, and other improvements/investments/changes to a given area. Neighborhood plans can encompass more than one neighborhood and are generally adopted as supplements to the Comprehensive Plan.

Neighborhood Resource Teams (NRTs): A citywide effort to coordinate and improve the delivery of City services to Madison's neighborhoods. NRTs provide a regular forum for City employees to meet, discuss, and support each other's efforts in delivering excellent City services. NRT membership can include alders, City staff, and non-City staff participants.

Official Map: A formal public record used to indicate where a government is likely to require right-of-way, easements, or land for future roads, drainageways, utilities, or recreation facilities. Within officially mapped areas, a property owner maintains control and use of their property but building permits cannot be issued. Official Mapping is established in Wisconsin State statute 62.23(6). (Source: UW Extension)

Operating costs: Expenses associated with the maintenance and administration of a business or government on a day-today basis, such as salaries. (Source: Investopedia)

Per- and polyfluoroalkyl substances (PFAS): A group of chemicals used to make products that resist heat, oil, stains, grease, and water. They are long lasting chemicals, which break down very slowly over time and exposure to them may be linked to harmful health effects. (Sources: US Environmental Protection Agency and Centers for Disease Control and Prevention)

Placemaking: Creation of an environment that fosters community, stimulates interaction, encourages entrepreneurship, generates innovation, and nurtures humanity. (Source: Project for Public Spaces)

Proactive rezoning: Proactive rezoning is when the City, rather than a property owner or developer, proposes to rezone land so that it is consistent with plan recommendations. When the City proposes to proactively rezone areas during a planning process, it informs property owners about the recommendation and whether it could affect them.

Property Tax Assistance for Seniors Program: A City program that pays all or a portion of property taxes for qualified homeowners who are over the age of 65 and own a single-family residence within the City of Madison.

Rectangular rapid flashing beacons (RRFBs): A flashing signal activated by pedestrians that alerts drivers to yield to crossing pedestrians.

Redevelopment: Construction of a new building where a building already exists.

Rental Rehab Loan Program: City of Madison program which offers financial assistance, including low-interest loans, for Madison property owners to renovate and improve rental housing.

SafeGrowth: A philosophy built on the belief that healthy and functioning small neighborhoods provide the safest way to build cities in the 21st Century. It is based on the premise that crime is best tackled within small neighborhoods by harnessing the creative energy of functioning neighborhood groups, by employing the latest crime prevention methods, and by adopting an annual SafeGrowth® Plan to address crime and fear. (Source: SafeGrowth.org)

SEED Program: A City of Madison program administered by the Madison Food Policy Council that provides grants to improve the local food system and make food more accessible to Madison residents.

Sense of Place: The characteristics of a location that make it readily recognizable as being unique and different from its surroundings and that provides a feeling of belonging to or being identified with that particular place. (Source: Scottsdale, AZ)

Setbacks: A building design where there are fewer stories closer to the lot line (for example, near sidewalks and adjacent properties) than the rest of the building.

Shared-use path: a path or lane shared by pedestrian, bicycle, and other non-motorized users.

Smaller-scale housing: Also referred to as missing middle housing, it is a range of smaller multi-unit or clustered housing types compatible in scale with single-family homes. (Source: Opticos Design, Inc.)

Stepbacks: A building design element that is typically applied to the upper-story of a development to establish compatibility with surrounding development and maintain a pedestrian-oriented scale. A stepback requires that any portion of a building above a certain height is further pushed-in towards the center of the property.

Stormwater: Untreated runoff from rainfall and snowmelt. It flows across impervious surfaces (such as streets), through fields, and over construction sites, crossing municipal boundaries and can carry contaminants to lakes and streams. (Source: Dane County Office of Lakes & Watersheds)

Tabletop crossing or intersection: An intersection where the roadway ramps up to the sidewalk level to create greater visibility for pedestrians, improve usability for individuals who are mobility impaired, and slow vehicles down. Also known as a raised crossing or intersection.

Tax Increment Financing (TIF): A governmental finance tool to provide funds to construct public infrastructure, promote development opportunities, and expand the tax base.

Terrace: The space between the sidewalk and the curb along a street.

Through movement: within an intersection, the through movement refers to vehicles or users going straight and not turning onto the cross street.

Transit-Oriented Development (TOD): Compact, walkable, mixed-use development that is centered on quality public transit. It typically includes a mix of housing, office, retail, neighborhood amenities, and other uses within walking distance of a transit station. TOD reduces the need for driving by creating compact, vibrant, walkable neighborhoods with convenient access to activities and destinations connected by transit.

Tree canopy: The part of a city or area that is shaded by trees; The layer of leaves, branches, and stems of trees that obscure the ground when viewed from above. (Source: Center for Watershed Protection)

Underrepresented groups: Groups of people with a common race, ethnicity, immigration status, age, income level, gender identity, or sexual orientation who have not typically participated in City decision-making processes corresponding with the proportion of the population they comprise. These groups have often experienced discrimination or marginalization based on their identity.

Urban agriculture: The production of food for personal consumption, market sale, donation, or educational purposes within cities and suburbs.

Urban form: the patterns of building height and development intensity as well as the structural elements that define an area physically, such as natural features, transportation corridors, open space, public facilities, and other elements. (Source: City of Los Angeles)

Water quality: The condition of water, including its chemical, physical, and biological characteristics with respect to its expected use, for example, drinking, swimming, or fishing. (Source: Florida Brooks National Marine Sanctuary, Key West, Florida)

Watershed: Watersheds are an area of land that drain to the same location (the outlet).

Wayfinding: Wayfinding refers to information systems that guide people through a physical environment and enhance their understanding and experience of the space. (Source: The Society for Experiential Graphic Design)

Zoning Code: An ordinance that regulates land use, lot size, building placement, building height, and other aspects of the development of land.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84216

File ID: 84216 File Type: Claim Status: Risk Business

Version:1Reference:Controlling Body:Risk Manager

File Created Date: 06/28/2024

File Name: CLAIM: J. Martin - Vehicle Damage - \$6,000.00 Final Action:

Title: J. Martin - Vehicle Damage - \$6,000.00

Notes: EVL005624

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

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Text of Legislative File 84216

Title

J. Martin - Vehicle Damage - \$6,000.00

Body

Claim received 06/12/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84239

File ID: 84239 File Type: Claim Status: Risk Business

Version:1Reference:Controlling Body:Risk Manager

File Created Date: 07/01/2024

File Name: CLAIM: S. Abbas - Vehicle Damage - \$3,000.00 Final Action:

Title: S. Abbas - Vehicle Damage - \$3,000.00

Notes: EVL005692

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

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Text of Legislative File 84239

Title

S. Abbas - Vehicle Damage - \$3,000.00

Body

Claim received 6/26/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84244

File ID: 84244 File Type: Claim Status: Risk Business

Version:1Reference:Controlling Body:Risk Manager

File Created Date: 07/01/2024

File Name: CLAIM: A. Moriarty - Vehicle Damage - \$675.00 Final Action:

Title: A. Moriarty - Vehicle Damage - \$675.00

Notes: EVL005693

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

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Text of Legislative File 84244

Title

A. Moriarty - Vehicle Damage - \$675.00

Body

Claim received 6/26/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84273

File ID: 84273 File Type: Claim Status: Risk Business

Version:1Reference:Controlling Body:Risk Manager

File Created Date: 07/02/2024

File Name: CLAIM: J. Owens - Vehicle Damage - \$500.00 Final Action:

Title: J. Owens - Vehicle Damage - \$500.00

Notes: EVL005694

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

 Ver- Acting Body:
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Text of Legislative File 84273

Title

J. Owens - Vehicle Damage - \$500.00

Body

Claim received 7/1/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84276

File ID: 84276 File Type: Claim Status: Risk Business

Version:1Reference:Controlling Body:Risk Manager

File Created Date: 07/02/2024

File Name: CLAIM: S. Riddle - Property Damage - \$1,740.00 Final Action:

Title: S. Riddle - Property Damage - \$1,740.00

Notes: EVL005695

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

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Text of Legislative File 84276

Title

S. Riddle - Property Damage - \$1,740.00

Body

Claim received 7/1/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84282

File ID: 84282 File Type: Claim Status: Risk Business

Version: 1 Reference: Controlling Body: Risk Manager

File Created Date: 07/02/2024

File Name: CLAIM: S. Caya and E. Tenebruso of Nowlan Law Final Action:

LLP for N. Sarenac - Personal Injury - \$50,000.00

Title: S. Caya and E. Tenebruso of Nowlan Law LLP for N. Sarenac - Personal Injury -

\$50,000.00

Notes: EVL005697

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

Ver- Acting Body: Date: Action: Sent To: Due Date: Return Result: sion: Date:

Text of Legislative File 84282

Title

S. Caya and E. Tenebruso of Nowlan Law LLP for N. Sarenac - Personal Injury - \$50,000.00

Body

Claim received 6/25/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84323

File ID: 84323 File Type: Claim Status: Risk Business

Version:1Reference:Controlling Body:Risk Manager

File Created Date: 07/05/2024

File Name: CLAIM: L. Albert - Vehicle Damage - \$13,387.38 Final Action:

Title: L. Albert - Vehicle Damage - \$13,387.38

Notes: EVL005703

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

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Text of Legislative File 84323

Title

L. Albert - Vehicle Damage - \$13,387.38

Body

Claim received 6/19/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84345

File ID: 84345 File Type: Claim Status: Risk Business

Version:1Reference:Controlling Body:Risk Manager

File Created Date: 07/09/2024

File Name: CLAIM: M. Gerding of Majestic Building LLC - Final Action:

Property Damage - \$53,898.00

Title: M. Gerding of Majestic Building LLC - Property Damage - \$53,898.00

Notes: EVL005713

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

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Text of Legislative File 84345

Title

M. Gerding of Majestic Building LLC - Property Damage - \$53,898.00

Body

Claim received 7/3/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84354

File ID: 84354 File Type: Claim Status: Risk Business

Version:1Reference:Controlling Body:Risk Manager

File Created Date: 07/09/2024

File Name: CLAIM: C. Eakins - Property Damage - \$539.83 Final Action:

Title: C. Eakins - Property Damage - \$539.83

Notes: EVL005714

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

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Text of Legislative File 84354

Title

C. Eakins - Property Damage - \$539.83

Body

Claim received 7/4/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84357

File ID: 84357 File Type: Claim Status: Risk Business

Version:1Reference:Controlling Body:Risk Manager

File Created Date: 07/09/2024

File Name: CLAIM: T. Germanotta of LawnCare Plus, Inc. - Final Action:

Vehicle Damage - \$12,271.13

Title: T. Germanotta of LawnCare Plus, Inc. - Vehicle Damage - \$12,271.13

Notes: EVL005715

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

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Text of Legislative File 84357

Titlo

T. Germanotta of LawnCare Plus, Inc. - Vehicle Damage - \$12,271.13

Body

Claim received 7/5/2024.



City of Madison Madison, WI 53703 www.cityofmadison.com

Master

File Number: 84365

File ID: 84365 File Type: Claim Status: Risk Business

Version:1Reference:Controlling Body:Risk Manager

File Created Date: 07/09/2024

File Name: CLAIM: J. Ward - Property Damage - \$650.00 Final Action:

Title: J. Ward - Property Damage - \$650.00

Notes: EVL005718

Sponsors: Effective Date:

Attachments: Enactment Number:

Author: Hearing Date:

Entered by: jaustin2@cityofmadison.com Published Date:

History of Legislative File

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Text of Legislative File 84365

Title

J. Ward - Property Damage - \$650.00

Body

Claim received 7/7/2024.