Kahler Slater

June 24, 2019

City of Madison Planning Division Attn: Heather Stouder & Colin Punt Madison Municipal Building 215 Martin Luther King, Jr. Blvd. Madison, WI 53701-2985

RE: Letter of Intent, Land Use Application for PD Amendment at 770/780 Regent St

Dear Heather & Colin:

Mortenson Development, Inc. is pleased to submit this Land Use Application for the amendment to the existing PUD GDP/SIP (PD GDP/SIP) for a nationally recognized, full-service hotel located at 770 Regent Street adjacent to 780 Regent St. This new hotel is intended to increase density on an underdeveloped parcel and provide an activated use at an important gateway to the campus while providing unparalleled accommodations for guests to Madison and the University of Wisconsin.

The hotel features multiple entrances, lobby, bar, and restaurant, and supporting functions on the first floor; prefunction, meeting space, and fitness room on the second floor; and 176 guestrooms on 4 levels above. A parking structure is embedded underneath the west portion of the hotel tower and contains approximately 247 parking spaces with a mix of full size and compact stalls. Just under 50% of the stalls are proposed as compact based on shared parking demand for the hotel and adjacent buildings. Additionally, new site parking has been added east of 780 Regent to offset the removal of parking on city property. We are requesting two loading zones at a reduced width of 9' by 50' as discussed with traffic.

The hotel will be operated by approximately 60 employees (45 full time and 15 part time) and operate 24 hours a day, 7 days a week. The building supports connections to the East Campus Mall, the Southwest Commuter Bike Trail and the greater neighborhood with outdoor amenities and pedestrian connections.

Primary improvements to the project include:

- Strong entry connection to the hotel, restaurant, and bar from the bike trail, including the development of the adjacent city property as an active landscaped terrace and walk, usable by guests and visitors.
- Exciting outdoor seating, dining and visual connections along the bike trail.
- Enhanced, well-lit entry to the building from the East Campus Mall.
- Simplified material palette and pedestrian scale features at all exterior elevations.
- Unique landscaping selections that integrate with the architecture.

Sincerely,

KAHLER SLATER, INC.

Glenn Roby, AIA Executive Vice President

cc: Mortenson Development, Inc. enc: LUA updates Milwaukee Madison Richmond Singapore

111 West Wisconsin Avenue Milwaukee, WI 53203 P 414.272.2000

UW CAMPUS HOTEL



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COVER 01 02 03 04

ATTACHMENTS

770 REGENT STREET | MADISON, WISCONSIN



UW CAMPUS HOTEL MADISON, WISCONSIN

CITY OF MADISON LUA UPDATES | JUNE 24, 2019

FIRST FLOOR PLAN PEDESTRIAN ACCESS DIAGRAM ENTRY VIEW FROM BIKE TRAIL

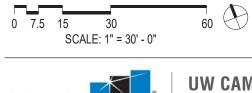
CIVIL AND LANDSCAPE DRAWINGS



PUBLIC HOTEL USE - CIRCULATION / SUPPORT ACTIVE SPACES INCLUDING BAR / RESTAURANT / LOUNGE BACK OF HOUSE

GUESTROOMS





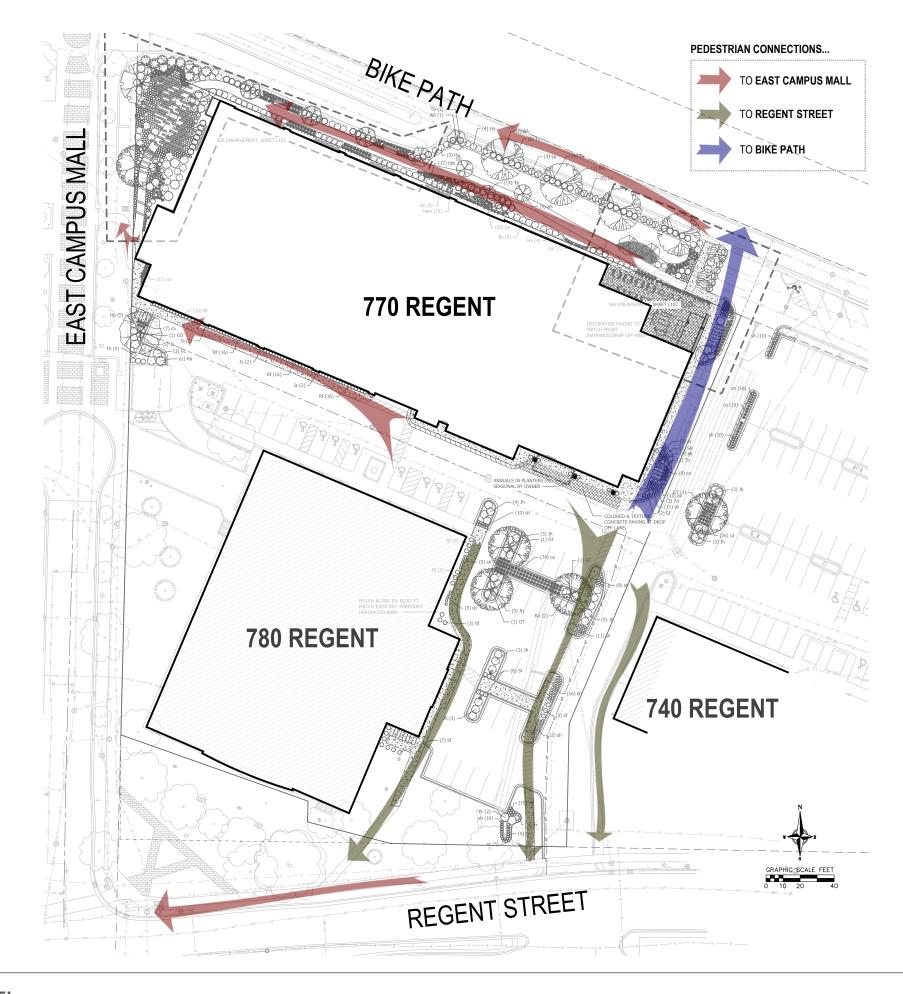
Mortenson

UW CAMPUS HOTEL MADISON, WISCONSIN

1ST FLOOR PLAN

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PEDESTRIAN ACCESS DIAGRAM

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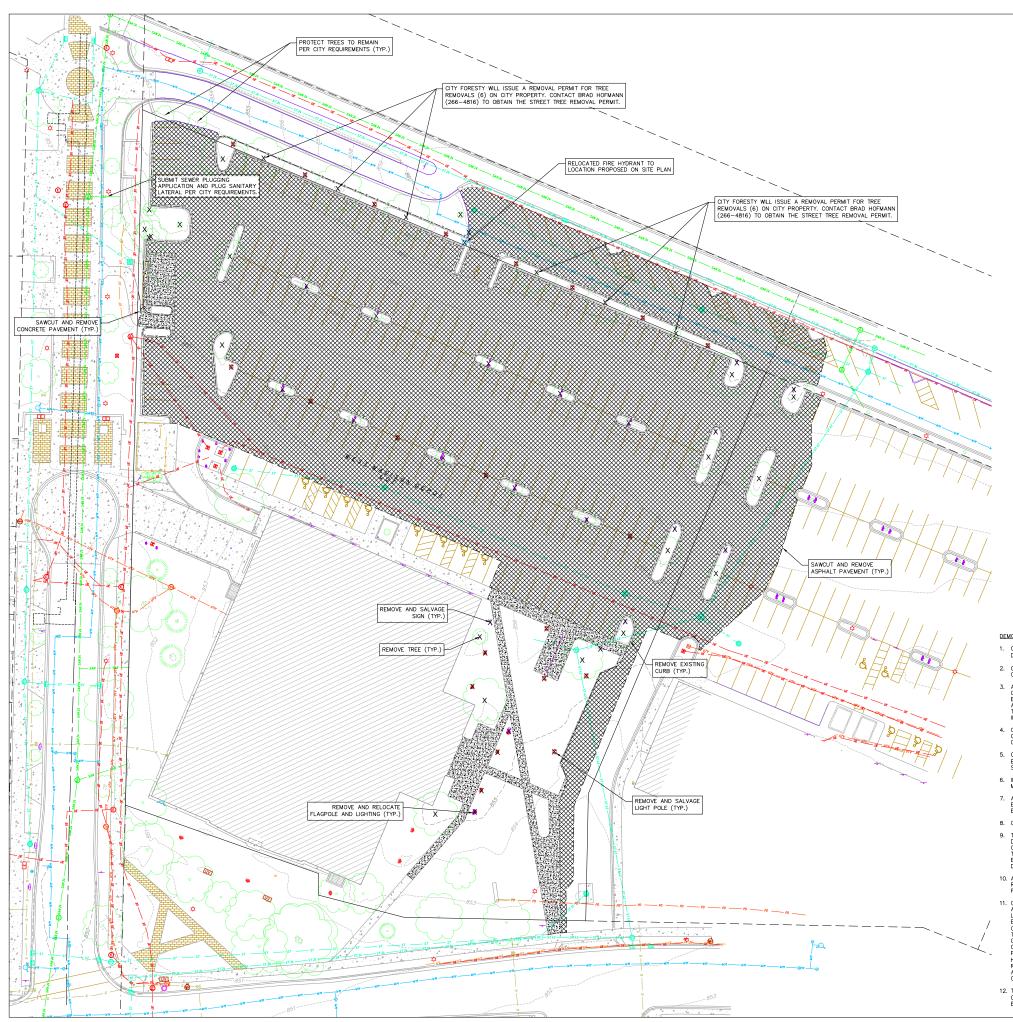


UW CAMPUS HOTEL MADISON, WISCONSIN

ENTRY VIEW FROM BIKE TRAIL

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GRAPHIC SCALE FEET

Revisions

DEMOLITION/EROSION CONTROL NOTES:

1. CONTRACTOR SHALL KEEP ALL CITY STREETS FREE AND CLEAR OF CONSTRUCTION RELATED DIRT/DUST/DEBRIS.

2. COORDINATE EXISTING UTILITY REMOVAL/ABANDONMENT WITH LOCAL AUTHORITIES AND UTILITY COMPANIES HAVING JURISDICTION.

3. ALL SAWCUTING SHALL BE FULL DEPTH TO PROVIDE A CLEAN EDGE TO MATCH NEW CONSTRUCTION, MATCH EXISTING ELEVATIONS AT POINTS OF CONNECTION FOR NEW AND EXISTING PAYEMENT, CURE, SIDEWALKS, ETC. ALL SAWCUT LOCATIONS SHOWN ARE APPROXIMATE AND MAY BE FIELD ADJUSTED TO ACCOMMODATE CONDITIONS, JOINTS, MATERIAL TYPE, ETC. REMOVE MINIMUM AMOUNT NECESSARY FOR INSTALLATION OF PROPOSED IMPROVEMENTS.

CONTRACTOR SHALL PROVIDE AND SHALL BE RESPONSIBLE FOR ANY NECESSARY TRAFFIC CONTROL SIGNAGE AND SAFETY MEASURES DURING DEMOLITON AND CONSTRUCTION OPERATIONS WITHIN OR URAR THE PUBLIC ROADWAY.

COORDINATE TREE REMOVAL WITH LANDSCAPE ARCHITECT. ALL TREES TO BE REMOVED SHALL BE REMOVED IN THEIR ENTIRETY AND STUMPS SHALL BE GROUND TO 12" BELOW PROPOSED SUBGRADE.

IF APPLICABLE, PROVIDE TREE PROTECTION FENCING PRIOR TO CONSTRUCTION OPERATIONS MAINTAIN THROUGHOUT CONSTRUCTION.

ALL LIGHT POLES TO BE REMOVED FROM PRIVATE PROPERTY SHALL BE REMOVED IN THEIR ENTIRETY, INCLUDING BASE AND ALL APPURTENANCES. COORDINATE ABANDONMENT OF ELECTRICAL ENERGINET LECTRICAL ENGINEER AND OWNER PRIOR TO DEMOLITON.

8. CONTRACTOR SHALL OBTAIN ANY NECESSARY DEMOLITION AND UTILITY PLUGGING PERMITS.

9. THE LOCATION OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THE PLANS HAS BEEN DETERMINED FROM THE BEST AVAILABLE INFORMATION AND IS GIVEN FOR THE CONVENIENCE OF THE CONTRACTOR. THE OWNER AND THE ENGINEER DO NOT ASSUME RESPONSIBILITY IN THE EVENT THAT DURING CONSTRUCTION, UTILITIES OTHER THAN THOSE SHOWN MAY BE ENCOUNTERED, AND THAT THE ACTUAL LOCATION OF THOSE WHICH ARE SHOWN MAY BE DIFFERENT FROM THE LOCATION AS SHOWN ON THE PLANS.

10. ANY DAMAGE TO THE CITY PAVEMENT, INCLUDING DAMAGE RESULTING FROM CURB REPLACEMENT, MILL REQUIRE RESTORATION IN ACCORDANCE WITH THE CITY ENGINEERING PATCHING CRITERIA.

PAICHING GRIERA. 11. CONTRACTOR SHALL INSTALL TREE PROTECTION FENCING IN THE AREA BETWEEN THE CURB AND SIDEWALK AND EXTEND IT AT LEAST 5 FEET FROM BOTH SIDES OF THE TREE ALONG THE LENGTH OF THE TERRACE. NO EXCAVATION IS PERMITTED WITHIN 5 FEET OF THE OUTSIDE EDGE OF A TREE TRUNK. IF EXCAVATION WITHIN 5 FEET OF ATW TREE IS NECESSARY, CONTRACTOR SHALL CONTACT CITY FORESTRY (266-4816) PRIOR TO RECONTANT ON THE START CONTRACTOR SHALL CONTACT CITY FORESTRY (266-4816) PRIOR TO EXCAVATION TO ASSESS THE IMPACT TO THE TREE AND ROOT SYSTEM. TREE PROVING SHALL BE CORDINATED WITH CITY FORESTRY PRIOR TO THE START OF CONSTRUCTION. TREE PROTECTION SPECIFICATIONS CAN BE FOUND IN SECTION 107.13 OF CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUTION -http://www.cityformdison.com/business/pw/documents/StdSpecs/2018/Part1.pdf. ANY TREE REMOVALS THAT ARE REQUIRED FOR CONSTRUCTION AFTER THE DEVELOPMENT PLAN IS APPROVED WILL REQUIRE AT LEAST 27 HOUR WAIT PERIOD BEFORE A TREE REMOVAL PERMIT CAN BE ISSUED BY FORESTRY, TO NOTFY THE ALDER OF THE CHANGE IN THE PLAN.

12. THE RIGHT-OF-WAY IS THE SOLE JURISDICTION OF THE CITY OF MADISON AND IS SUBJECT TO CHANGE AT ANYTIME PER THE RECOMENTATION/PLAN OF TRAFFIC ENGINEERING AND CITY ENGINEERING DEPTS.

CITY OF MADISON LUA UPDATE

SCOPE DOCUMENTS Drawing Date JUNE 24, 2019

UW CAMPUS HOTEL

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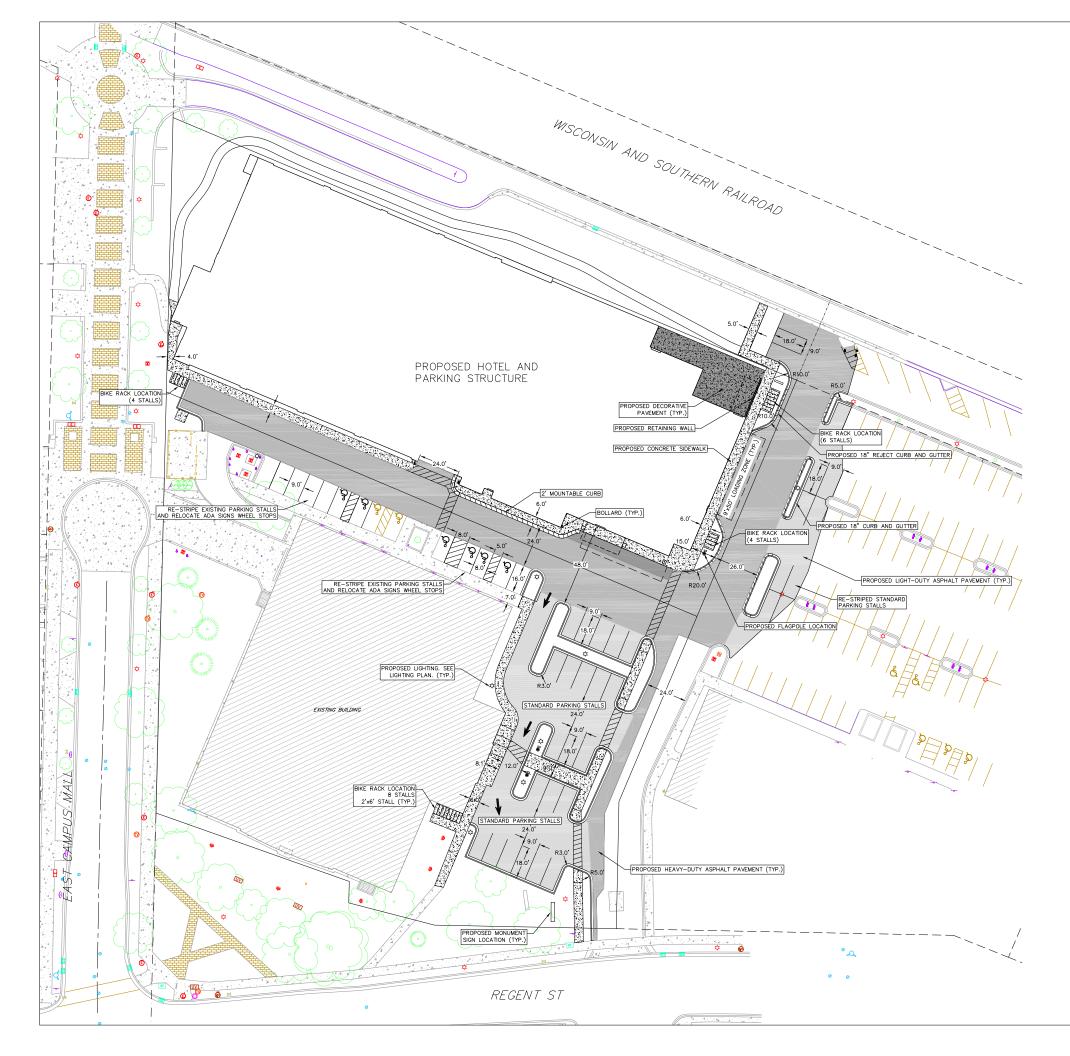
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Sheet Title DEMOLITION PLAN

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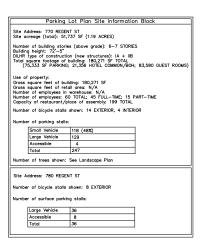
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C101





PROPOSED HANDICAP PARKING



GENERAL NOTES:

- THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE CAUSED DURING CONSTRUCTION TO PUBLIC PROPERTY. PRIVATE PROPERTY OR UTILITES. CONTRACTOR SHALL REPLACE ALL SIDEWALK AND CURB AND GUITER THAT ABUTS THE PROPERTY THAT IS DAMAGED BY THE CONSTRUCTION, OR ANY SIDEWALK AND CURB AND GUITER, WHICH THE CITY ENGINEER DETERMINES NEEDS TO BE REPLACED BECAUSE IT IS NOT AT A DESIRABLE GRADE, REGRADLESS OF WHETHER THE CONDITION EXISTED PRIOR TO BEGINNING CONSTRUCTION.
- 2. THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR REVIEW BY THE ENGINEER, PRIOR TO PLACING AN ORDER OF ANY SUCH ITEM.
- RIGHT OF WAY (ROW) AND PROPERTY LINES ARE APPROXIMATE. CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING PROPERTY CORRER MOUMENTATION. ANY MONUMENTS DISTINGED BY CONTRACTOR SHALL BE REPLACED AT THE CONTRACTORS EXPENSE.
- 4. DIMENSIONS RELATING TO CURB ARE TO FACE OF CURB.
- 5. CROSS-SLOPE OF SIDEWALKS SHALL BE 2% UNLESS OTHERWISE NOTED.
- LONGITUDINAL GRADE OF SIDEWALK RAMPS SHALL NOT EXCEED 8.33% (1:12) AND SHALL BE IN ACCORDANCE WITH ADA REQUIREMENTS.
- LONGITUDINAL GRADE OF SIDEWALK SHALL NOT EXCEED 5.0% OR THE ADJACENT STREET GRADE WHICHEVER IS GREATER.
- ACCESSIBLE ROUTES SHALL BE 5% MAX LONGITUDINAL SLOPE AND 2% MAX CROSS SLOPE. ACCESSIBLE LOADING AREAS OR LANDINGS SHALL BE 2% MAX SLOPE IN ANY DIRECTION. RAMPS SHALL BE 8.33% MAX SLOPE.
- ALL WORK IN THE PUBLIC RIGHT OF WAY SHALL BE PERFORMED BY A CITY-LICENSED CONTRACTOR.
- 10. PROVIDE ALL NECESSARY TEMPORARY TRAFFIC CONTROL PER MUTCD AND CITY OF MADISON REQUIREMENTS.
- THE ACCESS TO THE WESTERNMOST DRIVEWAY ON RECENT STREET SHALL BE ALTERED, FINAL GEOMETRY TO BE DETERMINED BY THE CITY TRAFFIC ENGINEER, WITH THE REDEVELOPMENT OF THE PARCEL(S) TO THE SOUTH SDE OF REGENT STREET.
- THE RIGHT-OF-WAY IS THE SOLE JURISDICTION OF THE CITY OF MADISON AND IS SUBJECT TO CHANGE AT ANYTIME PER THE RECOMENTATION/PLAN OF TRAFFIC ENGINEERING AND CITY ENGINEERING DEPTS.



GRAPHIC SCALE FEET

Revisions

CITY OF MADISON LUA UPDATE

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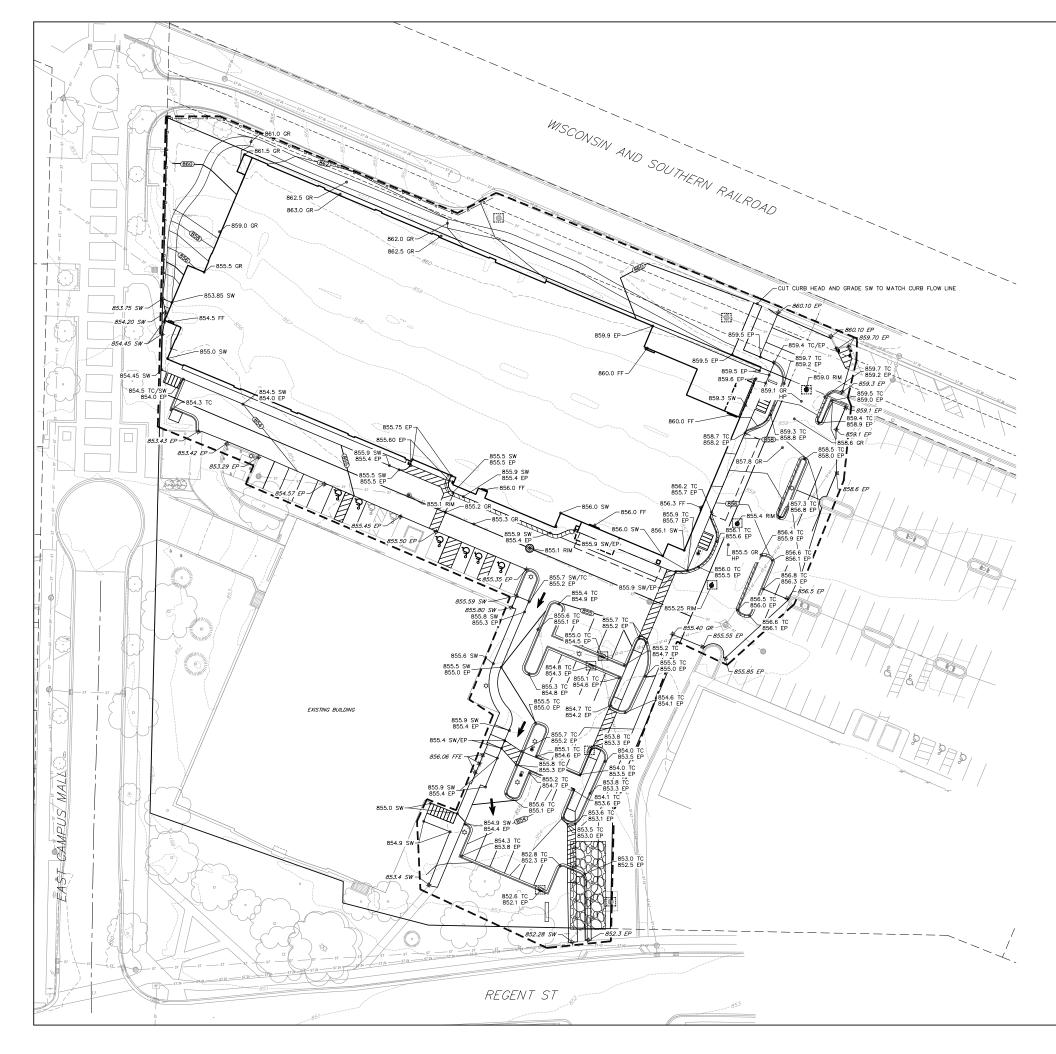
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Sheet Title SITE PLAN

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Sheet No. C200





(GRADING LEGEND	
-	EXISTING MAJOR CONTOURS	1
	EXISTING MINOR CONTOURS	р
	PROPOSED MAJOR CONTOURS	
	PROPOSED MINOR CONTOURS	
	DITCH CENTERLINE	
o	SILT FENCE	
	DISTURBED LIMITS	
	BERM	
\Rightarrow	DRAINAGE DIRECTION	
2.92%	PROPOSED SLOPE ARROWS	
- \$ -1048.61	EXISTING SPOT ELEVATIONS	
e 1048.61	PROPOSED SPOT ELEVATIONS	
	STONE WEEPER	
6333333	VELOCITY CHECK	
Ø	INLET PROTECTION (W/ OIL & GREASE FILTER IN PARKING AREAS)	
	EROSION MAT CLASS I, TYPE A	
	EROSION MAT CLASS II, TYPE B	
222	TRACKING PAD	
	RIP RAP	



GRADING NOTES:

- ALL GRADES SHOWN ARE FINISHED GRADES. CONTOURS ARE SHOWN FOR PURPOSES OF INDICATING ROUGH GRADING. FINAL GRADE SHALL BE ESTABLISHED ON PAVED SURFACES BY USING SPOT GRADES ONLY.
- SEE DETAIL SHEETS FOR EROSION CONTROL NOTES AND CONSTRUCTION SEQUENCE.

ABBREVIATIONS

TC - TOP OF CURB FF - FINISHED FLOOR FL - FLOW LINE SW - TOP OF WALK TW - TOP OF WALK BW - BOTTOM OF WALK EP - EDGE OF PAVEMEN GR - GRADE

- INSTALL A 50'L X 20'W X 1.5'D TRACKING PAD AT THE SITE ENTRANCE. THE TRACKING PAD SHALL BE MAINTAINED/REPAIRED AS NECESSARY TO ACCOMMODATE CONSTRUCTION.
- 4. THE CONTRACTOR IS REQUIRED TO MAKE EROSION CONTROL INSPECTIONS AT THE END OF EACH WEEK AND WHEN 0.5 INCHES OF RAIN FALLS WITHIN 24 HOURS. INSPECTION REPORTS SHALL BE PREPARED AND FILED AS REQUIRED BY THE DNR. ALL MAINTENANCE/REPAR WILL FOLLOW AN INSPECTION WITHIN 24 HOURS.
- 5. INSTALL WI DOT TYPE D INLET PROTECTION IN EXISTING CURB INLETS AND WI DOT TYPE A IN FIELD INLETS.
- 6. THE CONTRACTOR SHALL REMOVE ANY SEDIMENT TRACKED ONTO ADJACENT ROADS BY MEANS OF STREET SWEEPING (NOT FLUSHING) AT A MINIMUM OF THE END OF EACH WORK DAY OR MORE AS NEEDED.
- THE RIGHT-OF-WAY IS THE SOLE JURISDICTION OF THE CITY OF MADISON AND IS SUBJECT TO CHANGE AT ANYTIME PER THE RECOMENTATION/PLAN OF TRAFFIC ENGINEERING AND CITY ENGINEERING DEPTS.

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CITY OF MADISON LUA UPDATE

SCOPE DOCUMENTS Drawing Date JUNE 24, 2019

JUNE 24, 2019

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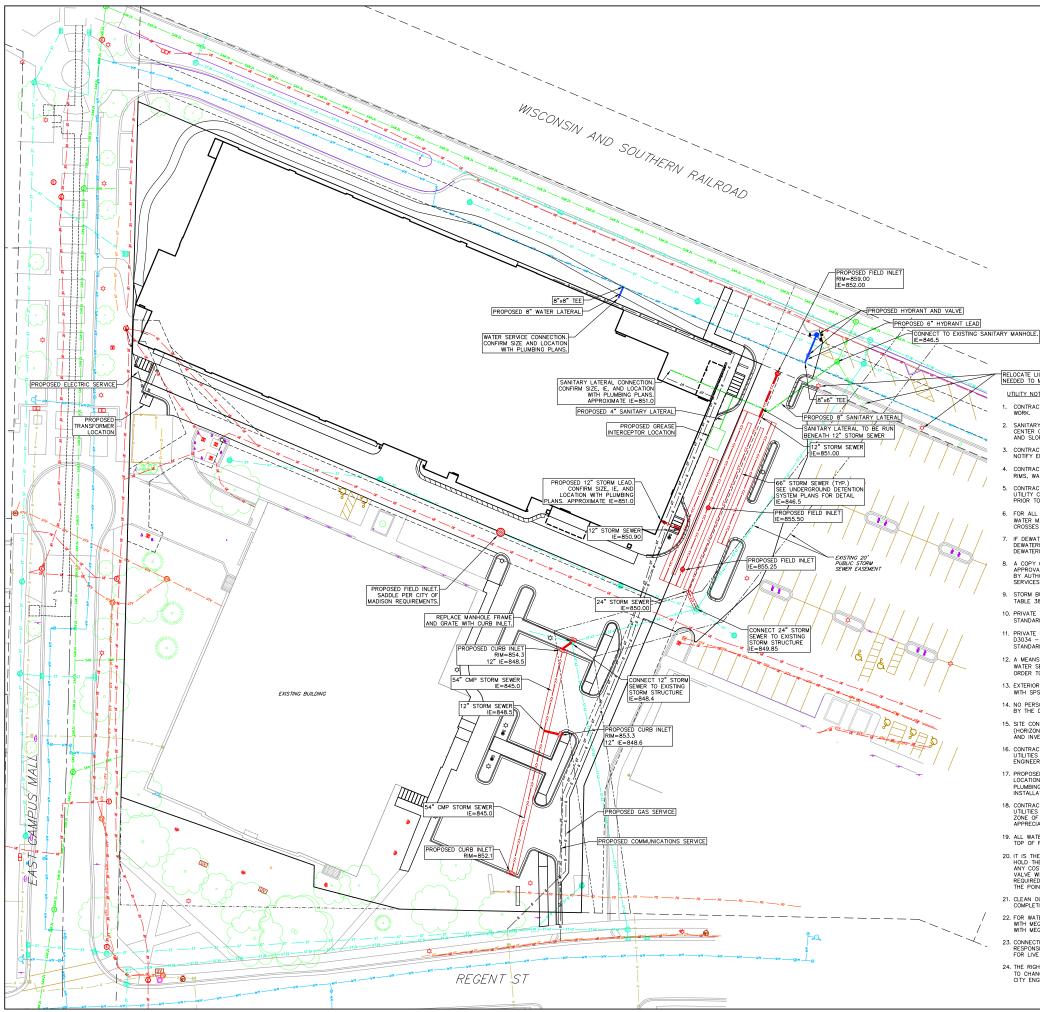
Project No. 218051 Sheet Title

GRADING & EROSION CONTROL PLAN

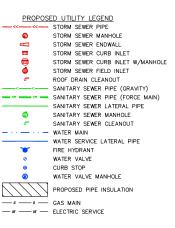
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Sheet No.









RELOCATE LIGHTING/LANDSCAPING AS NEEDED TO MATCH FINISHED GRADES

UTILITY NOTES:

- CONTRACTOR TO VERIFY EXISTING UTILITY LOCATIONS AND ELEVATIONS PRIOR TO STARTING
- SANITARY & STORM SEWER LENGTHS SHOWN ARE FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE. STORM SEWER END SECTIONS ARE INCLUDED IN THE LENGTH AND SLOPE OF THE PIPE.
- CONTRACTOR SHALL INVESTIGATE ALL UTILITY CROSSINGS PRIOR TO CONSTRUCTION AND NOTIFY ENGINEER OF ANY CONFLICTS.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING ALL UTILITY STRUCTURES (MANHOLE RIMS, WATER VALVES, AND CURB STOPS), IF NECESSARY.
- CONTRACTOR SHALL OBTAIN ANY NECESSARY WORK IN RIGHT-OF WAY, EXCAVATION, UTILITY CONNECTION, PLUGGING, ABANDONMENT, AND DRIVEWAY CONNECTION PERMITS PRIOR TO CONSTRUCTION.
- 6. FOR ALL SEWER AND WATER MAIN CROSSINGS: PROVIDE MINIMUM 18" SEPARATION WHEN WATER MAIN CROSSES BELOW SEWER AND MINIMUM 6" SEPARATION WHEN WATER MAIN CROSSES ABOVE SEWER.
- IF DEWATERING OPERATIONS EXCEED 70 GALLONS PER MINUTE OF PUMPING CAPACITY, A DEWATERING WELL PERMIT SHALL BE OBTAINED FROM THE DNR PRIOR TO STARTING ANY DEWATERING ACTIVITIES
- 8. A COPY OF THE APPROVED UTILITY PLANS, SPECIFICATIONS AND PLUMBING PERMIT APPROVAL LETTER SHALL BE ON-SITE DURING CONSTRUCTION AND OPEN TO INSPECTION BY AUTHORIZED REPRESENTATIVES OF THE DEPARTMENT OF SAFETY AND PROFESSIONAL SERVICES AND OTHER LOCAL INSPECTORS.
- STORM BUILDING SEWER PIPE SHALL CONFORM TO ONE OF THE STANDARDS LISTED IN TABLE 384.30-6 OF SPS 384.30(3)(c).
- PRIVATE WATER SERVICES AND PRIVATE WATER MAINS SHALL CONFORM TO ONE OF THE STANDARDS LISTED IN TABLE 384.30-7 OF SPS 384.30(4)(d).
- PRIVATE SANITARY SEWER AND LATERALS SHALL BE POLYVINYL CHLORIDE (PVC) ASTM D3034 SDR 35 OR APPROVED EQUAL MATERIAL THAT CONFORMS TO ONE OF THE STANDARDS LISTED IN TABLE 384.30-3 OF SPS 384.30(2)(c).
- 12. A MEANS TO LOCATE BURIED UNDERGROUND EXTERIOR NON METALLIC SEWERS/MAINS AND WATER SERVICES/MAINS MUST BE PROVIDED WITH TRACER WIRE OR OTHER METHODS IN ORDER TO BE LOCATED PER SPS 382.10(11)(h) AND SPS 382.40(8)(k).
- EXTERIOR WATER SUPPLY PIPING SETBACKS AND CROSSINGS SHALL BE IN ACCORDANCE WITH SPS 382.40(8)(b.).
- 14. NO PERSON MAY ENGAGE IN PLUMBING WORK IN THE STATE UNLESS LICENSED TO DO SO BY THE DEPARTMENT OF SAFETY AND PROFESSIONAL SERVICES PER \$.145.06.
- 15. SITE CONTRACTOR SHALL LEAVE SANITARY AND WATER LATERALS FIVE (5) FEET SHORT (HORIZONTALLY) FROM THE BUILDING. BUILDING PLUMBER SHALL VERIFY SIZE, LOCATION, AND INVERT ELEVATION OF PROPOSED SANITARY AND WATER LATERALS.
- CONTRACTOR SHALL FIELD VERIFY THE SIZE, TYPE, LOCATION, AND ELEVATION OF EXISTING UTILITIES PRIOR TO INSTALLING ANY ON-SITE UTILITIES OR STRUCTURES. CONTACT ENGINEER PRIOR TO INSTALLATION IF DISCREPANCY SAITS WITHIN THESE PLANS.
- 17. PROPOSED UTILITY SERVICE LINES SHOWN ARE APPROXIMATE. COORDINATE THE EXACT LOCATIONS WITH THE PLUMBING DRAWINGS. COORDINATE THE LOCATIONS WITH THE PLUMBING CONTRACTOR AND/OR OWNER'S CONSTRUCTION REPRESENTATIVE PRIOR TO INSTALLATION OF ANY NEW UTILITIES.
- 18. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING THE RELOCATION OF ANY UTILITIES ENCOUNTERED AND REPLACEMENT OF ANY UTILITIES DAMAGED WITHIN INFLUENCE ZONE OF NEW CONSTRUCTION. CONTACT ENGINEER IF THE EXISTING UTILITIES VARY APPRECIABLY FROM THE PLANS.
- 19. ALL WATER MAIN AND SERVICES SHALL BE INSTALLED AT A MINIMUM DEPTH OF 6.5' FROM TOP OF FINISHED GROUND ELEVATION TO TOP OF MAIN.
- 20. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THAT THE EXISTING VALVES WILL HOLD THE PRESSURE TEST PRIOR TO CONNECTION. THE CITY IS NOT RESPONSIBLE FOR ANY COSTS INCURRED DUE TO THE CONTRACTOR NOT VERIFYING THAT THE EXISTING VALVE WILL HOLD THE PRESSURE TEST PRIOR TO CONNECTION. IF A NEW VALVE IS REQUIRED, THE APPLICANT WILL BE REQUIRED TO INSTALL ONE AT THEIR EXPENSE, AT THE POINT OF CONNECTION.
- 21. CLEAN OUT ALL EXISTING AND PROPOSED STORM INLETS AND CATCH BASINS AT THE COMPLETION OF CONSTRUCTION.
- 22. FOR WATER SERVICES AND HYDRANT LEADS, ALL MECHANICAL JOINTS TO BE RESTRAINED WITH MEGALUG 1100 OR APPROVED EQUAL. PIPE JOINTS TO BE RESTRAINED AS INDICATED WITH MEGALUG 1700 HARNESS OR APPROVED EQUAL.
- 23. CONNECTIONS TO EXISTING WATER MAIN TO BE BY LIVE TAPPING. CONTRACTOR IS RESPONSIBLE FOR ALL CONNECTION FEES AND COSTS PAYABLE TO THE CITY OF MADISON FOR LIVE TAPPING, AND ALL OTHER WORK AND COSTS ASSOCIATED WITH LIVE TAPPING.
- 24. THE RIGHT-OF-WAY IS THE SOLE JURISDICTION OF THE CITY OF MADISON AND IS SUBJECT TO CHANGE AT ANYTIME PER THE RECOMENTATION/PLAN OF TRAFFIC ENGINEERING AND CITY ENGINEERING DEPTS.



Revisions

CITY OF MADISON LUA UPDATE

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UW CAMPUS HOTEL

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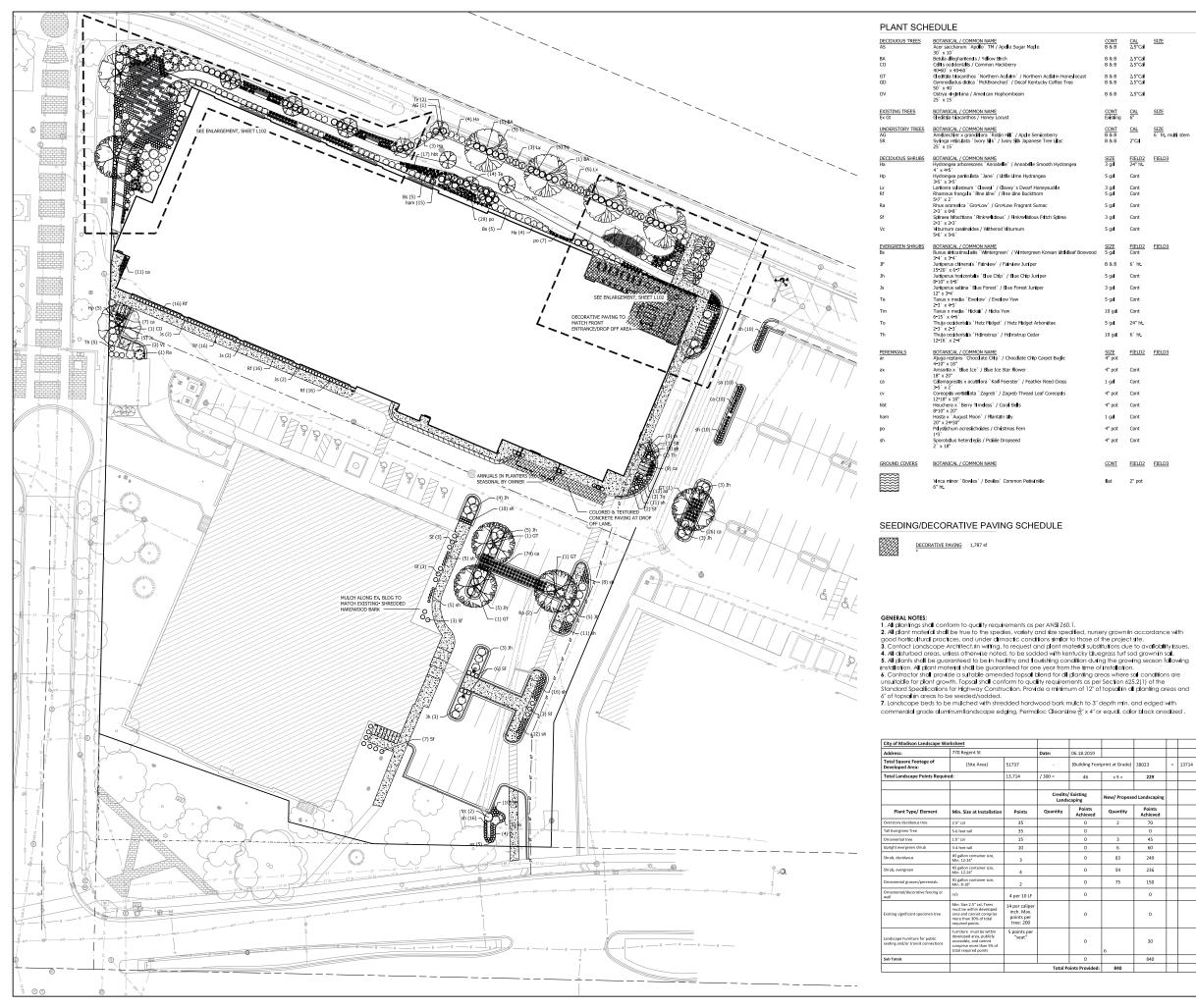
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UTILITY PLAN

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C400



Apc il o Sugar Maple	CONT B & B	CAL 2.5"Ca	SIZE		QTY 5
Irch ackberry	B & B B & B	2.5"Ca 2.5"Ca			4 1
Acd aim` / Northern Acd aim Honeylocust hed` / Decaf Kentucky Coffee Tree	B & B B & B	2 5"Ca 2 5"Ca			5 1
phombeam	B & B	2.5"Ca			3
cust	<u>CONT</u> Existing	CAL 6"	SIZE		QTY 2
in HII" / Apple Serviceberry Ivory SIIk Japanese Tree Lilac	CONT B & B B & B	<u>CAL</u> 2"Ca	SIZE 6'ht. mu lti stem		<u>QTY</u> 6 3
elle` / Annabelle Smooth Hydrangea	SIZE 3 ga	FIELD2 24" ht.	FIELD3		<u>QTY</u> 33
Uttle Ume Hydrangea	5 ga	Cont			20
Clavey`s Dwarf Honeysuckle 'fine Line Buckthorn	3 ga 5 ga	Cont Cont			18 64
o-Low Fragrant Sumac	5 ga	Cont			13
us' / Fink-a lid ous Fritch Spirea	3 ga	Cont			27
d Wburnum	5 ga	Cont			9
een` / Wintergreen Korean littleleaf Boxwood	<u>SIZE</u> 5 ga	FIELD2 Cont	FIELD3		<u>QTY</u> 25
/ Fairview Juniper	B & B	6`ht.			3
p` / Blue Chip Juniper	5 ga	Cont			58
/ Elue Forest Juniper	3 ga	Cont			20
low Yew	5 ga	Cont			70
Yew	10 ga	Cont			2
' / Hetz Midget Arbonitae	5 ga	24" ht.			14
/ Holmstrup Cedar	10 ga	6`ht.			8
/ Chocolate Chip Carpet Bugle	<u>SIZE</u> 4" pot	FIELD2	FIELD3		<u>QTY</u> 62
Star Hower	4" pot	Cont			110
Foerster' / Feather Reed Grass	1 ga	Cont			227
Zagreb Thread Leaf Coreops	4" pot	Cont			7
Coral Bells	4" pot	Cont			47
in Lily	1 ga	Cont			52
istmas Fern	4" pot	Cont			130
Dropseed	4" pot	Cont			317
	CONT	FIELD2	FIELD3	SPACING	QTY
`Common Periwinkle	flat	2" pot		12" o.c.	275 sf



commercial grade aluminum landscape edging, Permaloc CleanLine $\frac{3}{16}$ x 4" or equal, color black anoaized .

		Date:	06.18.2019							
	51737	-	(Building Footprint at Grade)		38023	=	13714	sf		
	13,714	/ 300 =	46	× 5 =	229					
		Credits/ Lands			d Landscaping					
on	Points	Quantity	Points Achieved	Quantity	Points Achieved					
	35		0	2	70					
	35		0		0					
	15		0	3	45					
	10		0	6	60					
	3		0	83	249					
	4		0	59	236					
	2		0	75	150					
	4 per 10 LF		0		0					
	14 per caliper inch. Max. points per tree: 200		0		0					
ŗ	5 points per "seat"		0	6	30					
			0		840					
	Total Points Provided: 840									





Revisions

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JUNE 24, 2019

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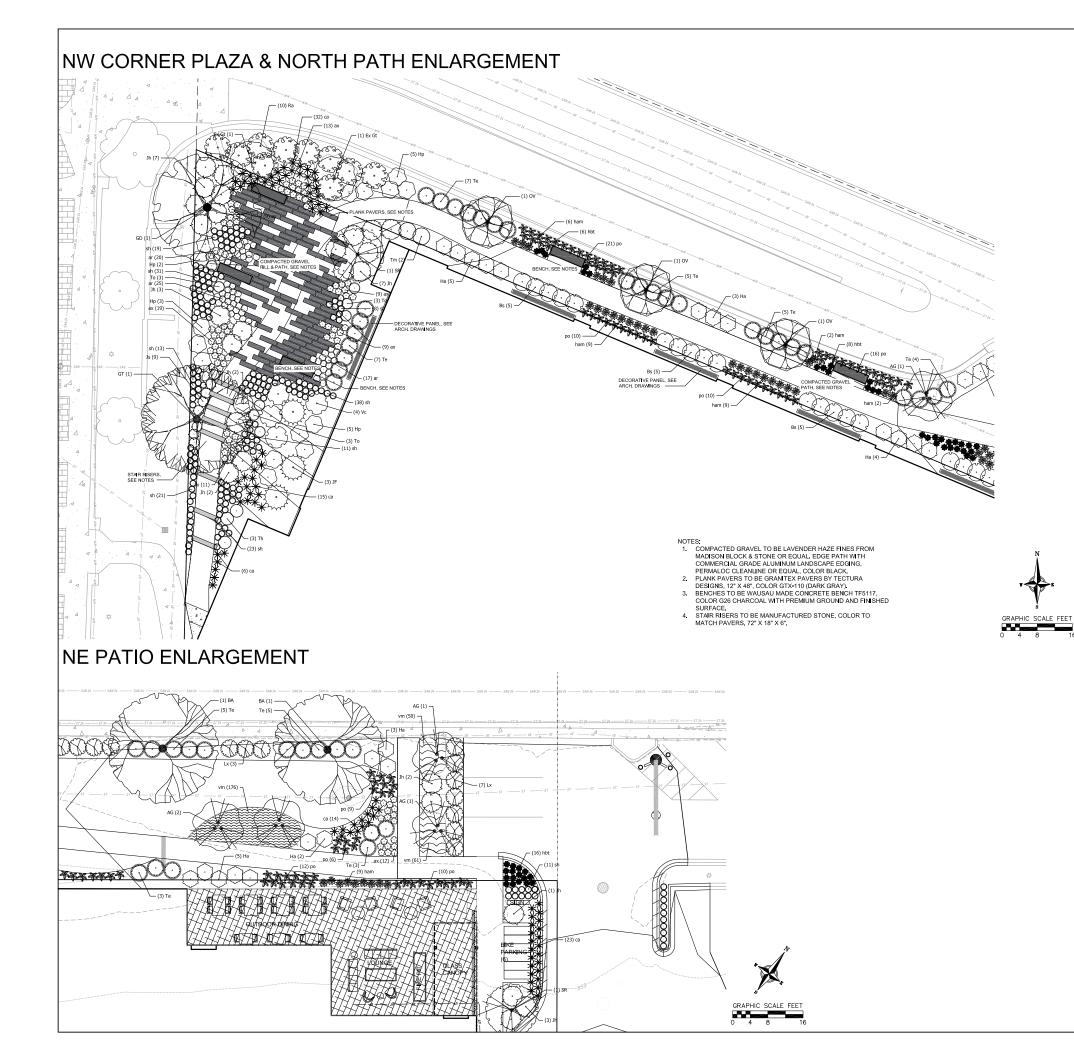
LANDSCAPE PLAN

Morten

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CITY OF MADISON LUA UPDATE

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Sheet Tite LANDSCAPE DETAIL

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