



































TOPOGRAPHIC LINEWORK LEGEND

- | | |
|---|---|
|  | EXISTING FIBER OPTIC LINE |
|  | EXISTING OVERHEAD TELEPHONE LINE |
|  | EXISTING UNDERGROUND TELEPHONE |
|  | EXISTING CHAIN LINK FENCE |
|  | EXISTING GENERAL FENCE |
|  | EXISTING GAS LINE |
|  | EXISTING GUY LINE |
|  | EXISTING OVERHEAD ELECTRIC LINE |
|  | EXISTING SANITARY FORCE MAIN (SIZE NOTED) |
|  | EXISTING SANITARY SEWER LINE (SIZE NOTED) |
|  | EXISTING STORM SEWER LINE (SIZE NOTED) |
|  | EXISTING EDGE OF TREES |
|  | EXISTING WATER MAIN (SIZE NOTED) |
| | EXISTING MAJOR CONTOUR |
| | EXISTING MINOR CONTOUR |

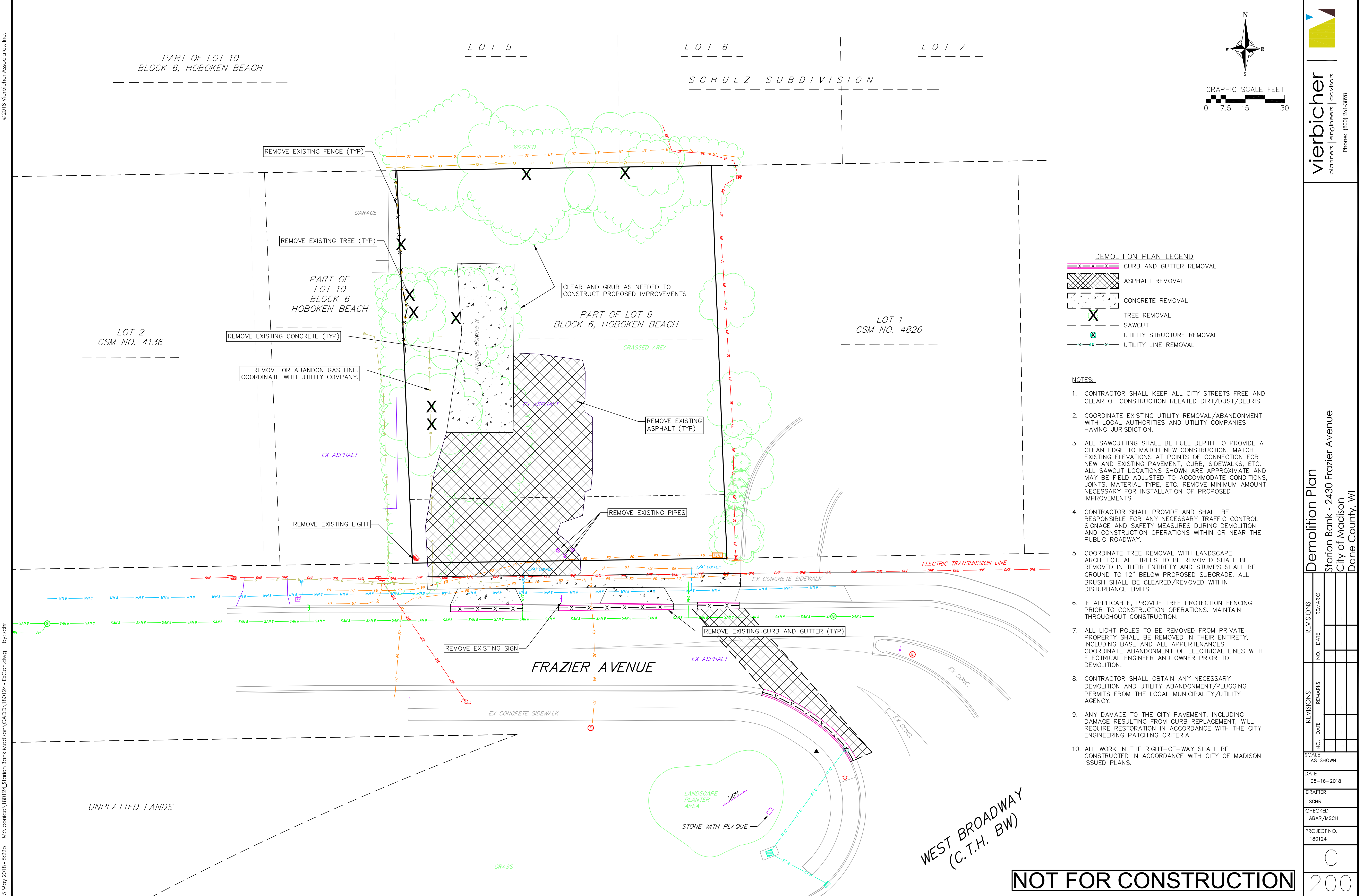
TOPOGRAPHIC SYMBOL LEGEND

-  EXISTING MAILBOX
-  EXISTING POST
-  EXISTING SIGN (TYPE NOTED)
-  EXISTING CURB INLET
-  EXISTING FIELD INLET RECTANGULAR
-  EXISTING SANITARY MANHOLE
-  EXISTING FIRE HYDRANT
-  EXISTING WATER MAIN VALVE
-  EXISTING CURB STOP
-  EXISTING GAS METER
-  EXISTING AIR CONDITIONING PEDESTAL
-  EXISTING DOWN GUY
-  EXISTING ELECTRIC MANHOLE
-  EXISTING ELECTRIC PEDESTAL
-  EXISTING TRANSFORMER
-  EXISTING LIGHT POLE
-  EXISTING GENERIC LIGHT
-  EXISTING UTILITY POLE
-  EXISTING TELEPHONE VAULT
-  EXISTING CONIFEROUS TREE
-  EXISTING DECIDUOUS TREE

NOTES:

- 1) Field work for this map was performed on April 2nd and completed on April 9, 2018. Any changes in site conditions after April 9, 2018 are not reflected on this map.
- 2) All underground utilities have been located per markings placed on the ground or maps provided by the utilities (owners) and/or their authorized representatives. Markings are per Digger's Hotline Ticket #20181303463. Verbiether does not warrant the locations marked or mapped by others.
- 3) This map is referenced to the Wisconsin County Coordinate System, Dane Zone NAD 83 (2007). Elevations are referenced to North American Vertical Datum NAVD 88 (1991). Field data was obtained using Robotic Total Station and GPS.
- 4) The property lines shown on this map are based on found property corners, existing surveys of record and the recorded Hoboken Beach plat.
- 5) Sizes shown for underground utilities are from the City GTWeb site. Contractor to verify sizes at time of construction.
- 6) This existing conditions map was prepared at the request of Curt Walth, Executive Vice President, Starion Bank, 333 N. 4th Street, Bismarck, ND 58501

NOT FOR CONSTRUCTION



15 May 2018 - 5:22p M:\nortica\180124\Station Bank Madison\CADD\180124 - ExCon.dwg by: sctr



vierbicher
planners | engineers | advisors
Phone: (800) 261-3898

Demolition Plan

Station Bank - 2430 Frazier Avenue
City of Madison
Dane County, WI

REVISIONS		REVISIONS	
NO.	DATE	NO.	DATE

SCALE
AS SHOWN

DATE
05-16-2018

DRAFTER
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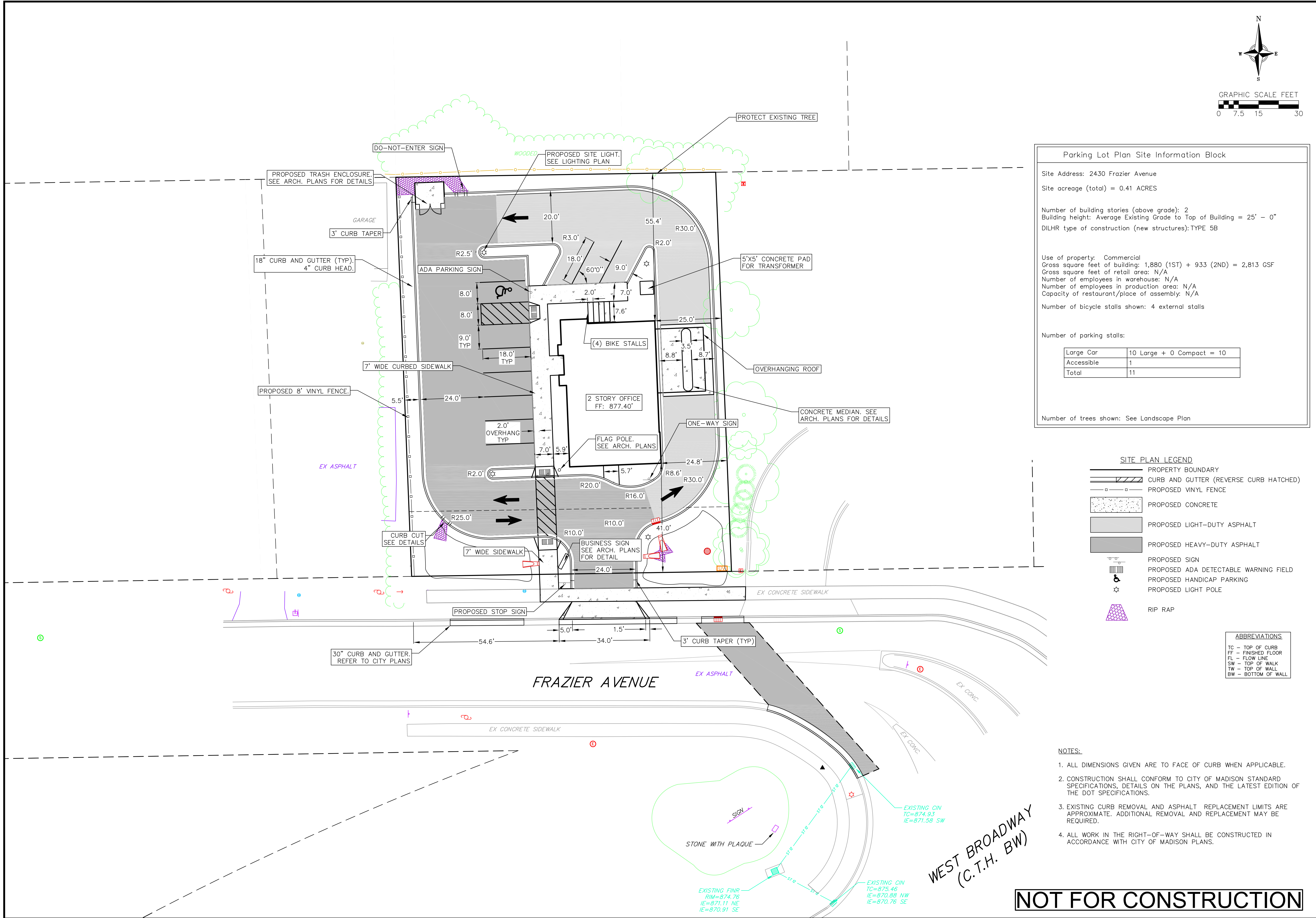
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PROJECT NO.
180124

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NOT FOR CONSTRUCTION



Parking Lot Plan Site Information Block

Site Address: 2430 Frazier Avenue

Site acreage (total) = 0.41 ACRES

Number of building stories (above grade): 2

Building height: Average Existing Grade to Top of Building = 25' - 0"

DILHR type of construction (new structures):TYPE 5B

Use of property: Commercial

Gross square feet of building: 1,880 (1ST) + 933 (2ND) = 2,813 GSF

Gross square feet of retail area: N/A

Number of employees in warehouse: N/A

Number of employees in production area: N/A

Capacity of restaurant/place of assembly: N/A

Number of bicycle stalls shown: 4 external stalls

Number of parking stalls:

Large Car	10 Large + 0 Compact = 10
Accessible	1
Total	11

Number of trees shown: See Landscape Plan

SITE PLAN LEGEND

PROPERTY BOUNDARY

CURB AND GUTTER (REVERSE CURB HATCHED)

PROPOSED VINYL FENCE

PROPOSED CONCRETE

PROPOSED LIGHT-DUTY ASPHALT

PROPOSED HEAVY-DUTY ASPHALT

PROPOSED SIGN

PROPOSED ADA DETECTABLE WARNING FIELD

PROPOSED HANDICAP PARKING

PROPOSED LIGHT POLE

RIP RAP

ABBREVIATIONS

TC = TOP OF CURB

FF = FINISHED FLOOR

FL = FLOW LINE

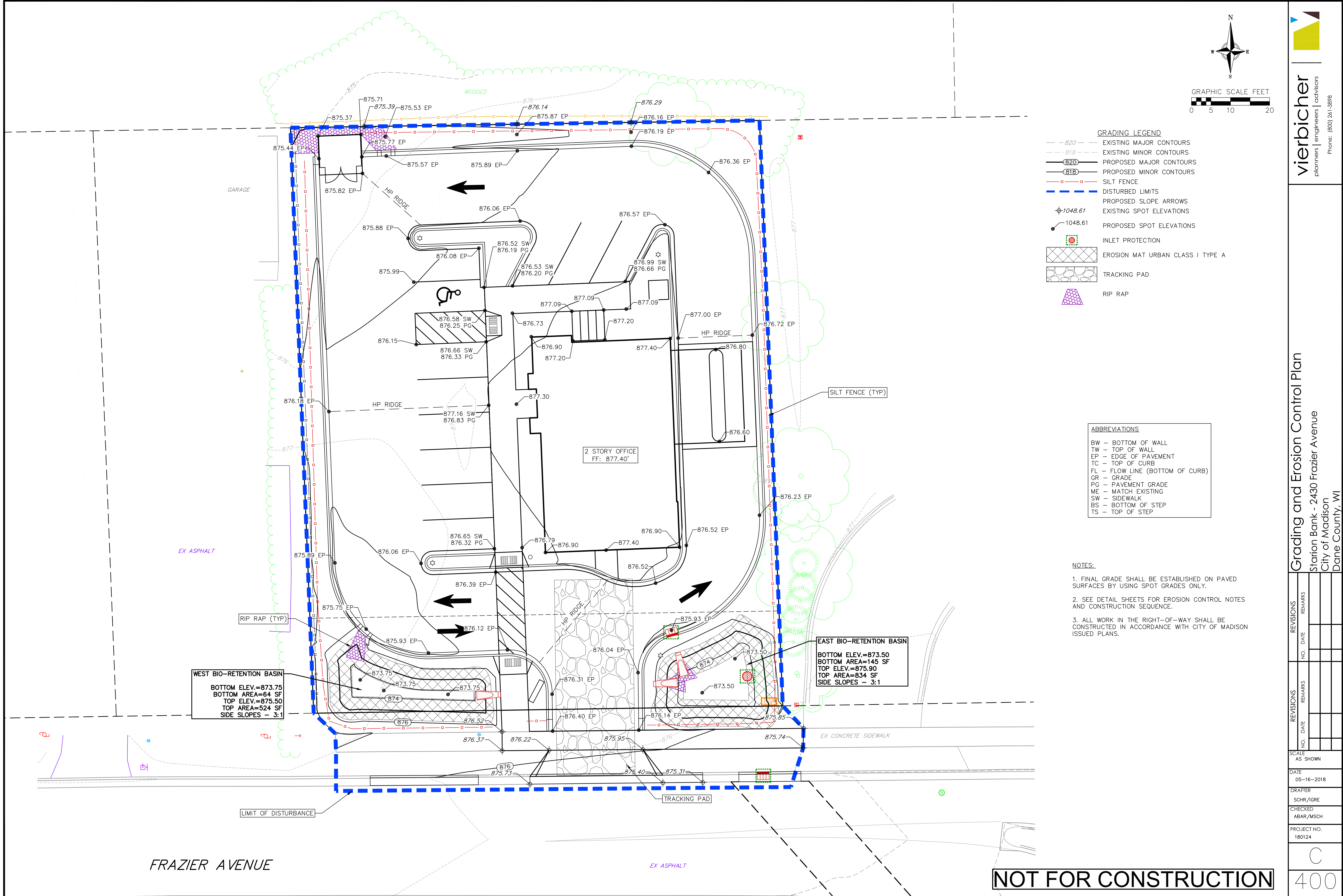
SW = TOP OF WALK

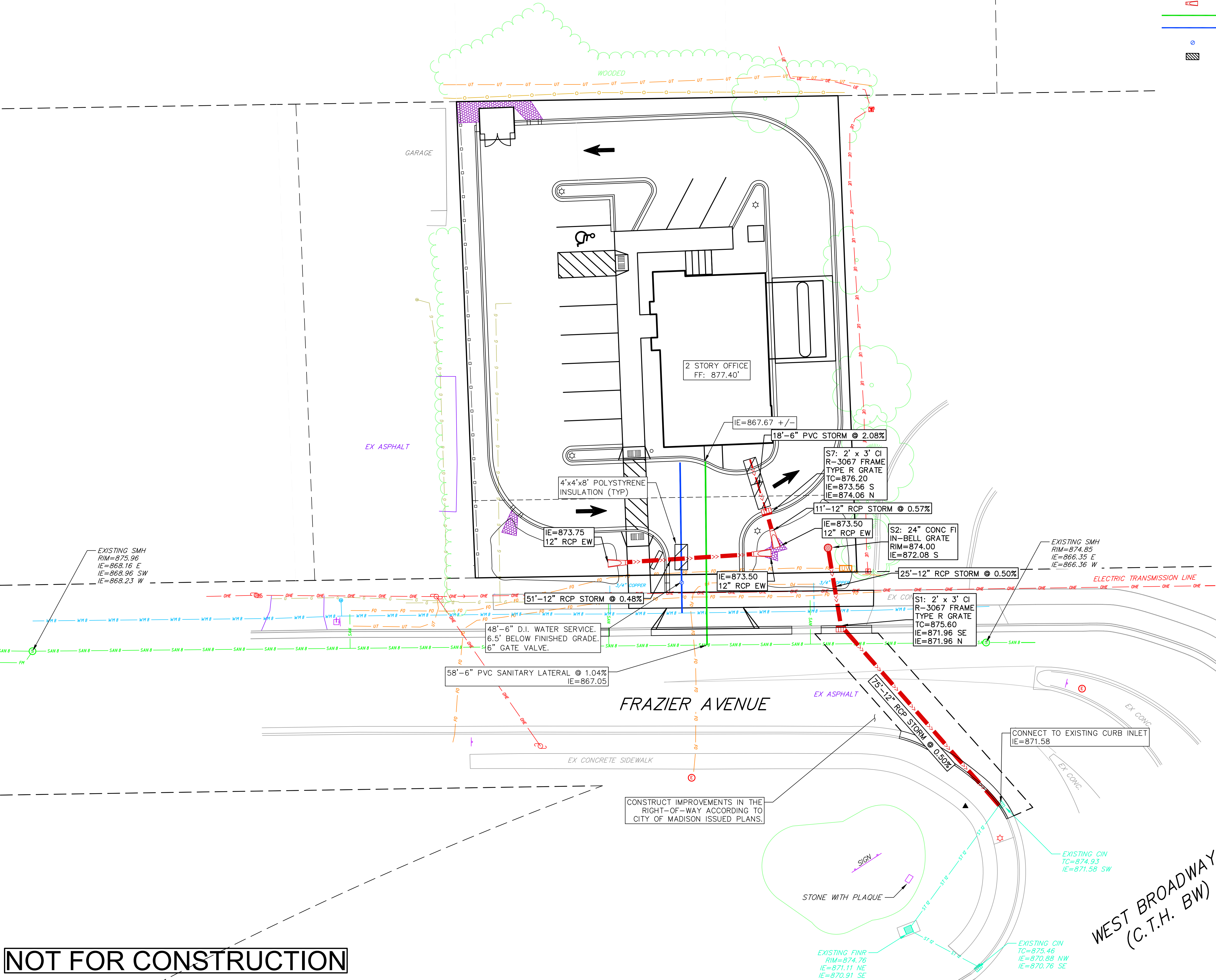
TW = TOP OF WALL

BW = BOTTOM OF WALL

- NOTES:
1. ALL DIMENSIONS GIVEN ARE TO FACE OF CURB WHEN APPLICABLE.
2. CONSTRUCTION SHALL CONFORM TO CITY OF MADISON STANDARD SPECIFICATIONS, DETAILS ON THE PLANS, AND THE LATEST EDITION OF THE DOT SPECIFICATIONS.
3. EXISTING CURB REMOVAL AND ASPHALT REPLACEMENT LIMITS ARE APPROXIMATE. ADDITIONAL REMOVAL AND REPLACEMENT MAY BE REQUIRED.
4. ALL WORK IN THE RIGHT-OF-WAY SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY OF MADISON PLANS.

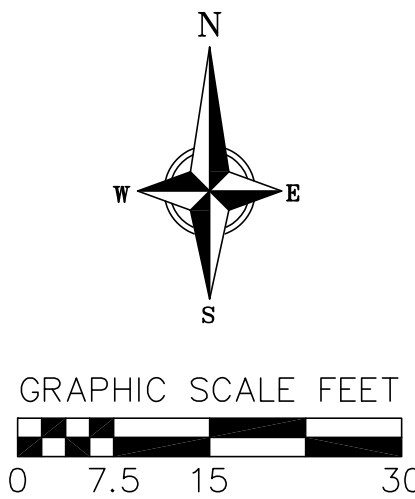
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PROPOSED UTILITY LEGEND

- STORM SEWER PIPE
- STORM SEWER CURB INLET
- STORM SEWER FIELD INLET
- STORM SEWER CLEANOUT
- STORM SEWER ENDWALL
- SANITARY SEWER LATERAL PIPE
- WATER SERVICE LATERAL PIPE
- WATER VALVE
- POLYSTYRENE INSULATION



ABBREVIATIONS	
STMH	STORM MANHOLE
FI	FIELD INLET
CI	CURB INLET
CB	CATCH BASIN
EW	ENDWALL
SMH	SANITARY MANHOLE

NOTES:

- SANITARY & STORM SEWER LENGTHS SHOWN ARE FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE. STORM SEWER END SECTIONS ARE INCLUDED IN THE LENGTH AND SLOPE OF THE PIPE.
- CONTRACTOR SHALL INVESTIGATE ALL UTILITY CROSSINGS PRIOR TO CONSTRUCTION AND NOTIFY ENGINEER OF ANY CONFLICTS.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING ALL UTILITY STRUCTURES (MANHOLE RIMS, WATER VALVES, AND CURB STOPS), IF NECESSARY.
- CONTRACTOR SHALL OBTAIN ANY NECESSARY WORK IN RIGHT-OF WAY, EXCAVATION, UTILITY CONNECTION, PLUGGING, ABANDONMENT, AND DRIVEWAY CONNECTION PERMITS PRIOR TO CONSTRUCTION.
- IF DEWATERING OPERATIONS EXCEED 70 GALLONS PER MINUTE OF PUMPING CAPACITY, A DEWATERING WELL PERMIT SHALL BE OBTAINED FROM THE DNR PRIOR TO STARTING ANY DEWATERING ACTIVITIES.
- A COPY OF THE APPROVED UTILITY PLANS, SPECIFICATIONS AND PLUMBING PERMIT APPROVAL LETTER SHALL BE ON-SITE DURING CONSTRUCTION AND OPEN TO INSPECTION BY AUTHORIZED REPRESENTATIVES OF THE DEPARTMENT OF SAFETY AND PROFESSIONAL SERVICES AND OTHER LOCAL INSPECTORS.
- STORM BUILDING SEWER PIPE SHALL CONFORM TO ONE OF THE STANDARDS LISTED IN TABLE 384.30-6 OF SPS 384.30(3)(c).
- PRIVATE WATER SERVICES AND PRIVATE WATER MAINS SHALL CONFORM TO ONE OF THE STANDARDS LISTED IN TABLE 384.30-7 OF SPS 384.30(4)(d).
- PRIVATE SANITARY SEWER AND LATERALS SHALL BE POLYVINYL CHLORIDE (PVC) ASTM D3034 - SDR 35 OR APPROVED EQUAL MATERIAL THAT CONFORMS TO ONE OF THE STANDARDS LISTED IN TABLE 384.30-3 OF SPS 384.30(2)(c).
- A MEANS TO LOCATE BURIED UNDERGROUND EXTERIOR NON METALLIC SEWERS/MAINS AND WATER SERVICES/MAINS MUST BE PROVIDED WITH TRACER WIRE OR OTHER METHODS IN ORDER TO BE LOCATED PER SPS 382.10(11)(h) AND SPS 382.40(8)(k).
- EXTERIOR WATER SUPPLY PIPING SETBACKS AND CROSSINGS SHALL BE IN ACCORDANCE WITH SPS 382.40(8)(b.).
- NO PERSON MAY ENGAGE IN PLUMBING WORK IN THE STATE UNLESS LICENSED TO DO SO BY THE DEPARTMENT OF SAFETY AND PROFESSIONAL SERVICES PER S.145.06.
- SITE CONTRACTOR SHALL LEAVE SANITARY AND WATER LATERALS FIVE (5) FEET SHORT (HORIZONTALLY) FROM THE BUILDING. BUILDING PLUMBER SHALL VERIFY SIZE, LOCATION, AND INVERT ELEVATION OF PROPOSED SANITARY AND WATER LATERALS.
- CONTRACTOR SHALL FIELD VERIFY THE SIZE, TYPE, LOCATION, AND ELEVATION OF EXISTING UTILITIES PRIOR TO INSTALLING ANY ON-SITE UTILITIES OR STRUCTURES. CONTACT ENGINEER PRIOR TO INSTALLATION IF DISCREPANCY EXISTS WITHIN THESE PLANS.
- PROPOSED UTILITY SERVICE LINES SHOWN ARE APPROXIMATE. COORDINATE THE EXACT LOCATIONS WITH THE PLUMBING DRAWINGS. COORDINATE THE SIZE, INVERTS, AND LOCATIONS WITH THE PLUMBING CONTRACTOR AND/OR OWNER'S CONSTRUCTION REPRESENTATIVE PRIOR TO INSTALLATION OF ANY NEW UTILITIES.
- CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING THE RELOCATION OF ANY UTILITIES ENCOUNTERED AND REPLACEMENT OF ANY UTILITIES DAMAGED WITHIN INFLUENCE ZONE OF NEW CONSTRUCTION. CONTACT ENGINEER IF THE EXISTING UTILITIES VARY APPRECIABLY FROM THE PLANS.
- ALL WATER MAIN AND SERVICES SHALL BE INSTALLED AT A MINIMUM DEPTH OF 6.5' FROM TOP OF FINISHED GROUND ELEVATION TO TOP OF MAIN.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THAT THE EXISTING VALVES WILL HOLD THE PRESSURE TEST PRIOR TO CONNECTION. THE CITY IS NOT RESPONSIBLE FOR ANY COSTS INCURRED DUE TO THE CONTRACTOR NOT VERIFYING THAT THE EXISTING VALVE WILL HOLD THE PRESSURE TEST PRIOR TO CONNECTION. IF A NEW VALVE IS REQUIRED, THE APPLICANT WILL BE REQUIRED TO INSTALL ONE AT THEIR EXPENSE, AT THE POINT OF CONNECTION.
- CLEAN OUT ALL EXISTING AND PROPOSED STORM INLETS AND CATCH BASINS AT THE COMPLETION OF CONSTRUCTION.
- ALL WORK IN THE RIGHT-OF-WAY SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY OF MADISON ISSUED PLANS, PROJECT #-----.

REVISIONS		REVISIONS	
NO.	DATE	NO.	DATE
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DATE 05-16-2018			
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EROSION CONTROL MEASURES

1. EROSION CONTROL SHALL BE IN ACCORDANCE WITH THE CITY OF MADISON EROSION CONTROL ORDINANCE AND CHAPTER NR 216 OF THE WISCONSIN ADMINISTRATIVE CODE.
2. CONSTRUCT AND MAINTAIN ALL EROSION AND SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH WISCONSIN DNR TECHNICAL STANDARDS (<http://dnr.wi.gov/runoff/stormwater/techstds.htm>) AND WISCONSIN CONSTRUCTION SITE BEST MANAGEMENT PRACTICE HANDBOOK.
3. INSTALL SEDIMENT CONTROL PRACTICES (TRACKING PAD, PERIMETER SILT FENCE, SEDIMENT BASINS, ETC.) PRIOR TO INITIATING OTHER LAND DISTURBING CONSTRUCTION ACTIVITIES.
4. THE CONTRACTOR IS REQUIRED TO MAKE EROSION CONTROL INSPECTIONS AT THE END OF EACH WEEK AND WHEN 0.5 INCHES OF RAIN FALLS WITHIN 24 HOURS. INSPECTION REPORTS SHALL BE PREPARED AND FILED AS REQUIRED BY THE DNR AND/OR CITY. ALL MAINTENANCE WILL FOLLOW AN INSPECTION WITHIN 24 HOURS.
5. EROSION CONTROL IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ACCEPTANCE OF THIS PROJECT. EROSION CONTROL MEASURES AS SHOWN SHALL BE THE MINIMUM PRECAUTIONS THAT WILL BE ALLOWED. ADDITIONAL EROSION CONTROL MEASURES, AS REQUESTED IN WRITING BY THE STATE OR LOCAL INSPECTORS, OR THE DEVELOPER'S ENGINEER, SHALL BE INSTALLED WITHIN 24 HOURS.
6. A 3" CLEAR STONE TRACKING PAD SHALL BE INSTALLED AT THE END OF ROAD CONSTRUCTION LIMITS TO PREVENT SEDIMENT FROM BEING TRACKED ONTO THE ADJACENT PAVED PUBLIC ROADWAY. SEDIMENT TRACKING PAD SHALL CONFORM TO WISDNR TECHNICAL STANDARD 1057. SEDIMENT REACHING THE PUBLIC ROAD SHALL BE REMOVED BY STREET CLEANING (NOT HYDRAULIC FLUSHING) BEFORE THE END OF EACH WORK DAY.
7. CHANNELIZED RUNOFF: FROM ADJACENT AREAS PASSING THROUGH THE SITE SHALL BE DIVERTED AROUND DISTURBED AREAS.
8. STABILIZED DISTURBED GROUND: ANY SOIL OR DIRT PILES WHICH WILL REMAIN IN EXISTENCE FOR MORE THAN 7-CONSECUTIVE DAYS, WHETHER TO BE WORKED DURING THAT PERIOD OR NOT, SHALL NOT BE LOCATED WITHIN 25-FEET OF ANY ROADWAY, PARKING LOT, PAVED AREA, OR DRAINAGE STRUCTURE OR CHANNEL (UNLESS INTENDED TO BE USED AS PART OF THE EROSION CONTROL MEASURES). TEMPORARY STABILIZATION AND CONTROL MEASURES (SEEDING, MULCHING, TARPING, EROSION MATTING, BARRIER FENCING, ETC.) ARE REQUIRED FOR THE PROTECTION OF DISTURBED AREAS AND SOIL PILES, WHICH WILL REMAIN UN-WORKED FOR A PERIOD OF MORE THAN 14-CONSECUTIVE CALENDAR DAYS. THESE MEASURES SHALL REMAIN IN PLACE UNTIL SITE HAS STABILIZED.
9. SITE DE-WATERING: WATER PUMPED FROM THE SITE SHALL BE TREATED BY TEMPORARY SEDIMENTATION BASINS OR OTHER APPROPRIATE CONTROL MEASURES. SEDIMENTATION BASINS SHALL HAVE A DEPTH OF AT LEAST 3 FEET, BE SURROUNDED BY SNOWFENCE OR EQUIVALENT BARRIER AND HAVE SUFFICIENT SURFACE AREA TO PROVIDE A SURFACE SETTLING RATE OF NO MORE THAN 750 GALLONS PER SQUARE FOOT PER DAY AT THE HIGHEST DEWATERING PUMPING RATE. WATER MAY NOT BE DISCHARGED IN A MANNER THAT CAUSES EROSION OF THE SITE, A NEIGHBORING SITE, OR THE BED OR BANKS OF THE RECEIVING WATER. POLYMERS MAY BE USED AS DIRECTED BY DNR TECHNICAL STANDARD 1061 (DE-WATERING).
10. IN NO CASE WILL RIP-RAP BE SMALLER THAN 3" TO 6".
11. INLET FILTERS ARE TO BE PLACED IN STORMWATER INLET STRUCTURES AS SOON AS THEY ARE INSTALLED. ALL PROJECT AREA STORM INLETS NEED WISCONSIN D.O.T. TYPE D INLET PROTECTION. THE FILTERS SHALL BE MAINTAINED UNTIL DISTURBED AREAS ARE PAVED AND VEGETATIVE AREAS ARE 70% ESTABLISHED.
12. RESTORATION (SEED, FERTILIZE AND MULCH) SHALL BE PER SPECIFICATIONS ON THIS SHEET UNLESS SPECIAL RESTORATION IS CALLED FOR ON THE LANDSCAPE PLAN.
13. VEGETATIVE AREAS SHALL BE RESTORED WITH 6" TOPSOIL, PERMANENT SEED, FERTILIZER AND MULCH.
14. SEED, FERTILIZER AND MULCH SHALL BE APPLIED WITHIN 7 DAYS AFTER FINAL GRADE HAS BEEN ESTABLISHED. IF DISTURBED AREAS WILL NOT BE RESTORED IMMEDIATELY AFTER ROUGH GRADING, TEMPORARY SEED SHALL BE PLACED.
15. FOR THE FIRST SIX WEEKS AFTER RESTORATION (E.G. SEED & MULCH, EROSION MAT, SOD) OF A DISTURBED AREA, INCLUDE SUMMER WATERING PROVISIONS OF ALL NEWLY SEEDED AND MULCHED AREAS WHENEVER 7 DAYS ELAPSE WITHOUT A RAIN EVENT.
16. EROSION MAT (CLASS I, TYPE A URBAN PER WISCONSIN D.O.T. P.A.L.) SHALL BE INSTALLED ON ALL SLOPES 3:1 OR GREATER BUT LESS THAN 1:1.
17. SILT FENCE TO BE USED ACROSS AREAS OF THE LOT THAT SLOPE TOWARDS A PUBLIC STREET OR WATERWAY. SEE DETAILS.
18. SEDIMENT SHALL BE CLEANED FROM CURB AND GUTTER AFTER EACH RAINFALL AND PRIOR TO PROJECT ACCEPTANCE.
19. ACCUMULATED CONSTRUCTION SEDIMENT SHALL BE REMOVED FROM ALL PERMANENT BASINS TO THE ELEVATION SHOWN ON THE GRADING PLAN FOLLOWING THE STABILIZATION OF DRAINAGE AREAS.
20. ANY PROPOSED CHANGES TO THE EROSION CONTROL PLAN MUST BE SUBMITTED AND APPROVED BY THE CITY OF MADISON AND DNR.
21. THE CITY, OWNER AND/OR ENGINEER MAY REQUIRE ADDITIONAL EROSION CONTROL MEASURES AT ANY TIME DURING CONSTRUCTION.

SEEDING RATES:

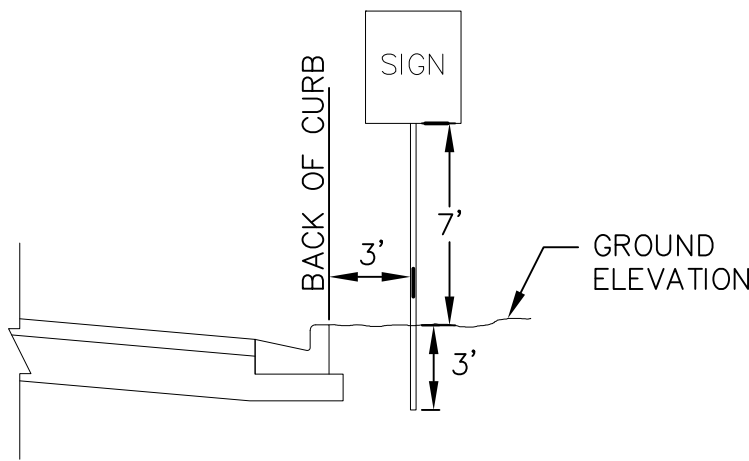
- TEMPORARY:
1. USE ANNUAL OATS AT 3.0 LB./1,000 S.F. FOR SPRING AND SUMMER PLANTINGS.
2. USE WINTER WHEAT OR RYE AT 3.0 LB./1,000 SF FOR FALL PLANTINGS STARTED AFTER SEPTEMBER 15.

- PERMANENT:
1. USE WISCONSIN D.O.T. SEED MIX #40 AT 2 LB./1,000 S.F.
- FERTILIZING RATES:
TEMPORARY AND PERMANENT:
USE WISCONSIN D.O.T. TYPE A OR B AT 7 LB./1,000 S.F.

- MULCHING RATES:
TEMPORARY AND PERMANENT:
USE ½" TO 1-½" STRAW OR HAY MULCH, CRIMPED PER SECTION 607.3.2.3, OR OTHER RATE AND METHOD PER SECTION 627, WISCONSIN D.O.T. STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION

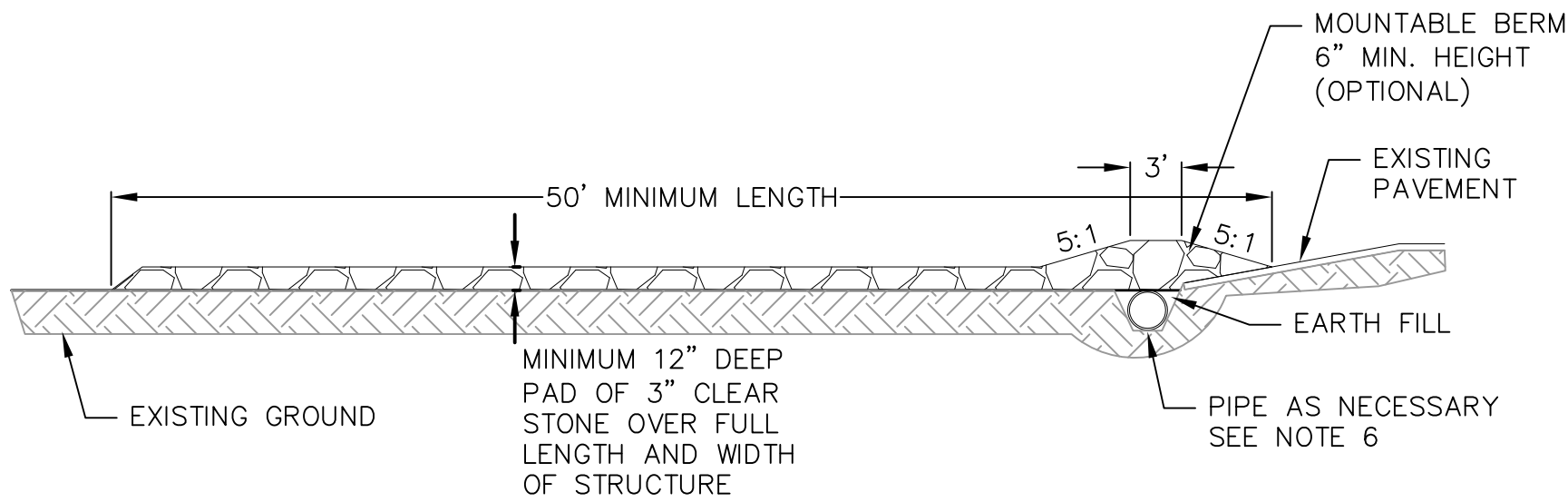
CONSTRUCTION SEQUENCE:

1. INSTALL EROSION CONTROL MEASURES
2. CONDUCT DEMOLITION
3. ROUGH GRADE – BIORETENTION BASINS
4. STRIP TOPSOIL – SITE
5. ROUGH GRADE – SITE
6. CONSTRUCT UNDERGROUND UTILITIES
7. INSTALL INLET PROTECTION IN NEW INLETS
8. CONSTRUCT BUILDING
9. CONSTRUCT RETAINING WALL
10. CONSTRUCT PAVEMENT
11. FINAL GRADE AND RESTORE DISTURBED AREAS
12. FINAL GRADE BIORETENTION BASINS, REMOVE CONSTRUCTION SEDIMENT, PLACE ENGINEERED SOIL AND RESTORE PER PLAN.
13. REMOVE EROSION CONTROL MEASURES AFTER DISTURBED AREAS ARE PAVED AND VEGETATIVE AREAS ARE 70% RESTORED

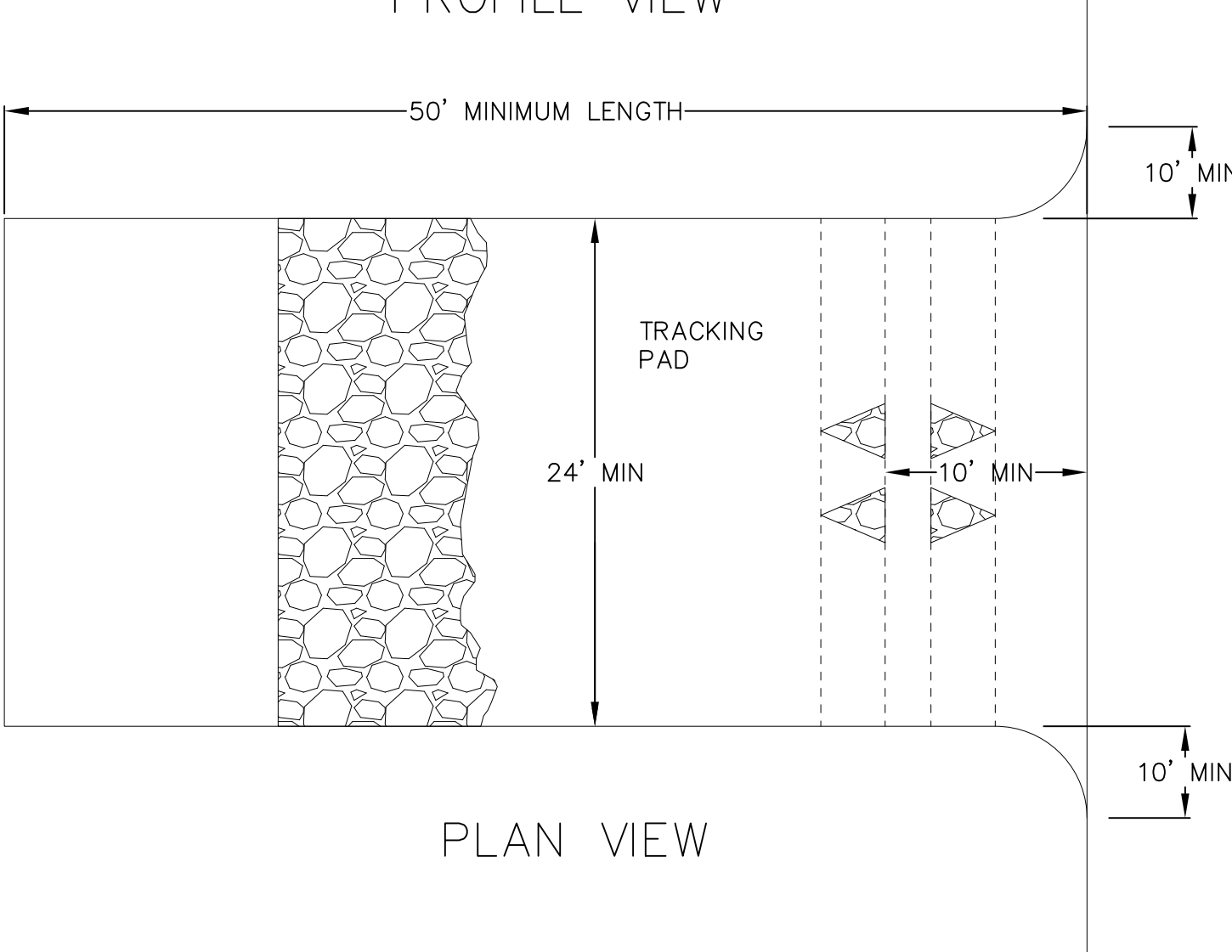


SIGNAGE NOTES:

1. ALL SIGNS SHALL BE FABRICATED AND INSTALLED IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
2. SIGNS SHALL BE A DISTANCE OF 7' FROM GROUND LEVEL TO THE BOTTOM OF THE SIGN MOUNTED ON THE POST AND LOCATED 3' BEHIND THE BACK OF CURB.
3. STREET NAME SIGNS SHALL HAVE WHITE LETTERS AND GREEN BACKGROUND.
4. SIGN POSTS SHALL BE 2-3/8" O.D., GALVANIZED 10 FT LONG, 13 GAUGE, AND 0.095 WALL THICKNESS. MOUNT SIGN AT TOP OF THE POST, AND INSTALL POSTS 3' DEEP AND MIX 1/2 BAG OF 80 LB SAKRETE CONCRETE, POURING IT AROUND THE POST BELOW THE GROUND BEFORE COVERING WITH 8" OF TOPSOIL.
5. LOADING ZONE SIGNS SHALL BE 12"x9" R8-3gP.
6. ONE-WAY SIGNS SHALL BE 18"x24" R6-2.
7. DO NOT ENTER SIGN SHALL BE 30"x30" R5-1.



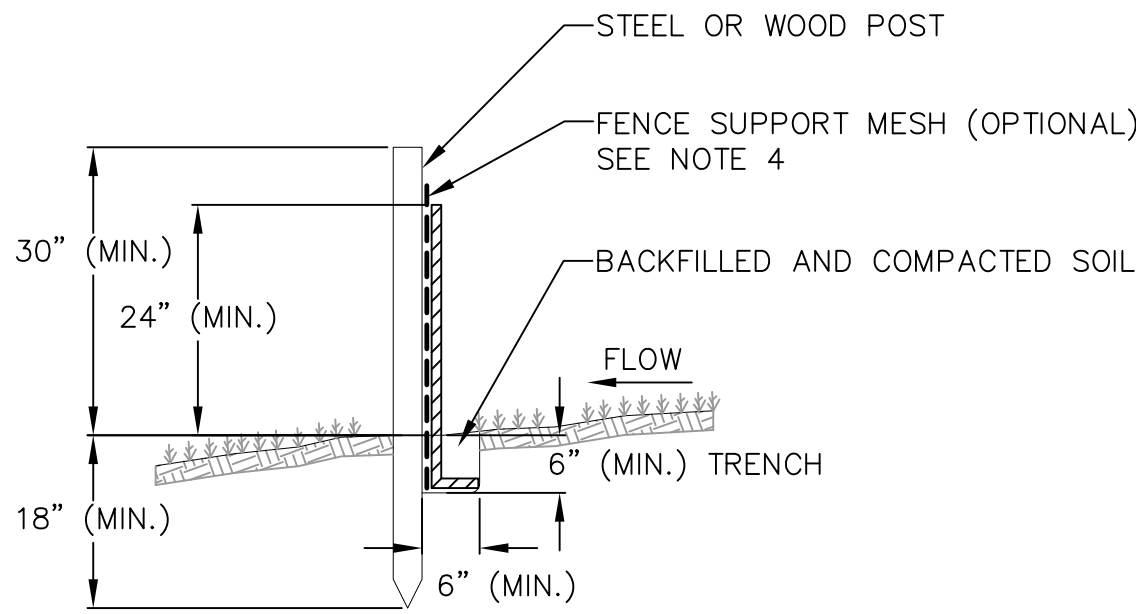
PROFILE VIEW



PLAN VIEW

1. FOLLOW WISCONSIN DNR TECHNICAL STANDARD 1057 FOR FURTHER DETAILS AND INSTALLATION.
2. LENGTH – MINIMUM OF 50'
3. WIDTH – 24' MINIMUM, SHOULD BE FLARED AT THE EXISTING ROAD TO PROVIDE A TURNING RADIUS.
4. ON SITES WITH A HIGH GROUND WATER TABLE OR WHERE SATURATED CONDITIONS EXIST, GEOTEXTILE FABRIC SHALL BE PLACED OVER EXISTING GROUND PRIOR TO PLACING STONE. FABRIC SHALL BE WISDOT TYPE-HR GEOTEXTILE FABRIC.
5. STONE – CRUSHED 3" CLEAR STONE SHALL BE PLACED AT LEAST 12" DEEP OVER THE ENTIRE LENGTH AND WIDTH OF ENTRANCE.
6. SURFACE WATER – ALL SURFACE WATER FLOWING TO OR DIVERTED TOWARDS CONSTRUCTION ENTRANCES SHALL BE PIPED THROUGH THE ENTRANCE. MAINTAINING POSITIVE DRAINAGE. PIPE INSTALLED THROUGH THE STABILIZED CONSTRUCTION ENTRANCE SHALL BE PROTECTED WITH A MOUNTABLE BERM WITH 5:1 SLOPES AND MINIMUM OF 6" STONE OVER THE PIPE. PIPE SHALL BE SIZED ACCORDING TO THE DRAINAGE REQUIREMENTS. WHEN THE ENTRANCE IS LOCATED AT A HIGH SPOT AND HAS NO DRAINAGE TO CONVEY A PIPE SHALL NOT BE NECESSARY. THE MINIMUM PIPE DIAMETER SHALL BE 6". CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF SAID PIPE.
7. LOCATION – A STABILIZED CONSTRUCTION ENTRANCE SHALL BE LOCATED WHERE CONSTRUCTION TRAFFIC ENTERS AND/OR LEAVES THE CONSTRUCTION SITE. VEHICLES LEAVING THE SITE MUST TRAVEL OVER THE ENTIRE LENGTH OF THE TRACKING PAD.

1 TRACKING PAD
1 NOT TO SCALE



NOTES:

1. INSTALL SILT FENCE TO FOLLOW THE GROUND CONTOURS AS CLOSELY AS POSSIBLE.
2. CURVE THE SILT FENCE UP THE SLOPE TO PREVENT WATER FROM RUNNING AROUND THE ENDS.
3. POST SPACING WITH FENCE SUPPORT MESH = 10 FT. (MAX.)
POST SPACING WITHOUT FENCE SUPPORT MESH = 6 FT. (MAX.)
4. SILT FENCE SUPPORT MESH CONSISTS OF 14-GAUGE STEEL WIRE WITH A MESH SPACING OF 6 IN. X 6 IN. OR PREFABRICATED POLYMERIC MESH OF EQUIVALENT STRENGTH

1 SILT FENCE
1 NOT TO SCALE

NOT FOR CONSTRUCTION



vierbicher
planners | engineers | advisors

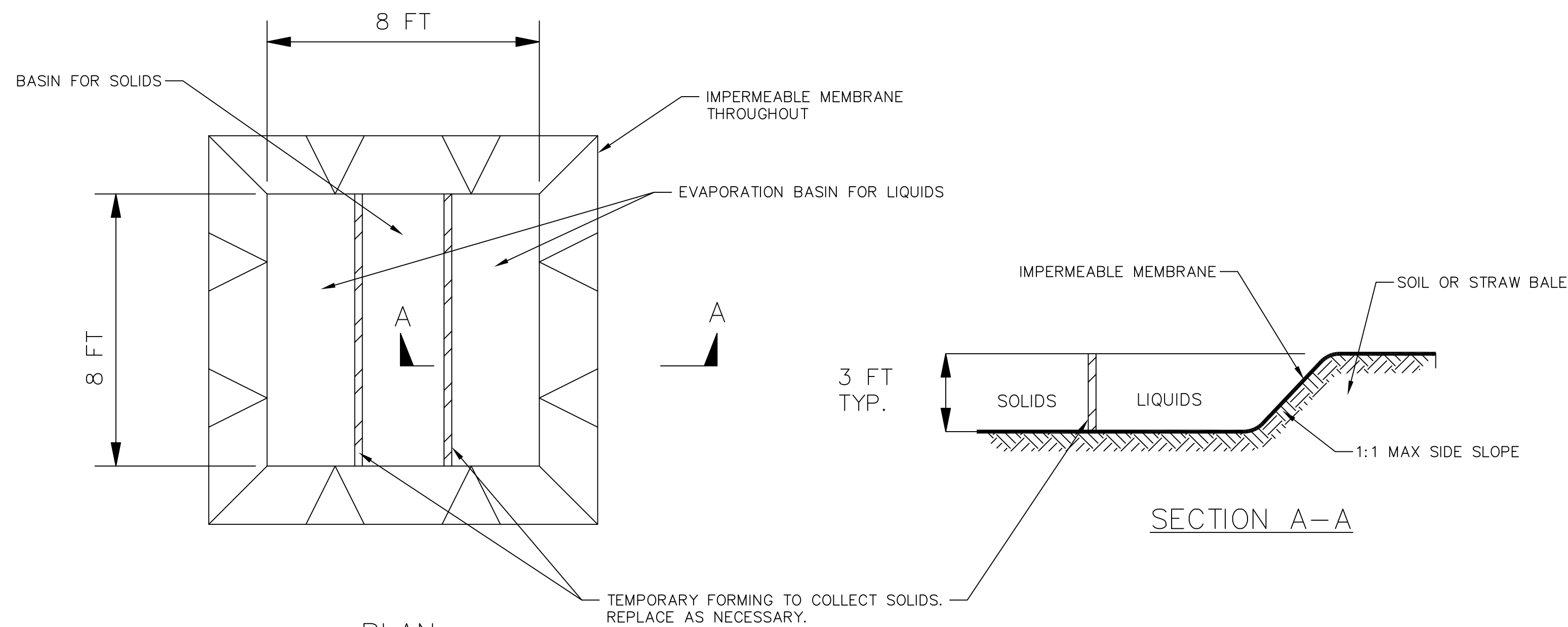
Phone: (800) 261-3898

Construction Details - 1
Station Bank - 2430 Frazier Avenue
City of Madison
Dane County, WI

REVISIONS	NO.	DATE	REMARKS
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SCALE AS SHOWN
DATE 05-16-2018
DRAFTER SCHR
CHECKED ABAR/MSCH
PROJECT NO. 180124

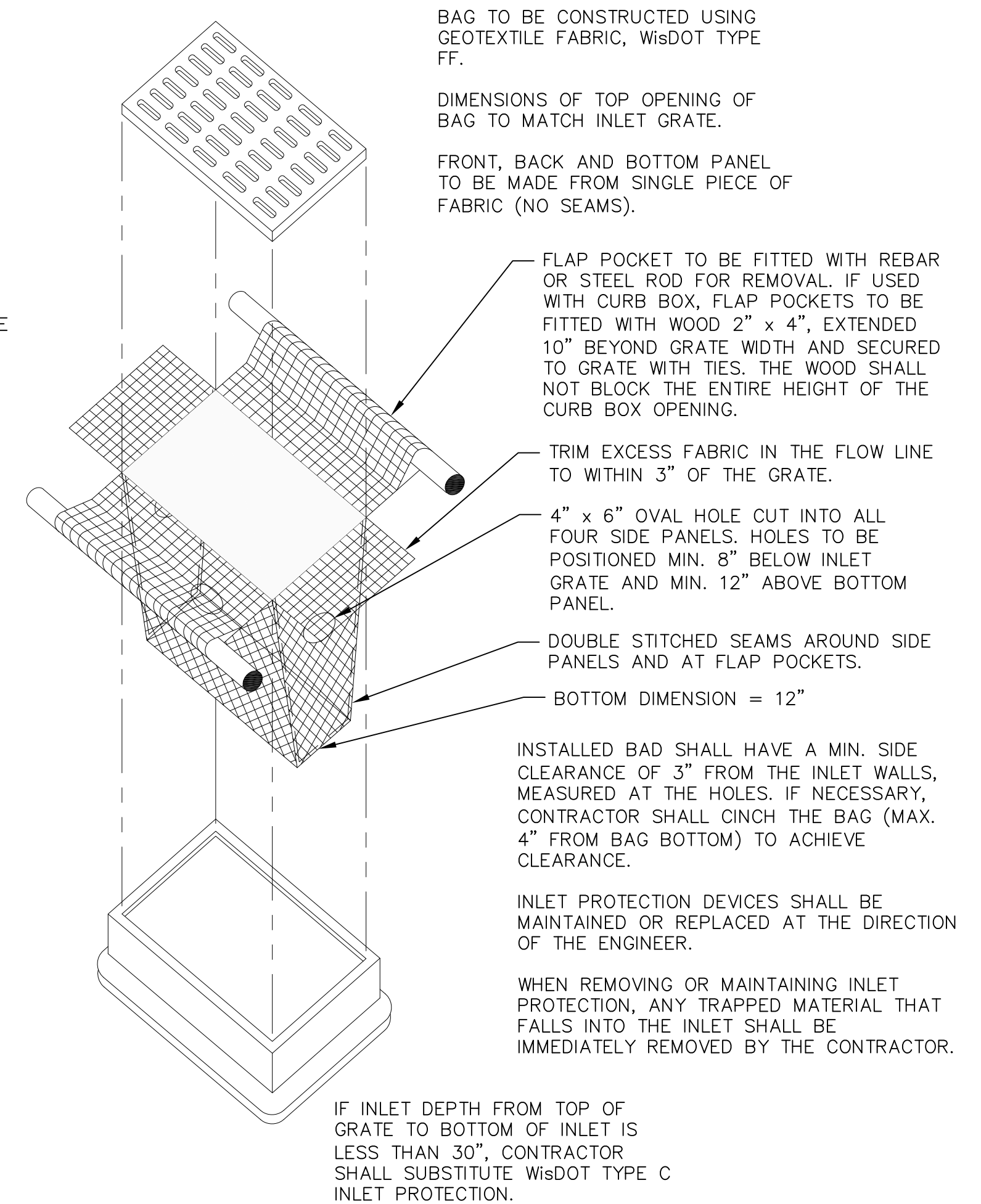
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PLAN

CONSTRUCTION SPECIFICATIONS

1. LOCATE WASHOUT STRUCTURE A MINIMUM OF 50 FEET AWAY FROM OPEN CHANNELS, STORM DRAIN INLETS, SENSITIVE AREAS, WETLANDS, BUFFERS AND WATER COURSES AND AWAY FROM CONSTRUCTION TRAFFIC.
2. PREPARE SOIL BASE FREE OF ROCKS OR OTHER DEBRIS THAT MAY CAUSE TEARS OR HOLES IN THE LINER. FOR LINER, USE 10 MIL OR THICKER UV RESISTANT, IMPERMEABLE SHEETING, FREE OF HOLES AND TEARS OR OTHER DEFECTS THAT COMPROMISE IMPERMEABILITY OF THE MATERIAL.
3. KEEP CONCRETE WASHOUT STRUCTURE WATER TIGHT. REPLACE IMPERMEABLE LINER IF DAMAGED (E.G., RIPPED OR PUNCTURED). EMPTY OR REPLACE WASHOUT STRUCTURE. THIS IS FOR FUEL AND OIL. REMOVE AND DISPOSE OF ACCUMULATED MATERIAL PROPERLY. DO NOT REUSE PLASTIC LINER. MET-VACUUM STORED LIQUIDS THAT HAVE NOT EVAPORATED AND DISPOSE OF IN AN APPROVED MANNER. REMOVE HARDENED SOLIDS, WHOLE OR BROKEN UP, FOR DISPOSAL OR RECYCLING. MAINTAIN RUNOFF DIVERSION AROUND EXCAVATED WASHOUT STRUCTURE UNTIL STRUCTURE IS REMOVED.



1 INLET PROTECTION TYPE D
1 NOT TO SCALE

CertainTeed
Bufftech
Vinyl Fence Products

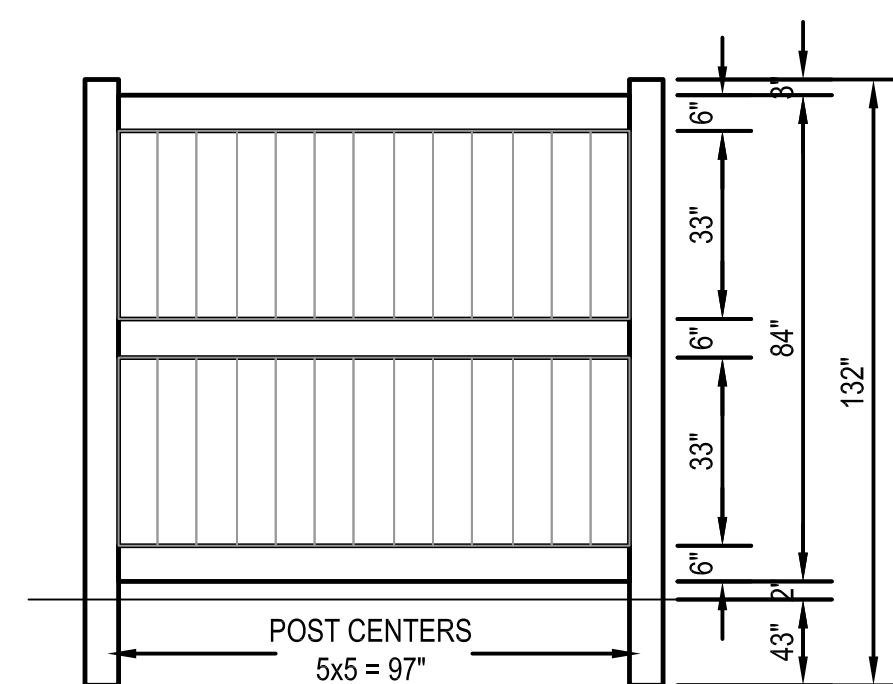
CERTAINTEED CORPORATION FENCE,
DECK & RAIL DIVISION
231 SHIP CANAL PARKWAY
BUFFALO, NY 14218
TOLL FREE:1-800-333-0569
PHONE: (716) 823-3023
FAX: (716) 823-2843
www.certaainteed.com

☐ SELECT DESIRED HEIGHT

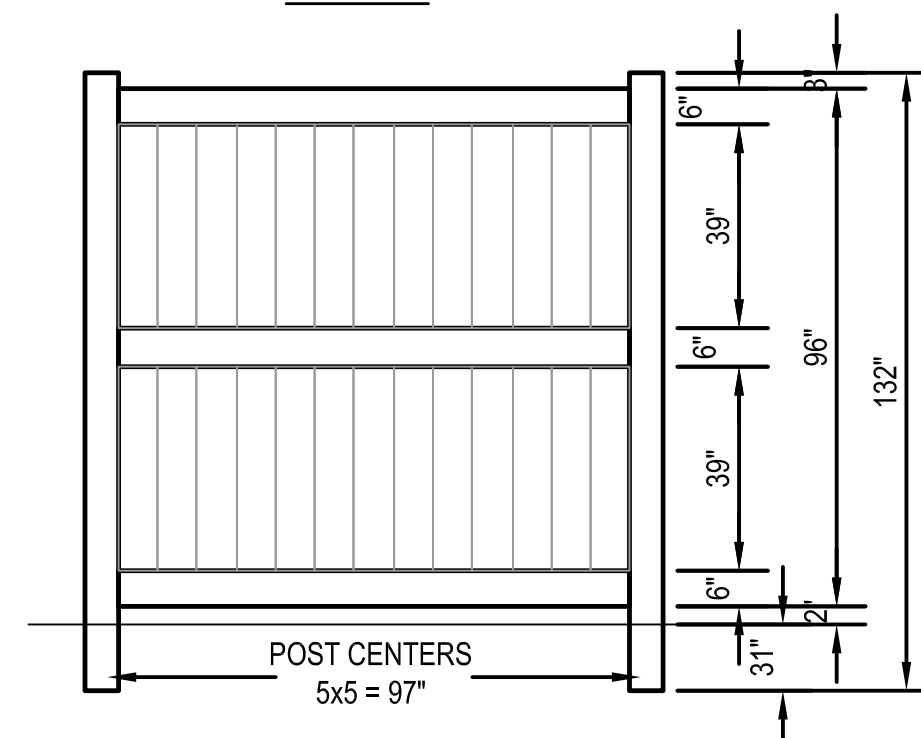
- 7'
8'

☐ SELECT DESIRED COLOR

- ☐ ARBOR BLEND
- ☐ ARCTIC BLEND
- ☐ BRAZILIAN BLEND
- ☐ FRONTIER BLEND
- ☐ HONEY BLEND
- ☐ NATURAL CLAY
- ☐ SIERRA BLEND
- ☐ WEATHERED BLEND



7FT FENCE



8FT FENCE

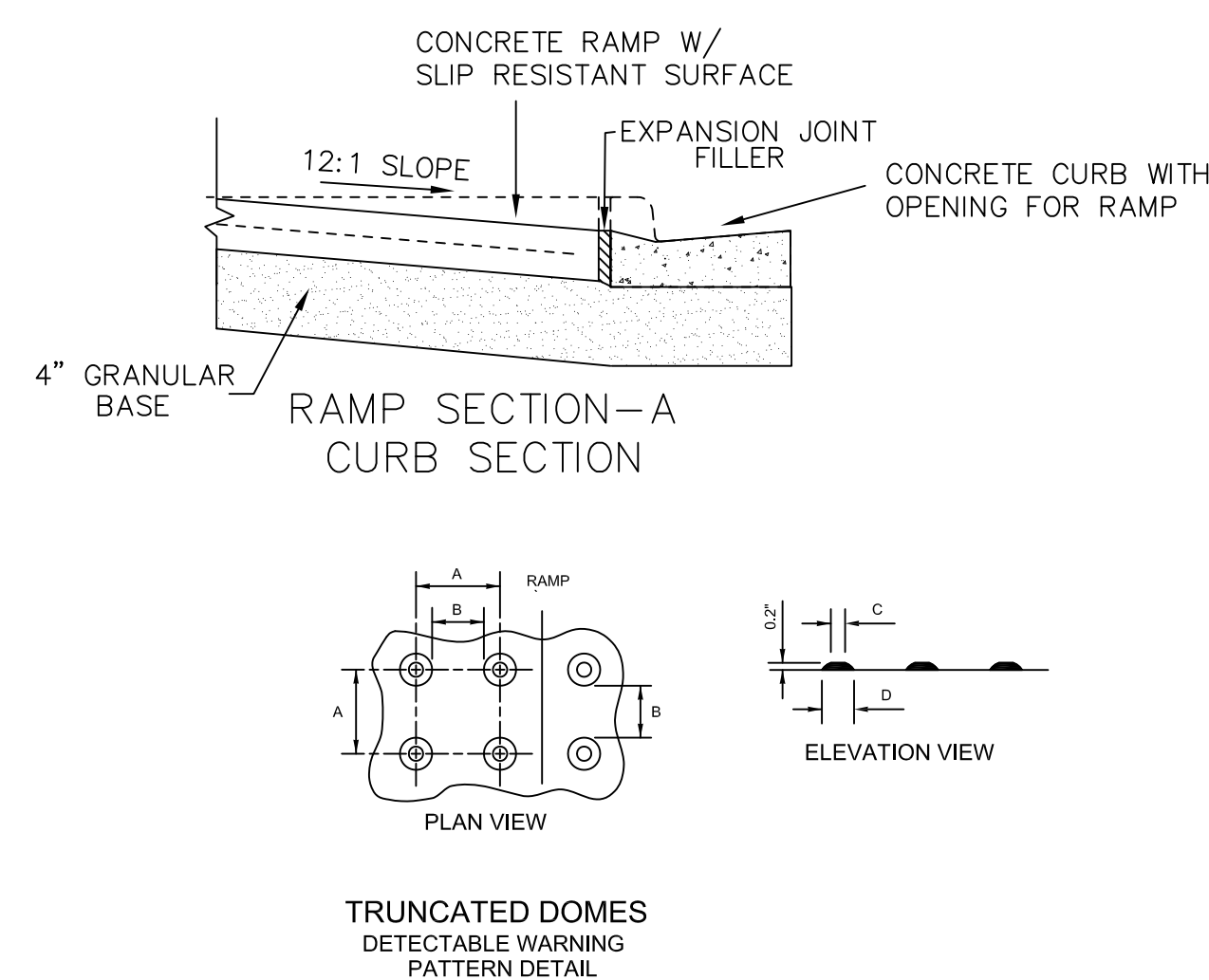
NOTES:

1. INSTALLATION TO BE COMPLETED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.
2. DO NOT SCALE DRAWINGS.
3. CONTRACTORS NOTE: FOR PRODUCT AND COMPANY INFORMATION VISIT www.CADdetails.com/info
REFERENCE NUMBER 035-088.

PROTECTED BY COPYRIGHT ©2013 CADdetails.com LTD.

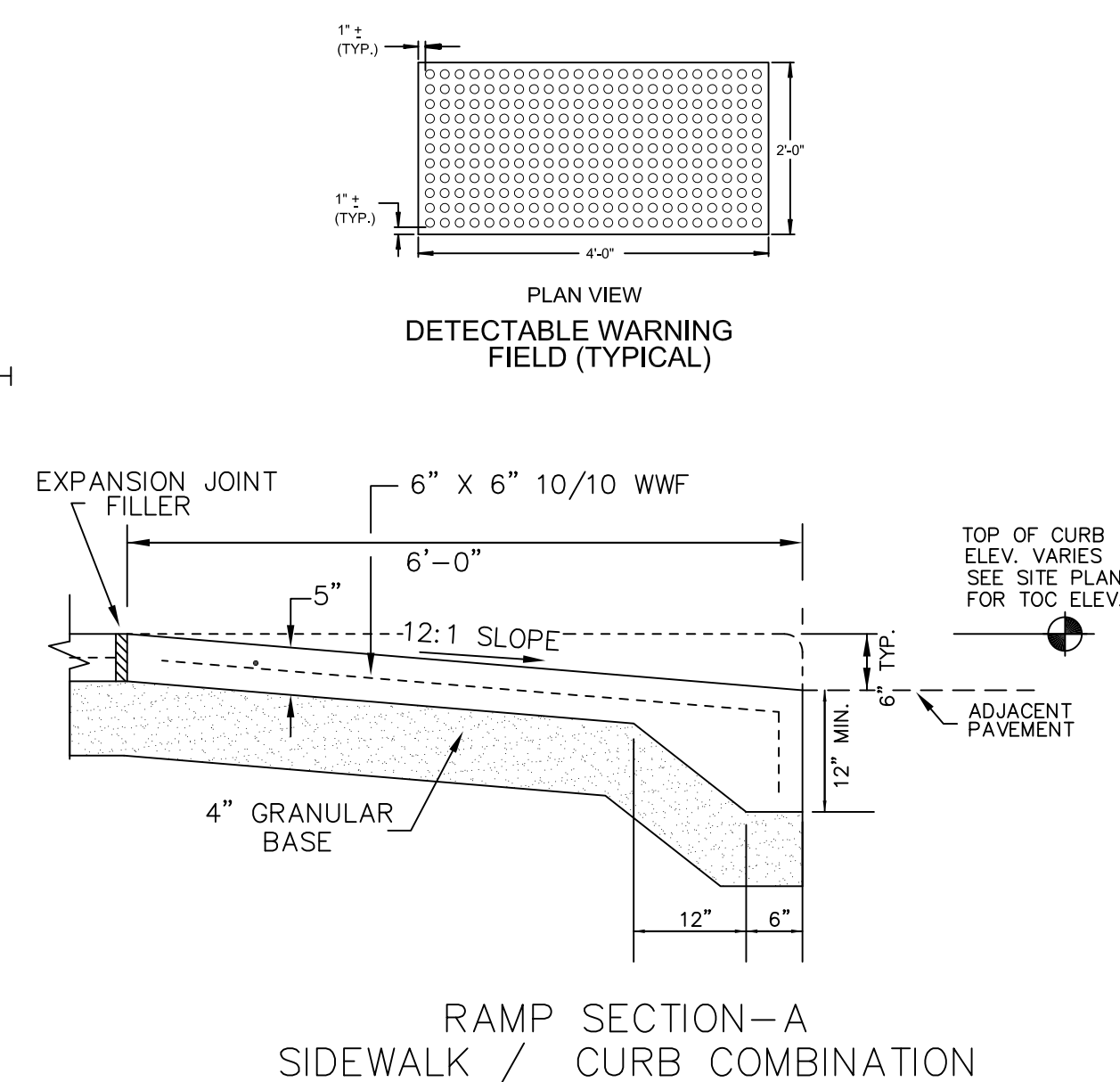
www.CADdetails.com

1 TEMPORARY CONCRETE WASHOUT
1 NOT TO SCALE



NOTE: 1) MATERIALS AND METHOD OF CONSTRUCTION FOR TRUNCATED
DOMES SHALL BE SPECIFIED IN SPECIAL PROVISIONS OR AS REQUIRED BY
THE VILLAGE ENGINEER
2) COLOR SHALL BE A DISSIMILAR COLOR FROM THE
ADJACENT PAVED SURFACE AND APPROVED BY THE OWNER AND
ENGINEER

1 CURB RAMP DETAIL
1 NOT TO SCALE



GENERAL NOTES
RAMPS SHALL BE BUILT AT 12:1 OR FLATTER.
WHEN NECESSARY, THE SIDEWALK ELEVATION
MAY BE LOWERED TO MEET THE HIGH POINT
ON THE RAMP.

NOT FOR CONSTRUCTION

REMARKS		REMARKS	
NO.	DATE	NO.	DATE

SCALE
AS SHOWN

DATE
05-16-2018

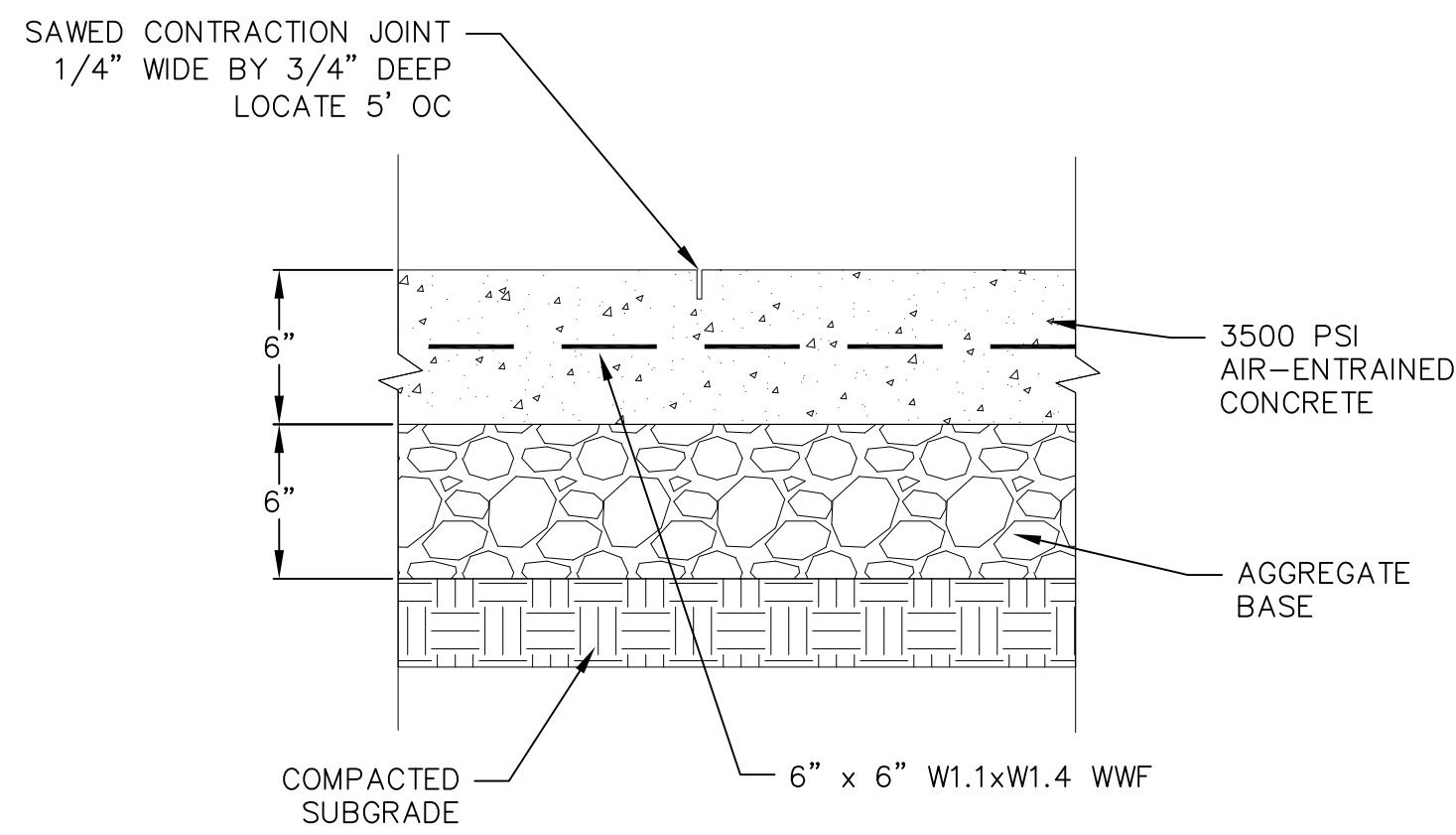
BEFORE	BEFORE
AFTER	AFTER
SCHR	SCHR

CHECKED ABAR/MSCH	DATE 11/1/00
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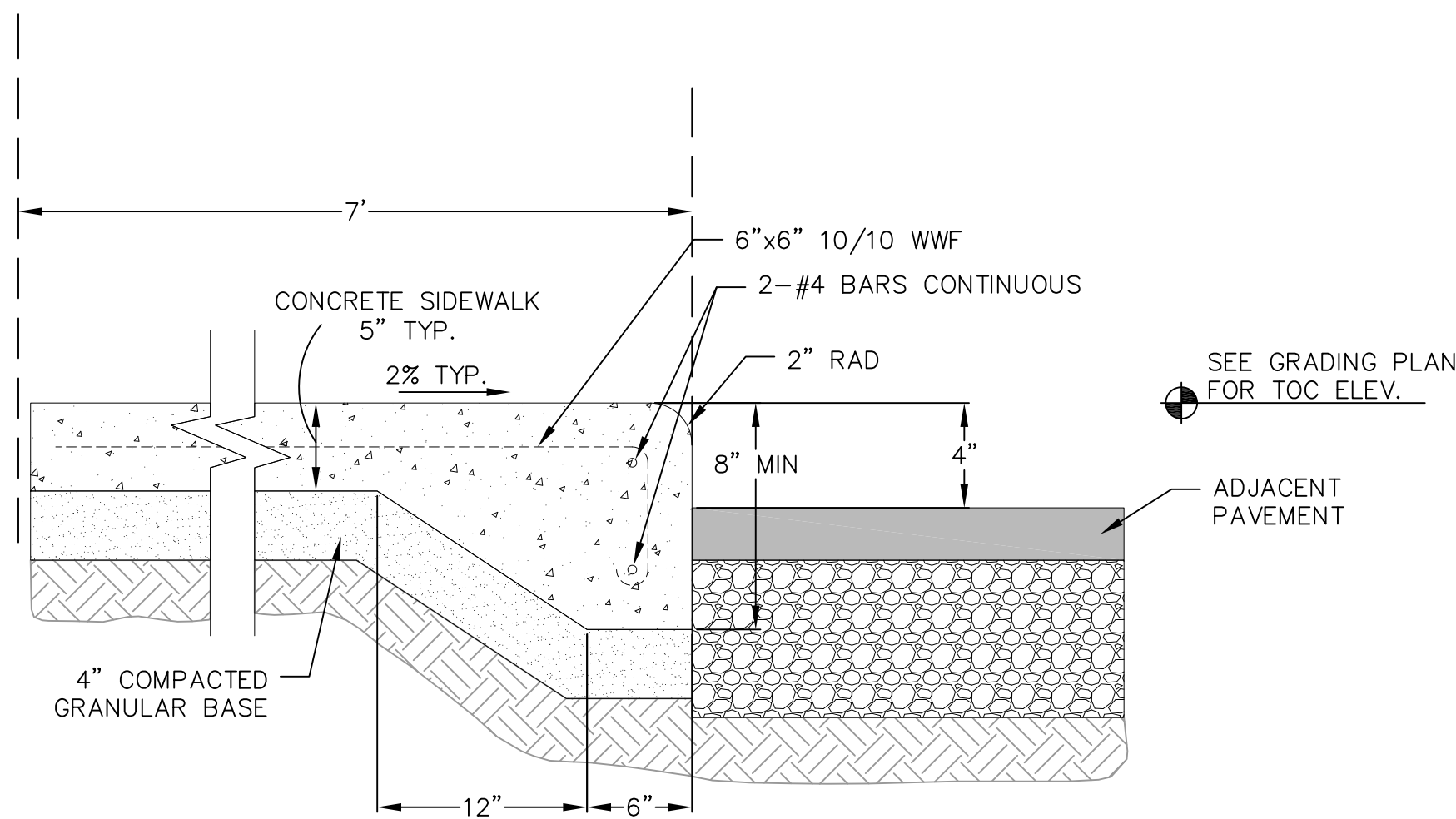
PROJECT NO.	180124
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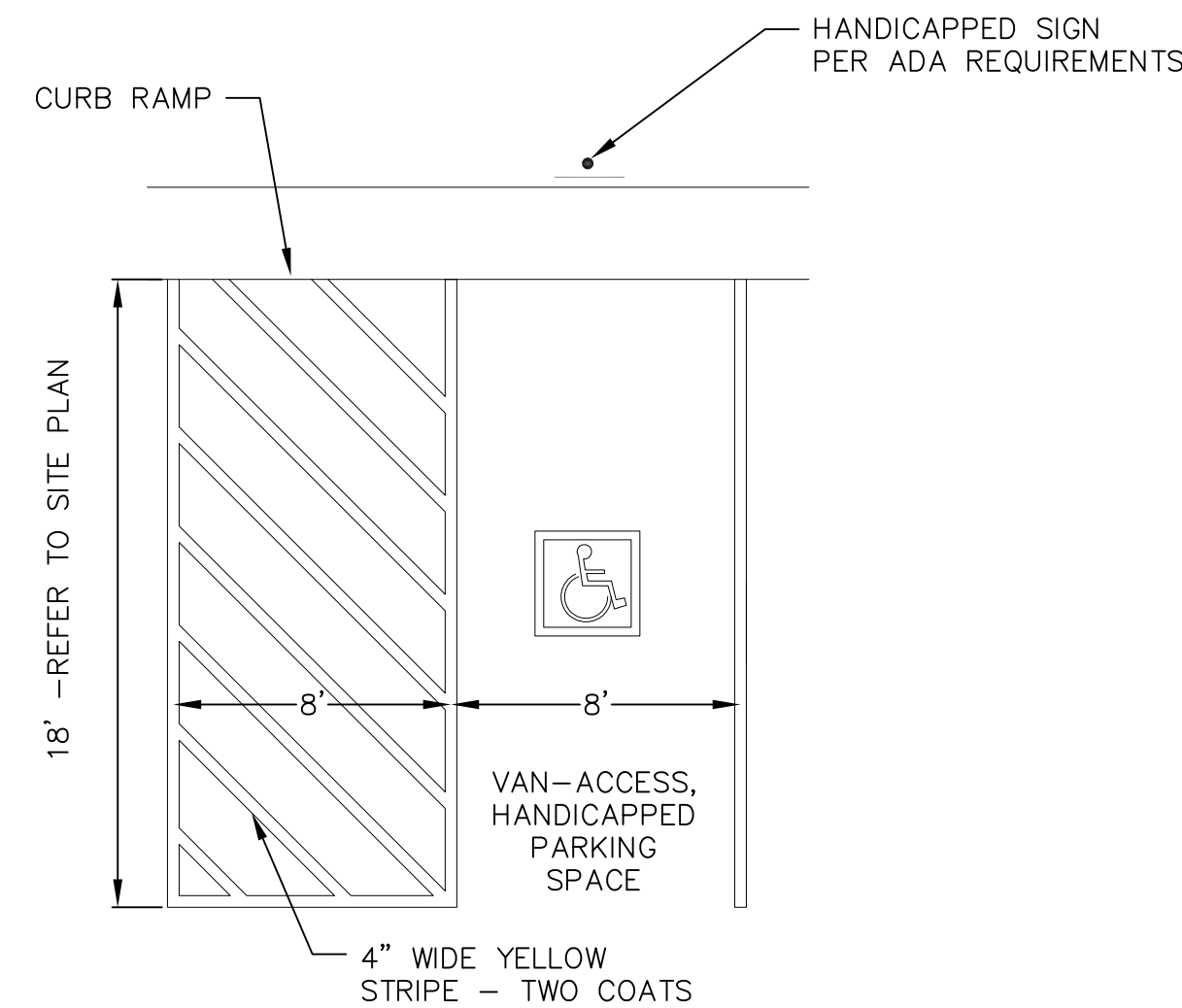
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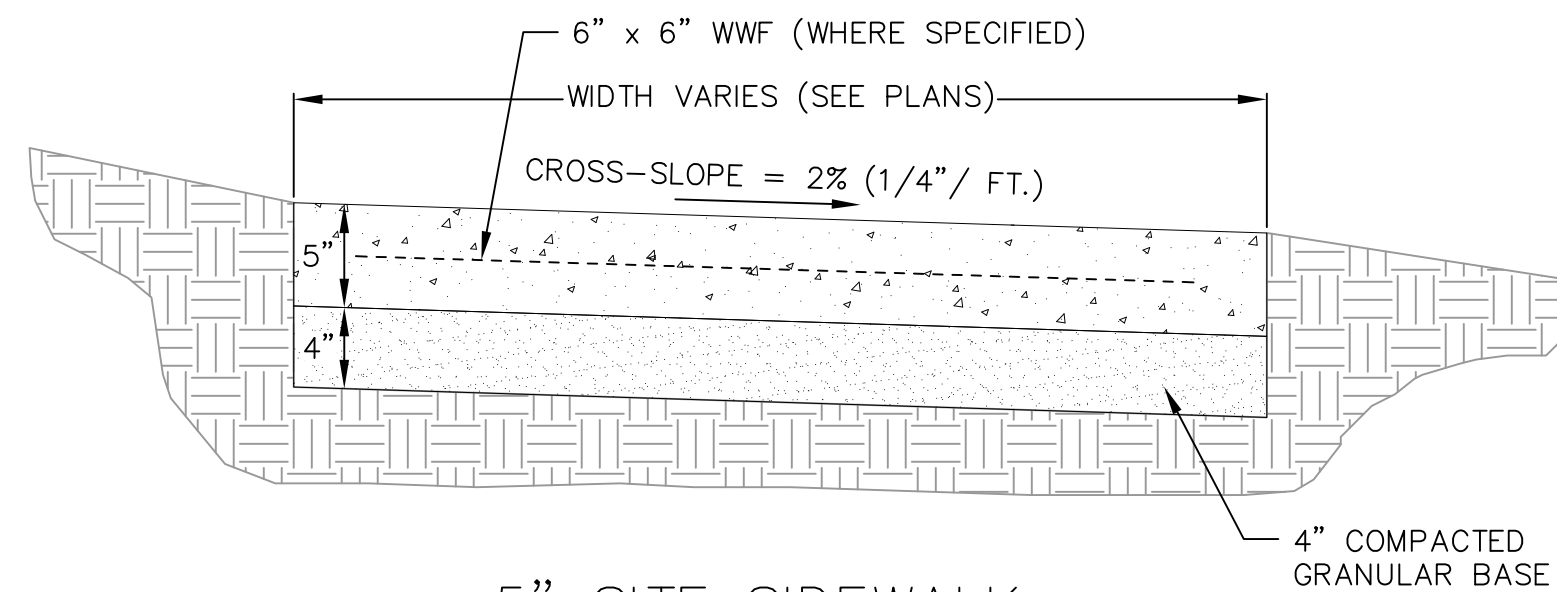
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1 CONCRETE PAD
NOT TO SCALE



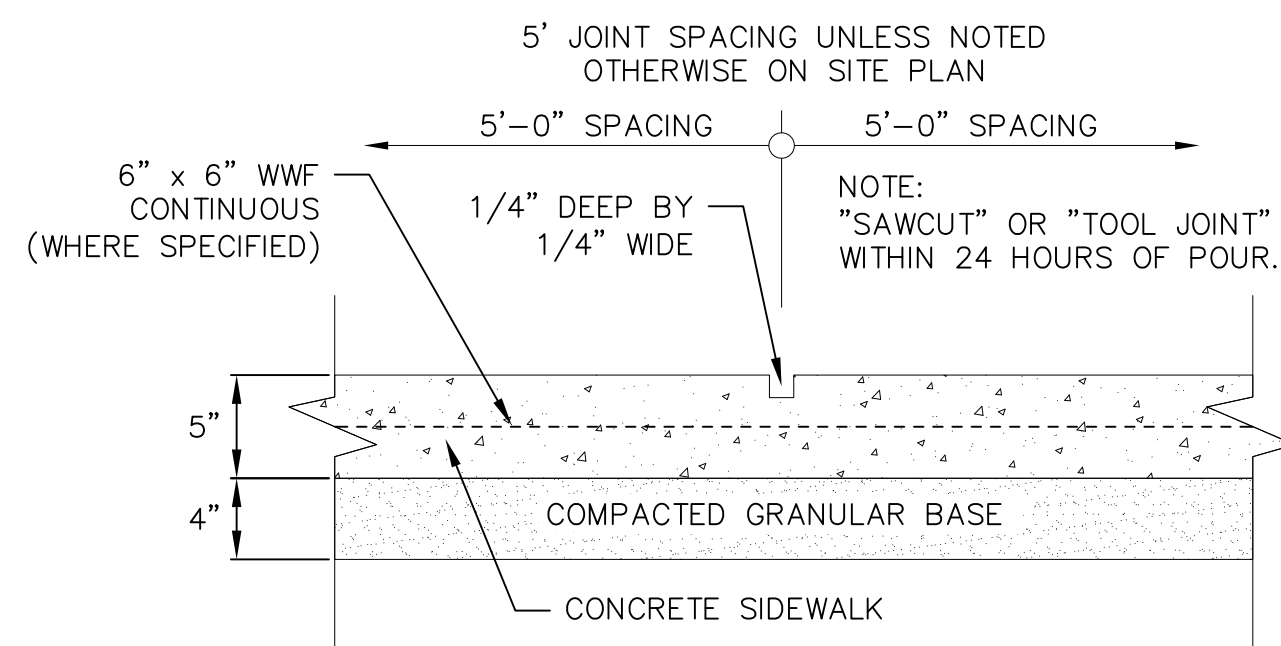
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1 CURBED SIDEWALK DETAIL
NOT TO SCALE



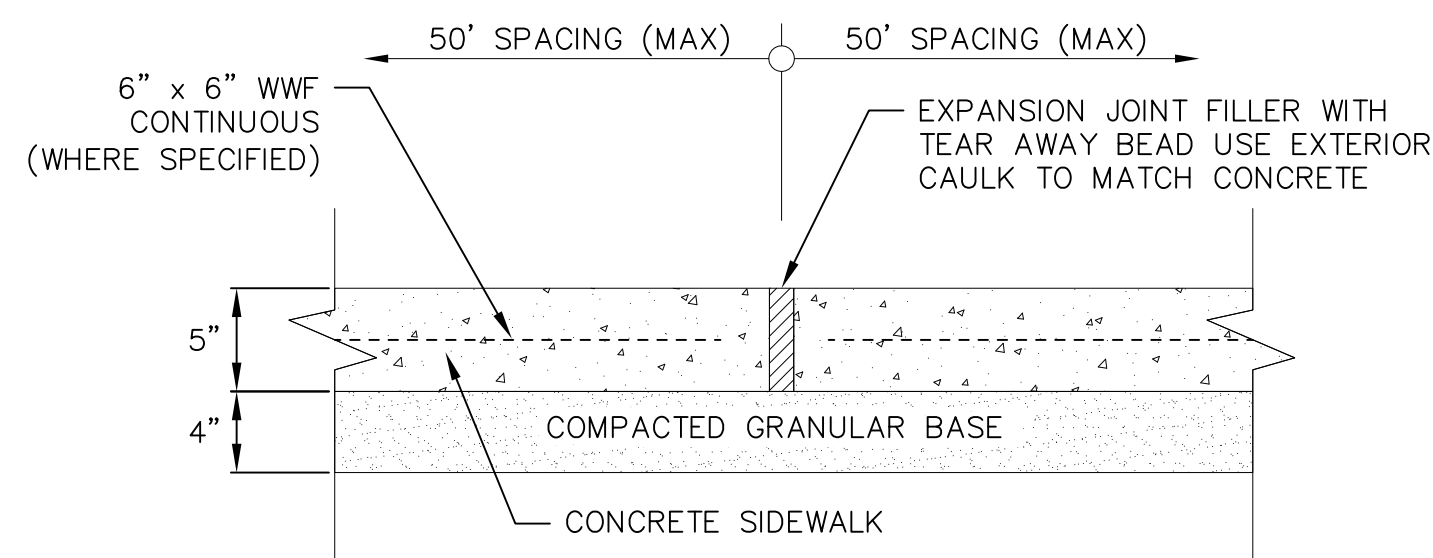
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1 ADA STRIPING
NOT TO SCALE



5" SITE SIDEWALK

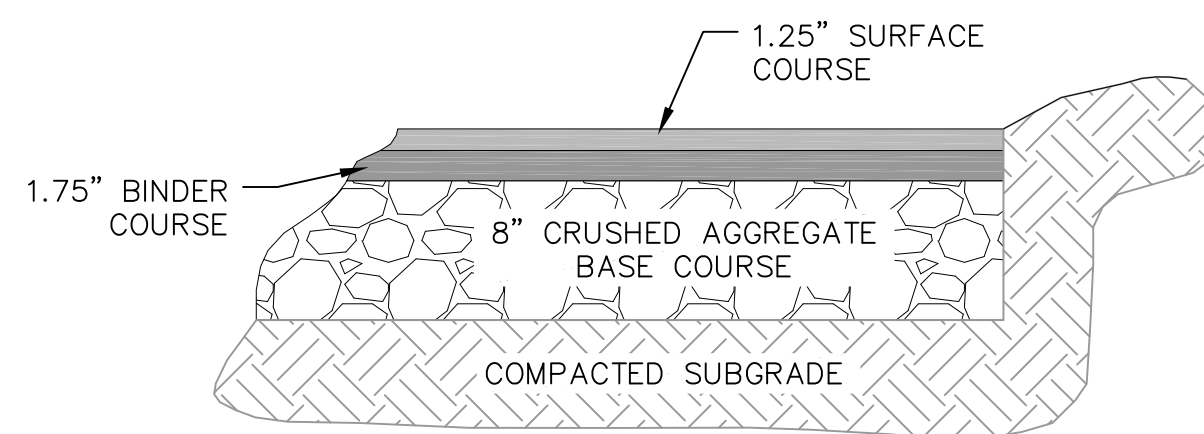


SIDEWALK CONTROL JOINT

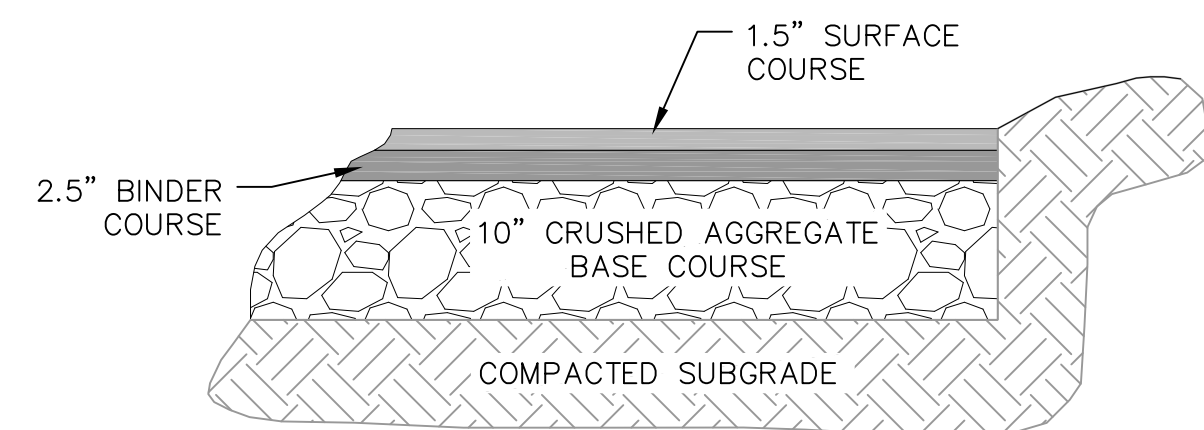


SIDEWALK EXPANSION JOINT

1
1 5" SIDEWALK
NOT TO SCALE

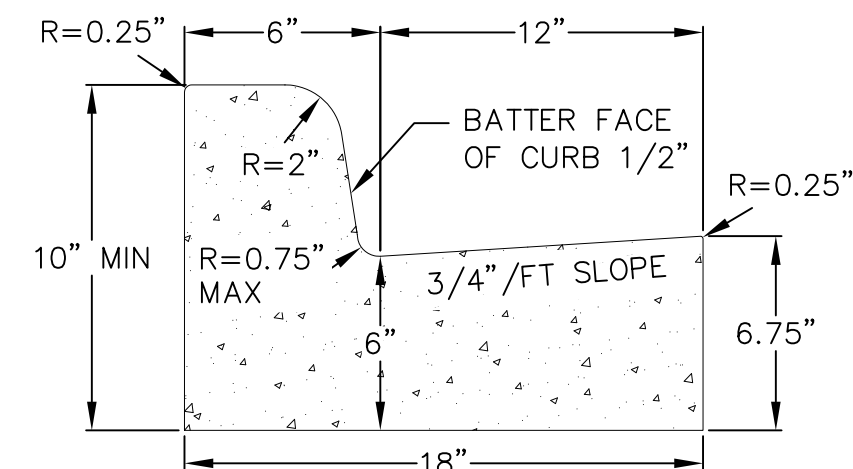


LIGHT-DUTY
BITUMINOUS PAVEMENT

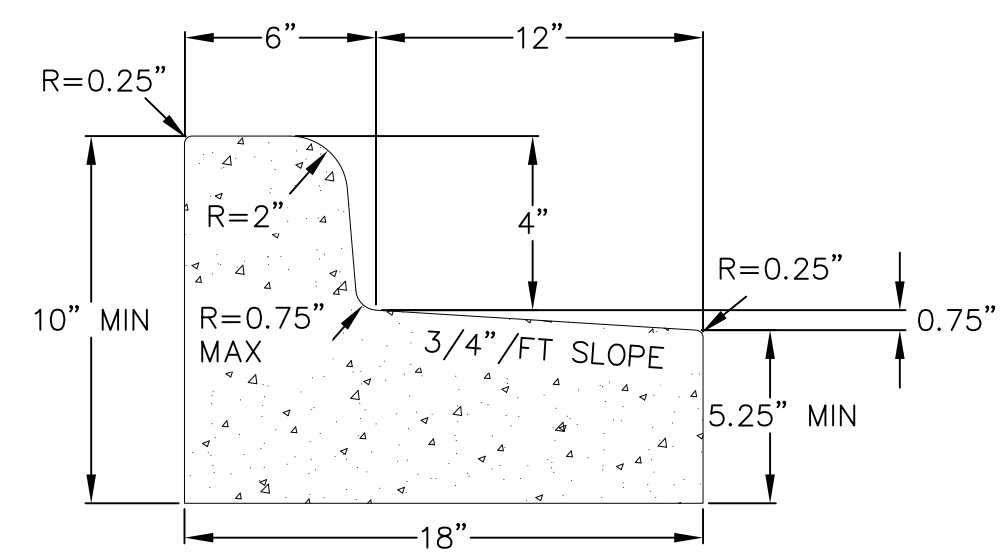


HEAVY-DUTY
BITUMINOUS PAVEMENT

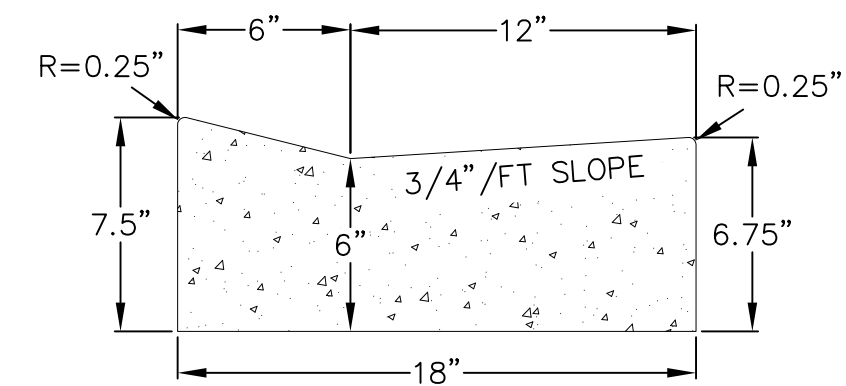
1
1 SITE PAVEMENT
NOT TO SCALE



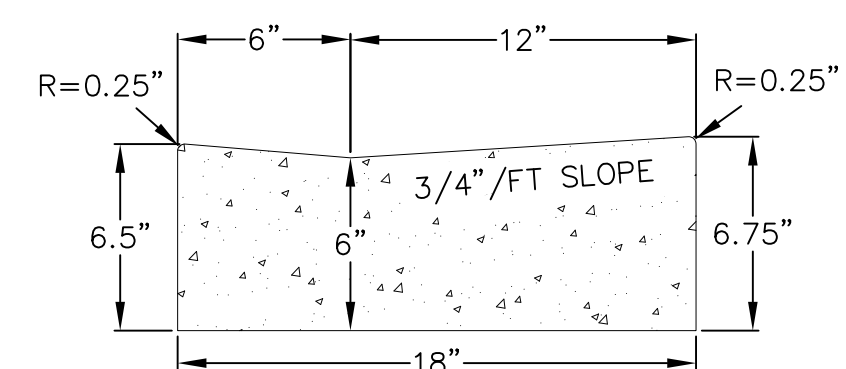
4" CURB AND GUTTER
CROSS SECTION



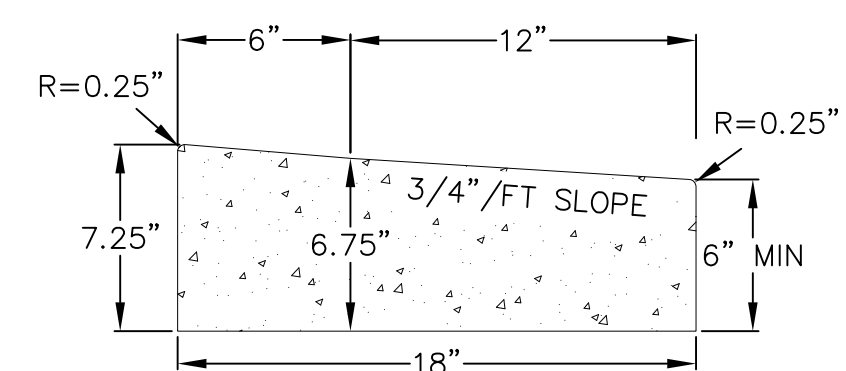
4" CURB AND GUTTER
REJECT SECTION



DRIVEWAY GUTTER
CROSS SECTION



HANDICAP RAMP
GUTTER CROSS SECTION



HANDICAP RAMP
GUTTER REJECT SECTION

1
1 18" CONCRETE CURB AND GUTTER
NOT TO SCALE

NOT FOR CONSTRUCTION



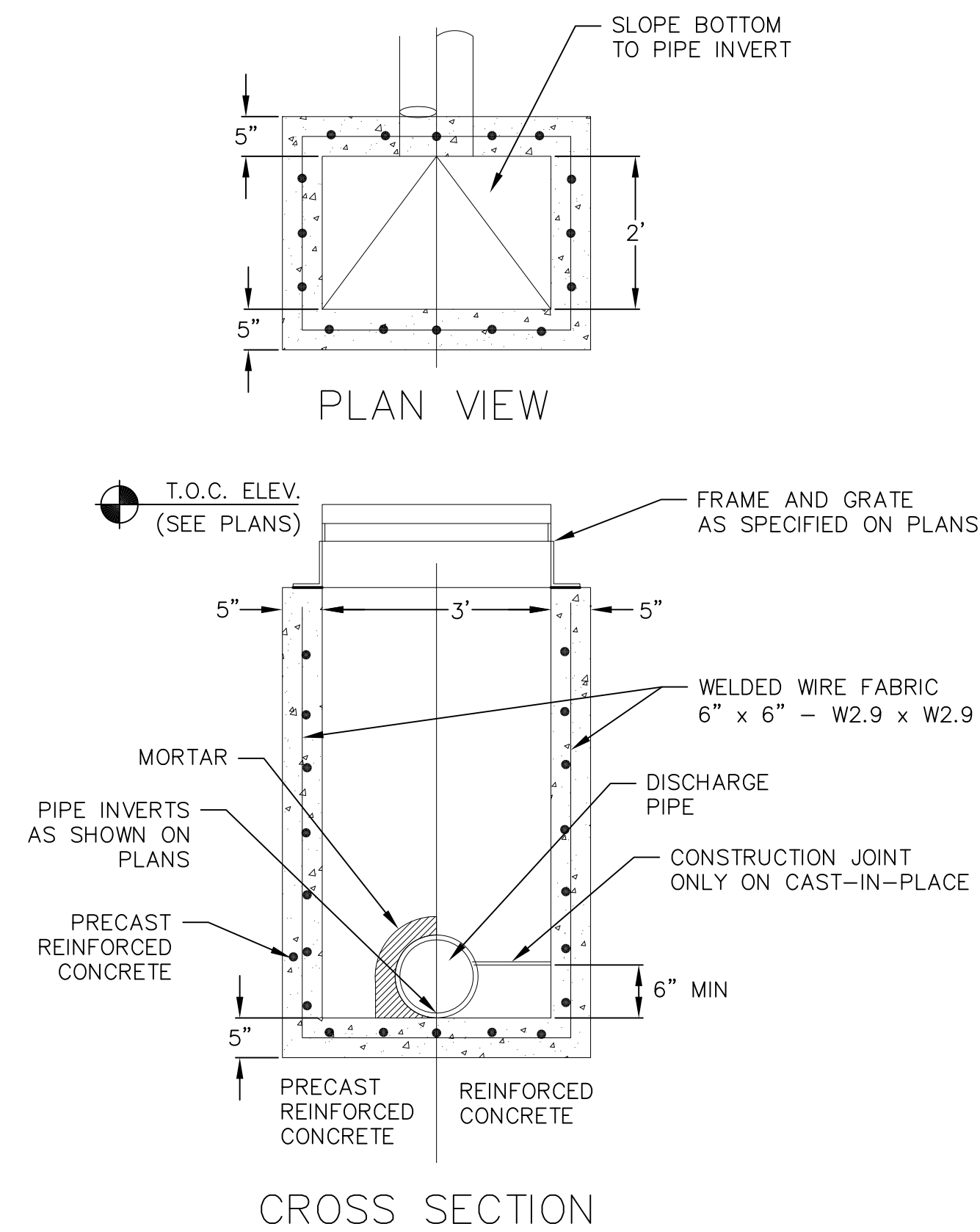
vierbicher
planners | engineers | advisors
Phone: (800) 261-3898

Construction Details - 3
Station Bank - 2430 Frazier Avenue
City of Madison
Dane County, WI

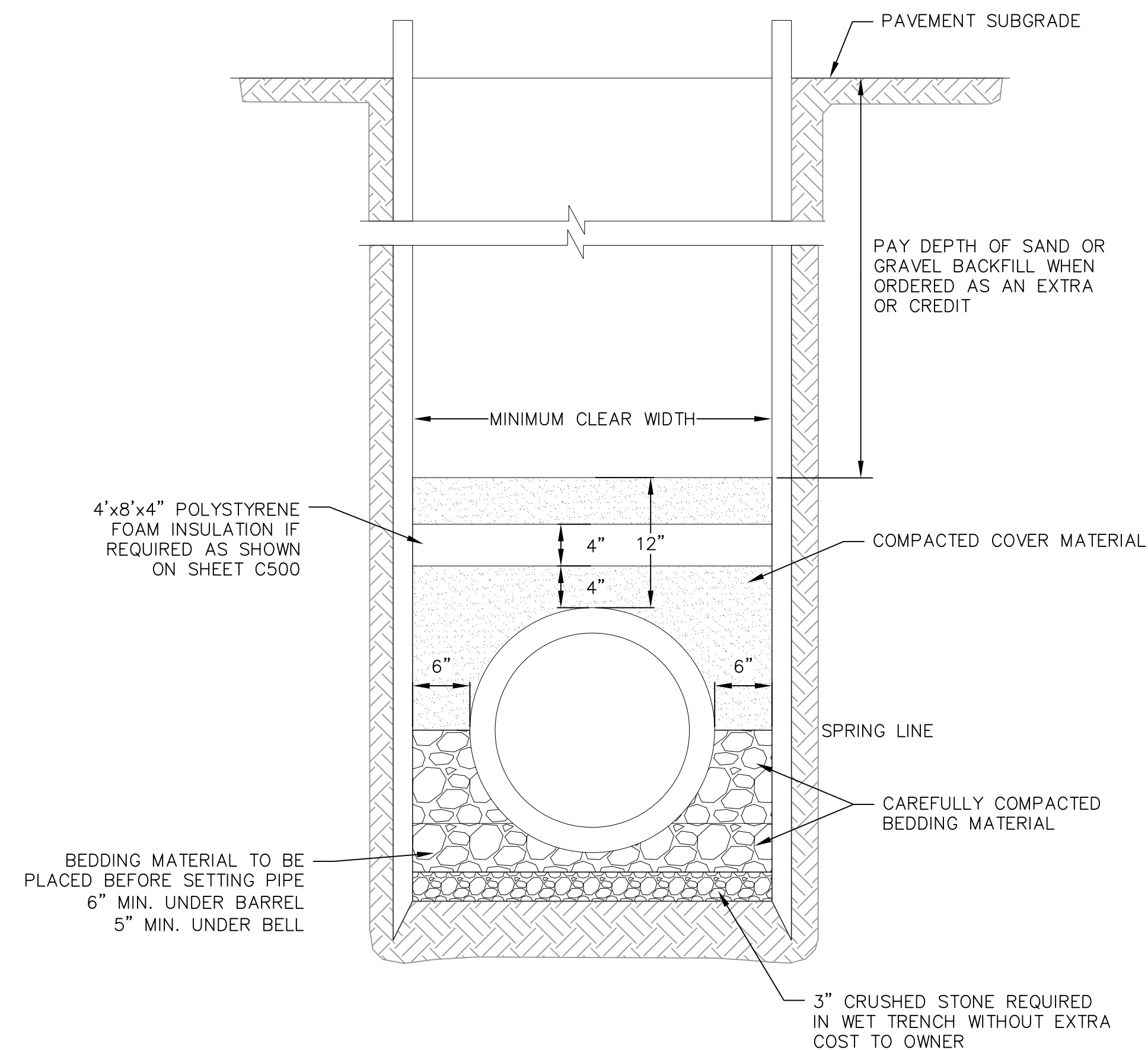
REVISIONS		REVISIONS	REMARKS
NO.	DATE	NO.	DATE

SCALE AS SHOWN
DATE 05-16-2018
DRAFTER SCHR
CHECKED ABAR/MSCH
PROJECT NO. 180124

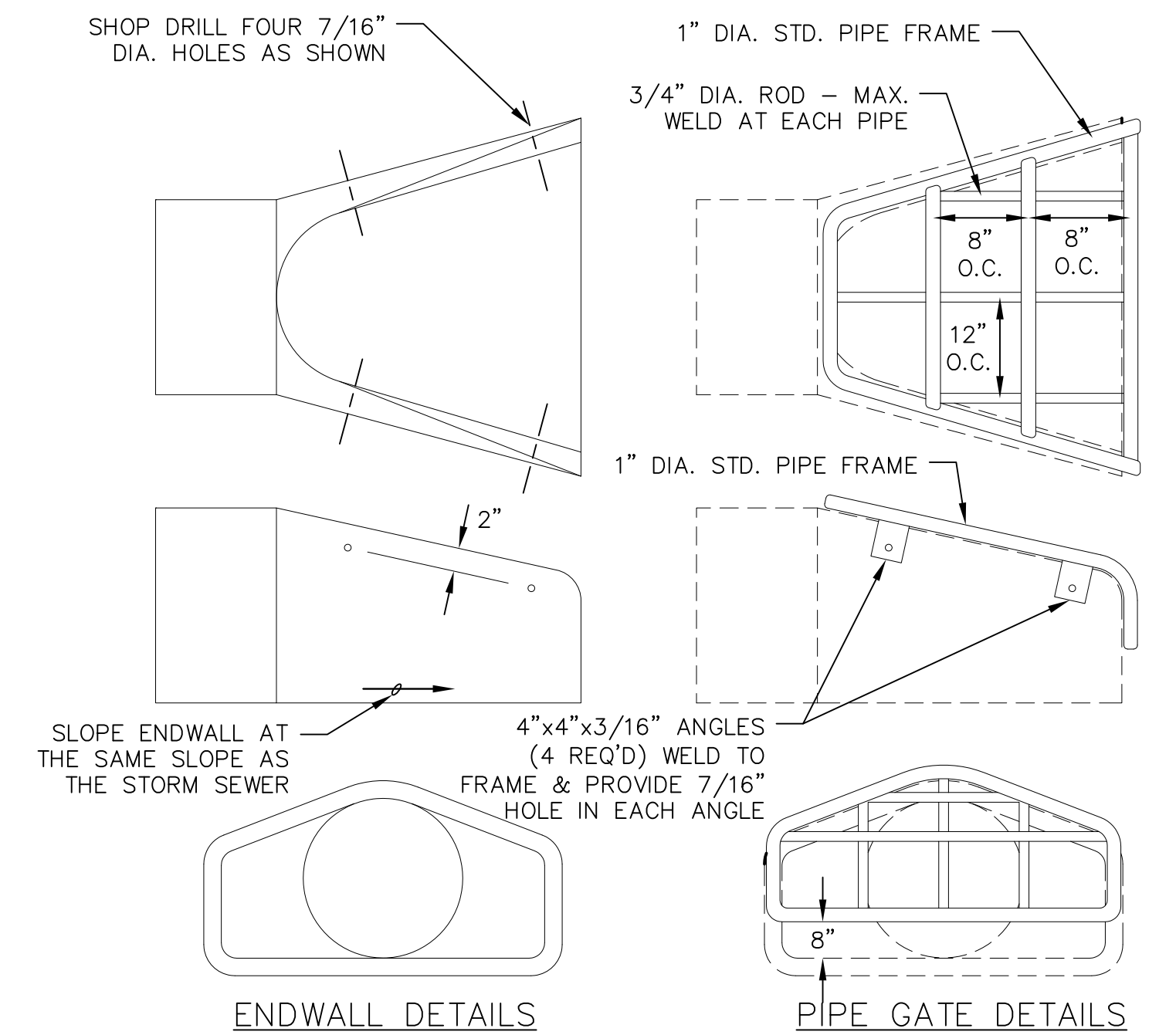
C
602



1 CURB INLET - TYPE 3, 2' x 3' BASIN
1 NOT TO SCALE



1 CLASS B BEDDING COMPACTED SECTION
1 NOT TO SCALE



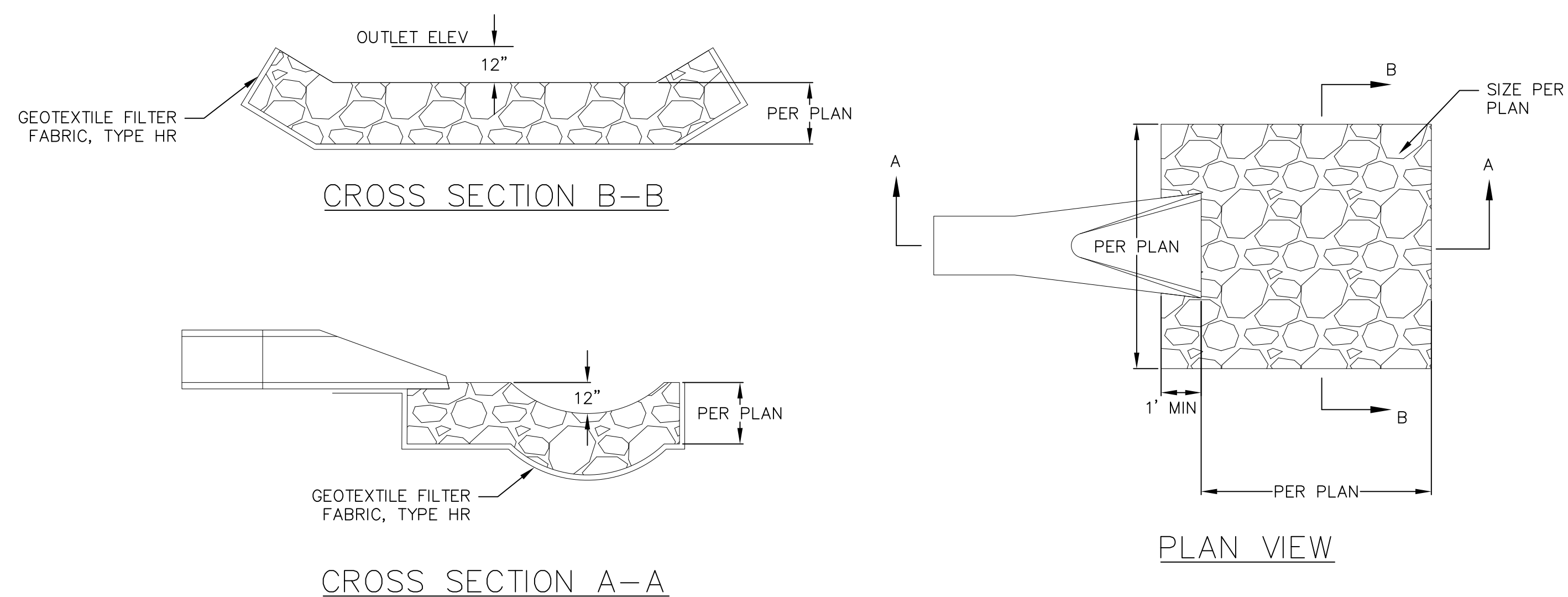
NOTES:
- THE CONTRACTOR SHALL BOLT THE PIPE GATE TO THE CONCRETE ENDWALL WITH FOUR 3/8"x6" MACHINE BOLTS WITH NUTS ON INSIDE WALL.

PAINTING SPECIFICATIONS:
 — THE PIPE GATE SHALL RECEIVE THE FOLLOWING PREPARATION & PAINTING. THE FIRST COAT SHALL BE RUS-OLEUM X-60 RED BARE METAL PRIMER OR APPROVED EQUAL. THE SECOND COAT SHALL BE RUS-OLEUM 960 ZINC CHROMATE PRIMER OR APPROVED EQUAL. THE THIRD COAT SHALL BE RUS-OLEUM 1282 HIGH GLOSS METAL FINISH OR APPROVED EQUAL.

PREPARATION STEPS:

1. BARE METAL SURFACES - TREAT WITH THE THREE-COAT PAINTING SYSTEM LISTED AFTER A THOROUGH SCRAPING, WIRE BRUSHING & CLEANING.
2. EACH COAT OF PAINT SHALL BE APPLIED OVER THE ENTIRE GATE SURFACE.
3. ALLOW 24-48 HOURS DRYING TIME AT 60° OR ABOVE BETWEEN COATS.

1 STANDARD ENDWALL
1 NOT TO SCALE



APRON SIZING

1 RIP-RAP OUTLET
1 NOT TO SCALE

NOT FOR CONSTRUCTION

[illegible]

SCALE
AS SHOWN

05-16-2018

RAFTER

SCHR

CHECKED
ABAR/MSCH

PROJECT NO.
180124

C

603



REVISIONS	NO.	DATE	REMARKS

SCALE
AS SHOWN

DATE
05-16-2018

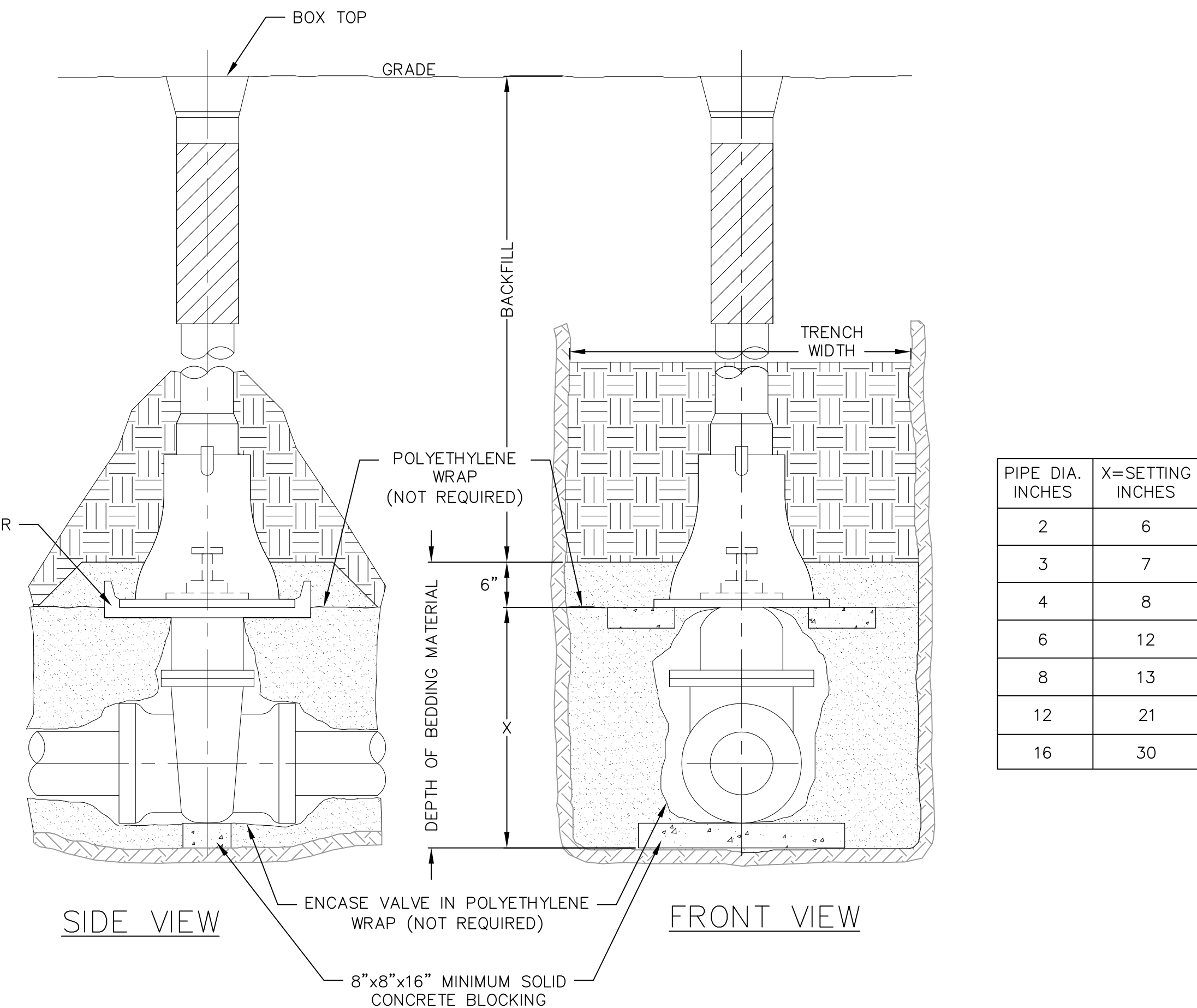
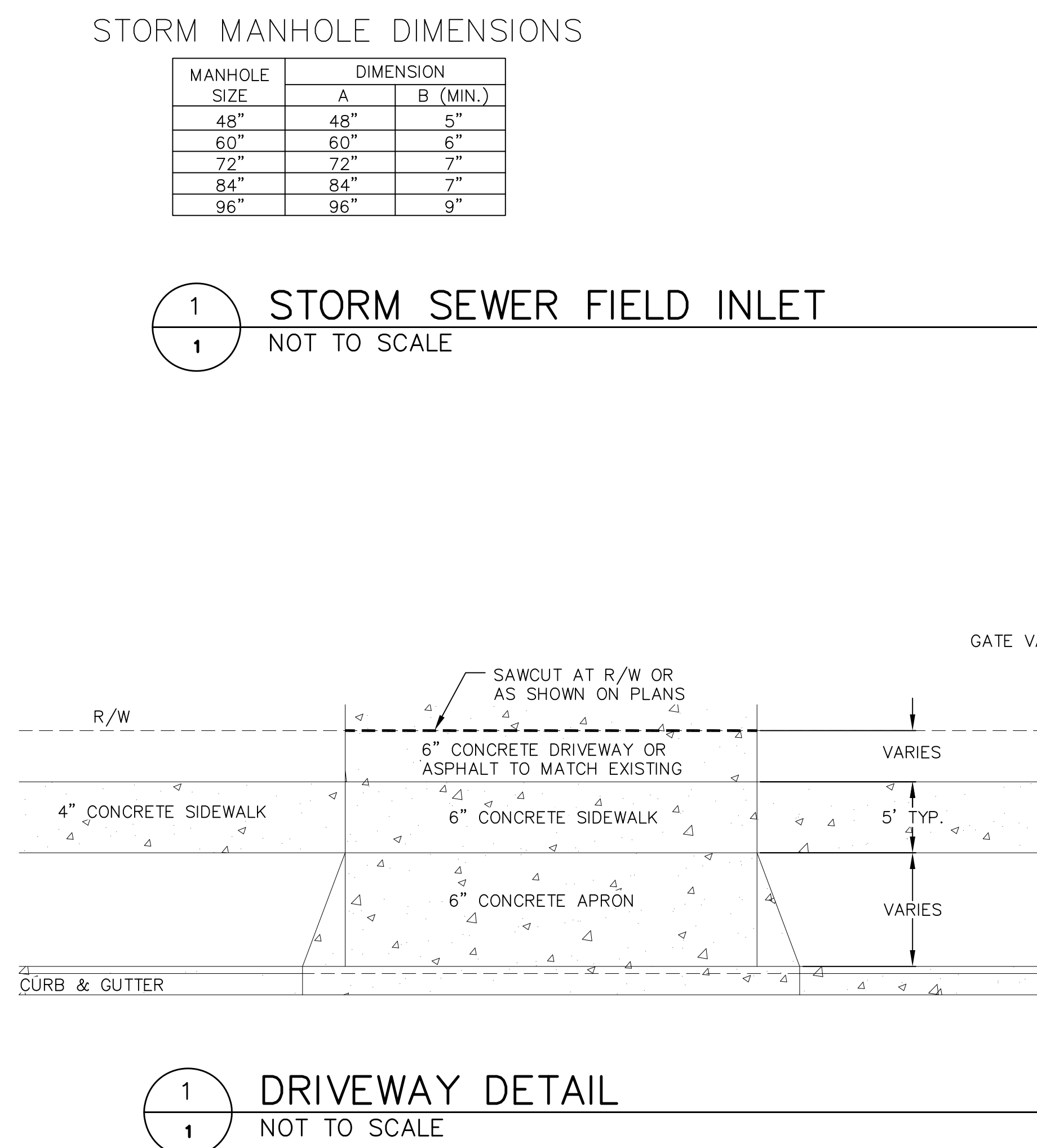
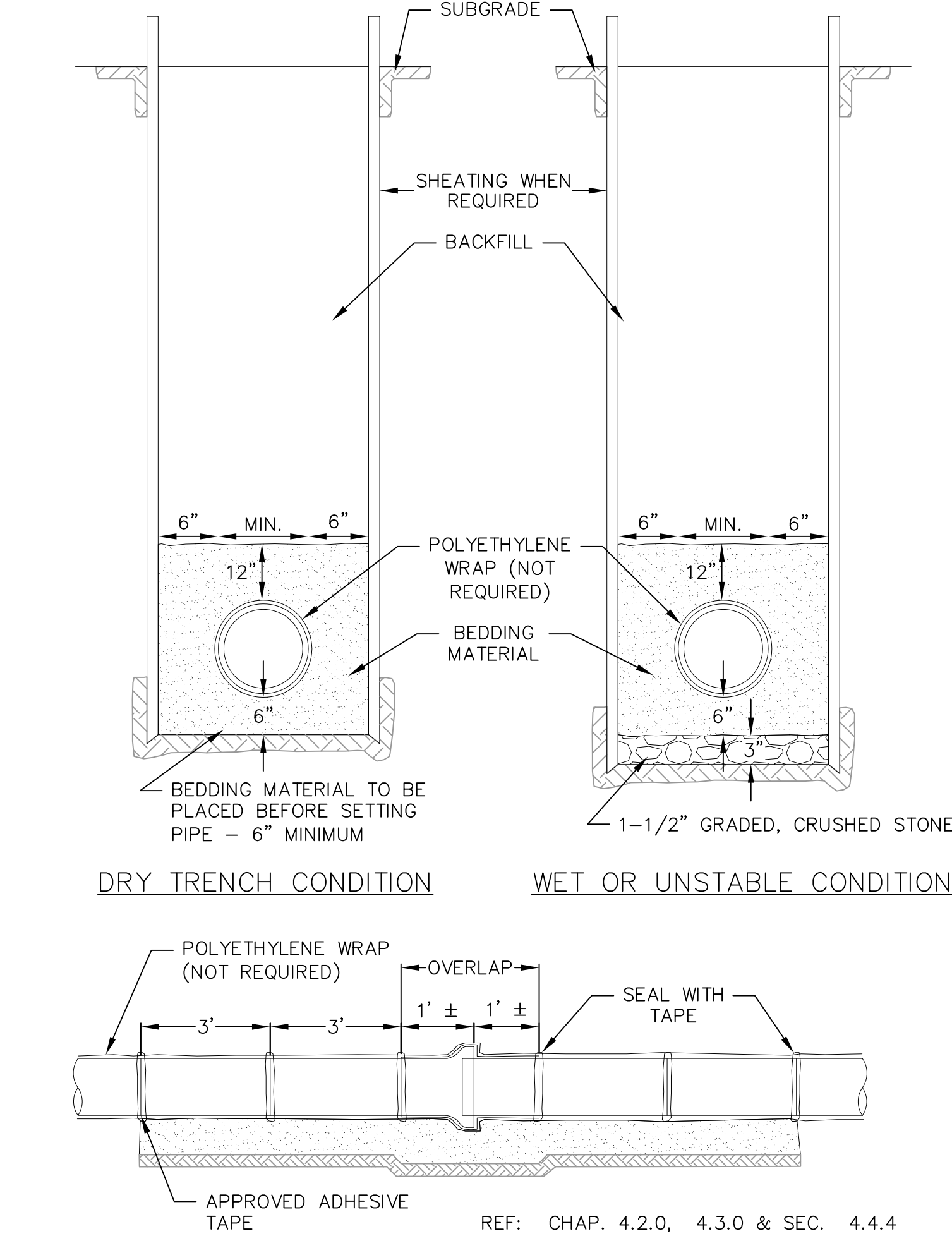
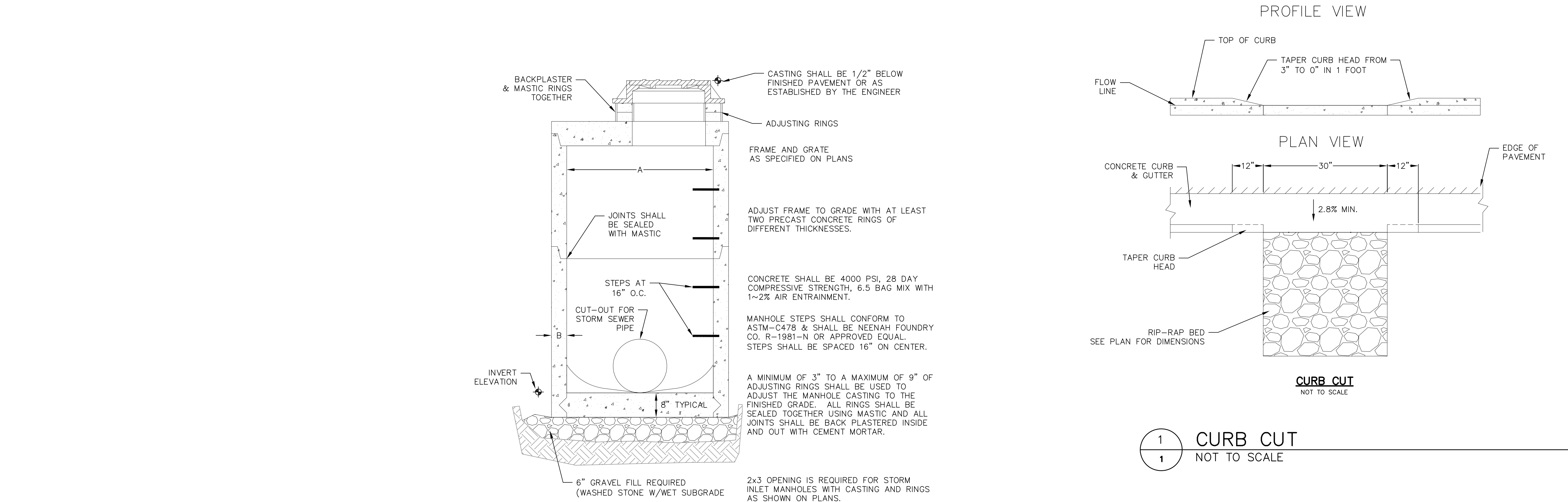
DRAFTER
SCHR

CHECKED
ABAR/MSCH

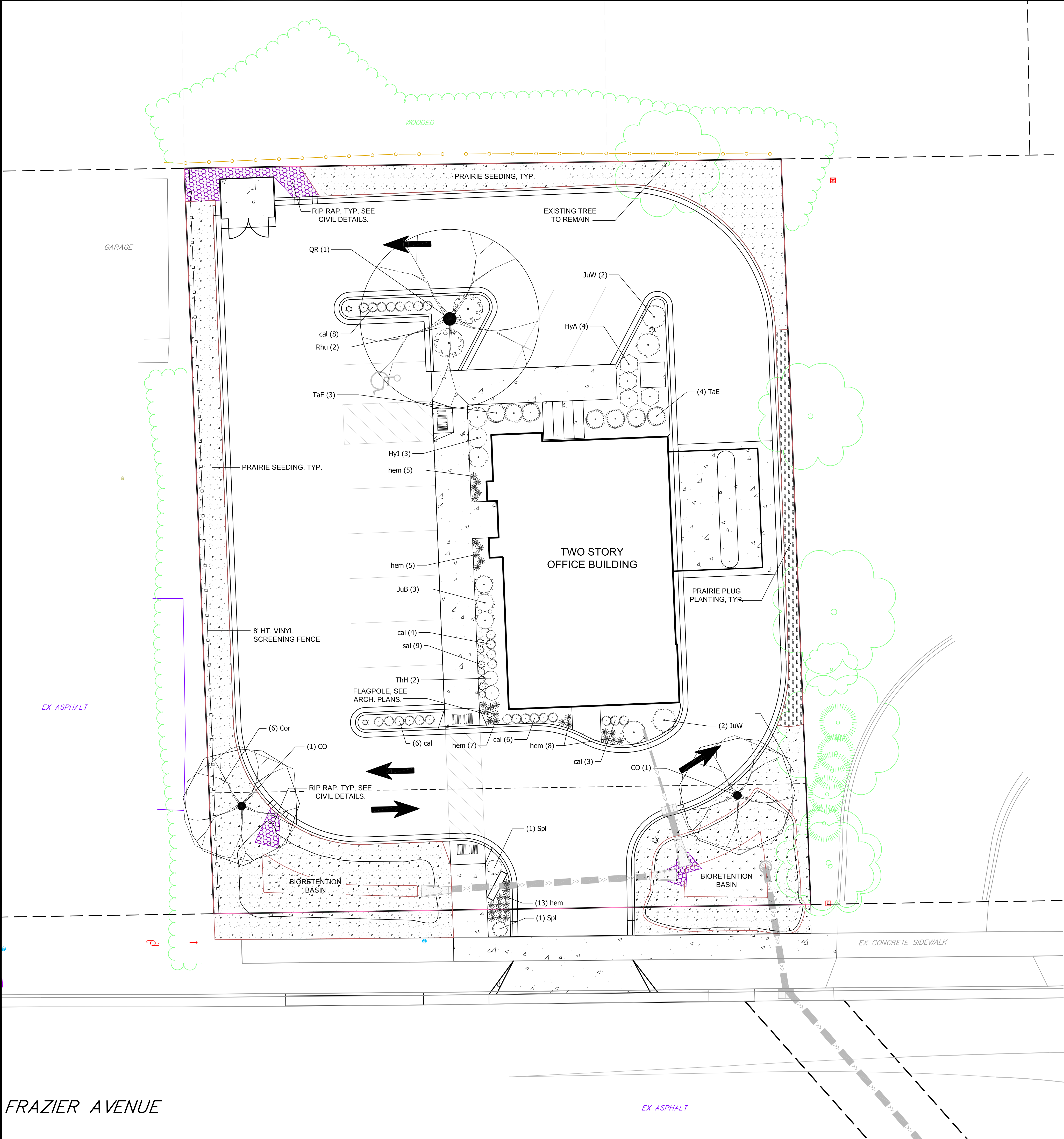
PROJECT NO.
180124

C

604



NOT FOR CONSTRUCTION



PLANT SCHEDULE

DECIDUOUS TREES	BOTANICAL NAME / COMMON NAME	CONT	CAL	QTY
CO	Celtis occidentalis / Common Hackberry	B & B	2.5" Cal	2
QR	Quercus rubra / Red Oak	B & B	2.5" Cal	1
ANNUALS/PERENNIALS	BOTANICAL NAME / COMMON NAME	SIZE	FIELD2	QTY
cal	Calamagrostis x acutiflora "Karl Foerster" / Feather Reed Grass	1 gal	Cont	27
hem	Hemerocallis x "Stella de Oro" / Stella de Oro Daylily	4" pot	Cont	38
sal	Salvia nemorosa "Caradonna" / Cardonna Perennial Salvia	4" pot	Cont	9
DECIDUOUS SHRUBS	BOTANICAL NAME / COMMON NAME	SIZE	FIELD2	QTY
HyA	Hydrangea arborescens "Annabelle" / Annabelle Smooth Hydrangea	5 gal	Cont	4
HyJ	Hydrangea paniculata "Jane" / Little Lime Hydrangea	5 gal	Cont	3
Rhu	Rhus aromatica "Gro-Low" / Gro-Low Fragrant Sumac	5 gal	Cont	2
Spl	Spiraea x bumalda "Anthony Waterer" / Anthony Waterer Spiraea	3 gal	Cont	2
EVERGREEN SHRUBS	BOTANICAL NAME / COMMON NAME	SIZE	FIELD2	QTY
JuW	Juniperus horizontalis "Wisconsin" / Wisconsin Juniper	5 gal	Cont	4
JuB	Juniperus sabina "Blue Forest" / Blue Forest Juniper	5 gal	Cont	3
TaE	Taxus x media "Everlow" / Yew	5 gal	Cont	7
THH	Thuja occidentalis "Holmstrup" / Holmstrup Cedar	10 gal	Cont	2

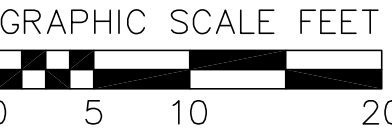
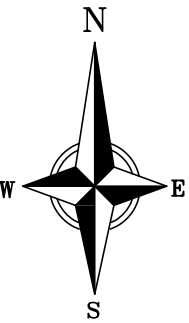
BIORETENTION PLUG/SEED SCHEDULE

	BIORETENTION PLUG MIX	245 sf
	Asclepias incarnata / Swamp Milkweed	23
	Carex cristatella / Crested Oval Sedge	21
	Carex lurida / Lurid Sedge	21
	Coreopsis tripteris / Tall Coreopsis	21
	Elymus virginicus / Virginia Wild Rye	21
	Iris virginica / Blue Flag Iris	23
	Liatris spicata / Spike Gayfeather	23
	Lobelia siphilitica / Great Lobelia	21
	Panicum virgatum / Switch Grass	21
	Rudbeckia triloba / Browneyed Susan	23
	Spartina pectinata / Prairie Cordgrass	21
	Symphoricarpon novae-angliae / New England Aster	21
	ECONOMY PRAIRIE SEED MIX	3,399 sf
	PRAIRIE PLUGS	212 sf
	Echinacea purpurea / Purple Coneflower	38
	Monarda fistulosa / Bergamot	38
	Panicum virgatum / Switch Grass	56
	Rudbeckia hirta / Black-eyed Susan	36
	Schizachyrium scoparium / Little Bluestem Grass	56

GENERAL NOTES:

1. All plantings shall conform to quality requirements as per ANSI Z60.1.
2. All plant material shall be true to the species, variety and size specified, nursery grown in accordance with good horticultural practices, and under climatic conditions similar to those of the project site.
3. Contact Landscape Architect, in writing, to request and plant material substitutions due to availability issues.
4. All disturbed areas, unless otherwise noted, to be seeded with Economy Prairie Mix by Cardno Native Plant Nursery, or equal, per manufacturer's specified application rates. All seeded areas are to be watered daily to maintain adequate soil moisture for proper germination. After vigorous growth is established, apply 1/2" water twice weekly until final acceptance.
5. All plants shall be guaranteed to be in healthy and flourishing condition during the growing season following installation. All plant material shall be guaranteed for one year from the time of installation.
6. Contractor shall provide a suitable amended topsoil blend for all planting areas where soil conditions are unsuitable for plant growth. Topsoil shall conform to quality requirements as per Section 625.2(1) of the Standard Specifications for Highway Construction. Provide a minimum of 12" of topsoil in all planting areas and 6" of topsoil in areas to be seeded/sodded.
7. Landscape beds to be mulched with undyed shredded hardwood bark mulch to 3" depth min. and edged with commercial grade aluminum landscape edging.

City of Madison Landscape Worksheet						
Total Square Footage of Developed Area:	16,108					
Total Landscape Points Required:	16,108	/ 300 =	54	x 5 =		268
		Credits/ Existing Landscaping		New/ Proposed Landscaping		
Plant Type/ Element	Min. Size at Installation	Points	Quantity	Points Achieved	Quantity	Points Achieved
Overstory deciduous tree	2.5" cal	35		0	3	105
Tall Evergreen Tree	5-6 feet tall	35		0		0
Ornamental tree	1.5" cal	15		0		0
Upright evergreen shrub	3-4 feet tall	10		0	2	20
Shrub, deciduous	#3 gallon container size, Min. 12-24"	3		0	11	33
Shrub, evergreen	#3 gallon container size, Min. 12-24"	4		0	14	56
Ornamental grasses/perennials	#1 gallon container size, Min. 8-18"	2		0	27	54
Ornamental/decorative fencing or wall	n/a	4 per 10 LF		0		0
Existing significant specimen tree	Min. Size 2.5" cal. Trees must be within developed area and cannot comprise more than 30% of total required points.	14 per caliper inch. Max. points per tree: 200		0		0
Landscape Furniture for public seating and/or transit connections	Furniture must be within developed area, publicly accessible, and cannot comprise more than 5% of total required points	5 points per "seat"		0		0
Sub Totals				0		268
				Total Points Provided: 268		



FRAZIER AVENUE

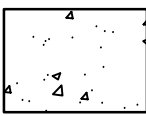
PLOT DATE: _____

DIGGERS HOTLINE

TOLL FREE 1-800-242-8511
MILWAUKEE AREA 414-259-1181
TDD 1-800-542-2289

STATE LAW REQUIRES YOU TO NOTIFY OWNERS AND
OPERATORS OF TRANSMISSION FACILITIES AT LEAST
THREE WORKING DAYS BEFORE YOU DIG

HARD SURFACE LEGEND



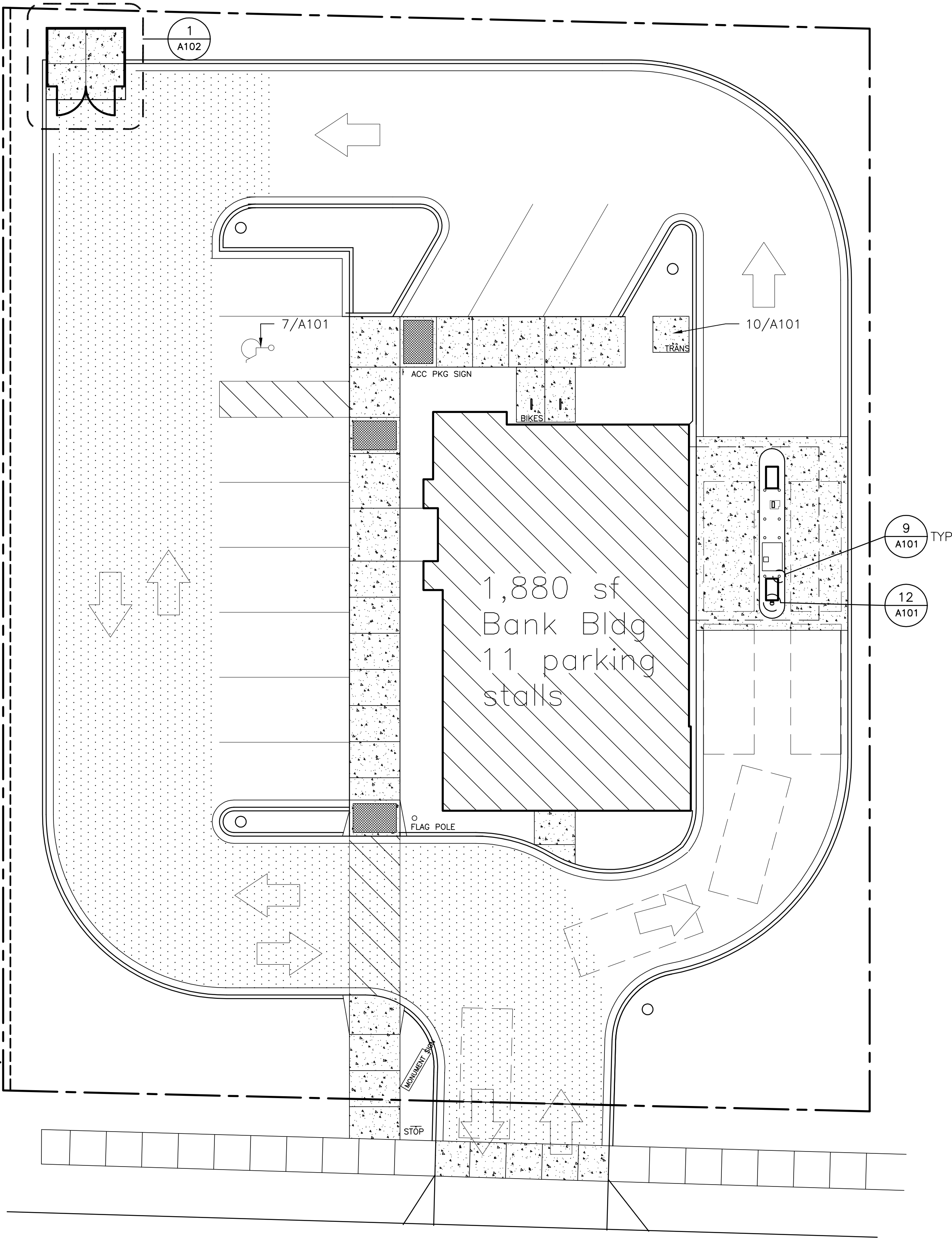
CONCRETE SIDEWALKS:
4" THICK SLAB W/ BROOM
FINISH OVER 4" THICK
COMPACTED SUBBASE.



STANDARD PAVEMENT: 3" BITUMINOUS PAVING (1 1/2"
SURFACE COURSE AND 1 1/2" BINDER COURSE) OVER AN 8"
THICK COMPACTED STONE BASE (±4" OF BREAKER RUN
STONE & ±4" OF 3/4" COMPACTED STONE)



REPAVING AT EXISTING PARKING LOT. SEE CIVIL
DRAWINGS FOR AREA IDENTIFICATION. BASE BID TO
INCLUDE SURFACE COURSE OF LISTED THICKNESS.
ALTERNATE ADD TO INCLUDE STANDARD PAVEMENT
DESIGN AS NOTED ABOVE.



1 SITE PLAN
A100 SCALE: 1" = 40'-0"



0' 10' 20'
1" = 10'-0"



ICONICA
True Design-Build

901 Deming Way // Madison, WI 53717
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iconicacreates.com

STARION - MADISON

2430 FRAZIER AVE - PARCEL #071019410089
MADISON, WISCONSIN 53713

STARION FINANCIAL

333 NORTH 4TH STREET
BISMARCK, NORTH DAKOTA 58501

ISSUE DATES:

PRELIMINARY: 05-16-18

CONDITIONAL USE: 05-16-18

REZONING: 05-16-18

RFI/SI DATE:

Schematic Design Phase:

This drawing indicates the
scale and relationship of
the project components.
This drawing is **not for
construction**.

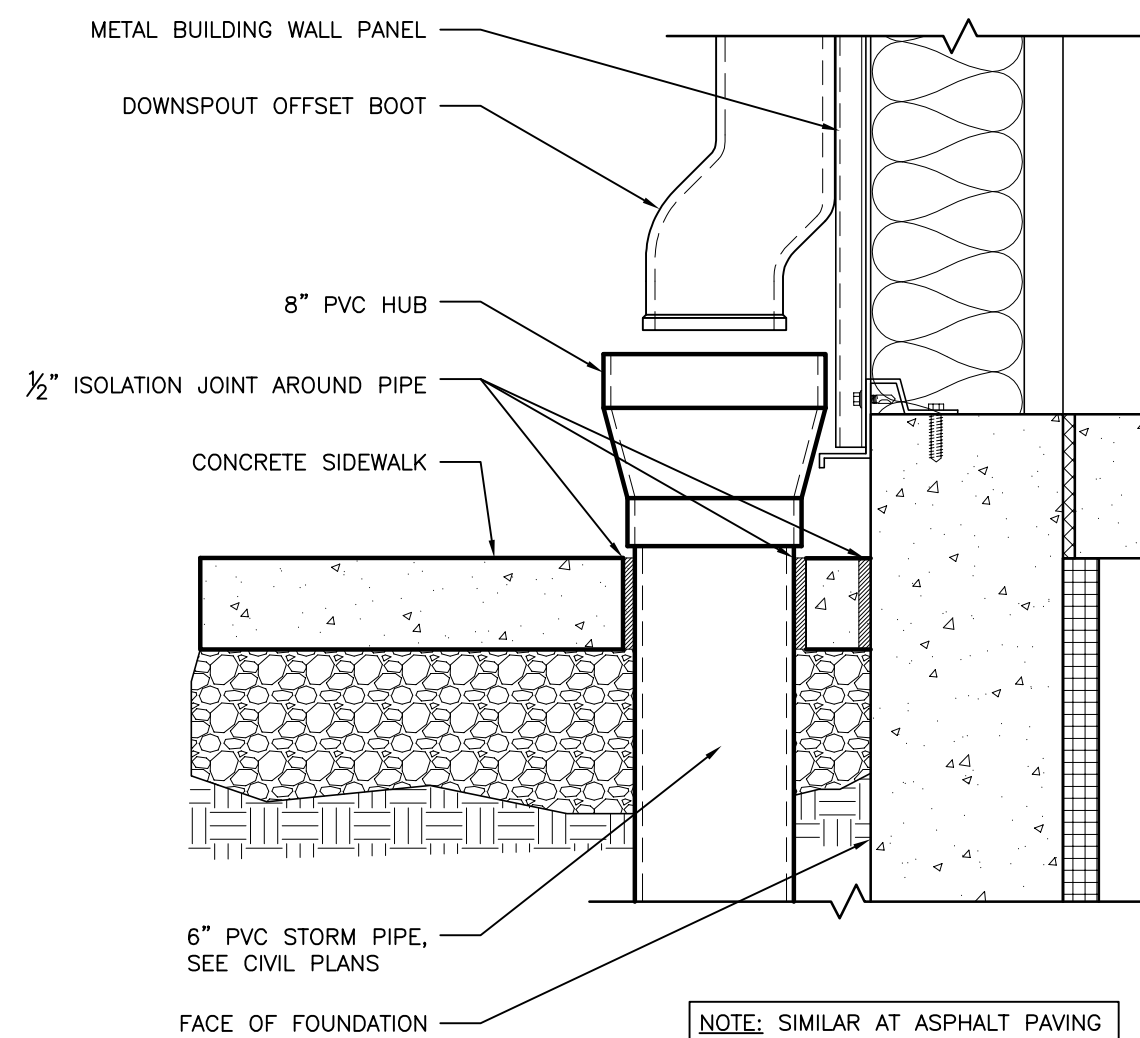
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PROJECT #: 20180130

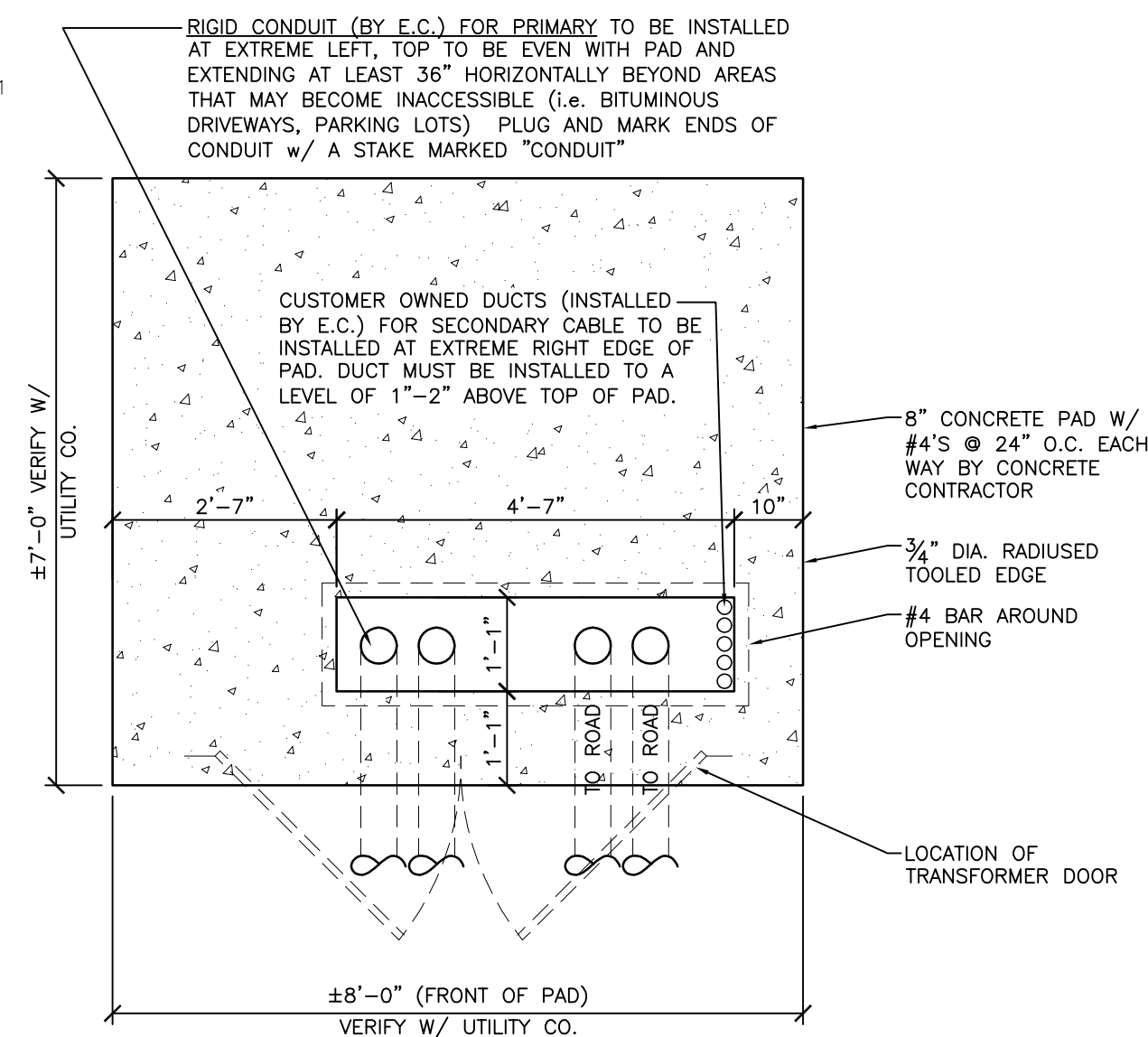
SHEET NUMBER

A100

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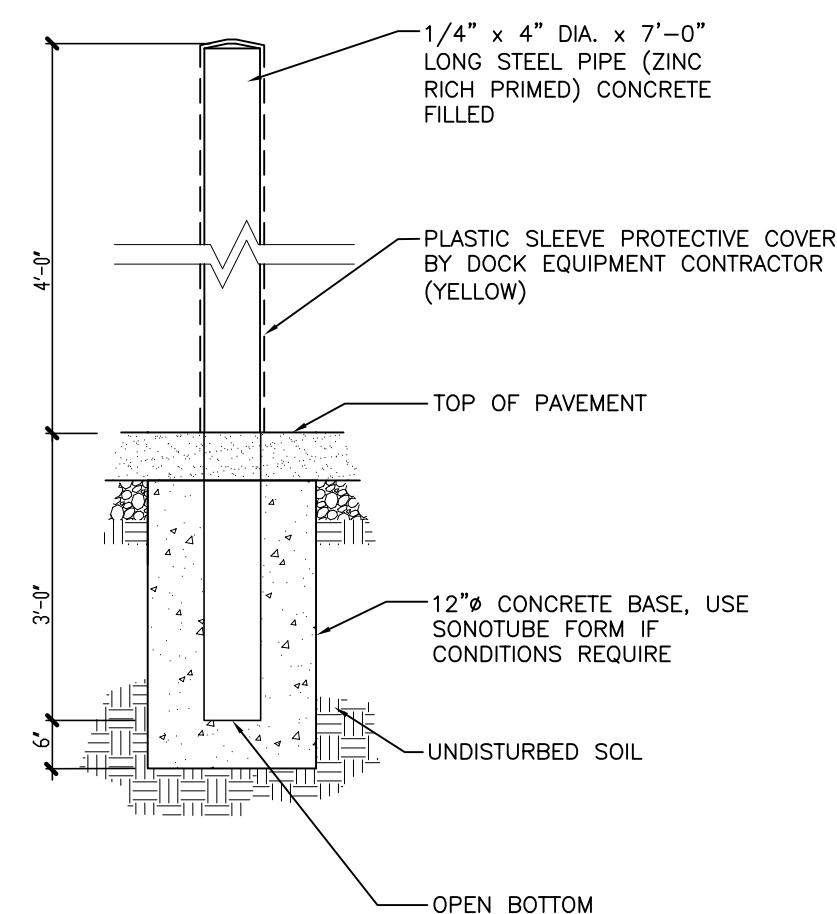


12 HUB DRAIN DETAIL @ SIDEWALK
A101 SCALE: 1 1/2" = 1'-0" 02751.00.00.101



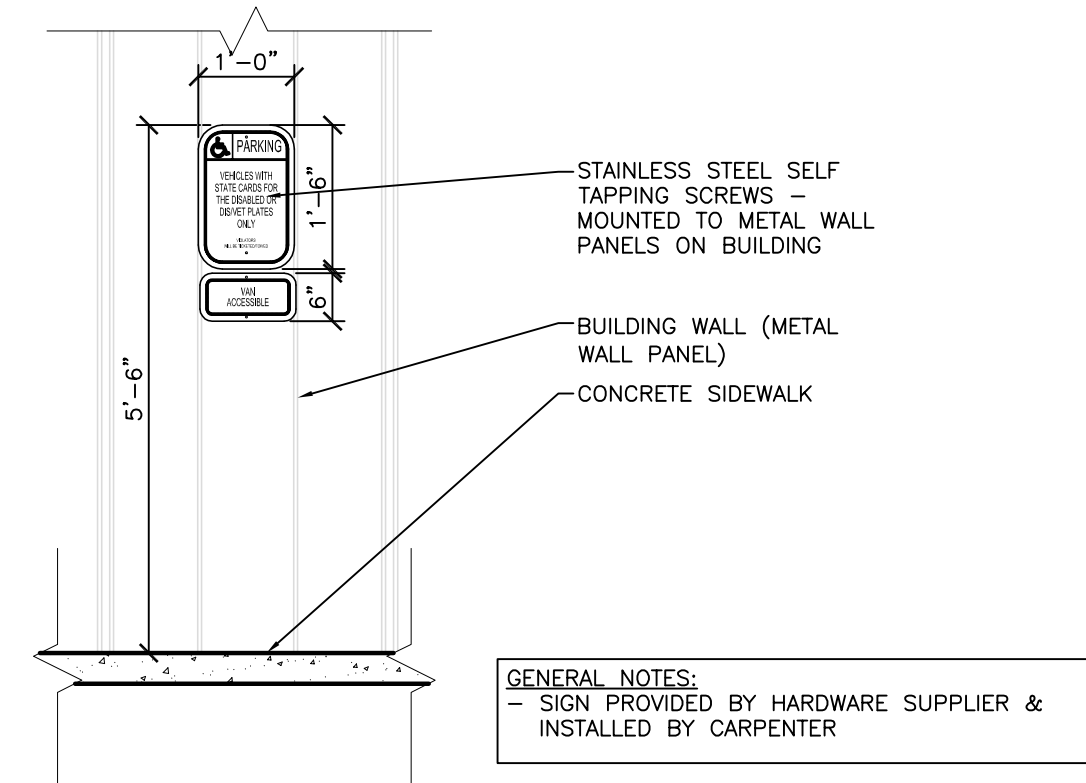
- GENERAL NOTES:
- ELECTRICAL CONTRACTOR TO CONFIRM EXACT DIMENSIONS & LOCATION WITH LOCAL UTILITY COMPANY
- CONFIRM TRANSFORMER DOOR LOCATION WITH DESIGN-BUILDER'S ARCHITECT & ELECTRICAL ENGINEER PRIOR TO ROUGH IN OF ELECTRICAL DUCTS
- DESIGN-BUILDER TO BE RESPONSIBLE FOR OBTAINING ALL PERMITS AND CONCURRENCES OF PAD WITH RESPECT TO SURROUNDING GRADES & NOTIFY ARCHITECT OF POTENTIAL GRADING CONFLICTS PRIOR TO POURING SET PAD LEVEL
- DESIGN-BUILDER'S SUPERINTENDENT TO VERIFY REQUIRED CLEARANCES WITH UTILITY COMPANY PRIOR TO SETTING PAD
- DESIGN-BUILDER AND ELECTRICAL CONTRACTOR TO VERIFY REQUIRED CLEARANCES TO BUILDING, POWER LINES AND UTILITIES WITH LOCAL UTILITY COMPANY PRIOR TO SETTING PAD

10 TRANSFORMER PAD CONCEPT
A101 SCALE: 1/2" = 1'-0" 02751.00.00.302



- GENERAL NOTES:
-STEEL BOLLARD (Z.R.P.) BY STEEL FABRICATOR
-CONCRETE CONTRACTOR TO EXCAVATE AND POUR BASE, SET PIPE BOLLARD, AND FILL BOLLARD WITH CONCRETE
-CONCRETE CONTRACTOR TO WIPE DOWN STEEL BOLLARD CLEAN OF ALL CONCRETE RESIDUE @ TIME OF POURING

9 BOLLARD DETAIL
A101 SCALE: 1/2" = 1'-0" 000000.000

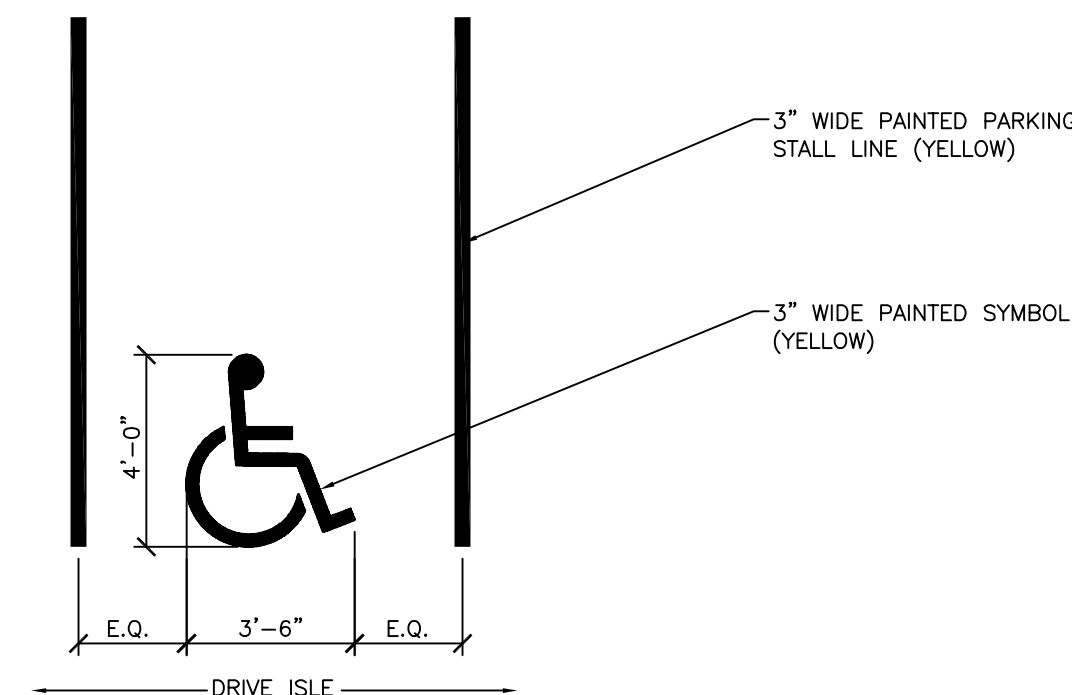


WISCONSIN
VAN ACCESSIBLE PARKING SIGN

8
A101

SCALE: $1/2" = 1'-0"$

02890.00.00.002



7 ACCESSIBLE PARKING STALL STRIPING
A101 SCALE: NONE 02760.00.00.001

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2430 FRAZIER AVE - PARCEL #07019410069
MADISON, WISCONSIN 53713

STARION FINANCIAL
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BISMARCK, NORTH DAKOTA 58501

ISSUE DATES:
UDC INITIAL/FINAL: 05-16-1
CONDITIONAL USE: 05-16-1
REZONING: 05-16-18

RFI/SI DATE:

Schematic Design Phase:

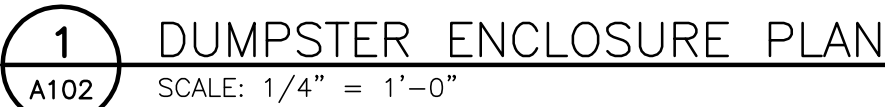
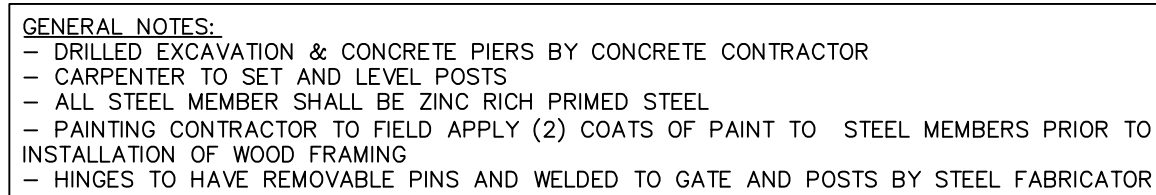
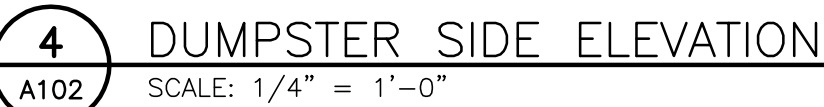
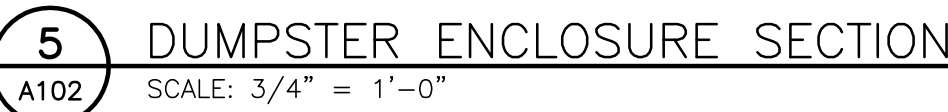
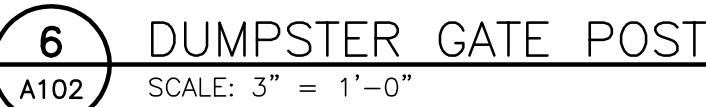
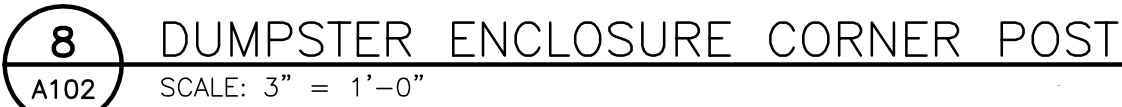
This drawing indicates the scale and relationship of the project components.

*This drawing is **not for construction.***

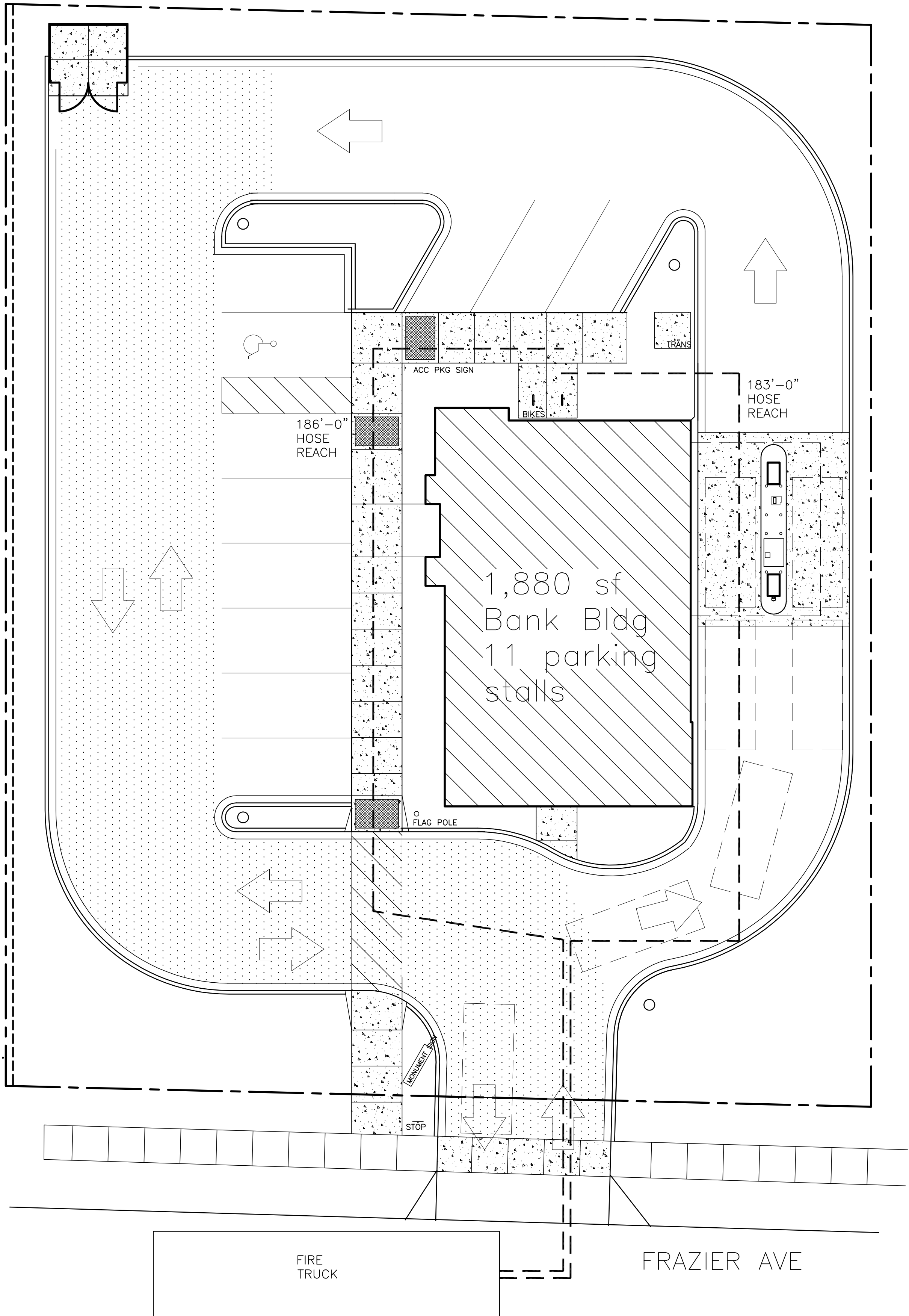
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PROJECT #:	20180130
SHEET NUMBER	

A101



PLOT DATE: 



1 FIRE ACCESS PLAN
A103 SCALE: 1" = 40'-0"



0' 10' 20'
1" = 10'-0"

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2430 FRAZIER AVE - PARCEL #071019410089
MADISON, WISCONSIN 53713
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333 NORTH 4TH STREET
BISMARCK, NORTH DAKOTA 58501

ISSUE DATES:
PRELIMINARY: 05-16-18
CONDITIONAL USE: 05-16-18
REZONING: 05-16-18

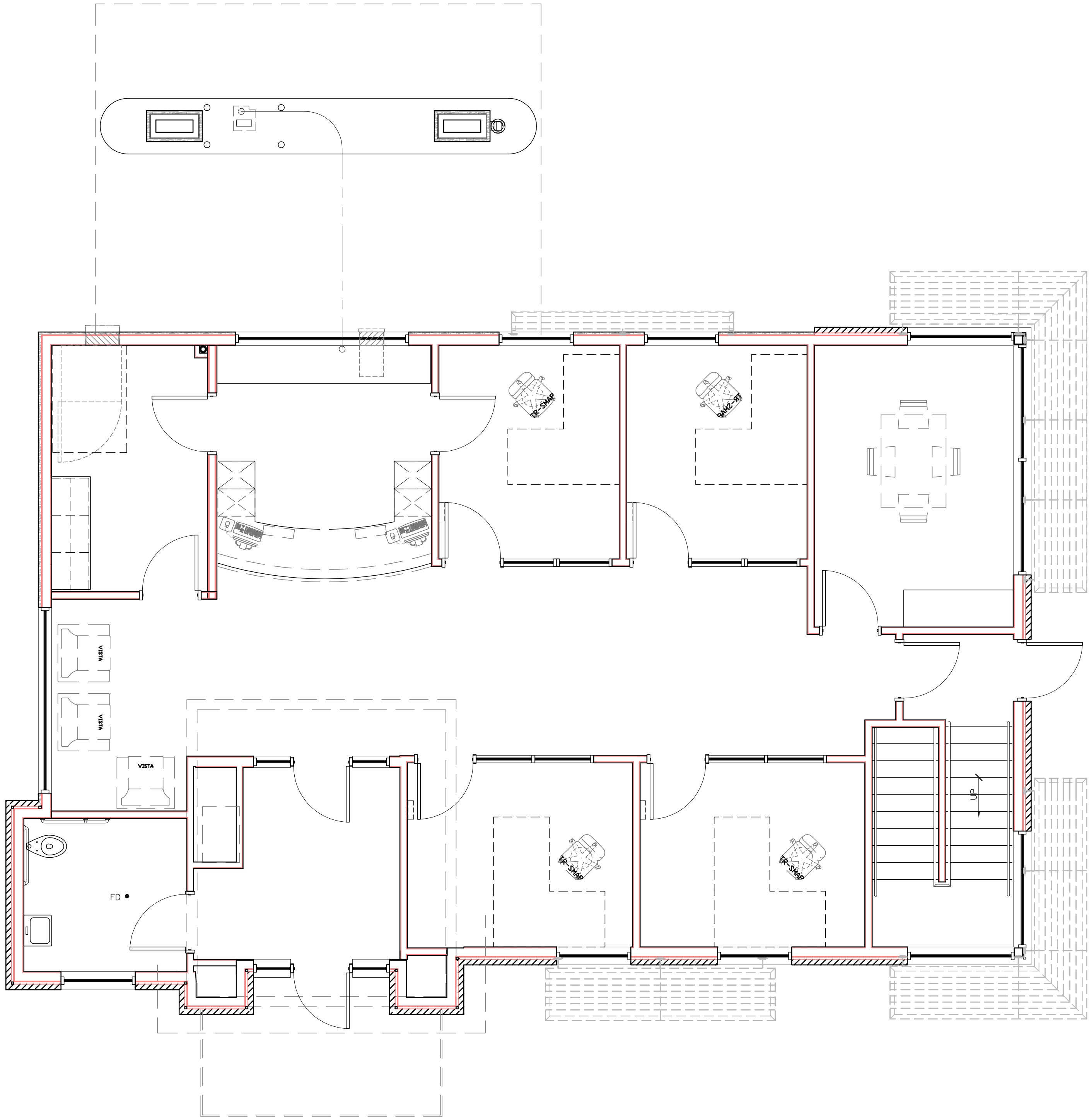
RFI/SI DATE:

Schematic Design Phase:
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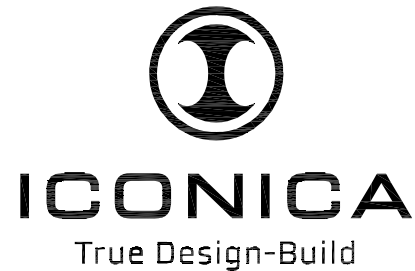
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PROJECT #: 20180130
SHEET NUMBER
A103

PLOT DATE:



1 FIRST FLOOR PLAN
A201 SCALE: 1/4" = 1'-0"



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BISMARCK, NORTH DAKOTA 58501

ISSUE DATES:
REVISIONS: 05-16-18
CONDITIONAL USE: 05-16-18
REZONING: 05-16-18

RFI/SI DATE:

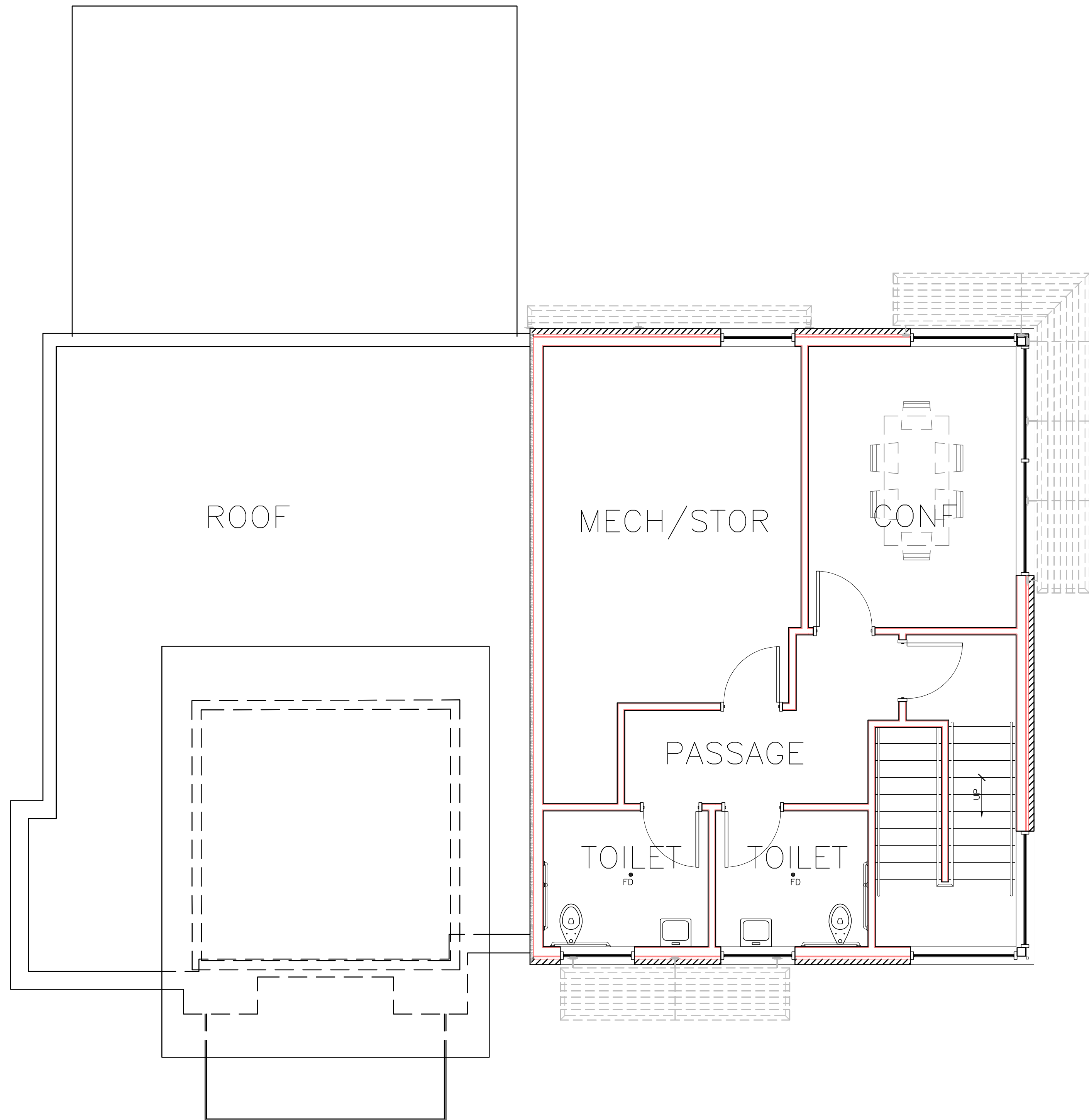
Schematic Design Phase:
*This drawing indicates the scale and relationship of the project components. This drawing is **not for construction**.*

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PROJECT #: 20180130
SHEET NUMBER

A201

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1 SECOND FLOOR PLAN
A202 SCALE: 1/4" = 1'-0"



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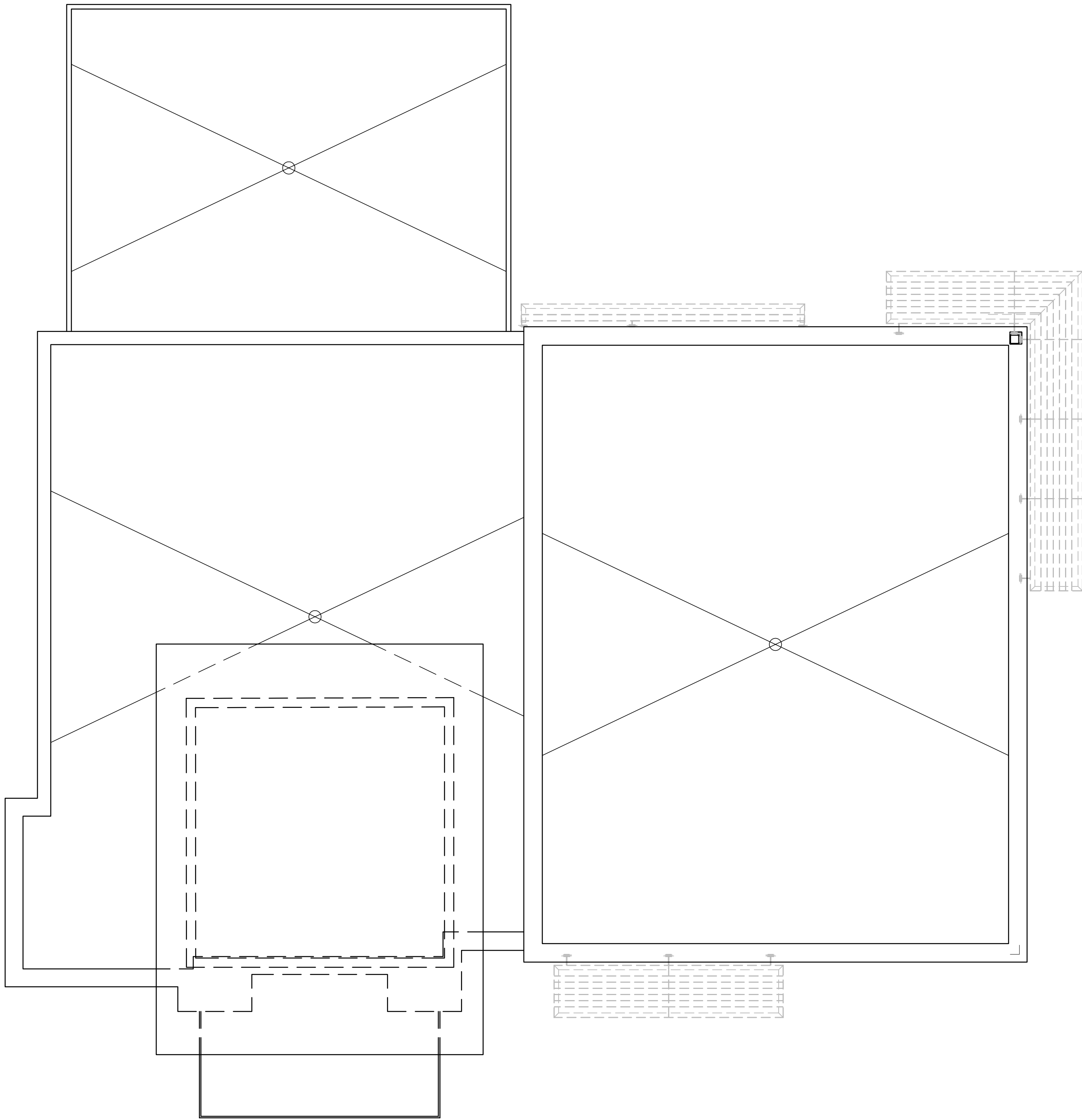
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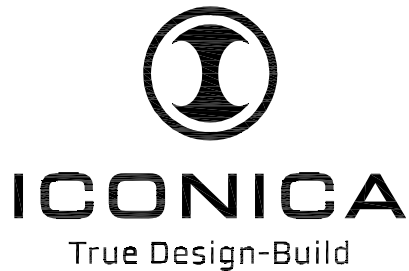
PROJECT #: 20180130
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A202

PLOT DATE:



1 ROOF PLAN
A203 SCALE: 1/4" = 1'-0"



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RFI/SI DATE:

Schematic Design Phase:

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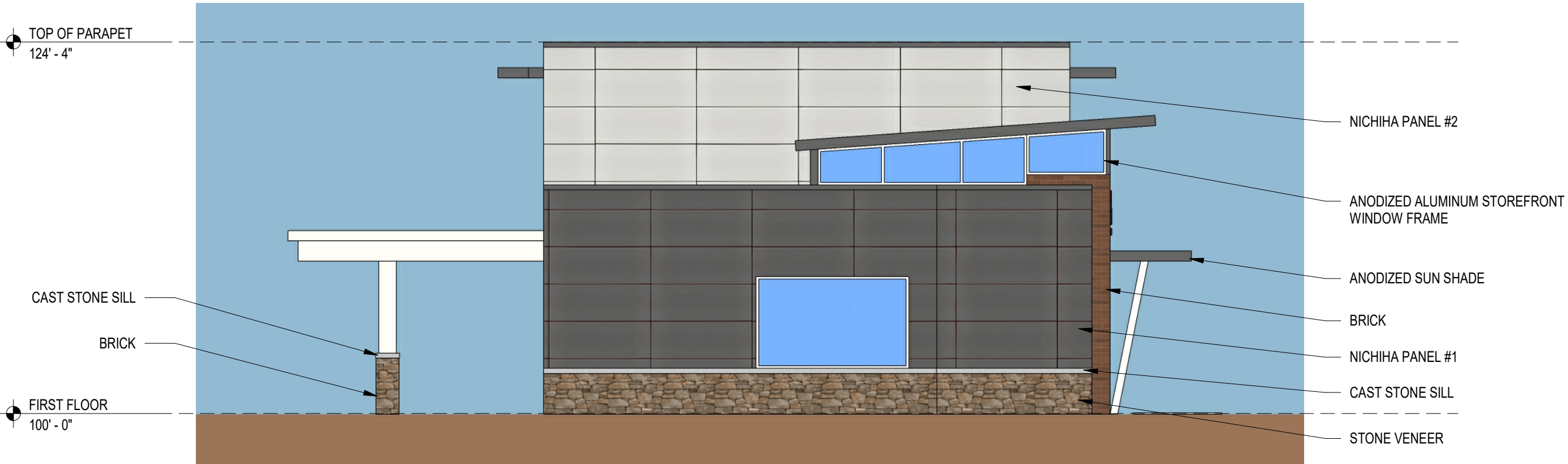
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PROJECT #: 20180130
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A203



1 WEST ELEVATION
A300 1/8" = 1'-0"



2 NORTH ELEVATION
A300 1/8" = 1'-0"



1 **EAST ELEVATION**
A301 1/8" = 1'-0"



2 **SOUTH ELEVATION**
A301 1/8" = 1'-0"



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RF/IS/ DATE:

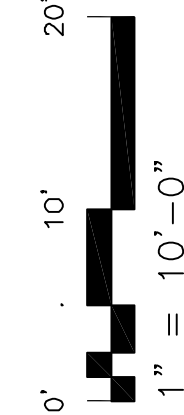
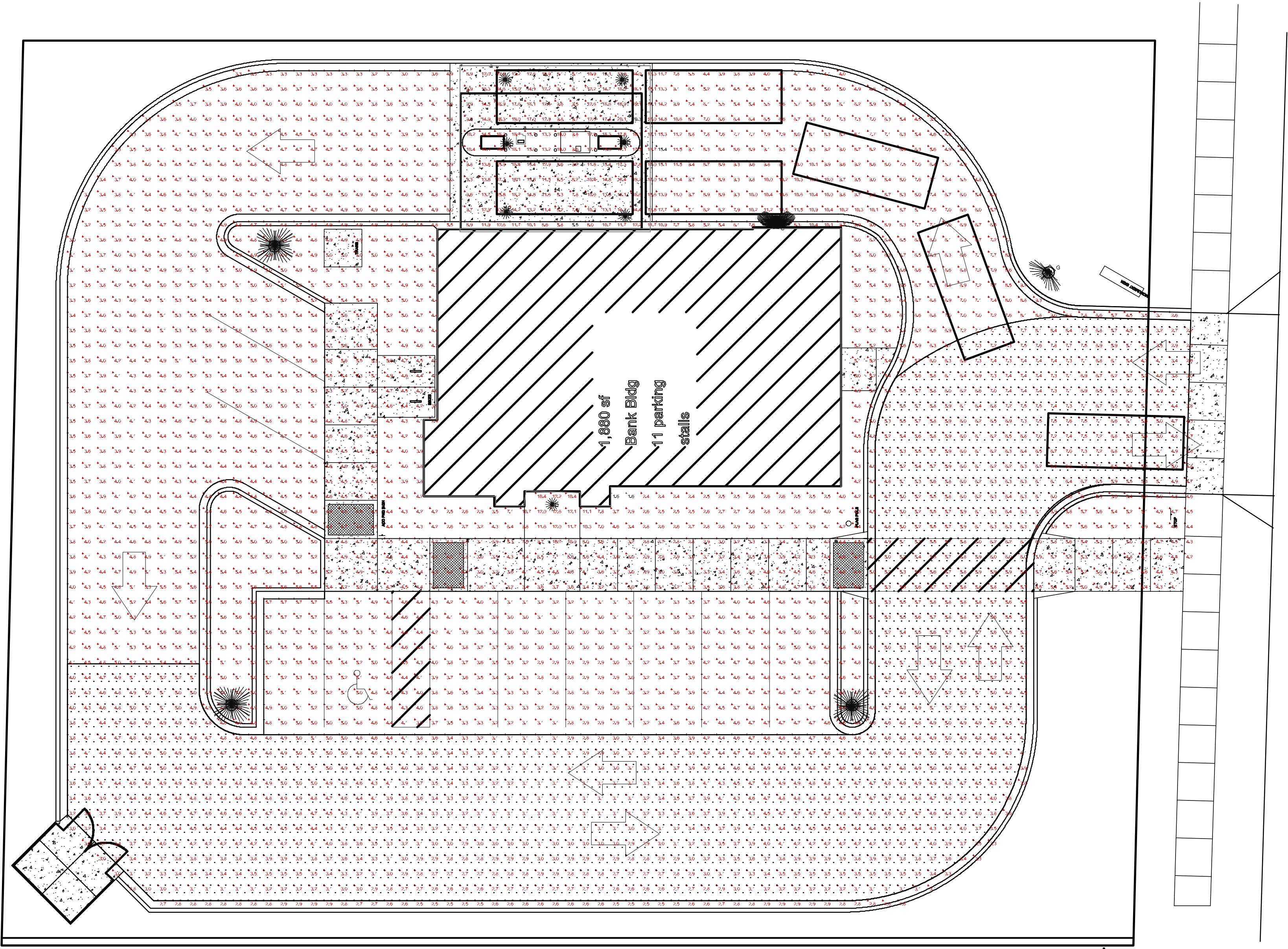
Schematic Design Phase:
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E100

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1
E100

SCALE: 1" = 40'-0"

PHOTOMETRICS PLAN – BUSINESS

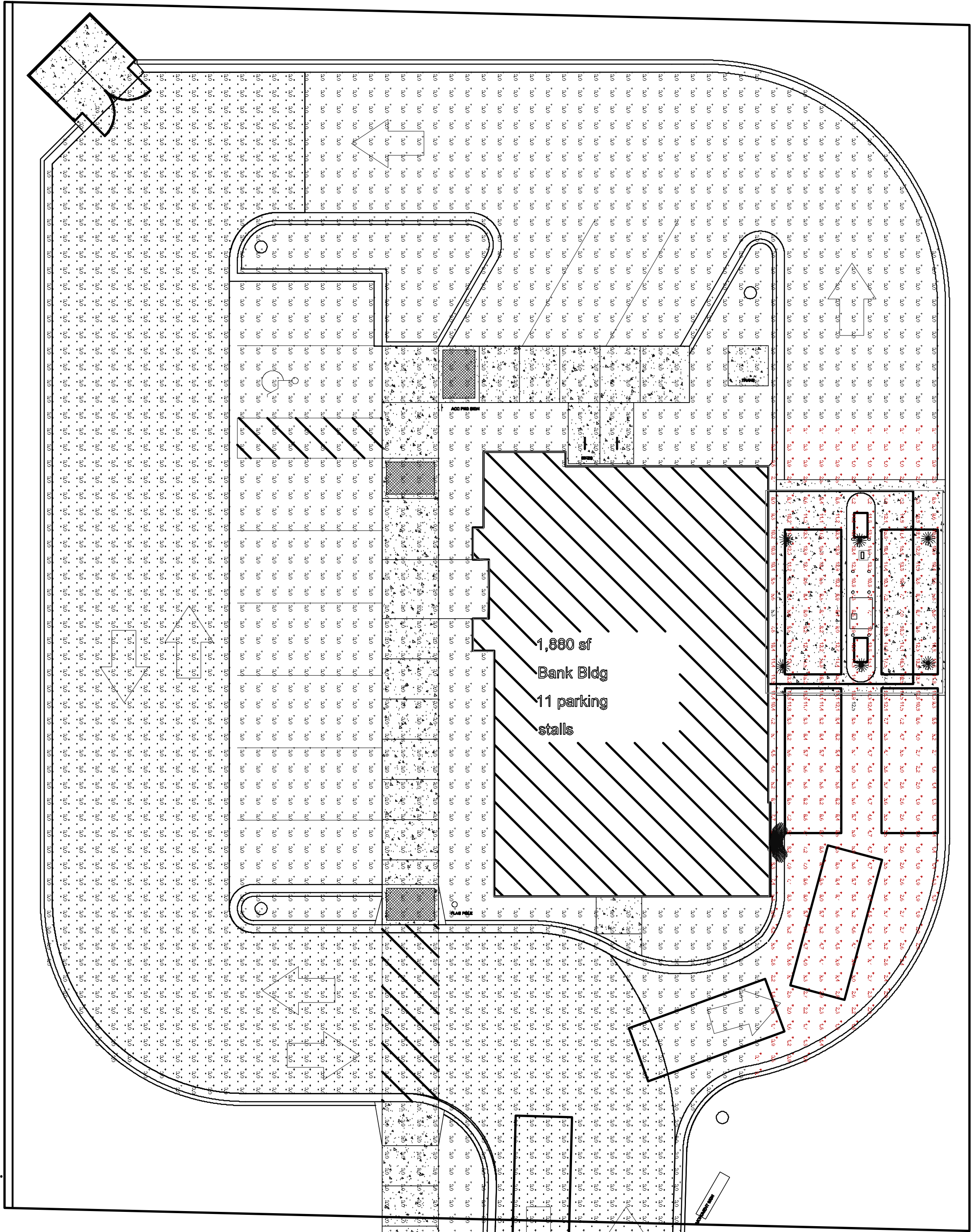
STATISTICS					
DESCRIPTION	AVG	MAX	MIN	MAX/MIN	AVG/MIN
DRIVE CANOPY	13.3 FC	20.3 FC	5.5 FC	3.7:1	2.4:1
PARKING LOT	4.8 FC	15.4 FC	1.6 FC	9.6:1	3.0:1
EAST LOT	0.9 FC	1.8 FC	0.1 FC	18.0:1	9.0:1
NORTH LOT	0.7 FC	2.0 FC	0.1 FC	20.0:1	7.0:1
WEST LOT	0.6 FC	1.2 FC	0.1 FC	12.0:1	6.0:1

LIGHT FIXTURE SCHEDULE						
ID	Description	Manufacturer	Model #	Voltage	Lamps	Mounting
C	WALL MOUNTED EXTERIOR LIGHT	LITHONIA LIGHTING (ACUITY BRANDS)	DSW1 LED 20C 700 40K T3S	WVOLT	LED (INCLUDED)	SURFACE
D	RECESSED CAN LIGHT	GOHAM LIGHTING (ACUITY BRANDS)	EVO 40/15 BAR MD LD	WVOLT	LED (INCLUDED)	RECESSED
F	SITE POLE LIGHT 20'	LITHONIA LIGHTING (ACUITY BRANDS)	DSX2 LED P2 40K T3M	WVOLT	LED (INCLUDED)	RECESSED
G	SITE POLE LIGHT 20'	LITHONIA LIGHTING (ACUITY BRANDS)	DSX2 LED P2 40K T5M	WVOLT	LED (INCLUDED)	RECESSED

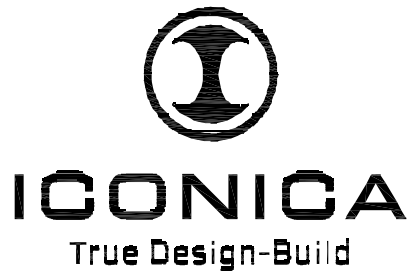
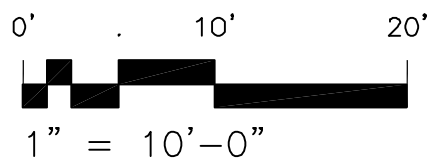
Remarks

STATISTICS					
DESCRIPTION	AVG	MAX	MIN	MAX/MIN	AVG/MIN
DRIVE CANOPY	11.0 FC	17.7 FC	5.3 FC	3.3:1	2.1:1
PARKING LOT	0.4 FC	12.5 FC	0.0 FC	N/A	N/A
EAST LOT	0.0 FC	0.0 FC	0.0 FC	N/A	N/A
NORTH LOT	0.0 FC	0.0 FC	0.0 FC	N/A	N/A

LIGHT FIXTURE SCHEDULE							
ID	Description	Manufacturer	Model #	Voltage	Lamps	Mounting	Remarks
C	WALL MOUNTED EXTERIOR LIGHT	LITHONIA LIGHTING (ACUITY BRANDS)	DSXW1 LED 20C 700 40K T3S	MVOLT	LED (INCLUDED)	SURFACE	
D	RECESSED CAN LIGHT	GOTHAM LIGHTING (ACUITY BRANDS)	EVO 40/15 6AR MD LD	MVOLT	LED (INCLUDED)	RECESSED	



1 PHOTOMETRICS PLAN – NIGHT
E101 SCALE: 1" = 10'-0"



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PROJECT #: 20180130
SHEET NUMBER

E101



City of Madison Fire Department

314 W Dayton Street, Madison, WI 53703-2506
Phone: 608-266-4420 • Fax: 608-267-1100 • E-mail: fire@cityofmadison.com

Project Address: 2430 Frazier Ave, Madison, WI 53713

Contact Name & Phone #: Bob Feller (608) 664-3591

FIRE APPARATUS ACCESS AND FIRE HYDRANT WORKSHEET

1. Is the building completely protected by an NFPA 13 or 13R automatic fire sprinkler system? If non-sprinklered , fire lanes extend to within 150-feet of all portions of the exterior wall? If sprinklered , fire lanes are within 250-feet of all portions of the exterior wall?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> Yes <input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No <input type="checkbox"/> No <input type="checkbox"/> No	<input type="checkbox"/> N/A <input checked="" type="checkbox"/> N/A <input type="checkbox"/> N/A
2. Is the fire lane constructed of concrete or asphalt, designed to support a minimum load of 85,000 lbs? a) Is the fire lane a minimum unobstructed width of at least 20-feet? b) Is the fire lane unobstructed with a vertical clearance of at least 13½-feet? c) Is the minimum inside turning radius of the fire lane at least 28-feet? d) Is the grade of the fire lane not more than a slope of 8%? e) Is the fire lane posted as fire lane? (Provide detail of signage.) f) Is a roll-able curb used as part of the fire lane? (Provide detail of curb.) g) Is part of a sidewalk used as part of the required fire lane? (Must support +85,000 lbs.)	<input checked="" type="checkbox"/> Yes <input checked="" type="checkbox"/> Yes <input checked="" type="checkbox"/> Yes <input type="checkbox"/> Yes <input checked="" type="checkbox"/> Yes <input type="checkbox"/> Yes <input type="checkbox"/> Yes <input type="checkbox"/> Yes	<input type="checkbox"/> No <input type="checkbox"/> No <input type="checkbox"/> No <input type="checkbox"/> No <input checked="" type="checkbox"/> No <input checked="" type="checkbox"/> No <input checked="" type="checkbox"/> No <input checked="" type="checkbox"/> No	<input type="checkbox"/> N/A <input type="checkbox"/> N/A <input type="checkbox"/> N/A <input checked="" type="checkbox"/> N/A <input type="checkbox"/> N/A <input type="checkbox"/> N/A <input type="checkbox"/> N/A <input type="checkbox"/> N/A
3. Is the fire lane obstructed by security gates or barricades? If yes: a) Is the gate a minimum of 20-feet clear opening? b) Is an approved means of emergency operations installed, key vault, padlock or key switch?	<input type="checkbox"/> Yes <input type="checkbox"/> Yes <input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No <input type="checkbox"/> No <input type="checkbox"/> No	<input type="checkbox"/> N/A <input checked="" type="checkbox"/> N/A <input checked="" type="checkbox"/> N/A
4. Is the Fire lane dead-ended with a length greater than 150-feet? If yes, does the area for turning around fire apparatus comply with IFC D103?	<input type="checkbox"/> Yes <input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No <input type="checkbox"/> No	<input type="checkbox"/> N/A <input checked="" type="checkbox"/> N/A
5. Is any portion of the building to be used for high-piled storage in accordance with IFC Chapter 3206.6 If yes, see IFC 3206.6 for further requirements.	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No	<input type="checkbox"/> N/A
6. Is any part of the building <u>greater than 30-feet</u> above the grade plane? If yes, answer the following questions: a) Is the aerial apparatus fire lane parallel to one entire side of the building and covering at least 25% of the perimeter? b) Is the near edge of the aerial apparatus fire lane between 15' and 30' from the building? c) Are there any overhead power or utility lines located across the aerial apparatus fire lane? d) Are there any tree canopies expected to grow across the aerial fire lane? (Based on mature canopy width of tree species) e) Does the aerial apparatus fire lane have a minimum unobstructed width of 26-feet? f) Is the space between the aerial lane and the building free of trees exceeding 20' in heights?	<input type="checkbox"/> Yes <input type="checkbox"/> Yes <input type="checkbox"/> Yes <input type="checkbox"/> Yes <input type="checkbox"/> Yes <input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No <input type="checkbox"/> No <input type="checkbox"/> No <input type="checkbox"/> No <input type="checkbox"/> No <input type="checkbox"/> No	<input type="checkbox"/> N/A <input checked="" type="checkbox"/> N/A <input checked="" type="checkbox"/> N/A <input checked="" type="checkbox"/> N/A <input checked="" type="checkbox"/> N/A <input checked="" type="checkbox"/> N/A
7. Are all portions of the required fire lanes within 500-feet of at least (2) hydrants? <i>Note: Distances shall be measured along the path of the hose lay as it comes off the fire apparatus.</i> a) Is the fire lane at least 26' wide for at least 20-feet on each side of the hydrants? b) Is there at least 40' between a hydrant and the building? c) Are the hydrant(s) setback no less than 5-feet nor more than 10-feet from the curb or edge of the street or fire lane? d) Are hydrants located in parking lot islands a minimum of 3½-feet from the hydrant to the curb? e) Are there no obstructions, including but not limited to: power poles, trees, bushes, fences, posts located, or grade changes exceeding 1½-feet, within 5-feet of a fire hydrant?	<input checked="" type="checkbox"/> Yes <input checked="" type="checkbox"/> Yes <input checked="" type="checkbox"/> Yes <input type="checkbox"/> Yes <input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No <input type="checkbox"/> No <input type="checkbox"/> No <input type="checkbox"/> No <input type="checkbox"/> No	<input type="checkbox"/> N/A <input type="checkbox"/> N/A <input type="checkbox"/> N/A <input checked="" type="checkbox"/> N/A <input type="checkbox"/> N/A

Note: Hydrants shall be installed and in-service prior to combustible construction on the project site.

Attach an additional sheet if further explanation is required for any answers.

This worksheet is based on **MGO 34.503** and **IFC 2015 Edition Chapter 5 and Appendix D**; please see the codes for further information.