

# Darbo-Worthington-Starkweather

## Neighborhood Plan

Prepared by the City of Madison  
Planning Division



Adopted by the Common Council on September 19, 2017, File ID No. 47509  
By Enactment No. Res-15-00436

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- Mentoring Positives, Inc.
- Salvation Army – Darbo

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- Rethke Family
- MIG Commercial Real Estate
- Madison East Shopping Center – Executive Management, Inc.

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PROJECT WEBSITE: [www.cityofmadison.com/dpced/planning/darbo-worthington-starkweather-neighborhood-plan/1661/](http://www.cityofmadison.com/dpced/planning/darbo-worthington-starkweather-neighborhood-plan/1661/)

## EXECUTIVE SUMMARY

The Darbo-Worthington-Starkweather planning area includes elements of a vibrant and healthy community. There are residents of diverse backgrounds, including a variety of family types and people of all ages. The area is also rich with parks, open space, and bike paths. Despite these assets, the neighborhood is somewhat disconnected. It includes four distinct areas with different demographics, land use, housing characteristics, and degree of social and economic capital. The neighborhood planning process can help to bridge these social and physical differences, bringing hope for a stronger, more connected community.

This plan includes recommendations that pave the way for mindful, coordinated reinvestment in people and place for the future. The planning process was grounded in the idea that the community wanted and needed to engage the city in a long-overdue conversation about what is relevant to them. Planning Division staff, other City staff, and non-City organizations stepped back to listen to the community.

Building trust takes longer in communities that have not had positive experiences. Rebuilding organizational capacity and having better representation is necessary for the greater Darbo-Worthington-Starkweather and town of Blooming Grove area. This process involves developing trusted advocates that work together to build resident capacity around important activities and projects.

An array of plan strategies was identified to address community priorities, and a Health Impact Assessment (HIA) was used to better understand health impacts and consequences of plan recommendations.

Although this plan provides recommendations to improve public and private places, the recommendations are mere stepping stones to build a stronger, more cohesive neighborhood. Residents often shared that

*"My hope for the neighborhood is to come together as a community and to support each other with issues people are having." - Darbo resident*

the neighborhood is a good place to live and raise a family. However, there was also discussion of social isolation and separation as barriers, which hindered collaborative improvements to the area.

### Residents would like to see...

- **A multi-service community facility** where residents provide input and decisions on the use, hours of operation, and programming; local residents are employed there; and services include health and education, employment training, recreational activities, and more.
- **Support for education, employment, health and wellness** and other enrichment programs for all ages.
- **Placemaking activities** that will bring people together to create a united community or other structured events to provide culturally sensitive, meaningful interaction among residents of varying age, race, and income.
- **Redevelopment of E. Washington Frontage Road** and other abutting properties to support civic or neighborhood-serving businesses with integrated housing on upper floors.
- **Improved food access and security** by incorporating community or urban gardens, farmers' markets, and other outlets for healthy food in close proximity to the neighborhood. Fresh food should be accessible via travel on foot, by bike, or by transit.
- Other plan strategies that **promote livability**.

## Themes

The document is structured around five themes. The actions and strategies proposed within each theme are detailed further in the implementation strategy.

1. **Land Use, Housing, and Urban Design** – Strategies and actions for development of high quality buildings and livable places for residents, and better care of ecosystems. Land use, zoning, local activity centers, streets, housing choices, ecologically sustainable development, energy efficient buildings, and other measures are detailed.
2. **Transportation Connectivity and Access** – Strategies and actions to improve walking, biking, and public transit. Special emphasis is placed on health, safety, and connectivity to neighborhood or nearby destinations.
3. **Community and Individual Health and Wellness** – Strategies and actions to rehabilitate existing places to benefit the personal and social growth of families and community. Examples of places include: community centers, libraries, schools; and unused places with potential for creative, cultural, and educational pursuits.
4. **Economic Development and Employment** – Strategies and actions to better connect people to employment opportunities and to promote and direct resources for economic development; improve communications of apprenticeships, internships, and other training.
5. **Parks and Open Space** – Strategies and actions to improve the built and natural environment; increase accessibility of parks and open spaces; and increase the organized recreational, educational, and other programming that takes place in and around these green spaces.

## CHAPTER 1. INTRODUCTION

Over the past several decades, the City of Madison has worked with residents and the business community to prepare geographically based plans focused on identifying ways to improve the area.

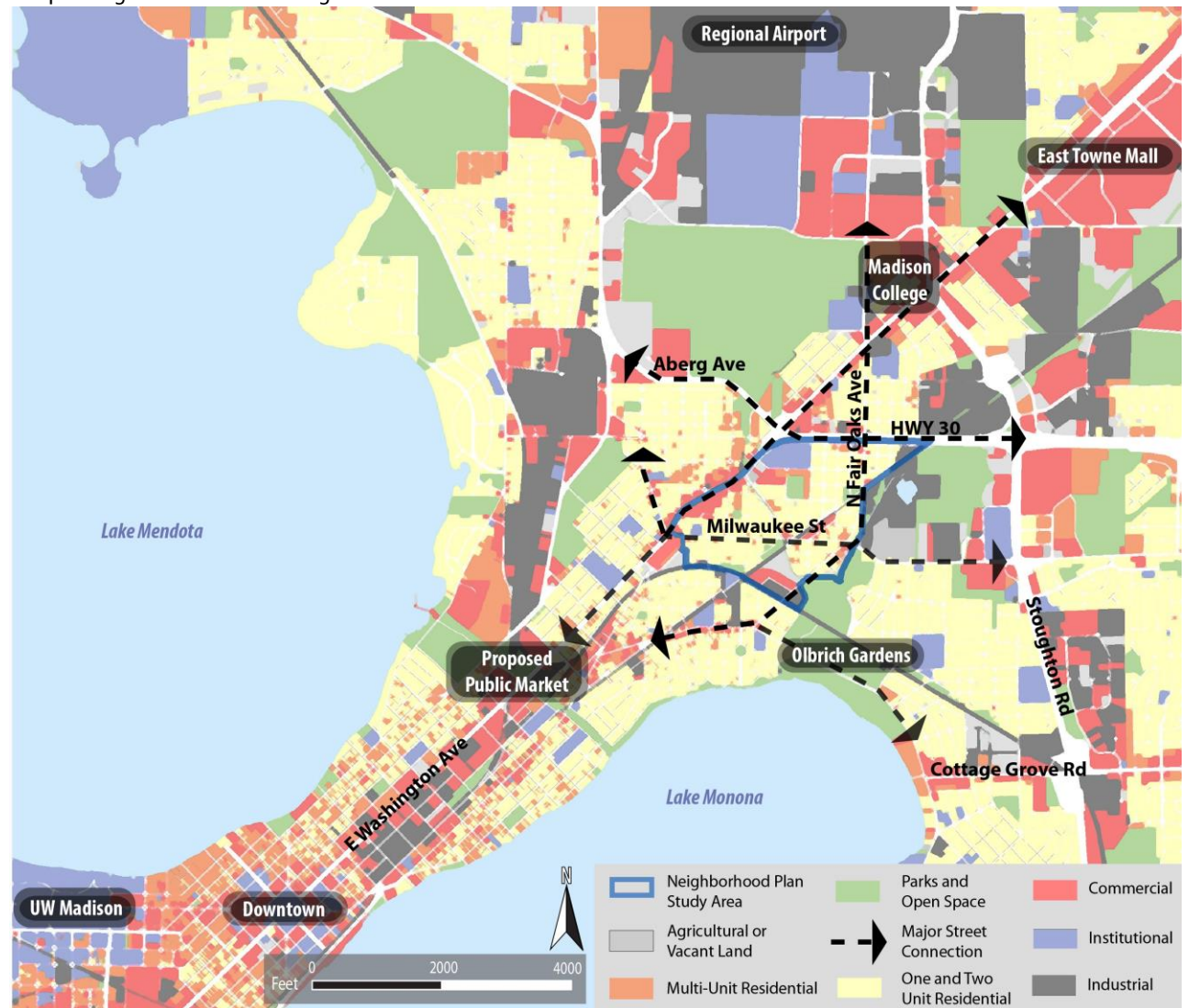
- Starkweather Action Program (1987-1991)
- [Schenk-Atwood-Starkweather-Worthington Park Neighborhood Plan](#) (adopted 2000)
- [East Washington Avenue Old East Side Master Plan](#) (adopted 2000) and supplement to it, [Gateway Revitalization Plan](#) (adopted 2003)
- [Schenk-Atwood Neighborhood Business District Master Plan](#) (adopted 2001)
- [Starkweather Creek Master Plan Update](#) (adopted 2004)

In addition, reports, such as the Madison Public Library's *Communities Inspiring Libraries: A Strategic Plan for Eastside Growth* (2016), were used to inform priorities for the area.

Over the last seventeen years, public and private investments have occurred in the study area. This update to the original neighborhood plan revisits what has been done and what is still desired by the residents and business community. The plan update identifies priority

activities and projects to improve existing conditions, and identifies opportunities to set in motion over the next ten years.

Map 1: Regional Context: Existing Land Use



Source: City of Madison Planning Division 2017

## Background

In 2015, the City of Madison Community Development Block Grant (CDBG) Committee selected the Darbo-Worthington-Starkweather Neighborhood Plan Study Area to receive planning services. The planning study area includes portions of the city of Madison and the town of Blooming Grove (*Map 1: Regional Context, and Map 2: Neighborhood Plan Study Area*).

The CDBG Committee recommended planning services in this area for two reasons. First, there is a higher proportion of low and moderate income households compared to the city as a whole. Second, because the last neighborhood plan for the area was completed 17 years ago, there was need for an update.

The study area for the planning process differs from the previous neighborhood plan as it encompasses a smaller portion of the Schenk-Atwood-Starkweather-Yahara neighborhood and includes a portion of the town of Blooming Grove, which is expected to be annexed by the City of Madison in 2027 or sooner.

For the first time in a neighborhood planning process, a Health Impact Assessment (HIA) was conducted. A Dane County BUILD (Better Urban Infill Development) grant was used to hire a consultant to investigate health determinants in four priority areas which were identified during the planning process: safety; mobility and accessibility; social connectedness and empowerment; and environmental and built conditions. This was followed by assessing preliminary plan strategies and recommendations based upon short-term and long-term health impacts to the community.



*Hippo sculpture at Wirth Park created by Sid Boyum*



*Make Music Madison at Worthington Park, Summer 2016*

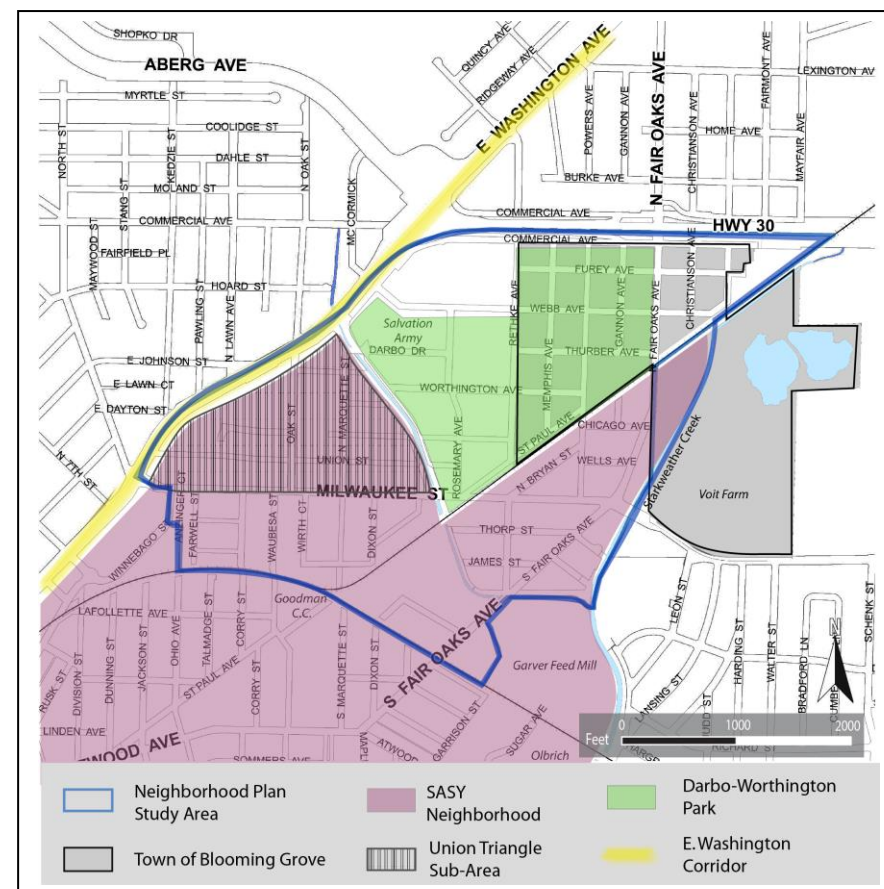
## Distinct, Separate Sub-Areas

Four distinct sub-areas make up the planning area:

- **Darbo-Worthington Park** – Mix of single-family homes and a cluster of small scale multi-family housing. The makeup is predominantly low to moderate income households. Residents are in the process of reactivating the Worthington Park Neighborhood Association. There are also several trusted service providers and community-based organizations working in the area.
- **SASY and Union Triangle** – SASY and a subarea of SASY referred to as Union Triangle. It has a mix of single-family and two-family housing and includes primarily moderate-income households.
- **Town of Blooming Grove** – Predominantly single-family housing with moderate income households. Part of the area is within the Worthington Park Neighborhood Association boundary. Residents have long history of working with Town government rather than the City. This subarea will be annexed to the City by 2027, or sooner.
- **E. Washington Corridor** – Heavily travelled corridor with a mix of fast food, gas stations, and a strip mall and two public services: a library branch and public health clinic. This area has a tired appearance with limited reinvestment over the last ten years.

The planning study area is bounded by E. Washington Avenue, Highway 30, railroad tracks, and Starkweather Creek. These manmade barriers can impede easy access and strong connections to the surrounding area.

Map 2: Neighborhood Plan Study Area



Source: City of Madison Planning Division 2017

## Planning Process

The City of Madison Planning Division led the planning process. Planning staff, neighborhood associations, and community stakeholders began gathering public input with an open house in February 2016. Over 15 months, residents, the business community, and stakeholders participated in the neighborhood plan (*Figure 1: Planning Process and Public Participation*).



*Town of Blooming Grove Neighborhood Walkthrough, August 2016*

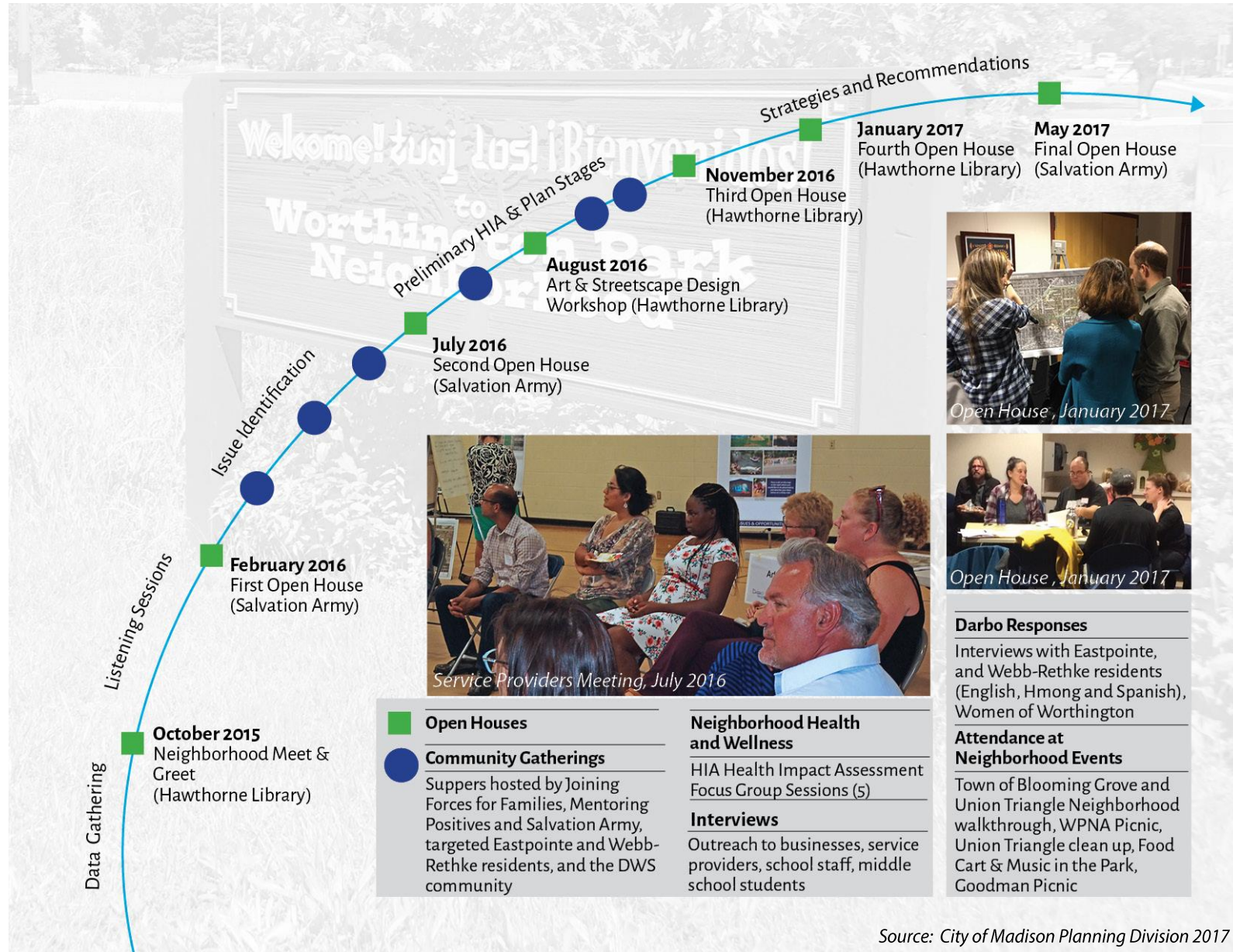
Planning staff worked closely with trusted community advocates to help ensure representative input from the racially and ethnically diverse residents of the area. Five open houses, various focus groups, a service providers meeting, neighborhood walkthroughs, and other community conversations provided the foundation for plan strategies and recommendations. Additionally, the Health Impact Assessment



*Worthington Park Neighborhood Picnic, 2016*

(HIA) for this planning process included a service provider questionnaire and interviews, two focus groups and three public open houses.

Figure 1: Planning Process and Public Participation



Source: City of Madison Planning Division 2017

## Neighborhood History

While the isthmus was developing during the 1800s, near-east neighborhoods, such as the Darbo-Worthington-Starkweather area, remained farmland. Despite its agrarian use, the neighborhood was not immune from development and residential construction, which began in the late 1800s. In 1901, the East Side Land Company began platting the Fair Oaks neighborhood. Both industrial and residential buildings were constructed during this time and the area was promoted as being “well suited for the working man.” Fair Oaks was established as a village in 1906, and in 1913, the community was incorporated into the City of Madison.

Today, many streets in the neighborhood carry the names of prominent developers who were affiliated with the East Side Land Company or were land owners of the area. Examples include:

- **Clyde Gallagher** (1890-1947) left his mark on the neighborhood by overseeing the development of hundreds of homes in the planning area, particularly the area around what is now Worthington Park.
- **Rolf Darbo** (1907-1984) was a prominent realtor on the east side throughout the 1940s, procuring land from the Rethke family. Darbo Drive carries his name. See sidebar on the next page.
- **William Rethke** (c. 1897-1969) owned a sizeable farm on the outskirts of Madison. The family’s land was used to expand E. Washington Avenue, then Sun Prairie Road, and develop much of the current Darbo area. The family still owns two parcels along E. Washington Avenue.

## Business and Commercial

The rise in car ownership rates shaped the development pattern in the area. As suburban commuter patterns developed, new businesses

sprang up to accommodate customers. In 1952, the Madison East Shopping Center was built on E. Washington Avenue. It was the first suburban-style shopping destination in the city. It boasted a Kroger grocery store and a Manchester’s East department store (the first store to use escalators!). The national trend of suburbanization and development outside of the urban core was also seen in the same year when Farmers Mutual Automobile Insurance Company (a.k.a. American Family Insurance) located its headquarters on E. Washington Avenue, where the Wisconsin Department of Corrections is now located. The commercial boom in the area continued into the 1960s, evidenced by the 1964 opening of the A & P Supermarket at 3030 Darbo Drive, where the Salvation Army is currently located.

However, this boom time did not last for long. In 1971, the Madison East Kroger closed and in the following year, the A & P Supermarket closed. During the years following, the 3030 Darbo Drive location was used as a document depository for Madison’s public libraries. In 1981, the site was bought by American Family Insurance to be used as its printing shop and for supply storage.

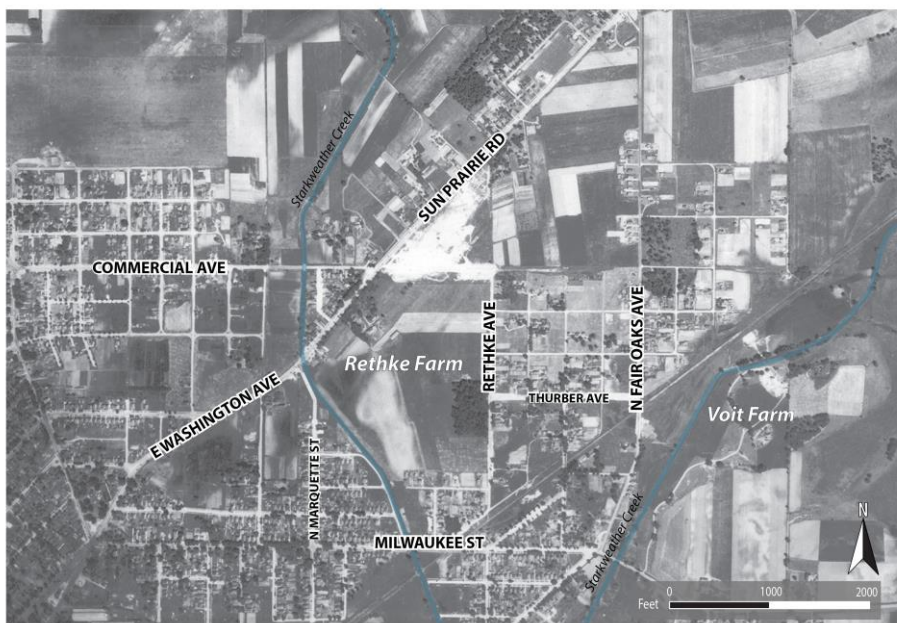
During the early 1990s, the Darbo neighborhood was experiencing a time of unrest. Crime, drugs, and violence were common occurrences. The president of the Worthington Park Neighborhood Association successfully organized the neighborhood in identifying the need for a community center where residents would take an active role to improve the neighborhood.

In August of 1994, after American Family Insurance’s relocation, the Salvation Army purchased the property. The neighborhood wanted a nearby full-service community center with residents taking an active role in determining services and programming. Over the years, the Salvation Army provided limited space for neighborhood-based programming, and therefore, there is still the need of having a more comprehensive community facility with resident-driven programming in the area.

## Residential and Social

During the 1960s, multi-family housing was built in the area. The Easton Square Apartments (a.k.a Eastpointe Apartments) were constructed in 1961, and the City's Community Development Authority built housing bordering Worthington Park in 1965. Worthington Park was created a few years earlier in 1962.

The Worthington Park Neighborhood Association was first founded in 1977. After being defunct for a period of time, the organization resumed operation in 1993 and residents focused their activism on establishing a community center in the neighborhood.



1937 Aerial View of Planning Study Area

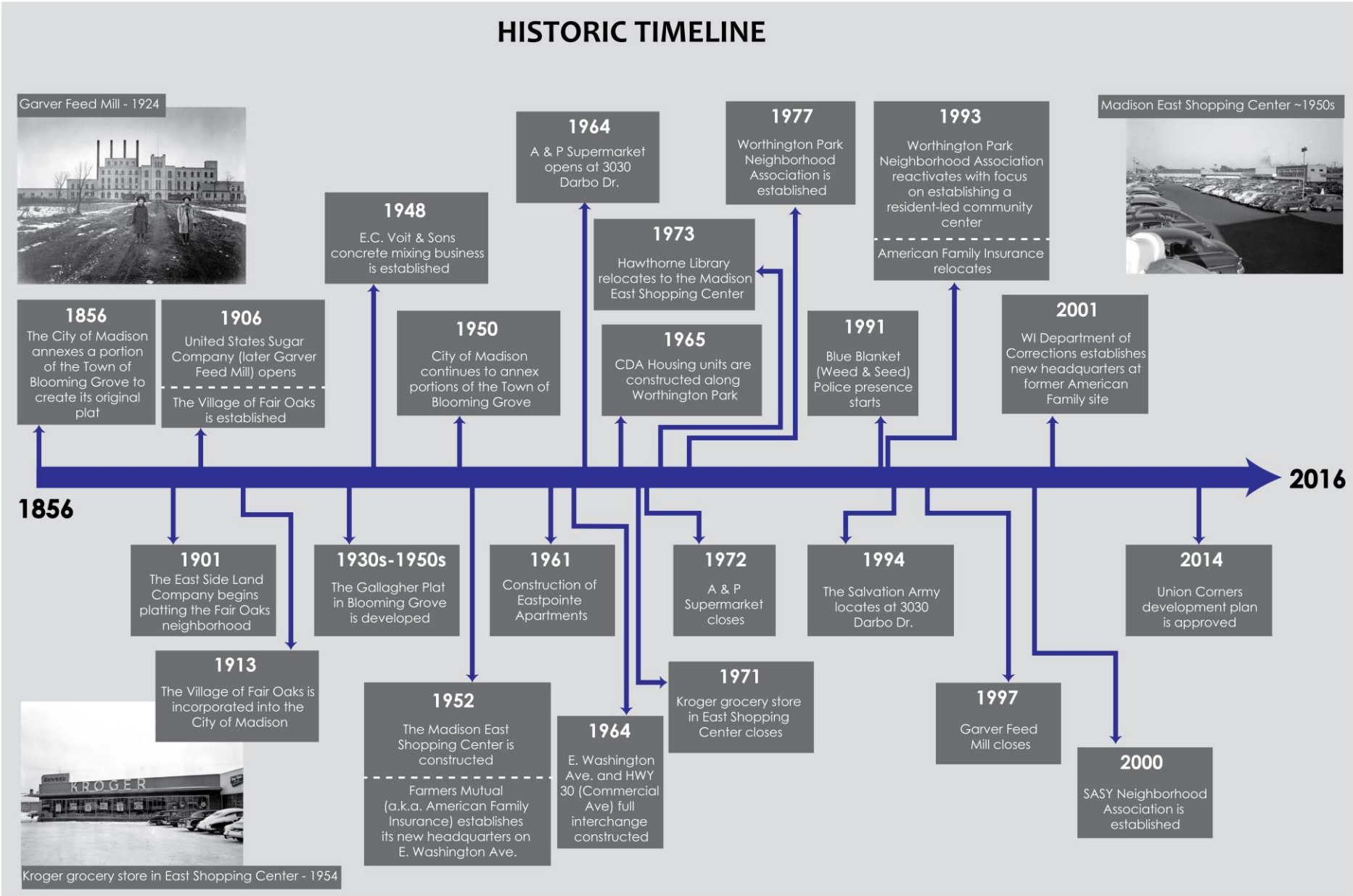
### Rethke Family Farm

William A Rethke (Bill) was born around 1897 on a farm just outside of Madison's city limits. It was a mid-sized farm with crops (tobacco, hemp and sugar beets), dairy cows, chickens, a three story barn, large farm house with a wrap-around porch and numerous out-buildings. There was a second smaller house for a hired farm worker to live in. Sun Prairie Road, which later became E. Washington Avenue, ran along the northern boundary of the property. The Rethke family farmed the land for many years. Bill Rethke was an only child and inherited the farm after World War II. Around this time, the city of Madison was expanding and decided to widen and improve Sun Prairie Road. This meant that the farm house had to be moved a substantial distance. This road improvement became the present day E. Washington Avenue. The Rethke family continued to live in the house during the move. My uncle talked about lifting the floor boards of the house to see how far the house had moved that day. In addition to moving the farmhouse, there was a large hill called Rethke Hill that had to be taken down to facilitate the road improvements. In the winter, kids would come from all over to sled down the hill. It was aptly nicknamed "Wreck" Hill, a modification of Rethke Hill, as a testament to its great sledding capabilities.

Bill Rethke decided to sell the outlying acres of the farm to a gentleman named Darbo to be developed into a combination of residential and commercial properties. However, he retained a cluster of three parcels of land. One parcel he gifted to the City of Madison for a park. Another parcel was eventually sold to American Family Insurance Company and the third parcel was on a small hill that marked the final resting spot for the old farm house after it was moved for the post World War II road expansion. This new spot was also near to the original barn and was beautifully placed amongst a grove of hickory trees. Only a few of the trees can be seen today near the old American Family Insurance site. The area was nicknamed Hickory Hill by the family and remained their personal residence during the 50's and 60's. There were wonderful views of the capitol building from the second story of the farmhouse. The barn was remodeled to house 6 individual apartments and a garage and was used as such until the mid-70s when the land was subdivided into three commercial lots (3059, 3077 E. Washington Avenue and the old McDonald's property near the pedestrian bridge).

- Written by Denise Christy, granddaughter of William Rethke

Figure 2: Neighborhood Historic Timeline



Source: City of Madison Planning Division 2017

## CHAPTER 2. ISSUES AND OPPORTUNITIES

Many residents and stakeholders shared neighborhood assets, issues, and improvement ideas during the planning process. The most important issues and opportunities shown below provide a foundation for the plan's recommendations and strategies.

### Land Use, Housing, and Urban Design

- **Aging, Dated Single-Family & Multi-Family Housing** – Although the housing stock is well-constructed, the average home is 80 years old. Upgrading the exterior and interior of area homes will improve living conditions and provide reinvestment for the greater neighborhood. City and County housing programs exist but are not widely known.
- **Path to Homeownership** – Over time, increased neighborhood desirability will lead to higher home values. A concentrated approach to create a path for homeownership for lower income households and ensuring that single-family properties remain affordable will create additional housing options.
- **Range of Housing Options** – A range of single-family and multi-family housing is located in the neighborhood. The largest concentration of affordable, multi-family housing is in the Darbo area. There is no senior or assisted-living housing in the planning area. Redevelopment along major transportation corridors could provide new housing opportunities.
- **Preserve Small Houses on Small Lots** – There are many small homes on small lots in the planning area, providing an affordable homeownership option, and helping to define the neighborhood character. Efforts should be made to preserve small houses on small lots to provide low to moderate income residents the opportunity to become homeowners.
- **Neighborhood Makeover** – Public art, attractive plantings, benches, or other friendly amenities can make an area more



*Salvation Army Mural*

interesting. Properties along E. Washington Avenue have a dated appearance. Some parking lots are not well-landscaped and buildings need a touch up. Picking up trash along bike paths, parks, and waterways will also improve the area's appearance and show that people care.

### Transportation Connectivity and Access

- **Bus Frequency/Service** – Weekend bus service in the planning area is infrequent, with gaps of 60 minutes in some cases. Transit dependence in the neighborhood is relatively high. Increasing bus service frequency, installing amenities at heavily used boarding locations, providing local outlets for Metro pass purchases, and installing grocery bag storage racks on buses will be important steps toward improving access.
- **Vehicular Volume, Speed, and Parking Issues on Local Streets** – Commuter parking on Rosemary and Webb Avenues causes conflicts with area residents by limiting park use, inhibiting safe street crossing, and reducing tenant parking spaces. Cut-through traffic on Oak and N. Marquette Streets, as well as peak hour cut-through on Rosemary and Webb Avenues are safety issues.

- **Biking** – Completing gaps in the bike trail system can improve route efficiency to employment centers and other destinations. Bike skills and safety can be promoted through bike rodeos, adult bike workshops, and bike clinics at schools. Bike ridership can be promoted through bike share or B-cycle stations and bike routes on low volume streets. Improving bike facilities on Fair Oaks Avenue and Milwaukee Street will improve access to community centers, grocery stores, and schools.
- **Walkability** – Improving safety for pedestrians crossing E. Washington Avenue at Johnson Street, Starkweather Creek at Hermina and Dawes Streets, and constructing sidewalks on high priority streets in the town of Blooming Grove would facilitate pedestrian movement.

## Community and Individual Health and Wellness

- **Access to Fresh Food** – Four full service grocery stores are nearby but difficult to access without a vehicle. A full service grocery store is not likely to open closer to the neighborhood, and gas stations, convenience stores, and Walgreen's offer limited fresh and healthy food products. Community gardens and farmers' markets offer seasonal produce, but their presence is limited. Fast food restaurants are very prevalent along E. Washington Avenue. Improving access to healthy food outlets and urban agriculture can improve food security and food safety.
- **Fitness Center, Classes, and Programs** – The Salvation Army provides some youth recreation activities, but there are limited activities for adults. Affordable classes, walking and biking clubs, or other creative ways to provide fitness opportunities for all ages will improve health outcomes.
- **Health Services Resource Advocate and Center** – It is difficult to navigate the complicated landscape of social services. A one-stop resource center could assist persons/families with accessing,

planning, and navigating the system. This could lead to improvements in residents' health.

- **Locally Based Programming and Center Activities** – The Salvation Army is a regional facility with a focus on serving a territory greater than the nearby neighborhood. The neighborhood desires a facility where residents determine the programs and services that will be offered and the hours of operation.
- **Neighborhood Perception** – Efforts should be made to address how outsiders perceive the greater neighborhood. This is a neighborhood with great potential, while some perceive that this is not a place to invest in.
- **Service Coordination and Collaboration** – All households should be able to thrive, have their basic needs met, and be able to access and utilize educational, physical, and mental health services. Programs and services desired at a locally based neighborhood center include: computer access, ESL classes, exercise classes, fitness center, job training, health screening, mental health care, nutrition and cooking classes, pre-natal care, and tutoring.
- **Resident Engagement** – People care about their neighbors and the neighborhood. Better ways to engage with neighbors and allow people to contribute their time and energy are needed. Tapping into local talents to take on quick, smaller-scale projects in parks, parking lots, and other public places would have a big impact. Other initiatives to improve residents' lives are wanted, especially those planned and executed by residents.
- **Cohesiveness** – Four different neighborhood areas are included in this planning effort. Bringing leaders and residents of these subareas together to build relationships and share resources is key to cultivating a more fulfilling neighborhood environment. An active and engaged neighborhood can become an effective advocate for what the neighborhood wants to the City, County, nonprofits, and others.

## Economic Development and Employment Opportunities

- **Employment, Entrepreneurship, and Economic Development along the E. Washington Avenue Corridor** – The Capitol East District is undergoing a renaissance with new business and housing development. Union Corners and Garver Feed Mill redevelopment projects will influence the business mix of the 2600-3000 blocks of E. Washington Avenue. Economic incentives can be used to start-up or expand businesses, especially those that will employ neighborhood residents. Older commercial and industrial buildings can be repurposed for locally owned businesses, and those that offer career ladder, wage earning potential.
- **Apprenticeship, Job Training, and Entrepreneurship** – Job placement and performance can be improved by securing major employers who provide a pipeline for employment. Further neighborhood economic benefits can be realized by establishing first source hiring agreements/contracts to tap into the community base, securing employment/community benefits agreements for city leveraged projects, and working with local contacts to improve partnerships with educational and trade organizations. Garver Feed Mill redevelopment will have the potential to add urban agricultural spaces and job opportunities for local residents.
- **Urban Agriculture, Locally Produced Products** – Food security, nutrition, and income generation are some key motivators for urban food production. Off the Block Salsa is one locally made product. Exploring other cash crops – such as honey, fish, or other high income generators – creates potential for employment and business generation. Building upon the growing food industry and bringing locally produced products to market could also promote new job growth.

- **Digital Divide** – There is high demand for the Hawthorne Branch Library computers, which can be used for job searches and job skills improvement. Low-cost fiber optic internet service was installed in the Darbo area in 2017. Equipment and basic education, technical support, and household costs still need to be addressed to help close the digital divide.

## Parks and Open Space

- **Isolated, Limited Sight Lines at Reger Park** – Reger Park is a mid-block park surrounded by private residences and has an isolated feel, which can lead to inappropriate park uses. Overgrown shrubs, limited sight lines from the park entrance and backyards, and infrequent structured activities create a safety issue.
- **Upgrades to and Activation of Worthington Park** – Worthington Park is a highly valued open space for residents. A new shelter was installed in 2016. New recreational amenities, park programming, improvements to lighting and sidewalks, and solving parking issues will make the park more attractive.
- **Starkweather Waterway** – Two branches of the Starkweather Creek flow through the neighborhood. The waterway is navigable, but in many areas the banks are steep and lack landings for fishing, boating, or viewing. Improving access to this waterway will create an urban boating experience that connects the neighborhood to Lake Monona by paddling.
- **Thurber Park** – A shelter, playground, and play field are recreational features at this town of Blooming Grove park, which is expected to be attached to the City of Madison by 2027. Recreational programming, community gardening, and possible use of the former Waunona Sanitary District office as an artist in residence studio could help activate the park. Retaining the outside access to the bathrooms for the public is important.

## CHAPTER 3. PLAN ELEMENTS

### Land Use, Housing, and Urban Design

A City's Comprehensive Plan is the main policy document that shapes land use and addresses housing, transportation, economic development, and other aspects of community life. The City of Madison's Comprehensive Plan (2006) was used as the framework to assess current and future scenarios for this planning area, see *Map 3: Future Land Use*. The E. Washington corridor of the planning area is part of the Urban Design District 5. Other factors that influenced the future land use recommendations are included in the adjacent sidebar.

The **Future Land Use Map** recommends maintaining current land uses and intensity of development in most parts of the neighborhood. There are some areas of potential land use change, which fall within designated focus areas. *Map 4: Focus Areas* and the focus area descriptions, include recommended land use changes, site concepts, and design guidelines. *Map 5: Maximum Building Heights* includes maximum building heights to guide development and redevelopment in a way that respects the character of the surrounding neighborhood.

### Housing

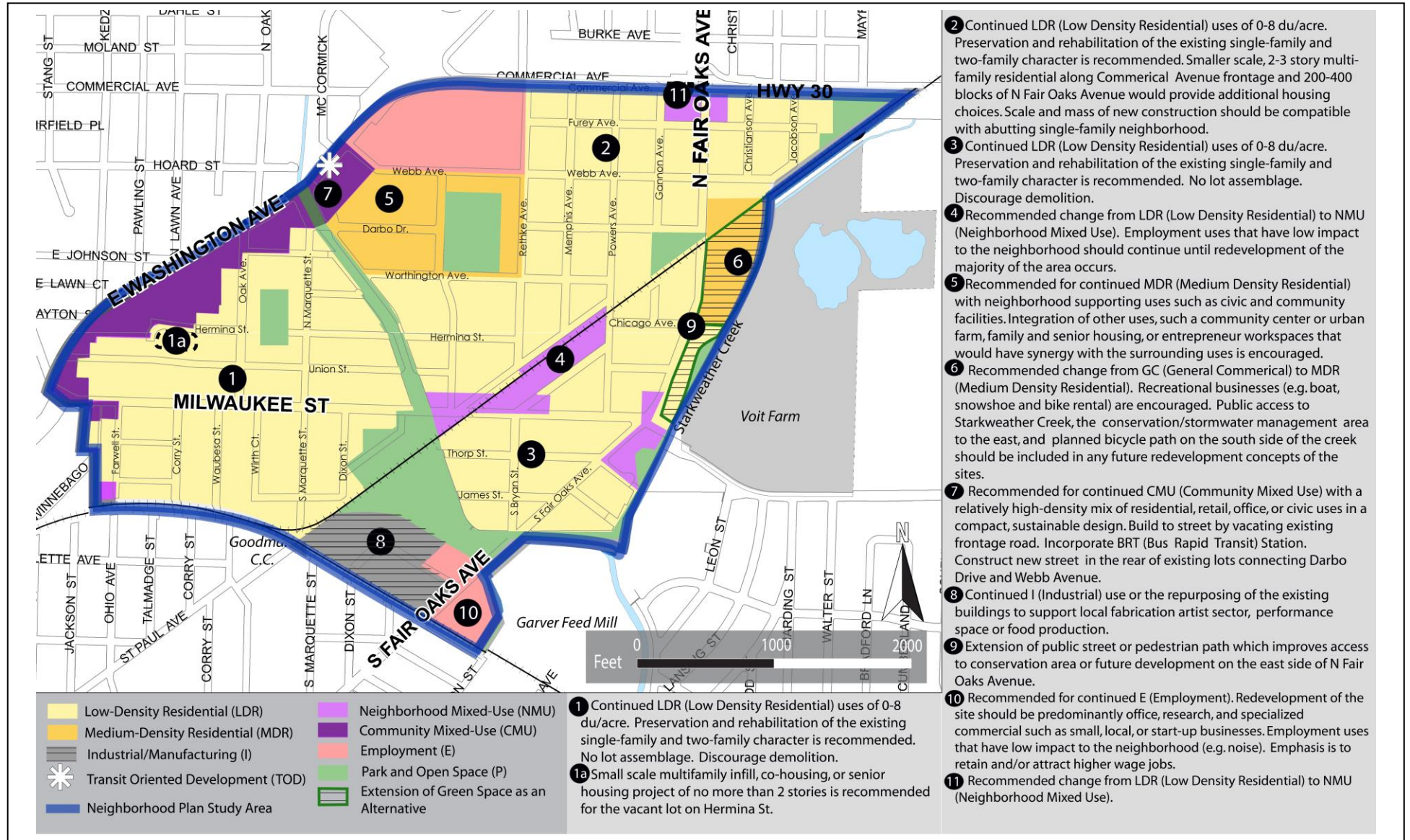
Residents would like to preserve the single-family character of the neighborhood. A priority recommendation is retrofitting older housing stock to make it more visually appealing, longer lasting, energy efficient, and accommodating for varying family situations. To help facilitate these improvements, information can be provided to property owners and tenants on programs for: 1) weatherization and other energy improvements; 2) adaptive features for seniors or persons with disabilities; and 3) rehabilitation/upgrades to rental housing. Providing pathways to homeownership, especially for lower income households, will provide opportunities for residents to stay in the neighborhood.

When redevelopment is pursued, a variety of housing options should be considered with a range of affordability levels. This will create a stronger, more vital neighborhood where people of many different backgrounds are welcome.

#### Factors influencing the Future Land Use Map

- **Community Serving Uses** – Many people living in the planning area could benefit from vital services and recreation opportunities located close by to help improve their quality of life. Neighborhood serving businesses where residents could enjoy a healthy meal or buy nutritious groceries are desired.
- **Single-Family Character** – Preserve the housing character in portions of the neighborhood that are primarily single-family housing. Emphasize rehabilitating older housing stock through local, state and other programs to accommodate varying family situations, make the neighborhood more visually appealing, longer lasting, and energy efficient.
- **Housing Diversity** – When considering new development, incorporate a variety of housing options with a range of affordability levels. This will create a stronger, more vital neighborhood where people of many different backgrounds feel welcome.
- **Convenient Access to Services, Workplaces, and Recreation** – Preserve or reinvigorate existing industrial and commercial sites with continued or new employment opportunities and amenities within close proximity to residents. This will benefit the long-term attractiveness and stability of the planning area.

Map 3: Proposed Future Land Use



Source: City of Madison Planning Division 2017

Note: Refer to Plan Commission staff report (08-28-17) for changes to the 2006 and proposed 2017 Land Use Map.

## Focus Areas

Based on review and analysis of the 2006 Comprehensive Plan and input from site visits, property owner interviews, public meetings, surveys, and city agency discussions, project staff identified four focus areas where some combination of preservation, redevelopment, and revitalization is recommended (*Map 4: Focus Areas*):

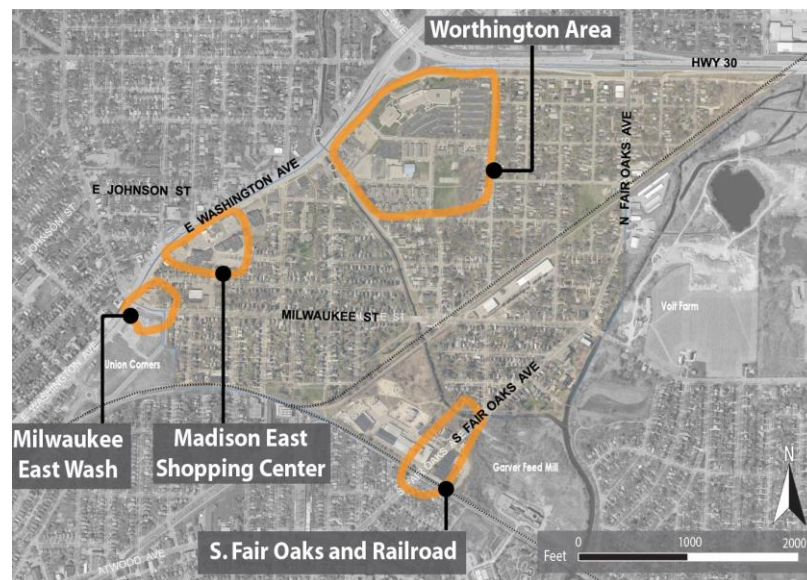
- Worthington Focus Area
- Madison East Shopping Center Focus Area
- Milwaukee/East Washington Focus Area
- South Fair Oaks and Railroad Focus Area

The focus areas have some or all of the following characteristics: potential for land use change, underutilized land, neighborhood gateway enhancement potential, and access to and visibility from major thoroughfares.

Land use recommendations and concepts, building height limits (*Map 5: Maximum Building Heights, adjacent*) and design guidelines were developed for each focus area. Design guidelines are in *Table 1*.

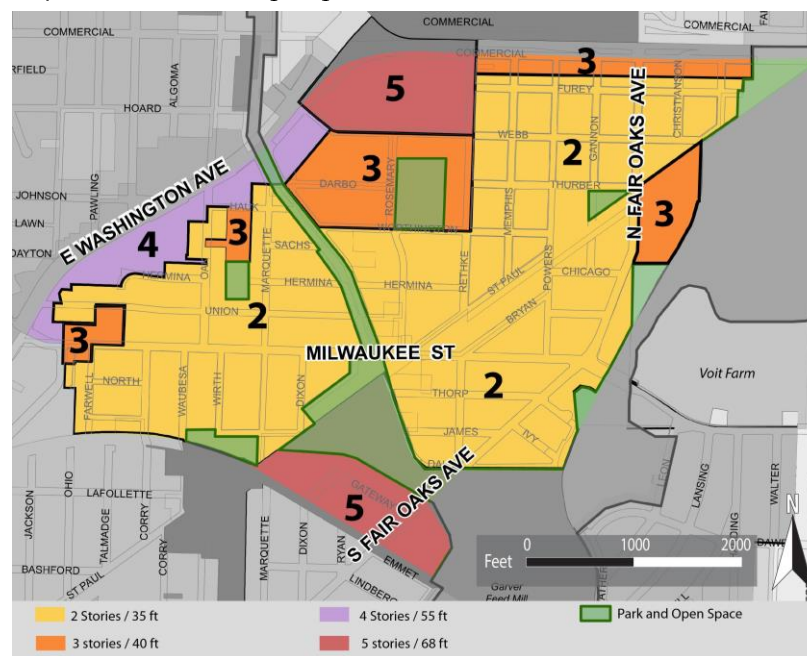
Existing and potential property owners are encouraged to use the land use recommendations, concepts, and design guidelines when considering future development and redevelopment. Higher quality, more sustainable design will improve the aesthetics and environmental sensitivity of the neighborhood, and generally help improve the quality of life.

Map 4: Focus Areas



Source: City of Madison Planning Division 2017

Map 5: Maximum Building Height



Source: City of Madison Planning Division 2017

Table 1: Design Guidelines

|                           | Building Characteristics, Massing, and Articulation                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                    | Circulation, Landscape, and Accessibility                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                 | Parking and Driveways                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                         |
|---------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| General Design Guidelines | <ul style="list-style-type: none"> <li>• <b>Respect the context</b> – Development projects should have a logical transition between the mass of current and proposed buildings. The neighborhood is comprised mostly of two-story single-family residential properties, and this character should have a sense of permanence.</li> <li>• <b>Human scale</b> – Add character to higher density buildings using public spaces, B-cycle facilities, and landscaping. Provide sufficient space between buildings with visual breaks to diminish the scale and massing.</li> <li>• <b>Textures and materials that complement</b> – Selection of materials should respect the adjacent buildings' materials and characteristics.</li> <li>• <b>Respect for privacy</b> – For new low or medium density residential development, the buildings could be positioned 3 feet above the public sidewalk to provide a sense of residential separation and demarcation of privacy from the public realm.</li> </ul> | <ul style="list-style-type: none"> <li>• <b>Pedestrian and environmentally friendly</b> – Upgrade sidewalks along collector streets, such as North Fair Oaks Avenue and Milwaukee Street.</li> <li>• <b>Inviting neighborhood</b> – enhance appearance of neighborhood gateways to provide welcoming nodes.</li> <li>• <b>Place identity</b> – Encourage the participation of local artisans in detailing and placemaking efforts at appropriate public areas identified by residents and stakeholders.</li> </ul>                                                                                                                                                                        | <ul style="list-style-type: none"> <li>• <b>Run-off prevention</b> – Humanize surface parking and consider innovative stormwater management features with attractive landscaping like bioretention islands. Even though surface parking may be more economically feasible, it increases runoff and visual contamination. Underground parking and/or structured parking is desirable.</li> <li>• <b>Pedestrian prioritization</b> – Build distinguishable pedestrian access paths separate from the driveway. Locate service entrances and parking areas at the rear of sites.</li> <li>• <b>Improve parking aesthetics</b> – Surface parking spaces should be spatially defined and screened by landscaping, walls, and/or fences.</li> </ul> |
| Worthington Focus Area    | <ul style="list-style-type: none"> <li>• Eliminate the frontage road on E. Washington Avenue, between N. Marquette Street and Webb Avenue, and replace it with bike and pedestrian features.</li> <li>• Include pedestrian and bike amenities at the BRT (Bus Rapid Transit) station proposed at N. Marquette Street and E. Washington Avenue.</li> <li>• Limit building height on E. Washington Avenue to 4 to 5 stories to avoid excessive height difference between new development and adjacent buildings.</li> </ul>                                                                                                                                                                                                                                                                                                                                                                                                                                                                              | <ul style="list-style-type: none"> <li>• The E. Washington Avenue and N. Marquette Street intersection is the prominent neighborhood gateway and it should welcome residents and visitors. Elements could include decorative paving, better landscaping with native plants, and pedestrian lighting.</li> <li>• Include a plant pollinator habitat at the Darbo Drive and N. Marquette Street corner. Promote pedestrian convenience and safety on the new Darbo/Webb connection with shared and paved space, landscaping and street furniture.</li> <li>• Plant tree canopy on the public terrace along Darbo Drive to offer a more human scale entrance to Worthington Park.</li> </ul> |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                               |

Table 1 continued: Design Guidelines

|                                          | Building Characteristics, Massing, and Articulation                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                               | Circulation, Landscape, and Accessibility                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                    | Parking and Driveways                                                                                                                                                                                                                                                                                                      |
|------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| Madison East Shopping Center Focus Area  | <ul style="list-style-type: none"> <li>Promote high quality architecture that brings uniqueness to this commercial anchor. All building façades should offer a visual distinction among floors and complement the character of residential areas with height transitions.</li> <li>Buildings along E. Washington Avenue should be no more than 4 stories to respond to the neighborhood scale.</li> <li>The front setback of new buildings on E. Washington Avenue should provide enough space for a pleasant walking experience, and street furniture such as benches and trash bins.</li> </ul> | <ul style="list-style-type: none"> <li>Activate the Hawthorne Library alley on Hermina Street by providing an outdoor open space that customers could use during the day and also during the evenings for movie nights.</li> <li>Build dumpster enclosures behind Madison East Shopping Center or decorate them. This could offer more pleasant aesthetics for the rear façade of the building. Business owners must do daily maintenance of dumpsters.</li> <li>Add light poles on Hermina Street between Madison East Shopping Center and Oak Street to improve visibility of sidewalks and activate this currently quiet area.</li> </ul> | <ul style="list-style-type: none"> <li>Develop low-rise townhomes with primary entrances facing Hermina Street on the currently underutilized parking lot located behind Madison East Shopping Center.</li> </ul>                                                                                                          |
| Milwaukee/East Washington Focus Area     | <ul style="list-style-type: none"> <li>Buildings on E. Washington Avenue should have a maximum of 4 stories.</li> <li>New residential buildings on Milwaukee Street, between Fair Oaks Avenue and E. Washington Avenue, should not be higher than 2 stories, except for the sites around Winnebago Street, as illustrated <i>Map 5: Maximum Building Heights</i>.</li> </ul>                                                                                                                                                                                                                      |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              |                                                                                                                                                                                                                                                                                                                            |
| South Fair Oaks Ave/ Railroad Focus Area | <ul style="list-style-type: none"> <li>If buildings are taller than 4 stories, all façades should complement the existing residential character, offer a visual distinction among floors, and create vertical breaks for façades that are 40 feet wide or over.</li> <li>New buildings in Industrial and Employment areas should be no more than 5 stories and should provide a height transition from single- and two-family homes.</li> </ul>                                                                                                                                                   | <ul style="list-style-type: none"> <li>Establish a physical ped/bike entrance/connection to the existing green spaces of Dixon Greenway and the Garver Feed Mill site, allowing for enjoyment of the natural features for employees, residents, and visitors.</li> </ul>                                                                                                                                                                                                                                                                                                                                                                     | <ul style="list-style-type: none"> <li>To slow traffic and promote pedestrian safety, maintain on-street parking along Fair Oaks Avenue.</li> <li>Identify areas for placemaking opportunities along Starkweather Creek. These areas should provide various opportunities to socialize and spend time outdoors.</li> </ul> |

## Worthington Focus Area

The Worthington Focus Area is home to multi-family apartments, the Salvation Army, Wisconsin Department of Corrections, and commercial properties along E. Washington Avenue, two of which are vacant. Despite being situated along two major transportation corridors; the area is difficult to access.

*Figure 3: Conceptual Street View* illustrates existing and potential redevelopment based on input from the public, property owners, and other stakeholders. *Figure 4: Worthington Focus Area Concepts A & B*, shows two alternatives of how property owners could redevelop the overall Worthington Focus Area.

### The Vision

*Establish a multi-service community facility at the heart of the area, a community or urban farm, family and senior housing, and entrepreneur workspaces. Improve access to neighborhood serving uses, rehab housing, and enhance Worthington Park.*

Land use recommendations for this area shown on *Map 3: Future Land Use Map* are consistent with the 2006 Comprehensive Plan. It is recommended that the frontage road be removed, and that properties be developed up to the street with a relatively high-density mix of residential, retail, office, or civic uses in a compact, sustainable building design. A public street should be constructed at the rear of the properties and the proposed space should be retained for a future BRT station.

Figure 3: Conceptual Street View - looking east along Darbo Drive from N. Marquette Street.



Source: City of Madison Planning Division 2017

Figure 4: Worthington Focus Area Concepts A & B



Source: City of Madison Planning Division 2017

## Inspiration from Homeboy Industries and the Dahlia Center



Creating a community service campus in the Darbo-Worthington area is a long-term goal and a complex endeavor. It would require the participation of property owners, private and public partners, and major financial investment. Centers in Denver and Los Angeles offer instructive examples.

### Homeboy Industries in Los Angeles

In 1988, members of the Dolores Mission Church began connecting former gang members to jobs. In 1992, a private foundation provided funds to start the social enterprise Homeboy Bakery. Homeboy Tortilleria soon followed. In 2001, Homeboy Industries became an independent nonprofit and, in 2007, the organization moved into its current Los Angeles headquarters. The primary focus of the organization (80%) is on jobs, and 20% is on therapeutic support and services. Participants are paid to receive job training, and part of their day must be spent on education, therapy, tattoo removal, substance abuse treatment, legal assistance, or job placement services as self-improvement.

With approximately 300 employees, Homeboy Industries impacts more than 9,200 people per year. There is an 18-month job training program for formerly incarcerated individuals, with about 250 people enrolled at a time, 70% of whom do not recidivate. About 500-600 people come for counseling or other services.

Homeboy Industries incurs \$15.8 million of expenses per year and takes in \$16 million (\$6.4 million social enterprises, \$4.3 million individuals, \$5.4 million programs, \$4 million trainee compensation). Homeboy's board of directors has raised \$10 million in each of the last two years through individual donors and foundations, and it has built up a reserve. Due to its success, the Homeboy model is sought out by other communities and it has been able to share its model with more than 40 organizations nationwide and internationally.



Dahlia Campus for Health and Well-Being in Denver – In late 2013, the Mental Health Center of Denver (MHCD) purchased four acres of the former Dahlia shopping mall site for \$1.8 million to develop the Dahlia Campus for Health and Well-Being. MHCD worked with nearby residents for months to build trust and was able to shape the facility to serve neighborhood needs.

In January 2016, after three years of planning, the \$16.5 million project was completed. The Denver Urban Renewal Authority helped to leverage grant and Tax Increment Financing (TIF) funds. Grants from private foundations were also procured. The campus includes a traditional clinic space, a preschool, dental clinic, school classrooms, community meeting space, a demonstration kitchen, and recreation facilities. The Dahlia Campus also includes an urban farm, an aquaponics greenhouse, experiential gardens for children, horticultural therapy gardens, a food pantry, and farmers' market. Urban farm products are distributed to local restaurants, markets, catering companies and Community Supported Agriculture (CSA) programs.

## Madison East Shopping Center Focus Area

This focus area includes the Madison East Shopping Center and the Walgreen's, Burger King, and McDonald's parcels. Primarily retail uses are present at the shopping center. This is coupled with a few offices, the Hawthorne Branch Library, and Public Health Madison/Dane County. A parking lot behind the shopping center is primarily used for overflow parking and to store food carts. There are some concerns about safety in this area, and ped/bike access to and within the site could be improved.

*Figure 5: Madison East Shopping Center Focus Area Concepts*, includes two land use concepts that illustrate potential ways that property owners could redevelop the shopping center area. The primary emphasis is to bring buildings to the street, reduce surface parking, improve ped/bike access, enhance landscaping, and activate and beautify the area behind the shopping center. Map 3 depicts future land use recommendations and design guidelines, which will help facilitate the recommended land use concepts.

### Land Use Recommendations

Land use recommendations shown on *Map 3: Future Land Use*, propose CMU (Community Mixed-Use), which is consistent with the 2006 Comprehensive Plan. Small-scale multi-family infill, co-housing, or senior housing of no more than two stories is recommended for the parking lot behind the shopping center.



*Proposed Concept for Hawthorne Branch Library Alley Courtyard*



*Proposed wrapping of utility boxes and dumpsters such as Chris Tobar Rodriguez dumpster art*

Figure 5: Madison East Shopping Center Focus Area Concept



Source: City of Madison Planning Division 2017

## E. Washington Avenue/Milwaukee Street Focus Area

E. Washington Avenue at Milwaukee Street is one of the most highly visible and heavily traveled intersections on the city's east side. Thousands of people pass through this intersection every day as they head into Madison towards the Capitol and beyond. It is also a primary gateway to the Emerson and Eken Park Neighborhoods to the north, and the Schenk-Atwood-Starkweather-Yahara Neighborhood to the south.

### The Vision

*This prominent intersection should feature iconic buildings that anchor each of the four corners. Neighborhood serving uses are recommended in commercial or commercial/residential mixed-use buildings.*

*Architecture should complement the surrounding building design and decorative identification elements should be incorporated at each corner to welcome people as they enter the adjacent neighborhoods.*

### Land Use and Site Design Recommendations

The Union Corners redevelopment is in full swing and a five-story commercial building is planned to anchor the southeast corner of E. Washington Avenue /Milwaukee Street to the southwest and northwest corners. The Emerson East-Eken Park-Yahara Neighborhood Plan recommends that four-story, mixed-use commercial/residential buildings be developed where Red Letter News and the Wisconsin Vision shopping center are currently located (Figure 6).

Figure 6: East Washington Avenue/Milwaukee Street Focus Area



Source: City of Madison Planning Division 2017

To complete anchoring of the intersection, this plan recommends a maximum four-story building be developed for the northeast corner where existing businesses, such as the Malt House, could remain on the first floor and commercial or residential spaces could be located above. This recommendation is consistent with the 2006 Comprehensive Plan, which recommends neighborhood mixed-use at this corner. Any new buildings should be oriented to the streets, but with wider sidewalks, landscape enhancements, B-cycle parking, and terraces to improve the area for pedestrians.

### **S. Fair Oaks Ave/ Railroad Focus Area**

Located at the intersection of S. Fair Oaks Avenue and the Wisconsin and Southern Railroad tracks, this manufacturing and employment area includes Madison Kipp, Kessenich's, Fair Oaks Nursery, the Dixon Greenway, and Starkweather Creek.

#### The Vision

*Over time, it is recommended that this area continue to be a place where manufacturing and employment uses can locate, providing good paying jobs for neighborhood residents. Some residential buildings could be included, but the primary focus should be on providing employment that is compatible with the surrounding residential area and green space.*

#### Land Use Recommendations

To realize the vision for continued manufacturing and employment uses, *Map 3: Future Land Use Map* is consistent with the 2006 Comprehensive Plan, which shows industrial uses for the Madison Kipp site and employment for the Fair Oaks Nursery and Kessenich's parcels. Over time, continued use of the existing buildings or repurposing of these buildings is recommended to support local fabrication, artist studios, performance space, and food production uses.

### **N. Fair Oaks Avenue Land Use Recommendations**

As shown on the *Map 3: Future Land Use Map* - Recommendation 4 identifies a change to the 2006 Comprehensive Plan for the McCormick Lumber site and adjacent parcels along the railroad tracks from Low-Density Residential (LDR) to Neighborhood Mixed-Use (NMU) with some employment uses that have low impact on the surrounding neighborhood. This will help preserve the area as a place for local fabrication, artist studios, performance space, and food production type uses. Residential uses are discouraged in this area.

The *Map 3: Future Land Use Map* - Recommendation 6, identifies a change to the 2006 Comprehensive Plan from General Commercial (GC) to Medium-Density Residential (MDR) or Parks and Open Space (P). Over time, pursuing either alternative will provide a compatible transition from the anticipated redevelopment of the southern one-half of the Voit Farm to more intense uses and the northern one-half to a conservation area. Maximum building heights of three stories are recommended. Public access to the Starkweather Green Corridor should be established and recreation type businesses, such as boat rental and concessions, are encouraged.

## Transportation Connectivity, Access, and Infrastructure

The planning area is easily accessible from E. Washington Avenue, Highway 30, and I-90/94. It can be difficult, however, to get around within the neighborhood. In particular, access is cut off to the Darbo area. Darbo features a higher population density than the other residential areas in the neighborhood. It is home to the Wisconsin Department of Corrections (WDOC), with approximately 650 employees, most of whom drive in and out of Darbo via Webb Avenue every weekday. There are also three underutilized or vacant commercial sites, and the Salvation Army is in the heart of the area. This area would benefit from better vehicle, bus, pedestrian, and B-cycle access. See *Maps 6 – 7*.

There are sidewalks along both sides of most city of Madison streets, but minimal sidewalks are present in the town of Blooming Grove portion of the neighborhood. For example, there are almost no sidewalks, even on primary routes, between Rethke Avenue and Fair Oaks Avenue. There is also no sidewalk along the north side of Milwaukee Street between Fair Oaks Avenue and the East Transfer Point, and between and the U.S. Post Office. Lastly, there is no sidewalk between the east side of Fair Oaks Avenue between Milwaukee Street and Highway 30. Fair Oaks Avenue is a primary route to Madison College and East Towne Mall. Milwaukee Street is the main route to Woodman's and the East Transfer Point.

Woodman's Grocery Store and Madison College, both approximately 1.5 miles away, are primary destinations for DWS residents. The percentage of households with no vehicle is highest in the part of Starkweather area, between Waubesa Street and Starkweather Creek (17 percent), followed by Darbo (16 percent). In Darbo, where incomes are lower than other parts of the neighborhood, household incomes limit the likelihood of owning a vehicle.

On weekdays, all day east-west bus service runs on E. Washington Avenue and Milwaukee Street. All day north-south service runs on E. Washington Avenue, between Sixth and Oak Streets, Oak Street, and N. Fair Oaks Avenue. There is only one bus stop on N. Fair Oaks Avenue where it intersects Commercial Avenue, in the northeast corner of the neighborhood. On weekends, bus service frequency is reduced from every 30 minutes to every hour for three of the four routes that serve the Darbo-Worthington-Starkweather Neighborhood area.

*"One study\* found that residents who live in neighborhoods with high walkability walked an average of 34-47 minutes a week more than residents in low-walkability neighborhoods. The study showed that for those living in highly-walkable neighborhoods, obesity rates were 35% lower than for those living in neighborhoods with low-walkability."*

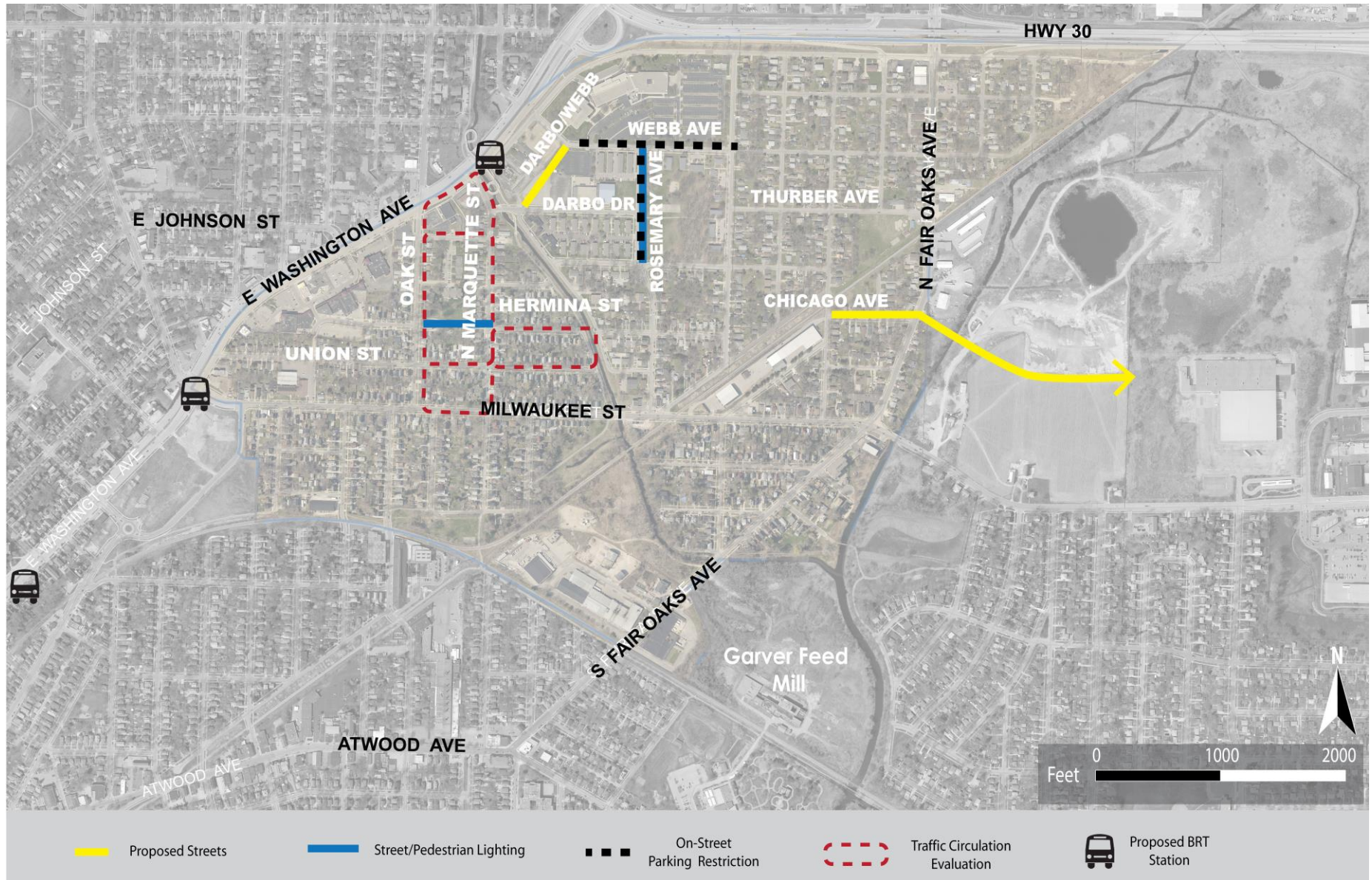
*\* DWS Health Impact Assessment; From Sallis, J.F., Saelens, B.E., et al. (2009) Neighborhood built environment and income: Examining multiple health outcomes. Social Science & Medicine, 68(7): p. 1285-1293.*

Map 6: Pedestrian and Bike Improvements



Source: City of Madison Planning Division 2017

Map 7: Street Improvements



Source: City of Madison Planning Division 2017

Table 2: Transportation Short-Term Actions - 1 to 2 years

| Description                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                               | Responsible Parties                                                                                                                                                                                                                   |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 1. Repaint crosswalks along E. Washington Avenue and Milwaukee Street in planning area (see Map 6: Pedestrian and Bike Improvements).                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                     | <u>Lead:</u> Traffic Engineering Division<br><u>Partner:</u> Engineering Division                                                                                                                                                     |
| 2. Install continental crosswalks at the following locations: Oak Street and N. Marquette Street entrances to Reger Park; intersections of Rosemary Avenue/ Darbo Drive; Rethke/Thurber Avenue; Fair Oaks/Commercial Avenue; and S. Fair Oaks/Thurber Avenue (see Map 7: Street Improvements).                                                                                                                                                                                                                                                                                                                                                                            | <u>Lead:</u> Traffic Engineering Division<br><u>Partner:</u> Engineering Division                                                                                                                                                     |
| 3. Install pedestrian flashing beacon at Milwaukee Street/Starkweather Creek Path (see Map 6: Pedestrian and Bike Improvements).                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                          | <u>Lead:</u> Engineering Division<br><u>Partners:</u> Traffic Engineering Division                                                                                                                                                    |
| 4. Assess and increase lighting levels as necessary on Starkweather Creek Path from Darbo Drive to Waubesa Street.                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                        | <u>Lead:</u> Traffic Engineering Division<br><u>Partner:</u> Engineering Division                                                                                                                                                     |
| 5. Promote biking and walking among residents of all ages: <ul style="list-style-type: none"> <li>Establish regular bike skills education activities.</li> <li>Conduct a bike/walking audit with residents to determine safe routes.</li> <li>Develop a neighborhood-scale B-cycle map with residents.</li> <li>Help facilitate distribution of lower cost bikes to residents.</li> <li>Establish B-cycle locations at Madison East Shopping Center, Department of Corrections, Salvation Army, and Woodman's East, and/or East Transfer Point; work with Trek B-cycle Corporation and other stakeholders to provide affordable options for B-cycle customers.</li> </ul> | <u>Lead:</u> UW Healthy Kids Collaborative<br><u>Partners:</u> Public Health-Madison and Dane County, Traffic Engineering Division, Madison School District, Safe Communities Coalition, Wheels for Winners; Trek B-cycle Corporation |
| 6. When the E. Washington Avenue Frontage Road is removed, construct a multi-use path in its place from Starkweather Path to Webb Avenue.                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                 | <u>Lead:</u> Engineering Division<br><u>Partner:</u> Traffic Engineering Division                                                                                                                                                     |

Table 2 continued: Transportation Short-Term Actions - 1 to 2 years

| Description                                                                                                                                                                                                                                                                                                                                                                                    | Responsible Parties                                                                                            |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------|
| 7. Mark bike and parking lanes on Darbo Drive from N. Marquette Street to Rosemary Avenue; install bump outs at Darbo Drive/Rosemary and Darbo Drive/N. Marquette intersections; or make other traffic calming improvements. Install a raised crossing where the Starkweather Creek Path crosses Darbo Drive.                                                                                  | <u>Lead:</u> Traffic Engineering Division<br><u>Partner:</u> Engineering Division                              |
| 8. Improve the ped/bike commute from the Worthington Park area to the East Transfer Point and grocery store: Install improved pedestrian crossing at the Milwaukee Street/Corporate Drive intersection by East Transfer Point, complete sidewalks on the north side of Milwaukee Street, and further investigate B-cycle or other B-cycle sharing options to help residents access fresh food. | <u>Lead:</u> Traffic Engineering Division<br><u>Partners:</u> Engineering Division, B-cycle                    |
| 9. Improve pedestrian connection from the Madison East Shopping Center to Milwaukee Street by removing steel road barricade at end of Hermina Street and adding lighting along sidewalk.                                                                                                                                                                                                       | <u>Lead:</u> Traffic Engineering Division<br><u>Partners:</u> Engineering Division and private property owners |
| 10. Evaluate traffic movement on N. Marquette Street, Oak Street, Hauk Street and Hermina Street. Explore changes to traffic flow to help reduce drug trafficking.                                                                                                                                                                                                                             | <u>Lead:</u> Traffic Engineering Division<br><u>Partners:</u> Engineering Division and private property owners |
| 11. Install school bus boarding pad at Rosemary/Webb Avenue.                                                                                                                                                                                                                                                                                                                                   | <u>Lead:</u> Metro Transit<br><u>Partner:</u> Engineering Division                                             |
| 12. Increase weekend route frequency from every 60 minutes to every 30 minutes on E. Washington Avenue.                                                                                                                                                                                                                                                                                        | <u>Lead:</u> Metro Transit                                                                                     |
| 13. Assess E. Washington Avenue bus stop on south side of street underneath bridge, for wind and rain exposure; consider installing an overhang to divert water from waiting passengers, and a wind shield or a bus shelter to protect passengers from wind and cold.                                                                                                                          | <u>Lead:</u> Engineering Division<br><u>Partner:</u> Metro Transit                                             |
| 14. Evaluate turning movements at the E. Washington Avenue /Milwaukee Street/North Street intersection and consider measures to improve pedestrian safety.                                                                                                                                                                                                                                     | <u>Lead:</u> Traffic Engineering Division<br><u>Partner:</u> Engineering Division                              |

Table 3: Transportation Mid-Term Actions - 3 to 5 years

| Description                                                                                                                                                                                                                                   | Responsible Parties                                                               |
|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------|
| 1. Mark bike lanes or other bike markings on Thurber Avenue from S. Fair Oaks Avenue to the Worthington Park Path, and continue bike markings on Darbo Drive.                                                                                 | <u>Lead:</u> Traffic Engineering Division<br><u>Partner:</u> Engineering Division |
| 2. Continue the bike path from O.B. Sherry Park along Starkweather Creek to connect to both branches of the green corridor, from Thurber Park along the railroad track, from St. Paul Avenue to Farwell Street.                               | <u>Lead:</u> Engineering Division<br><u>Partner:</u> Traffic Engineering Division |
| 3. Improve B-cycle and pedestrian access and amenities at East Madison Shopping Center. Additions include benches, bike racks, B-cycle sharing, etc.                                                                                          | <u>Lead:</u> Property owners<br><u>Partner:</u> Traffic Engineering Division      |
| 4. Complete bike lane marking on Milwaukee Street from Schenk Street to Fair Oaks Avenue and complete bike lane marking on N. Fair Oaks Avenue between Milwaukee Street and Highway 30 overpass.                                              | <u>Lead:</u> Traffic Engineering Division                                         |
| 5. Study noise levels along high-speed transportation corridors such as Highway 30 and E. Washington Avenue. Consider mitigation steps such as living walls, noise abatement walls, and other measures, if outside source funding is secured. | <u>Lead:</u> Engineering Division<br><u>Partner:</u> WI DOT                       |
| 6. Evaluate feasibility of upgrading cobra head streetlights to LED lights to improve light quality and nighttime visibility, lower lighting costs, and reduce carbon emissions.                                                              | <u>Lead:</u> Traffic Engineering Division<br><u>Partner:</u> Engineering Division |

Table 4: Transportation Long-Term Actions - More than 5 years

| Description                                                                                                                                                                                                                                                                                                                                                                                                                     | Responsible Parties                                                                                                                                                                                                                                                                                       |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 1. In coordination with proposed development along Darbo Drive and the E. Washington Avenue frontage road, install a road connection from Darbo Drive to Webb Avenue ( <i>see Figure 6: Worthington Concepts A &amp; B</i> ).                                                                                                                                                                                                   | <u>Lead:</u> Engineering Division<br><u>Partner:</u> Private property owners                                                                                                                                                                                                                              |
| 2. When the town of Blooming Grove is attached to the city of Madison and streets are proposed for reconstruction: <ul style="list-style-type: none"> <li>Evaluate and repair drainage issues on Memphis Avenue and other streets as necessary.</li> <li>Evaluate need for sidewalks and curb and gutter on primary routes in residential and commercial areas (<i>see Map 6: Pedestrian and Bike Improvements</i>).</li> </ul> | <u>Lead:</u> Engineering Division<br><u>Partners:</u> Private property owners, Water Utility                                                                                                                                                                                                              |
| 3. If/when Voit Farm property is redeveloped, extend Chicago Avenue across Fair Oaks Avenue to the east for better connectivity ( <i>see Map 7: Street Improvements</i> ).                                                                                                                                                                                                                                                      | <u>Lead:</u> Engineering Division<br><u>Partners:</u> Property Owners                                                                                                                                                                                                                                     |
| 4. Continue to study the feasibility of constructing a pedestrian bridge over Starkweather Creek at Hermina Street.                                                                                                                                                                                                                                                                                                             | <u>Lead:</u> Engineering Division<br><u>Partners:</u> Property Owners                                                                                                                                                                                                                                     |
| 5. Install bus shelter at bus stop on the corner of Commercial Avenue/S. Fair Oaks Avenue and on S. Fair Oaks Avenue at Wisconsin and Southern rail road tracks when town of Blooming Grove is annexed to the city.                                                                                                                                                                                                             | <u>Lead:</u> Metro Transit<br><u>Partners:</u> Engineering Division                                                                                                                                                                                                                                       |
| 6. Implement Bus Rapid Transit (BRT) route on E. Washington Avenue as recommended by the <i>Madison in Motion Plan</i> and include stops as proposed at Union Corners and Marquette Street. BRT vehicles would run every 10 to 15 minutes.                                                                                                                                                                                      | <u>Lead:</u> Madison Metro<br><u>Partners:</u> Planning Division<br><br>( <i>Timing of BRT system implementation is dependent on federal funding and ability to accommodate additional service in the Metro Transit operating budget. Potential implementation could be 5-10 years into the future.</i> ) |



*Town of Blooming Grove street with no curb and gutter*



*Continental crosswalk at Fair Oaks/Milwaukee Street intersection*



*Bicyclist riding along rail tracks between Wirth Court and Corry Street*

## Community and Individual Health and Wellness

The neighborhood includes many of the elements and characteristics of a vibrant and healthy community. The population is racially and ethnically diverse and has people of all ages, including many young families with children. While some buildings are in disrepair, most residences and businesses appear to be well-maintained. There is a wealth of park and open spaces, and pedestrian and bike trails weave through the area. There are also a number of community and neighborhood-based organizations that serve the area.

Through stakeholder interviews, public open house input, and service provider discussions, several important issues were highlighted. There is a lack of connectivity between the four neighborhood sub-areas, there is higher unemployment and lower income as compared to the entire city of Madison, and there are outsiders who frequent parts of the neighborhood to conduct illegal activities. In addition, the area is a food desert as defined by the U.S. Department of Agriculture, meaning that a grocery store is not located within one mile radius from most residences.

Transit dependence also makes it more difficult to access adult education, job skills training, kids' recreational activities, and affordable and healthy food. Issues of public safety include that some commercial areas feel burdened by persistent crime and safety issues. Some residents report feeling unsafe when out in the neighborhood and are uncomfortable sending their kids to walk to school. In addition, some report feeling disturbed and stressed at home.

When people feel a sense of stability in their homes and neighborhoods, they experience a higher quality of life. Stable and inviting places to live include quality housing which is owned and rented by people who take responsibility for its appearance and maintenance. These residents might enjoy working with each other through neighborhood associations and other community groups to

help maintain public gathering spaces and address neighborhood problems. They may also be interested in organizing block parties, picnics, neighborhood fundraisers, and other community building activities.

Stable and inviting places to live deter negative behavior. Where there are engaging neighborhood activities and attractive surroundings, there are also residents with a sense of civic respect and pride. An inviting place to live tends to attract increasingly more residents who also take pride in their homes and neighborhoods.

Table 5: Community Health and Wellness Short-Term Actions 1 to 2 Years

| Description                                                                                                                                                                                                                                                                                                                                                           | Responsible Parties                                                                                                                                                                   |
|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 1. Work with community leaders and service providers to provide electronic or written communications which inform residents of current issues, programming and services, community activities, and volunteer opportunities.                                                                                                                                           | <u>Lead:</u> Residents, Neighborhood Associations<br><u>Partners:</u> Community Centers, Property Managers, and Service Providers                                                     |
| 2. Provide literature to residents (especially to families with young children) about improving air quality at home through use of products with low VOCs, vacuums with allergen reducing filters, opening windows when weather is warm, keeping humidity at an acceptable level, testing for radon, and encouraging smoke free homes to decrease respiratory issues. | <u>Lead:</u> Public Health Madison/Dane County<br><u>Partners:</u> Neighborhood based groups, schools, Darbo-Worthington Park Neighborhood Resource Team (NRT)                        |
| 3. Host a health fair at the Salvation Army or another centrally located facility; address individual health, indoor and outdoor environment health, and community health.                                                                                                                                                                                            | <u>Lead:</u> Public Health Madison/Dane County<br><u>Partners:</u> Neighborhood based groups, schools, Darbo-Worthington Park Neighborhood Resource Team (NRT)                        |
| 4. Seek partnerships with current commercial kitchens in the area such as Salvation Army and the tenant at Madison East Shopping Center for shared/expanded food production.                                                                                                                                                                                          | <u>Lead:</u> Neighborhood based groups<br><u>Partners:</u> Business community, FoodWise program, service providers, neighborhood associations, Darbo Neighborhood Resource Team (NRT) |

Table 5: Community Health and Wellness Short-Term Actions 1 to 2 Years

| Description                                                                                                                                                                                                                              | Responsible Parties                                                                                                                                                |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 5. Establish a farmers' market in or near Darbo Neighborhood that is affordable to all, including lower income individuals, through supplemental income programs such as Women, Infants and Children, Supplemental Security Income, etc. | <u>Lead:</u> Neighborhood based-groups<br><u>Partners:</u> Property owners, Community Groundworks, City of Madison Food Policy Coordinator/Council, Parks Division |
| 6. Support and expand community gardens. Explore potential sites at Salvation Army (south side), O.B. Sherry Park, and Thurber Park.                                                                                                     | <u>Lead:</u> Community Groundworks<br><u>Partners:</u> Neighborhood associations, gardeners, City Parks                                                            |
| 7. Continue to work with the Neighborhood Police Officer and East District to communicate about safety issues.                                                                                                                           | <u>Lead:</u> City of Madison Police<br><u>Partners:</u> Neighborhood-based groups, business community, service providers, Darbo Neighborhood Resource Team (NRT)   |
| 8. Continue to work with businesses with high level of police calls for service to develop and monitor action plan to address management and security measures.                                                                          | <u>Lead:</u> Madison Police Department<br><u>Partners:</u> Attorney Office, Building Inspection                                                                    |

Table 6: Community Health and Wellness Mid-Term Actions 3 to 5 Years

| Description                                                                                                                                                                                                                                                                                                                                                                                                            | Responsible Parties                                                                                                                                                                                                                                       |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <p>1. Establish multi-service community facility where residents provide input and decisions on its use, operation hours and programming. Provide health and education services, employment training, recreational activities, and employ local residents at facility.</p>                                                                                                                                             | <p><u>Lead:</u> Planning Division, in conjunction with Community Development Division and other city agencies</p> <p><u>Partners:</u> Salvation Army, Nonprofit organizations, and business community</p>                                                 |
| <p>2. Increase programming and care options for children:</p> <ul style="list-style-type: none"> <li>• Identify gaps in before and after school care.</li> <li>• Establish affordable childcare options.</li> <li>• Increase educational and recreational programming for children and youth.</li> <li>• Provide certification processes for First Aid, CPR, and licensing for in-home childcare provision.</li> </ul> | <p><u>Lead:</u> Non-profits or health organizations</p> <p><u>Partners:</u> Madison School and Community Recreation, East High School, Lowell Elementary School, O’Keeffe Middle School, Joining Forces for Families, Salvation Army, YMCA and others</p> |
| <p>3. Increase access to social services by creating a “one-stop shop” at a centralized location that could include tenant and landlord resources, health screenings, senior services, domestic abuse prevention, multi-lingual services, and more.</p>                                                                                                                                                                | <p><u>Lead:</u> City of Madison Community Services</p> <p><u>Partners:</u> Joining Forces for Families, health organizations, non-profit organizations, neighborhood associations</p>                                                                     |
| <p>4. Increase health and fitness programs for all ages. Programs could include: fitness classes for women, walking/running groups, healthy cooking and nutrition classes, youth sports, etc.</p>                                                                                                                                                                                                                      | <p><u>Lead:</u> Non-profit or other appropriate organizations</p> <p><u>Partners:</u> UW Health Clinic, Madison School and Community Recreation, Goodman Community Center, YMCA, Salvation Army</p>                                                       |

## Economic Development and Employment Opportunities

The planning area is well situated for employment and is a place where limited goods and services can be purchased. It is directly accessible via major thoroughfares and there is convenient access to the Dane County Regional Airport, State Highway 30, U.S. Highway 151 and I-94. Thousands of vehicles travel the area's major corridors daily, transporting commuters, customers, and goods.

There are approximately 1,000 employees in the area, over one-half of whom work in the government sector. The Wisconsin Department of Corrections (DOC) has approximately 650 employees, Public Health Madison/Dane County employs 30 people, and Hawthorne Branch Library has 10 employees. The approximately 300 remaining employees work for restaurants and bars, gas stations, auto body shops, and other retail and service locations. In the planning area, people can access employment assistance at the Hawthorne Branch Library and Salvation Army. Just outside the area, employment training and education is offered at the Dane County Job Center and the Madison College Commercial Avenue Education Center and Truax Campus. Actions to improve local economic development and employment are included in *Table 7*. Highlights include identity and branding for the community, business attraction and retention, education and work skills development, business incubators and other new business space, and coordinating it all with some sort of organization or group of partners, and develop strategies for residents/local businesses to sell, produce or work at Garver Feed Mill or the public market.

Physical improvements, such as pedestrian and bike infrastructure and curb and gutter, may encourage private property owners to make façade and landscape improvements. Visual and functional improvements can promote a greater sense of community pride and attract visitors to enjoy the area's recreation, shopping, and business opportunities.



*Kessenich's Ltd, kitchen supplier located on S. Fair Oaks Avenue*



*Madison-Kipp Corporation, aluminum die cast producer located on S. Fair Oaks Avenue*

Table 7: Economic Development Short-Term Actions – 1 to 2 Years

| Description                                                                                                                                                                                                                                         | Responsible Parties                                                                                                                                                            |
|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 1. Monitor major commercial properties and work to retain and promote businesses and retailers that align with the neighborhood's vision and goals.                                                                                                 | <u>Lead:</u> Economic Development Division<br><u>Partners:</u> Neighborhood groups and residents, commercial property owners, and businesses                                   |
| 2. Encourage neighborhood businesses to partner with the internship and apprenticeship programs at East High School and market the program to high school students in the area.                                                                     | <u>Lead:</u> Economic Development Division<br><u>Partners:</u> MadREP, Greater Madison Chamber of Commerce, East High School, business owners, and Joining Forces for Families |
| 3. Expand and promote business opportunities in the Madison East Shopping Center. Businesses could benefit by the installation of an elevator, renovation of second story to new/upgraded office space, and lease of artist space/gallery           | <u>Lead:</u> Shopping center owner<br><u>Partners:</u> Economic Development Division                                                                                           |
| 4. Incorporate multi-cultural features and affordability into planning for Public Market to appeal to residents in Darbo and other neighborhoods with concentrations of lower-income residents and people of international and diverse backgrounds. | <u>Lead:</u> Economic Development Division<br><u>Partners:</u> Brothers Aligned Coalition, Darbo area neighborhood groups, Goodman Community Center, Mentoring Positives       |
| 5. Identify locations to sell Metro Transit bus passes and tickets locally.                                                                                                                                                                         | <u>Lead:</u> Metro Transit<br><u>Partners:</u> Businesses or governmental offices                                                                                              |
| 6. Develop an income eligible bus voucher program through Madison College to serve Darbo residents enrolled at the College.                                                                                                                         | <u>Lead:</u> Madison College<br><u>Partners:</u> Metro Transit (eventual partner, if approached by the College)                                                                |

Table 7: Economic Development Short-Term Actions – 1 to 2 Years

| Description                                                                                                                                                                                                                                                                           | Responsible Parties                                                                                                                                                      |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 7. Inform businesses about the City's Façade Grant Improvement Program, which can be used for physical improvements to commercial buildings.                                                                                                                                          | <u>Lead:</u> Economic Development Division<br><u>Partners:</u> Business owners                                                                                           |
| 8. Promote the City's financial assistance programs and incentives to encourage businesses to locate in the neighborhood.                                                                                                                                                             | <u>Lead:</u> Economic Development Division<br><u>Partners:</u> Businesses                                                                                                |
| 9. Work with the City's Healthy Food Retail Program, SEED Grant Program, and other City programs to strengthen neighborhood access to healthy foods. <ul style="list-style-type: none"> <li>Identify and promote retail spaces and other locations for healthy food sales.</li> </ul> | <u>Leads:</u> Madison Food Policy Council<br><u>Partners:</u> Economic Development Division, Office of Business Resources and Mayor's Office, commercial property owners |

Table 8: Economic Development Mid-Term Actions – 3 to 5 Years

| Description                                                                                                                                                                                                                      | Responsible Parties                                                                                                                                                                                                                                                                |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 1. Develop a neighborhood brand and identity with gateway enhancements, such as iconic imagery, that embodies the identity and character of the area in design of landscaping, street furniture, public art and other amenities. | <u>Lead:</u> Planning Division<br><u>Partners:</u> Local artists, landscape architects                                                                                                                                                                                             |
| 2. Expand and strengthen employment services offered in Darbo by helping providers secure office space, classroom space, funding, and other resources.                                                                           | <u>Lead:</u> Community Development Division<br><u>Partners:</u> YWCA, Urban League of Greater Madison, Madison College, Salvation Army, Joining Forces for Families, Mentoring Positives, Dane County Job Center, Hawthorne Branch Library, Brothers Aligned Coalition, and others |
| 3. As properties redevelop in the Worthington Focus Area, identify sites for a business incubator/maker space that could serve area residents.                                                                                   | <u>Lead:</u> Economic Development Division<br><u>Partners:</u> Private and non-profit organizations                                                                                                                                                                                |

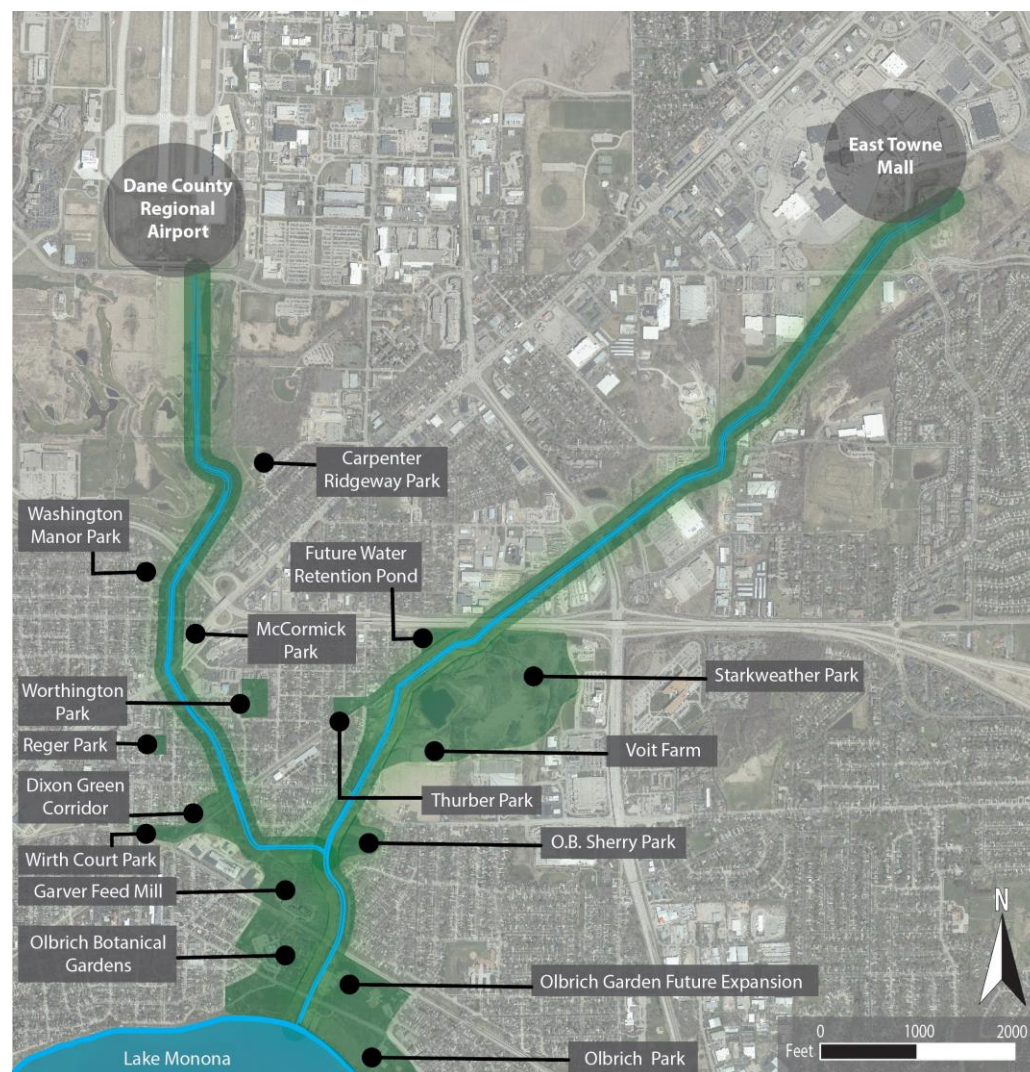
## Parks and Open Space

The Starkweather Creek Corridor (*Figure 7: Starkweather Creek Corridor*) meanders its way from the Dane County Regional Airport and East Towne Mall south to Lake Monona, connecting neighborhoods and linking green spaces along the way. The west branch of the creek links the Darbo, Worthington, Starkweather, and town of Blooming Grove portions of the planning area and provides access to Dixon Greenway, Garver Feed Mill, and Olbrich Botanical Gardens. The east branch links Starkweather Park, the Voit Farm, and O.B. Sherry Park, and joins with the west branch at Olbrich Park before flowing into Lake Monona. *Figure 8* illustrates recommended enhancements to the Starkweather Green Corridor. *Figure 9* illustrates City Engineering improvements planned for the entrance to the retention pond, which is located along the west side of the railroad tracks just south of Highway 30.

Each residential cluster in the planning area is organized around a park, with Worthington Park in Darbo, Reger Park in Union Triangle, Wirth Court Park and O.B. Sherry Park in Starkweather, and Thurber Park in the town of Blooming Grove. Bike paths also wind through the area, creating a transportation link to nearby neighborhoods and beyond, and offer a place for walking, biking, rollerblading and other non-motorized recreation.

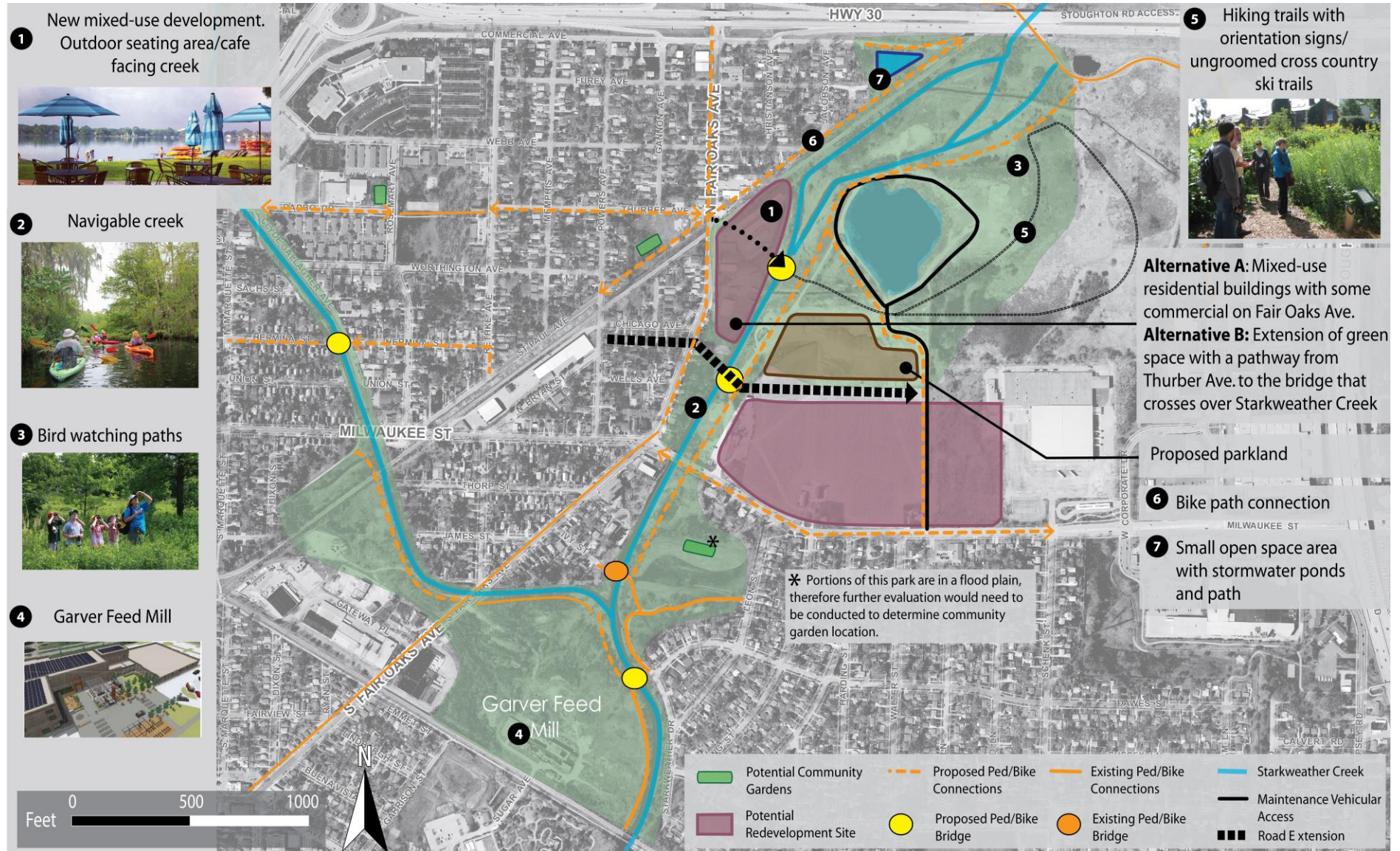
*Tables 9-11* include recommended actions to enhance the planning area's parks and open spaces, creating more engaging and active places, preserving and enhancing native habitats, and protecting environmental quality, as well as creating additional community gardens.

Figure 7: Starkweather Creek Corridor



Source: City of Madison Planning Division 2017

Figure 8: Starkweather Creek Corridor Concept



Source: City of Madison Planning Division 2017

Figure 9: Retention Pond Entrance



Source: City of Madison Planning Division 2017

Table 9: Parks and Open Space Short-Term Actions – 1 to 2 Years

| Description                                                                                                                                                                                                                                              | Responsible Parties                                                                                                                                                                                                |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 1. Activate parks and open space with placemaking activities, events, organized programs, and other activities.                                                                                                                                          | <u>Leads:</u> Neighborhood associations<br><u>Partners:</u> Salvation Army, Mentoring Positives, Madison School & Community Recreation, schools, YMCA, Goodman Community Center and other groups and organizations |
| 2. Enhance Reger Park                                                                                                                                                                                                                                    |                                                                                                                                                                                                                    |
| <ul style="list-style-type: none"> <li>Encourage property owners bordering the park to replace solid fences with materials that offer better visibility into the park.</li> </ul>                                                                        | <u>Lead:</u> Property owners                                                                                                                                                                                       |
| <ul style="list-style-type: none"> <li>Install pedestrian-scale lighting along pathway leading from Oak to N. Marquette Street.</li> </ul>                                                                                                               | <u>Lead:</u> Parks Division<br><u>Partners:</u> Traffic Engineering                                                                                                                                                |
| <ul style="list-style-type: none"> <li>Enhance entrance of Reger Park at Oak Street and N. Marquette Street with low maintenance vegetation.</li> </ul>                                                                                                  | <u>Lead:</u> Parks Division                                                                                                                                                                                        |
| <ul style="list-style-type: none"> <li>Upgrade existing playground equipment. Consider adding park amenities such as additional seating with benches/picnic tables, boulders, tree stumps; rent portable restrooms for events and activities.</li> </ul> | <u>Lead:</u> Parks Division<br><u>Partner:</u> Neighborhood based groups                                                                                                                                           |
| 3. Expand current Salvation Army garden site with approximately 60 plots of 10 by 10 feet.                                                                                                                                                               | <u>Lead:</u> Salvation Army<br><u>Partners:</u> Residents, gardeners, Community Groundworks, and Food Policy Council                                                                                               |

Table 9: Parks and Open Space Short-Term Actions – 1 to 2 Years

| Description                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                 | Responsible Parties                                                                         |
|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------|
| 4. Enhance Starkweather Creek                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                               | <u>Leads:</u> Engineering Division                                                          |
| <ul style="list-style-type: none"> <li>Install viewing areas, benches, and canoe/kayak launches at 2-3 locations. Priority locations include grassy area by pedestrian bridge (near former McDonald's site) and Hermina Steet.</li> <li>Install interpretive signage.</li> <li>Construct a stormwater treatment system at the quarry pond on the Voit Farm. The system, to be built and maintained by the City, will treat diverted storm flow from Starkweather Creek and discharge clean water back to the creek downstream of the system. A chemical coagulant will be used to enhance phosphorus and sediment removal from the water.</li> <li>Continue to remove invasive plants and establish native plantings for pollinator and other important habitat.</li> </ul> | <u>Partner:</u> Friends of Starkweather Creek                                               |
| 5. Madison East Shopping Center                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                             | <u>Leads:</u> Madison East Shopping Center                                                  |
| <ul style="list-style-type: none"> <li>Create temporary public use space in delivery alley off of Hermina Steet. An exit door leads to the alley from Hawthorne Library. Organize summer movie nights and other placemaking events.</li> <li>Paint murals on non-brick exterior walls.</li> </ul>                                                                                                                                                                                                                                                                                                                                                                                                                                                                           | <u>Partners:</u> Madison Public Library, Madison Arts Commission, neighborhood based groups |

Table 10: Parks and Open Space Mid-Term Actions - 3 to 5 Years

| Description                                                                                                                                                                                                                                                                                                                                                                           | Responsible Parties                                                                                       |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------|
| 1. Enhance Worthington Park <ul style="list-style-type: none"> <li>Create a continuous east-west bike connection through Worthington and Darbo areas. <ul style="list-style-type: none"> <li>Mark bike lanes/route on Thurber Avenue and Darbo Drive.</li> <li>Connect the two bike routes by upgrading the path through Worthington Park to a City bike path.</li> </ul> </li> </ul> | <u>Leads:</u> Traffic Engineering Division (bike boulevards)<br><u>Partner:</u> Parks Division            |
| <ul style="list-style-type: none"> <li>Install a water fountain closer to park shelter and basketball courts.</li> </ul>                                                                                                                                                                                                                                                              | <u>Lead:</u> Parks Division                                                                               |
| <ul style="list-style-type: none"> <li>Establish an ice rink at the northwest corner of the park. Work with neighborhood based groups, Salvation Army, and others to collect and check-out ice skates and other equipment.</li> </ul>                                                                                                                                                 | <u>Lead:</u> Parks Division<br><u>Partners:</u> Worthington Park Neighborhood Association, Salvation Army |
| <ul style="list-style-type: none"> <li>Install street lighting along Rosemary Avenue.</li> </ul>                                                                                                                                                                                                                                                                                      | <u>Lead:</u> Traffic Engineering<br><u>Partner:</u> Parks Division                                        |
| <ul style="list-style-type: none"> <li>Increase the number of places to sit in the park with benches, boulders, tree stumps, and more.</li> </ul>                                                                                                                                                                                                                                     | <u>Leads:</u> Parks Division<br><u>Partners:</u> Neighborhood associations                                |
| <ul style="list-style-type: none"> <li>Rent portable restrooms for events and activities.</li> </ul>                                                                                                                                                                                                                                                                                  | <u>Leads:</u> Neighborhood groups and other event organizers<br><u>Partner:</u> Parks Division            |

Table 11: Parks and Open Space Long-Term Actions – More Than 5 Years

| Description                                                                                                                                                                                                                                                                                                                                                                                                              | Responsible Parties                                                                                                                                |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------|
| 1. Consider expanding Worthington Park <ul style="list-style-type: none"> <li>At the time CDA housing (Webb-Rethke) is redeveloped, explore the feasibility of expanding Worthington Park to the east. Future CDA buildings could be more compact, which would allow using a smaller building footprint, and preservation of oak trees flanking the southern edge of the CDA property.</li> </ul>                        | <u>Leads:</u> Community Development Authority (CDA)<br><u>Partners:</u> Parks Division                                                             |
| 2. Enhance Thurber Park <ul style="list-style-type: none"> <li>Establish community gardens if there is resident support.</li> <li>Rent the former Waunona Sanitary District administrative building to the City of Madison-Dane County Library System. Repurpose for art in residency and hold community art workshops monthly.</li> <li>Keep public access to bathrooms at former Waunona Sanitary District.</li> </ul> | <u>Lead:</u> Town of Blooming Grove<br><u>Partners:</u> City of Madison-Dane County Library System, Madison Arts Commission, Community Groundworks |

## CHAPTER 4.

### ADOPTION AND IMPLEMENTATION

The Darbo-Worthington-Starkweather Neighborhood Plan was introduced to the Madison Common Council on June 20, 2017 and adopted on September 19, 2017 by Enactment No. RES -15-00436, Legislative File ID 47509. The resolution text is included in this document. The plan is a supplement to the City's Comprehensive Plan. It is a framework for lead implementation agencies and organizations to improve the Darbo-Worthington-Starkweather planning area. Lead implementers will incorporate neighborhood improvement projects in capital or operating budgets and work plans, and project staff will work with the City's Community Development Block Grant (CDBG) Office to finalize expenditure of CDBG funds targeted for use by the Darbo-Worthington-Starkweather Neighborhood Plan. Other implementation tools may also be used, such as the City's Tax Incremental Finance program and public and private grant monies. At the time projects are approved in the capital or operating budget, the lead agency will work with district alderperson(s) and the public prior to implement projects. In some cases, the lead agency will need to amend existing plans, such as Parks Master Plans, before work commences.

#### Inclusion of Projects

The Darbo-Worthington-Starkweather Plan recommendations provide direction for proposed improvements, and these projects and programs are contingent upon available resources. Policy makers are responsible for the allocation of resources for the entire city and thus funding for the neighborhood plan recommendations will be weighed against other worthy projects citywide. During the community input process, participants selected their top 13 neighborhood plan recommendations. *Table 12* illustrates this prioritization.

Table 12: Top DWS Neighborhood Plan Recommendations

| Priority | Top Neighborhood Plan Recommendations                                                                                                                                                                                                                                                        |
|----------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 1        | Establish a multi-service community facility where residents provide input and decisions on its use, operation hours, and programming. Provide health and education services, employment training, recreational activities, and employ local residents at facility (Darbo-Worthington Area). |
| 2        | Improve food access/security by the start-up of a farmers market, community gardens, or other options.                                                                                                                                                                                       |
| 3        | Activate and make improvements at Worthington Park – water fountain, ice rink, WiFi.                                                                                                                                                                                                         |
| 4        | Increase weekend frequency of bus service from 1 hour to ½ hour service on E Washington Avenue.                                                                                                                                                                                              |
| 5        | Improve the ped/bike commute from Darbo-Worthington area to the East Transfer Point and the grocery store, e.g. street crossings, sidewalks.                                                                                                                                                 |
| 6        | Build public swimming pool that serves the neighborhood.                                                                                                                                                                                                                                     |
| 7        | Improve walking and biking in the neighborhood by conducting a walk/bike audit, creating safe walk/bike to school routes, bringing B-cycle to neighborhood, and offering bike helmets, locks, and repair workshops.                                                                          |
| 8        | Establish an outdoor gathering place in the alley behind Hawthorne Library. Activate the space with community events such as food cart nights, summer movies, or artist in residency programs.                                                                                               |
| 9        | Create 1-2 water access points and/or seating places along Starkweather Creek (between East Washington and Milwaukee Street). Canoe/Kayak launches (can be used for seating), benches and interpretative signs.                                                                              |
| 10       | Activate and make improvements at Reger Park – lighting, playground equipment, landscaped entrance.                                                                                                                                                                                          |
| 11       | Establish business incubator in neighborhood.                                                                                                                                                                                                                                                |
| 12       | Create a Bubbler Outpost (an extension of the Center Library Bubbler Program) at Madison East Shopping Center or other locations in the neighborhood or close by.                                                                                                                            |
| 13       | Build a bridge between communities over Starkweather creek to connect Hermina Street.                                                                                                                                                                                                        |

Because of scarce resources, it will be important to understand that City/public funding of proposed improvements is, and will be, in competition with existing projects, and in many cases will require special earmarking of funding to undertake projects as well as ongoing maintenance costs. Securing funding from outside sources, leveraging funding with other available funding, or dovetailing proposed new projects with planned projects will help in implementing the desired activities and projects.

### Community Development Block Grant Fund Expenditures

The Community Development Block Grant (CDBG) Committee allocated \$145,000 of CDBG funds for implementation of eligible plan recommendations. *Table 13* identifies the priority recommendations by residents for CDBG funding. Detailed implementation projects are being finalized by City staff in coordination with lead implementers.

Table 13: CDBG Top Projects

| Priority | CDBG Top Projects                                                                                                                           |
|----------|---------------------------------------------------------------------------------------------------------------------------------------------|
| 1        | Worthington Park improvements – improve water access to create ice rink and install a new water fountain at eastern of park by the shelter. |
| 2        | Develop community gardens at Salvation Army expanded garden site.                                                                           |
| 3        | Replace current light street light fixtures with energy efficient fixtures.                                                                 |
| 4        | Improvements to rental space adjoined Hawthorne library for artist-in residency project.                                                    |
| 5        | Improvements to Starkweather Creek – benches and one canoe/kayak launch site.                                                               |
| 6        | Reger Park improvements – entrance improvements and pedestrian lighting.                                                                    |
| 7        | Mural as part of outdoor patio space at Hawthorne Library.                                                                                  |
| 8        | Develop community gardens at O.B. Sherry Park.                                                                                              |
| 9        | Install B-cycle site.                                                                                                                       |

### Resident-Led Action Teams

During the planning process, some residents stepped forward to form Action Teams in response to neighborhood needs:

1. The Farmers Market and Community Gardens Team aims to offer fresh food access to the Darbo-Worthington area.
2. The Worthington Park Winter Carnival Team is working on an ice skating rink for Worthington Park and a winter carnival.
3. The Neighborhood Clean-Up Team monitors trash along Starkweather Creek and in other highly visible locations and periodically gets together for cleanup days.
4. The Worthington Park Neighborhood Association Reactivation Team is bringing back the association through the leadership and diligent efforts of a number of area residents.
5. The Sustainable Lighting Team is discussing the feasibility of upgrading cobra head roadway lighting to LED lighting to improve light quality in higher crime areas, lower lighting costs and reduce carbon emissions.
6. Community Unity – Art in Worthington Park stages art making events for expressing creativity, having fun, and building community.

### Community Groups

Action Teams, Mentoring Positives, Inc., Joining Forces for Families, Goodman Community Center, Women of Worthington, Hawthorne Branch Library, and other groups and organizations continue to provide important programming and services in the neighborhood. There are a couple of newer initiatives in the neighborhood as well. Carts for Community hosts food cart and music nights at Worthington Park, and Brothers Aligned Coalition provides youth and young men of color resources that they need to be successful in their lives and the larger community.

## Adopting Resolution

**Enactment No. 15-00436 Legislative File ID 47509**

**Adoption: September 19, 2017**

### Fiscal Note

No City appropriation is required with the adoption of the Darbo-Worthington-Starkweather Neighborhood Plan. Implementing recommendations within the plan will require the inclusion of expenditures in future capital and operating budgets, subject to Common Council approval at that time.

### Title

Adopting the Darbo-Worthington-Starkweather Neighborhood Plan as a Supplement to the City of Madison Comprehensive Plan.

### Body

WHEREAS, the City of Madison Comprehensive Plan (adopted January 17, 2006) recommends the adoption of neighborhood plans for established residential neighborhoods within the City; and

WHEREAS the Community Development Block Grant (CDBG) Committee designated the Darbo-Worthington-Starkweather Plan Study Area (a portion of Census Tract 20) as a CDBG Concentration Neighborhood for the purposes of providing planning assistance and CDBG funding for eligible improvement projects; and

WHEREAS, the Planning Division solicited input from residents, businesses, and stakeholders through four public open house events, interviews, surveys, and neighborhood association and other community meetings; and

WHEREAS, the plan describes the desired vision for the future of the study area and provides recommendations to guide land use and

investment decisions, to improve the safety and efficiency of transportation, to improve community health and wellness, to expand community programming and services, to enhance parks and open spaces, to increase affordable housing and improve housing quality, and to improve access to job skills training and employment resources; and

WHEREAS, the planning process was augmented by a grant from Dane County B.U.I.L.D. (Better Urban Infill Development) Program to perform a Health Impact Assessment (HIA) for the Darbo-Worthington-Starkweather Neighborhood Plan Study Area; and

WHEREAS, the CDBG Committee and CDBG Office will work with the Department of Planning and Community and Economic Development and other agencies to implement eligible projects with CDBG funding in 2017 and 2018.

NOW THEREFORE BE IT RESOLVED, that the Common Council does hereby adopt the Darbo-Worthington-Starkweather Neighborhood Plan and recommendations contained therein as a supplement to the City's Comprehensive Plan.

BE IT FUTHER RESOLVED, that the appropriate City agencies consider including the recommendations of the Darbo-Worthington-Starkweather Neighborhood Plan in future work plans and budgets.

BE IT FINALLY RESOLVED, that the Department of Planning and Community and Economic Development shall monitor the implementation status of recommendations included in the Plan and shall work to update the Plan within 10-15 years.

## APPENDIX 1 - DATA

### Population

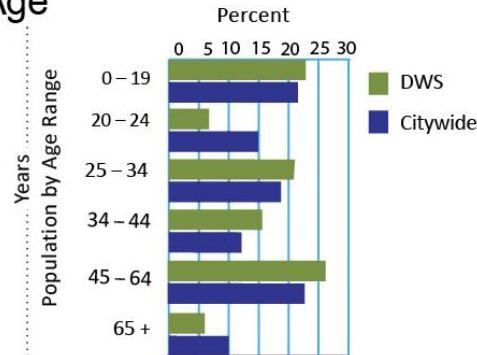
**4,298** DWS total population  
vs. 233,209 citywide



### Gender

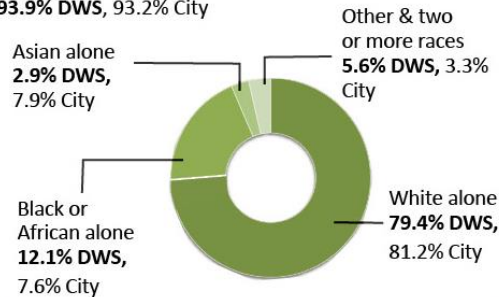
DWS area  
**49%** Male **51%** Female

### Age



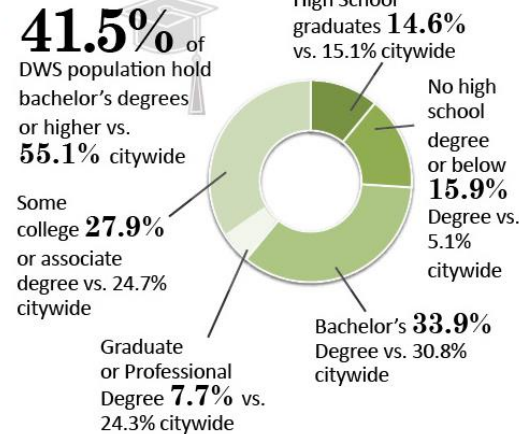
### Race

Non-Hispanic or Latino  
**93.9% DWS, 93.2% City**



**Ethnicity 6.1%** of total population is Latino or Hispanic vs. 6.8% of City

### Education



### Median Income

Median HOUSEHOLD Income **\$46,228** vs. \$61,074 citywide  
Median FAMILY Income **\$59,065** vs. \$46,989 citywide



### Labor Force

(20-64 year olds)  
**6.8%** unemployed vs. 3.7% citywide



Households with Income Below Poverty Level **13.3%** vs. 19.2% citywide

### Total Households

**2,066** vs. 102,516 citywide



### Household by Type

**43%** of DWS households are family households vs. 46.7% citywide

Total Families w/ minor children

**11.3%** vs. 20.8% citywide



Female Single Parents

**8.1%** vs. 5.3% citywide



**57%** of DWS households are non-family households vs. 53.3% citywide

Householder 65 years old or older living alone

**6.7%** vs. 7.4% citywide



Single Householder

**16.6%** vs. 33.9% citywide



### Dwelling Units

DWS (dwelling) Housing Units  
**2,195** vs. 108,843 citywide



## Housing Type

995 Total DWS dwelling units vs. 52,863 citywide

**83%**  
single family  
vs. **88.2%**  
citywide



**4.8%**  
multifamily vs. **5.4%**  
citywide



**12.2%**  
two units vs.  
**6.4%**  
citywide

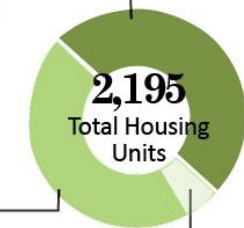


## Owner vs. Renter-Occupied Units



Owner-occupied  
units **51.1%**  
vs. **46.4%**  
citywide

Renter-occupied  
units **43%** vs.  
**47.7%** citywide



Non-Occupied **5.9%** vs.  
**5.8%** citywide

## Average Single-Family Home Values

**\$158,499**

vs. **\$252,296** citywide in 2016



It does not include the Town of Blooming Grove data

## Housing Burden

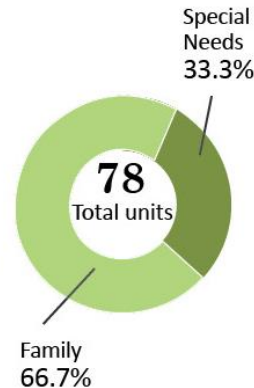
**19%** of owners (compared to 24% citywide) and **50.2%** of renters (compared to 51.5% citywide) are paying 30% or more of their income toward housing.



## Assisted Housing by the City

**78**

(1%) of the city's 7,649 assisted housing units are in the DWS planning area.



## Means of Transportation to Work



DWS **71.1%** Madison **71.8%**



**13.1%** **8.8%**



**12.2%** **5.3%**



**3.7%** **9.6%**

## No Motor Vehicles



**15.7%** of DWS households without vehicles vs. **7.9%** citywide

## Commuting Time to Work

workers 16 years of age or older

**13.5%**



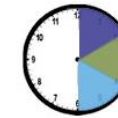
<10 minutes

**40.1%**



10-20 minutes

**22.8%**



21-30 minutes

**23.6%**



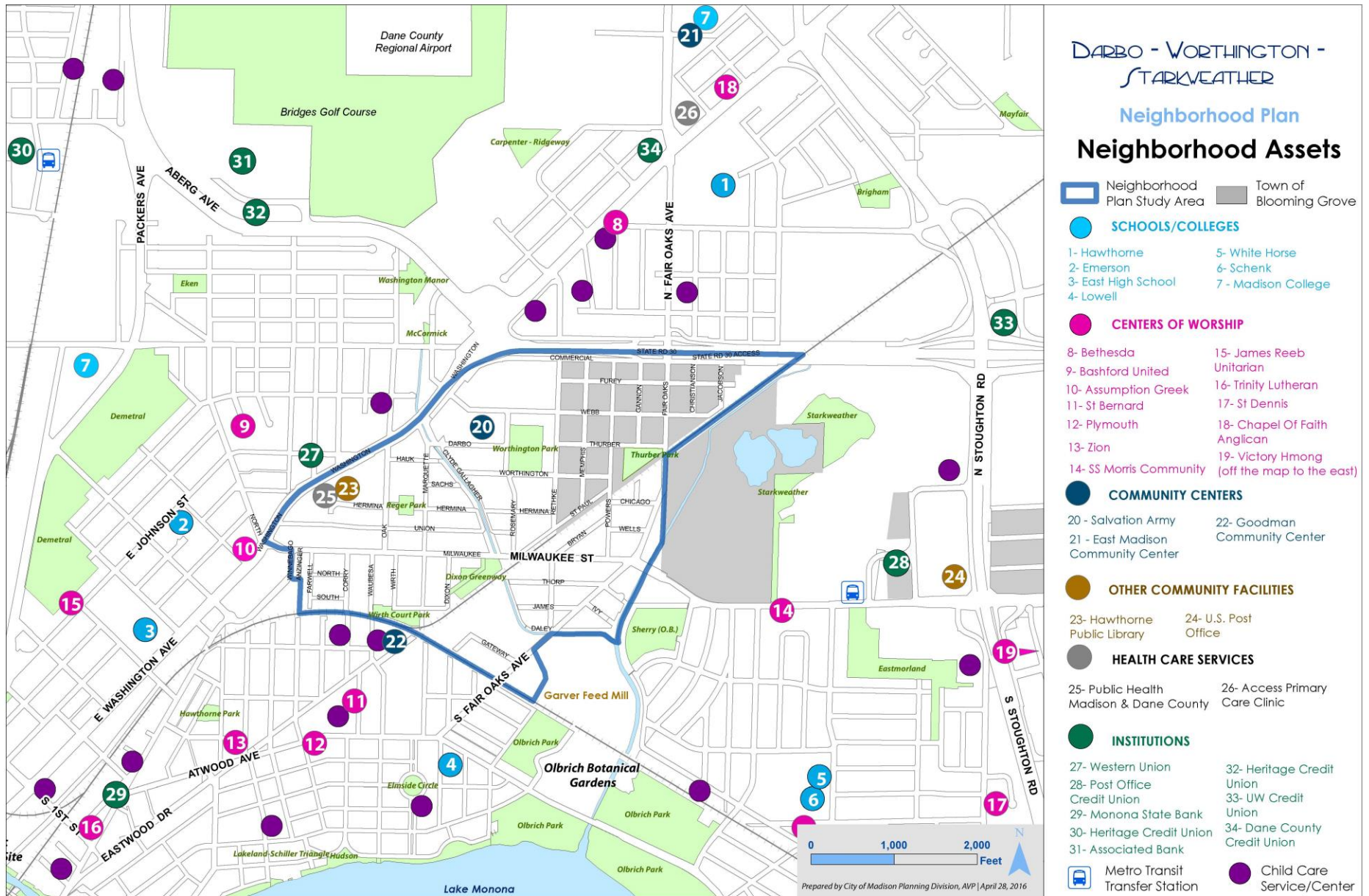
31+ minutes

## Data Source

Population, age, race, and some housing data were collected from the 2010 Census data. Income and employment, transportation and some education data were collected from ACS\* 2015 Census data. Some housing data was collected from Madison parcel data from the City of Madison. For more detailed information please visit our Neighborhood Planning website: [www.cityofmadison.com/dpced/planning/darbo-worthington-starkweather-neighborhood-plan/1661/](http://www.cityofmadison.com/dpced/planning/darbo-worthington-starkweather-neighborhood-plan/1661/)

\* The American Community Survey (ACS) is an ongoing statistical survey by the U.S. Census Bureau.

## APPENDIX 2 - MAPS





## DARBO - WORTHINGTON - STARKWEATHER Neighborhood Plan

### Bicycle Facilities

#### Shared-Use Paths:

Paths designed to accommodate bicycles, pedestrians, and other non-motorized traffic

- Existing
- Planned

#### Bike Lanes:

Marked lanes dedicated to bicycle use

- Existing

#### Bike Boulevards:

Low speed streets with priority given to bicycle traffic

- Existing

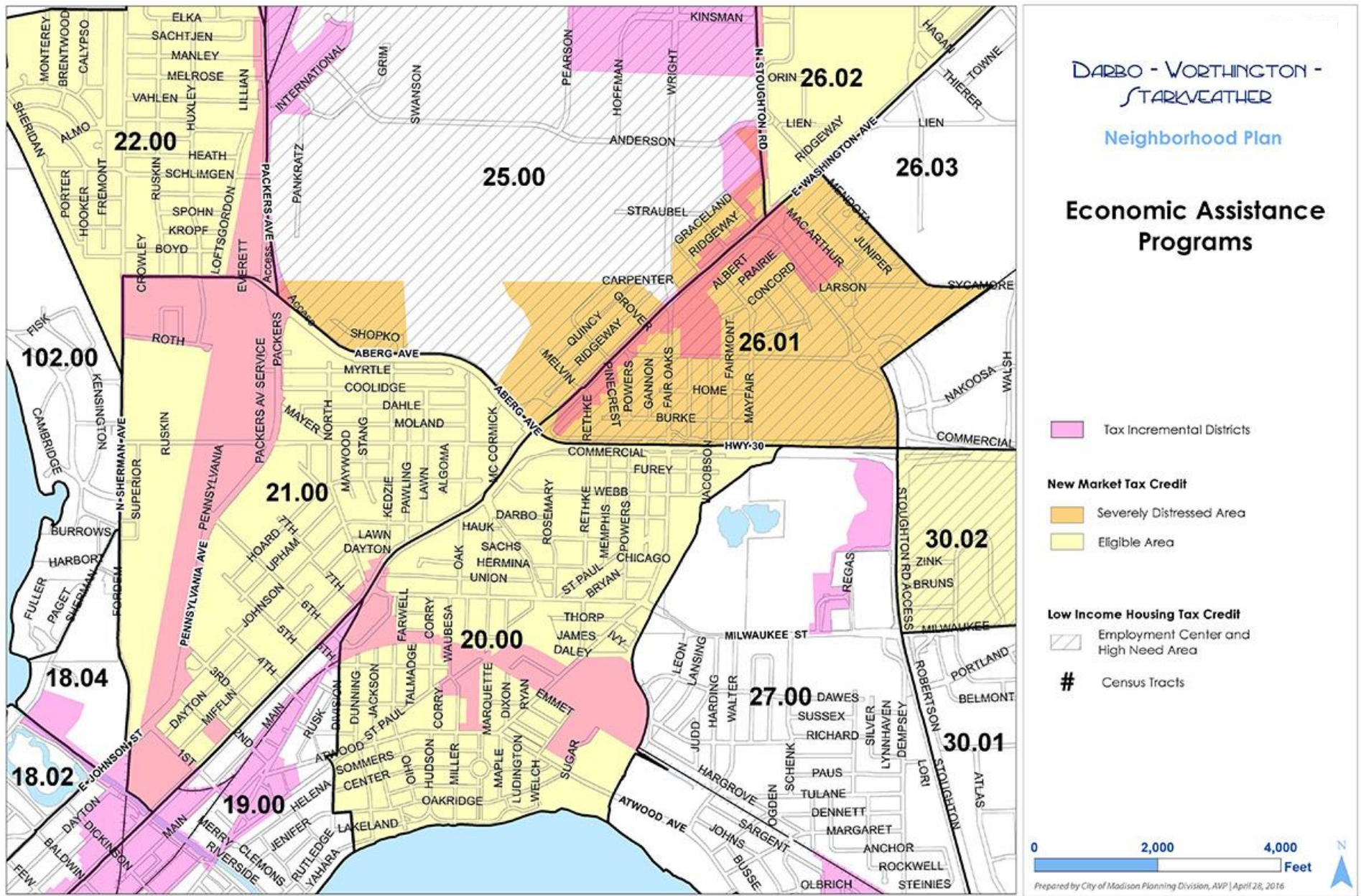
#### Bridge or Tunnel

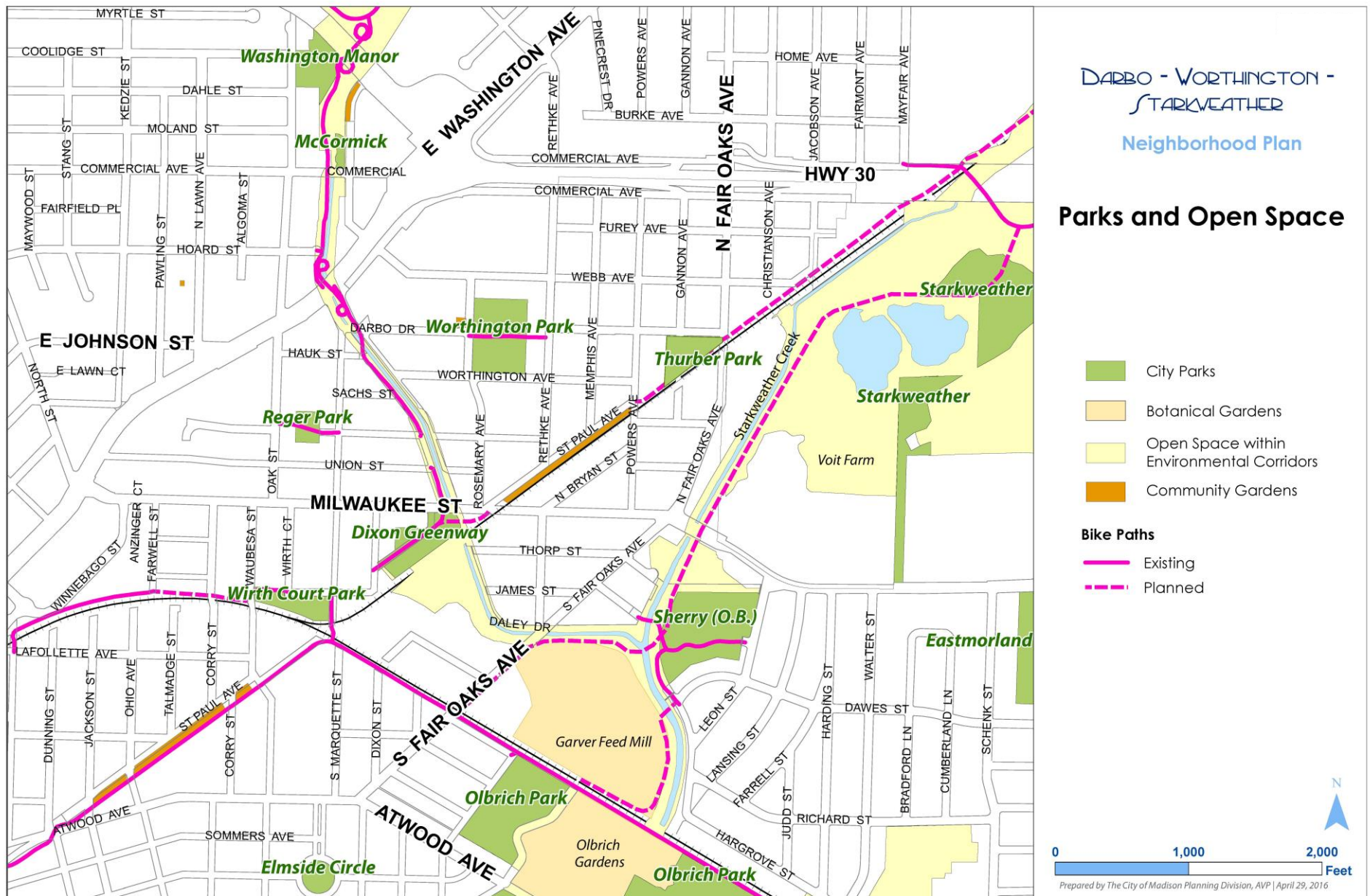
- Existing
- Planned

-  Bike Share Station

0 2,000 4,000  
Feet

Prepared by City of Madison Planning Division, AVP | April 29, 2016







## APPENDIX 3 – WEBSITE CONTENT OF THE NEIGHBORHOOD PLAN

### Website Link

<http://www.cityofmadison.com/dpced/planning/darbo-worthington-starkweather-neighborhood-plan/1661/>

### Published information:

- [Planning Process](#)
  - Neighborhood Groups
  - Community Organizations
- [Public Input Summary](#)
  - Public Input Community Meetings and Open Houses
  - Previous Public Input
- [Health Impact Assessment \(HIA\) Report](#)
- [Existing Conditions Maps:](#)
  - Aerial Map
  - Planning Area
  - Neighborhood Associations
  - Community Assets and Food Access
  - Age of Housing Stock
  - B-cycle Facilities
  - Bus Amenities
  - Comprehensive Plan
  - Economic Assistance Programs
  - Entertainment
  - Existing Land Use
  - Surface and Groundwater Soils
  - Assessed Single-Family Housing Value
- Parks and Open Space
- Transit Dependent Households
- Type of Housing Structure
- City of Madison Zoning Districts
- Town of Blooming Grove Zoning Districts
- Bus Routes
- [Darbo – Worthington – Starkweather History](#)
  - Previous Implemented Plans
  - Newspaper Articles
- [Proposed Future Land Use Changes](#)

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