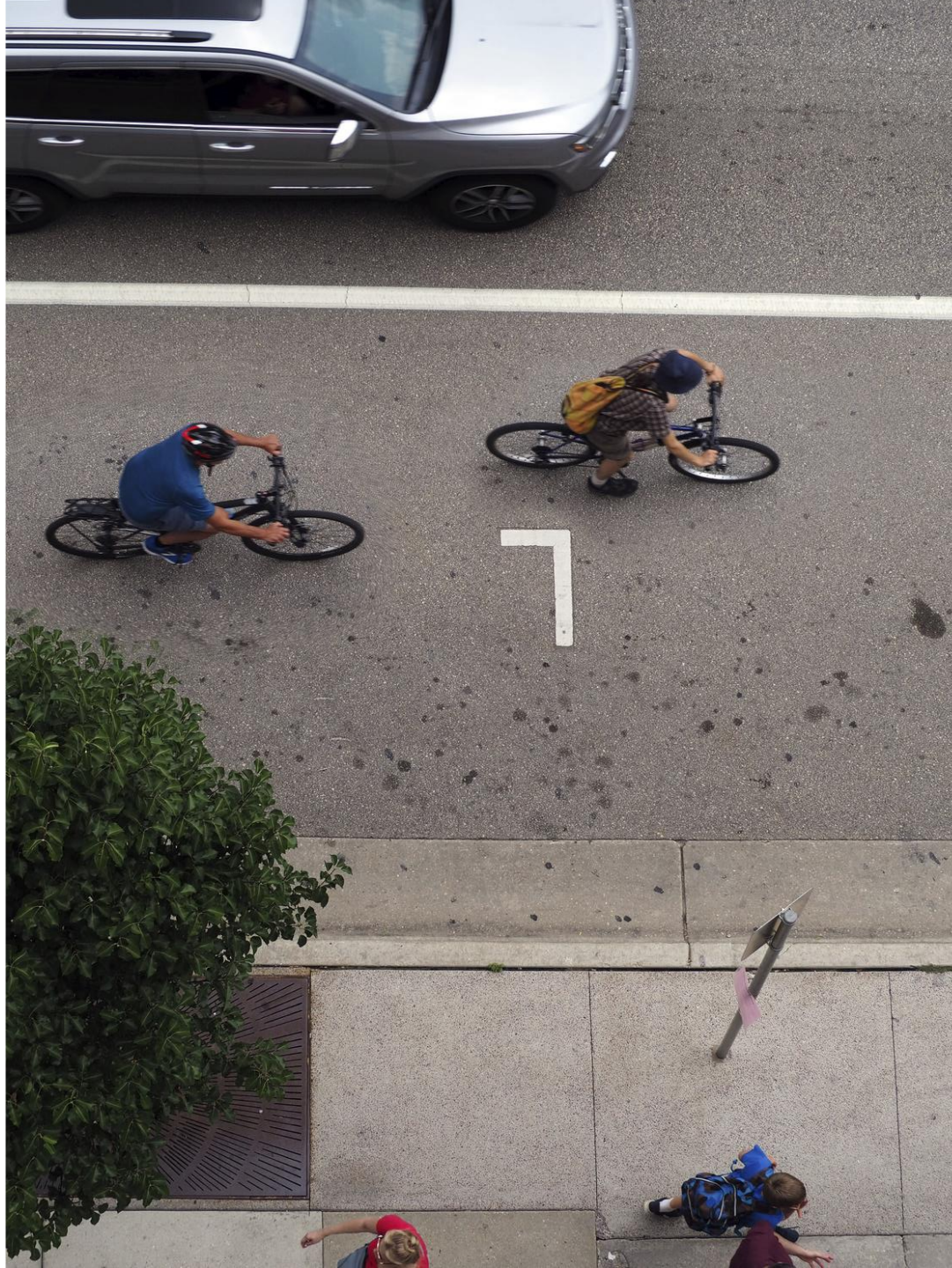


City of Madison

# Complete Green Streets Guide

Approved January 6, 2023



## Transportation Commission Review

September 4, 2024

# Guide Implementation

- Engineering and Traffic Engineering staff training in August 2023
- Completed the CGS Checklist for 5 reconstruction projects
- Subdivision ordinance updated to follow Complete Green Streets Guide on July 25, 2023
  - One new development came to Transportation Commission for variance to standards
  - Four other new development projects followed Guide without requesting variance
- Northeast and West Area Plans reviewed Street Types and All Ages and Ability Bike Network routing
  - Small number of changes recommended

# Overlay Recommendations

- Update to Equity Priority Areas
- All Ages and Abilities Bike Network project starting in Fall
  - Approve update with that project
- No other changes recommended

## **Equity Priority Areas**

(includes additional process elements)

## **Transit Priority Network**

(prioritizes transit on high frequency transit corridors)

## **All Ages and Abilities Bike Network**

(key corridors to prioritize high-comfort bikeways)

## **Tree Canopy Priority Areas**

(influences width and design of terraces)

## **Green Infrastructure Priority Areas**

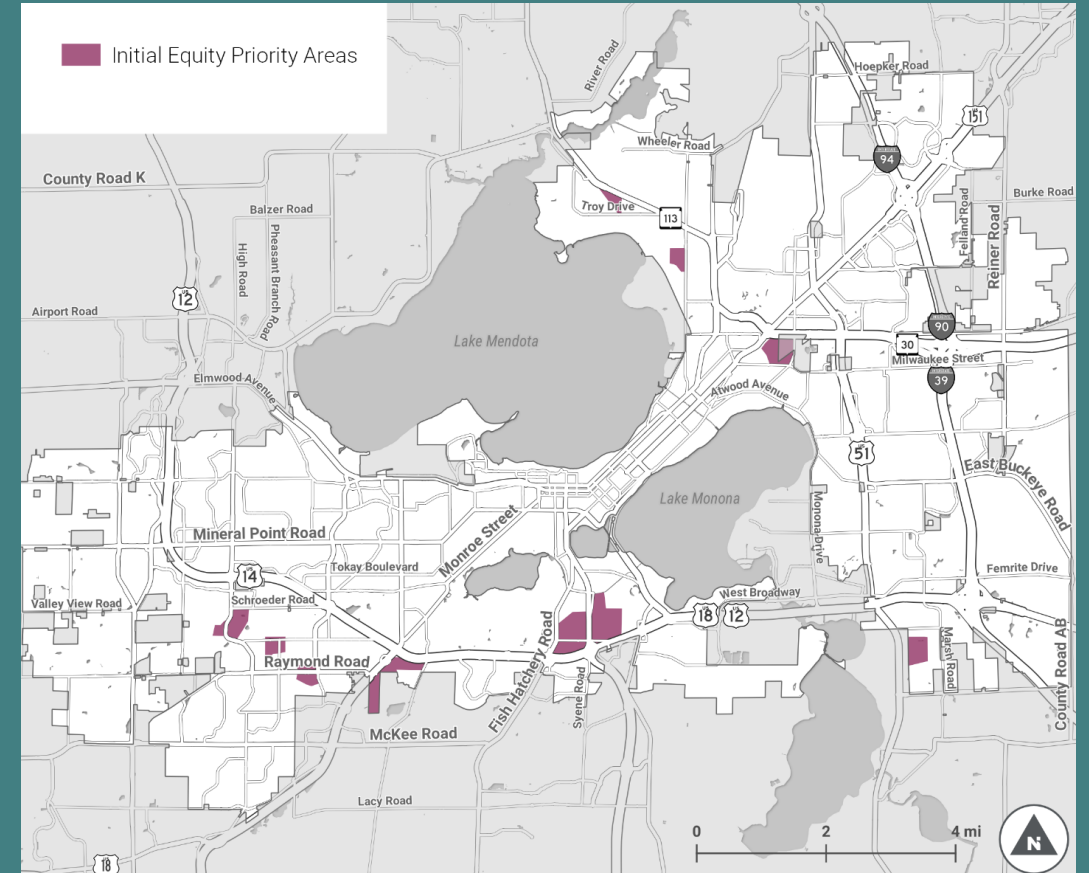
(influences width and design of terraces)

## **National Highway System & Truck Routes**

(higher traffic streets)

# Equity Priority Areas

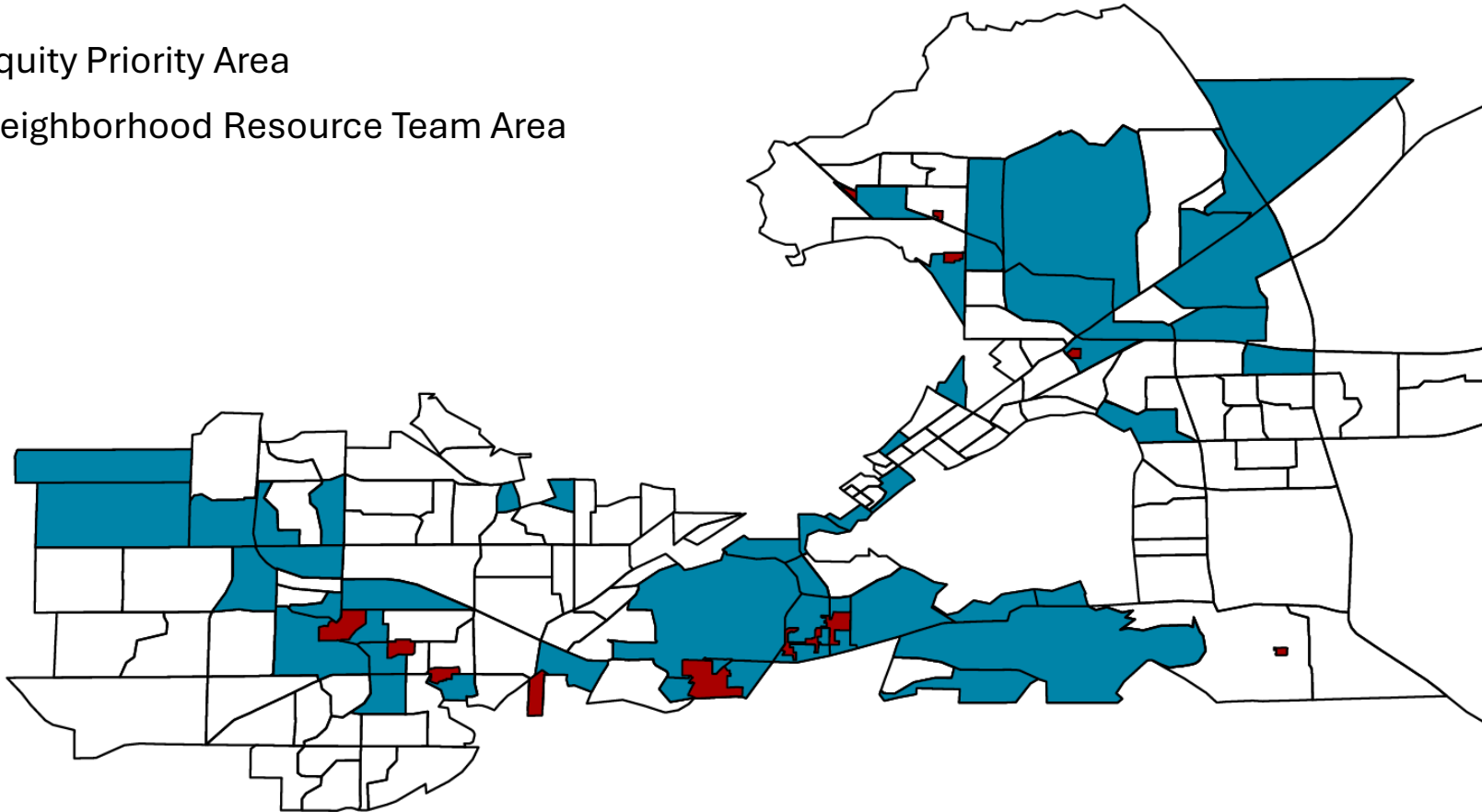
- Approved Guide calls for expanding this Overlay
- Align with City Equity Areas
  - Align with Transportation Improvement Program scoring



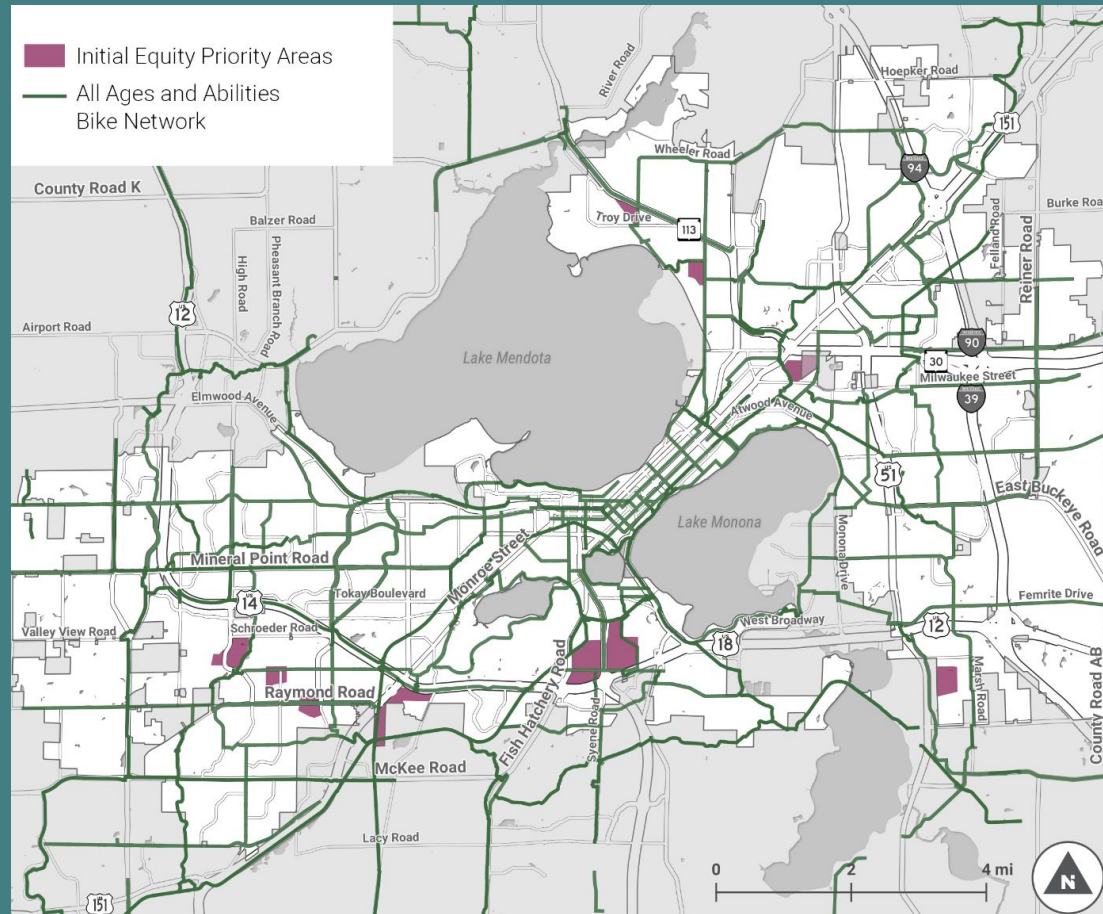


# Equity Priority Areas - Recommendation

- Equity Priority Area
- Neighborhood Resource Team Area



# All Ages and Abilities Bike Network



Draft Map

# All Ages and Abilities Bike Network

- Current map is draft
- Project starting in Fall 2024 to analyze network, do equity analysis, public engagement and develop a recommended final map.
  - Funding through Safe Streets for All grant to do further network analysis and engagement
  - Toole Design Group selected to assist with finalizing network
  - Project will also review current conditions of final network to prioritize projects

# Street Types - Recommendation

- Boulevard - recommend adding reference to trees as significant feature:
  - Connecting major streets conveying large numbers of people. Feature tree lined sidewalks, terraces and medians. Frequently part of the Transit Priority Network. May be part of the National Highway System and/or serve as a Truck Route.



# 6.1 Design Parameter Tables

- Total Walkway Width – Recommend changing to Total Walkway Zone Width
  - Confusion over that reflecting both the sidewalk width and buffer space to ROW line
- Flex Zone Footnotes
  - Add a footnote: New streets or dedications require an 8' minimum terrace
  - Add a footnote: Measurement for Flex Zone is from face of curb when no parking
- Typical ADT – Remove and leave in 6.2 table

## 6.2 Roadway Design & Space Allocation

### Parameters Table

- Typical ADT – Recommend changing to Estimated ADT
  - Allows more flexibility in assigning Street Type
- Neighborhood Yield Street
  - Add a footnote that streets without driveways need to meet the fire lane requirements noted in Section 6.5
- Add new footnote to Community Connector and Community Main Street related to transit:
  - Transit vehicles need a dynamic envelope of a minimum of 10' lane with a buffer such as a 2' gutter.

# Add New Section 6.4

## 6.4 Tree Canopy Enhancement Guidance

The table below identifies the compatibility of various tree canopy enhancement options based on the street type. Options that are “M” or maybe require further evaluation based on the individual street. These options can be considered in areas both in and outside of the Tree Priority Areas.

- Consider the location of any bus stop, bike parking, and similar items and how these can be placed to preserve and/or enhance the tree canopy.
- Review should be done as to options that would improve the health of current canopy trees and allow for future canopy tree additions.
- All projects involving street plantings should include additional excavation and soil replacement in the terrace to provide better growing environment for the tree plantings, consistent with section 201.3(d) of the City’s Standard Specifications.

Street Type	Parking Reduction or Elimination	Curb Bump Out	Terrace Support System	Sidewalk Alignment Changes	Median Trees
Urban Ave	Y	M	M	M	Y
Boulevard	Y	M	M	M	Y
Parkway	Y	M	M	M	Y
Mixed-Use Connector	Y	Y	Y	M	M
Community Main Street	Y	Y	Y	M	M
Community Connector	Y	Y	N	Y	M
Mixed-Use Neighborhood Street	Y	Y	M	Y	M
Neighborhood Street	Y	Y	N	Y	NA
Neighborhood Yield Street	Y	Y	N	Y	NA
Civic Space	Y	Y	Y	Y	M
Neighborhood Shared Street	Y	Y	M	Y	NA

★

# Add New Section 6.5

## 6.5 Complete Green Streets Guide & Alignment with Fire Code

The fire lane requirement referenced is summarized as follows:

- 20' clear for commercial buildings up to 30' in height
- 26' clear is required for buildings over 30' where Aerial Apparatus is required. Required on one side of the building, at least 25% of the building perimeter.

A development team can also design their site to include fire lane requirements on the parcel and not in the street. Where this isn't feasible a development team could choose to upgrade construction methods to follow the equivalency guide such as upgrading a wood frame to steel, providing multiple staircases to the roof or installing an enhanced sprinkler system.

The table below identifies street design solutions for new development that intend to use the street to meet the fire lane requirements.

If no options work for the development, a variance to the Complete Green Streets Guide can be requested from the Transportation Commission. The Transportation Commission would review the project and propose a motion to either approve or deny the variance request.



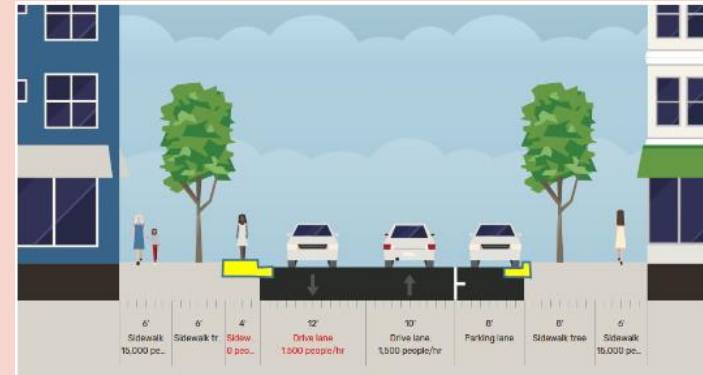
## Street Design Option

### Option 1 - Limit parking to one side of street

- Include a 4' paved terrace
- Include a 4' rolled curb

#### Notes:

- Signs and light poles would be allowed in terrace
- Verify 4' curb works with storm management plan
- Sow plowing of terrace will be responsibility of property owner, not City.



### Option 2 - Build 26' street with no parking

- 22' pavement with 2' curb pans



## Street Design Option

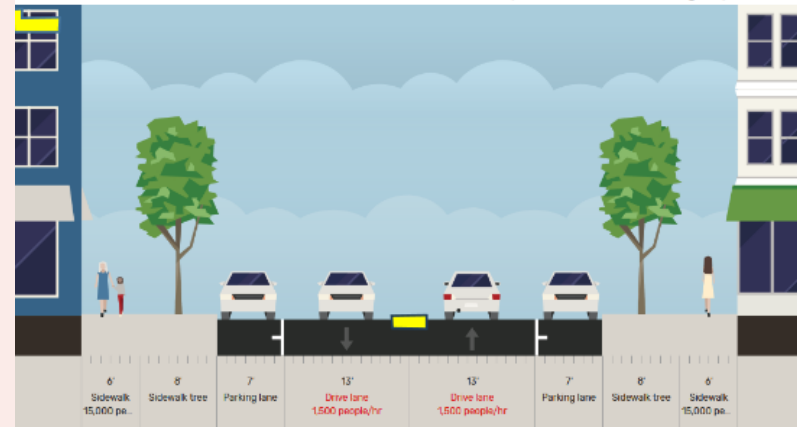
### Option 3 - Limit parking to one side of street

- Include a 4' curb pan
- Similar to Option 1. Can be used if stormwater management plan requires full curb head



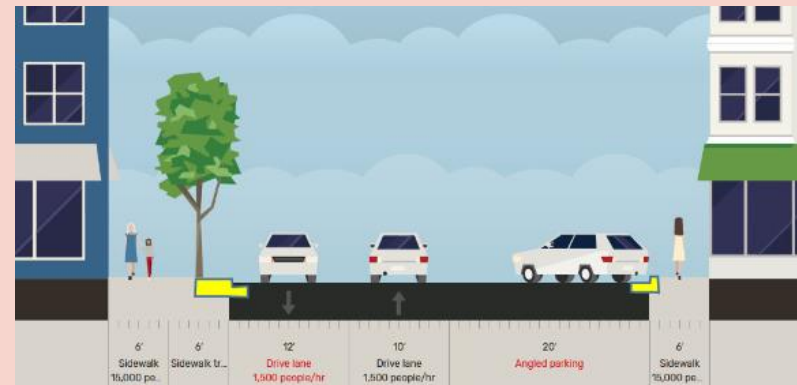
### Option 4 – Build a small island in the center of the street

- Not full curb height



### Option 5 – Limit parking to one side with back in angled parking

- Include 4' paved terrace



# Project Checklist - Does this add value?

From the Guide:

The *CGS Project Checklist* is used to document project-level decisions and implementation of the policy and should accompany project documentation through the approval process. The checklist will document the project decisions and input that have been considered as part of project development as well as reflect steps taken to ensure equity in street projects. Small spot improvements or maintenance only projects will not be required to complete the checklist. However, the overall planning for such activities will reflect the City's modal hierarchy and street values.

Key elements of the project checklist include:

- Record of project extents, improvement type, schedule, project staff, and context.
- Identification of the Street Type, Overlays, and nearby facilities and destinations that influence street design.
- Inventory of conditions (including crash data, speeds, traffic volumes, infrastructure present, pavement condition, school zones, parking, etc.)
- Identification of engagement efforts and outcomes—What concerns were raised? What are top priorities? How are concerns being addressed?
- Illustration of the initial proposed cross section and the approved cross section.
- Description of how the final design will be communicated to the community and the final design elements included in the project.

If the project is within an EPA (see Sections 3 and 4.2), the checklist also requires City staff to:

- Use the City's [RESJII Public Participation Guide](#) to assist in developing an engagement plan appropriate for the project.
- Identify and document opportunities to work together with other departments on engagement.
- Identify how previous plans or engagement efforts in the area help inform priorities or concerns for the project.
- Document feedback on the engagement and design process.
- List issues or concerns identified that were *not* addressed by the street project, along with listing departments or staff notified of the issues.

## Example Checklist – [Doncaster, Beverly Danbury Project](#)

# Staff Recommended Changes

- Update Equity Priority Areas using new city map
- Update Boulevard description to add “Feature tree lined sidewalks, terraces and medians.”
- Changes to 6.1 Design Parameters table
  - Change Column Name to: Total Walkway Zone Width
  - Add footnote: New streets or dedications require an 8’ minimum terrace
  - Add footnote that measurement for Flex Zone is from face of curb when no parking
  - Remove typical ADT from this table
- Changes to 6.2
  - Change Typical ADT to Estimated ADT
  - New footnote on Neighborhood Yield Street - Streets without driveways need to meet the fire lane requirements noted in Section 6.5
  - New footnote on Community Connector and Community Main St - Transit vehicles need a dynamic envelope of a minimum of 10’ lane with a buffer such as a 2’ gutter.
- Add Section 6.4 Tree Canopy Enhancement Guidance
- Add Section 6.5 Complete Green Streets Guide & Alignment with Fire Code