

APPLICATION FOR DEMOLITION OF PRINCIPAL BUILDINGS

Complete all sections of this application, including signature on page 2.

To request an interpreter, translation, or accommodations, call (608)266-4910.

Para solicitar un intérprete, traducción o acomodaciones llame al (608)266-4910.

Koj muaj txoj cai tau txais kev txhais lus, kev pes lus los sis kev pab cuam txhawm rau

kev tsis taus uas tsis muaj nqi rau koj: Xav paub ntxiv tiv tauj rau (608)266-4910

如需口譯、翻譯或其他便利服務，請致電 (608)266-4910.

City of Madison

Building Inspection Division

215 Martin Luther King Jr Blvd, Ste 017

PO Box 2985

Madison, WI 53701-2985 (608) 266-4551



Submit the following via email to:

- Building Inspection at sprapplications@cityofmadison.com and
- Landmarks Commission at LandmarksCommission@cityofmadison.com (see [submittal schedule](#))

Part 1: General Application Information

Street Address:			
Alder District:		Zoning District:	
Project Contact Person Name		Role	
Company Name			
Phone		Email	
<input type="checkbox"/>	Completed Application (this form)		
<input type="checkbox"/>	Property Owner Permission (signature on this form or an email providing authorization to apply)		
<input type="checkbox"/>	Copy of Notification sent to the Demolition Listserv		Date Sent _____
<input type="checkbox"/>	Copy of Email Pre-Application Notification of Intent to Demolish a Principal Structure sent to District Alder , City-registered neighborhood association(s) , and City-listed business association(s) .		Date Sent _____
<input type="checkbox"/>	\$600 Demolition Application Fee (additional fees may apply depending on full scope of project)		
Are you also seeking a Zoning Map Amendment (Rezoning) or Conditional Use? <input type="checkbox"/> Yes <input type="checkbox"/> No			

Part 2: Information for Landmarks Historic Value Review

<input type="checkbox"/>	Letter of Intent describing the proposed structure to be demolished, description of proposed method and timeline of demolition
<input type="checkbox"/>	Construction Information (Dates of construction and alterations, architect name, builder name, history of property, historic photos)
<input type="checkbox"/>	Existing Condition Photos (Interior and exterior digital photos of each principal building to be demolished sufficient to indicate its character and condition)
<input type="checkbox"/>	Will existing structure be relocated? <input type="checkbox"/> Yes <input type="checkbox"/> No If "yes" include preliminary assessment that relocation is likely to be structurally and legally feasible
<input type="checkbox"/>	Optional: Proposed mitigation plans for properties with possible historic value

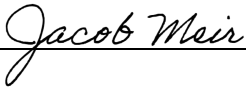
APPLICATION FOR DEMOLITION OF PRINCIPAL BUILDINGS

Part 3: Application for Plan Commission Review (if applicable)

- When Landmarks Commission finds a building has Historic Value, the demolition application must be considered by the Plan Commission.
- If Plan Commission review is required, staff will schedule the public hearing based on the [published schedule](#).
- Applicant must [make an appointment](#) to pick up “Public Hearing” sign from Zoning Counter and post the sign on property at least 21 days before Plan Commission hearing.

Demolition requests will be scheduled concurrently with other related requests before the Plan Commission, where applicable. A schedule confirmation will be emailed to the designated project contact. Contact staff at pcapplications@cityofmadison.com with questions.

Part 4: Signature

Property Owner Authorizing Signature (or authorized via attached email)			
Property Owner Name			
Company Name			
Street Address			
Phone		Email	

For Office Use Only	
Date:	
Accela ID No.:	

May 27, 2025

Building Demolition Letter of Intent

1802-1804 Roth Street

Housing Initiatives is seeking demolition of the existing building at 1802 Roth Street in Madison, WI. The demolition is necessary for the construction of a new multifamily building.

The proposed structure to be demolished is a 2-story townhome of wood construction on concrete block foundation with basement. Demolition of the existing building would begin immediately prior to the start of construction and in sequence with excavation.

The proposed method of demolition is as follows:

- Submit DNR notice for demolition.
- File Waste Management and Recycling Plan.
- Obtain sewer plug permit(s) and disconnect sewer and water lateral at the lot line.
- Coordinate disconnect of gas, electric and any other private utility connections.
- Install temporary construction fencing and required erosion control measures around the work area.
- Demolition and removal of all building materials from the footprint of the building.
- Materials shall be recycled, salvaged, or properly disposed of. Materials not recycled or salvaged shall be disposed of at a C&D landfill and properly documented.
- Concrete, brick, and block materials will be removed and disposed of offsite. Material will be used as clean fill or recycled.

Construction Information:

Year Built: 1932

Interior Alterations: 1980's

Square footage: 2,409

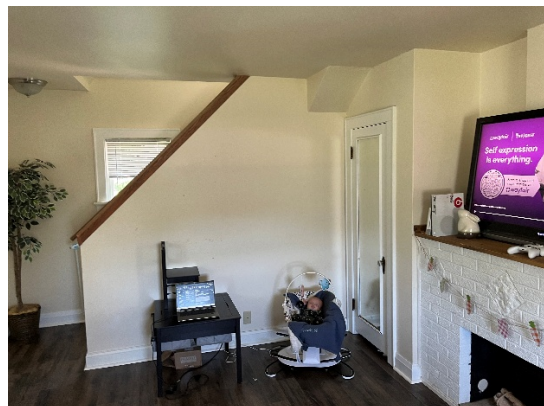
Architect/Building name: unknown

History of Property: private residence

Historic photos: n/a

Existing Condition Photos:





LOTS 8 AND 9, C.E. ROTH PLAT, AS RECORDED IN VOLUME 7 OF PLATS, ON
PAGE 33, AS DOCUMENT NUMBER 531689, DANE COUNTY REGISTRY, LOCATED
IN THE NORTHWEST QUARTER OF THE SOUTHWEST QUARTER OF SECTION
31, TOWNSHIP 08 NORTH, RANGE 10 EAST, CITY OF MADISON, DANE COUNTY,
WISCONSIN

S3 HOUSING LLC.

16 Unit Apartment Building

1802 ROTH STREET.
MADISON WI, 53704



S3 HOUSING LLC.
16 Unit Apartment Building

1802 ROTH STREET.
MADISON WI, 53704

HOUSING INITIATIVES
1110 Ruskin St, Madison, WI 53704

PROJECT NUMBER: 253833

CIVIL

BURSE SURVEYING AND ENGINEERING
2801 International Lane, Suite 101
Madison, WI 53704
PH (608) 250-9263
www.bursesurveyengr.com

LANDSCAPE

PAUL SKIDMORE
13 Red Maple Trail
Madison, WI 53717
(608) 826-1529

PHOTOMETRICS

SPECTRUM
N8 W22520 JOHNSON DRIVE UNIT E
WAUKESHA, WI 53186
PH 262-970-0300

ARCHITECTURAL

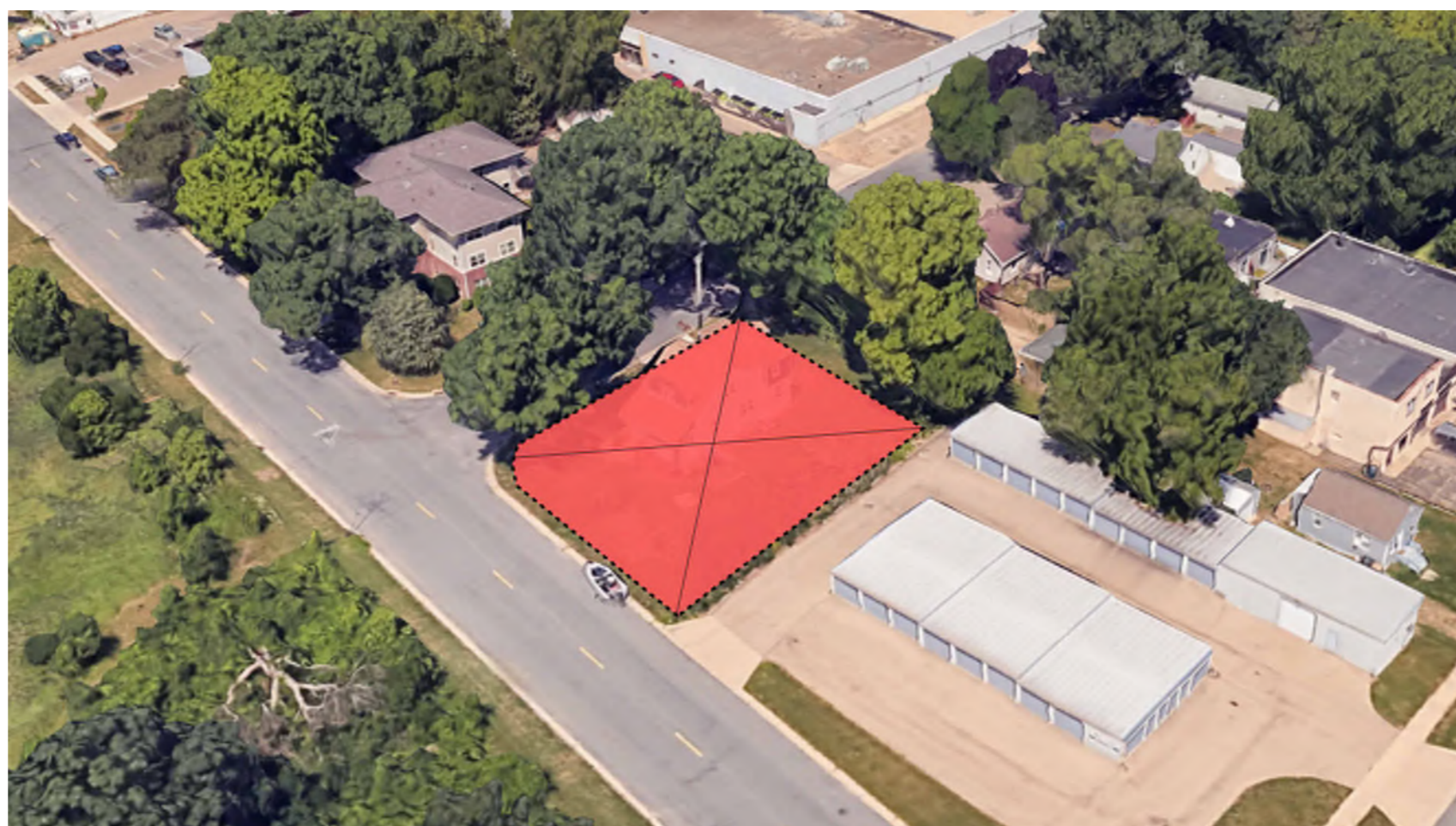
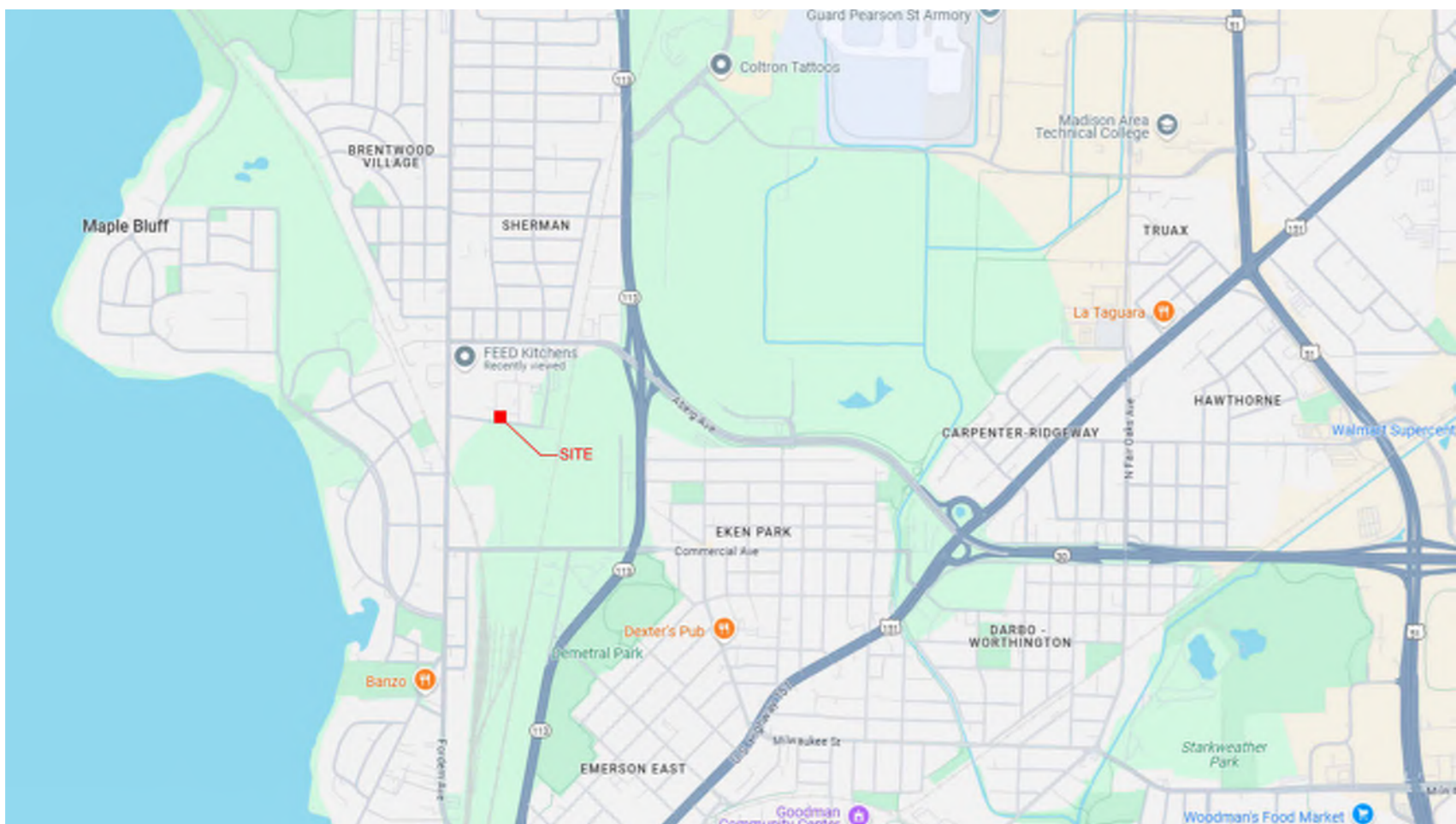
ENGBERG ANDERSON
320 EAST BUFFALO STREET, SUITE 500
MILWAUKEE, WI 53202
PH (414) 944-9000
www.engberganderson.com

C001 CIVIL NOTES & DETAILS
C002 CIVIL NOTES & DETAILS
C100 EXISTING CONDITIONS
C200 DEMOLITION PLAN
C300 EROSION CONTROL PLAN
C400 SITE PLAN
C500 GRADING PLAN
C600 UTILITY PLAN
C700 FIRE ACCESS PLAN

L100 LANDSCAPE PLAN

001 SITE PLAN LIGHTING

A101 ARCHITECTURAL SITE PLAN
A102 FLOOR PLAN
A103 ROOF PLAN
A301 ENLARGED UNIT PLAN
A401 BUILDING ELEVATION
A501 BUILDING SECTION
A501 BUILDING SECTION



ISSUED FOR:

LAND USE APPLICATION 05 27 2025

REVISIONS:

NO.	DESCRIPTION	DATE
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LANDUSE APPLICATION
NOT FOR CONSTRUCTION

DRAWN BY: Author

CHECKED BY: Checker

TITLE SHEET

G001

EROSION CONTROL NOTES/SPECIFICATIONS:

1. EROSION CONTROL DEVICES AND/OR STRUCTURES SHALL BE INSTALLED PRIOR TO CLEARING AND GRUBBING OPERATIONS. THESE SHALL BE PROPERLY MAINTAINED FOR MAXIMUM EFFECTIVENESS UNTIL VEGETATION IS RE-ESTABLISHED.
2. EROSION CONTROL IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ACCEPTANCE OF THIS PROJECT. EROSION CONTROL MEASURES AS SHOWN SHALL BE THE MINIMUM PRECAUTIONS THAT WILL BE ALLOWED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RECOGNIZING AND CORRECTING ALL EROSION CONTROL PROBLEMS THAT ARE THE RESULT OF CONSTRUCTION ACTIVITIES. ADDITIONAL EROSION CONTROL MEASURES, AS REQUESTED IN WRITING BY THE STATE OR LOCAL INSPECTORS, OR THE DEVELOPER'S ENGINEER, SHALL BE INSTALLED WITHIN 24 HOURS.
3. ALL EROSION CONTROL MEASURES AND STRUCTURES SERVING THE SITE MUST BE INSPECTED AT LEAST WEEKLY OR WITHIN 24 HOURS OF THE TIME 0.5 INCHES OF RAIN IS PRODUCED. ALL MAINTENANCE WILL FOLLOW AN INSPECTION WITHIN 24 HOURS. INSPECTION SCHEDULE AND RECORD KEEPING SHALL COMPLY WITH NR 216.46(9), WIS. ADM. CODE.
4. CONSTRUCTION ENTRANCES – PROVIDE A STONE TRACKING PAD AT EACH POINT OF ACCESS. INSTALL ACCORDING TO WDNR STANDARD 1057. REFER TO WDNR'S STORMWATER WEB PAGE OF TECHNICAL STANDARDS AT: [HTTP://DNR.WI.GOV/TOPIC/STORMWATER/STANDARDS/CONST_STANDARDS.HTML](http://dnr.wi.gov/topic/stormwater/standards/const_standards.html). THE TRACKING PAD MUST BE MAINTAINED IN A CONDITION THAT PREVENTS THE TRACKING OF MATERIAL ONTO THE PUBLIC STREET.
5. SOIL STOCKPILES – A ROW OF SILT FENCE PLACED DOWNSLOPE AND AT LEAST 10 FEET AWAY FROM THE STOCKPILE SHALL PROTECT ALL STOCKPILES. SOIL STOCKPILES THAT ARE INACTIVE FOR MORE THAN 14 CONSECUTIVE DAYS SHALL BE STABILIZED WITH SEED & MULCH, EROSION MAT, POLYMER, OR COVERED WITH TARPS OR SIMILAR MATERIAL. NO STOCKPILE SHALL BE PLACED WITHIN 20 FEET OF A DRAINAGE WAY.
6. DEWATERING – WATER PUMPED FROM THE SITE SHALL BE TREATED BY USING A TEMPORARY SEDIMENTATION BASIN, PORTABLE DEWATERING BASIN, GEOTEXTILE BAG, OR AN EQUIVALENT DEVICE. SHOW ON THE PLAN THE ANTICIPATED LOCATIONS OF DEWATERING ACTIVITY, AND PROVIDE AN ENGINEERING DETAIL OF THE DEWATERING SYSTEM. DEVICES SHALL COMPLY WITH WDNR TECHNICAL STANDARD 1061 FOUND AT: [HTTP://DNR.WI.GOV/TOPIC/STORMWATER/STANDARDS/CONST_STANDARDS.HTML](http://dnr.wi.gov/topic/stormwater/standards/const_standards.html). THIS WATER SHALL BE DISCHARGED IN A MANNER THAT DOES NOT INDUCE EROSION OF THE SITE OR ADJACENT PROPERTY.

PUMP SIZE (MAX GPM)	TYPE I BAG SIZE (SQ-FT)
50	25
100	50
150	75

7. STORM SEWER INLETS – PROVIDE WDOT TYPE D "CATCHALL" INLET PROTECTION OR EQUIVALENT. REFER TO WDOT PRODUCT ACCEPTABILITY LIST AT: [HTTP://WWW.DOT.WISCONSIN.GOV/BUSINESS/ENGRSERV/PAL/HTM](http://www.dot.wisconsin.gov/business/engrser/vp/alm.htm). INLET PROTECTION SHALL BE INSTALLED PRIOR TO THE STORM SEWER SYSTEM RECEIVING SITE RUNOFF. OTHER THAN FOR PERFORMING MAINTENANCE, THESE DEVICES SHALL NOT BE REMOVED UNTIL PLAT-LEVEL STABILIZATION IS COMPLETE.
8. BUILDING AND WASTE MATERIALS SHALL BE PREVENTED FROM RUNNING-OFF THE SITE AND ENTERING WATERS OF THE STATE IN CONFORMANCE WITH NR151.12(6M).
9. NO SOLID MATERIAL SHALL BE DISCHARGED OR DEPOSITED INTO WATERS OF THE STATE IN VIOLATION OF CH. 30 OR 31 OF THE WISCONSIN STATE STATUTES OR 33 USC 1344 PERMITS.
10. EROSION CONTROL DEVICES SHALL ADHERE TO THE TECHNICAL STANDARDS FOUND AT: [HTTP://DNR.WI.GOV/RUNOFF/STORMWATER/TECHSTDSTM](http://dnr.wi.gov/runoff/stormwater/techstds.htm) AND COMPLY WITH ALL CITY OF MADISON ORDINANCES.
11. ALL DEBRIS TRACKED ONTO PUBLIC STREETS SHALL BE BE SWEEP OR SCRAPED CLEAN BY THE END OF EACH WORKDAY.
12. ALL BUILDING AND WASTE MATERIAL SHALL BE HANDLED PROPERLY TO PREVENT RUNOFF OF THESE MATERIALS OFF OF THE SITE.
13. ALL DISTURBED AREAS SHALL BE SEEDED IMMEDIATELY AFTER GRADING ACTIVITIES HAVE BEEN COMPLETED.
14. ALL DISTURBED AREAS, EXCEPT PAVED AREAS, SHALL RECEIVE A MINIMUM OF FOUR (4) INCHES OF TOPSOIL, FERTILIZER, SEED, AND MULCH. SEED MIXTURES SHALL BE SELECTED APPROPRIATE TO THE INTENDED FUNCTION. A QUALIFIED LANDSCAPING CONTRACTOR, LANDSCAPE ARCHITECT OR NURSERY CAN BE CONSULTED FOR RECOMMENDATIONS. SEEDING RATES SHALL BE BASED ON POUNDS OR OUNCES OF PURE LIVE SEED PER ACRE AND SHALL BE PROVIDED BY THE SEED SUPPLIER. FERTILIZER CAN BE APPLIED TO HELP PROMOTE GROWTH, BUT A SOIL TEST IS RECOMMENDED TO DETERMINE THE TYPE AND AMOUNT OF FERTILIZER TO BE APPLIED. ALL SEEDING AND RESTORATION SHALL BE IN CONFORMANCE TO WDNR TECHNICAL STANDARD 1058 FOUND AT [HTTP://DNR.WI.GOV/TOPIC/STORMWATER/STANDARDS/CONST_STANDARDS.HTML](http://dnr.wi.gov/topic/stormwater/standards/const_standards.htm). SEEDING AND SOODING MAY ONLY BE USED FROM MAY 1ST TO SEPTEMBER 15TH OF ANY YEAR. TEMPORARY SEED SHALL BE USED AFTER SEPTEMBER 15. IF TEMPORARY SEEDING IS USED, A PERMANENT COVER SHALL ALSO BE REQUIRED AS PART OF THE FINAL SITE STABILIZATION.
15. FOR THE FIRST SIX (6) WEEKS AFTER THE INITIAL STABILIZATION OF A DISTURBED AREA, WATERING SHALL BE PERFORMED WHENEVER MORE THAN SEVEN (7) DAYS OF DRY WEATHER ELAPSE.

EMERGENCY CONTACT:

Brod Hinkfuss
1110 Ruskin Street
Madison, WI 53704
bhinkfuss@housinginitiaives.org
(608) 620-1751

SCHEDULE:

JUNE 30, 2025	INSTALL SILT FENCE AND CONSTRUCTION ENTRANCE.
JULY 1, 2025	BEGIN DISTURBANCE OF SITE GROUND COVER.
SEPTEMBER 15, 2025	BASE COURSE INSTALLED. APPLY SEED AND MULCH TO ALL DISTURBED AREAS.
NOVEMBER 15, 2025	VEGETATION ESTABLISHED.

GENERAL NOTES:

1. ALL PRIVATE UTILITIES (GAS, ELECTRIC, AND TELECOMMUNICATIONS) SERVING EXISTING BUILDINGS SCHEDULED FOR DEMOLITION TO BE ABANDONED OR REMOVED BY CORRESPONDING UTILITY COMPANY.
2. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION OF UNDERGROUND UTILITIES. UTILITIES WERE LOCATED BY OBSERVED EVIDENCE, MARKINGS PROVIDED BY DIGGER'S HOTLINE, AND RECORD DRAWINGS FROM THE CITY OF MADISON.
3. CONTRACTOR SHALL VERIFY THE SIZE, TYPE, SLOPE, AND INVERTS OF ALL EXISTING STORM AND SANITARY LATERALS CALLED OUT TO BE CONNECTED TO. CONTRACTOR SHALL SUBMIT THE INFORMATION ON THE PIPES TO THE CITY INSPECTOR AND PROJECT CIVIL ENGINEER.
4. ANY SIDEWALK, CURB, OR OTHER PUBLIC PROPERTY DAMAGED AS PART OF THE CONSTRUCTION OF THE UTILITIES AND BUILDING SHALL BE REPLACED IN-KIND PER THE CITY OF MADISON STANDARD SPECIFICATIONS.
5. THE RIGHT-OF-WAY IS THE SOLE JURISDICTION OF THE CITY OF MADISON AND IS SUBJECT TO CHANGE AT ANY TIME PER THE RECOMMENDATION/PLAN OF THE CITY.
6. CONTRACTOR SHALL INSTALL TREE PROTECTION FENCING IN THE AREA BETWEEN THE CURB AND SIDEWALK AND EXTEND IT AT LEAST 5 FEET FROM BOTH SIDES OF THE TREE ALONG THE LENGTH OF THE TERRACE. NO EXCAVATION IS PERMITTED WITHIN 5 FEET OF THE OUTSIDE EDGE OF A TREE TRUNK. IF EXCAVATION WITHIN 5 FEET OF ANY TREE IS NECESSARY, CONTRACTOR SHALL CONTACT CITY FORESTRY (266-4816) PRIOR TO EXCAVATION TO ASSESS THE IMPACT TO THE TREE AND ROOT SYSTEM. TREE PRUNING SHALL BE COORDINATED WITH CITY FORESTRY PRIOR TO THE START OF CONSTRUCTION. TREE PROTECTION SPECIFICATIONS CAN BE FOUND IN SECTION 107.13 OF CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION <http://www.cityofmadison.com/business/pw/documents/addresses/2018/par11.pdf>. ANY TREE REMOVALS THAT ARE REQUIRED FOR CONSTRUCTION AFTER THE DEVELOPMENT PLAN IS APPROVED WILL REQUIRE AT LEAST A 72 HOUR WAIT PERIOD BEFORE A TREE REMOVAL PERMIT CAN BE ISSUED BY FORESTRY, TO NOTIFY THE ALDER OF THE CHANGE IN THE TREE PLAN.
7. CONTRACTOR SHALL BE RESPONSIBLE FOR SITE SAFETY DURING THE CONSTRUCTION OF IMPROVEMENTS.

DEMOLITION NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR THE DEMOLITION, REMOVAL, AND DISPOSING IN A LOCATION APPROVED BY ALL COVERING AUTHORITIES, OF ALL STRUCTURES, PADS, WALLS, FLUMES, FOUNDATIONS, PARKING DRIVES, DRAINAGE, STRUCTURES, UTILITIES, ETC., SUCH THAT THE IMPROVEMENTS SHOWN ON THE REMAINING PLANS CAN BE CONSTRUCTED. ALL FACILITIES TO BE REMOVED SHALL BE UNDERCUT TO SUITABLE MATERIAL AND BROUGHT TO GRADE WITH SUITABLE COMPACTED FILL MATERIAL PER THE SPECIFICATIONS.
2. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED FOR DEMOLITION AND DISPOSAL.
3. THE CONTRACTOR SHALL COORDINATE WITH THE RESPECTIVE UTILITY COMPANIES PRIOR TO THE REMOVAL AND/OR RELOCATION OF UTILITIES. THE CONTRACTOR SHALL COORDINATE WITH THE UTILITY COMPANY CONCERNING PORTIONS OF WORK WHICH MAY BE PERFORMED BY THE UTILITY COMPANY. FORCING ANY FEES WHICH ARE TO BE PAID TO THE UTILITY COMPANY FOR THEIR SERVICES. THE DEVELOPER IS RESPONSIBLE FOR PAYING ALL FEES AND CHARGES.
4. THE LOCATIONS OF ALL EXISTING UTILITIES SHOWN ON THIS PLAN HAVE BEEN DETERMINED FROM THE BEST INFORMATION AVAILABLE AND ARE GIVEN FOR THE CONVENIENCE OF THE CONTRACTOR. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THEIR ACCURACY. PRIOR TO THE START OF ANY DEMOLITION ACTIVITY, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES FOR ONSITE LOCATIONS OF EXISTING UTILITIES. NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
5. ALL EXISTING SEWERS, PIPING, AND UTILITIES SHOWN ARE NOT TO BE INTERPRETED AS THE EXACT LOCATION, OR AS THE ONLY OBSTACLES THAT MAY OCCUR ON THE SITE. VERIFY EXISTING CONDITIONS AND PROCEED WITH CAUTION AROUND ANY ANTICIPATED FEATURES, GIVE NOTICE TO ALL UTILITY COMPANIES REGARDING DESTRUCTION AND REMOVAL OF ALL SERVICE LINES BEFORE PROCEEDING WITH THE WORK. UTILITIES DETERMINED TO BE ABANDONED AND LEFT IN PLACE SHALL BE GROUTED IF UNDER BUILDINGS.
6. ELECTRICAL, TELEPHONE, CABLE, WATER, FIBER OPTIC CABLE, AND/OR GAS LINES NEEDING TO BE REMOVED OR RELOCATED SHALL BE COORDINATED WITH THE AFFECTED UTILITY COMPANY. ADEQUATE TIME SHALL BE PROVIDED FOR RELOCATION AND CLOSE COORDINATION WITH THE UTILITY COMPANY IS NECESSARY TO PROVIDE A SMOOTH TRANSITION IN UTILITY SERVICE.
7. CONTRACTOR SHALL PROTECT THE PUBLIC AT ALL TIMES WITH FENCING, BARRICADES, ENCLOSURES, COVERED WALKWAYS, ETC. CONTRACTOR SHALL SUBMIT THEIR STREET OCCUPANCY PLAN TO TRAFFIC ENGINEERING FOR APPROVAL.
8. PRIOR TO DEMOLITION OCCURRING, ALL APPLICABLE EROSION CONTROL DEVICES ARE TO BE INSTALLED.
9. THE CONTRACTOR MAY LIMIT SAWCUT AND PAVEMENT REMOVAL TO ONLY THOSE AREAS WHERE IT IS REQUIRED AS SHOWN ON THESE CONSTRUCTION PLANS, BUT IF ANY DAMAGE IS INCURRED ON ANY OF THE SURROUNDING PAVEMENT, ETC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ITS REMOVAL AND REPAIR IN KIND.
10. DAMAGE TO ALL EXISTING CONDITIONS TO REMAIN WILL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
11. CONTINUOUS ACCESS SHALL BE MAINTAINED FOR THE SURROUNDING PROPERTIES AT ALL TIMES DURING DEMOLITION OF THE EXISTING FACILITIES.

SITE PLAN NOTES:

1. PAVEMENT DESIGN SHALL BE PER THE RECOMMENDATION OF THE SOILS CONSULTANT.
2. THE RIGHT-OF-WAY IS THE SOLE JURISDICTION OF THE CITY OF MADISON AND IS SUBJECT TO CHANGE AT ANY TIME PER THE RECOMMENDATION/PLAN OF TRAFFIC ENGINEERING AND CITY ENGINEERING DEPARTMENTS.
3. TRAFFIC CONTROL SIGNAGE SHALL BE IN ACCORDANCE WITH FEDERAL, STATE, COUNTY, CITY, AND LOCAL CODE, WHICHEVER HAS JURISDICTION.
4. CITY SIDEWALK AND CURB DAMAGED IN THE COURSE OF CONSTRUCTION TO BE REPLACED PER GRADING PLAN AND PER CITY OF MADISON STANDARD SPECIFICATIONS.
5. NEW APRONS SHALL BE CONSTRUCTED AND PLACED IN CONFORMANCE WITH THE CITY OF MADISON STANDARD DETAIL 3.02 FOR COMMERCIAL OPENINGS.

GRADING PLAN NOTES:

1. ALL GRADES ARE FINISH ELEVATION UNLESS NOTED OTHERWISE.

UTILITY PLAN NOTES:

1. ALL WORK WITHIN THE CITY RIGHT OF WAY AND EASEMENTS SHALL BE COMPLIANT WITH THE CITY OF MADISON STANDARD SPECIFICATIONS CURRENT AT THE TIME OF CONSTRUCTION.
2. UTILITY INSTALLATION SHALL BE COORDINATED WITH ENGINEER AT LEAST 4 WEEKS PRIOR TO INSTALLATION TO ENSURE BUILDING INSPECTION APPROVAL IS OBTAINED.

LEGEND/ ABBREVIATIONS

---	ACCESSIBLE ROUTE
----	EXISTING EASEMENT
-----	PROPERTY BOUNDARY
⬡	PARKING STALL COUNT
L.S.	LANDSCAPED AREA
BIT.	BITUMINOUS PAVEMENT
CONC.	CONCRETE PAVEMENT
1041.02	SPOT GRADE
FG	FINISH GRADE
EG	EXISTING GRADE
SW	SIDEWALK
EP	EDGE OF PAVEMENT
FL	FLOW LINE
LP	LOW POINT
HP	HIGH POINT
TC	TOP OF CURB
TW	TOP OF WALL
BW	BOTTOM OF WALL
BOW	BACK OF WALK
FOW	FRONT OF WALK
EXP	EXPOSURE
—SAN—	PROPOSED SANITARY SEWER
—ST—	PROPOSED STORM SEWER
—W—	PROPOSED WATER LATERAL
~~~~~	UTILITY LINE DEMOLITION
X	TREE REMOVAL
-- 1041 --	EXISTING MINOR CONTOUR
-- 1040 --	EXISTING MAJOR CONTOUR
— 1041 —	PROPOSED MINOR CONTOUR
— 1040 —	PROPOSED MAJOR CONTOUR
— — —	PROPOSED RIDGE LINE
- - -	PROPOSED SWALE/DITCH
— · —	ACCESSIBLE PARKING SIGN
▲	VISION TRIANGLE (NO VISUAL OBSTRUCTIONS BETWEEN HEIGHTS OF 30' AND 10')
⬢	RIPRAP
⬢	CONSTRUCTION ENTRANCE
---	SAW CUT / REMOVAL LIMITS
----	DISTURBANCE LIMITS
— 0 — 0 —	SILT FENCE
— 0 — 0 —	SILT SOCK
— 0 — 0 —	CHECK DAM
— 0 — 0 —	DIVERSION BERM
⬢	INLET PROTECTION
— · — · —	USLE FLOW PATH

SHEET INDEX	
SHEET NUMBER	SHEET TITLE
C001	CIVIL DETAILS & NOTES
C002	CIVIL DETAILS
C100	EXISTING CONDITIONS
C200	DEMOLITION PLAN
C300	EROSION CONTROL PLAN
C400	SITE PLAN
C500	GRADING PLAN
C600	UTILITY PLAN
C700	FIRE ACCESS PLAN

ISSUED FOR:	
LAND USE APPLICATION	09/27/2025

REVISIONS:		
NO.	DESCRIPTION	DATE

LAND USE APPLICATION  
NOT FOR CONSTRUCTION

DRAWN BY:	DRH
CHECKED BY:	PDF

CIVIL NOTES & DETAILS

C001

**HOUSING INITIATIVES - 3S  
HOUSING LLC**

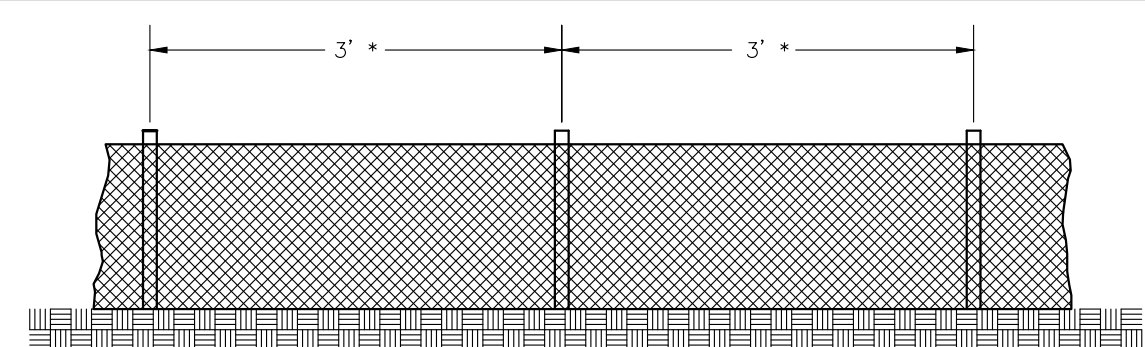
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<CLIENT NAME>

<OWNER NAME>

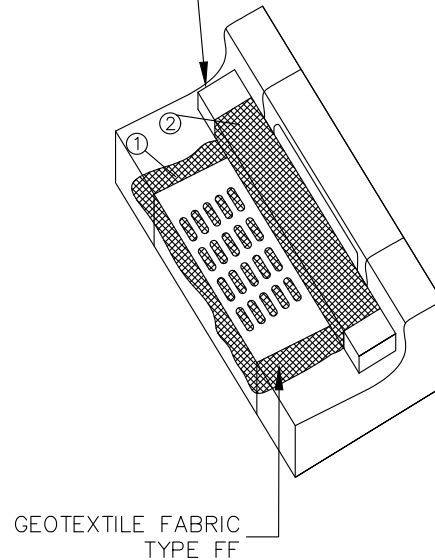
<OWNER ADDRESS>

PROJECT NUMBER: 253833



**1 SILT FENCE CONSTRUCTION**  
C-002/NTS

WOOD 2X4 EXTENDS 8"  
BEYOND GRADE WITH ON BOTH  
SIDES. LENGTH = 52" FOR H-S  
INLETS, 94" FOR H-H INLETS.



**GENERAL NOTES**

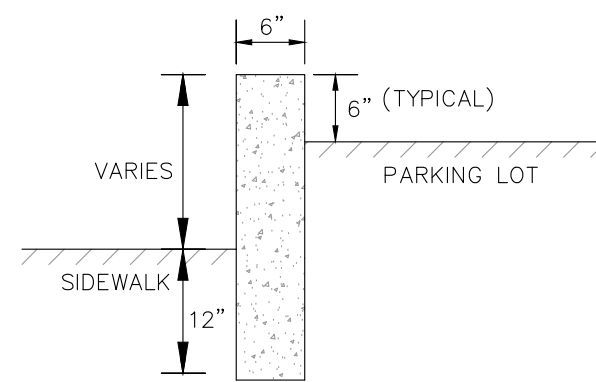
FABRIC SHALL BE REPLACED AT ENGINEER'S DISCRETION.  
THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE  
CURB BOX.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON  
THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY  
LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE  
SHALL BE TAKEN SO THAT THE SEDIMENT DOES NOT FALL  
INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET  
SHALL BE REMOVED IMMEDIATELY.

- FABRIC SIZE SHALL BE 8" (MIN) GREATER ON ALL  
SIDES OF THE INLET COVER TO PROVIDE A HAND  
HOLD WHEN MAINTENANCE OR REMOVAL IS  
REQUIRED.
- FOR INLET PROTECTION, TYPE C WITH CURB  
BOX AN ADDITIONAL 18" OF FABRIC IS  
WRAPPED AROUND THE WOOD AND SECURED  
WITH STAPLES.

**2 INLET PROTECTION**  
C-002/NTS

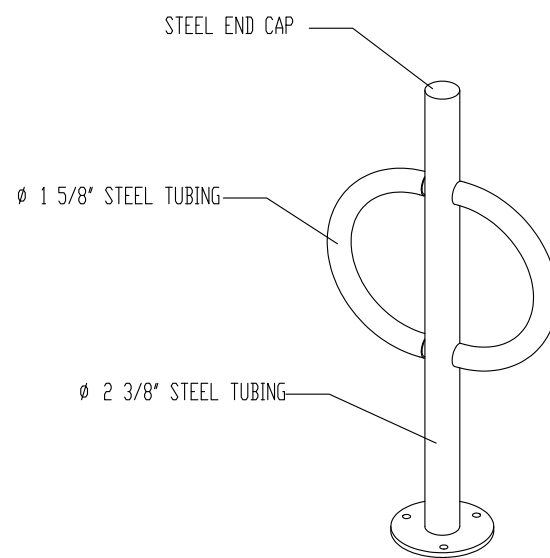
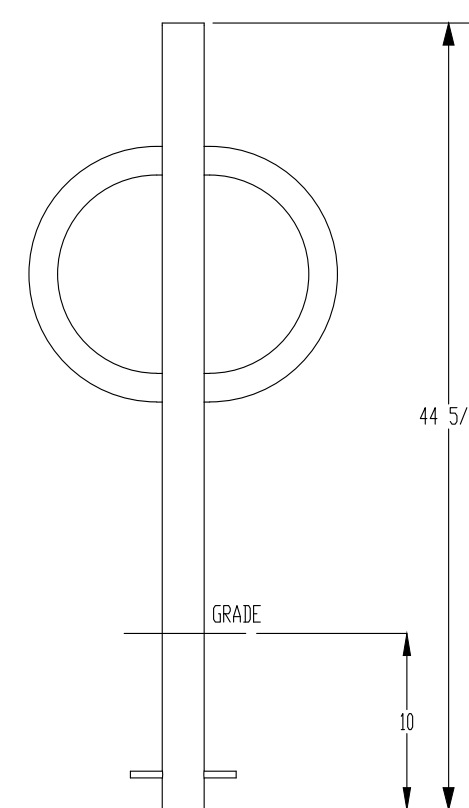
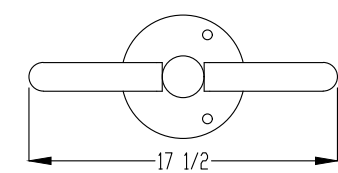


**CURB DETAIL**

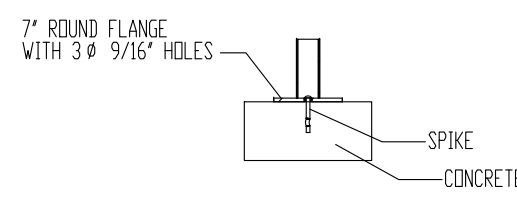
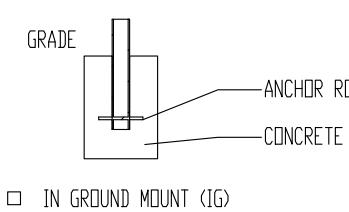
**3 6" CONCRETE CURBHEAD**  
C-002/NTS



MADRAX DIVISION  
GRABER MANUFACTURING, INC.  
1801 INLET DRIVE  
WATKINS, WI 53090  
PHONE 440-7921, 7923, 7949, 7948, 7940, 849-1888  
WWW.MADRAX.COM E-MAIL: SALES@MADRAX.COM



**CHECK DESIRED MOUNT**



PRODUCT: PAR-2-ST-020  
DESCRIPTION: POST AND RING BIKE RACK WITH FLAT CAP, TUBE, STEEL ARMS  
DATE: 10-17-18  
END: SNC  
CONFIDENTIAL DRAWING AND INFORMATION IS NOT TO BE COPIED OR DISCLOSED  
TO OTHERS WITHOUT THE CONSENT OF GRABER MANUFACTURING, INC.  
SPECIFICATIONS ARE SUBJECT TO CHANGE WITHOUT NOTICE.  
©2018 GRABER MANUFACTURING, INC. ALL PROPRIETARY RIGHTS RESERVED.

- NOTES:
- INSTALL BIKE RACKS ACCORDING TO MANUFACTURER'S SPECIFICATIONS.
  - CONSULTANT TO SELECT COLOR (OPTIONAL). SEE MANUFACTURER'S SPECIFICATIONS.
  - SEE SITE PLAN FOR LOCATION OR CONSULT TOWNS.

**ISSUED FOR:**

LAND USE APPLICATION 09/27/2025

**REVISIONS:**

NO. DESCRIPTION DATE

LAND USE APPLICATION  
NOT FOR CONSTRUCTION

DRAWN BY:

DRH

CHECKED BY:

PDF

**CIVIL NOTES & DETAILS**

**C002**



**HOUSING INITIATIVES - 3S**  
**HOUSING LLC**  
<PROJECT DESCRIPTION>

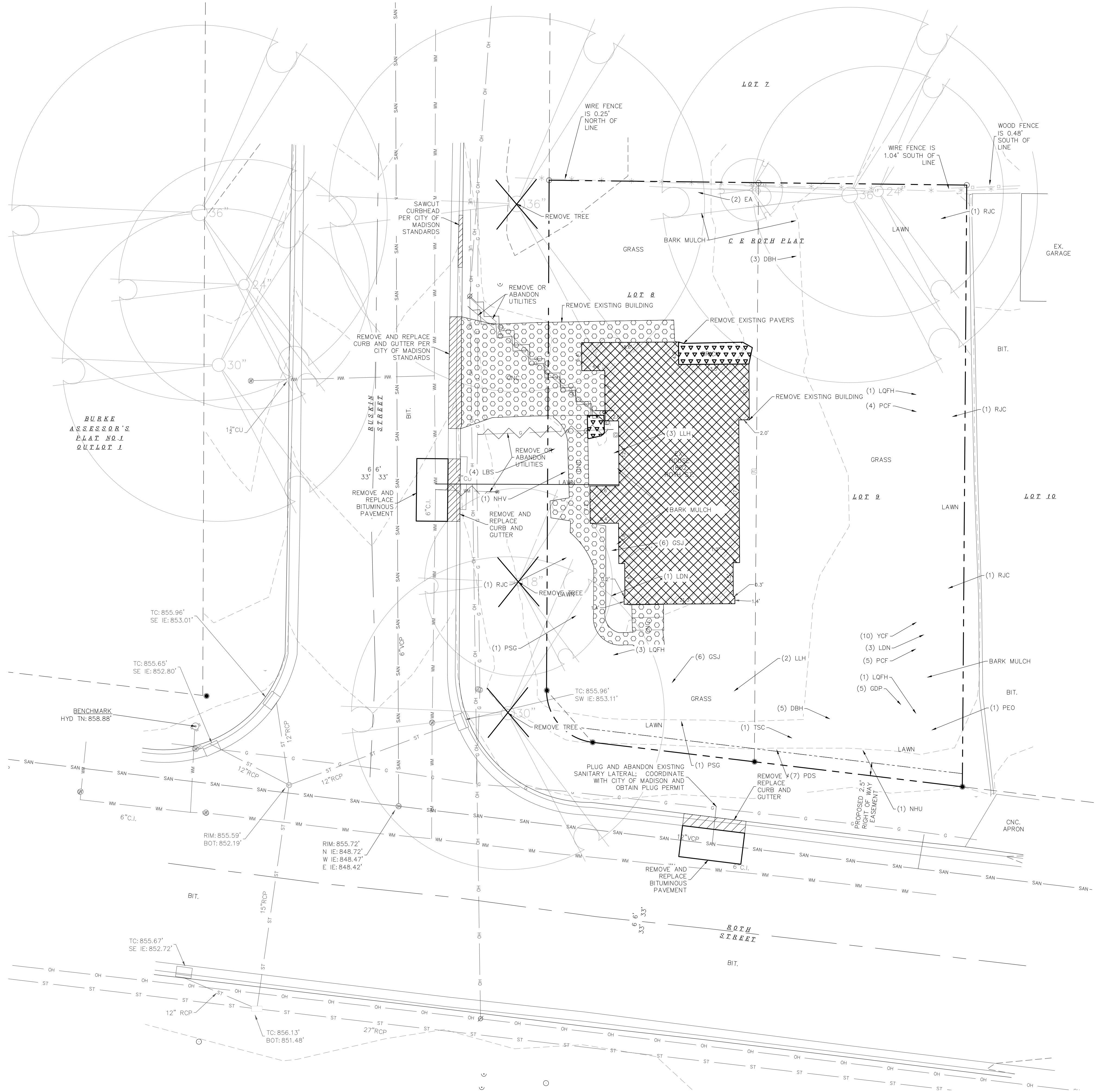
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<CLIENT NAME>

<OWNER NAME>

<OWNER ADDRESS>

PROJECT NUMBER: 253833

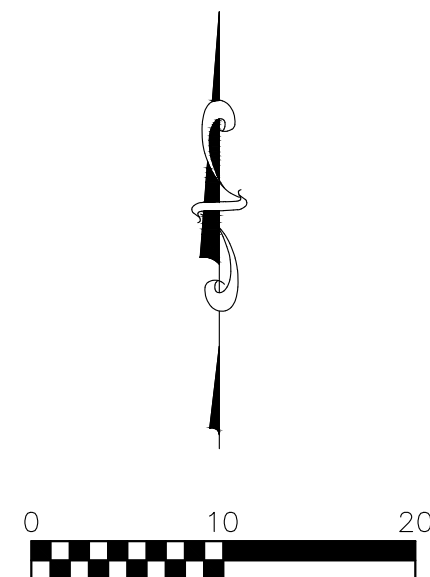


**LEGEND**

	REMOVE CURB		REMOVE PAVERS
	REMOVE CONCRETE		RAZE BUILDING
	REMOVE UTILITY LINE		TREE REMOVAL

**DEMOLITION NOTES:**

1. THE CONTRACTOR IS RESPONSIBLE FOR THE DEMOLITION, REMOVAL, AND DISPOSING IN A LOCATION APPROVED BY ALL GOVERNING AUTHORITIES, OF ALL STRUCTURES, PADS, WALLS, FLUMES, FOUNDATIONS, PARKING DRIVES, DRAINAGE, STRUCTURES, UTILITIES, ETC., SUCH THAT THE IMPROVEMENTS SHOWN ON THE REMAINING PLANS CAN BE CONSTRUCTED. ALL FACILITIES TO BE REMOVED SHALL BE UNDERCUT TO SUITABLE MATERIAL AND BROUGHT TO GRADE WITH SUITABLE COMPACTED FILL MATERIAL PER THE SPECIFICATIONS.
2. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED FOR DEMOLITION AND DISPOSAL.
3. THE CONTRACTOR SHALL COORDINATE WITH THE RESPECTIVE UTILITY COMPANIES PRIOR TO THE REMOVAL AND/OR RELOCATION OF UTILITIES. THE CONTRACTOR SHALL COORDINATE WITH THE UTILITY COMPANY CONCERNING PORTIONS OF WORK WHICH MAY BE PERFORMED BY THE UTILITY COMPANY FORCING ANY FEES WHICH ARE TO BE PAID TO THE UTILITY COMPANY FOR THEIR SERVICES. THE DEVELOPER IS RESPONSIBLE FOR PAYING ALL FEES AND CHARGES.
4. THE LOCATIONS OF ALL EXISTING UTILITIES SHOWN ON THIS PLAN HAVE BEEN DETERMINED FROM THE BEST INFORMATION AVAILABLE AND ARE GIVEN FOR THE CONVENIENCE OF THE CONTRACTOR. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THEIR ACCURACY. PRIOR TO THE START OF ANY DEMOLITION ACTIVITY, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES FOR ONSITE LOCATIONS OF EXISTING UTILITIES. NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
5. ALL EXISTING SEWERS, PIPING, AND UTILITIES SHOWN ARE NOT TO BE INTERPRETED AS THE EXACT LOCATION, OR AS THE ONLY OBSTACLES THAT MAY OCCUR ON THE SITE. VERIFY EXISTING CONDITIONS AND PROCEED WITH CAUTION AROUND ANY ANTICIPATED FEATURES. GIVE NOTICE TO ALL UTILITY COMPANIES REGARDING DESTRUCTION AND REMOVAL OF ALL SERVICE LINES BEFORE PROCEEDING WITH THE WORK. UTILITIES DETERMINED TO BE ABANDONED AND LEFT IN PLACE SHALL BE GROUTED IF UNDER BUILDING.
6. ELECTRICAL, TELEPHONE, CABLE, WATER, FIBER OPTIC CABLE, AND/OR GAS LINES NEEDING TO BE REMOVED OR RELOCATED SHALL BE COORDINATED WITH THE AFFECTED UTILITY COMPANY. ADEQUATE TIME SHALL BE PROVIDED FOR RELOCATION AND CLOSE COORDINATION WITH THE UTILITY COMPANY IS NECESSARY TO PROVIDE A SMOOTH TRANSITION IN UTILITY SERVICE.
7. CONTRACTOR SHALL PROTECT THE PUBLIC AT ALL TIMES WITH FENCING, BARRICADES, ENCLOSURES, COVERED WALKWAYS, ETC. CONTRACTOR SHALL SUBMIT THEIR STREET OCCUPANCY PLAN TO TRAFFIC ENGINEERING FOR APPROVAL.
8. PRIOR TO DEMOLITION OCCURRING, ALL EROSION CONTROL DEVICES ARE TO BE INSTALLED.
9. THE CONTRACTOR MAY LIMIT SAWCUT AND PAVEMENT REMOVAL TO ONLY THOSE AREAS WHERE IT IS REQUIRED AS SHOWN ON THESE CONSTRUCTION PLANS, BUT IF ANY DAMAGE IS INCURRED ON ANY OF THE SURROUNDING PAVEMENT, ETC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ITS REMOVAL AND REPAIR IN KIND.
10. DAMAGE TO ALL EXISTING CONDITION TO REMAIN WILL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
11. CONTINUOUS ACCESS SHALL BE MAINTAINED FOR THE SURROUNDING PROPERTIES AT ALL TIMES DURING DEMOLITION OF THE EXISTING FACILITIES.



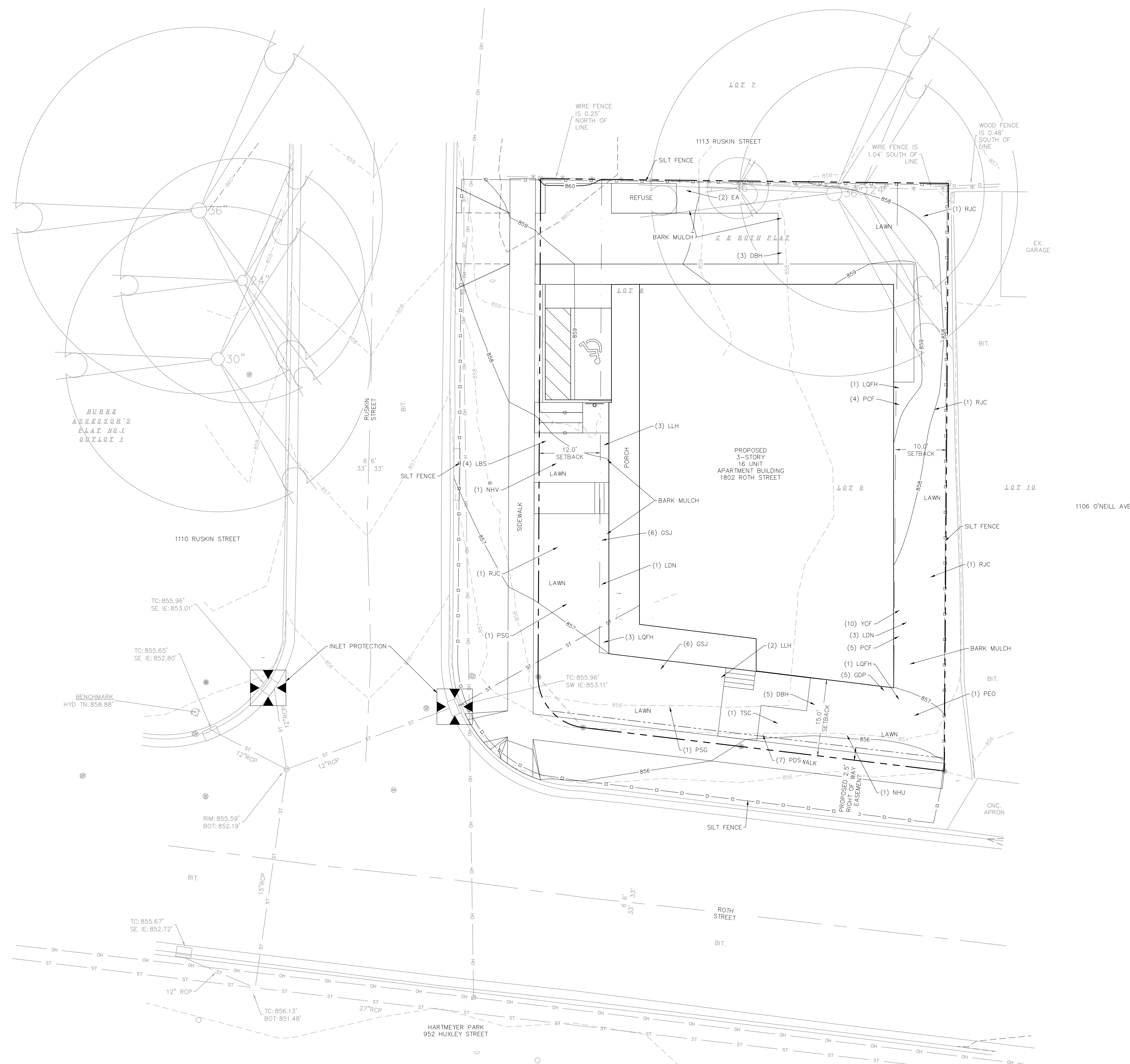
ISSUED FOR:		
LAND USE APPLICATION		09/27/2025
REVISIONS:		
NO.	DESCRIPTION	DATE

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**DEMOLITION PLAN**

**C200**



ISSUED FOR:

LAND USE APPLICATION 05/27/2025

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NO.	DESCRIPTION	DATE
	LAND USE APPLICATION NOT FOR CONSTRUCTION	

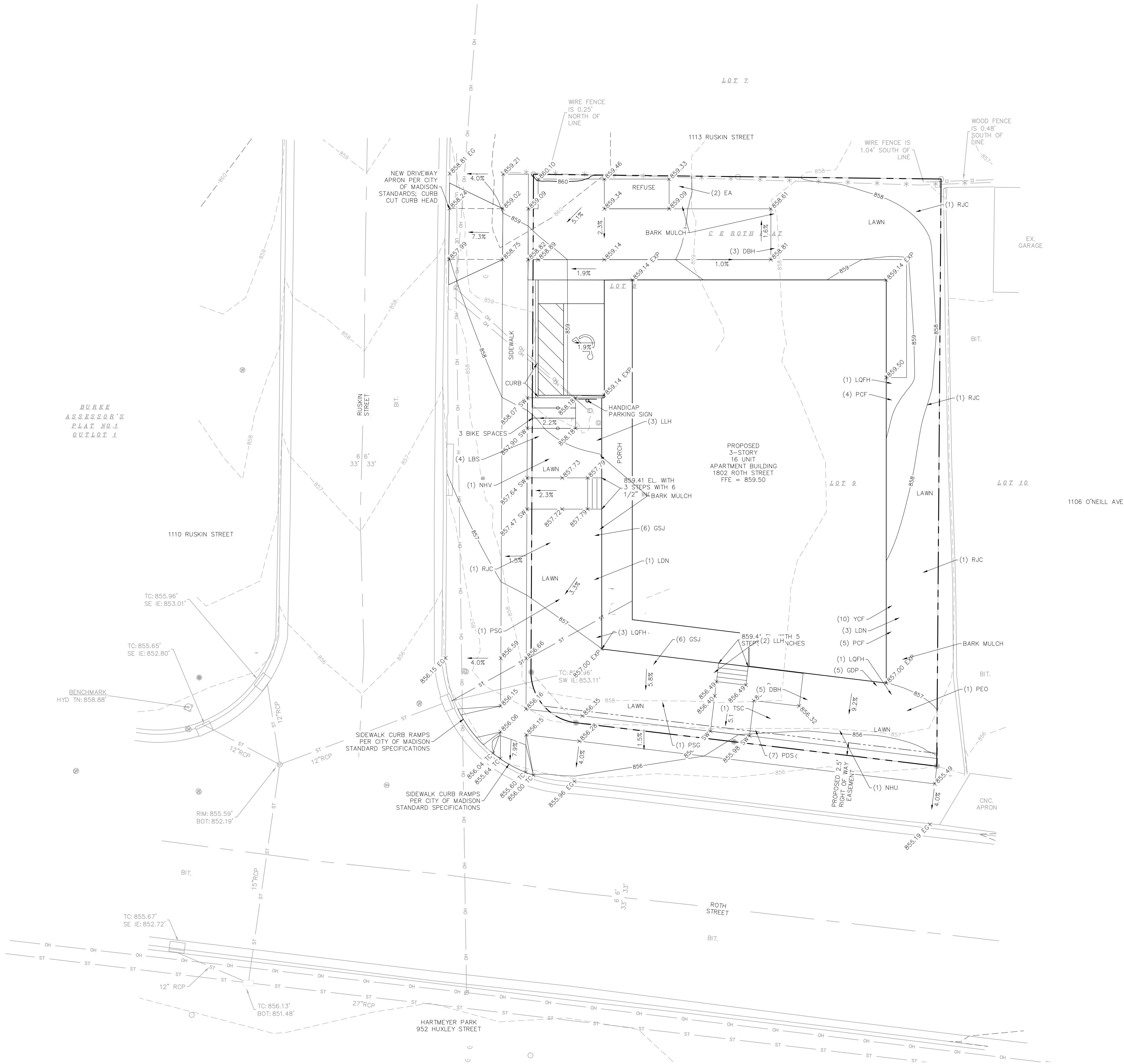
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CHECKED BY: PDF

## EROSION CONTROL PLAN

# C300





ISSUED FOR:

LAND USE APPLICATION 09/27/2025

REVISIONS:

NO.	DESCRIPTION	DATE
LAND USE APPLICATION NOT FOR CONSTRUCTION		

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CHECKED BY: PDF

GRADING PLAN

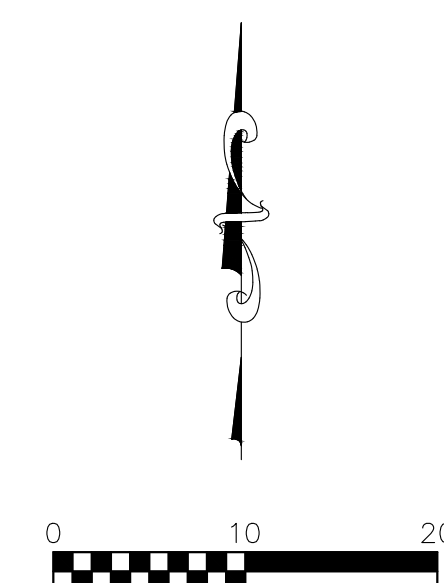
**<PROJECT DESCRIPTION>**

<PROJECT ADDRESS>

<CLIENT NAME>

&lt;OWNER NAME&gt;

PROJECT NUMBER: 253833



ISSUED FOR:

LAND USE APPLICATION 05/27/2025

REVISIONS:

NO.	DESCRIPTION	DATE
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77

**LAND USE APPLICATION  
NOT FOR CONSTRUCTION**

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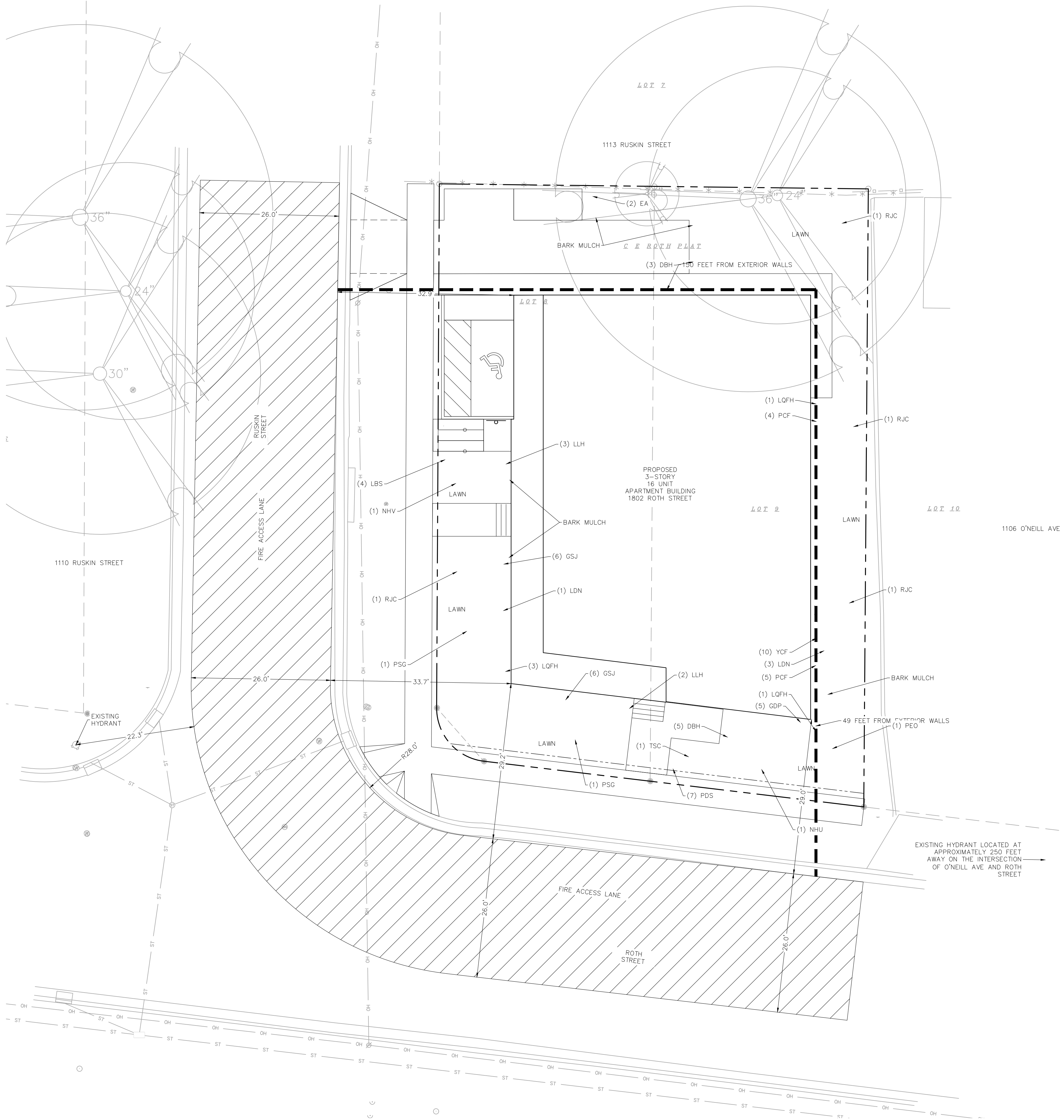
DRH

CHECKED BY _____

PDF

## UTILITY PLAN

# C600




20' WIDE FIRE ACCESS LANE

NOTES:

MAX 150' DISTANCE FROM FIRE ACCESS LANE TO ALL EXTERIOR WALLS OF A NONSPRINKLERED BUILDING

MAX 250' DISTANCE FROM FIRE ACCESS LANE TO ALL EXTERIOR WALLS OF A SPRINKLERED BUILDING

500' MAX HOSE LAY DISTANCE FROM AT LEAST (2) FIRE HYDRANTS TO FIRE ACCESS LANE



City of Madison Fire Department

314 W Dayton Street, Madison, WI 53703  
Phone: 608-266-4420 • Fax: 608-267-1100 • E-mail: fire@cityofmadison.com

Project Address: 1802 ROTH STREET

Contact Name & Phone #: BRAD HINKFUSS (608) 620-1751

FIRE APPARATUS ACCESS AND FIRE HYDRANT WORKSHEET

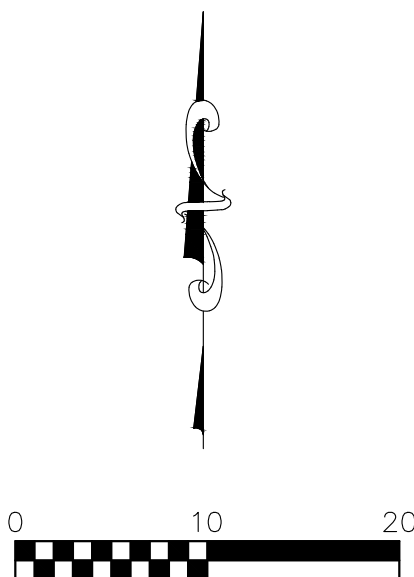
1. Is the building completely protected by an NFPA 13 or 13R automatic fire sprinkler system?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
If non-sprinklered, fire lanes extend to within 150-feet of all portions of the exterior wall?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
If sprinklered, fire lanes are within 250-feet of all portions of the exterior wall?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
2. Is the fire lane constructed of concrete or asphalt, designed to support a minimum load of 85,000 lbs?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
a) Is the fire lane a minimum unobstructed width of at least 20-feet?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
b) Is the fire lane unobstructed with a vertical clearance of at least 13½-feet?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
c) Is the minimum inside turning radius of the fire lane at least 28-feet?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
d) Is the grade of the fire lane not more than a slope of 8%?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
e) Is the fire lane posted as fire lane? (Provide detail of signage.)	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
f) Is a roll-able curb used as part of the fire lane? (Provide detail of curb.)	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
g) Is part of a sidewalk used as part of the required fire lane? (Must support +85,000 lbs.)	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
3. Is the fire lane obstructed by security gates or barricades? If yes:	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
a) Is the gate a minimum of 20-feet clear opening?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
b) Is an approved means of emergency operations installed, key vault, padlock or key switch?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
4. Is the Fire lane dead-ended with a length greater than 150-feet? If yes, does the area for turning around fire apparatus comply with IFC D103?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
5. Is any portion of the building to be used for high-piled storage in accordance with IFC Chapter 3206.6. If yes, see IFC 3206.6 for further requirements.	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No	<input type="checkbox"/> N/A
6. Is any part of the building greater than 30-feet above the grade plane? If yes, answer the following questions:	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
a) Is the aerial apparatus fire lane parallel to one entire side of the building and covering at least 25% of the perimeter?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
b) Is the near edge of the aerial apparatus fire lane between 15' and 30' from the building?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
c) Are there any overhead power or utility lines located across the aerial apparatus fire lane?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
d) Are there any tree canopies expected to grow across the aerial fire lane? (Based on mature canopy width of tree species)	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
e) Does the aerial apparatus fire lane have a minimum unobstructed width of 26-feet?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
f) Is the space between the aerial lane and the building free of trees exceeding 20' in height?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
7. Are all portions of the required fire lanes within 500-feet of at least (2) hydrants? Note: Distances shall be measured along the path of the hose lay as it comes off the fire apparatus.	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
a) Is the fire lane at least 26' wide for at least 20-feet on each side of the hydrants?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
b) Is there at least 40' between a hydrant and the building?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
c) Are the hydrant(s) setback no less than 5-feet nor more than 10-feet from the curb or edge of the street or fire lane?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
d) Are hydrants located in parking lot islands a minimum of 3½-feet from the hydrant to the curb?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A
e) Are there no obstructions, including but not limited to: power poles, trees, bushes, fences, posts located, or grade changes exceeding 1½-feet, within 5-feet of a fire hydrant?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> N/A

Note: Hydrants shall be installed and in-service prior to combustible construction on the project site.

Attach an additional sheet if further explanation is required for any answers.

This worksheet is based on MGO 34.583 and IFC 2021 Edition Chapter 5 and Appendix D; please see the codes for further information.

Revised 06/2022



**<PROJECT DESCRIPTION>**

<PROJECT ADDRESS>

&lt;CLIENT NAME&gt;

&lt;OWNER NAME&gt;

PROJECT NUMBER: 253833

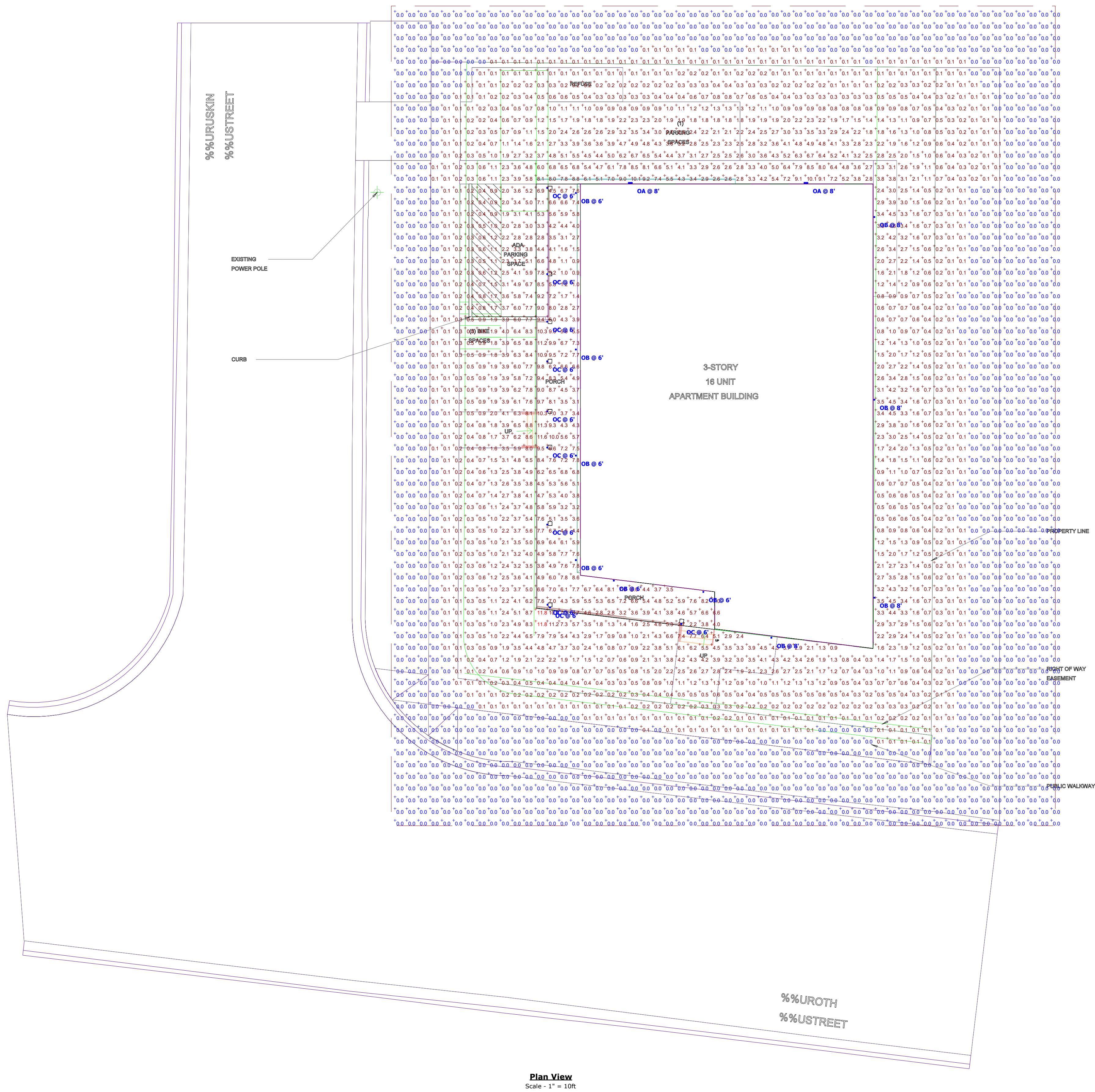


DRAWN BY: _____ DR _____

CHECKED BY: _____ PD _____

LANDSCAPE PLAN

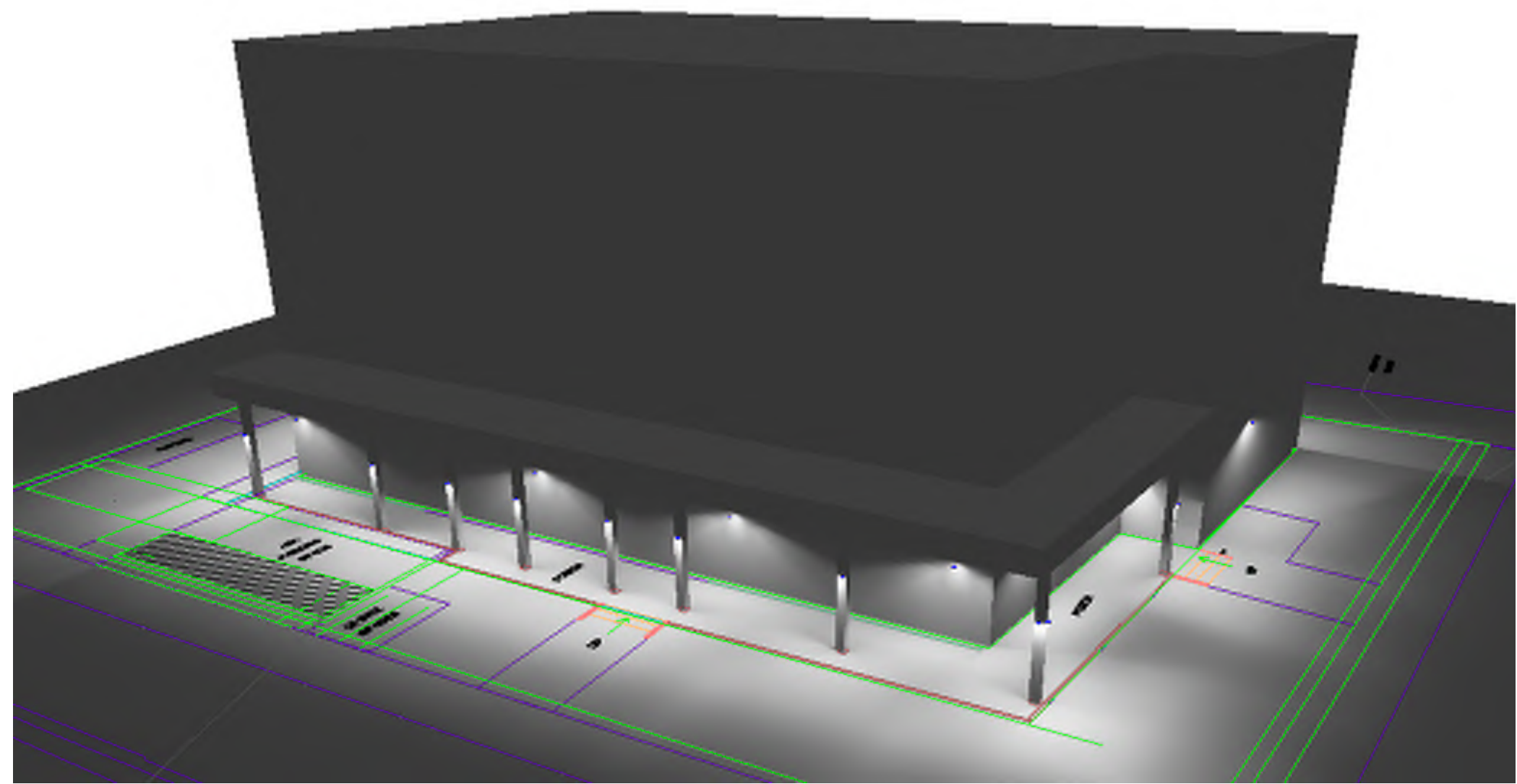
# L100



Symbol	Label	Image	QTY	Manufacturer	Catalog Number	Description	Lamp	Number Lamps	Lumens per Lamp	LLF	Wattage	Polar Plot
	OA		2	LIGMAN	ULEW-20W-T3-W40-**-120/27°V	Leeds Wedge 3 Surface wall luminaires	12 LED 4000K	1	2176	1	19.6	
	OB		10	LIGMAN	ULEW-5.5W-T2-W40-**-120/27°V	Leeds Wedge 1 Surface wall luminaires	4 LED 4000K	1	632	1	7.7	
	OC		10	LIGMAN	ULEW-5.5W-T4-W40-**-120/27°V	Leeds Wedge 1 Surface wall luminaires	4 LED 4000K	1	642	1	7.6	

*No substitutions shall be allowed without prior approval from engineer of record and lighting designer.

Description	Symbol	Avg	Max	Min	Max/Min Avg/Min
Property Line @ 4ft AFF		0.0 ft	0.1 ft	0.0 ft	N/A



**ULEW-30021**  
Leeds 3 Large Surface Wedge Downlight

**microVos TECHNOLOGY**  
**LIGMAN LIGHTING USA**

Clean, beautiful, surface wall fixtures with class leading performance. Minimalist form, yet the most powerful and flexible lighting tool of its type, offering packages up to 2,400 lumens and microVos technology.

A range of small, square and rectangular, ADA compliant wall mounted luminaires with options of upward or downward light distributions. Ideally suited to illuminate the wall and surfaces in front of wall and for light accents on vertical surfaces using high efficiency LEDs. The Leeds is suitable for indoor and outdoor applications and provides a clean, visually appealing solution for small, unobtrusive wall mounted luminaires.

This luminaire is available in 3 different sizes and in combinations of down, up or up/down light distributions.

This feature utilizes microVos technology, meaning the ability to do Type I, II, III, IV & V distributions as well as hybrid distributions to suit the designer's requirements.

Using the microVos optics allows for very wide spacing to mounting height ratios, while still providing perfect uniformity and code compliant light levels.

To meet International Dark Sky criteria, 3000K or warmer LEDs must be selected and luminaires be mounted (4'-11" allowable to permit leveling).

Additional Options (Contact Factory for Pricing)

microVos TECHNOLOGY

**ULEW-30001**  
Leeds 1 Small Surface Wedge Downlight

**microVos TECHNOLOGY**  
**LIGMAN LIGHTING USA**

Clean, beautiful, surface wall fixtures with class leading performance. Minimalist form, yet the most powerful and flexible lighting tool of its type, offering packages up to 2,400 lumens and microVos technology.

A range of small, square and rectangular, ADA compliant wall mounted luminaires with options of upward or downward light distributions. Ideally suited to illuminate the wall and surfaces in front of wall and for light accents on vertical surfaces using high efficiency LEDs. The Leeds is suitable for indoor and outdoor applications and provides a clean, visually appealing solution for small, unobtrusive wall mounted luminaires.

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Additional Options (Contact Factory for Pricing)

microVos TECHNOLOGY

All light levels are approximate. Values will vary due to actual LED/driver combinations, reflectances (currently set at 80/50/20), environmental conditions, and obstructions. Calculations are based on our interpretation of information provided to us. This design remains the property of Spectrum Lighting and may not be altered without written permission.



3S Housing LLC  
1802 Rock St  
Madison, WI

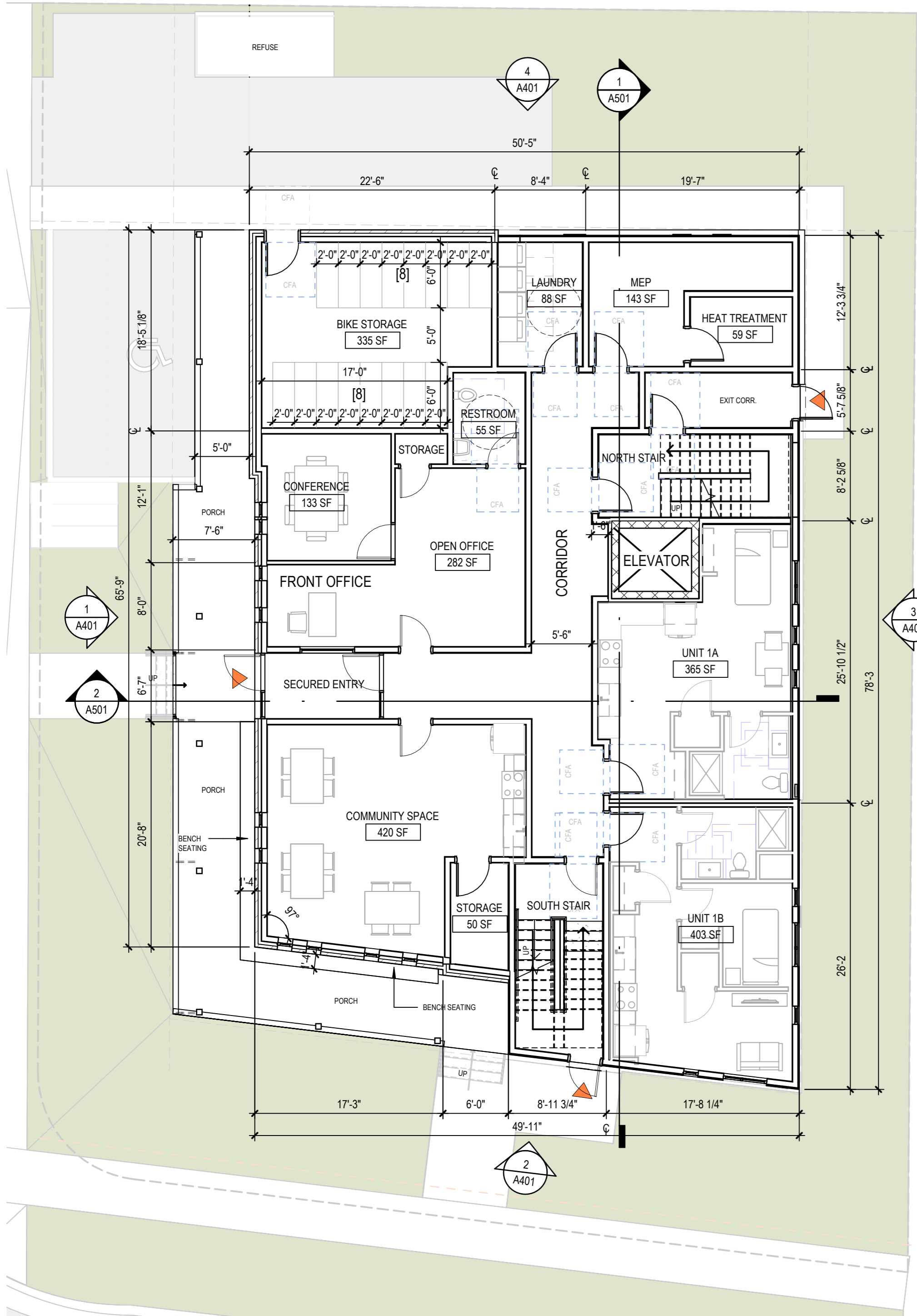
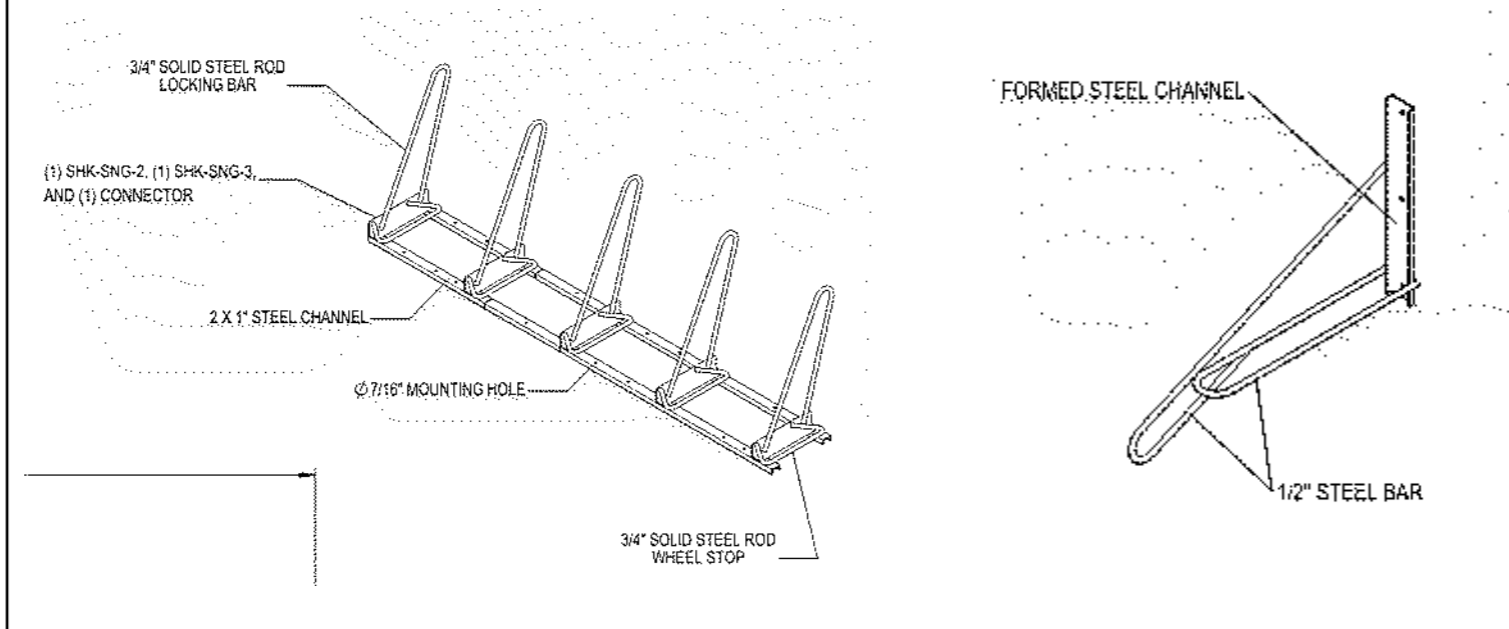
Designer  
BGC  
Date  
05/20/2025  
Scale  
Not to Scale  
Drawing No.  
Summary



GENERAL NOTES - FLOOR PLANS

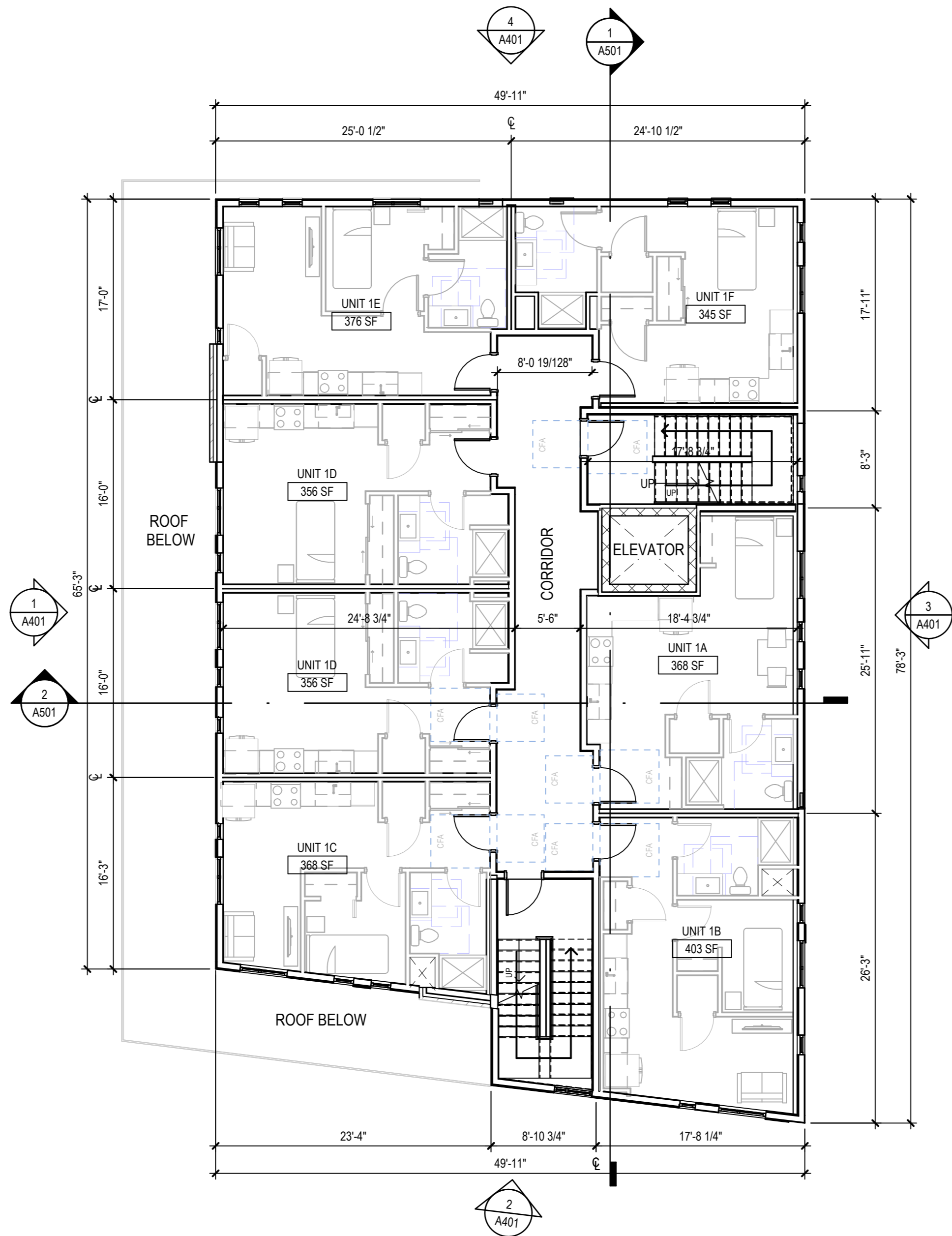
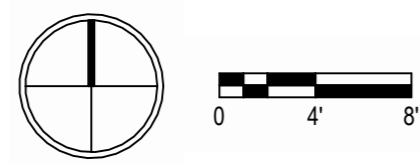
1. 1/8"=1'-0" CIVIL = 100'-0" ARCHITECTURAL
2. DIMENSIONS AT BUILDING EXTERIOR MASONRY VENEER ARE TO EXTERIOR FACE OF MASONRY.
3. DIMENSIONS AT BUILDING EXTERIOR NON-MASONRY VENEER ARE TO EXTERIOR FACE OF SHEATHING.
4. DIMENSIONS AT BUILDING INTERIOR ARE TO NOMINAL FACE OF FINISHED GYPSUM BOARD UNLESS OTHERWISE NOTED.
5. 4" DIMENSION TYPICAL FROM DOOR JAMB TO ADJACENT WALL, UNLESS NOTED OTHERWISE.
6. REFER TO ENLARGED PLANS FOR ADDITIONAL WALL AND DOOR TAGS.
7. UNLESS NOTED OTHERWISE, UNIT DEMISING WALLS ARE TYPE Q22 AND UNIT TO CORRIDOR WALLS ARE TYPE R22. SEE WALL TYPES ON SHEET A600 FOR THESE AND ADDITIONAL DETAILS.
8. REFER TO SHEET A601 FOR ROOF, CEILING AND FLOOR ASSEMBLIES.
9. MECHANICAL EQUIPMENT FOR COORDINATION ONLY. SEE ENLARGED UNIT PLANS FOR DWELLING UNIT INFORMATION.
10. DWELLING UNITS SHOWN FOR REFERENCE ONLY. SEE ENLARGED UNIT PLANS FOR DWELLING UNIT INFORMATION.
11. SEE FINISH SCHEDULE AND FINISH PLANS FOR FINISH INFORMATION.
12. VERIFY ALL EXTERIOR ROUGH OPENING DIMENSIONS WITH PRODUCT MANUFACTURERS.
13. REFER TO CODE CONFORMANCE PLANS FOR FIRE EXTINGUISHER AND FIRE EXTINGUISHER CABINET LOCATIONS.
14. COORDINATE REQUIRED SHEAR WALL CONSTRUCTION AND LOCATIONS AS CALLED FOR ON STRUCTURAL DRAWINGS.
15. GENERAL CONTRACTOR TO PROVIDE PROPOSED CONTROL JOINT PLAN FOR REVIEW BY ARCHITECT AND STRUCTURAL ENGINEER.

BIKE PARKING RACK INFO



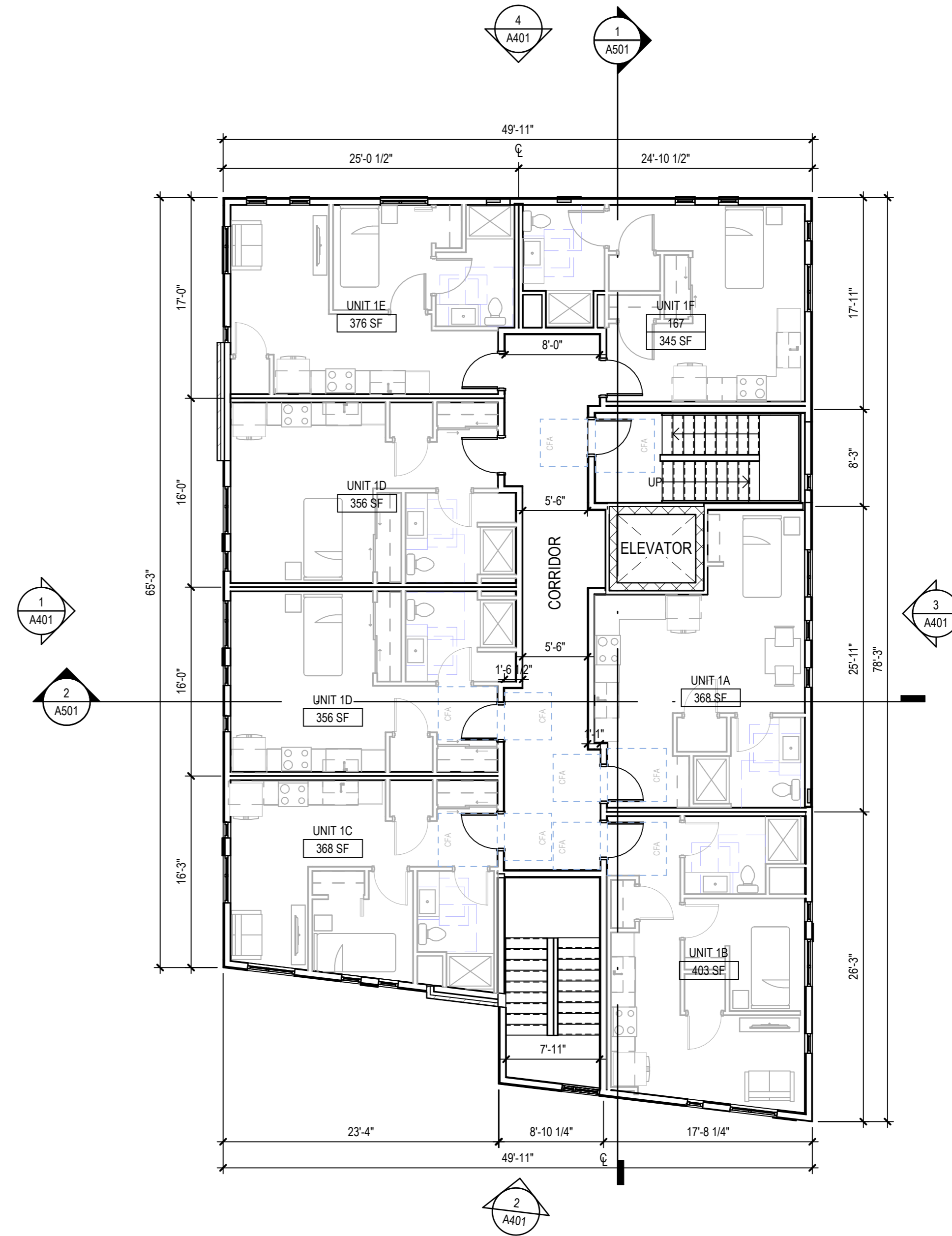
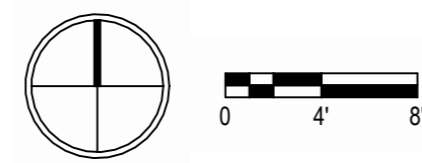
1 LEVEL 1 PLAN

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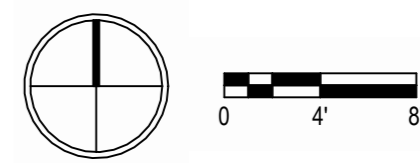
2 LEVEL 2 PLAN

1/8" = 1'-0"



3 LEVEL 3 PLAN

1/8" = 1'-0"



S3 HOUSING LLC.

16 Unit Apartment Building

1802 ROTH STREET,  
MADISON WI. 53704

HOUSING INITIATIVES  
1110 Ruskin St, Madison, WI 53704

PROJECT NUMBER:

253833

ISSUED FOR:

LAND USE APPLICATION

05/27/2025

REVISIONS:

NO.	DESCRIPTION	DATE
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LANDUSE APPLICATION  
NOT FOR CONSTRUCTION

DRAWN BY:

Author

CHECKED BY:

Checker

FLOOR PLAN

S3 HOUSING LLC.  
16 Unit Apartment Building

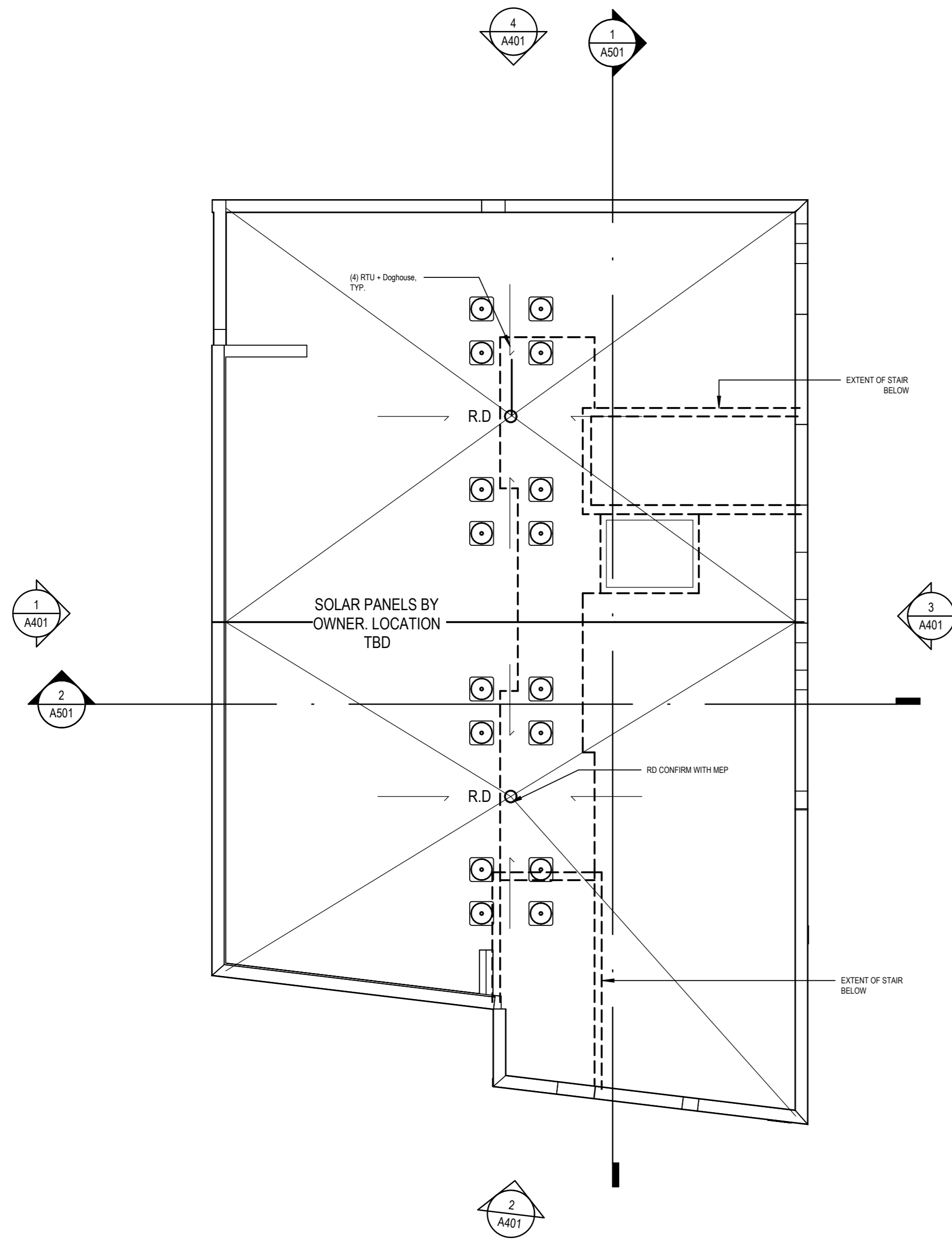
1802 ROTH STREET,  
MADISON WI. 53704

HOUSING INITIATIVES  
1110 Ruskin St, Madison, WI 53704

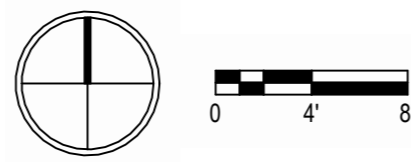
PROJECT NUMBER: 253833

GENERAL NOTES - ROOF PLANS

1. ALL ROOF EQUIPMENT TO BE CENTRALIZED AT THE MIDDLE OF ROOF.
2. SLOPE ROOF TO DRAINS, TYPICAL.
3. ROOF DRAINS SHOWN FOR INTENT ONLY. FINAL DESIGN AND QUANTITIES TO BE PROVIDED BY DESIGN-BUILD CONTRACTOR & COORDINATED WITH ROOFER.
4. SOLAR PANELS BY OWNER.



1 ROOF  
1/8" = 1'-0"



ISSUED FOR:

LAND USE APPLICATION 05 27 2025

REVISIONS:

NO.	DESCRIPTION	DATE
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LANDUSE APPLICATION  
NOT FOR CONSTRUCTION

DRAWN BY: Author

CHECKED BY: Checker

ROOF PLAN

S3 HOUSING LLC.

16 Unit Apartment Building

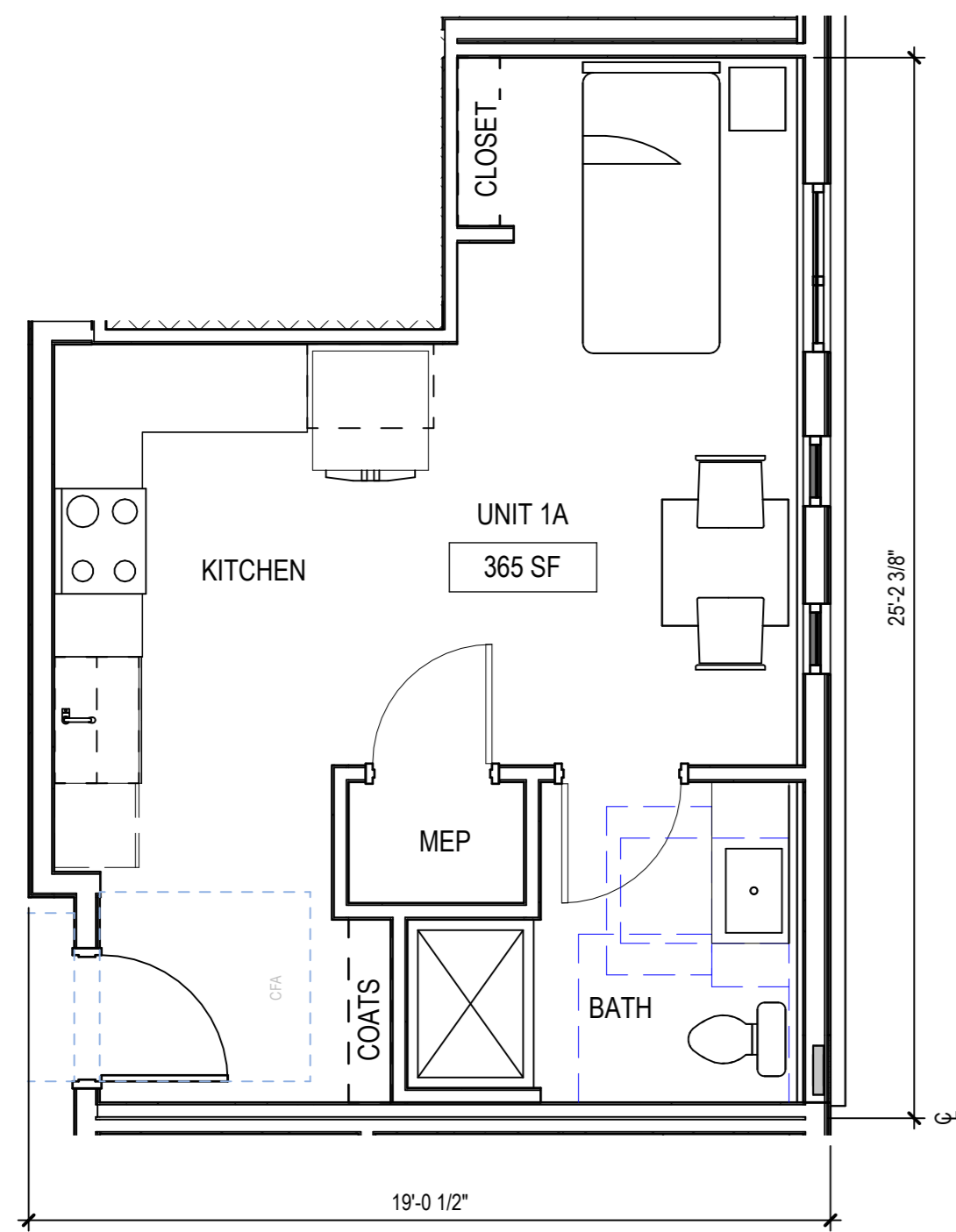
1802 ROTH STREET,  
MADISON WI. 53704

HOUSING INITIATIVES  
1110 Ruskin St, Madison, WI 53704

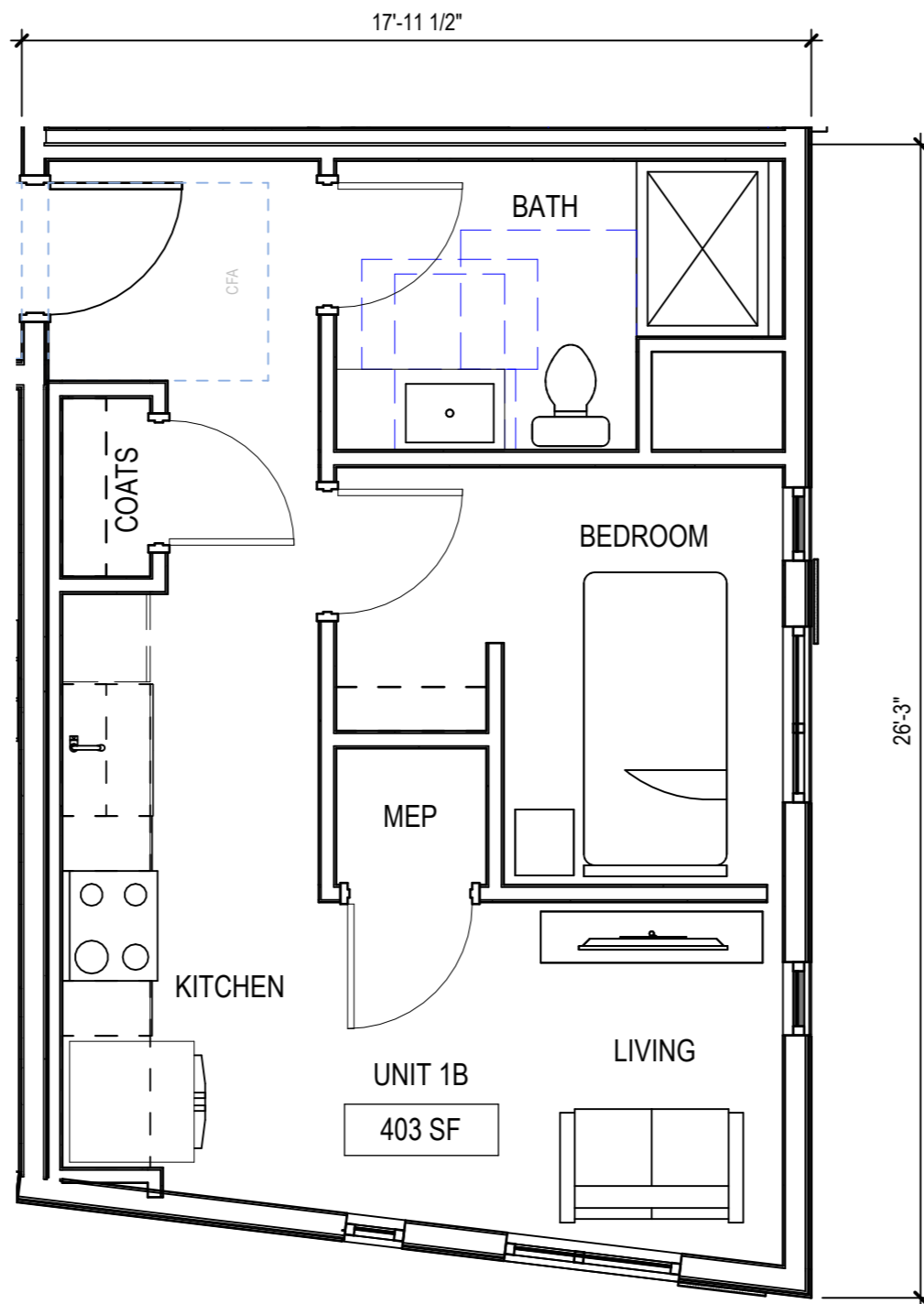
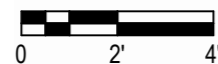
PROJECT NUMBER: 253833

GENERAL NOTES - UNIT PLANS

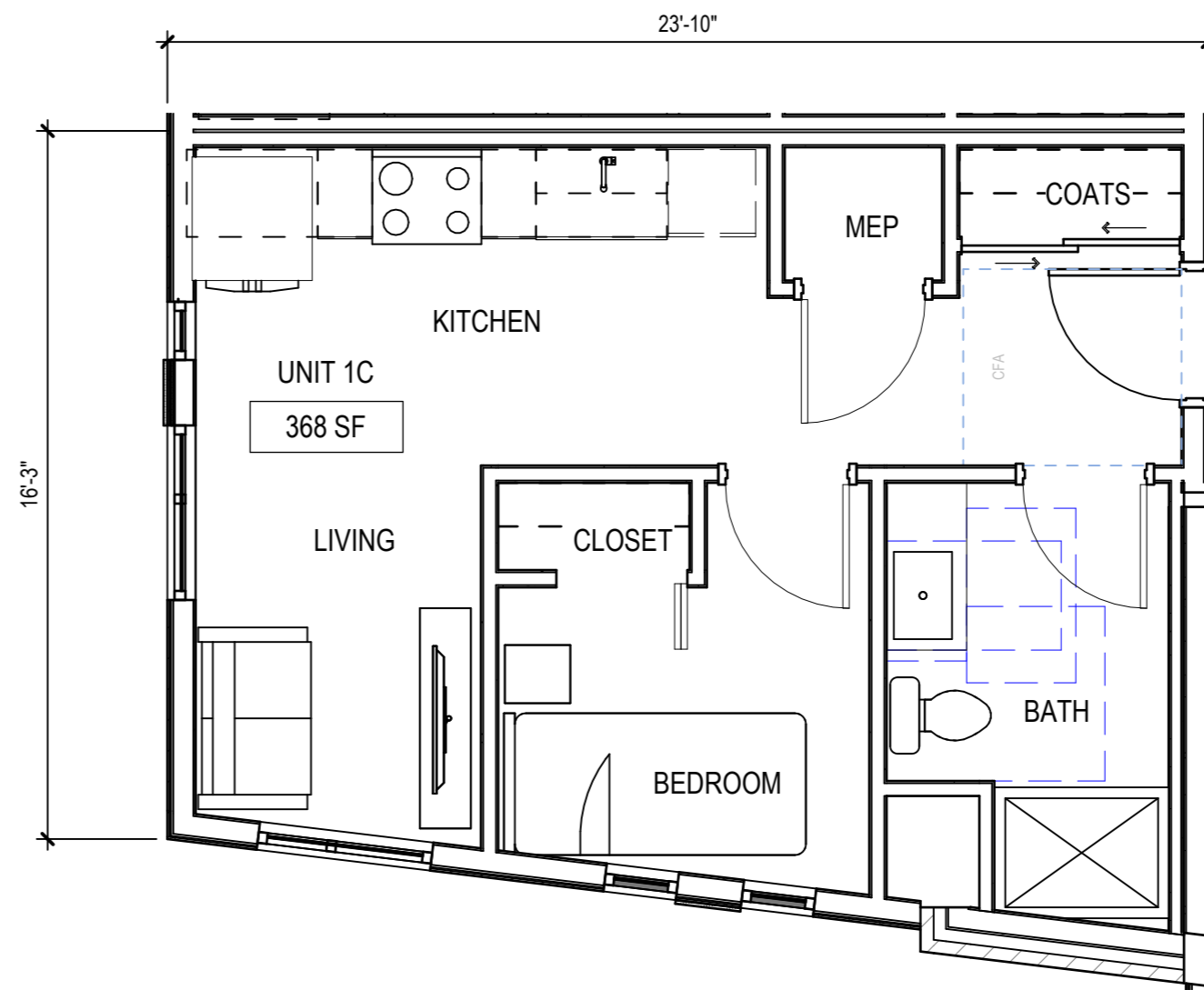
1. VERIFY LOCATION AND SIZES OF COLUMNS, BEAMS, AND FLOOR SLAB THICKNESS PRIOR TO CORING. COORDINATE WITH STRUCTURAL ENGINEER FOR CORING RESTRICTIONS IN ALL FLOORS, BEAMS, COLUMNS AND EXTERIOR WALLS.
2. BUILDING GRID DIMENSIONS MAY DIFFER SLIGHTLY FROM ACTUAL LOCATIONS. FIELD VERIFY AS REQUIRED FOR PLACEMENT OF NEW CONSTRUCTION.
3. ALL DIMENSIONS ON PLANS SHOWN ARE TO THE FACE OF GYPSUM WALL BOARD UNLESS NOTED OTHERWISE.
4. ALL WALLS TO BE Q02 UNLESS OTHERWISE NOTED.
5. 4" DIMENSION TYPICAL FROM DOOR JAMB TO ADJACENT WALL (STUD) UNLESS OTHERWISE NOTED.
6. WOOD CASING AROUND INTERIOR SWING DOORS (BOTH SIDES).
7. VERIFY CABINET, APPLIANCE, AND PLUMBING FIXTURE CLEARANCE REQUIREMENTS W/ MANUFACTURER.
8. INSULATE WALLS SURROUNDING MECHANICAL CLOSETS AND BATHROOMS.
9. PROVIDE FINISHED SURFACE AT ALL EXPOSED CABINET ENDS, INCLUDING UNDERNEATH CABINETS OVER SINKS IF EXPOSED.
10. GYPSUM WALL BOARD SHALL CONTINUE BEHIND CABINETRY.
11. PROVIDE GYPSUM BOARD RATED FOR WET LOCATIONS BEHIND SHOWERS STALLS AND OTHER WET LOCATIONS.
12. PROVIDE GYPSUM BOARD RETURNS AT ALL SLIDING DOOR OPENINGS.
13. PROVIDE TENSION ROD AT ALL TUBS AND SHOWERS.
14. PROVIDE ROBE HOOK ON WALL AT SAME END AS SHOWER HEAD.
15. WHENEVER POSSIBLE, INSTALL TOILET PAPER ON DRYWALL RATHER THAN CABINETRY.
16. REFER TO SHEETS G004 & G005 FOR ADDITIONAL WHEDE REQUIREMENTS, ACCESSIBILITY INFORMATION, AND REQUIRED BLOCKING IN WALLS FOR FUTURE GRAB BAR INSTALLATIONS.
17. SEE OVERALL PLANS FOR UNIT DEMISING WALL LOCATIONS AND WALL TAGS.
18. ALL UNIT-TO-UNIT DEMISING WALLS TO BE Q02 UNLESS OTHERWISE NOTED; UNIT-TO-CORRIDOR DEMISING WALLS TO BE Q02 UNLESS OTHERWISE NOTED; UNIT-TO-CORRIDOR DEMISING WALLS TO BE Q02 UNLESS OTHERWISE NOTED; SEE SHEET A600.
19. PROVIDE ARTIFICIAL LIGHTING AND MECHANICAL VENTILATION AT ALL INBOARD BEDROOMS AS REQUIRED BY CODE.
20. REFER TO STRUCTURAL DRAWINGS FOR BEARING WALL LOCATIONS.
21. PROVIDE SINGLE-LEVER FAUCETS IN BATHS AND KITCHENS.



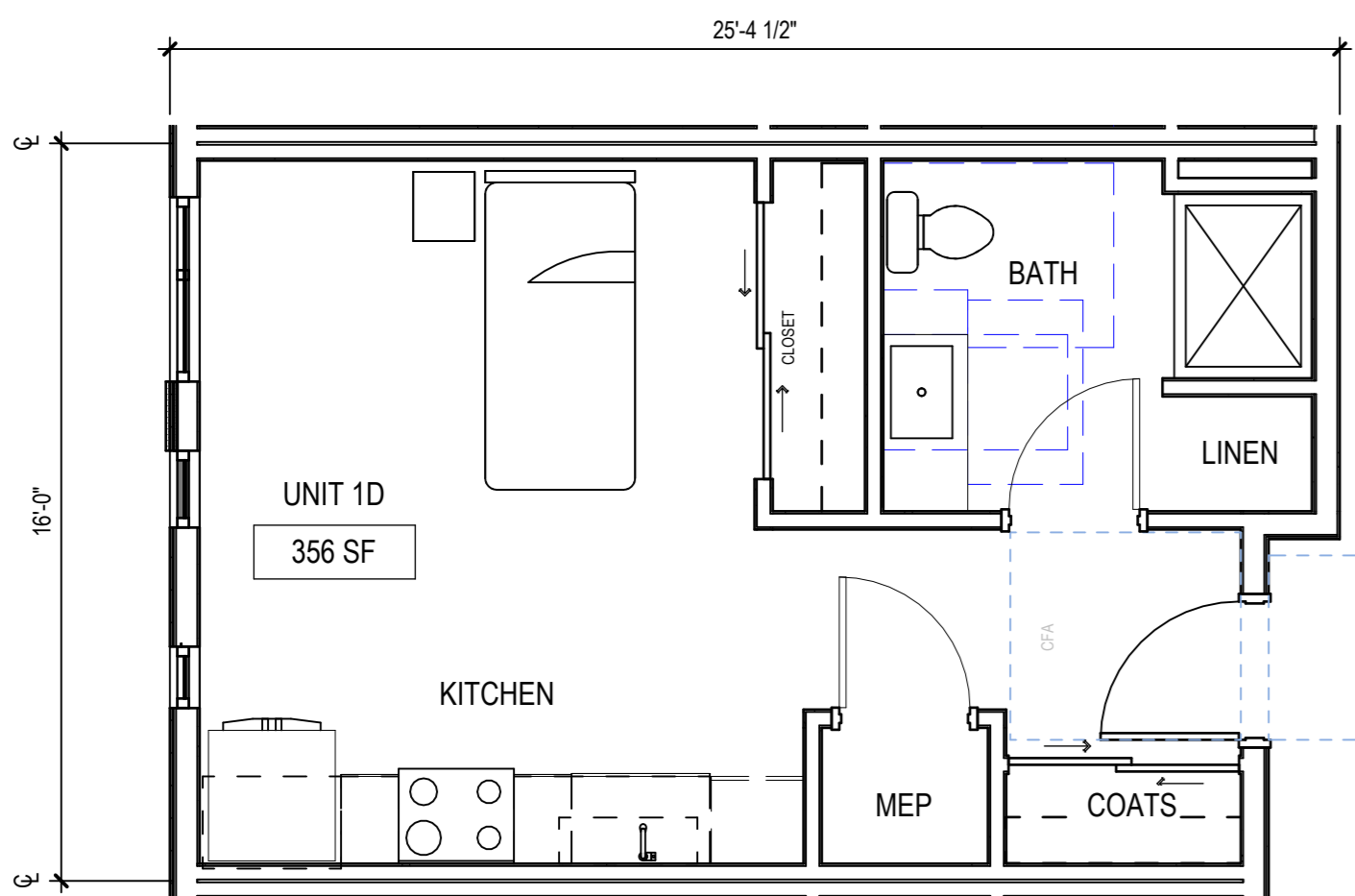
2 EFFICIENCY UNIT 1A  
1/4" = 1'-0"



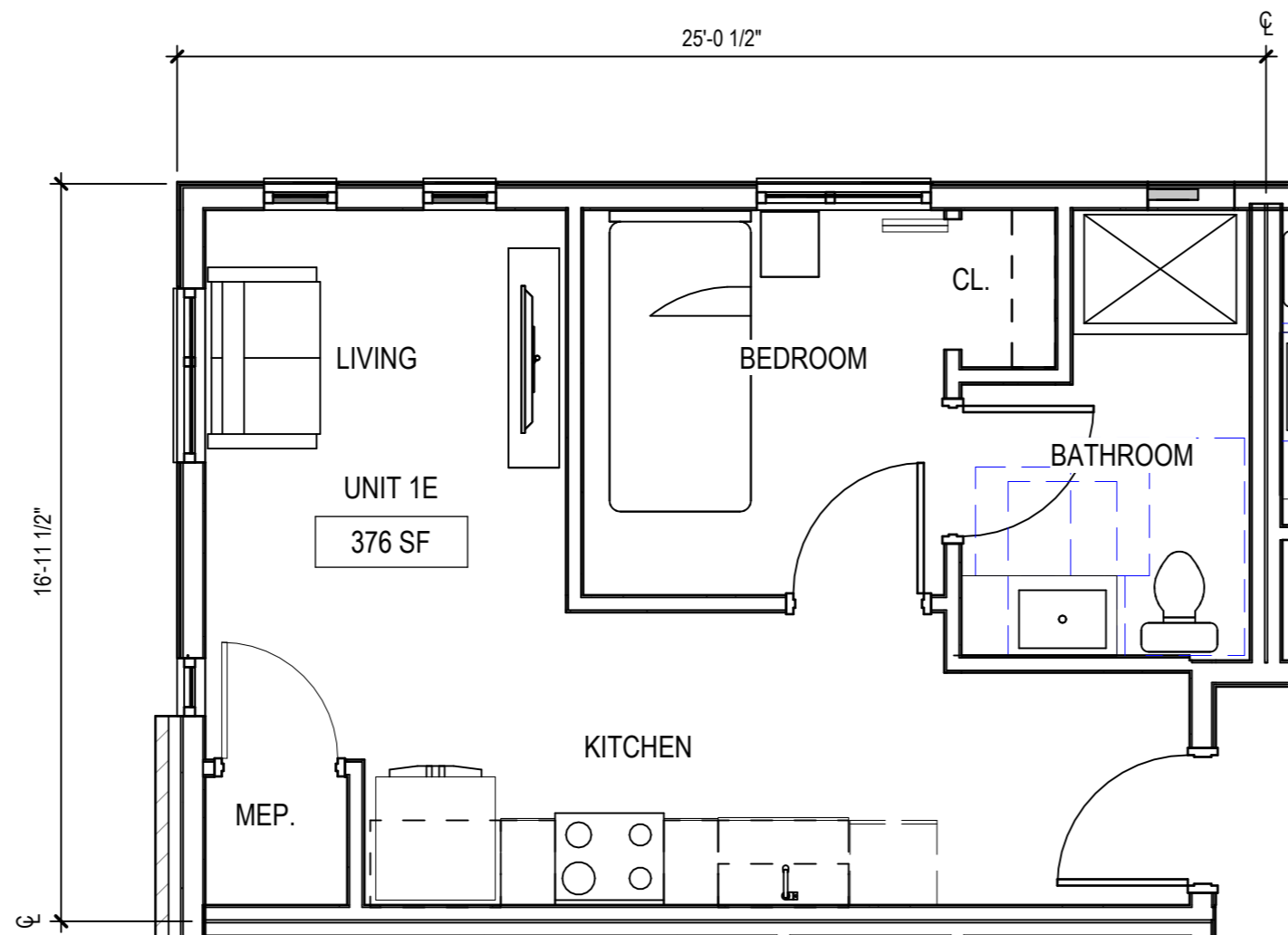
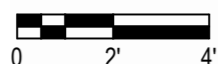
4 1-BED UNIT 1B  
1/4" = 1'-0"



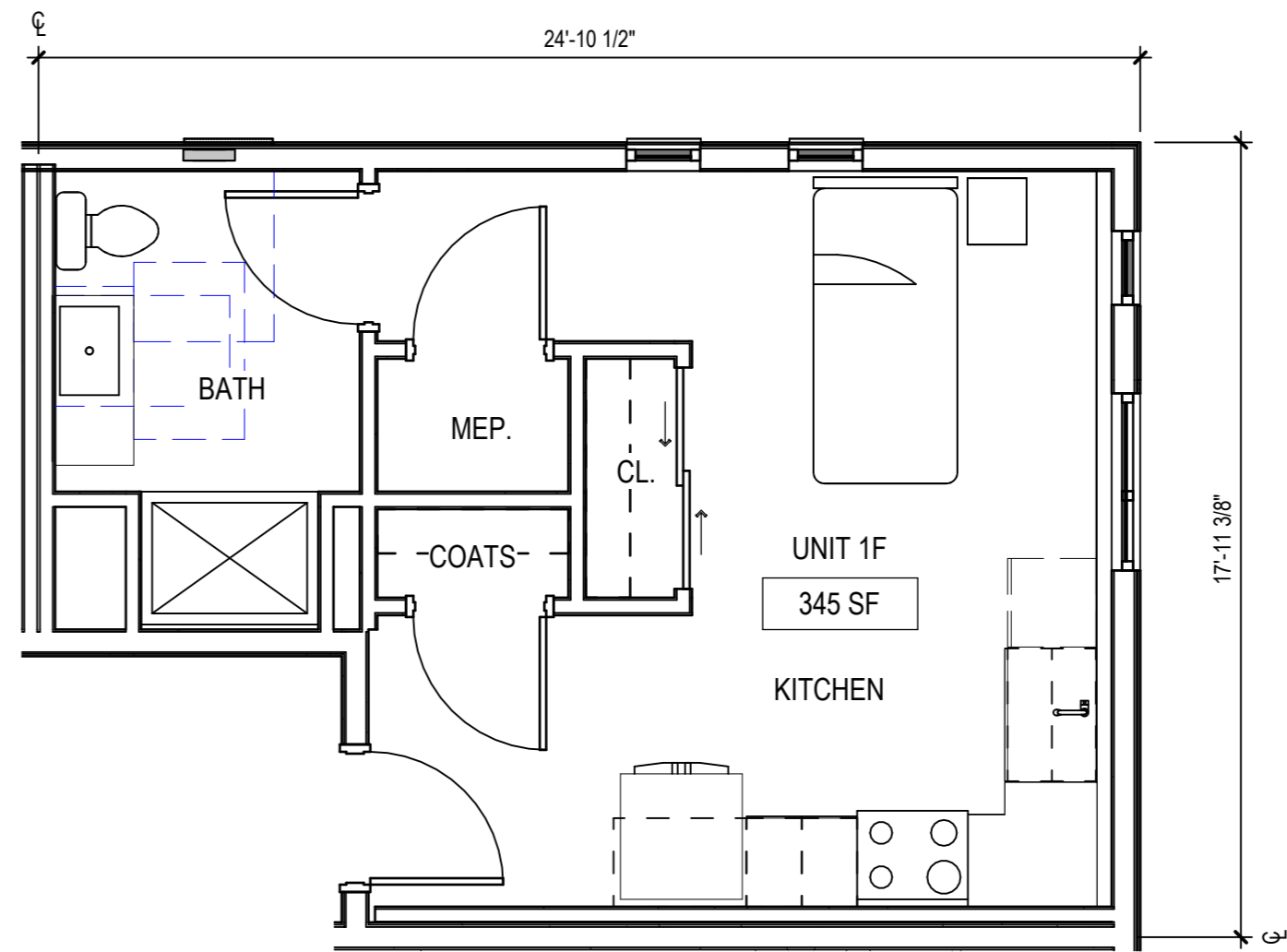
5 1-BED UNIT 1C  
1/4" = 1'-0"



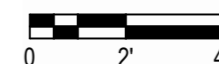
3 EFFICIENCY UNIT 1D  
1/4" = 1'-0"



6 1-BED UNIT 1E  
1/4" = 1'-0"



1 EFFICIENCY UNIT 1F  
1/4" = 1'-0"



ISSUED FOR:

LAND USE APPLICATION

05/27/2025

REVISIONS:

NO.	DESCRIPTION	DATE
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LANDUSE APPLICATION  
NOT FOR CONSTRUCTION

DRAWN BY:

<AUTHOR>

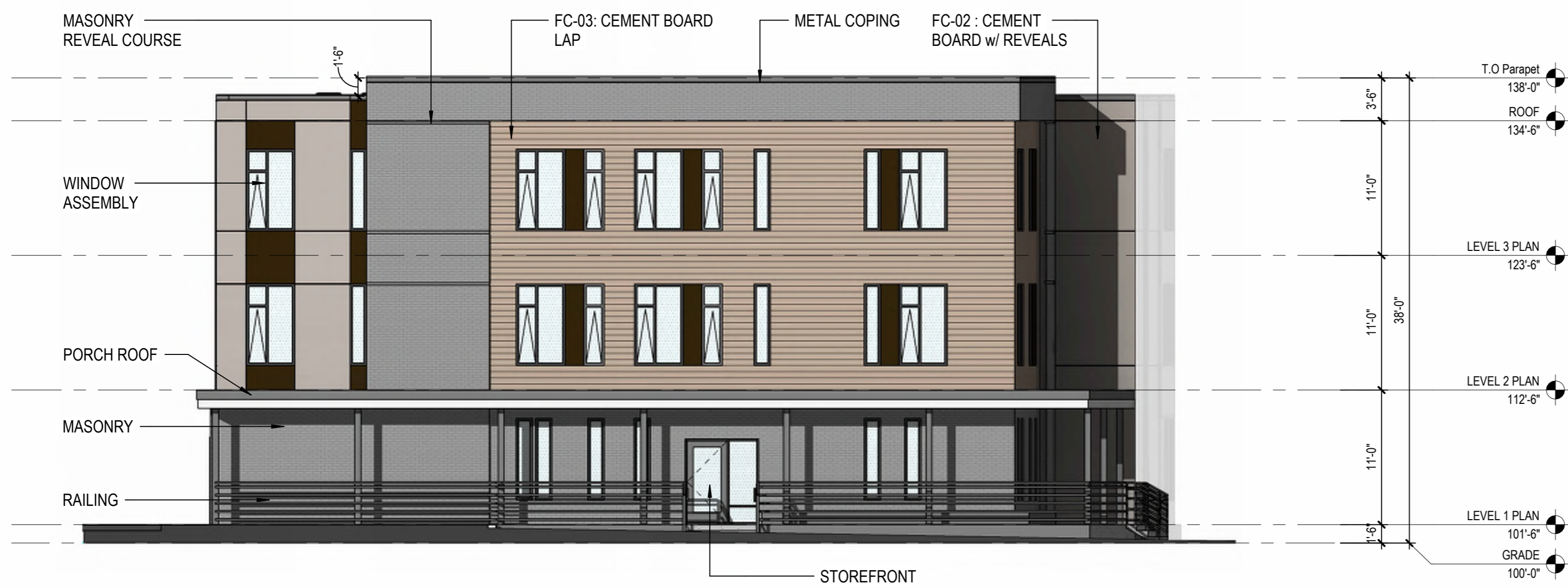
CHECKED BY:

<CHECKER>

ENLARGED UNIT PLAN

GENERAL NOTES - BUILDING ELEVATION

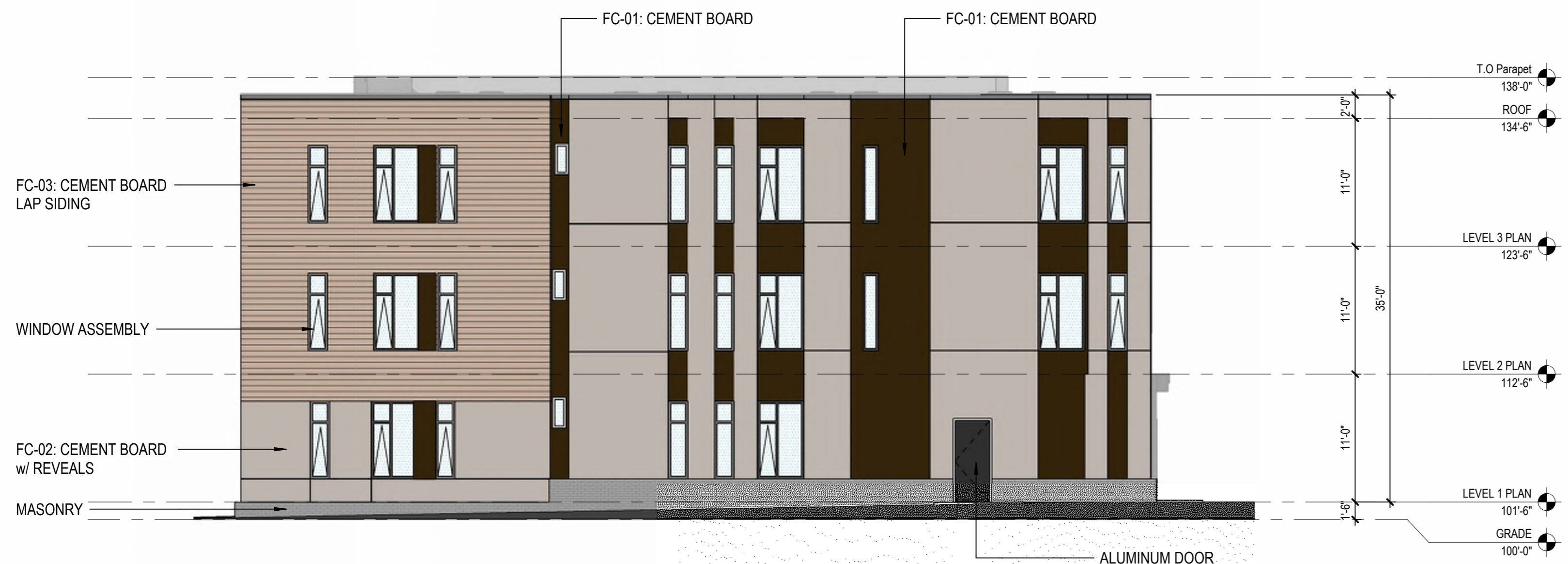
- MASONRY BLEND  
1. BASIS OF DESIGN: xxx-TEXTURE xxx  
COLOR xxx  
COLOR xxx
- FC-01: FIBER CEMENT BOARD WALL PANEL  
COLOR: RICH ESPRESSO  
BASIS OF DESIGN: JAMES HARDIE, FLAT PANEL SIDING, SMOOTH FINISH  
NOTE: JOINTS ARE TO BE TAMLIN EXTREME TRIM, EXTRUDED ALUMINUM REVEAL TRIM PROFILE, PAINTED TO MATCH PANEL COLOR.
- FC-02: FIBER CEMENT BOARD WALL PANEL  
COLOR: COBBLE STONE  
BASIS OF DESIGN: JAMES HARDIE, FLAT PANEL SIDING, SMOOTH FINISH  
NOTE: JOINTS ARE TO BE TAMLIN EXTREME TRIM, EXTRUDED ALUMINUM REVEAL TRIM PROFILE, PAINTED TO MATCH PANEL COLOR.
- FC-03: FIBER CEMENT BOARD WALL PANEL  
COLOR: XXXX  
BASIS OF DESIGN: JAMES HARDIE, FLAT PANEL SIDING, SMOOTH FINISH
- NO SINGLE WINDOW PANE LARGER THAN 50 sq ft. - NO BIRDSAFE GLASS REQUIREMENT



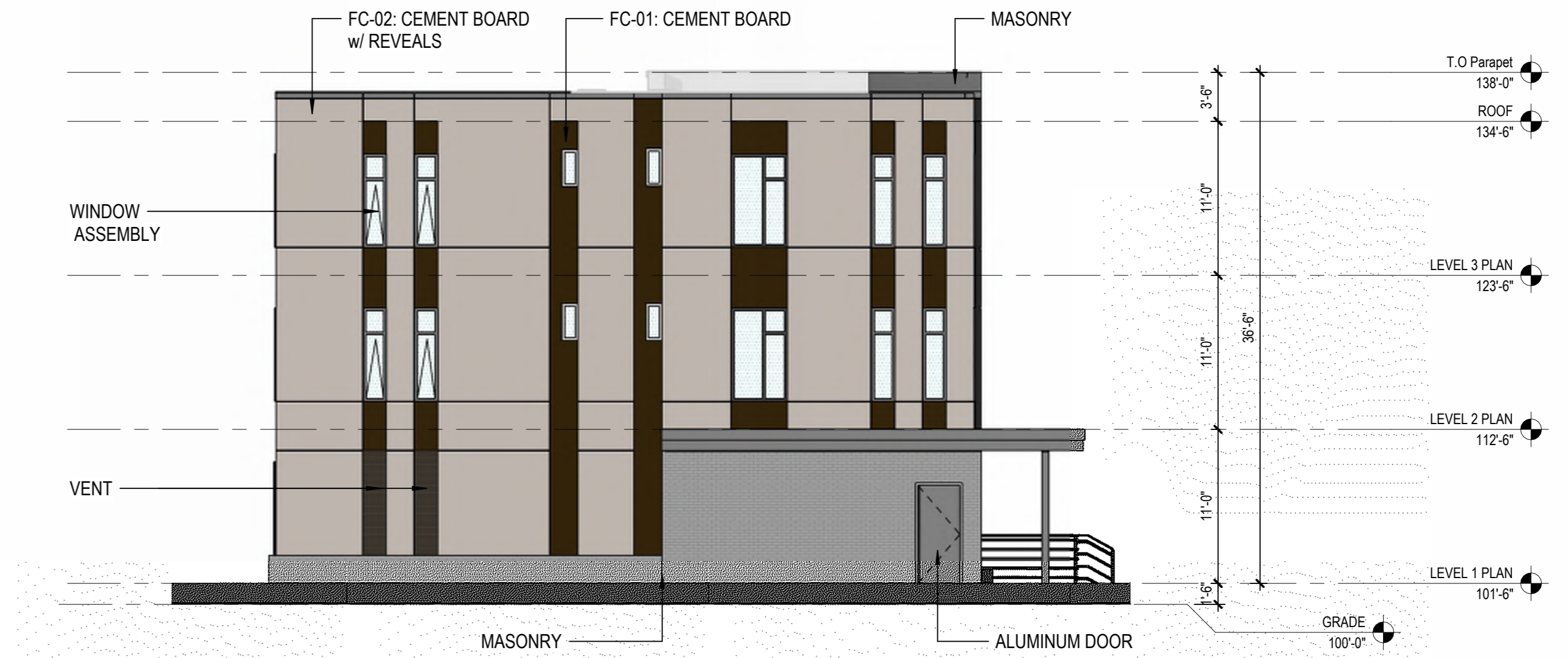
1 West Elevation  
1/8" = 1'-0"



2 South Elevation  
1/8" = 1'-0"



3 East Elevation  
1/8" = 1'-0"



4 North Elevation  
1/8" = 1'-0"

S3 HOUSING LLC.  
16 Unit Apartment Building

1802 ROTH STREET,  
MADISON WI. 53704

HOUSING INITIATIVES  
1110 Ruskin St, Madison, WI 53704

PROJECT NUMBER: 253833

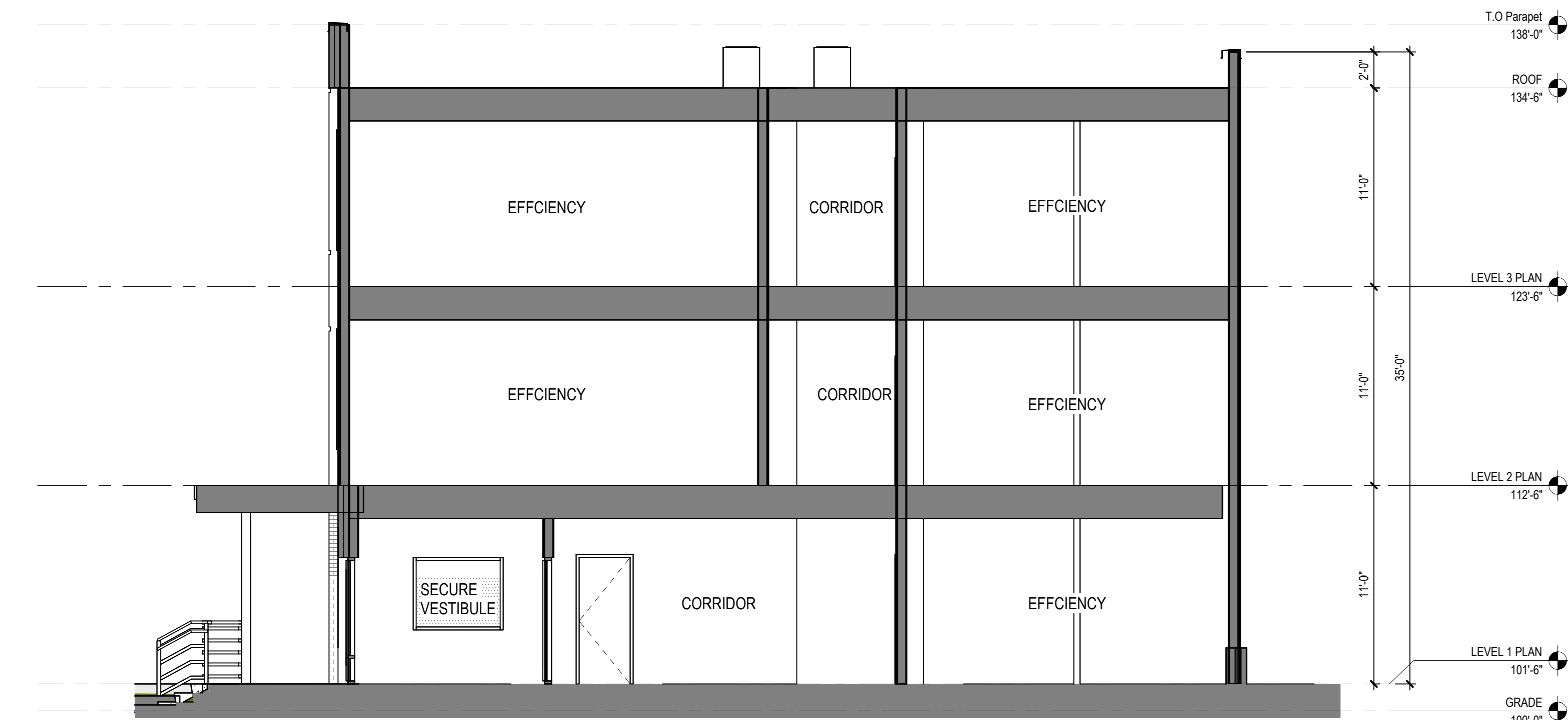
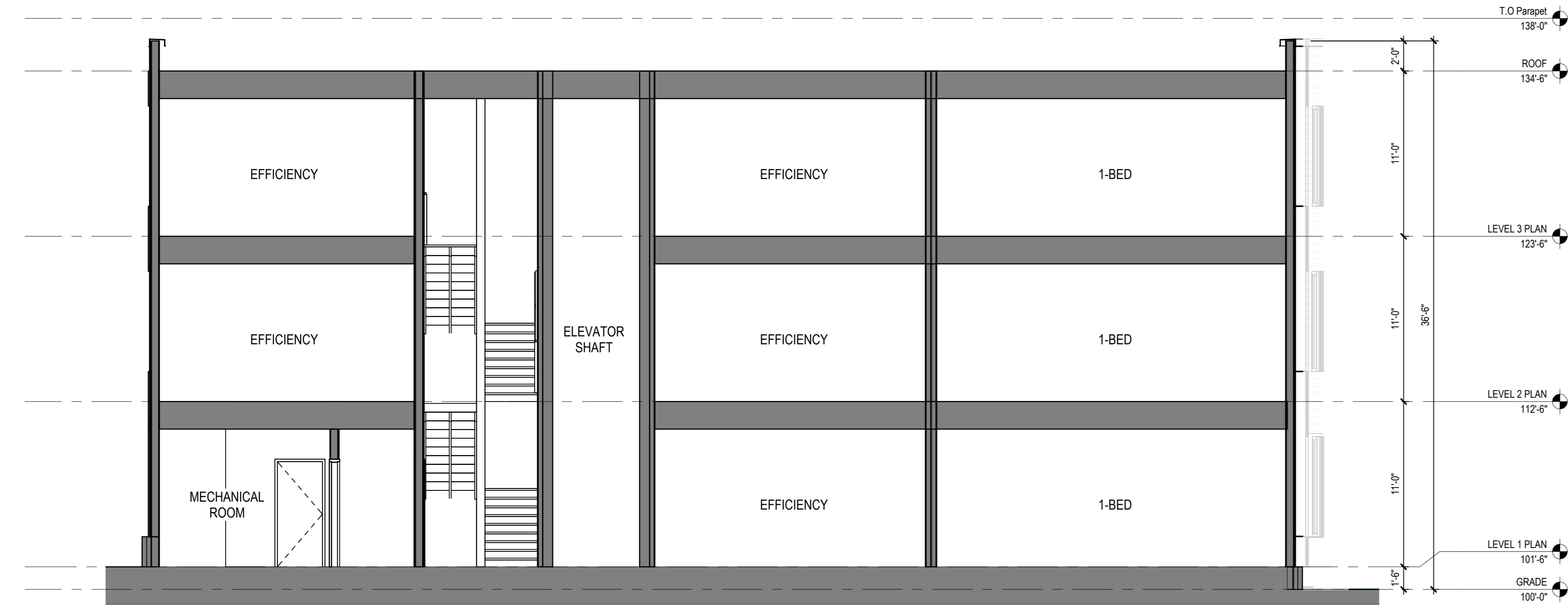
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ELEVATION

A401



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BUILDING SECTION



1 MATERIAL PALETTE



2 POTENTIAL MURAL LOCATION  
*SHOWN MURAL IS A VISUAL PLACEHOLDER