


CITY OF MADISON
INTER-DEPARTMENTAL
CORRESPONDENCE

DATE: January 18, 2006

TO: Urban Design Commission

FROM: Bill Fruhling, Principal Planner, Planning Unit  02779

SUBJECT: Park Street Urban Design District Ordinance

Attached is a copy of the draft ordinance creating the Park Street Urban Design District (UDD No. 7) and establishing requirements and guidelines for the District. As those of you who were on the Commission at the time may recall, these requirements are reflective of the concepts and ordinance language proposed in a plan entitled Design Guidelines for Private Property Improvements and Public Streetscape Design, that was unanimously recommended for adoption by the UDC in 2004, and subsequently approved by the Common Council. I have also attached a copy of the resolution adopting that document.

This item is on the agenda for January 25 for an informational presentation to provide an opportunity to learn about and discuss the ordinance prior to being formally introduced and scheduled for a public hearing before the UDC. We are also working to schedule a meeting with business and property owners within the proposed District prior to the ordinance's introduction.

Thank you.

35819

AGENDA ITEM # _____

Copy Mailed To Alderperson _____

City of Madison, Wisconsin

A SUBSTITUTE RESOLUTION _____

Adopting the *Design Guidelines for Private Property Improvements and Public Streetscape Design* document for the Park Street corridor, and directing staff to prepare the necessary ordinances to create an Urban Design District for this corridor.

Presented February 16, 2004

Referred Urban Design Commission, Board of Estimates, Plan Commission,

Pedestrian/Bicycle/Motor Vehicle Commission,

Board of Public Works

Referred _____

Reported Back _____

Adopted _____ POF _____

Rules Suspended _____

Public Hearing _____

Drafted By: William A. Fruhling, AICP
Principal Planner, Planning Unit

Date: January 14, 2004

Fiscal Note: It is anticipated that any increase in Urban Design Commission cases can be absorbed by existing staff. Implementation of certain plan recommendations may have future budgetary impacts needing Common Council approval.

Sponsors: Ald. Matt Sloan (District 13)
Ald. Tim Bruer (District 14)

APPROVAL OF FISCAL NOTE IS NEEDED
BY THE COMPTROLLER'S OFFICE
Approved By _____

ID NUMBER _____

RESOLUTION NUMBER _____

WHEREAS, the Park Street corridor serves as the "Main Street" for Madison's Southside, and is a prominent entry into the City of Madison-- specifically to the downtown and University of Wisconsin-Madison; and

WHEREAS, the functional and aesthetic importance of Park Street is recognized in *Park Street Revitalization: Possibilities to Reality* plan and the *South Madison Neighborhood Plan*, both of which recommend that guidelines be established for the corridor to ensure high quality design and long-term economic vitality for new and existing development; and

WHEREAS, design guidelines, implemented through the creation of urban design districts, provide an effective mechanism to enhance the aesthetic quality of a district on a project-by-project basis; and

WHEREAS, Park Street Partners received funding from the City of Madison Neighborhood Planning Grant Program, raised the required matching funds from Southside businesses, facilitated faculty and student resources from the University of Wisconsin, and retained Schreiber/Anderson Associates to develop design guidelines; and

WHEREAS, the aforementioned groups worked closely with the Southside businesses and residents and the City to develop a comprehensive set of design guidelines as described in *Design Guidelines for Private Property Improvements and Public Streetscape Design*.

NOW, THEREFORE, BE IT RESOLVED, that the *Design Guidelines for Private Property Improvements and Public Streetscape Design* document for the Park Street corridor is adopted, and staff is hereby directed to prepare the necessary ordinances to create an Urban Design District for this corridor.

35819

Ordinance Establishing the Park Street Urban Design District

22.DEC.05

Creating Sec. 33.02(14)

(14) Urban Design District No. 7

- (a) Statement of Purpose. Urban Design District No. 7 is hereby established to improve the appearance and function of Park Street. Park Street is a major gateway corridor to Madison's Downtown and the University of Wisconsin-Madison, and is a critical street for the vitality of adjoining neighborhoods. The purpose of these design requirements and guidelines is to provide clear direction for how property owners can make improvements to their properties to collectively improve the visual character and safety of Park Street. When applied, they will ensure against fragmented or incompatible development and will help prevent the negative visual and functional impacts of uncoordinated design decisions. These requirements and guidelines are intended to preserve and enhance the property values in the District, and avoid substantial depreciation of the property values and help to ensure long-term economic vitality. The goal is not to create a uniform "style" or character for the street, but rather to allow Park Street to evolve as a distinctive place that builds on the strengths of its culturally diverse businesses and neighborhoods.
- (b) Property Included in the District. The District shall include all properties having any frontage on South Park Street between the West Beltline Highway on the south and Regent Street on the north. The District shall also include properties which, due to their size and/or location, are likely to have a significant visual impact on the Park Street Corridor should they be redeveloped. Any properties within the boundaries of the District which are not currently within the corporate limits of the City of Madison shall be automatically included in the District upon annexation to the City. If any portion of a zoning lot is in the District, the entire lot is within the District. A map of the District is available from the Department of Planning and Development.
- (c) Design Review Required. All development in the District (including, but not limited to, new buildings or structures, additions to existing buildings or structures, major exterior alterations of existing buildings or structures, painting of existing unpainted exterior surfaces, street graphics, and new parking facilities or alterations to existing parking facilities), except residential buildings containing four (4) or fewer dwelling units, shall require approval of the Urban Design Commission and shall be designed, erected, and maintained in compliance with this ordinance, all applicable federal and other state laws, and the Building Code, Zoning Ordinance and other applicable codes of the City of Madison not in conflict with this ordinance. The Planning Unit Director, or his/her designee, may approve minor alterations to existing and/or approved buildings or structures and site. The Planning Unit Director, or his/her designee, may also approve the design of street graphics that are permissible under the Street Graphics Ordinance. Approval of the Urban Design Commission under this subsection shall not be required for an awning unless it is part of other development requiring

approval under this subsection. The applicable regulations of other codes shall continue to apply with full force and effect to all properties in the District. However, if this ordinance conflicts with other City regulations, the regulations which are more restrictive or which impose higher standards or requirements shall govern.

- (d) Basis for Design Review. In reviewing plans for development in the District, the Urban Design Commission shall consider the following requirements and guidelines as may be appropriate. The development shall meet the requirements and conform as much as possible to the guidelines. Both the requirements and guidelines apply to new construction, renovations, additions, and exterior alterations unless stated otherwise for a specific item. The overall design of each development shall be of high quality. Further explanation of the concepts and rationale conveyed through these guidelines is provided in *Design Guidelines for Private Property Improvements and Public Streetscape Design for the Park Street Corridor* (adopted by Res. 61397, 3-30-04).

1. Building Height.

a. Requirements

- i. New buildings shall be at least two (2) stories in height, except as provided in Par. 10, 11, 12 or 13 or in the guidelines below.
- ii. New buildings in excess of three (3) stories shall incorporate a front façade stepback of at least fifteen (15) feet from the building face at the third floor.

b. Guidelines

- i. New buildings should generally be limited to four (4) stories in height. However, building height bonuses of up to two (2) additional floors may be allowed depending on the quality of the design, the affect of the development on the adjoining neighborhood, and the contribution of the project to: the use mix and activity in the vicinity, and the character of the street.
- ii. New infill buildings should not vary by more than one (1) story from the average building height in the block when that block exhibits a concentration of existing buildings and a well-defined blockface.
- iii. Greater flexibility for building height and stepbacks may be allowed as part of a coordinated redevelopment project approved by the City as described in Par. 11, 12, 13 or 14.
- iv. Additions to existing buildings are expected to comply with the applicable height requirements unless the applicant can demonstrate that the siting or layout of the existing building would pose hardships for its functional relationship with the new addition, in which case the Urban Design Commission may waive said requirements.
- v. In special cases, such as gas stations, the Urban Design Commission may reduce the minimum building height requirement provided the buildings incorporate elements such as extended parapet or tower features to convey the appearance of a taller building. Such elements shall be substantially integrated into the design of the building so they do not read as false facades.

2. Building Setbacks and Orientation.

a. Requirements

- i. New buildings shall have a setback between one (1) to ten (10) feet from the front property line. Where new buildings are designed for existing blockfaces, the building setback shall be consistent with adjoining buildings but shall not exceed ten (10) feet. However, the Urban Design Commission may allow buildings to be set back greater than ten (10) feet to allow for the development of usable public open spaces, such as pedestrian plazas.
- ii. In special cases, such as gas stations, setbacks can exceed ten (10) feet with provisions for walkways and landscaping that make these uses more attractive and inviting.

b. Guidelines

- i. The front yard setback should be designed to provide for amenities that will enhance the visual and pedestrian character of the street.
- ii. In areas with minimal sidewalk/terrace width (eight feet or less), building setback areas should include additional pavement to expand the sidewalk zone to a width of at least eight (8) feet.
- iii. Walkways should be provided to connect the building entrance to the public sidewalk.
- iv. The front facade of the building and the primary entrance should face the primary street. If the public entrance is allowed on the side of the building, it should be positioned close to the primary street and preferably as a corner feature of the building.
- v. Additions to existing buildings should help bring the building closer to the street and minimize any “gap” in the street wall.

3. Parking and Service Areas.

a. Requirements

- i. Off-street parking facilities for new buildings shall be located behind or on the sides of the building.
- ii. At least one (1) tree island, sized and landscaped pursuant to the Zoning Ordinance, shall be provided per twelve (12) parking spaces provided. This requirement is in addition to any other landscaping requirements of the Zoning Ordinance.
- iii. All trash areas shall be screened from public view.

b. Guidelines

- i. For existing properties, where the site area permits, parking in the front should be relocated to the side and/or rear of the building. When this is not possible, walkways, landscaping, architectural features, and lighting should be provided to make these areas more attractive and inviting. Decorative fences, walls and/or landscaped edges should screen front parking areas from the public sidewalk. Screening should not exceed 3’6” in height.
- ii. All parking areas should be well landscaped and appropriately lighted.
- iii. All parking areas should include walkways to allow safe pedestrian access to the building entrance.

- iv. Shared parking areas are encouraged. Whenever possible, adjoining parking lots should be linked to provide internal traffic circulation.
- v. Driveways along Park Street should be minimized to improve traffic flow and reduce pedestrian conflicts.
- vi. Pedestrian areas and customer parking areas should be separated from loading, service, and drive through areas.
- vii. If possible, trash areas should be located inside buildings.
- viii. New parking ramps fronting on Park Street should include ground-floor commercial uses with attractive commercial facade design. The facade design for the upper stories should obscure the parking ramp and present an attractive building face for Park Street. The design of parking ramps should also complement the quality and design of the buildings they serve.

4. Landscaping and Open Space.

a. Requirements

- i. Screen fences and/or landscaped buffers shall be provided at property edges. Where a commercial property adjoins residential properties, this separation shall be provided pursuant to the Zoning Ordinance.

b. Guidelines

- i. Property owners are encouraged to provide well-designed landscaped outdoor spaces for the use and enjoyment of employees and customers.
- ii. Landscaping and fencing should be designed to complement the character of the building and provide a pleasing relationship with adjoining properties and the public sidewalk.
- iii. The use of attractive landscaping is encouraged to establish continuity between buildings and to define the blockface where there are no buildings.
- iv. The use of rain gardens and bio-retention basins to collect runoff and filter pollutants is encouraged, where practical.
- v. Landscape islands, open spaces and porous pavements should be provided, where practical, for additional stormwater infiltration.

5. Site Lighting and Furnishings.

a. Requirements

- i. Cut-off light fixtures shall be used to illuminate the site.

b. Guidelines

- i. Pedestrian use areas should be adequately, but not excessively lit. Low-level building and landscape accent lighting is encouraged, where appropriate.
- ii. Lighting and site furnishings (benches, trash receptacles, bicycle racks, etc.) should be designed to complement the character of the building and provide a pleasing relationship with adjoining properties and the public sidewalk.
- iii. Bicycle storage facilities should be located near the building entrance.
- iv. Decorative, colored paving is encouraged for walkways and outdoor use areas.

6. Building Massing and Articulation.

a. Requirements

- i. All visible sides of the building shall be designed with details that complement the front facade. Side facades that are visible from the primary street shall receive complementary design attention.
- ii. Blank building walls with little detail or variety along primary facades shall be avoided. Improvements to these buildings shall include details at the street level to create a more comfortable pedestrian scale and character.
- iii. Architectural details at the ground floor shall be provided to enhance the pedestrian character of the street. Details shall include window and door trim, recessed entries, awnings, and/or other features.
- iv. Mechanical equipment shall be screened from view by using screen designs that are architecturally integrated with the building design.

b. Guidelines

- i. "Green" building design that promotes energy efficiency is encouraged.
- ii. For large buildings, variation to the building face design should be provided through the use of materials and color, and/or by dividing the building into bays to break up large facades to create pedestrian interest at the street level. This is particularly important for existing large industrial and commercial buildings on Park Street.
- iii. Building facades should be designed to create a visual distinction between the upper and lower floors of the building.
- iv. Flat roofs are preferred for new mixed-use and commercial buildings.
- v. A positive visual termination at the top of the building with decorative cornices or parapets should be provided.
- vi. Designs for buildings that adjoin historic buildings should complement the proportion, scale and architectural details of the historic building.
- vii. Buildings should be designed as products of their own time. Copying historic appearance and details is discouraged.
- viii. Franchise businesses are encouraged to modify their corporate designs, if necessary, to fit the Park Street character.
- ix. Where possible, existing one-story buildings should be renovated with extended facades and parapets to increase building height and provide a more pleasing scale for Park Street.
- x. New buildings and additions should complement the character of adjoining buildings in the blockface.
- xi. Creative architectural designs and details are encouraged so long as designs do not conflict or draw attention away from other buildings in the block.
- xii. New corner buildings should be located near the sidewalk edge and should define the street intersection with distinctive architectural features such as towers, rounded walls, recessed entries or other design features.

7. Materials and Colors.

a. Requirements

- i. Exterior materials shall be durable, high-quality materials and appropriate for external use.

b. Guidelines

- i. Brick, stone and terra cotta are preferred primary materials for new buildings or additions.
- ii. The use of brick veneer or other “faux” sidings is discouraged.
- iii. Color choice should complement the style and materials of the building’s facade and provide a pleasing relationship with adjoining buildings.
- iv. Property owners are encouraged to use a three-color paint scheme with a “base” color on the majority of the building surface, “trim” colors used on building features such as window and door trims and cornices and “accent” colors on signs, awnings and other architectural details.
- v. Painting, covering or removal of natural brick and stone is discouraged, but staining may be acceptable.

8. Windows and Entrances.

a. Requirements

- i. The ground floors of commercial retail buildings shall have at least sixty (60) percent of the street wall area devoted to windows to enhance the pedestrian character of the primary street.
- ii. Office buildings and other non-retail buildings should have at least forty (40) percent of the street wall devoted to windows.
- iii. Windows on the ground floor shall be transparent, and not be darkly tinted, colored or have a mirrored finish.

b. Guidelines

- i. Building entrances should be designed as the focal point of the front facade.
- ii. Entrances to new buildings or additions located close to the sidewalk should include recessed entries to allow for pedestrian movement.

9. Signage.

a. Guidelines

- i. Preferred sign types include building mounted signs, window signs, projecting signs, and awning signs.
- ii. Signs should be simple and easy to read.
- iii. Sign colors should relate to and complement the primary colors of the building facade.
- iv. Sign design and placement should fit the character of the building and not obscure architectural details.
- v. Signage should generally be centered within the prescribed signage area of the building.
- vi. Plastic box signs are highly discouraged.
- vii. Opaque backgrounds and white or light colored letters are preferred for backlit signs.
- viii. Individually mounted backlit letters are an encouraged form of signage.
- ix. The use of small, well-designed building-mounted light fixtures is a preferred method of illuminating signage.

- x. Freestanding signs should be attractively designed. Signs should be coordinated with adjoining properties and public street signage to avoid visual clutter.

10. Badger Road Area

The Badger Road Area has been identified in the *South Madison Neighborhood Plan*, the *Park Street Possibilities to Reality Report*, and the *Design Guidelines for Private Property Improvements and Public Streetscape Design for the Park Street Corridor* as possessing numerous large parcels that represent potential redevelopment opportunities that would improve the use mix, character, density and activity within the District. It continues to be the focus of more detailed and on-going planning efforts which will further articulate important site-specific design considerations for these unique parcels. Design recommendations included in subsequent adopted plans for this area shall take precedence over requirements listed below, which will otherwise govern. If not specifically preempted by the plans or this Section, the provisions of this Chapter shall apply.

a. Requirements

- i. Substantial portions of new buildings shall be at least two (2) stories in height. In special cases, such as gas stations, the Urban Design Commission may reduce the minimum building height requirement provided the building incorporate elements such as extended parapet or tower features to convey the appearance of a taller building. Such elements shall be substantially integrated into the design of the building so they do not read as false facades.
- ii. The front façade of buildings shall be stepped back at least fifteen (15) feet from the building face at the third floor.

b. Guidelines

- i. Redevelopment sites should incorporate high quality architecture and site design.
- ii. Buildings should be positioned to serve as attractive visual termination points for streets leading into the development from Park Street and Badger Road.
- iii. Buildings should be positioned to develop an attractive blockface along Park Street and internal streets. Buildings should be located with minimal setbacks at corners to define intersections.
- iv. Streetscaping and traffic calming features should be incorporated to enhance the pedestrian character of the development.
- v. Shared parking is encouraged with a preference for enclosed parking integrated as part of the mixed-use building design. Surface parking is preferred first behind buildings and then on the side of the building.
- vi. Developments are encouraged to incorporate well-designed open spaces for customer, employee, resident, and community use.

11. Wingra Creek Area

The Wingra Creek Area has been identified in the *South Madison Neighborhood Plan*, the *Park Street Possibilities to Reality Report*, and the *Design Guidelines for Private Property Improvements and Public Streetscape Design for the Park Street Corridor* as possessing numerous large parcels that represent potential redevelopment opportunities

that would improve the use mix, character, density and activity within the District. It continues to be the focus of more detailed and on-going planning efforts which will further articulate important site-specific design considerations for these unique parcels. Design recommendations included in subsequent adopted plans for this area shall take precedence over requirements listed below, which will otherwise govern. If not specifically preempted by the plans or below, the provisions of this Chapter shall apply.

a. Requirements

- i. Buildings located away from Park Street may be built up to six (6) stories provided their placement is not detrimental to adjoining residential areas and they are consistent with existing zoning.
- ii. The front façade of buildings shall be stepped back at least fifteen (15) feet from the building face at the third floor, unless an irregularly shaped parcel makes this requirement unfeasible.

b. Guidelines

- i. Redevelopment sites should incorporate high quality architecture and site design.
- ii. Buildings should be positioned to serve as attractive visual termination points for streets leading into the development from Park Street.
- iii. Buildings should be positioned to develop an attractive blockface along Park Street and internal streets. Buildings should be located with minimal setbacks at corners to define intersections.
- iv. Where appropriate, internal streets should be included as part of the development plan to improve connections between Park Street and the adjoining neighborhoods.
- v. Developments should promote narrow, “pedestrian friendly” streets with on-street parking.
- vi. Streetscaping and traffic calming features should be incorporated to enhance the pedestrian character of the development.
- vii. Shared parking is encouraged with a preference for enclosed parking integrated as part of the mixed-use building design. Surface parking is preferred first behind buildings and then on the side of the building.
- viii. Developments are encouraged to incorporate well-designed open spaces for customer, employee, resident, and community use.

12. Meriter and St. Marys Hospital Campuses

St. Marys and Meriter Hospitals occupy sites that are essentially surrounded by residential neighborhoods and are unique because of their size, institutional use, and campus-type development pattern. Each hospital has, or will have, a master plan that has been, or will be, approved by the City. These approved master plans, as they may from time to time be modified or expanded, shall govern development/redevelopment activities on these sites, as a result this ordinance shall not govern these sites.

13. Multi-Tenant Commercial Buildings

Park Street has a number of multi-tenant commercial buildings or “strip centers” located primarily in the southern section of the corridor. The design of new centers and improvements to existing centers should follow the design guidelines presented in this

document. This section identifies design requirements and guidelines to address the special conditions that exist for these sites. If not specifically preempted by the provisions below, the provisions of this Chapter shall apply.

a. Requirements

- i. Substantial portions of buildings in new centers shall be at least two (2) stories in height.
- ii. New centers shall front directly on the primary street with parking located at the rear or on the side of the building(s).

Guidelines

- i. New centers should include residential or office uses on the upper floors.
- ii. Rear and side facades should be designed to blend with the design of the front facade, with complementary colors and materials.

14. Restoration/Preservation Activities

Several buildings, located primarily at the north end of Park Street, are older commercial structures which have historic value and interest. The ground floors of many of these structures have been altered over time while the upper stories have generally retained their historic appearance. When considering building improvements, property owners of older commercial structures are encouraged to restore the original character of the building. This section identifies additional design requirements and guidelines to address the special conditions that exist for these sites.

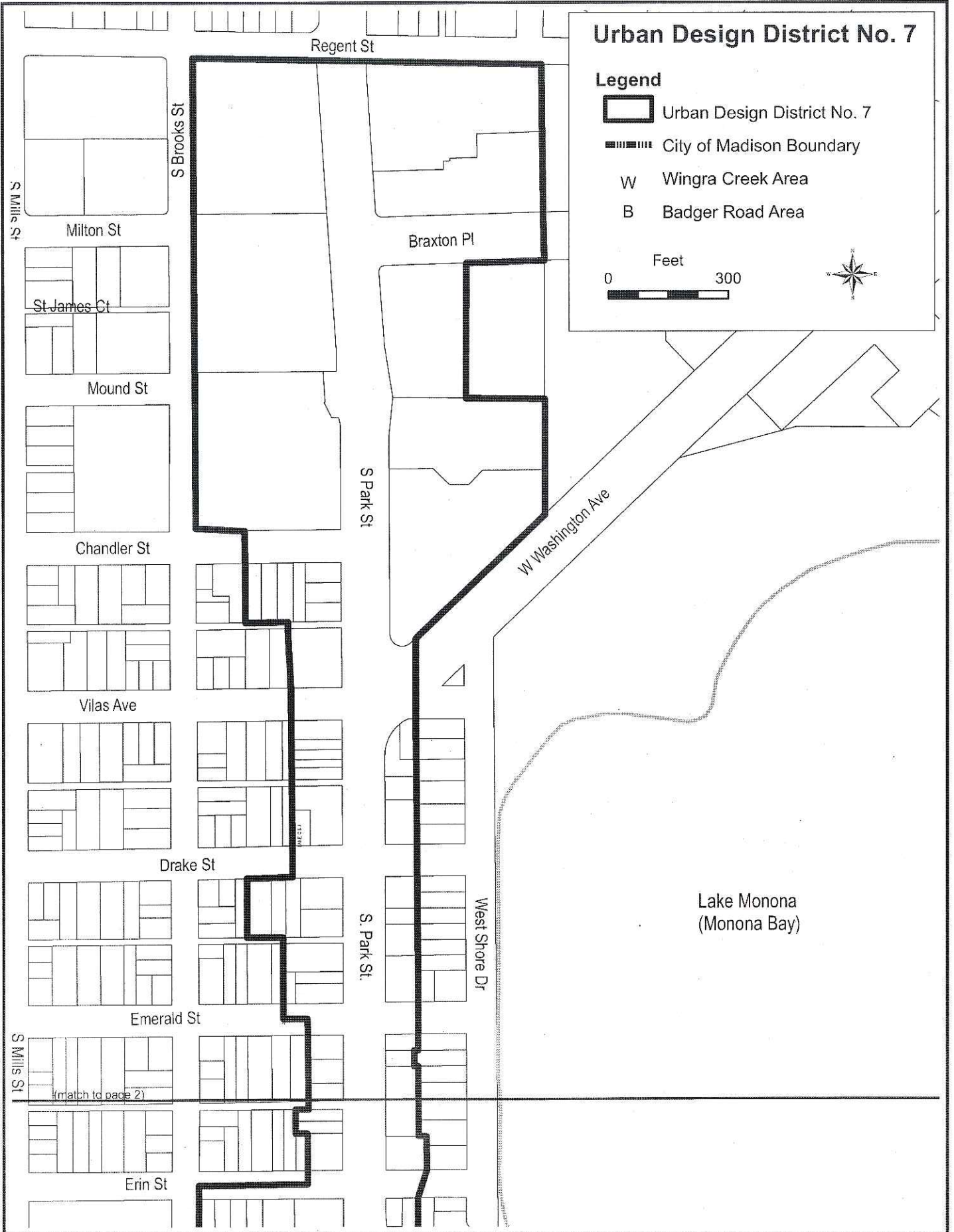
a. Guidelines

- i. The distinguishing features of the original building should be preserved. The removal or alteration of historic materials or distinctive architecture features should be avoided, whenever possible.
- ii. Where practical, the original masonry should be restored and missing elements such as cornices, windows and storefronts that were part of the original building design should be reconstructed. If restoration is not feasible, new elements should be designed to compliment the character, materials and design of the original building.
- iii. Any inappropriate elements, signs, canopies, etc. that cover details and features of the original building should be removed.
- iv. Painting of natural brick or stone is strongly discouraged when those materials are in good condition.

Urban Design District No. 7

Legend

-  Urban Design District No. 7
-  City of Madison Boundary
- W Wingra Creek Area
- B Badger Road Area



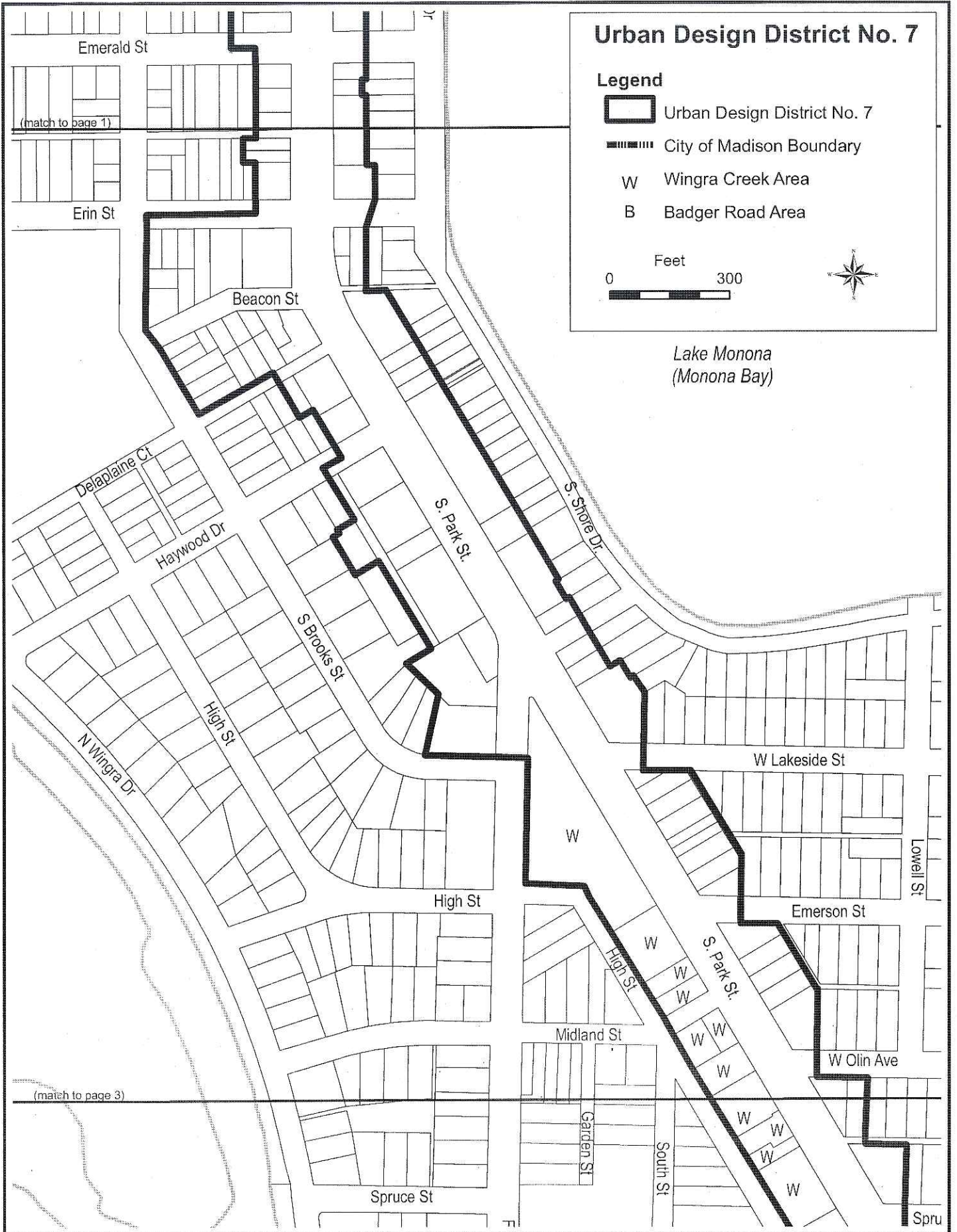
Urban Design District No. 7

Legend

-  Urban Design District No. 7
-  City of Madison Boundary
-  Wingra Creek Area
-  Badger Road Area



Lake Monona
(Monona Bay)



Urban Design District No. 7

Legend

-  Urban Design District No. 7
-  City of Madison Boundary
-  Wingra Creek Area
-  Badger Road Area

