APPLICATION FOR URBAN DESIGN COMMISSION REVIEW AND APPROVAL

AGENDA ITEM#	
Project #	

DATE SUBMITTED UDC MEETING DA	D: July 29, 2009 ATE: August 5, 2009	Action Requested Informational Presentation Initial Approval and/or Recommendation _X Final Approval and/or Recommendation							
PROJECT ADDRES	SS: 2 S. Bedford Street								
ALDERMANIC DI	STRICT: Michael Verveer- District #	<u></u>							
OWNER/DEVELO	PER (Partners and/or Principals)	ARCHITECT/DESIGNER/OR AGENT:							
Jim Meier/ Badge	r Bus	Knothe & Bruce Architects, LLC							
5501 Femrite Driv	ve	7601 University Avenue, Suite 201							
Madison, WI		Middleton, Wisconsin 53562							
CONTACT PERSON	: J. Randy Bruce/Knothe & Bruce Archi	itects, LLC							
Address:	7601 University Avenue, Suite 201								
	Middleton, Wisconsin 53562								
Phone:	608-836-3690								
Fax:	608-836-6934								
E-mail addres	s: <u>rbruce@knothebruce.com</u>								
TYPE OF PROJECT: (See Section A For:) X Planned Unit Development (PUD) — General Development Plan (GDP) X Specific Implementation Plan (SIP) — Planned Community Development (PCD) — General Development Plan (GDP) — Specific Implementation Plan (SIP) — Planned Residential Development (PRD) — New Construction or Exterior Remodeling in an Urban Design District* (A public hearing is required as well as a fee) — School, Public Building or Space (Fee may be required) — New Construction or Addition to or Remodeling of a Retail, Hotel, or Motel Building Exceeding 50,000 Sq.Ft. — Planned Commercial Site (See Section B for:)									
(See Section C for:)	 New Construction or Exterior Remodeling in C4 District (Fee required) (See Section C for:) R.P.S.M. Parking Variance (Fee required) 								
Street Graphics	Design Review* (Fee required) Variance* (Fee Required)								
Other*Public Hearing Required (Submission Deadline 3 Weeks in Advance of Meeting Date)									



July 29, 2009

Mr. Brad Murphy
Director of Planning
Department of Planning & Development
City of Madison
215 Martin Luther King Jr. Blvd
PO Box 2985
Madison, Wisconsin 53701

Re: Letter of Intent

Rezoning from M-1 to PUD-GDP-SIP

2 South Bedford Street Madison, Wisconsin

Dear Mr. Murphy:

The following is submitted together with the plans, application and zoning text for staff, Plan Commission and Common Council consideration of approval.

Organizational structure:

Owner: Jim Meier Architects, LLC

2 South Bedford, LLC 7601 University Avenue, Ste 201 5501 Femrite Drive Middleton, WI 53562

Madison, WI 53718 608-836-3690 608-255-1511 608-836-6934 fax

jim@meiertruckservice.com

Contact: J. Randy Bruce
rbruce@knothebruce.com

Engineer: I & S Group, Inc.

2690 Research Park Dr., Suite H Landscape Ken Saiki Design

Madison, WI 53711 Design: 303 S. Paterson St. 608-442-9500 Suite 1

608-442-9501 fax

Contact: Dave Glusick

dave glusick@is gracom

Contact: Abbia Mailian

dave.glusick@is-grp.com

Contact: Abbie Moilien ksaiki@ksd-la.com

Letter of Intent The Depot July 29, 2009 Page 2 of 4

Introduction:

The proposed site is located on South Bedford Street between West Washington Avenue and West Main Street. Much of this block was historically an industrial warehousing district and is now home to a mix of uses including the bus depot, storage buildings and office uses. The West Washington Avenue and Bedford Street corner is a small retail node with businesses such as Williamson Street Bikes, Indocara Interiors, Electric Earth Café and the Heartland Credit Union.

For approximately the past 30 years the site has been operated as the Badger Bus depot. However, the structural and economic changes in the inter-city bus transit industry have prompted the owners to evaluate development options for this property. The owners, with input from the alderperson and neighborhood, have been working on a redevelopment plan for this property. The proposed plan calls for a higher density mixed-use development that includes approximately 12,000 square feet of first floor commercial space and 82 apartment and townhomes.

City of Madison Comprehensive Plan

This site is within the Downtown Plan area of the newly adopted Comprehensive Plan. The Downtown Plan is not completed at this time; however, the drafts and public comments that have been generated as part of this process support the proposed development. They include the following objectives:

- Providing higher density along transit corridors.
- Mixed-use multi-story buildings.
- Building heights up to 6 to 8 stories.
- Redevelopments that support business and retail uses.
- Buildings should be placed close together and close to the street.
- A consistent architectural theme through the preservation of older buildings and the construction of new ones.
- Optimize pedestrian friendly features and minimize commutes.
- Eliminate the "unsafe feeling" of the existing bus depot area.

The Basset Neighborhood Plan

The property is located in the Basset neighborhood district and more specifically, the West Washington Avenue / Main Street Mixed Use District. A mixed use redevelopment with ground level retail in combination with upper level residential is recommended for the site. Residential housing densities are recommended at high (40-60 du/acre) to very high (greater than 60 du/acre) density.

Preservation of the historic character of the district is also encouraged.

Deconstruction:

This proposed development will require the deconstruction of the existing Badger Bus Depot and parking area. The developer will provide a Recycling and Re-Use plan for the building materials prior to obtaining a demolition permit.

Although the zoning code encourages the preservation of existing structures, the zoning code's purpose also includes promoting the general welfare of the city, encouraging the most appropriate land uses and conserving and enhancing the taxable value of lands and buildings.

Letter of Intent The Depot July 29, 2009 Page 3 of 4

This proposal will provide significant reinvestment in the area and implement many of the goals of the Neighborhood Master Plan and proposed Downtown Plan. We believe that when the benefits of this project are considered that the Plan Commission can determine that the demolition standards are met.

Project Description:

The development proposes a series of three linked buildings that line the edges of the site with a surface parking area internalized at the rear of the property. The buildings are designed to fit within the architectural fabric of the neighborhood. The masonry warehouse buildings that surround the property form the basis for the historically inspired architecture that is used.

The buildings are three, four and five stories respectively with a lower scale on Main Street stepping up in height to the five stories on West Washington Avenue. The three-story building on West Main Street contains a series of two story townhomes with single-level apartment flats on the third floor. The four-story building on Bedford Street consists apartment flats. The five-story building that is located on the corner of Bedford Street and West Washington Avenue includes ground level commercial space on the first floor with four levels of apartment homes above.

The buildings create an active street edge and a pedestrian friendly environment. The streets and sidewalks are punctuated with individual porches and townhome entries that connect the buildings to the street. The commercial use is designed for flexibility with the ability to provide entries and an active street level space on both streets. Compact landscaped front yards surround the residential street edge and street trees will line all three street sides of the site to soften the architecture and shade the sidewalks.

Vehicular access is available on all three streets. The Bedford Street entry is designed for pedestrian and vehicular access with a textured and colored pavement surface that will calm vehicular traffic. The surface parking area will provide parking for the commercial users during business hours and allow for residential visitor parking when the commercial parking needs subside. The residential parking will be primarily accommodated at the basement level, which extends below all three buildings and the surface parking above. Bicycle parking will be provided both underground and on surface to accommodate the needs of the public and residents.

Site Development Data:

Densities:				
Lot Area		43,560 sf		
Acres		1.0		
Commercial Area		11,985 s.f.		
Dwelling Units		82 units		
Lot Area/D.U.		531 s.f./un	İt	
Density		83 units/ac	re	
Dwelling Unit Mix:	Bldg 1	Bldg 2	Bldg 3	Total
Studio Apartments	0	4	0	4
One-Bedroom	27	20	0	47
One-Bedroom + Den	6	3	0	9
Two-Bedroom	12	2	2	16
Two-Bedroom Townhome	0	0	4	4
Two-Bedroom+Den	11	1	0	2
Total dwelling Units	46	30	6	82
Two-Bedroom+Den		0 1 30	4 0 6	4 2 82

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Building Heights:	3-5 Stories
Floor Area:	
Bldg #1	55,425 s.f.
Bldg #2	30,250 s.f.
Bldg #3	7,075 s.f.
Gross Floor Area	92,750 s.f.
(Excludes Underground parking)	
Floor Area Ratio:	2.12
Vehicle Parking Stalls	
Surface	25
Underground	95
Total	120
Ratio	1.44 stalls/unit
Bicycle Parking Stalls	
Surface	16
Underground	90
Total	106
Ratio	1.27 stalls/unit

Loading:

One off-street loading zone will be provided for this project located outside the drive aisle and adjacent to the commercial space.

Project Schedule:

It is currently anticipated that construction will begin in summer of 2009 with construction for the entire project completed within twelve months. The scheduled start date may vary depending on market conditions.

Social & Economic Impacts:

This development will have a positive social and economic impact. It meets the goals of "Smart Growth" and will substantially increase the city tax base on an infill site with minimal increased cost to the City. The mixed-use redevelopment achieves the goals of the Bassett Neighborhood Master Plan and provides new commercial space to further support the Bedford Street / West Washington Avenue commercial node. The proposed development will also provide high-quality rental housing for longer-term residents in a location that will minimize automobile traffic.

Thank you for your time in reviewing our proposal.

Very Truly Yours,

J. Randy Bruce, AIA Managing Member

Zoning Text

PUD-SIP 2 South Bedford Street July 29, 2009

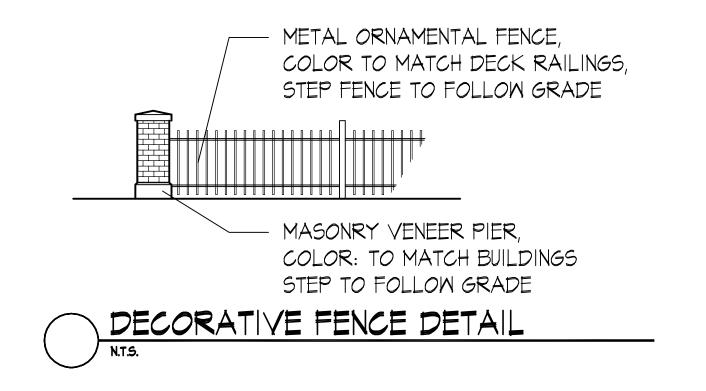
Legal Description: The lands subject to this Planned Unit Development District shall include Lots 8, 9, 10 & 11, Block 25, Original Plat of Madison, Dane County, Wisconsin.

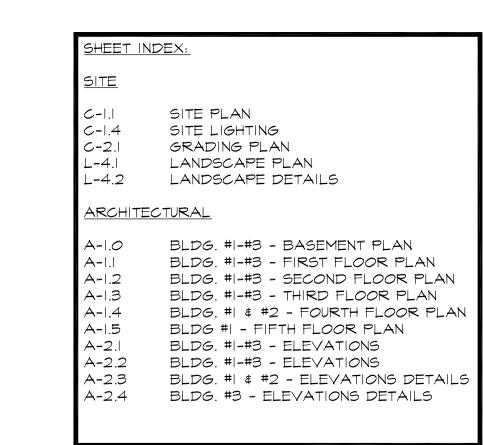
- A. **Statement of Purpose:** This zoning district is established to allow for the construction of a mixed-use development with 82 dwelling units and approximately 12,000 square feet of commercial space.
- B. *Permitted Uses:* Following are permitted uses within this P.U.D.
 - 1. Multifamily residential uses as shown on the approved plans.
 - 2. Commercial uses as allowed in the C-2 zoning district.
 - 3. Office uses as allowed in the O-1 zoning district.
 - 4. Accessory uses including but not limited to:
 - a. Accessory uses directly associated with those permitted uses, including parking for residents and guests, outdoor eating areas or drive-up windows as shown on the approved plans.
 - b. Temporary buildings for storage of building materials and equipment for construction purposes when on the same lot as a principle use for a period not to exceed the duration of such construction.
- C. *Lot Area*: As shown on the approved plans.
- D. *Height Regulations*: As shown on the approved plans.
- E. *Yard Regulations*: As shown on the approved plans.
- F. *Landscaping*: Site Landscaping will be provided as shown on the approved plans.
- G. *Usable Open Space Requirements*: Usable open space will be provided as shown on the approved plans.
- H. **Parking & Loading**: Off-street parking shall be provided as shown on the approved plans.
- I. *Family Definition*: The family definition shall coincide with the definition given in M.G.O. 28.03 for the R-5 zoning district.
- J. *Signage*: Signage for the commercial uses shall be limited to the maximum permitted in the C2 zoning district and to the maximum permitted in the R-5 district for the residential uses and as approved by the Urban Design Commission and Zoning Administrator.
- K. *Alterations and Revisions:* No alteration or revision of this Planned Unit Development shall be permitted unless approved by the City Planning Commission, however, the Zoning Administrator may issue permits for minor alterations or additions which are approved by the Architectural Review Committee, Director Of Planning and Development and the alderperson of the district and are compatible with the concept approved by the City Planning Commission.

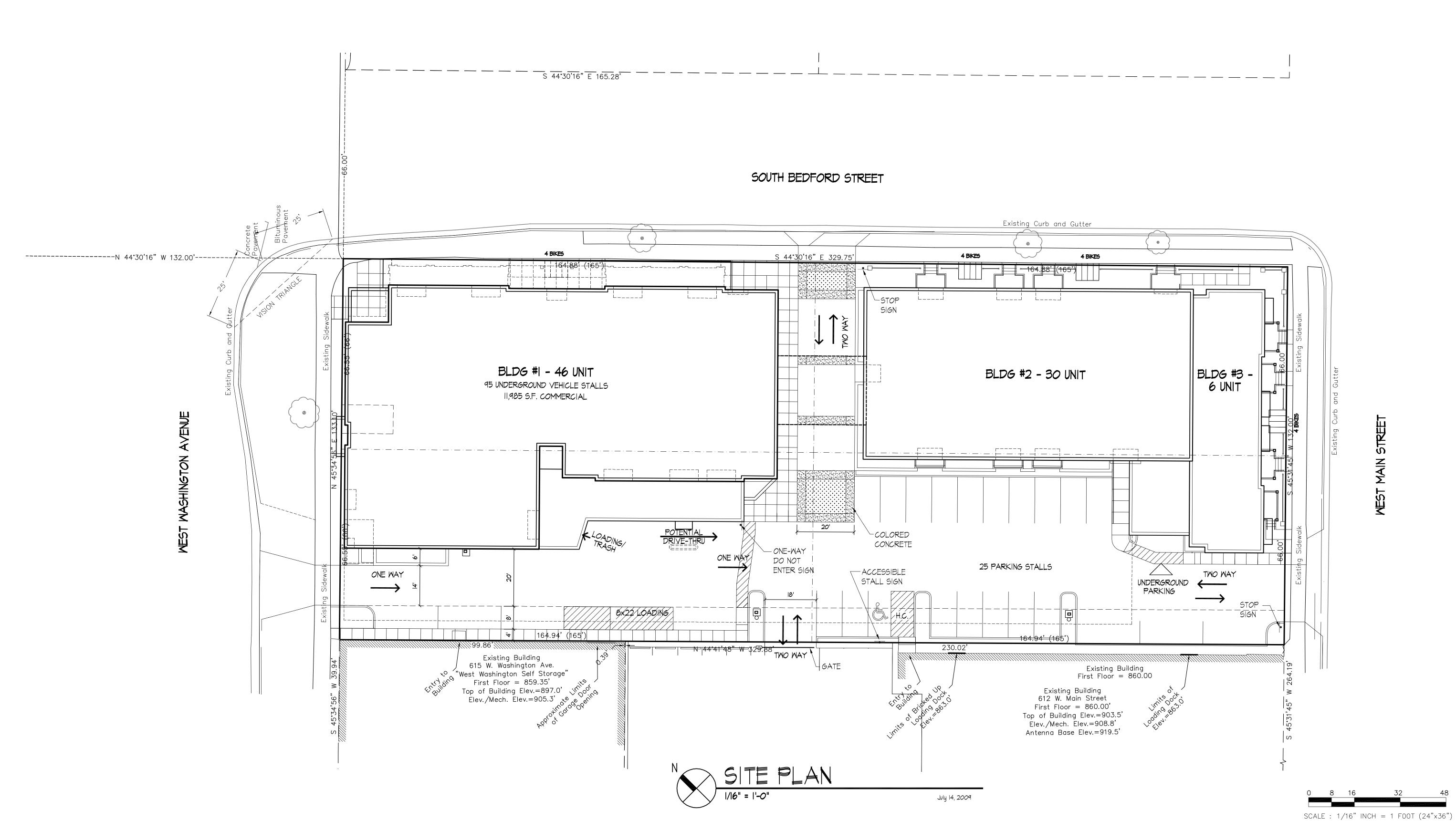


Site Locator Map

2 South Bedford Street







KNOTHE SBRUCE

7601 University Avenue Suite 201 Middleton, Wisconsin 53562 608-836-3690 Fax 836-6934

Consultant

Notes

I. A SIDE WALK WITH A PITCH GREATER THAN 1:20 IS A RAMP. THE MAXIMUM PITCH OF ANY RAMP OR WALK SHALL BE 1:12. RAMPS OVERCOMING MORE THAN 6" CHANGE IN ELEVATION TO HAVE HANDRAILS ON BOTH SIDES.

2. ALL DRIVEWAYS, CURB ADJACENT TO DRIVEWAYS, AND SIDEWALK CONSTRUCTION WITHIN THE PUBLIC RIGHT-OF-WAY, SHALL BE COMPLETED IN ACCORDANCE WITH CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION BY A CONTRACTOR CURRENTLY LICENSED BY THE CITY.

3. ALL NEW DRIVES TO BE CONSTRUCTED WITH CLASS III APPROACHES IN ACCORDANCE WITH MADISON GENERAL ORDINANCE SECTION 10.08(4)

IN TRASH ROOMS IN THE BUILDING BASEMENTS.

5. SEE SITE GRADING AND UTILITY PLANS FOR

4. ALL TRASH AND RECYCLING TO BE COLLECTED

ADDITIONAL INFORMATION.

6. COMM 62.0500(II)-REQUIRED FIRE LANES SHALL

BE PROVIDED PRIOR TO THE PLACEMENT OF COMBUSTIBLE MATERIALS AT THE BUILDING SITE, OR THE CONSTRUCTION OF ANY PORTION OF A BUILDING ABOVE THE FOOTING AND FOUNDATION.

7. ALL WORK IN THE RIGHT OF WAY SHALL BE PERFORMED BY A CITY LICENSED CONTRACTOR.

8. BIKE STALLS TO BE IN ACCORDANCE TO MADISON GENERAL ORDINANCE 28.11, (SEE SUBSECTIONS (3)(e) AND (3)(h)2d.). BIKE RACKS SHALL BE SINGLE POLE "CLASSIC BOLLARD" STYLE BY MADROX OR EQUIVALENT.

9. ALL SIDEWALK, CURB, AND GUTTER ABUTTING THE PROPERTY WHICH IS DAMAGED DURING CONSTRUCTION OR THAT THE CITY ENGINEER DETERMINES TO BE NECESSARY SHALL BE REPLACED.

Revisions

GDP-SIP Submittal - April 15, 2009 July 7, 2009 UDC Final Submittal - July 29, 2009

Project Title

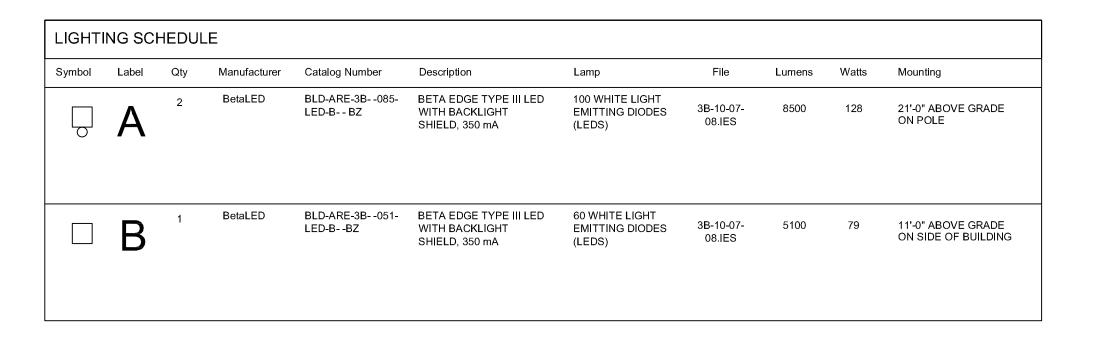
The Depot

2 S. Bedford Street

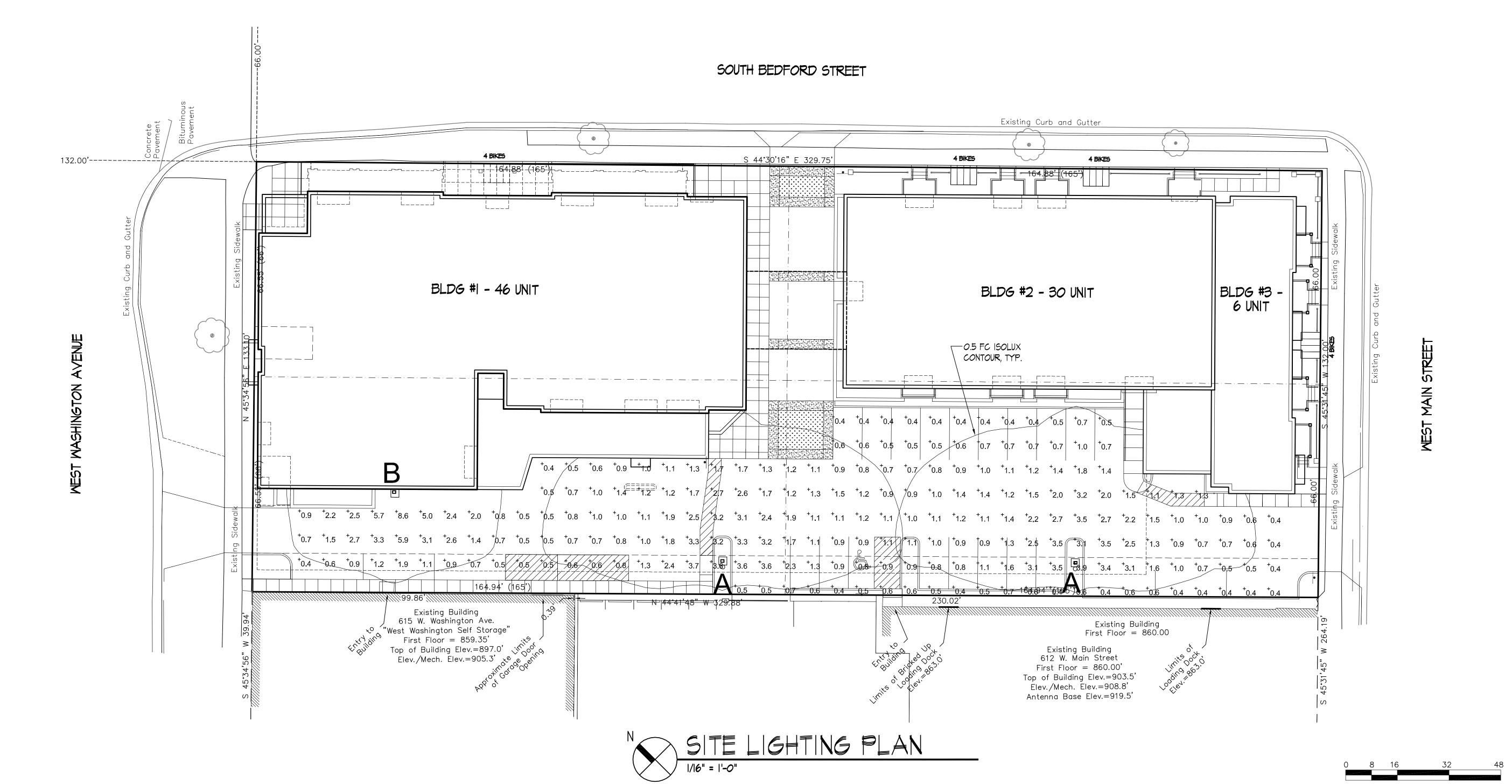
Drawing Title
Site Plan

Project No.

Drawing No.



LIGHTING STATIS	STICS					
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Parking Lot	+	1.4 fc	8.6 fc	0.4 fc	21.5:1	3.5:1



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Revisions GDP-SIP Submittal - April 15, 2009 July 28, 2009 UDC Final Submittal - July 29, 2009

Project Title The Depot

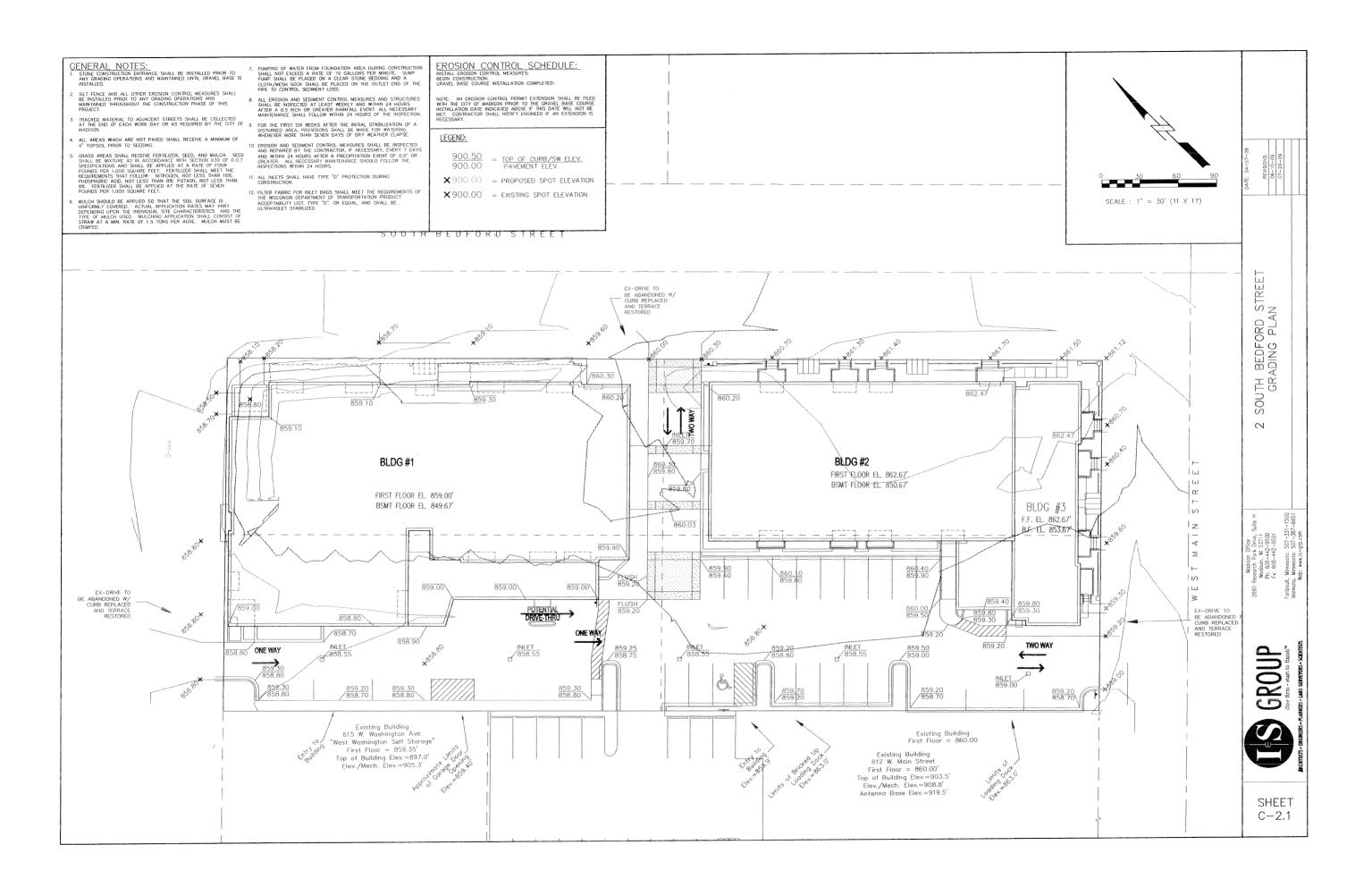
2 S. Bedford Street
Drawing Title
Site Lighting Plan

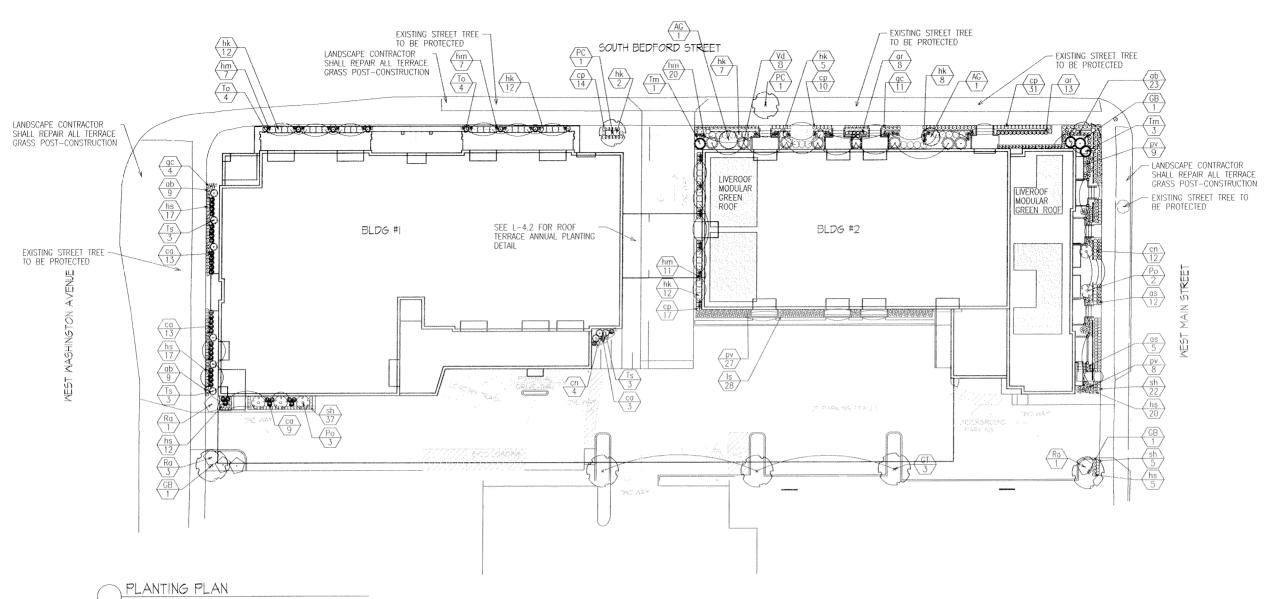
Project No.

SCALE: 1/16" INCH = 1 F00T (24"x36")

Drawing No.

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ant List							
Key	Botanical Name	Common Name	Quantity	Install Size			e Comments
					height	width	
	Deciduous Trees	Parties of the Control of the Contro					
AG	Amelanchier x grandiflora 'Cole'	Cole's Select Serviceberry	5	6' ht. B&B	15-20'		multi-stem
GB	Ginkgo biloba 'Princeton Sentry'	Princeton Sentry Ginkgo	3	2.5" cal. B&B	40'	16'	
GT	Gleditsia triacanthos 'Skyline'	Skyline Honeylocust	3	2.5" cal. B&B	45'	35'	
PC	Pyrus calleryana 'Chanticleer'	Cleveland Select Callery Pear	2	2.5" cal. B&B	35'	16'	
	Deciduous Shrubs			L. Maria			
Ро	Physocarpus opulifolius 'Seward'	Summer Wine Eastern Ninebark	3	2' ht. cont.	5~6'	5-6'	
Ra	Rhus aromatica 'Gro-Low'	Gro-Low Fragrant Sumac	10	18" spd. Cont.	2.5'	6-8'	
Vd	Viburnum dentatum 'Christom'	Blue Muffin Viburnum	9	3' ht. cont.	4-6'	4-6'	
30164834614	Evergreen Shrubs						
Tm	Taxus x media 'Tauntonii'	Taunton Yew	8	1.5' ht. cont.	3-4'	4-5'	
Ts	Thuja occidentalis 'Smaragd'	Emerald Green Arborvitae	12	4' ht. cont.	8-12'	3-4"	
То	Thuja occidentalis 'Holmstrup'	Holmstrup Arborvitae	8	4' ht. cont.	5'	2-3'	
	Perennials/Grasses/Groundcovers						
ar	Ajuga reptans 'Catlin's Giant'	Catlin's Giant Carpet Bugle	55	l qt. cont.	8-12"	1-1.5'	
as	Allium tanguticum 'Summer Beauty'	Summer Beauty Ornamental Onion	20	l qt. cont.	1-1.5'	1.5-2	
ab	Amsonia x 'Blue Ice'	Blue Ice Star Flower	73	l qt. cont.	1-1.5'	1.5-2	
ca	Calamagrostis acutiflora 'Karl Foerster'	Karl Foerster Feather Reed Grass	30	I gal. cont.	3-4'	2-3'	
cn	Calamintha nepeta ssp. Nepeta	Lesser Calamintha	5	l qt. cont.	1.5-2'	1.5~2'	
ср	Carex pensylvanica	Pennsylvania Sedge	30	1 qt. cont.	6-12"	1.5-2'	
gc	Geranium x cantabrigiense 'Biokovo'	Biokovo Geramnium	200	l qt. cont.	6-8"	12-18"	
hm	Hakonechloa macra 'Aureola'	Golden-Leaved Japanese Forest Grass	45	l qt. cont.	2-3'	1.5-2"	
hs	Helictotrichon sempervirens 'Saphirsprudel'	Blue Oat Grass	40	I qt. cont.	18-30"	1.5~2'	
hk	Hosta 'Krossa Regal'	Krossa Regal Hosta	20	l qt. cont.	2-3'	2.5'	
ls	Liatris spicata 'Kobold'	Blazing Star	28	l qt. cont.	1.5-2'	6"-1'	
pv	Panicum virgatum 'Northwind'	Northwind Switch Grass	69	l gal. cont.	4-6'	2-3'	
sh	Sporobolus heterolepis	Prairie Dropseed	45	l qt. cont.	2-3'	1.5-2	

7601 University Avenue Suite 201 Middleton, Wisconsin 53562 608-836-3690 Fax 836-6934

Consultant

LANDSCAPE ARCHITECTS

303 S PATERSON S U I T E O N E MADISON, W 53793 Phone: 608 251-3600 Fax: 608 251-2330

- 1. See sheet L-4.2 for planting details
- 2. Install tree protection fencing at least 5'-0" on each side of all existing trees for the width of the terrace area to protect the structural support roots. If any excavation is needed within 5'-0" of the trees, City Forestry shall be called to evaluate the excavation and approve any root cutting.
- Landscape contractor shall contact City
 Forestry at least 48 hours prior to planting street trees to approve planting stock and mark the tree locations. Contact Dean Kahl, 608-266-4816, DKahl@cityofmadison.com

Revisions GDP-SIP Submittal - April 15, 2009 July 29, 2009

Project Title The Depot

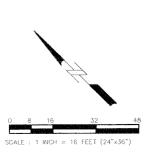
2 S. Bedford Street Drawing Title Planting Plan

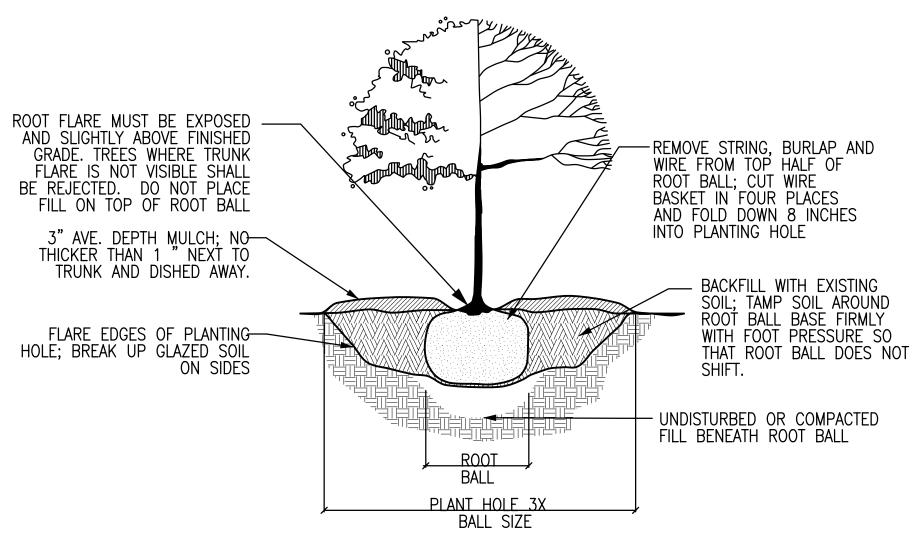
Project No.

Drawing No.

L-4.

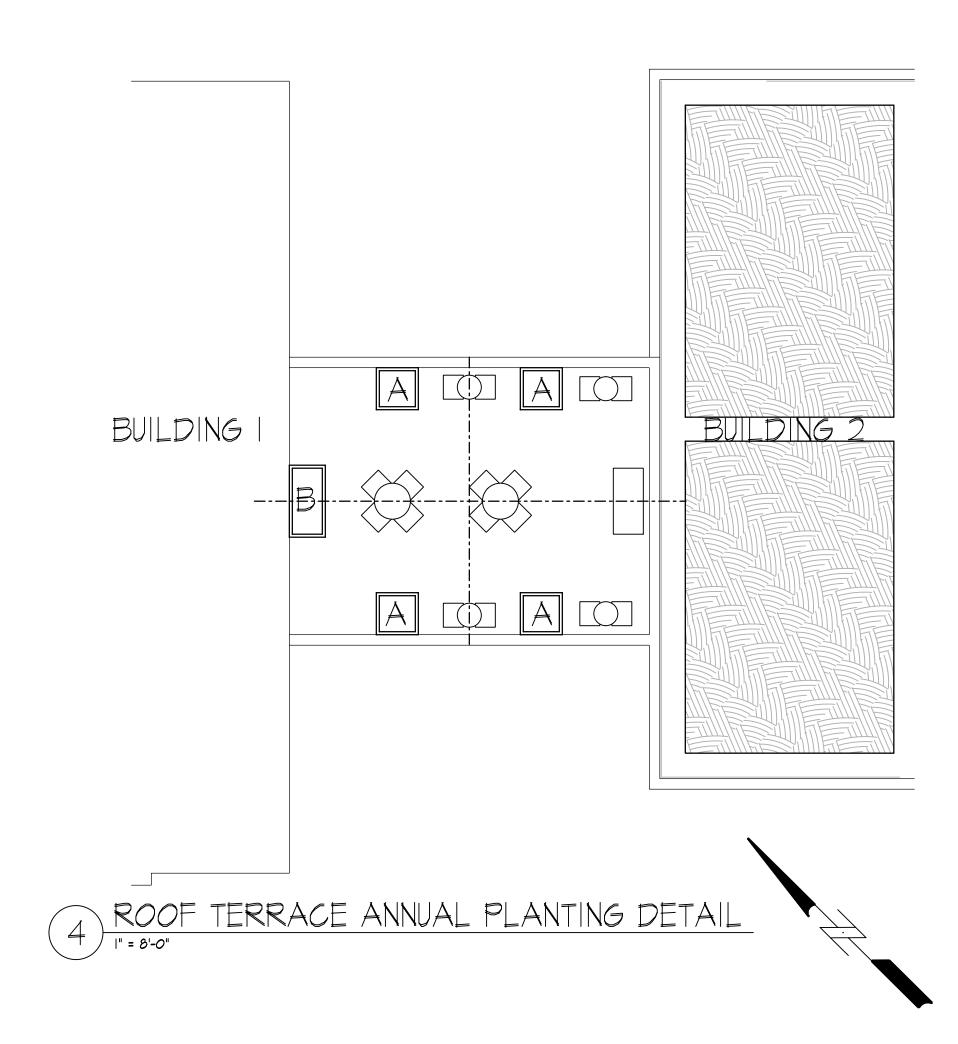
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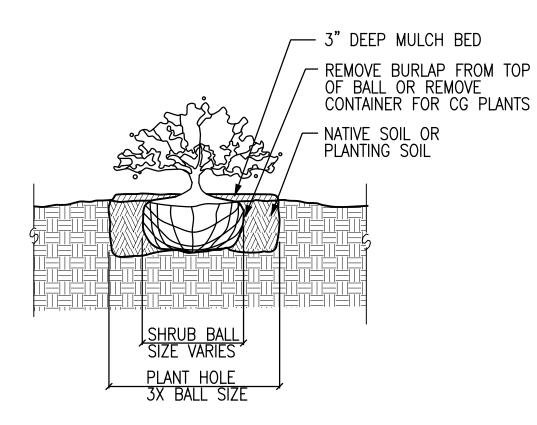




TREE PLANTING DETAIL

NTS

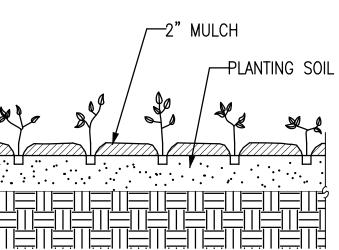




2 SHRUB PLANTING DETAIL

NTS

NOTE: SPACE AS INDICATED ON PLANS



3 PERENNIAL PLANTING DETAIL

NTS

NOTES:

TOPSOIL

 Topsoil shall be clean salvaged or imported material capable of passing the 1" sieve and meeting the requirements of Section 625.2(1) of the Standard Specifications for Highway Construction. The material shall be free of rocks, gravel, wood, debris, and of noxious weeds and their seeds.

2. Remove all rocks, debris, and litter from the site and all planting/seeding/sodding areas. Till or disc the subsoil to a depth of 6-8" to allow aeration. Provide a minimum of 6" topsoil in all lawn areas. Provide a minimum of 12" topsoil in all plant beds.

3. Topsoil shall be amended with compost: lawn areas 1 1/2", plant beds 3". Apply to the soil surface and till in.

PLANTS

1. See Sheet L-4.1 for planting plan & plant list.

2. All plant material shall conform to the American Standards of Nursery Stock.

Contact Landscape Architect, in writing, to request any plant material subsitutions.

4. Trees and shrubs shall have superior form, compactness, and symmetry. Damaged trees and shrubs, by weather, insects, fungus, knots, abraisions, or cut limbs or leaders and trees with multiple leaders, unless specified otherwise, will be rejected.

5. Trees and shrubs shall have healthy, well-developed root systems, free from physical damage.

6. All plant material shall be guaranteed for one (1) year from time of installation. Only one replacement per plant shall need to be made.

7. All plant beds shall be mulched per planting details with shredded bark, free of material detrimental to healthy plant growth.

8. All plant beds shall have a shovel-cut edge. See detail 2/L-1.2

10. Maintenance for plant material shall be 30 days. Plants shall be watered, staked, pruned, dead-headed; plant beds shall be weeded. Apply pre-emergent herbicide to all mulch beds at completion of isntallation.

LAWNS & GRASSES

1. The Contractor shall guarantee the germination of seed installed during the regular seeding season (May 1 - October 15).

2. Grass seed shall meet the requirements of Section 630.2.1 of Standards Specifications for Highway Construction. Lawn seed mix shall consist of a minimum of 3 improved varieties of Kentucky Bluegrass, 25% creeping red or chewings fescue, and a minimum of 2 improved varieties of turf-type perennial rye grass. Seeding rate is 3-4 pounds per 1000SF.

3. Mulch with clean straw or hay mulch that is well-seasoned, and free of rot, mildew, and seeds of noxious weeds over the seeded area according to the methods outlined by Section 627.3 of Standards Specifications for Highway Construction.

4. Slopes greater than 4:1 shall be mulched with erosion control blanket.

5. Maintenance of sodded lawn is to begin immediately after each area is planted and continued until acceptable lawn is established, but not less than 30 days from date of substantial completion.

6. Maintain and establish seeded and sodded lawn areas by watering, fertilizing, weeding, mowing, trimming, replanting, and other operations. Roll, regrade, and replant bare or eroded areas and remulch to produce a uniformly smooth lawn.

7. At the end of the maintenance period, a healthy, well-rooted, even-colored, viable lawn will have been established, free of weeds, open joints, bare areas, and surface irregularities.

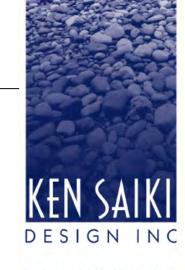
Total Points

& SCHITFOTS

7601 University Avenue Suite 201 Middleton, Wisconsin 53562 608-836-3690 Fax 836-6934

Consultant

Notes



LANDSCAPE ARCHITECTS 303 S. PATERSON

S U I T E O N E MADISON, WI 53703 Phone: 608 251-3600 Fax: 608 251-2330 www.ksd-la.com

1. See sheet L-4.1 for planting plan.

2. Install tree protection fencing at least 5'-0" on each side of all existing trees for the width of the terrace area to protect the structural support roots. If any excavation is needed within 5'-0" of the trees, City Forestry shall be called to evaluate the excavation and approve any root cutting.

3. Landscape contractor shall contact City Forestry at least 48 hours prior to planting street trees to approve planting stock and mark the tree locations. Contact Dean Kahl, 608-266-4816, DKahl@cityofmadison.com

Revisions GDP-SIP Submittal - April 15, 2009 July 29, 2009

City of Madison – Landscape Worksh	eet			
Number of Parking Stalls				24
Total Sq. Footage of Storage Area				_
Number of Canopy Shade Trees				2
Required - 2" - 2 1/2" cal.				
No. of Landscape Points Required				117
Points for Loading Area Required				_
Number of Points Required				117
-			•	:
			Credits	

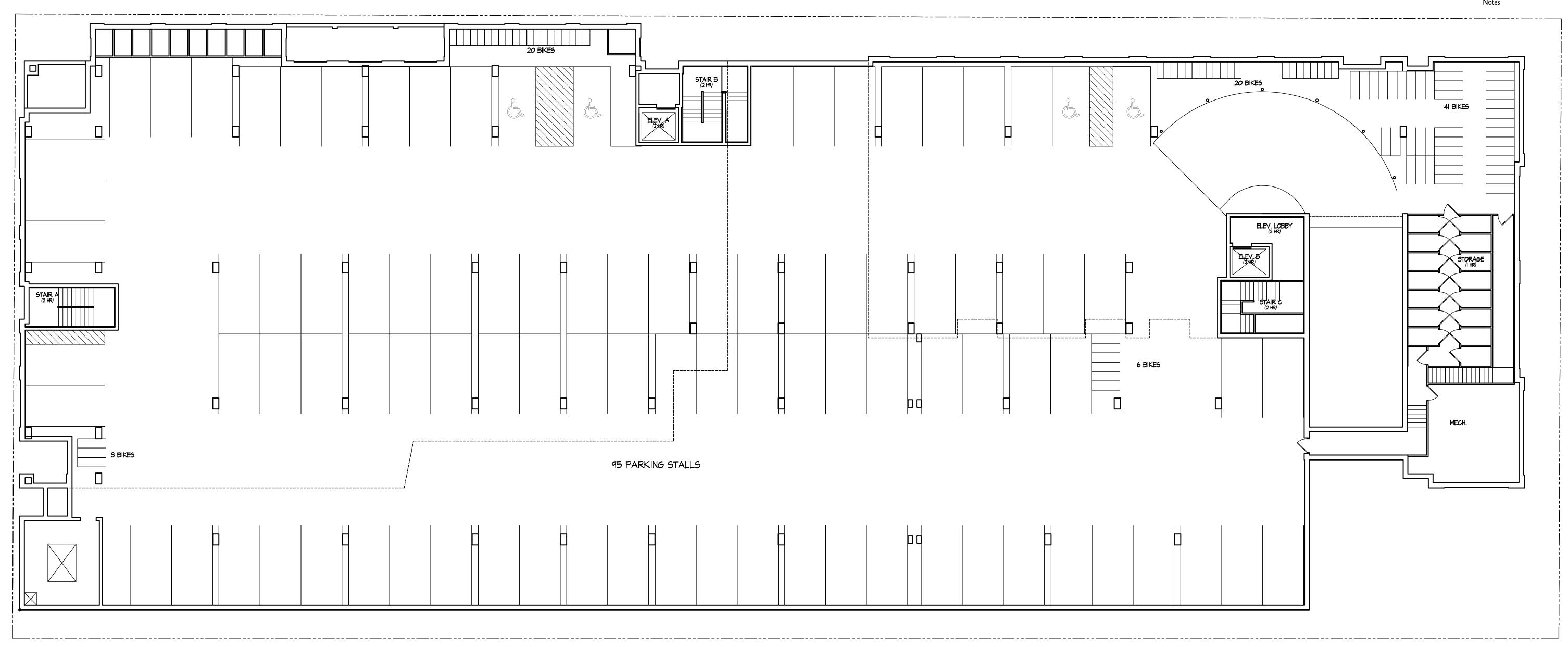
				Credits		
Element	Point		Points			
	Value	Quantity	Achieved	Quantity	Points	
Canopy Tree: 2" - 21/2"	35	3	105		0	
Deciduous Shrub	2	4	8		0	
Evergreen Shrub	3	3	9		0	
Decorative Wall or Fence	5		0		0	
(per 10 L.F.)						
Earth Berm (per 10 L.F.)					0	
Avg. Height 30"	5		0		0	
Avg. Height 15"	2		0		0	
Evergreen Trees	15		0		0	
3' height minimum						
Canopy Tree or Small Tree	15		0		0	
1 1/2" – 2" Caliper						
(i.e., Crab, Hawthorn)						
		Sub-Totals	122		0	

Project Title
The Depot

2 S. Bedford Street Drawing Title Planting Details

Proiect No.





BASEMENT FLOOR PLAN

3/32" = 1'-0"

Project Title
The Depot

Revisions - July 23, 2009 UDC Final Submittal - July 29, 2009

2 S. Bedford Street
Drawing Title
Basement Floor Plan

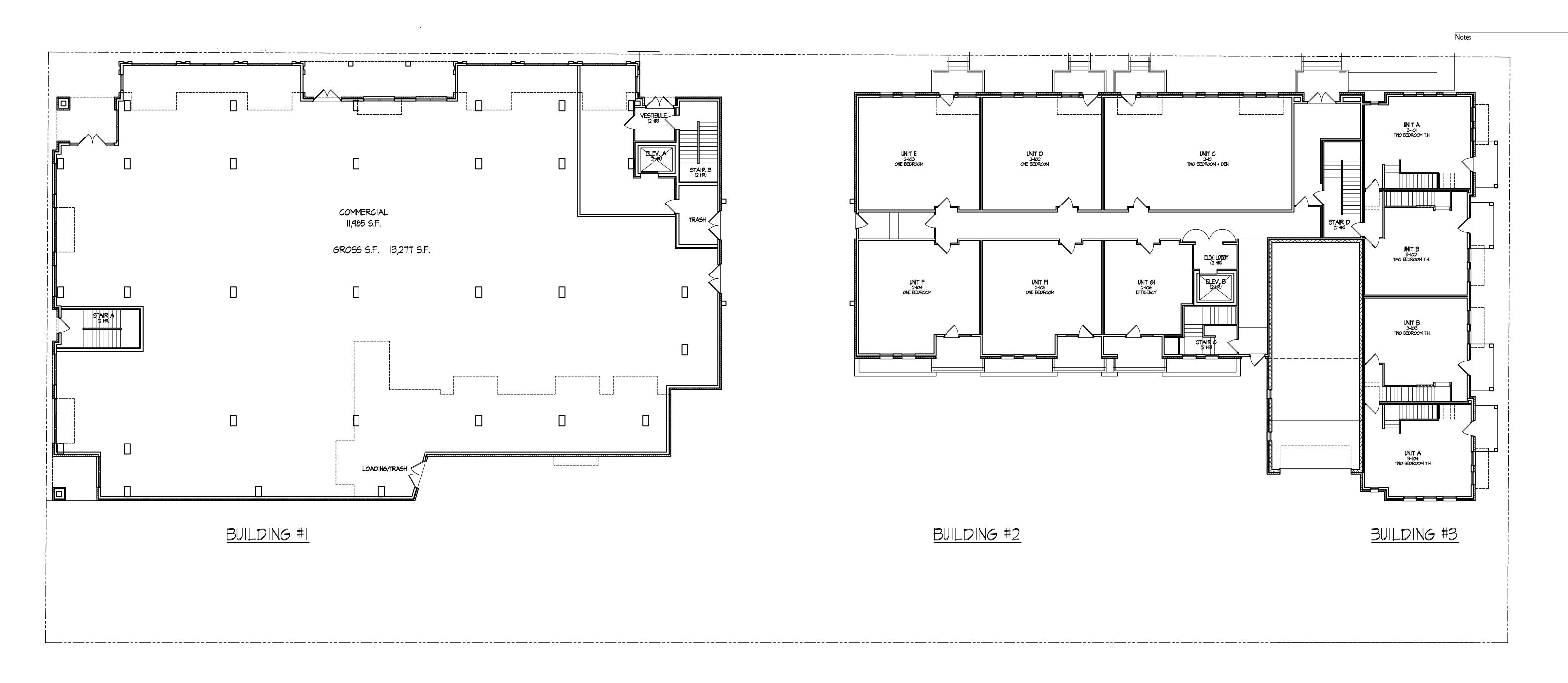
Drawing No.

0842 A-1.0

KNOTHE
SBRUCE
BRUCE
ARCHITECTS

7601 University Avenue Suite 201
Middleton, Wisconsin 53562
608-836-3690 Fax 836-6934

S. BEDFORD STREET



Project Title

The Depot



Revisions - July 23, 2009 UDC Final Submittal - July 29, 2009 2 S. Bedford Street

Drawing Title

Floor Floor Plans

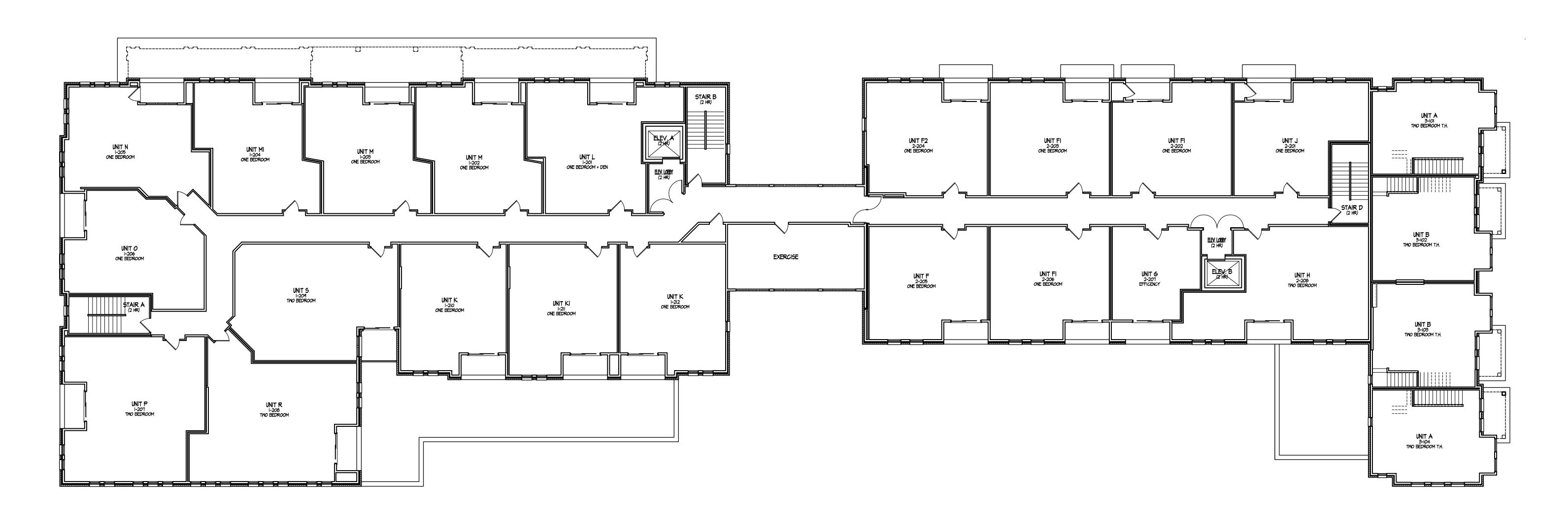
Buildings #1 - #3
Project No.

O842

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Drawing No.





BUILDING #1 BUILDING #2 BUILDING #3

SECOND FLOOR PLANS - Buildings #|- #3

2 S. Bedford Street
Drawing Title
Second Floor Plans

Project Title

The Depot

Buildings #1- #3
Project No. Drawing No.

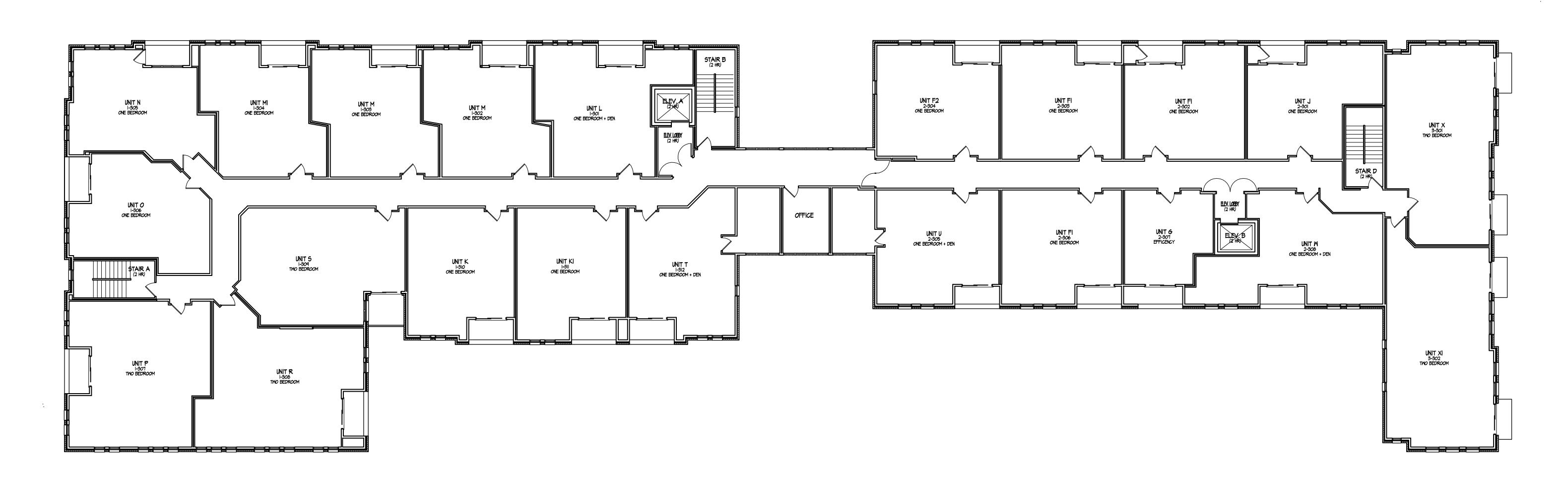
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Revisions - July 23, 2009 UDC Final Submittal - July 29, 2009



es



BUILDING #1 BUILDING #3

N THIRD FLOOR PLANS - Buildings # | - #3

Revisions - July 23, 2009 UDC Final Submittal - July 29, 2009 2 S. Bedford Street

Drawing Title

Third Floor Plans
Buildings #1 - #3

Project No.

Drawing No.

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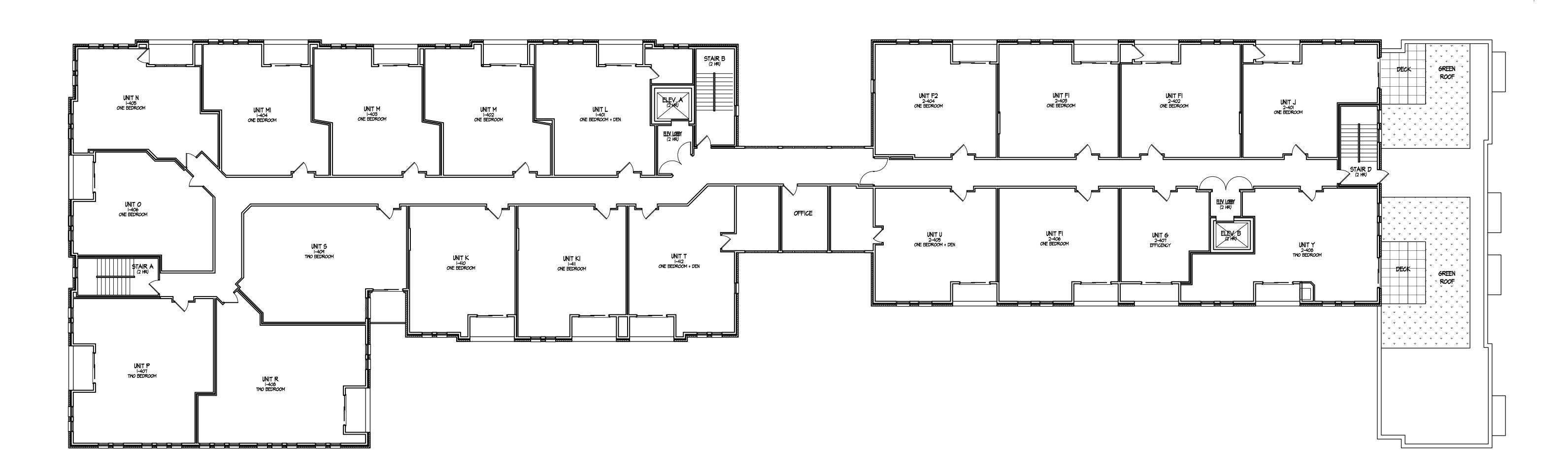
Project Title

The Depot

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A-1.3





BUILDING #1

N FOURTH FLOOR PLANS - Buildings # # #2

3/32" = 1'-0"

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Drawing Title

Fourth Floor Plans
Buildings #| \$ #2

Project No.

Drawing No.

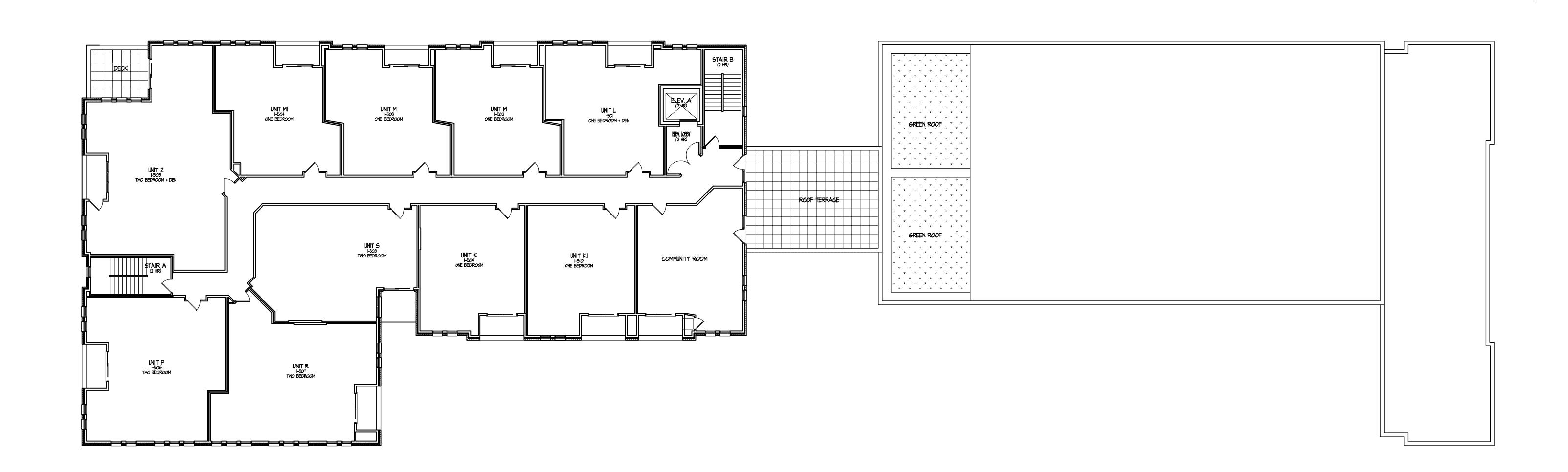
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A-1.4



Votes



BUILDING #1

FIFTH FLOOR PLAN - Building #1

Project Title

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Drawing Title

Fifth Floor Plan

Fifth Floor Plans

Building #1

Project No.

0842 A-1.5

Drawing No.



NORTH EAST ELEVATION AT S. BEDFORD ST.

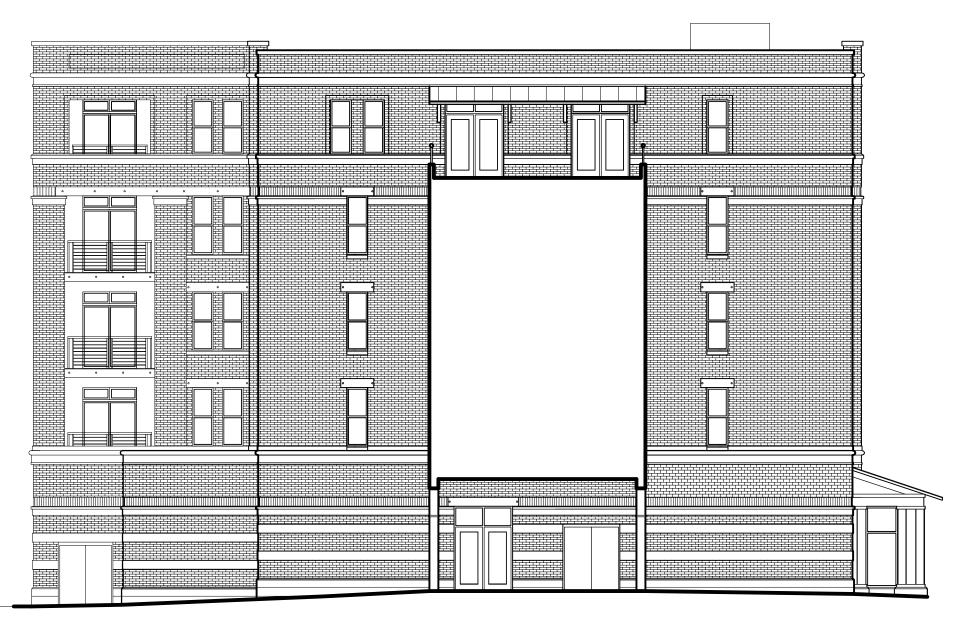
A-2.1 3/92" - 11-0"



NORTHWEST ELEVATION AT W. WASHINGTON

B A-2.I BUILDING #2

BUILDING #3



BUILDING #1

SOUTHEAST ELEVATION AT PRIVATE DRIVE

A-2.1 3/92" - 1'-0"

BUILDING #1

•

The Depot

- July 23, 2009 UDC Final Submittal - July 29, 2009

Revisions

Project Title

2 S. Bedford Street
Drawing Title
Elevations

Buildings #1-#3

Drawing No.





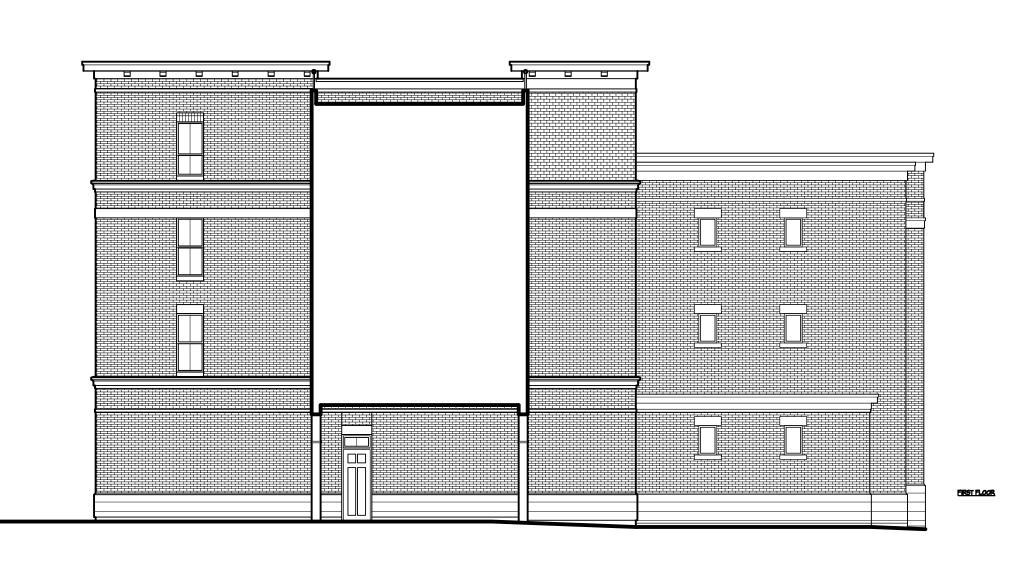
SOUTHWEST ELEVATION AT PARKING LOT

A-2.1 3/32" - 11-0"



SOUTHEAST ELEVATION AT WEST MAIN ST.

A-2.1 3/92" - 1'-0"



BUILDING #2

NORTHWEST ELEVATION AT PRIVATE DRIVE

A-2.1
3/32" - 1'-0"

Revisions - July 23, 2009 UDC Final Submittal - July 29, 2009

Project Title
The Depot

2 S. Bedford Street
Drawing Title
Elevations

Elevations Buildings #1-#3

Drawing No.

0842 A-3



3/8" - 1'-0"



Consultant

- July 23, 2009 UDC Final Submittal - July 29, 2009

The Depot

2 S. Bedford Street

Elevation Details Buildings #1 \$ #2

0842



- IX4 ON IXIO PAINTED TRIM - 12' PRECAST BAND - BRICK VENEER - 4" PRECAST BAND W/ I' PROJECTION - CORBELED BRICK - SOLDIER COURSE - 4" PRECAST BAND W/ I' PROJECTION - SOLDIER COURSE - BRICK VENEER, RECESSED 2" - BRICK VENEER - 6" PRECAST SILL W/ 1/2" PROJECTION - ALUM. RAILING - 5/4x6 COMPSITE TRIM - HEAVY TIMBER FRAMING - IX6 COMPOSITE TRIM ON HARDI-PANEL - BRICK VENEER - IX6 COMPSITE TRIM ON HARDI-PANEL -IX6 COMPOSITE TRIM ON $\frac{5}{4}$ COMPOSITE TRIM _5/4x6 COMPSITE TRIM HEAVY TIMBER CANOPY - HEAVY TIMBER COLUMN - ALUM. RAILING - 6" PRECAST SILL W/ 1/2" PROJECTION -12" PRECAST WATER TABLE W/ 2" PROJECTION BUILDING #3

SOUTH EAST ELEVATION AT W. MAIN ST.

3/8" - 1'-0"

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2 S. Bedford Street
Drawing Title
Elevation Details

Building #3
Project No.

0842

A-2.4

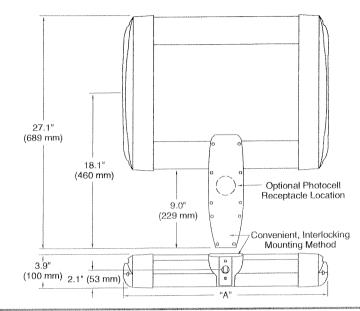
Drawing No.

Beta Catalog Number: BLD - ARE - 3B - DA - 085 - LED-B -



Notes:





Product	Housing			Initial Delivered	LED	molyce and the date of the control o	Color	Factory-Installed Options
Family	Indicator	Optics	Mounting	Lumens (00's)	Performance	Voltage	Option	f choosing more than one option, please type
BLD BLD	ARE	Opties ☐ T3¹ ☑ 3B²	DA ³	□017 □034 □051 □068 ▼085 □102 □119 □136 □153	LED-B	UL (120–277V Universal) UH	□SV □BZ □BK □WH	in manually on the lines provided above. □ EM-Emergency ⁴ □ F-Fuse □ HL_Hi/Low (175/350/525, dual circuit input) ⁵⁻⁸ □ P-Photocell ⁶ □ R-NEMA Photocell Receptacle □ TL-2-Level (175/525 w/ integrated sensor control) ^{5,7} □ TL1-2-Level (350/700 w/ integrated sensor control) ^{7,9} □ TL2-2-Level (0/350 w/ integrated sensor control) ^{7,9,10}
Footnotes		www.ujuneku sa balanda siska ku numah mili siska Amerika	opposition de vana is met vien fan factor e Religio (M. F.). Door	□ 170 □ 187 □ 204	e to begrown the top great conv. opposits accuration at \$466 or \$450 conv.	iussy pomy yan waarin kalon ki dagoo ka kakiin kalon ka ka	pusco omano produce con describiración de la constantidad de la constantidad de la constantidad de la constanti	□ TL3-2-Level (0/525 w/ integrated sensor control) ^{5,7} □ TL4-2-Level (0/700 w/ integrated sensor control) ^{5,7} □ 35K-3500k Color Temperature ¹¹ □ 43K-4300k Color Temperature ¹¹ □ 525-525mA Drive Current ¹²
1-IESNA Type	: III distribution	ı		4-Emergene	cy mode delivers I ligh	t bar 350mA lumen	output,	9-May be ordered with 525mA drive current

- 1-IESNA Type III distribution
- 2-IESNA Type III distribution with backlight control
- 3-Direct mounting arm

525mA

1.3 Based on fixture operating at 15° C Refer to LED Ambient spec sheet.

Te	Color mperature	Ī	Lumen Multiplier		
6000	K (Standard	1.00			
	4300K			0.80	
	3500K			0.75	
	Ambient perature (°C	Lumen Multiplier			
	-20		1.11		
	10		1.04		
	25		1.00		
	40		0.96		
Drive Current	Lumen Multiplier	ımen Pow İtiplier Multij		L ₇₀ Life* (hours)	
175mA	75mA 0.6 0.5			> 150,000	
350mA (Standard)	1.0	> 150,000			

Output Multipliers

- 4-Emergency mode delivers 1 light bar 350mA lumen output, consult LED Emergency Spec Sheet for further details
- 5-Available for 1-6 light bar fixtures
- 6-Must specify voltage other than UL or UH
- 7-Refer to multi level spec sheet for more information
- 8-Sensor not included

- 10-Available for 1-12 light bar fixtures
- 11-Color temperature per fixture
- 12-Driver operates at 525mA instead of the standard 350mA providing a higher lumen output and a shorter life

Light Bars	Initial Delivered Lumens – Type III Optic	Initial Delivered Lumens – Type III Optic w/ Backlight Shield	System Watts 120-277V	System Watts 347-480V	Dim. "A"/in.
1	1,750 (017)	1,160 (017)	28	30	11.75
2	3,500 (034)	2,320 (034)	55	59	11.75
3	5,250 (051)	3,480 (051)	79	84	13.75
4	7,000 (068)	4,640 (068)	104	109	15.75
5	8,750 (085)	5,800 (085)	128	133	17.75
6	10,500 (102)	6,960 (102)	153	156	19.75
7	12,250 (119)	8,120 (119)	183	194	21.75
8	14,000 (136)	9,280 (136)	207	218	23.75
9	15,750 (153)	10,440 (153)	232	242	25.75
10	17,500 (170)	11,600 (170)	257	266	27.75
11	19,250 (187)	12,760 (187)	281	290	29.75
12	21,000 (204)	13,920 (204)	306	313	31.75

1200 92nd Street Beta LED

1.5

70,000

Sturtevant, WI 53177

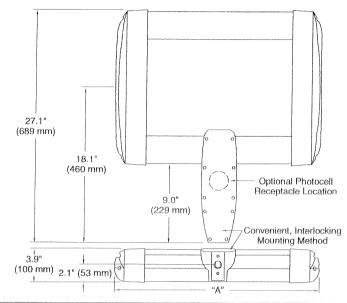
800-236-6800

www.BetaLED.com

Beta Catalog Number: BLD - ARE - 3B - DA - O5 | - LED-B -



Notes:



TORNIA DE	Product	Housing		North method and the south of the south of the south	Initial Delivered	LED	Salan Carlo (Maries India) of a nil and also also consist access about a nil	Color	Factory-Installed Options
	Family	Indicator	Optics	Mounting	Lumens (00's)	Performance	Voltage	Option	Y If choosing more than one option, please type
	BLD	ARE	□ T3 ¹ ⊠ 3B ²	$\mathbf{D}\mathbf{A}^3$	□017 □034	LED-B	□UL (120–277V		in manually on the lines provided above. □ EM—Emergency ⁴ □ F—Fuse
					№ 051		Universal)	□BK □WH	☐ HL-Hi/Low (175/350/525, dual circuit input) ⁵⁻⁸
					□ 085		(347–480V		■ P-Photocell ⁶
					□102		Universal)		R-NEMA Photocell Receptacle
					119		□ 12		☐ TL-2-Level (175/525 w/ integrated sensor control) ^{5,7}
					□ 136		2 7		☐ TL1-2-Level (350/700 w/ integrated sensor control) ^{5,7}
					□ 153		□34		☐ TL2-2-Level (0/350 w/ integrated sensor control) ^{7,9,10}
					□ 170				TL3-2-Level (0/525 w/ integrated sensor control) ^{5,7}
					1 87				☐ TL4–2-Level (0/700 w/ integrated sensor control) ^{5,7}
					□ 204				■ 35K–3500k Color Temperature ¹¹
									☐ 43K–4300k Color Temperature ¹¹
Alta Magneya	Footnotes								525–525mA Drive Current ¹²
	1-IESNA Type	III distribution		The second secon	4-Emergene	v mode delivers 1 light	bar 350m A luman	MATERIAL PROPERTY AND A STATE OF THE STATE O	O May be analoged with 525 at A deine consequent

- 2-IESNA Type III distribution with backlight control
- 3-Direct mounting arm

Output Multipliers				
Color Temperature		Lumen Multiplier		
6000K (Standard))	1.00	
4300K		0.80		
3500K		0.75		
Ambient Temperature (°C)		Lumen Multiplier		
	-20		1.11	
10			1.04	
25		1.00		
40		0.96		
Drive	Lumon	Poste	r I Life	

Drive Current			L ₇₀ Life* r (hours)	
175mA	0.6	0.5	> 150,000	
350mA (Standard)	1.0	1.0	> 150,000	
525mA	1,3	1.5	70,000	

* Based on fixture operating at 15° C. Refer to LED Ambient spec sheet.

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