

Drawing name: K:\GIS\DATA\268025002_Culvers_Madison_MV2 Design\PlanSheets\C0.0 - TITLE SHEET.dwg C0.0 May 02, 2022 11:41am By: Travis.Cyewski
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FINAL ENGINEERING PLANS

CULVER'S

4301 E TOWNE BLVD

MADISON, WISCONSIN 53704



UTILITY AND GOVERNING AGENCY CONTACTS

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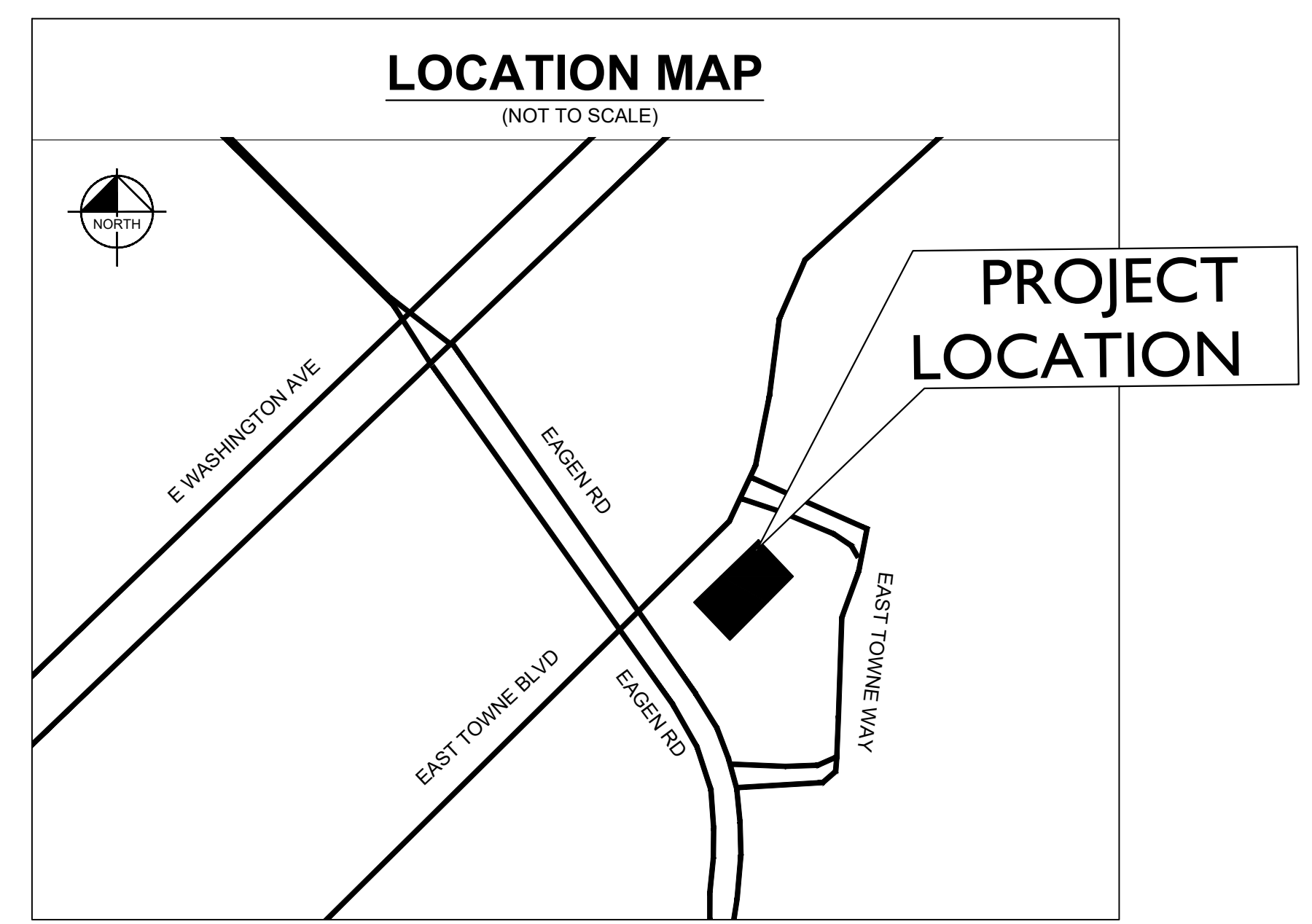
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Sheet List Table	
Sheet Number	Sheet Title
C0.0	TITLE SHEET
V0.0	TOPOGRAPHIC SURVEY
C1.0	GENERAL NOTES & DETAILS
C2.0	EXISTING CONDITIONS & DEMO PLAN
C3.0	SITE PLAN
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L1.0	LANDSCAPE PLAN

BENCHMARKS

SEE SURVEY (SHEETV0.0) FOR LOCATION OF SITE BENCHMARKS.

BENCHMARK:

SBM#1: CUT CROSS ON TOP OF CURB ALONG EAST PROPERTY LINE

ELEVATION = 867.82' (NAVD 88)

LEGAL DESCRIPTION

LOT 3 OF CERTIFIED SURVEY MAP 14486 (VOL. 296-314) IN THE NORTHEAST AND SOUTHEAST 1/4S OF THE SOUTHEAST 1/4 OF SECTION 28, TOWN 8 NORTH, RANGE 10 EAST, CITY OF MADISON, DANE COUNTY, WISCONSIN.

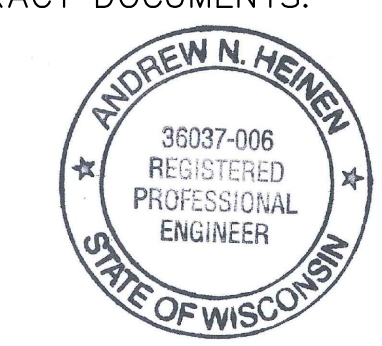
PROFESSIONAL ENGINEER'S CERTIFICATION

I, ANDREW HEINEN, A LICENSED PROFESSIONAL ENGINEER OF WISCONSIN, HEREBY CERTIFY THAT THIS SUBMISSION, PERTAINING ONLY TO THE "C" SERIES CIVIL SHEETS LISTED ABOVE BUT EXCLUDING DETAILS PREPARED BY OTHERS, WAS PREPARED ON BEHALF OF CULVER'S AND VALLEY FOOD OPS INC. BY KIMLEY-HORN AND ASSOCIATES, INC. UNDER MY PERSONAL DIRECTION. THIS TECHNICAL SUBMISSION IS INTENDED TO BE USED AS AN INTEGRAL PART OF AND IN CONJUNCTION WITH THE PROJECT SPECIFICATIONS AND CONTRACT DOCUMENTS.

DATED THIS 13TH DAY OF AUGUST, A.D., 2021.

A. Heinen

WISCONSIN LICENSED PROFESSIONAL ENGINEER 36037-006
 MY LICENSE EXPIRES ON NOVEMBER 30, 2021
 DESIGN FIRM REGISTRATION NUMBER: 184002012-0006

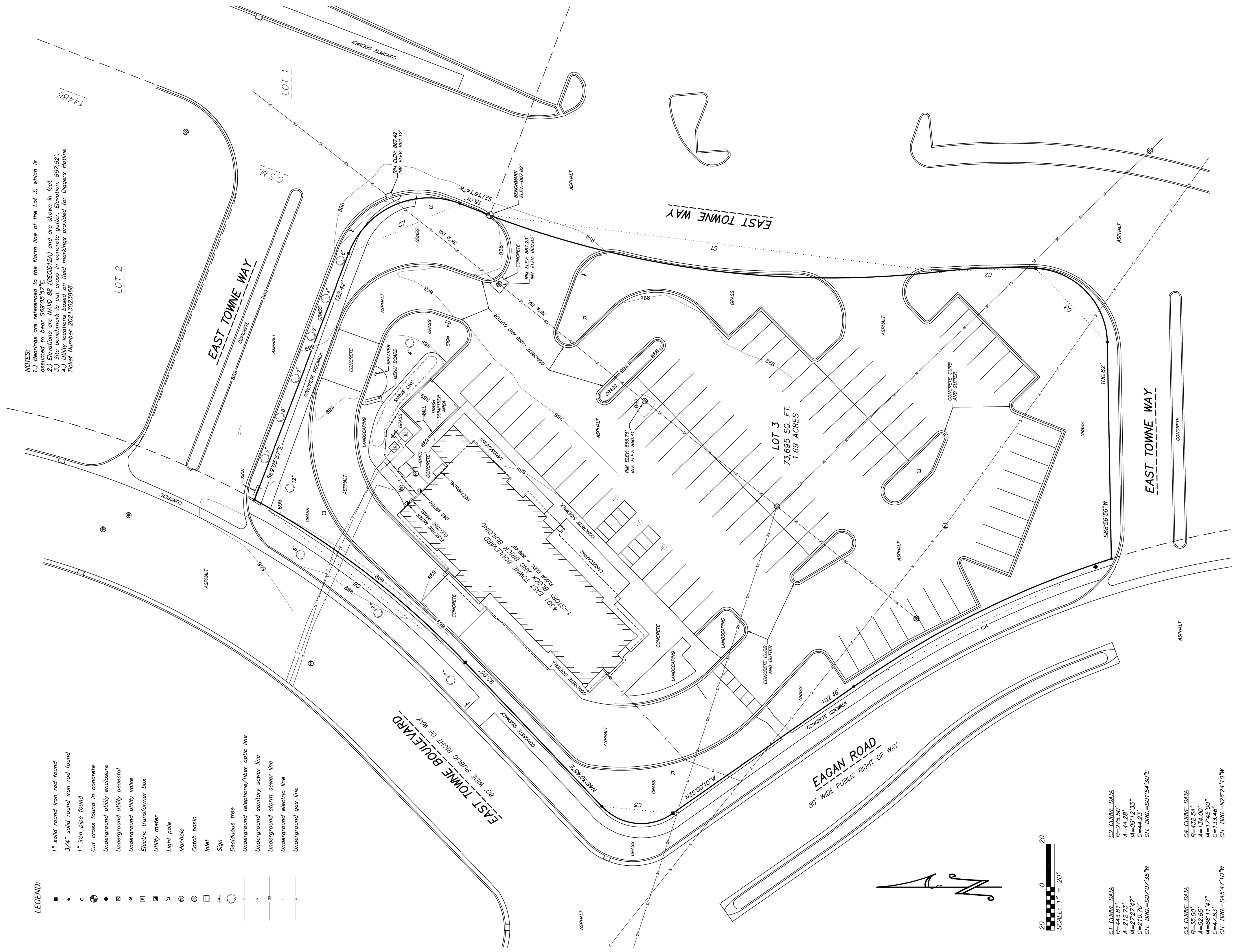


TITLE SHEET	TITLE SHEET
CULVER'S DRIVE-THRU IMPROVEMENTS 4301 E TOWNE BLVD MADISON, WISCONSIN 53704	REVISIONS No. DATE BY

PLAT OF SURVEY WITH TOPOGRAPHIC DATA

Lot 3 of Certified Survey Map 14486 (Vol. 99, Pages 296-314) in the Northeast and Southeast 1/4s of the Southeast 1/4 of Section 28, Town 8 North, Range 10 East, City of Madison, Dane County, Wisconsin.

NOTES:
 1.) Bearings are referenced to the North line of the Lot 3, which is assumed to bear S69°05'57"E.
 2.) Elevations are NAVD 88 (GEOID12A) and are shown in feet.
 3.) Site benchmark is cut cross in concrete gutter. Elevation: 867.82'.
 4.) Utility locations are shown with field markings provided for Diggers Hotline Ticket Number: 20213023868.



LEGEND:

- 1" solid round iron rod found
- 3/4" solid round iron rod found
- 1" iron pipe found
- ⊕ Cut cross found in concrete
- ◆ Underground utility enclosure
- ◇ Underground utility pedestal
- Underground utility valve
- ⊞ Electric transformer box
- ⊞ Utility meter
- ⊞ Light pole
- ⊞ Manhole
- ⊞ Catch basin
- ⊞ Inlet
- ⊞ Sign
- Deciduous tree
- Underground telephone/fiber optic line
- Underground sanitary sewer line
- Underground storm sewer line
- Underground electric line
- Underground gas line



C1 CURVE DATA R=443.91' A=17°14'47" I=277°27'47" C=210.70' CH. BRG.=S07°07'35"W	C2 CURVE DATA R=275.80' A=17°14'50" I=091°21'33" C=44.23' CH. BRG.=S01°54'30"E	C3 CURVE DATA R=35.00' A=52.65' I=86°11'47" C=4.74' CH. BRG.=S45°47'10"W	C4 CURVE DATA R=432.54' A=134.00' I=17°14'50" C=117.46' CH. BRG.=N26°24'10"W	C5 CURVE DATA R=25.00' A=36.21' I=82°59'54" C=33.13' CH. BRG.=N05°33'26"E	C6 CURVE DATA R=406.97' A=124.13' I=90°00'11" C=56.57' CH. BRG.=N37°39'51"E	C7 CURVE DATA R=40.00' A=62.83' I=90°00'11" C=56.57' CH. BRG.=S24°05'57"E
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I hereby certify that this survey complies with Chapter A-E 7 of the Wisconsin Administrative Code, that the same has been made in accordance with the laws of this State, and that this map is a correct representation thereof in accordance with the information provided.

August 6, 2021
 James M. Baker, F.L.S.



GENERAL NOTES

- 1. EXISTING SITE TOPOGRAPHY, UTILITIES, RIGHT-OF-WAY AND HORIZONTAL CONTROL SHOWN ON THE DRAWINGS WERE OBTAINED FROM A SURVEY PREPARED BY: TALARCZYK LAND SURVEYS... 2. COPIES OF SOILS INVESTIGATION REPORTS MAY BE OBTAINED FROM THE OWNER... 3. THE CONTRACTOR SHALL PHOTOGRAPH THE WORK AREA PRIOR TO CONSTRUCTION... 4. EXCEPT WHERE MODIFIED BY THE CONTRACT DOCUMENTS, ALL PROPOSED WORK SHALL BE IN ACCORDANCE WITH THE FOLLOWING SPECIFICATIONS WHICH ARE HEREBY MADE A PART HEREOF: A. "ROADWAY STANDARD SPECIFICATIONS, WISCONSIN DEPARTMENT OF TRANSPORTATION," AS PREPARED BY WISDOT... 5. STANDARD SPECIFICATIONS, SUPPLEMENTAL SPECIFICATIONS, AND RECURRING SPECIAL PROVISIONS, CONSTRUCTION PLANS, AND SUBSEQUENT DETAILS ARE ALL TO BE CONSIDERED AS PART OF THE CONTRACT... 6. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT ALL ITEMS REQUIRED FOR CONSTRUCTION OF THE PROJECT, AS SHOWN ON THE PLANS, ARE INCLUDED IN THE CONTRACT... 7. THE CONTRACTOR IS RESPONSIBLE FOR HAVING A SET OF "APPROVED" ENGINEERING PLANS WITH THE LATEST REVISION DATE ON THE JOB SITE PRIOR TO THE START OF CONSTRUCTION... 8. THE CONTRACTOR SHALL SUBSCRIBE TO ALL GOVERNING REGULATIONS AND SHALL OBTAIN ALL NECESSARY PUBLIC AGENCY PERMITS PRIOR TO STARTING WORK... 9. THE ENGINEER AND OWNER ARE NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS OR FOR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR... 10. CONSTRUCTION MATERIALS AND/OR EQUIPMENT MAY NOT BE STORED IN THE RIGHT-OF-WAY, AS DIRECTED BY THE OWNER... 11. EASEMENTS FOR THE EXISTING UTILITIES, BOTH PUBLIC AND PRIVATE, AND UTILITIES WITHIN PUBLIC RIGHT-OF-WAYS ARE SHOWN ON THE PLANS ACCORDING TO AVAILABLE RECORDS... 12. THE CONTRACTOR SHALL PRESERVE ALL CONSTRUCTION STAKES UNTIL THEY ARE NO LONGER NEEDED... 14. NOTIFICATION OF COMMENCING CONSTRUCTION: 14.A. THE CONTRACTOR SHALL NOTIFY AFFECTED GOVERNMENTAL AGENCIES IN WRITING AT LEAST THREE FULL WORKING DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION... 14.B. FAILURE OF THE CONTRACTOR TO ALLOW PROPER NOTIFICATION TIME WHICH RESULTS IN THE TESTING COMPANIES TO BE UNABLE TO VISIT THE SITE AND PERFORM TESTING WILL CAUSE THE CONTRACTOR TO SUSPEND THE OPERATION TO BE TESTED UNTIL THE TESTING AGENCY CAN SCHEDULE TESTING OPERATIONS... 15. ALL CONTRACTORS SHALL KEEP ACCESS AVAILABLE AT ALL TIMES FOR ALL EMERGENCY TRAFFIC, AS DIRECTED BY THE CITY OF MADISON... 16. ANY EXISTING SIGNS, LIGHT STANDARDS, AND UTILITY POLES THAT INTERFERE WITH CONSTRUCTION OPERATIONS AND ARE NOT NOTED ON THE PLANS FOR DISPOSAL SHALL BE REMOVED AND RESET BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE... 17. ALL TREES TO BE SAVED SHALL BE IDENTIFIED PRIOR TO CONSTRUCTION BY THE LANDSCAPE ARCHITECT AND SHALL BE PROTECTED PER WSDOT SECTION 202... 18. LIMB PRUNING SHALL BE PERFORMED UNDER THE SUPERVISION OF AN APPROVED LANDSCAPE ARCHITECT, FORESTER, OR ARBORIST... 19. ALL EXISTING PAVEMENT OR CONCRETE TO BE REMOVED SHALL BE SAWCUT ALONG LIMITS OF PROPOSED REMOVAL BEFORE COMMENCEMENT OF PAVEMENT REMOVAL... 20. ALL EXISTING UTILITIES OR IMPROVEMENTS, INCLUDING WALKS, CURBS, PAVEMENT, AND PARKWAYS DAMAGED OR REMOVED DURING CONSTRUCTION SHALL BE PROMPTLY RESTORED TO THEIR RESPECTIVE ORIGINAL CONDITION... 21. REMOVAL OF SPECIFIED ITEMS, INCLUDING BUT NOT LIMITED TO, PAVEMENT, SIDEWALK, CURB, GUTTER AND GUTTER, CURBVERTS, ETC., SHALL BE DISPOSED OF OFF-SITE BY THE CONTRACTOR... 22. THE CONTRACTOR SHALL COLLECT AND REMOVE ALL CONSTRUCTION DEBRIS, EXCESS MATERIALS, TRASH, OIL AND GREASE RESIDUE, MACHINERY, TOOLS, AND OTHER UNDESIRABLE ITEMS WHICH WERE NOT PRESENT PRIOR TO PROJECT COMMENCEMENT... 23. NO UNDERGROUND WORK WITHIN THE PUBLIC RIGHT-OF-WAY SHALL BE COVERED UNTIL IT HAS BEEN APPROVED BY THE CITY OF MADISON... 24. WHERE SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER, EXISTING DRAINAGE STRUCTURES AND PIPE SHALL BE CLEANED OF DEBRIS AND PATCHED AS NECESSARY TO ASSURE INTEGRITY OF THE STRUCTURE... 25. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING THE UTILITY COMPANIES LOCATE THEIR FACILITIES IN THE FIELD PRIOR TO CONSTRUCTION... 26. THE GENERAL CONTRACTOR SHALL COORDINATE WITH UTILITY COMPANIES TO PROVIDE CABLE TV, PHONE, ELECTRIC, GAS AND IRRIGATION SERVICES... 27. CONTRACTOR IS TO VERIFY ALL EXISTING STRUCTURES AND FACILITIES AT ALL PROPOSED UTILITY CONNECTION LOCATIONS AND NOTIFY ENGINEER OF ANY DISCREPANCIES PRIOR TO ORDERING MATERIAL AND STARTING WORK... 28. ANY FIELD TILES ENCOUNTERED SHALL BE INSPECTED BY THE ENGINEER... 29. ALL FRAMES AND LIDS FOR STORM AND SANITARY SEWERS, VALVE VAULT COVERS, FIRE HYDRANTS, AND B-BOXES ARE TO BE ADJUSTED TO MEET FINISHED GRADE...

- ADJUSTMENTS AS REQUIRED BY THE CITY OF MADISON UPON FINAL INSPECTION OF THE PROJECT. 30. HYDRANTS SHALL NOT BE FLUSHED DIRECTLY ON TO THE ROAD SUBGRADES... 31. TRENCH BACKFILL WILL BE REQUIRED TO THE FULL DEPTH ABOVE SEWERS AND WATERMAIN WITHIN TWO (2) FEET HORIZONTAL OF PROPOSED OR EXISTING PAVEMENT... 32. IF SOFT, SPONGY, OR OTHER UNSUITABLE SOILS WITH UNCONFINED COMpressive STRENGTH LESS THAN 0.5 TSE ARE ENCOUNTERED AT THE BOTTOM OF THE TRENCH... 33. THE TRENCHES FOR PIPE INSTALLATION SHALL BE KEPT DRY AT ALL TIMES DURING PIPE PLACEMENT... 34. AFTER THE STORM SEWER SYSTEM HAS BEEN CONSTRUCTED, THE CONTRACTOR SHALL PLACE PROPER INLET PROTECTION EROSION CONTROL AT LOCATIONS INDICATED BY THE ENGINEER... 35. AT THE CLOSE OF EACH WORKING DAY AND AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES AND FLOW LINES SHALL BE FREE FROM DIRT AND DEBRIS... 36. EROSION CONTROL MEASURES SHALL BE INSTALLED IN ACCORDANCE WITH WISCONSIN DEPARTMENT OF NATURAL RESOURCES REGULATIONS AND WSDOT STANDARDS... 37. THE CONTRACTOR SHALL CONFORM TO ALL EROSION CONTROL REQUIREMENTS AS SET FORTH BY THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES THROUGH THE NPDES PHASE II PERMIT PROGRAM... 38. THE PAVEMENT SHALL BE KEPT FREE OF MUD AND DEBRIS AT ALL TIMES... 39. ALL DISTURBED AREAS OF THE RIGHT-OF-WAY SHALL BE FULLY RESTORED TO PRE-CONSTRUCTION CONDITIONS WITH A MINIMUM OF SIX (6) INCHES OF TOPSOIL... 40. ALL PROPOSED GRADES SHOWN ON PLANS ARE FINISHED SURFACE ELEVATIONS... 41. ALL TESTING SHALL BE THE RESPONSIBILITY AND EXPENSE OF THE CONTRACTOR... 42. PROVIDE SMOOTH VERTICAL CURVES THROUGH HIGH AND LOW POINTS INDICATED BY SPOT ELEVATIONS... 43. WHEN REQUIRED, THE CONTRACTOR SHALL NOTIFY THE OWNER WHEN RECORD DRAWINGS CAN BE PREPARED... 44. BEFORE ACCEPTANCE, ALL WORK SHALL BE INSPECTED BY THE CITY OF MADISON, AS NECESSARY.

EARTHWORK NOTES

- 1. GENERAL 1.1. IT IS THE CONTRACTOR'S RESPONSIBILITY TO UNDERSTAND THE SOIL AND GROUNDWATER CONDITIONS AT THE SITE. 1.2. ANY QUANTITIES IN THE BID PROPOSAL ARE INTENDED AS A GUIDE FOR THE CONTRACTOR'S USE IN DETERMINING THE SCOPE OF THE COMPLETED PROJECT... 1.3. THE CONTRACTOR WILL NOTE THAT THE ELEVATIONS SHOWN ON THE CONSTRUCTION PLANS ARE FINISHED GRADE AND THAT PAVEMENT THICKNESS, TOPSOIL, ETC., MUST BE ACCOUNTED FOR... 1.4. THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE DURING CONSTRUCTION AND PREVENT STORMWATER FROM RUNNING INTO OR STANDING IN EXCAVATED AREAS... 1.5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTATION OF THE SOIL EROSION AND SEDIMENTATION CONTROL MEASURES... 1.6. PRIOR TO COMMENCEMENT OF GRADING ACTIVITIES, THE CONTRACTOR SHALL ERECT A CONSTRUCTION FENCE AROUND ANY TREE DESIGNATED TO BE PRESERVED... 1.7. EXISTING SUBSURFACE CONDITIONS WERE OBTAINED FROM A GEOTECHNICAL PREPARED BY:

- 1. TOPSOIL EXCAVATION INCLUDES: 1.1. EXCAVATION OF TOPSOIL AND OTHER STRUCTURALLY UNSUITABLE MATERIALS WITHIN THOSE AREAS THAT WILL REQUIRE EARTH EXCAVATION OR COMPACTED EARTH FILL MATERIAL... 1.2. PLACEMENT OF EXCAVATED MATERIAL IN OWNER-DESIGNATED AREAS FOR FUTURE USE WITHIN AREAS WHERE MAINTAINING PROPER DRAINAGE... 1.3. TOPSOIL STOCKPILED FOR RESPREAD SHALL BE FREE OF CLAY AND SHALL NOT CONTAIN ANY OF THE TRANSITIONAL MATERIAL BETWEEN THE TOPSOIL AND CLAY... 1.4. TOPSOIL RESPREAD SHALL INCLUDE HAULING AND SPREADING SIX (6) INCHES OF TOPSOIL DIRECTLY OVER AREAS TO BE LANDSCAPED... 1.5. MODERATE COMPACTION IS REQUIRED IN NON-STRUCTURAL FILL AREAS.

- 2. EARTH EXCAVATION INCLUDES: 2.1. EXCAVATION OF SUBSURFACE MATERIALS WHICH ARE SUITABLE FOR USE AS STRUCTURAL FILL... 2.2. PLACEMENT OF SUITABLE MATERIALS SHALL BE WITHIN THOSE AREAS REQUIRING STRUCTURAL FILL... 2.3. STRUCTURAL FILL MATERIAL MAY BE PLACED WITHIN THOSE PORTIONS OF THE SITE NOT REQUIRING STRUCTURAL FILL... 2.4. COMPACTION OF SUITABLE MATERIALS SHALL BE TO AT LEAST 93% OF THE MODIFIED PROCTOR DRY DENSITY WITHIN PROPOSED PAVEMENT AREAS... 3. UNSUITABLE MATERIAL: UNSUITABLE MATERIALS SHALL BE CONSIDERED MATERIAL THAT IS NOT SUITABLE FOR THE SUPPORT OF PAVEMENT AND BUILDING CONSTRUCTION... 4. MISCELLANEOUS: THE CONTRACTOR SHALL: 4.1. SPREAD AND COMPACT UNIFORMITY TO THE DEGREE SPECIFIED ALL EXCESS TRENCH SPOIL AFTER COMPLETION OF THE UNDERGROUND IMPROVEMENTS... 4.2. SCARIFY, DISC, AERATE, AND COMPACT, TO THE DEGREE SPECIFIED, THE UPPER TWELVE (12) INCHES OF THE SUITABLE SUBGRADE MATERIAL IN ALL AREAS THAT MAY BE SOFT DUE TO EXCESS MOISTURE... 4.3. PROVIDE WATER TO ADD TO DRY MATERIAL IN ORDER TO ADJUST THE MOISTURE CONTENT FOR THE PURPOSE OF ACHIEVING THE SPECIFIED COMPACTION... 4.4. BACKFILL THE CURB AND GUTTER AFTER ITS CONSTRUCTION AND PRIOR TO THE PLACEMENT OF THE BASE COURSE MATERIAL... 5. TESTING AND FINAL ACCEPTANCE 5.1. THE CONTRACTOR SHALL PROVIDE AS A MINIMUM A FULLY LOADED SIX-WHEEL TANDEM AXLE TRUCK FOR PROOF ROLLING THE PAVEMENT SUBGRADE PRIOR TO THE PLACEMENT OF THE CURB AND GUTTER... 5.2. ANY UNSUITABLE AREA ENCOUNTERED AS A RESULT OF PROOF ROLLING SHALL BE REMOVED AND REPLACED WITH SUITABLE MATERIAL OR OTHERWISE CORRECTED AND APPROVED BY THE ENGINEER.

PAVING NOTES

- 1. GENERAL 1.1. PAVING WORK INCLUDES FINAL SUBGRADE SHAPING, PREPARATION, AND COMPACTION... 1.2. COMPACTION REQUIREMENTS [REFERENCE ASTM D-1557 (MODIFIED PROCTOR)] SUBGRADE = 95% SUBGRADE = 95% AGGREGATE BASE COURSE = 95% BITUMINOUS COURSE = 95% OF MAXIMUM DENSITY... 1.3. IT SHALL BE THE CONTRACTOR'S SOLE RESPONSIBILITY TO PROVIDE PROPER BARRICADING WARNING DEVICES... 2. SUBGRADE PREPARATION 2.1. EARTHWORK FOR PROPOSED PAVEMENT SUBGRADE SHALL BE FINISHED TO WITHIN 0.1 FOOT, PLUS OR MINUS, OF PLAN ELEVATION... 2.2. PRIOR TO THE PLACEMENT OF THE BASE COURSE, THE SUBGRADE MUST BE PROOF-ROLLED AND INSPECTED FOR UNSUITABLE MATERIALS AND/OR EXCESSIVE MOVEMENT... 2.2.1. SCARIFY, DISC, AND AERATE. 2.2.2. REMOVE AND REPLACE WITH STRUCTURAL CLAY FILL. 2.2.3. REMOVE AND REPLACE WITH GRANULAR MATERIAL. 2.2.4. USE OF GEOTEXTILE FABRIC. 2.3. PRIOR TO THE CONSTRUCTION OF THE CURB AND GUTTER AND THE PLACEMENT OF THE BASE MATERIAL, THE PAVEMENT AREA SHALL BE FINE-GRADED TO WITHIN 0.04 FEET (1/2 INCH) OF FINAL SUBGRADE ELEVATION... 2.4. PRIOR TO PLACEMENT OF THE BASE COURSE, THE SUBGRADE SHALL BE APPROVED BY THE TESTING ENGINEER. 3. CONCRETE WORK 3.1. ALL EXTERIOR CONCRETE SHALL BE PORTLAND CEMENT CONCRETE WITH AIR ENTRAINMENT OF NOT LESS THAN FIVE (5%) OR MORE THAN EIGHT (8%) PERCENT... 3.2. CONCRETE CURB AND/OR COMBINATION CURB AND GUTTER SHALL BE OF THE TYPE SHOWN ON THE PLANS... 3.3. CURBS SHALL BE DEPRESSED AND MEET THE SLOPE REQUIREMENTS OF THE FEDERAL ADA STANDARDS... 3.4. THE CURBS SHALL BE BACKFILLED AFTER THEIR CONSTRUCTION AND PRIOR TO THE PLACEMENT OF THE BASE COURSE... 3.5. CONCRETE SIDEWALK SHALL BE IN ACCORDANCE WITH THE ABOVE AND THE PLANS... 3.6. CONCRETE CURING AND PROTECTION SHALL BE PER WSDOT STANDARDS... 3.7. THE COST OF AGGREGATE BASE OR SUBBASE UNDER CONCRETE WORK SHALL BE INCLUDED IN THE COST OF THE RESPECTIVE CONCRETE ITEM. 4. FLEXIBLE PAVEMENT 4.1. THE PAVEMENT MATERIALS FOR BITUMINOUS STREETS, PARKING LOTS, AND DRIVE AISLES SHALL BE AS DETAILED ON THE PLANS... 4.2. ALL TRAFFIC SHALL BE KEPT OFF THE COMPLETED AGGREGATE BASE UNTIL THE BINDER COURSE IS LAID... 4.3. PRIOR TO PLACEMENT OF THE SURFACE COURSE, THE BINDER COURSE SHALL BE CLEANED AND TACK-COATED IF DUSTY OR DIRTY... 4.4. SEAMS IN BAM, BINDER, AND SURFACE COURSE SHALL BE STAGGERED A MINIMUM OF 6 INCHES. 5. TESTING AND FINAL ACCEPTANCE 5.1. THE CONTRACTOR SHALL FOLLOW THE QUALITY CONTROL TESTING PROGRAM FOR CONCRETE AND PAVEMENT MATERIALS ESTABLISHED BY THE MATERIALS TESTING ENGINEER... 5.2. PRIOR TO PLACEMENT OF THE BITUMINOUS CONCRETE SURFACE COURSE, THE CONTRACTOR, WHEN REQUIRED BY THE CITY OF MADISON, SHALL OBTAIN SPECIMENS OF THE BINDER COURSE WITH A CORE DRILL WHERE DIRECTED FOR THE PURPOSE OF THICKNESS VERIFICATION... 5.3. WHEN REQUIRED BY THE CITY OF MADISON, THE CONTRACTOR SHALL OBTAIN SPECIMENS OF THE FULL DEPTH BITUMINOUS CONCRETE PAVEMENT STRUCTURE WITH A CORE DRILL WHERE DIRECTED IN ORDER TO CONFIRM THE PLAN THICKNESS... 5.4. FINAL ACCEPTANCE OF THE TOTAL PAVEMENT INSTALLATION SHALL BE SUBJECT TO THE TESTING AND CHECKING REQUIREMENTS CITED ABOVE.

SIGNAGE AND PAVEMENT MARKING NOTES

- 1. ALL SIGNING AND PAVEMENT MARKING SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE WISCONSIN DEPARTMENT OF TRANSPORTATION (WSDOT) STANDARDS. 2. SIGNS: SIGNS SHALL BE CONSTRUCTED OF 0.090-INCH THICK FLAT ALUMINUM PANELS WITH REFLECTORIZED LEGEND ON THE FACE... 3. POSTS: SIGN POSTS SHALL BE A HEAVY-DUTY STEEL "U" SHAPED CHANNEL WEIGHING 3.0 POUNDS/FOOT... 4. SIGNS AND POSTS SHALL BE INSTALLED IN ACCORDANCE WITH WSDOT STANDARDS. 5. PAVEMENT MARKINGS: ALL PAVEMENT MARKINGS IN THE PUBLIC RIGHT-OF-WAY, SUCH AS STOP LINES, CENTERLINES, CROSSWALKS, AND DIRECTIONAL ARROWS, SHALL BE REFLECTORIZED THERMOPLASTIC ON ASPHALT AND EPOXY ON CONCRETE OR AS APPROVED BY WSDOT. 6. PAVEMENT MARKINGS ON BIKE PATHS, PARKING LOT STALLS, AND SIMILAR "LOW-WEAR" APPLICATIONS, SHALL BE PAINT IN ACCORDANCE WITH WSDOT STANDARDS. 7. COLOR, WIDTH, STYLE, AND SIZE OF ALL MARKINGS SHALL BE IN ACCORDANCE WITH THE MUTCD AND LOCAL CODE. 8. THERMOPLASTIC MARKINGS SHALL BE INSTALLED WHEN THE PAVEMENT TEMPERATURE IS 55 DEGREES FAHRENHEIT AND RISING. PAINT MARKINGS MAY BE INSTALLED WHEN THE AIR TEMPERATURE IS 50 DEGREES FAHRENHEIT AND RISING.



Table with columns: No., DATE, REVISIONS

Table with columns: SCALE, AS NOTED, DESIGNED BY: JPM, DRAWN BY: ANA, CHECKED BY: ANH



CULVER'S DRIVE-THRU IMPROVEMENTS & DETAILS 4301 E TOWNE BLVD MADISON, WISCONSIN 53704

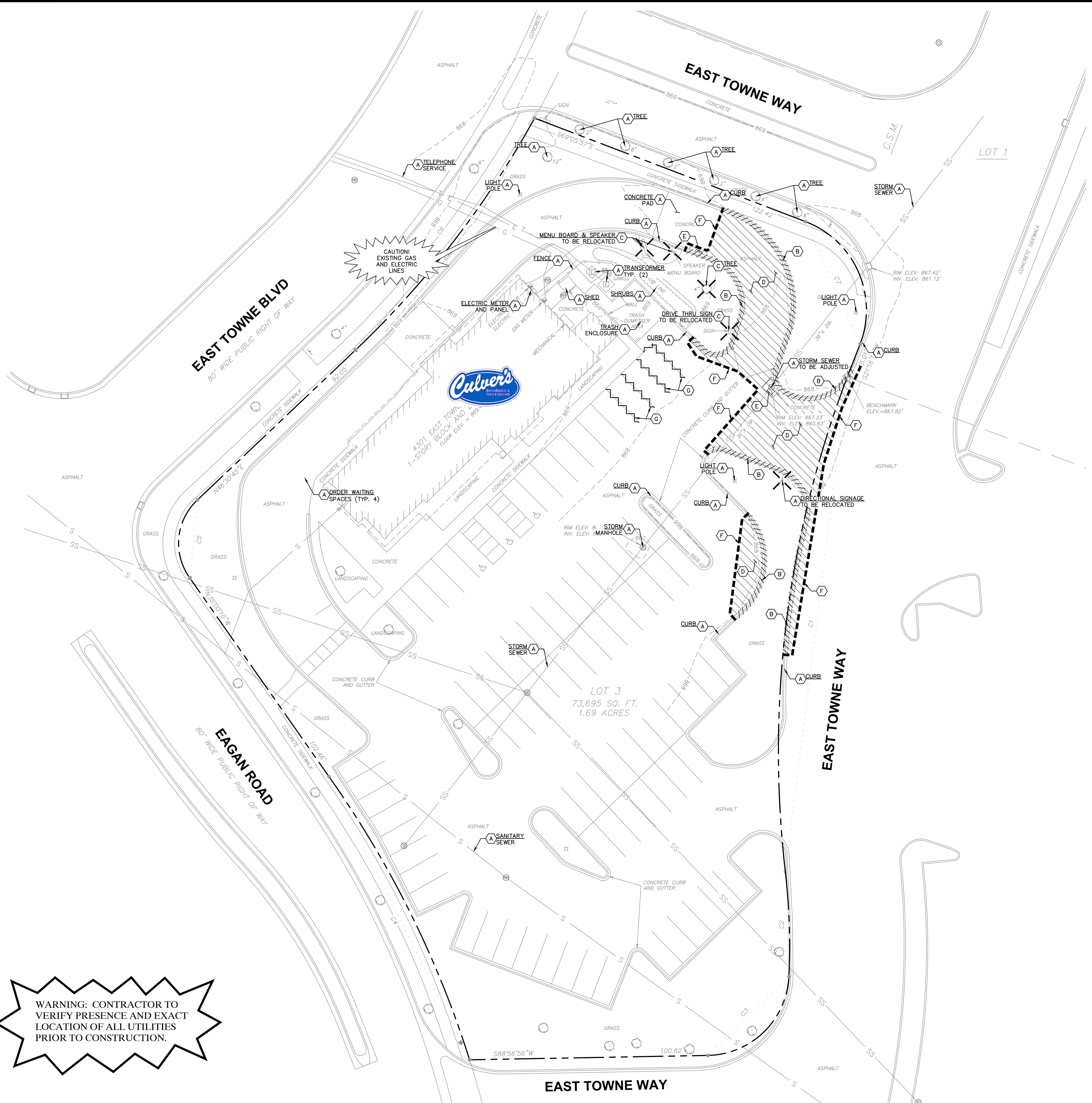
ORIGINAL ISSUE: 08/13/2021 KHA PROJECT NO. 268025002 SHEET NUMBER

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Drawing name: K:\GIS_LDEV\268025002\Culvers_Madison_MV_Design_VistaSheets\C1.0 - GENERAL NOTES AND DETAILS.dwg C1.0 May 02, 2022 11:10am by: Tomasz Gryniewski This document, together with the concepts and designs presented herein, is an instrument of service, and is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

Drawing name: K:\GIS\DEV\268025002\Culvers_Madison_MV2 Design\PlanSheets\C2.0 - EXISTING CONDITIONS AND DEMO PLAN.dwg C2.0 May 02, 2022 11:41am by: Troy.Coyne
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WARNING: CONTRACTOR TO VERIFY PRESENCE AND EXACT LOCATION OF ALL UTILITIES PRIOR TO CONSTRUCTION.



DEMOLITION NOTES

1. CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVAL OF THE EXISTING STRUCTURES, RELATED UTILITIES, PAVING, AND ANY OTHER EXISTING IMPROVEMENTS AS NOTED.
2. CONTRACTOR IS TO REMOVE AND DISPOSE OF ALL DEBRIS, RUBBISH AND OTHER MATERIALS RESULTING FROM PREVIOUS AND CURRENT DEMOLITION OPERATIONS. DISPOSAL WILL BE IN ACCORDANCE WITH ALL LOCAL, STATE AND/OR FEDERAL REGULATIONS GOVERNING SUCH OPERATIONS.
3. THE GENERAL CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT. THE CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR ANY DAMAGES TO THE ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASES OF THIS PROJECT. CONTRACTOR SHALL NOT DEMOLISH ANYTHING OUTSIDE THE OWNERS LEASE/PROPERTY LINE UNLESS SPECIFICALLY MENTIONED ON THIS SHEET.
4. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED UPON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES.
5. IF DEMOLITION OR CONSTRUCTION ON SITE WILL INTERFERE WITH THE ADJACENT PROPERTY OWNER'S TRAFFIC FLOW, THE CONTRACTOR SHALL COORDINATE WITH ADJACENT PROPERTY OWNER TO MINIMIZE THE IMPACT ON TRAFFIC FLOW. TEMPORARY RE-ROUTING OF TRAFFIC IS TO BE ACCOMPLISHED BY USING WISDOT APPROVED TRAFFIC BARRICADES, BARRELS, AND/OR CONES. TEMPORARY SIGNAGE AND FLAGMEN MAY BE ALSO NECESSARY.
6. REFER TO GEOTECHNICAL REPORT PROVIDED BY OTHERS FOR ALL SUBSURFACE INFORMATION.
7. CONTRACTOR SHALL BEGIN CONSTRUCTION OF ANY LIGHT POLE BASES FOR RELOCATED LIGHT FIXTURES AND RELOCATION OF ELECTRICAL SYSTEM AS SOON AS DEMOLITION BEGINS. CONTRACTOR SHALL BE AWARE THAT INTERRUPTION OF POWER TO ANY LIGHT POLES OR SIGNS SHALL NOT EXCEED 24 HOURS.
8. EROSION CONTROL MUST BE ESTABLISHED PRIOR TO ANY WORK ON SITE INCLUDING DEMOLITION.
9. THE EXTENT OF SITE DEMOLITION WORK IS AS SHOWN ON THE CONTRACT DOCUMENTS AND AS SPECIFIED HEREIN.
10. CONTRACTOR MUST RECEIVE APPROVAL FROM CIVIL ENGINEER AND GEOTECHNICAL ENGINEER FOR THE MATERIAL TYPE AND USE IF CONTRACTOR DESIRES TO REUSE DEMOLISHED SITE PAVEMENT AS STRUCTURAL FILL.
11. EXISTING UTILITIES, WHICH DO NOT SERVICE STRUCTURES BEING DEMOLISHED, ARE TO BE KEPT IN SERVICE AND PROTECTED AGAINST DAMAGE DURING DEMOLITION OPERATIONS. CONTRACTOR SHALL ARRANGE FOR SHUT-OFF OF UTILITIES SERVING STRUCTURES TO BE DEMOLISHED. CONTRACTOR IS RESPONSIBLE FOR TURNING OFF, DISCONNECTING, AND SEALING INDICATED UTILITIES BEFORE STARTING DEMOLITION OPERATIONS. EXISTING UTILITIES TO BE ABANDONED ARE TO BE CAPPED AT BOTH ENDS AND FILLED WITH F-1 OR APPROVED EQUAL. ALL UNDERGROUND UTILITIES TO BE REMOVED ARE TO BE BACKFILLED WITH ENGINEERED FILL OR SELECT EXCAVATED MATERIAL, AS APPROVED BY THE GEOTECHNICAL ENGINEER, TO 95% OF MODIFIED PROCTOR DENSITY WITHIN PAVED AREAS AND TO 90% OF MODIFIED PROCTOR DENSITY FOR GREEN SPACE AREAS, IN ACCORDANCE WITH THE EARTHWORK SPECIFICATIONS. ALL PRIVATE UTILITIES (ELECTRIC, CABLE, TELEPHONE, FIBER OPTIC, GAS) SHALL BE REMOVED AND RELOCATED PER THE UTILITY OWNER AND THE LOCAL MUNICIPALITY'S REQUIREMENTS.
12. UNDERGROUND UTILITIES SHOWN ARE BASED ON ATLASES AND AVAILABLE INFORMATION PRESENTED AT THE TIME OF SURVEY. CONTRACTOR SHOULD CALL 'DIGGERS HOTLINE' (1-877-500-9592) TO COORDINATE FIELD LOCATIONS OF EXISTING UNDERGROUND UTILITIES BEFORE ORDERING MATERIALS OR COMMENCING CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES IMMEDIATELY. CONTRACTOR SHALL LOCATE AND PROTECT EXISTING UNDERGROUND AND OVERHEAD UTILITIES DURING CONSTRUCTION. UTILITY PROTECTION SHALL BE COORDINATED WITH THE RESPECTIVE UTILITY OWNER AND AS DIRECTED BY THE GOVERNING MUNICIPALITY. DAMAGED CABLES/CONDUITS SHALL BE REPLACED IMMEDIATELY. ALL EXISTING STRUCTURES TO REMAIN SHALL BE PROTECTED THROUGHOUT THE CONSTRUCTION PROCESS. ALL DAMAGED STRUCTURES SHALL BE REPLACED IN-KIND AND THEIR REPLACEMENT COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. PROPER NOTIFICATION TO THE OWNERS OF THE EXISTING UTILITIES SHALL BE MADE AT LEAST 48 HOURS BEFORE CONSTRUCTION COMMENCES.
13. USE WATER SPRINKLING, TEMPORARY ENCLOSURES, AND OTHER SUITABLE METHODS TO LIMIT DUST AND DIRT RISING AND SCATTERING IN THE AIR TO THE LOWEST LEVEL, COMPLY WITH ALL GOVERNING REGULATIONS PERTAINING TO ENVIRONMENTAL PROTECTION. SEE EROSION CONTROL SHEETS FOR FURTHER EROSION CONTROL REQUIREMENTS.
14. COMPLETELY FILL BELOW-GRADE AREAS AND VOIDS RESULTING FROM DEMOLITION OF STRUCTURES TO THE FINAL LINES AND GRADES SHOWN ON THE CONTRACT DOCUMENTS. BACKFILL MATERIAL SHALL BE WISDOT APPROVED CRUSHED LIMESTONE OR APPROVED EQUAL. USE SATISFACTORY SOIL MATERIALS CONSISTING OF STONE, GRAVEL AND SAND, FREE FROM DEBRIS, TRASH, FROZEN MATERIALS, ROOTS AND OTHER ORGANIC MATTER. PRIOR TO PLACEMENT OF FILL MATERIALS, ENSURE THAT AREAS TO BE FILLED ARE FREE OF STANDING WATER, FROST, FROZEN MATERIAL, TRASH AND DEBRIS. PLACE FILL MATERIALS IN HORIZONTAL LAYERS NOT EXCEEDING 9" IN LOOSE DEPTH. COMPACT EACH LAYER AT OPTIMUM MOISTURE CONTENT OF FILL MATERIAL TO 95% OF MODIFIED PROCTOR DENSITY UNLESS SUBSEQUENT EXCAVATION FOR NEW WORK IS REQUIRED.

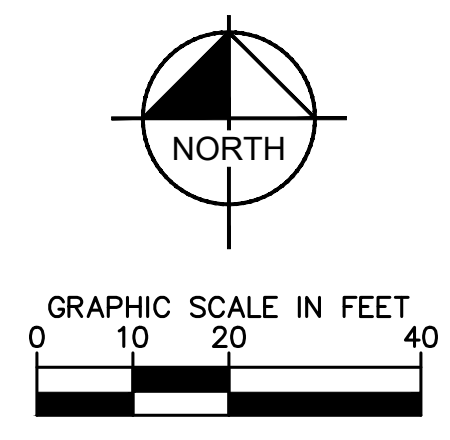
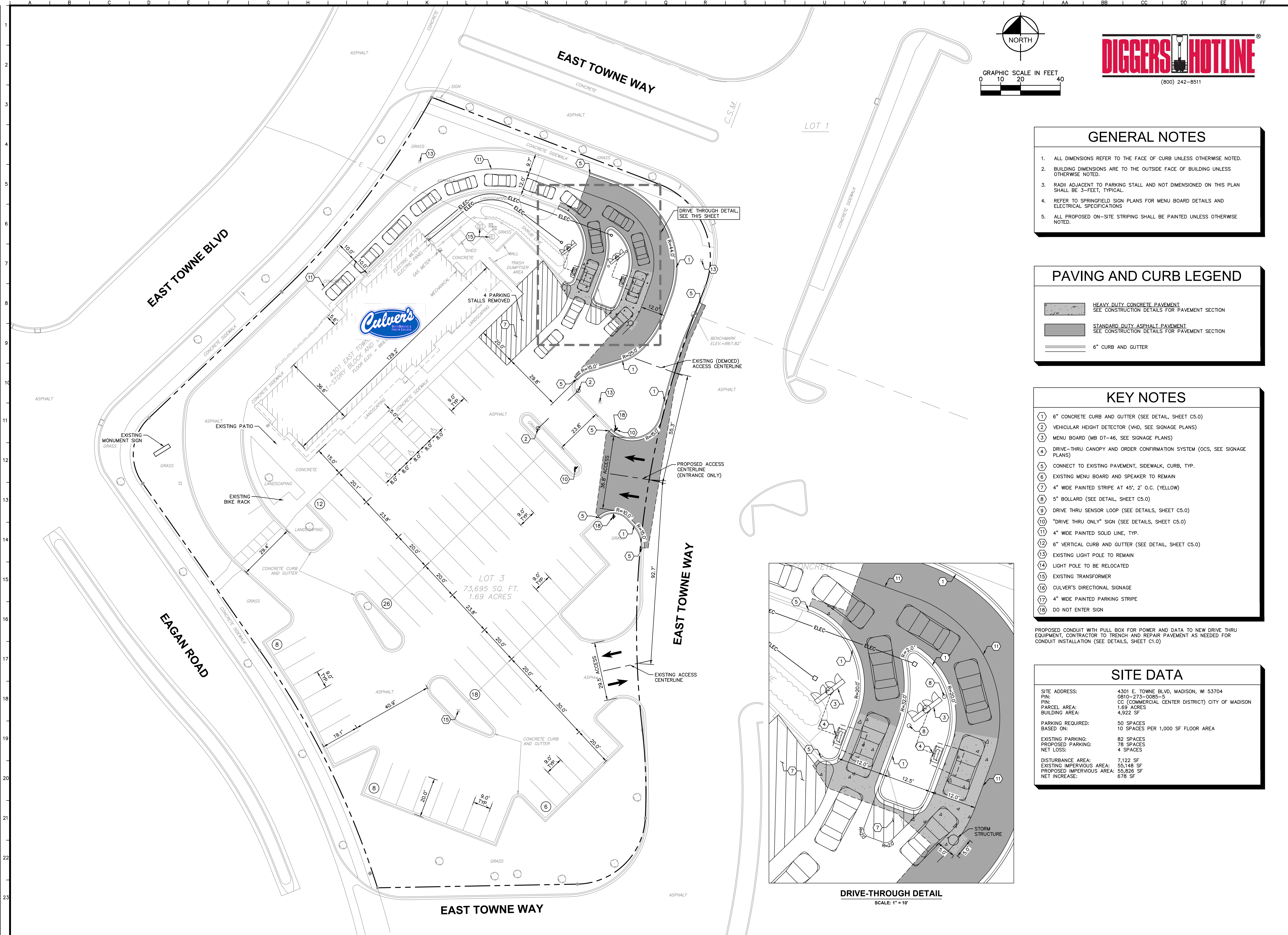
DEMOLITION LEGEND

- | | |
|-----|---|
| (A) | ITEM TO REMAIN, PROTECT DURING CONSTRUCTION |
| (B) | CURB REMOVAL |
| (C) | ITEM TO BE REMOVED |
| (D) | FULL-DEPTH ASPHALT PAVEMENT REMOVAL |
| (E) | CONCRETE REMOVAL |
| (F) | SAWCUT LINE |
| (G) | STRIPING REMOVAL |



NO.	DATE	BY
EXISTING CONDITIONS & DEMO PLAN		
CULVER'S DRIVE-THRU IMPROVEMENTS		
4301 E TOWNE BLVD MADISON, WISCONSIN 53704		
ORIGINAL ISSUE: 08/13/2021		
KHA PROJECT NO. 268025002		
SHEET NUMBER C2.0		

Drawing name: K:\GIS_DEVELOPMENT\268025002_Culvers_Madison_MV2 Design\PlanSheets\C3.0 - SITE PLAN.dwg C3.0 May 02, 2022 11:41am by: Travis Oyeswski
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- ### GENERAL NOTES
1. ALL DIMENSIONS REFER TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 2. BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 3. RADIi ADJACENT TO PARKING STALL AND NOT DIMENSIONED ON THIS PLAN SHALL BE 3'-FEET, TYPICAL.
 4. REFER TO SPRINGFIELD SIGN PLANS FOR MENU BOARD DETAILS AND ELECTRICAL SPECIFICATIONS
 5. ALL PROPOSED ON-SITE STRIPING SHALL BE PAINTED UNLESS OTHERWISE NOTED.

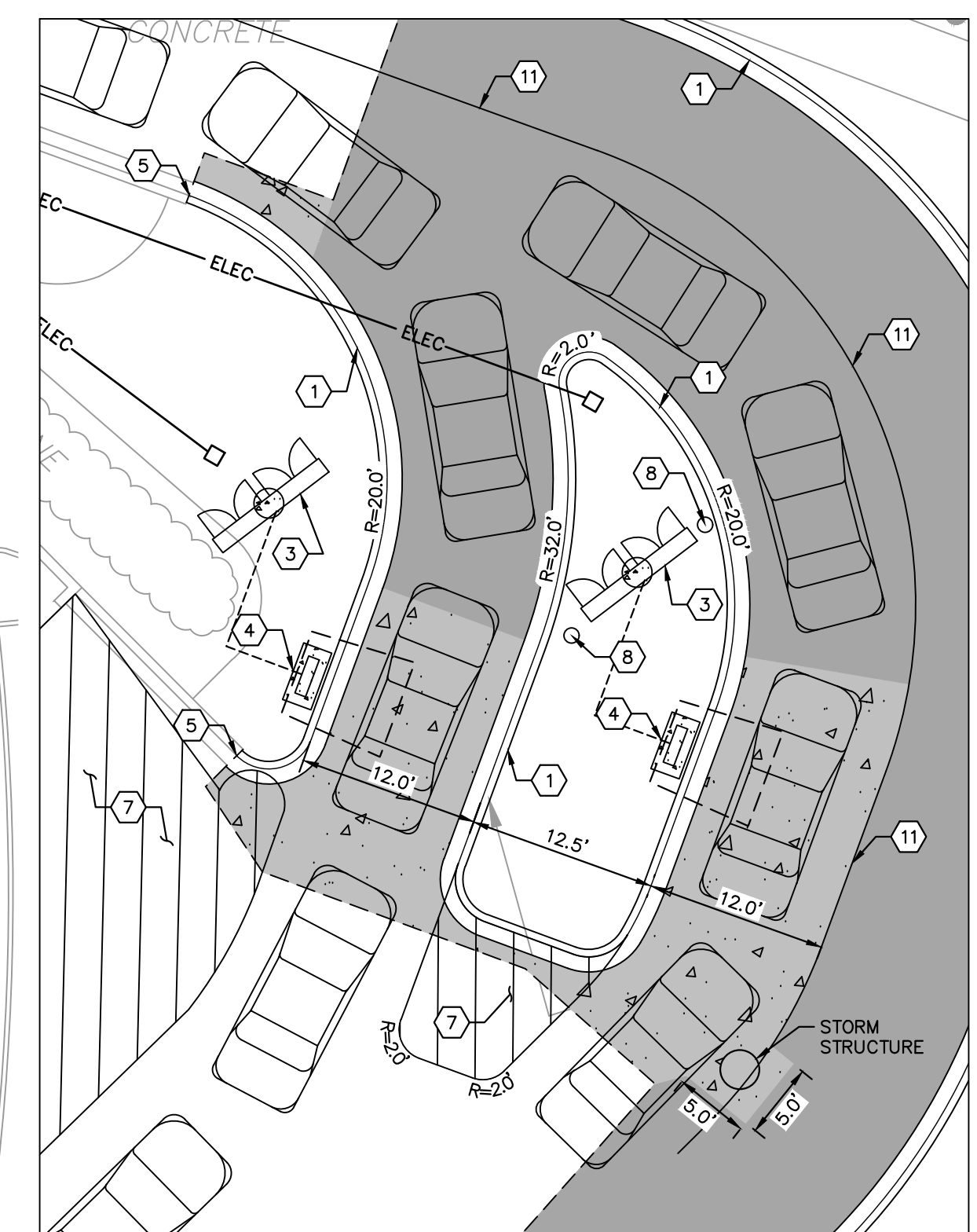
- ### PAVING AND CURB LEGEND
- HEAVY DUTY CONCRETE PAVEMENT
SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION
 - STANDARD DUTY ASPHALT PAVEMENT
SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION
 - 6" CURB AND GUTTER

- ### KEY NOTES
- 1 6" CONCRETE CURB AND GUTTER (SEE DETAIL, SHEET C5.0)
 - 2 VEHICULAR HEIGHT DETECTOR (VHD, SEE SIGNAGE PLANS)
 - 3 MENU BOARD (MB DT-46, SEE SIGNAGE PLANS)
 - 4 DRIVE-THRU CANOPY AND ORDER CONFIRMATION SYSTEM (OCS, SEE SIGNAGE PLANS)
 - 5 CONNECT TO EXISTING PAVEMENT, SIDEWALK, CURB, TYP.
 - 6 EXISTING MENU BOARD AND SPEAKER TO REMAIN
 - 7 4" WIDE PAINTED STRIPE AT 45°, 2' O.C. (YELLOW)
 - 8 5" BOLLARD (SEE DETAIL, SHEET C5.0)
 - 9 DRIVE THRU SENSOR LOOP (SEE DETAILS, SHEET C5.0)
 - 10 "DRIVE THRU ONLY" SIGN (SEE DETAILS, SHEET C5.0)
 - 11 4" WIDE PAINTED SOLID LINE, TYP.
 - 12 6" VERTICAL CURB AND GUTTER (SEE DETAIL, SHEET C5.0)
 - 13 EXISTING LIGHT POLE TO REMAIN
 - 14 LIGHT POLE TO BE RELOCATED
 - 15 EXISTING TRANSFORMER
 - 16 CULVER'S DIRECTIONAL SIGNAGE
 - 17 4" WIDE PAINTED PARKING STRIPE
 - 18 DO NOT ENTER SIGN

PROPOSED CONDUIT WITH PULL BOX FOR POWER AND DATA TO NEW DRIVE THRU EQUIPMENT, CONTRACTOR TO TRENCH AND REPAIR PAVEMENT AS NEEDED FOR CONDUIT INSTALLATION (SEE DETAILS, SHEET C1.0)

SITE DATA

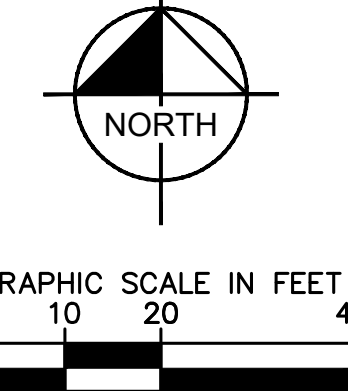
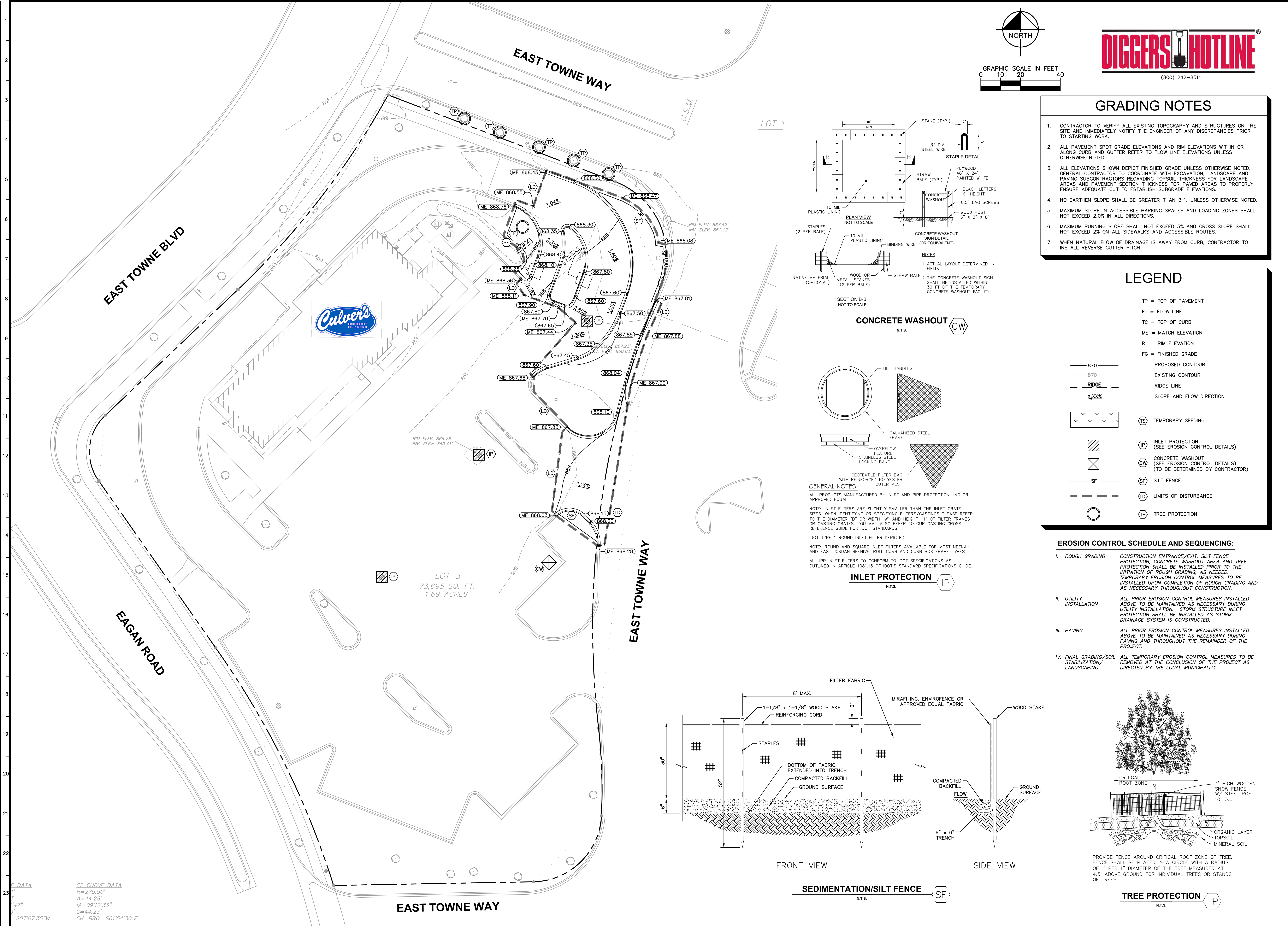
SITE ADDRESS:	4301 E. TOWNE BLVD, MADISON, WI 53704
PIN:	0810-273-0085-5
PARCEL AREA:	CC (COMMERCIAL CENTER DISTRICT) CITY OF MADISON
BUILDING AREA:	1.69 ACRES 4,922 SF
PARKING REQUIRED:	50 SPACES
BASED ON:	10 SPACES PER 1,000 SF FLOOR AREA
EXISTING PARKING:	82 SPACES
PROPOSED PARKING:	78 SPACES
NET LOSS:	4 SPACES
DISTURBANCE AREA:	7,122 SF
EXISTING IMPERVIOUS AREA:	55,148 SF
PROPOSED IMPERVIOUS AREA:	55,826 SF
NET INCREASE:	678 SF



DRIVE-THRU DETAIL
SCALE: 1" = 10'

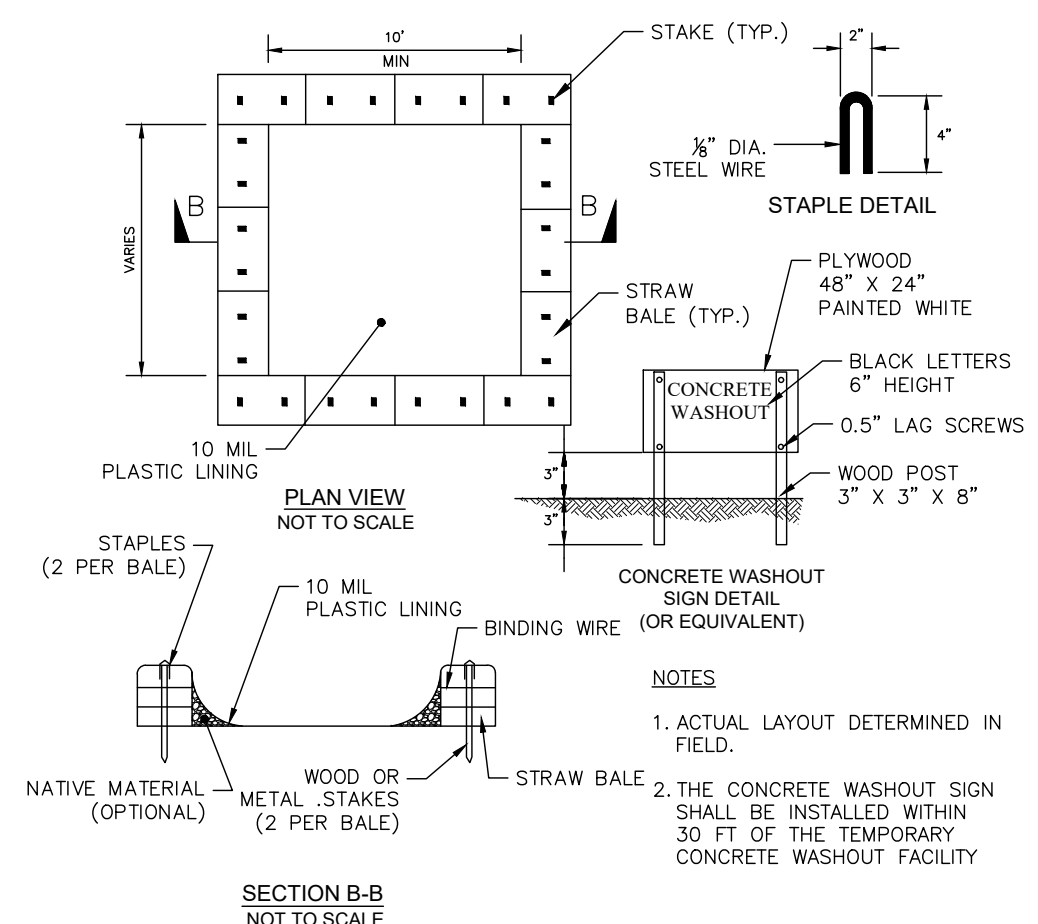
SITE PLAN	CULVER'S DRIVE-THRU IMPROVEMENTS
SCALE: AS NOTED DESIGNED BY: JPM DRAWN BY: ANA CHECKED BY: ANH	4301 E TOWNE BLVD MADISON, WISCONSIN 53704
ORIGINAL ISSUE: 08/13/2021	KHA PROJECT NO. 268025002
SHEET NUMBER C3.0	DATE REVISIONS

Drawing name: K:\CHS_LDEV\268025002_Culvers_Madison_MV2 Design\PlanSheets\C4.0 - GRADING AND EROSION CONTROL PLAN.dwg C4.0 May 02, 2022 11:41am By: Travis O'Connell
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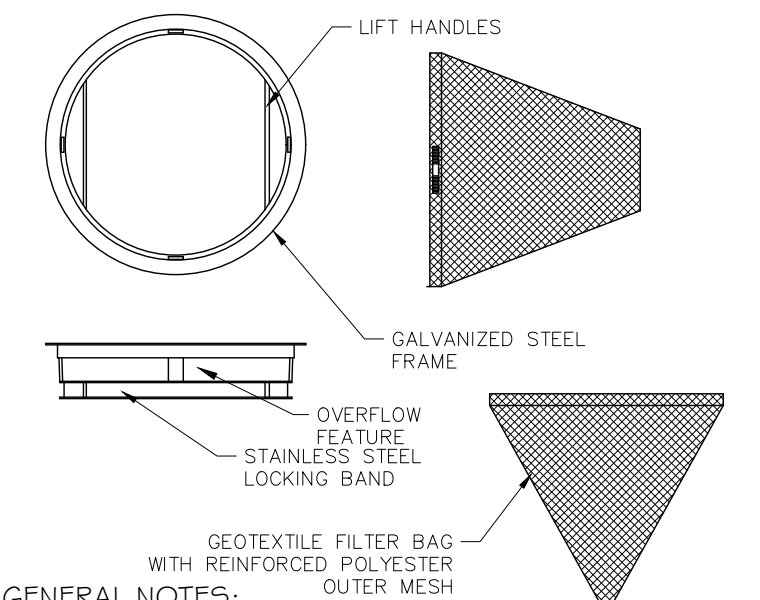


- ### GRADING NOTES
- CONTRACTOR TO VERIFY ALL EXISTING TOPOGRAPHY AND STRUCTURES ON THE SITE AND IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES PRIOR TO STARTING WORK.
 - ALL PAVEMENT SPOT GRADE ELEVATIONS AND RIM ELEVATIONS WITHIN OR ALONG CURB AND GUTTER REFER TO FLOW LINE ELEVATIONS UNLESS OTHERWISE NOTED.
 - ALL ELEVATIONS SHOWN DEPICT FINISHED GRADE UNLESS OTHERWISE NOTED. GENERAL CONTRACTOR TO COORDINATE WITH EXCAVATION, LANDSCAPE AND PAVING SUBCONTRACTORS REGARDING TOPSOIL THICKNESS FOR LANDSCAPE AREAS AND PAVEMENT SECTION THICKNESS FOR PAVED AREAS TO PROPERLY ENSURE ADEQUATE CUT TO ESTABLISH SUBGRADE ELEVATIONS.
 - NO EARTHEN SLOPE SHALL BE GREATER THAN 3:1, UNLESS OTHERWISE NOTED.
 - MAXIMUM SLOPE IN ACCESSIBLE PARKING SPACES AND LOADING ZONES SHALL NOT EXCEED 2.0% IN ALL DIRECTIONS.
 - MAXIMUM RUNNING SLOPE SHALL NOT EXCEED 5% AND CROSS SLOPE SHALL NOT EXCEED 2% ON ALL SIDEWALKS AND ACCESSIBLE ROUTES.
 - WHEN NATURAL FLOW OF DRAINAGE IS AWAY FROM CURB, CONTRACTOR TO INSTALL REVERSE GUTTER PITCH.

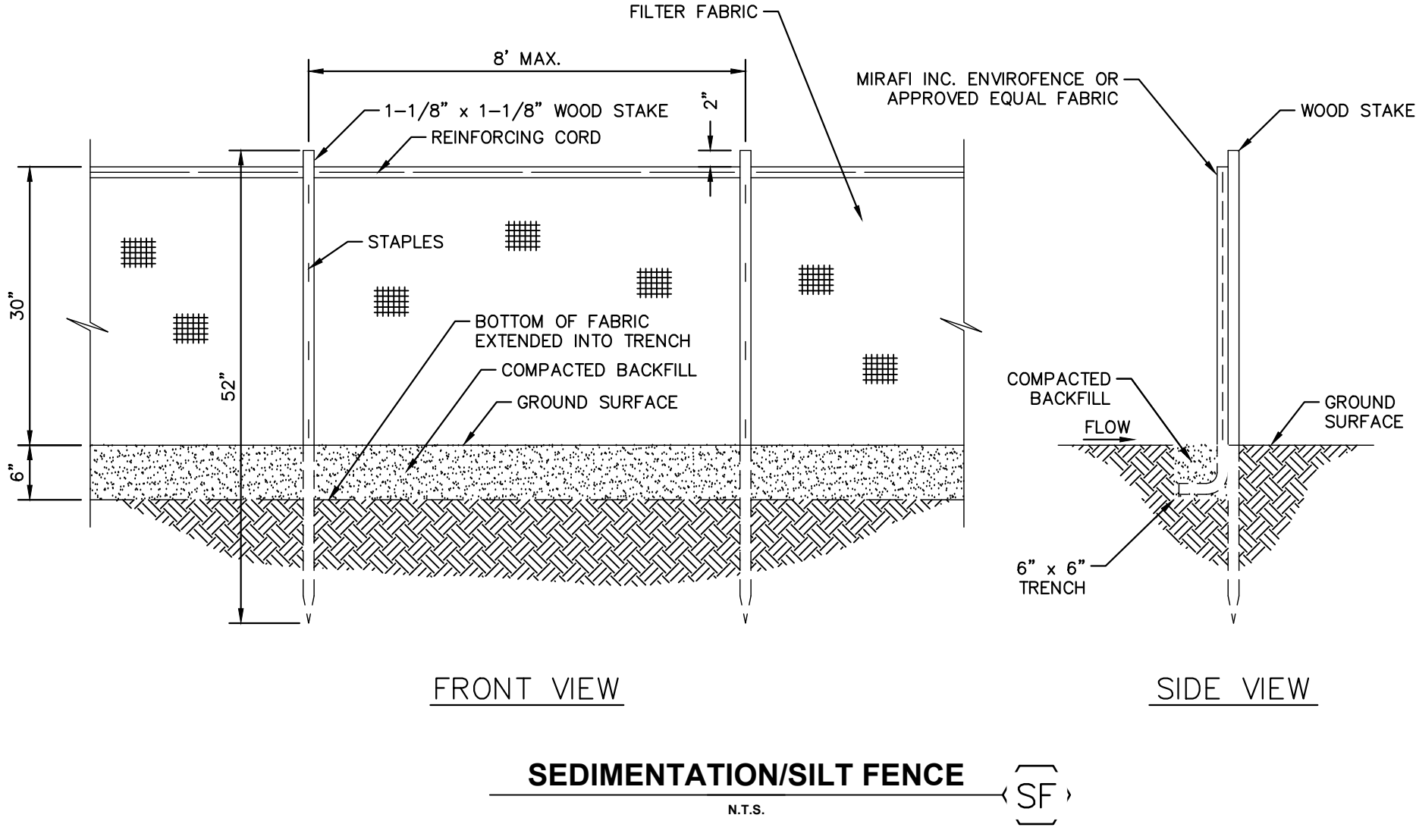
- ### LEGEND
- TP = TOP OF PAVEMENT
 - FL = FLOW LINE
 - TC = TOP OF CURB
 - ME = MATCH ELEVATION
 - R = RIM ELEVATION
 - FG = FINISHED GRADE
 - PROPOSED CONTOUR (870)
 - EXISTING CONTOUR (870)
 - RIDGE LINE (870)
 - SLOPE AND FLOW DIRECTION (X/XX%)
 - TEMPORARY SEEDING (TS)
 - INLET PROTECTION (SEE EROSION CONTROL DETAILS) (IP)
 - CONCRETE WASHOUT (SEE EROSION CONTROL DETAILS) (TO BE DETERMINED BY CONTRACTOR) (CW)
 - SILT FENCE (SF)
 - LIMITS OF DISTURBANCE (LD)
 - TREE PROTECTION (TP)



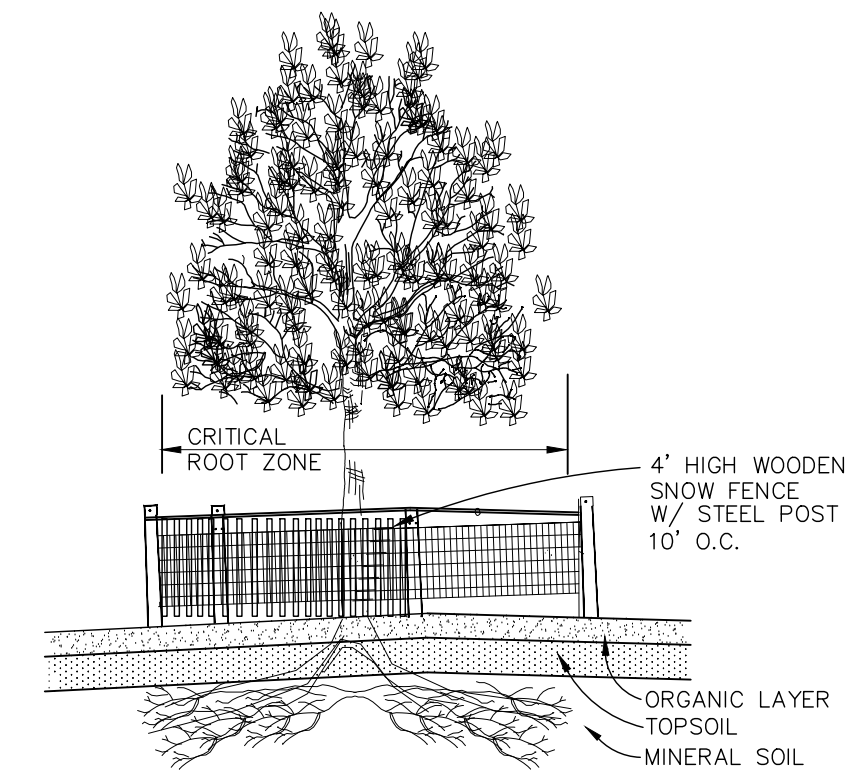
CONCRETE WASHOUT (N.T.S.)



INLET PROTECTION (N.T.S.)



SEDIMENTATION/SILT FENCE (N.T.S.)



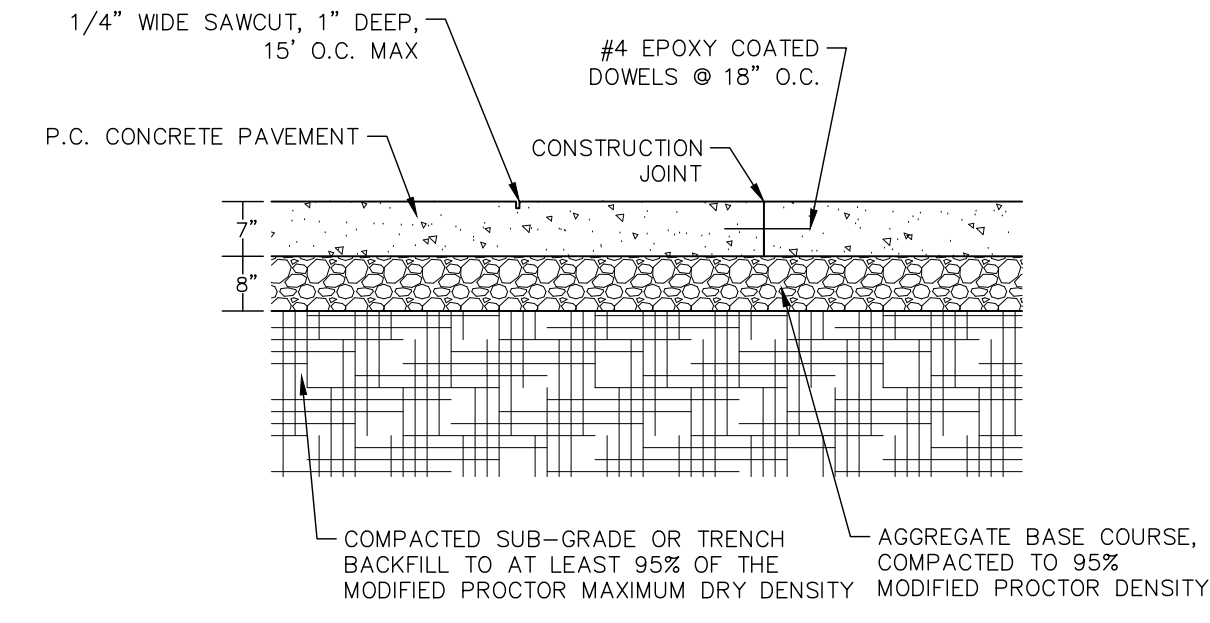
TREE PROTECTION (N.T.S.)

- ### EROSION CONTROL SCHEDULE AND SEQUENCING:
- ROUGH GRADING:** CONSTRUCTION ENTRANCE/EXIT, SILT FENCE PROTECTION, CONCRETE WASHOUT AREA AND TREE PROTECTION SHALL BE INSTALLED PRIOR TO THE INITIATION OF ROUGH GRADING, AS NEEDED. TEMPORARY EROSION CONTROL MEASURES TO BE INSTALLED UPON COMPLETION OF ROUGH GRADING AND AS NECESSARY THROUGHOUT CONSTRUCTION.
 - UTILITY INSTALLATION:** ALL PRIOR EROSION CONTROL MEASURES INSTALLED ABOVE TO BE MAINTAINED AS NECESSARY DURING UTILITY INSTALLATION. STORM STRUCTURE INLET PROTECTION SHALL BE INSTALLED AS STORM DRAINAGE SYSTEM IS CONSTRUCTED.
 - PAVING:** ALL PRIOR EROSION CONTROL MEASURES INSTALLED ABOVE TO BE MAINTAINED AS NECESSARY DURING PAVING AND THROUGHOUT THE REMAINDER OF THE PROJECT.
 - FINAL GRADING/SOIL STABILIZATION/LANDSCAPING:** ALL TEMPORARY EROSION CONTROL MEASURES TO BE REMOVED AT THE CONCLUSION OF THE PROJECT AS DIRECTED BY THE LOCAL MUNICIPALITY.

DATA
 C2 CURVE DATA
 R=275.50'
 A=44.28°
 IA=09°12'33"
 C=44.23'
 CH. BRG.=S01°54'30"E
 =S07°07'35"W

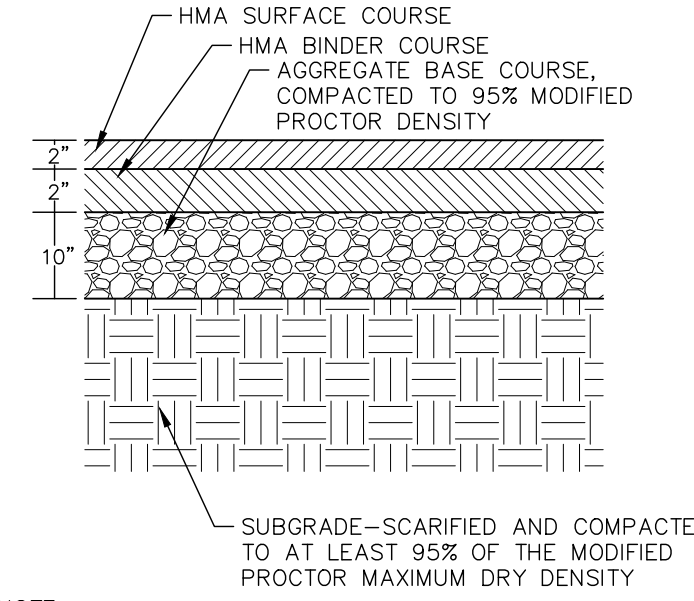
SCALE:	AS NOTED	DESIGNED BY: JPM	DRAWN BY: ANA	CHECKED BY: ANH	DATE:	BY:
GRADING & EROSION CONTROL PLAN						
CULVER'S DRIVE-THRU IMPROVEMENTS						
4301 E TOWNE BLVD MADISON, WISCONSIN 53704						
ORIGINAL ISSUE: 08/13/2021 KHA PROJECT NO. 268025002 SHEET NUMBER C4.0						

Drawing name: K:\CHS_DEVA\268025002_Culvers\PlanSheets\C5.0- CONSTRUCTION DETAILS.dwg C5.0 May 02, 2022 11:40am By: Travis Oyewski
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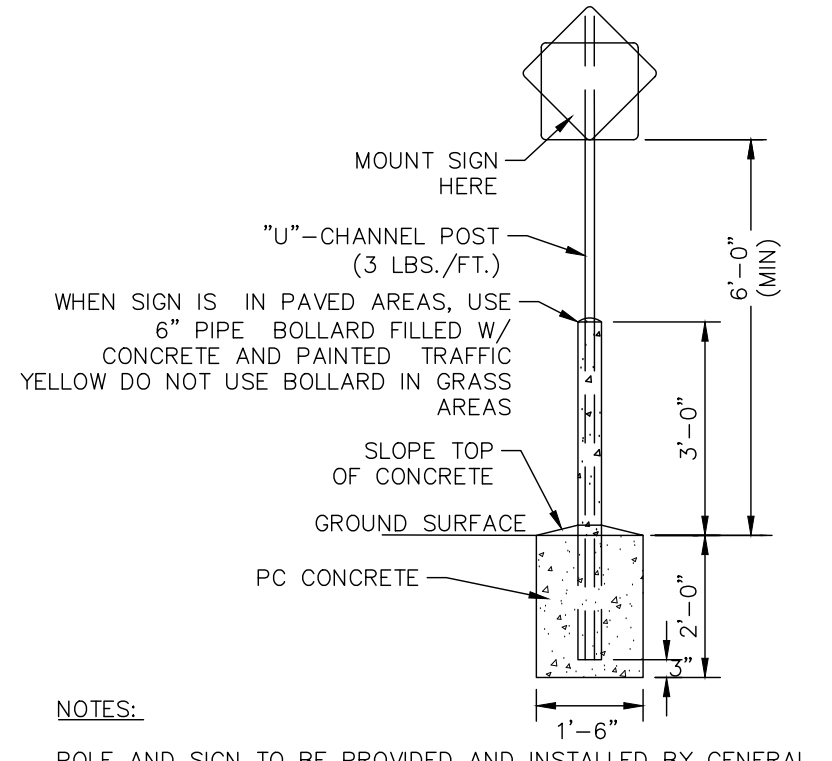
NOTE:
 PAVEMENT SECTION BASED ON GENERAL UNDERSTANDING OF PROJECT AREA. CONTRACTOR TO INSTALL RECOMMENDED SECTION OR MATCH EXISTING PAVEMENT SECTION, WHICHEVER IS GREATER. GEOTECHNICAL REPORT NOT PROVIDED BY OWNER

HEAVY DUTY CONCRETE PAVEMENT SECTION
 N.T.S.



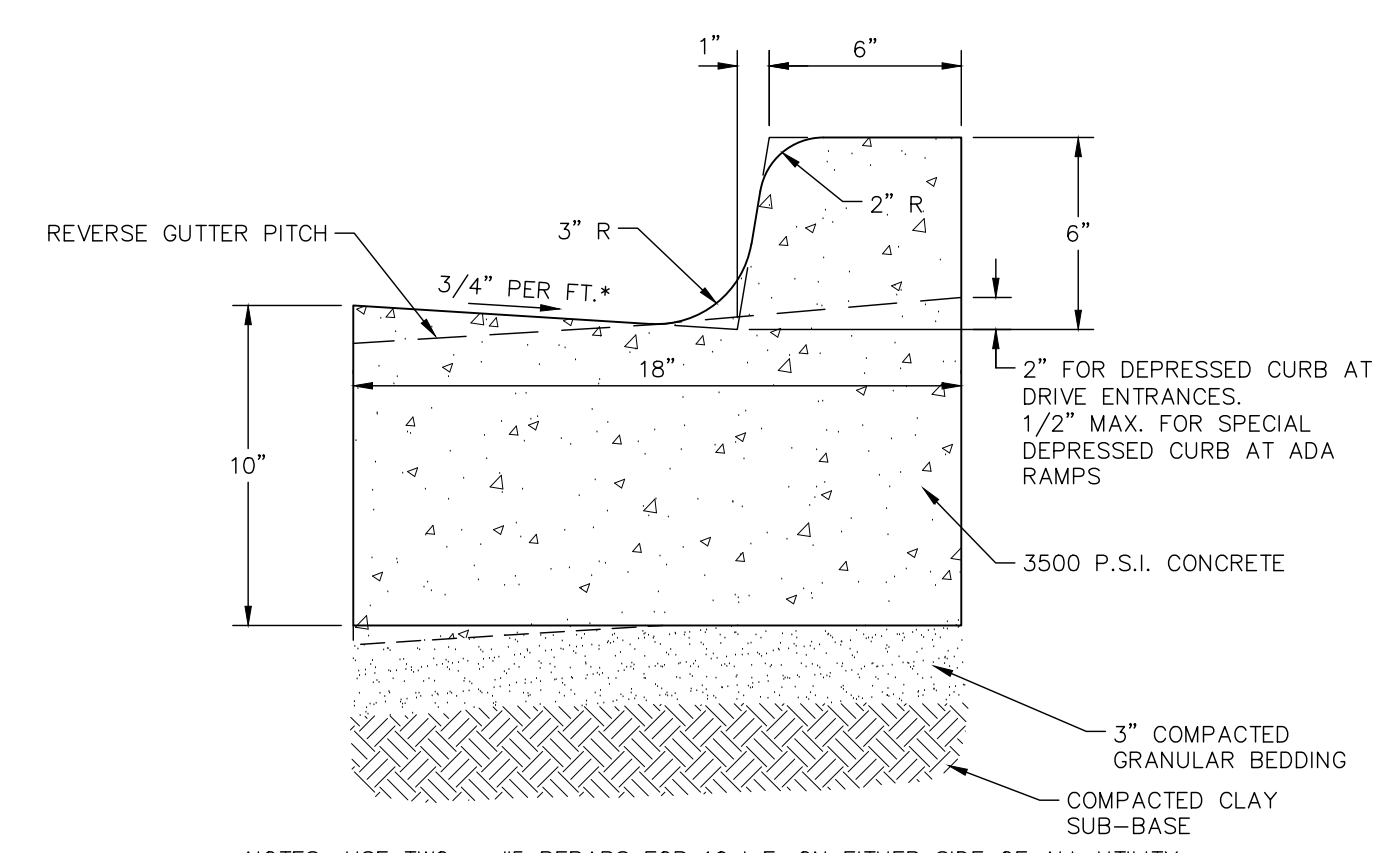
NOTE:
 PAVEMENT SECTION BASED ON GENERAL UNDERSTANDING OF PROJECT AREA. CONTRACTOR TO INSTALL RECOMMENDED SECTION OR MATCH EXISTING PAVEMENT SECTION, WHICHEVER IS GREATER. GEOTECHNICAL REPORT NOT PROVIDED BY OWNER

STANDARD DUTY ASPHALT PAVEMENT SECTION
 N.T.S. 10/19/20



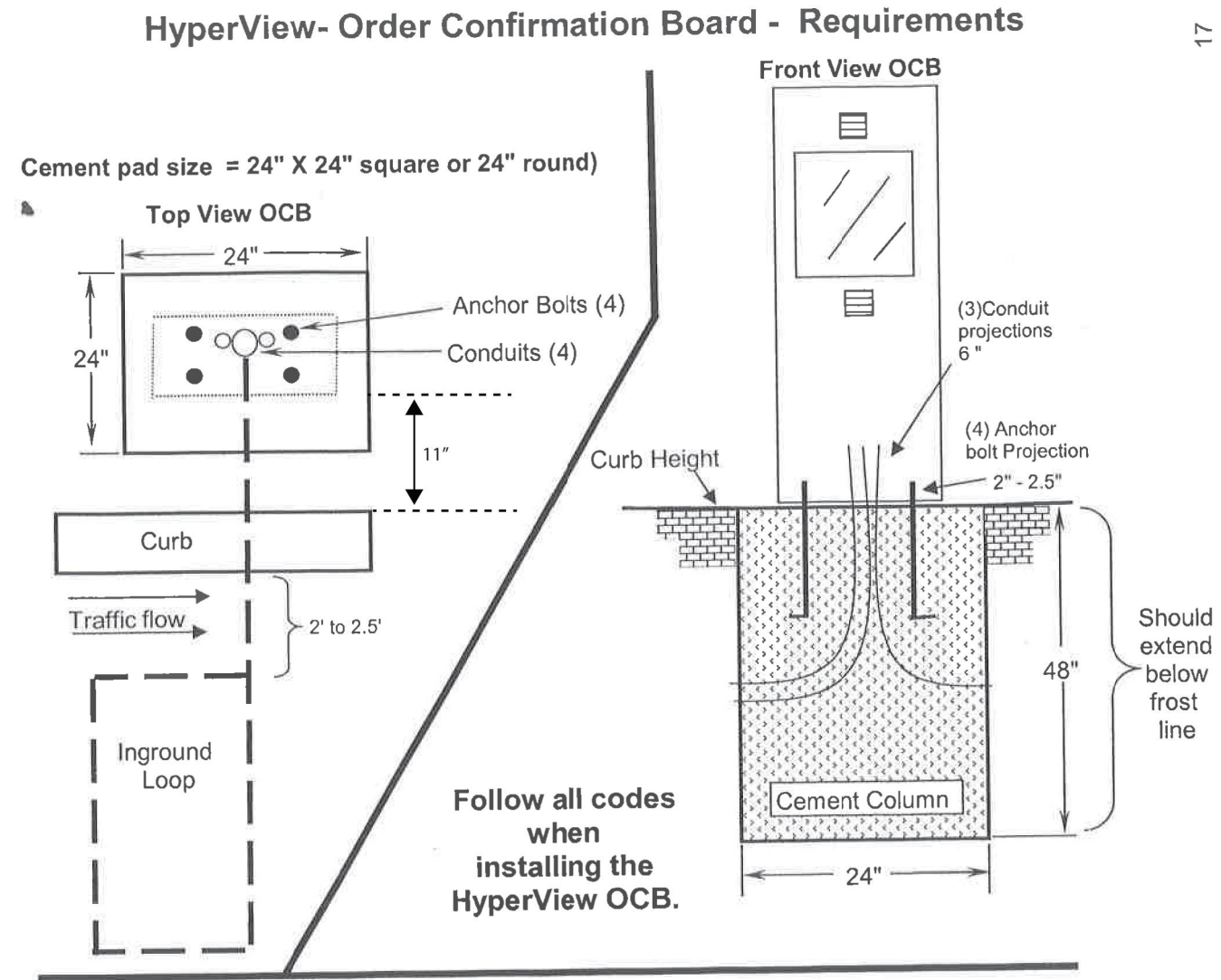
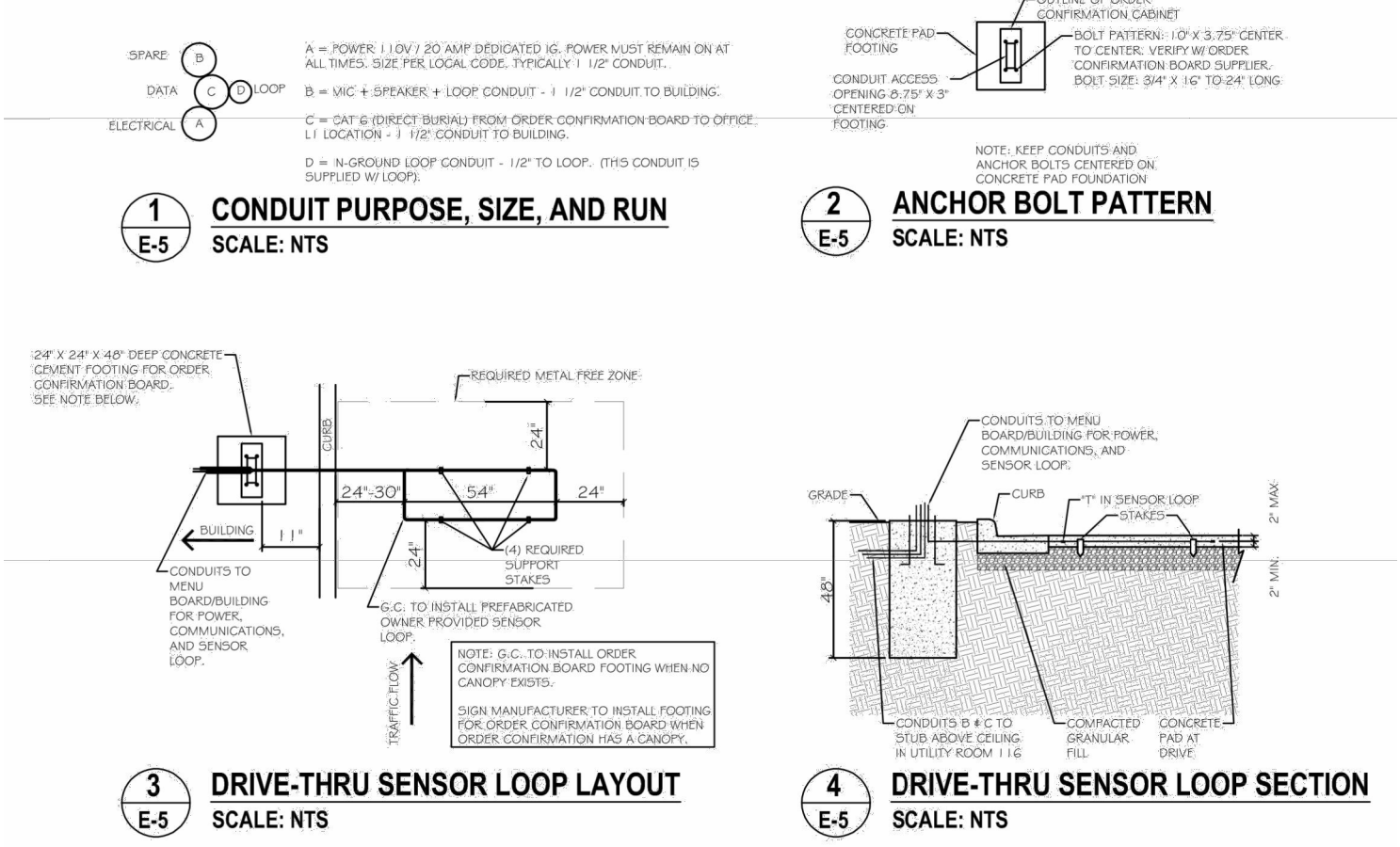
NOTES:
 POLE AND SIGN TO BE PROVIDED AND INSTALLED BY GENERAL CONTRACTOR.
 ALL SIGNS SHALL COMPLY WITH U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION'S "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES", LOCAL CODES AND AS SPECIFIED. MOUNT SIGNS TO POST IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS.

STANDARD SIGN BASE
 N.T.S.



NOTES:
 USE TWO #5 REBARS FOR 10 L.F. ON EITHER SIDE OF ALL UTILITY TRENCHES. INSTALL EXPANSION JOINTS EVERY 50 FT. AND SAW-CUT CONSTRUCTION JOINTS EVERY 25 FT.
 * MAXIMUM GUTTER SLOPE AT ADA RAMPS TO BE 5.0%

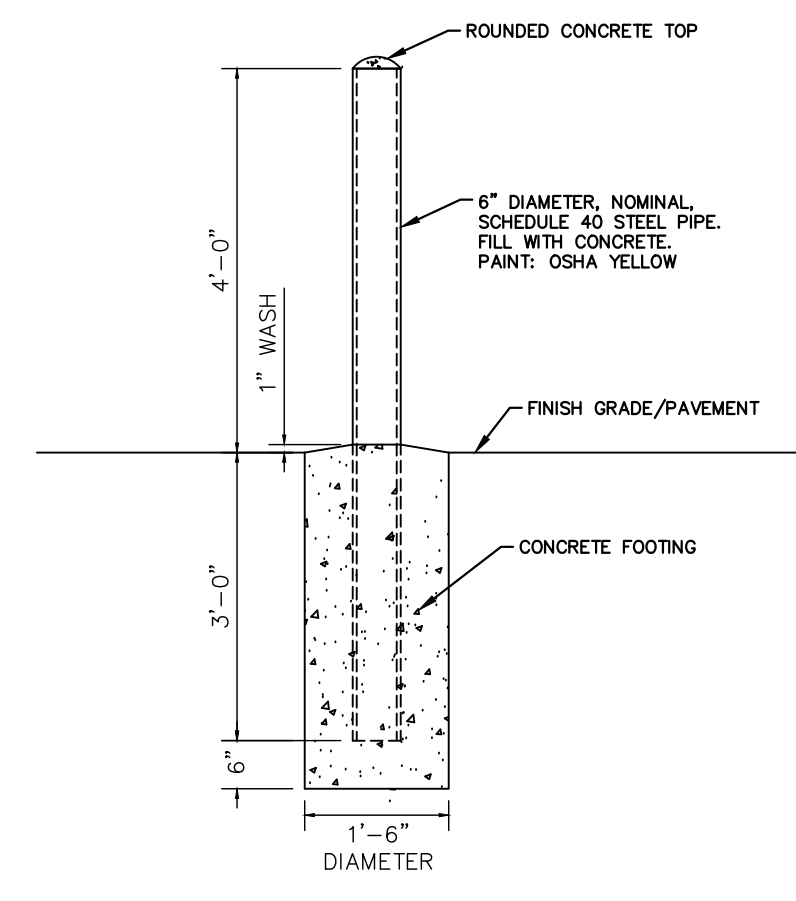
6-INCH CURB & GUTTER
 N.T.S.



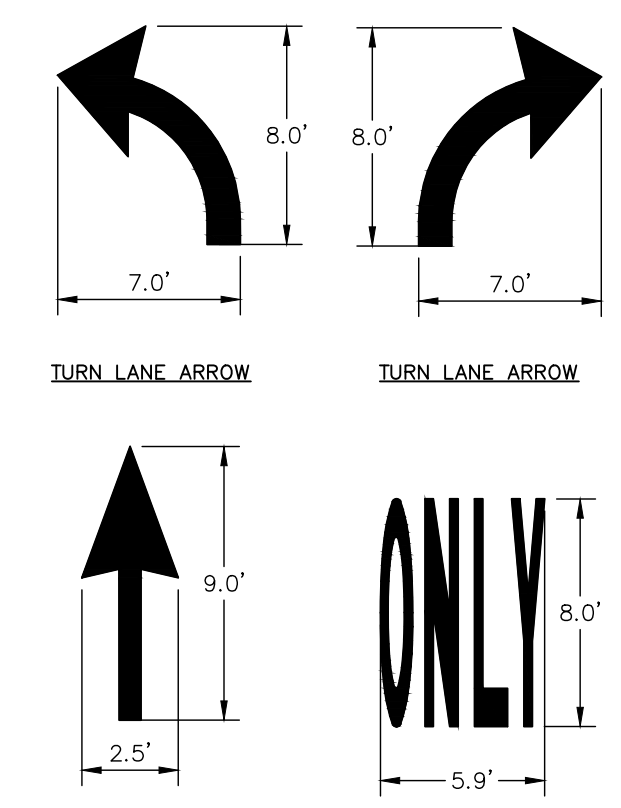
Conduits Purpose, Sizes (minimum), and Runs:
 A = Micspeakers' loop conduit - 3/4" to building.
 B = Cat 6 direct burial data conduit - 1" to building.
 C = Inground loop conduit - 1/2" to loop - (this conduit is supplied with loop).

Anchor Bolt:
 Bolt Pattern = 10" X 3.75" CC
 Bolt Size = 1/2" or 5/8" X 16" to 24" L

Important!
 Keep conduits and anchor bolts centered on cement pad.
 Be sure to place the menu board in a position that will not be obstructed by the Order Confirmation Board!



**6\"/>
 N.T.S.**



TRAFFIC FLOW ARROW
 N.T.S.

NO.	REVISIONS	DATE	BY

SCALE: AS NOTED
 DESIGNED BY: JPH
 DRAWN BY: ANA
 CHECKED BY: ANH



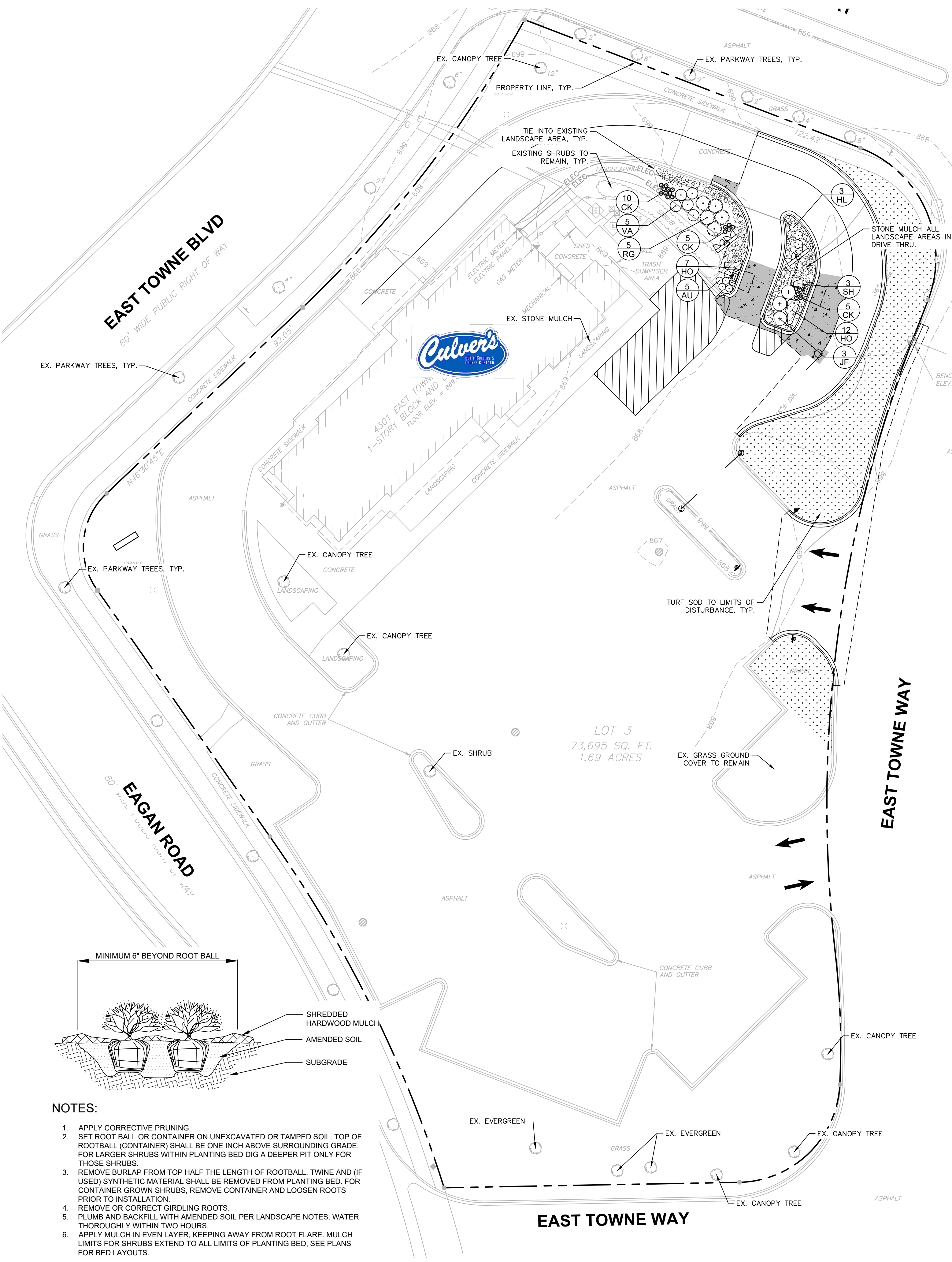
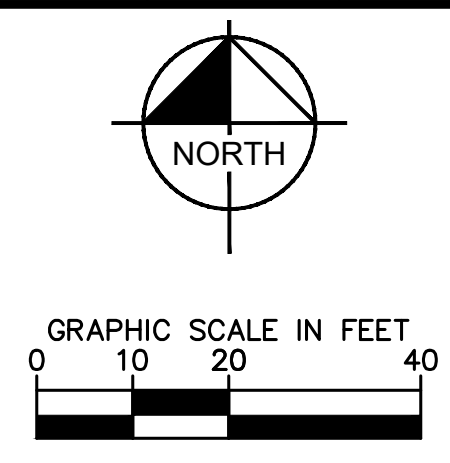
CONSTRUCTION DETAILS

CULVER'S DRIVE-THRU IMPROVEMENTS
 4301 E TOWNE BLVD
 MADISON, WISCONSIN 53704

ORIGINAL ISSUE: 08/13/2021
 KHA PROJECT NO. 268025002
 SHEET NUMBER

C5.0

Drawing name: K:\GIS\DEV\268025002_Culver's_Madison_MV2 Design\CAD\PlanSheets_V1.0 - LANDSCAPE PLAN.dwg L1.0 May 02, 2022 11:41am by: Trove,Cyreski
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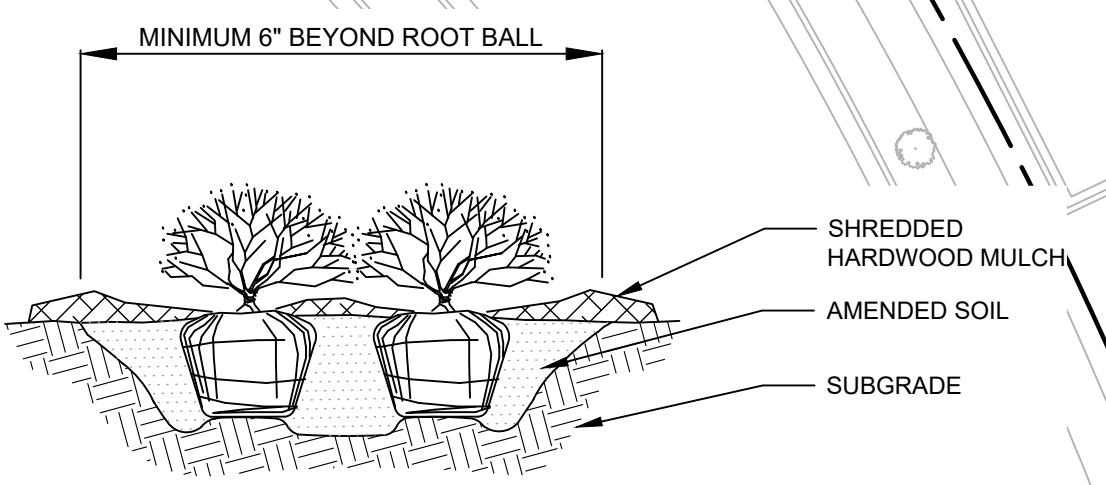


PLANT SCHEDULE

SHRUBS	CODE	QTY	BOTANICAL / COMMON NAME	CONT	SPACING	SIZE
	AU	5	ARONIA MELANOCARPA 'UCONNAM166' / LOW SCAPE HEDGER CHOKEBERRY	-	SEE PLAN	6" HT MIN
	HL	3	HYDRANGEA PANICULATA 'LITTLE LIME' / LITTLE LIME HYDRANGEA	3 GAL	SEE PLAN	18" HT MIN
	RG	5	RHUS AROMATICA 'GRO-LOW' / GRO-LOW FRAGRANT SUMAC	-	SEE PLAN	12" HT MIN
	VA	5	VIBURNUM DENTATUM 'CHRISTOM' / BLUE MUFFIN VIBURNUM	3 GAL	SEE PLAN	48" HT MIN
EVERGREEN SHRUBS	CODE	QTY	BOTANICAL / COMMON NAME	CONT	SPACING	SIZE
	JF	3	JUNIPERUS CHINENSIS 'SEA GREEN' / SEA GREEN JUNIPER	3 GAL	SEE PLAN	24" SPREAD
GRASSES	CODE	QTY	BOTANICAL / COMMON NAME	CONT	SPACING	SIZE
	CK	20	CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER' / FEATHER REED GRASS	1 GAL	SEE PLAN	
PERENNIALS AND GRASSES	CODE	QTY	BOTANICAL / COMMON NAME	CONT	SPACING	SIZE
	HO	19	HEMEROCALLIS X 'STELLA DE ORO' / STELLA DE ORO DAYLILY	1 GAL	18" OC	
	SH	3	SPOROBOLUS HETEROLEPIS / PRAIRIE DROPSEED	1 GAL	24" OC	
GROUND COVERS	CODE	QTY	BOTANICAL / COMMON NAME	CONT	SPACING	SIZE
			CM11 STONE MULCH			
			TURF SOD			

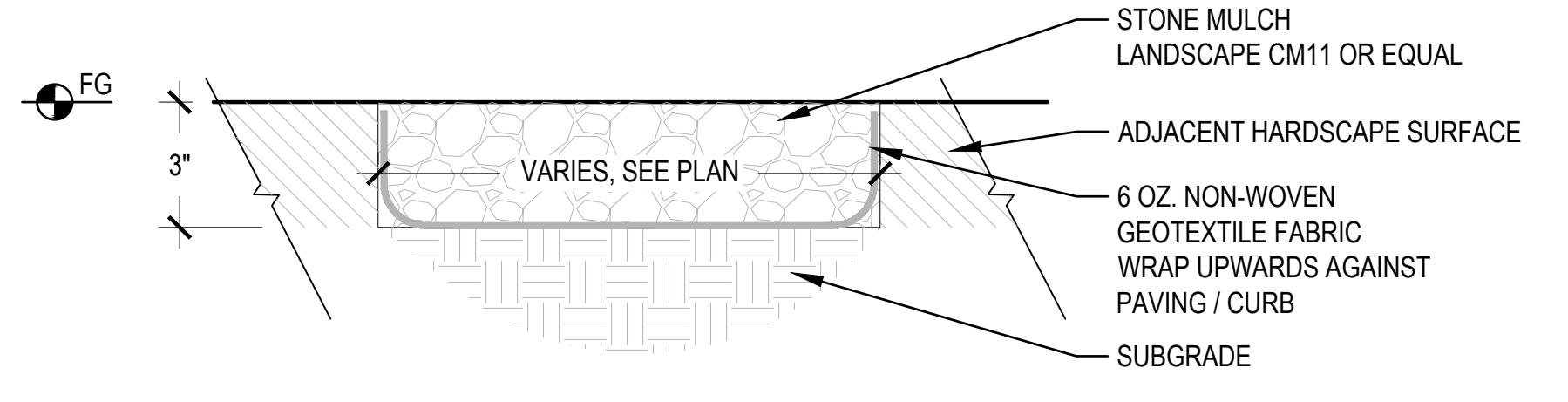
LANDSCAPE NOTES

- THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING MATERIALS AND PLANTS SHOWN ON THE LANDSCAPE PLAN. THE CONTRACTOR IS RESPONSIBLE FOR THE COST TO REPAIR UTILITIES, ADJACENT LANDSCAPE, PUBLIC AND PRIVATE PROPERTY THAT IS DAMAGED BY THE CONTRACTOR OR THEIR SUBCONTRACTOR'S OPERATIONS DURING INSTALLATION OR DURING THE SPECIFIED MAINTENANCE PERIOD. CALL FOR UTILITY LOCATIONS PRIOR TO ANY EXCAVATION.
- THE CONTRACTOR SHALL REPORT ANY DISCREPANCY IN PLAN VS. FIELD CONDITIONS IMMEDIATELY TO THE LANDSCAPE ARCHITECT, PRIOR TO CONTINUING WITH THAT PORTION OF WORK.
- NO PLANTING WILL BE INSTALLED UNTIL ALL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPAIR OF ANY OF THEIR TRENCHES OR EXCAVATIONS THAT SETTLE.
- ALL PLANTS TO BE SPECIMEN GRADE, WELL BRANCHED, HEALTHY, FULL, PRE-INOCULATED AND FERTILIZED. PLANTS SHALL BE FREE FROM DISEASE, PESTS, WOUNDS, AND SCARS. PLANTS SHALL BE FREE FROM NOTICEABLE GAPS, HOLES, OR DEFORMITIES. PLANTS SHALL BE FREE FROM BROKEN OR DEAD BRANCHES. TRUNKS WILL BE WRAPPED IF NECESSARY TO PREVENT SUN SCALD AND INSECT DAMAGE. THE LANDSCAPE CONTRACTOR SHALL REMOVE THE WRAP AT THE PROPER TIME AS PART OF THIS CONTRACT.
- THE OWNER'S REPRESENTATIVE MAY REJECT ANY PLANT MATERIALS THAT ARE DISEASED, DEFORMED, OR OTHERWISE NOT EXHIBITING SUPERIOR QUALITY.
- ALL NURSERY STOCK SHALL BE GUARANTEED, BY THE CONTRACTOR, FOR ONE YEAR FROM DATE OF FINAL INSPECTION. THE GUARANTEE BEGINS ON THE DATE OF THE LANDSCAPE ARCHITECT'S OR OWNERS WRITTEN ACCEPTANCE OF THE INITIAL PLANTING. REPLACEMENT PLANT MATERIAL SHALL HAVE A ONE YEAR GUARANTEE COMMENCING UPON PLANTING.
- PLANTS TO MEET AMERICAN STANDARD FOR NURSERY STOCK (ANSI Z60.1-2014 OR MOST CURRENT VERSION) REQUIREMENTS FOR SIZE AND TYPE SPECIFIED.
- PRUNE PLANTS AS NECESSARY - PER STANDARD NURSERY PRACTICE AND TO CORRECT POOR BRANCHING OF EXISTING AND PROPOSED TREES.
- TOPSOIL SHALL BE PROVIDED AND GRADED BY THE GENERAL CONTRACTOR UP TO 6 INCHES BELOW FINISHED GRADE IN TURF AREAS AND 18 INCHES IN PLANTING AREAS.
- PLANTING AREA TOPSOIL SHALL BE AMENDED WITH 25% SPHAGNUM PEATMOSS, 5% HUMUS AND 70% PULVERIZED SOIL FOR ALL NON TURF SEED MIX AREAS, SHRUB, ORNAMENTAL GRASS, PERENNIAL AND ANNUAL BEDS.
- SEED/SOD LIMIT LINES ARE APPROXIMATE. CONTRACTOR SHALL SEED/SOD ALL AREAS WHICH ARE DISTURBED BY GRADING WITH THE SPECIFIED SEED/SOD MIXES.
- EDGING TO BE A SPADED EDGE UNLESS INDICATED OTHERWISE ON THE PLANS. SPADED EDGE TO PROVIDE V-SHAPED DEPTH AND WIDTH TO CREATE SEPARATION BETWEEN MULCH AND GRASS. A SPADED BED EDGE SHALL SEPARATE MULCH BEDS FROM TURF OR SEEDED AREAS. A SPADED EDGE IS NOT REQUIRED ALONG CURBED EDGES.
- CONTRACTOR SHALL INSTALL SHREDDED HARDWOOD MULCH AT A 3" DEPTH TO ALL TREES, SHRUB, PERENNIAL, AND GROUND COVER AREAS. TREES PLACED IN AREA COVERED BY TURF SHALL RECEIVE A 4 FT WIDE MAXIMUM TREE RING WITH 3" DEPTH SHREDDED HARDWOOD MULCH.
- INSTALLATION OF TREES WITHIN PARKWAYS SHALL BE COORDINATED IN THE FIELD WITH LOCATIONS OF UNDERGROUND UTILITIES. TREES SHALL NOT BE LOCATED CLOSER THAN 5' FROM UNDERGROUND UTILITY LINES AND NO CLOSER THAN 10' FROM UTILITY STRUCTURES.
- DO NOT DISTURB THE EXISTING PAVING, LIGHTING, OR LANDSCAPING THAT EXISTS ADJACENT TO THE SITE UNLESS OTHERWISE NOTED ON PLAN.
- ALL DISTURBED AREAS TO BE SODDED OR SEEDED, UNLESS OTHERWISE NOTED. SOD/SEED SHALL BE LOCAL HARDY TURF GRASS MIX UNLESS, OTHERWISE NOTED.
- PLANT QUANTITIES SHOWN ARE FOR THE CONVENIENCE OF THE OWNER AND JURISDICTIONAL REVIEW AGENCIES. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL PLANT QUANTITIES AS DRAWN.



NOTES:

- APPLY CORRECTIVE PRUNING.
- SET ROOT BALL OR CONTAINER ON UNEXCAVATED OR TAMPED SOIL. TOP OF ROOTBALL (CONTAINER) SHALL BE ONE INCH ABOVE SURROUNDING GRADE. FOR LARGER SHRUBS WITHIN PLANTING BED DIG A DEEPER PIT ONLY FOR THOSE SHRUBS.
- REMOVE BURLAP FROM TOP HALF THE LENGTH OF ROOTBALL. TWINE AND (IF USED) SYNTHETIC MATERIAL SHALL BE REMOVED FROM PLANTING BED. FOR CONTAINER GROWN SHRUBS, REMOVE CONTAINER AND LOOSEN ROOTS PRIOR TO INSTALLATION.
- REMOVE OR CORRECT GIRDLING ROOTS.
- PLUMB AND BACKFILL WITH AMENDED SOIL PER LANDSCAPE NOTES. WATER THOROUGHLY WITHIN TWO HOURS.
- APPLY MULCH IN EVEN LAYER, KEEPING AWAY FROM ROOT FLARE. MULCH LIMITS FOR SHRUBS EXTEND TO ALL LIMITS OF PLANTING BED, SEE PLANS FOR BED LAYOUTS.



1 SHRUB PLANTING

2 CM11 STONE MULCH

3" = 1'-0"

No.	DATE	BY	 © 2021 KIMLEY-HORN AND ASSOCIATES, INC. 4201 WINFIELD ROAD, SUITE 600 MADISON, WI 53704 PHONE: 608-487-7550 WWW.KIMLEY-HORN.COM
SCALE:	DESIGNED BY:	DRAWN BY:	
AS NOTED	JPH	ANA	
			CHECKED BY: ANH
 BUILT FOR BUSINESS & FINANCIAL GROWTH			LANDSCAPE PLAN
CULVER'S DRIVE-THRU IMPROVEMENTS 4301 E TOWNE BLVD MADISON, WISCONSIN 53704			ORIGINAL ISSUE: 08/13/2021 KHA PROJECT NO. 268025002 SHEET NUMBER L1.0