

WARNER PATH RECONSTRUCTION AND BRIDGE REPLACEMENTS

As of March 13, 2013

SUMMARY OF PUBLIC COMMENTS TO DATE

The City has sought public comment in several ways throughout the development of the project. Engineering staff met with the Wild Warner advocacy group at their regular meeting on 12/04/12 and presented the project concept to a group of about 16, including Alder Weier. In general there seemed to be support for the project as a whole, with reservations about specific issues.

In February the City published a description and map of the proposed project on the Bike Madison portal of our website with links to this on Parks home page. This gives people a convenient way to submit comments, which are posted to the site along with our responses. Seven comments have been received to date, four in support of the project, one supporting with concerns about aesthetics and two opposed to removing the existing bridges or any trees to preserve the existing character of the park. These comments are attached to this summary.

About 2000 meeting notices were mailed or emailed to nearby residents, neighborhood associations and other potential stakeholders. Approximately 20 people attended the meeting, including Alders Rhodes-Conway and Weier. City staff presented the project scope with exhibits illustrating the existing deteriorated conditions of the path and bridges, potential design options for the new bridges, and they answered questions and solicited input.

Most participants appeared to be in general support of the project, including path width and aesthetic treatment of the new bridge(s), but with specific reservations. The major concerns expressed were as follows:

- Protection of the existing wooden bridges and trees.
- The lack of a dedicated pedestrian route between the shelter and the community center.
- Increasing the path width / impervious area in the park.
- Minimizing conflicts between pedestrians and bikes.
- The need to dredge the inlets to improve fish habitat.

Suggestions included:

- Leaving the south bridge in place as a dedicated pedestrian route.
- Routing the bike path around the northern inlet area to provide a route between the shelter and the community center and also preserve the existing north bridge.
- Preserve the existing character of the park by locating the path close to the lagoon.

About five written comments were received at or following the meeting, most in favor of routing the bike path between the shelter and the community center for the reasons above (some of these comments were received from the same individuals that commented at the Wild Warner meeting, the Public Information meeting and in writing).

Alternative 1:

Alternative 1 is the original proposal for the Warner Path Reconstruction and Bridge Replacements project as shown in the materials that have been posted on the website. This alternative provides a significantly more direct route for bicyclists and others using the path for transportation. Alternate 1 includes the removal of both wooden bridges due to their deteriorated condition and one large tree (40-inch cottonwood) growing around one of the bridge abutments; potentially 2-3 small cottonwood trees may need to be removed as well. It provides a better aesthetic experience for bicyclists and pedestrians as it continues to use the existing route adjacent to the lagoons.

It increases the total impervious area in the park by about 7500 SF, mainly due to the change in width from 8' to 10' wide. (See Table 1 for a summary of lengths, areas and costs of the two alternatives). The route utilizes the existing path alignment east of the shelter and reduces the bike path traffic directly to the shelter, without limiting access to the shelter. Alternate 1 would also include the installation of a sidewalk for pedestrians between the shelter and the community center, along the parking lot, and shifting the path closer to the lagoon on the northern segment, both to address neighborhood concerns that were raised through the public input process.

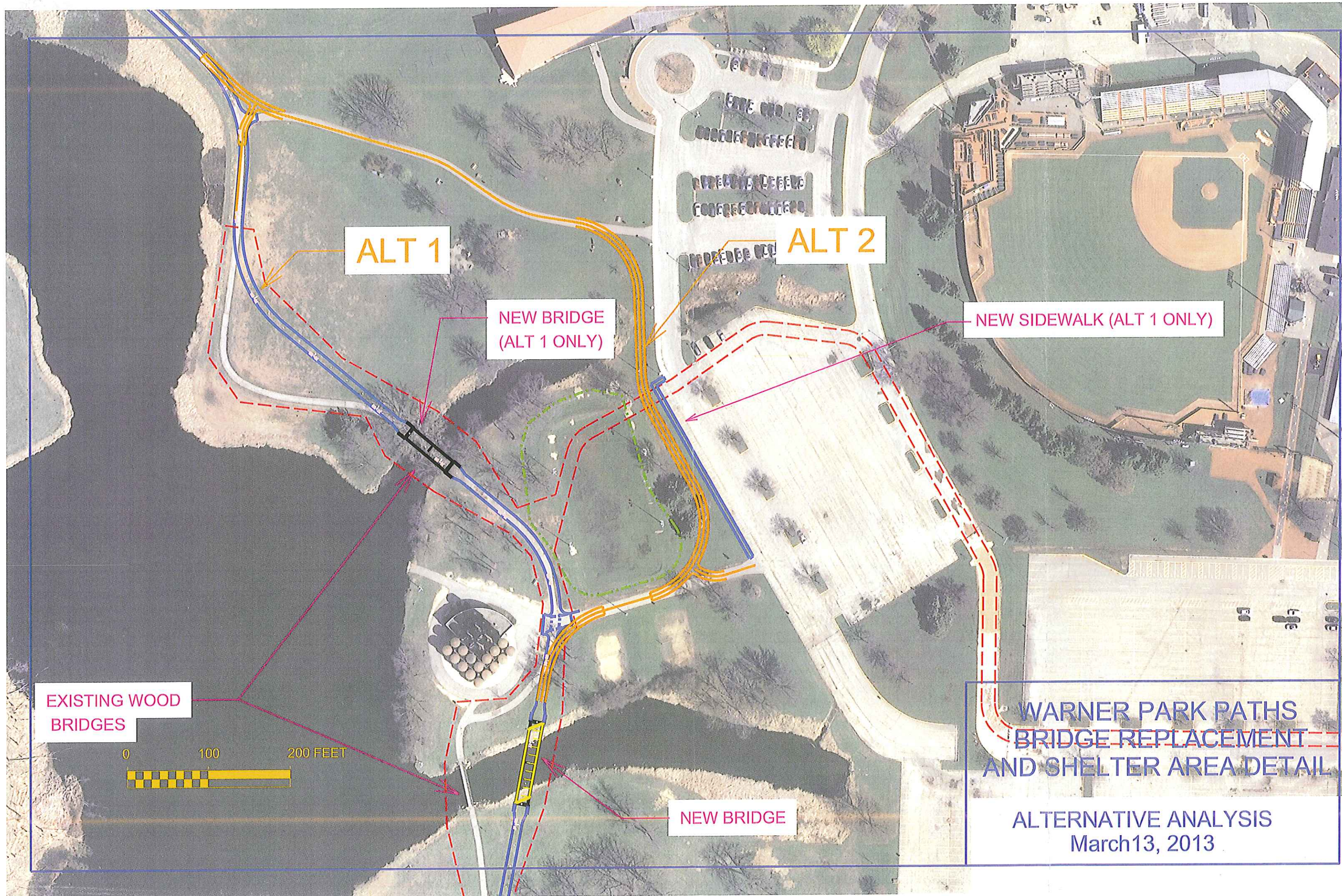
Initial construction costs (from Engineering budget) would be about \$124,500 more than Alternat 2, primarily due to the replacement of the north bridge in this alternative. However, this public expense would be partially if not entirely offset in the long run by eliminating the need to maintain the existing deteriorating wooden bridge. Due to the deteriorating condition of these bridges, the Parks Division has had to repair/replace boards on each bridge 3 out of the last 4 years and remove/replace the asphalt approaches to the bridges to remove tripping hazards.

Alternative 2:

This alternative adds the greatest distance (around 290 feet of asphalt path) to the route for bicyclists or others using the path for transportation. Since it also leaves the existing path and bridges in place it raises greater concern that some bicyclists will continue to use the shorter (and more scenic) route, creating redundant facilities and diminishing the value of the improvement. It significantly increases the total impervious area in the park by about 10,000 SF or 33% more than Alternate 1. It generally skirts the activity area near the rink with most bicycle traffic, though it subsequently reduces this open area somewhat and provides a potential conflict between pedestrians moving from activities in the open area to the parking lot. It also would route commuter bike traffic through the main activity area for the community center immediately southwest of the building.

It would complete a hard surface route for pedestrians between the shelter and the community center on new path, addressing a neighborhood concern but to some extent making a segment of existing sidewalk redundant. It appears to not require additional tree removals in the area near the rink area, but this has not been fully evaluated.

Initial construction costs (from Engineering budget) would be about \$124,500 less than Alternate 1, primarily by eliminating a need for a new north bridge. However, it would require Parks to maintain, repair and eventually replace the existing north bridge, partially if not entirely offsetting the savings in the long run.



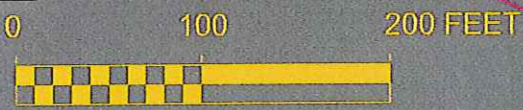
ALT 1

ALT 2

NEW BRIDGE
(ALT 1 ONLY)

NEW SIDEWALK (ALT 1 ONLY)

EXISTING WOOD
BRIDGES



NEW BRIDGE

WARNER PARK PATHS
BRIDGE REPLACEMENT
AND SHELTER AREA DETAIL

ALTERNATIVE ANALYSIS
March 13, 2013

WARNER PARK PATHS - ALTERNATIVES ANALYSIS

TABLE 1

3/13/2013 18:06

Rev:

CATEGORY	UNITS	ALT 1	ALT 2	PERCENT CHANGE
ROUTE LENGTH STA 32+50 TO 48+20	FT	958	1245	
ADDITIONAL ROUTE LENGTH	FT	0	287	29.96%
LENGTH NEW 10' MAIN PATH	FT	958	802	
LENGTH NEW CONNECTIONS	FT	15	178	
TOTAL NEW PATH	FT	973	980	
EXISTING 10' PATH TO REMAIN	FT	765	483	
EXISTING 8' PATH TO REMAIN	FT	0	815	
NEW 5' WIDE CONC. SIDEWALK	LF	250	0	
TOTAL IMPERVIOUS IN STUDY AREA	SF	18630	21150	
INCREASE IN IMPERVIOUS FROM EXISTING	SF	7500	10020	33.60%
CONSTRUCTION COST				
New 10' path @	/LF	\$58,380	\$58,800	
New South Bridge 95'	L SUM	\$130,000	\$130,000	
New North Bridge 65'	L SUM	\$110,000	\$0	
New Concrete Sidewalk	/SF	\$5,000	\$0	
Remove Path & North Bridge	L SUM	\$10,000	\$0	
TOTAL ALT COST IN STUDY AREA		\$313,380	\$188,800	
COST DIFFERENCE BETWEEN ALTERNATIVES			-\$124,580	

* Alternative 2 leaves 815 ft of path and one pedestrian bridge that will need to be replaced. This public expense would be partially if not entirely offset in the long run by eliminating the need to maintain the existing deteriorating wooden bridge.

Public Comments Received on City Bike Madison Website

Posted: 02/21/2013

Thank you for re-doing this area! I currently avoid biking Warner and add 5 minutes to my commute b/c of the conditions.

As a biker, I would like the bridges wider (it sounds like they will be, but how wide?). When there are folks fishing, it's hard to get by. I do like the arched look, so if enough arch can stay for aesthetics and still stay ADA compliant, that'd be great.
See you at the meeting.

City Staff Response:

We are proposing to have 14 foot horizontal clearance on the bridges to accommodate the 10' path and 2' wide shoulders. The extra width gives bikes some "shy distance" from the railings and provides a more comfortable experience when bikes and pedestrians or people fishing share the bridge. We are proposing a "bowstring" shape for the bridge, which will give an arched appearance to the bridge while keeping the path surface relatively flat, for aesthetics and to achieve ADA compliance.

Posted: 02/20/2013

Thank you for making this a priority this year; repaving of the paths will be a great improvement! As a bicyclist who rides through Warner Park on a daily basis (not during snow/ice season), I appreciate the attention you will be giving to replacing the wooden bridges and making the paths wider to accommodate bikes and pedestrians. I have called in repair requests on the bridges before when slats had crumbled or been removed; the entry onto the bridges is a hard bump and I am glad the new bridges will be ADA compliant. More room on the bridges will make it safer for all as well.

I ride through Warner Park as it is safer than being on the streets as I transport my child to school; it is a beautiful ride and I enjoy seeing the wildlife at different times of the year. Thank you for posting the plans.

City Staff Response:

Thanks for your comment.

Posted: 02/20/2013

I would try to flatten them out. Why do they need to be humped up so much?
Also putting a bench to sit on would be nice.

City Staff Response:

The new bridges being proposed would be flatter, providing smoother ride for bikes and less-steep grades for pedestrians and handicapped users. A bench could be considered.

Posted: 02/19/2013

I oppose any visual changes to the "Warner Park Bridges Look" and believe that increasing width and a cement based structure will certainly do just that.

Nevertheless I will remain open and come to the forum to see the proposed "look".

I do not believe that this part of the bike path would be heavily enough travelled by bikes for justifying such change. I would like to see numbers of how many bike riders actually use this path on a daily basis to go to work.

This trail is traditionally used in the majority by pedestrians and runners who want to enjoy one of the last romantic places in town and want to relax.

The present simplicity of the structures and the beautiful overarching trees close by provide calm and a rather slowing down time kind of effect due to it's tightness and roughness.

I feel that even if it's maintenance does cost somewhat more than a sanitized version of a bridge... let's stop and think if it might be worth it!

How many years of fixing up the bridge could be paid for by the money spent on changing it now?

Thank you for listening

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Thank you for listening

WARNER PARK PATH RECONSTRUCTION AND BRIDGE REPLACEMENTS

THANKS
TONY!

Public Information Meeting March 7, 2013

Project 53W1480
COMMENT SHEET

Please print:

Name: PAUL NOELDNER
Address: 136 KENSINGTON
City, State, Zip: 53704
Telephone Number: 608-698-0104
Email Address: PAUL-NOELDNER@HOTMAIL.COM

Comments: THANKS FOR DOING BIKE & NATURE ACCESS!

PLEASE USE THE EXISTING

PATH THAT GOES BY

WARNER CENTER, TURN

70° AND GO STRAIGHT

TO TIN CAN SHELTER

AREA & NEW BRIDGE

- CHEAPER BY \$100K (USE FOR MAINT)

- GREAT ACCESS FROM

WARNER CENTER FOR MIXED USE / HANDICAP / EVENTS

- PRESERVES BOTH SCENIC WALK ARCH

BRIDGES FOR LEISURE USE, PHOTOS
AND NATURE LOVERS

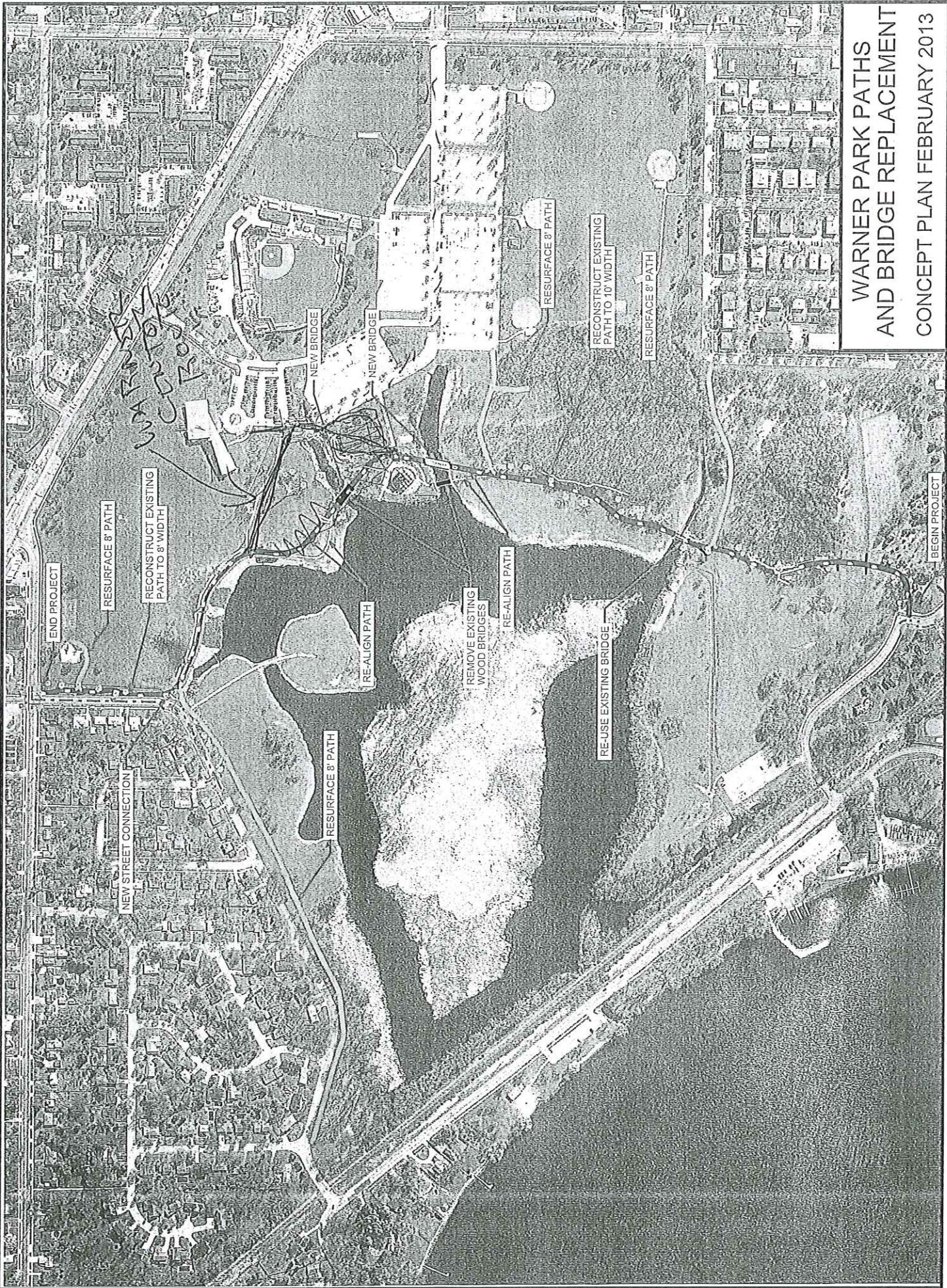
- MORE DIRECT FOR BIKES COMING/GOING
TO N SHERMAN AND TO FLYER ROUTE

Please place this sheet in the comment box or fold it and mail to Tony Fernandez at the address on the reverse side. Please submit your comments before March 21, 2013.

- MORE SPACE FOR AUDUBON ART FAIR
(& EASY ACCESS) AND OTHER EVENTS AT CENTER
OUTDOORS

WISNER PARK CENTER ROUTE

WARNER CENTER ROUTE



WARNER PARK PATHS
AND BRIDGE REPLACEMENT
CONCEPT PLAN FEBRUARY 2013

Rec'd 3-12-13
by email

WARNER PARK PATH RECONSTRUCTION AND BRIDGE REPLACEMENTS

Public Information Meeting March 7, 2013
Project 53WI480
COMMENT SHEET

Please print:

Name: Jeff Knudson
Address: 1526 Wyldewood Dr
City, State, Zip: Madison WI 53704
Telephone Number: _____
Email Address: jknudson2002@yahoo.com

Comments: Tony - great project, thank you for dedicating time
and resources to Warner Park. I've been exploring Warner
for 30+ years having grown up next to it. Here's a bullet
list of comments on the project:

- In a perfect world we could salvage or keep the existing
bridges, they have sentimental value.
- Would be great to have wood or fake-wood plank deck
for the new bridges, make them a little vintage-feeling.
- I like the path to stay along the water with 2 bridges.
- I jog the path 5 times a week spring-fall and see very
few commuter bikers... Though I don't jog during AM peak.
- I think a path realignment to avoid the north bridge trees
would be good. Then leave the existing bridge or remove
all but substructure to keep the tree.

Please place this sheet in the comment box or fold it and mail to Tony Fernandez at the address on the reverse side. Please submit your comments before March 21, 2013.

WARNER PARK PATH RECONSTRUCTION AND BRIDGE REPLACEMENTS

Public Information Meeting March 7, 2013

Project 53W1480
COMMENT SHEET

Please print:

Name: Jeff K (continued)

Address: _____

City, State, Zip: _____

Telephone Number: _____

Email Address: _____

Comments: _____

• Would be great if bridge deck could be arched slightly
like the existing bridges to fit the quaint look of
a country bridge... probably not possible with prefab

Please place this sheet in the comment box or fold it and mail to Tony Fernandez at the address on the reverse side. Please submit your comments before March 21, 2013.

WARNER PARK PATH RECONSTRUCTION AND BRIDGE REPLACEMENTS

Public Information Meeting March 7, 2013

Project 53W1480
COMMENT SHEET

Please print:

Name: Jack Hurst
Address: 1629 Randy Ln
City, State, Zip: Madison WI 53704
Telephone Number: 249 4720
Email Address: _____

Comments: make channels deeper by dredging + stabilize
banks before the new bridges are installed
You may save some money & anyone who
looks at the ^{channels} may enjoy it.
Use a plan to dredge all the lagoons
I'm sure the north side will approve
of it.

249-4720

Please place this sheet in the comment box or fold it and mail to Tony Fernandez at the address on the reverse side. Please submit your comments before March 21, 2013.

WARNER PARK PATH RECONSTRUCTION AND BRIDGE REPLACEMENTS

Public Information Meeting March 7, 2013

Project 53WI480

COMMENT SHEET

Please print:

Name: Jim Carrier
Address: 2301 Monterey Dr
City, State, Zip: Madison WI 53704
Telephone Number: 608 467-2662
Email Address: JimCarrier@usda.com

Comments: ① Keep the wood bridge + trees.
② maintain a pedestrian-only path around the "tin can" shelters.
③ Revert proposed bike path to the east, across the slough, + direct to the W Park Community Center.

Please place this sheet in the comment box or fold it and mail to Tony Fernandez at the address on the reverse side. Please submit your comments before March 21, 2013.

WARNER PARK PATH RECONSTRUCTION AND BRIDGE REPLACEMENTS

Public Information Meeting March 7, 2013

Project 53W1480 COMMENT SHEET

Please print:

Name: Steve Salt

Address: 3402 Goodland Dr

City, State, Zip: Mason

Telephone Number: 608 335 9888

Email Address: stevesalt@gmail.com

Comments: _____

Can the concrete be colored some from bright white,
especially if everything else is asphalt

I think I prefer a medium arch, less than your drawing,
more than the low one you don't like as much.

Any luck on "Sherman Flyer"? Riding on Sherman Ave
itself, regardless of current improvements & repainting
in 2013, is still quite dangerous to ride on. Will
this help connect to Sherman Flyer?

Please place this sheet in the comment box or fold it and mail to Tony Fernandez at the address on the reverse side. Please submit your comments before March 21, 2013.

Complete & shewable

Possible connection

Alt route

We need funds to repair bridge

Red leaf maple protect

Goal rather better opening



**WARNER PARK PATHS
AND BRIDGE REPLACEMENT**
CONCEPT PLAN FEBRUARY 2013

My doctor on Plover's verbal comments

COMMENTS ARE MTG

Fernandez, Anthony

From: Phillips, Rob
Sent: Wednesday, March 06, 2013 7:40 PM
To: 'Paul Noeldner'; Fernandez, Anthony; Rutledge, Kay
Cc: nanragland@att.net; emanuel_scarbrough@excite.com; madelyn.leopold@gmail.com; Mayor; annedave@chorus.net; Clear, Mark; Clausius, Joe; Rhodes-Conway, Satya; kcrowley@cityofmadison.com; Monks, Anne
Subject: RE: Warner Park Bike Path Request

Mr. Noeldner,

Thank you for your comments. I am forwarding this to our staff that are designing the paths. They will consider your comments as they collect comments on the project. In addition, they will be able to provide you more information and answer any questions that you have. The main designers are Tony Fernandez in the Engineering Division and Kay Rutledge in the Parks Division. Both are copied.

Robert Phillips, P.E.
City Engineer
rphillips@cityofmadison.com
608.266.4090

From: Paul Noeldner [mailto:paul_noeldner@hotmail.com]
Sent: Wednesday, March 06, 2013 6:53 PM
To: Monks, Anne; kcrowley@cityofmadison.com; Rhodes-Conway, Satya; Clausius, Joe; Clear, Mark; annedave@chorus.net; nanragland@att.net; emanuel_scarbrough@excite.com; madelyn.leopold@gmail.com; Phillips, Rob; Mayor
Subject: Warner Park Bike Path Request

Dear Mayor Soglin, Alders, and Staff who are helping preserve and enhance our beloved Parks -

This is in regard to plans to improve the walk and bike paths through Warner Park, and a request to combine the best of what we have, with addressing additional needs.

I'm Paul Noeldner, with Madison Audubon and also now on the boards of Wild Warner and Friends of Cherokee. I love helping with the Bluebird Trails, Audubon Art Fair, bird walk field trips, and weekly Sherman Nature Explorer activities at Warner. I much enjoyed the Parks Volunteer dinner and just want to say Thank You for your support for the things we do in both facilities based activities and in natural areas and outdoor recreation in Warner and other parks.

This is a request to support an alternative, much cheaper plan for the bike path improvements that will keep the wonderful natural area, trees and walk bridges by the shelter while adding better access for biked and the handicapped to the shelter and natural areas of Warner Park. The better alternative is route bikes via the existing walk and bike path that comes past the Warner Park Center to the parking lot, and add a segment to connect from that point near the Warner Center directly to the shelter area and the new proposed added flat bridge west of the shelter. This only requires one expensive new bridge, rather than two. It provides much better bike, handicap, and walk access from Warner Center to the shelter and natural areas of Warner Park..

There currently is no good walk or bike path connecting Warner Center with the shelter. There already plans for filling the old hockey rink area, and routing the path along that area will keep the bike path and faster bike traffic a bit further away from activities at the shelter area where kids run out onto the paths a lot.

This alternative cheaper plan, which I understand has already been mapped by staff for consideration, also preserves the

Fernandez, Anthony

From: Fernandez, Anthony
Sent: Wednesday, February 20, 2013 9:26 AM
To: 'Erin Peot'
Cc: Maglio, Tom
Subject: RE: Pathways reconstructed in Warner Park

Erin,

I am very sorry to say that the path most likely will not be finished by Oct 19. It could be paved by then but there is no guarantee. Also, the restoration work along the path (topsoil, seeding, etc.) will almost certainly not be done so the path will not be clean and open for use even if it is paved. However, we are going to limit the contractor to weekday work only. So there should be no active construction on Saturday. It will only disturb a strip about 15' so 20' wide, so it should be possible to walk on the grass from Sheridan to the bridge. Not sure how scenic it will be, though.

Congratulations and best wishes, and I hope you will be able to work around this somehow. I'm glad to provide updates as we get closer.

Tony Fernandez
afernandez@cityofmadison.com
voice: 608-266-9219
fax: 608-264-9275

From: Erin Peot [<mailto:eepeot@gmail.com>]
Sent: Wednesday, February 20, 2013 9:13 AM
To: Fernandez, Anthony
Cc: Maglio, Tom
Subject: Pathways reconstructed in Warner Park

Hello Tony,

I live near Warner Park and I just found out about the project to reconstruct the pathways in the park. My partner and I have been planning on getting married in Warner Park on Oct 19th, 2013. We are going to be using the pathway off of Sheridan St. to get to our ceremony site.

The project is planned to begin August 2013 and end Nov 2013 and according to this map you will be starting on Sheridan Street and working your way to Troy Street. My question is-how long will it take to finish the path from Sheridan Street to the first foot bridge (it is labeled "reuse existing bridge" on the map)? Do you think the work will be done by October 19th?

<http://www.cityofmadison.com/bikemadison/documents/warnerParkPathLocation.pdf>

Thanks,
eRin Peot

--

don't be afraid to go out on a limb...that's where the fruit is.
-h.jackson browne

Fernandez, Anthony

From: Fernandez, Anthony
Sent: Wednesday, February 20, 2013 9:06 AM
To: 'Satya Rhodes-Conway'
Cc: Monks, Anne; Phillips, Rob; Briski, Kevin; Rhodes-Conway, Satya; Bachmann, Christy; Rutledge, Kay; Maglio, Tom
Subject: RE: [northsidediscuss] Re: Warner Park Path Reconstruction & Bridge Replacements

Alder,

Here is a little more info on path width:

10' is our normal minimum width for all new and reconstructed paths intended to accommodate more than an occasional bike. This is federal AASHTO policy and State WisDOT policy, which is reflected in City Engineering's practice even where State or federal funds are not involved. Virtually every path we have built in the last 20 years has been 10' wide (or wider in many cases).

This includes paths we have built or rebuilt in parks, such as last year's resurfacing project in Reindahl Park, Pilgrim Park and Odana golf course and a new path in Rennebohm Park. Previous projects include paths in Vilas park, Brittingham Park, Country Grove Park, Acewood Park, Yahara Parkway, Tenney Park and many others. The main reasons for this are safety and comfort of users. Bikes passing in opposite directions need the extra separation to feel safe. Perhaps more importantly, the extra width gives pedestrians a higher level of comfort when they are passed by bikes. It becomes a much better experience for the users, while the visual impact of the extra surface width is hardly noticeable to the average person.

A secondary benefit is the increased durability of paths when they also get some use by maintenance vehicles. Vehicles can drive on an 8' path, but their wheels are very near the edge, resulting in premature deterioration of the pavement at the edges. In the long run this saves us money and non-renewable resources.

The Warner Park paths experience daily use by maintenance vehicles. Parks staff have explained to me that, because vehicles cannot use the two wooded bridges, they have to make greater use of the minor paths in the park to get around. If this main path were rebuilt to better handle their vehicles they could arrange the trash receptacles along this path and reduce both use of the minor paths and overall vehicle mileage.

City Engineering's primary interest in this project is to maintain and improve the City's network for non-motorized transportation, a sustainability goal that has been given high priority in all our planning and policy documents. From this standpoint, the 10' width (along with other geometric standards and ADA compatibility) is so well established in federal, State and local standards as well as current bicycle literature that we would be very reluctant to depart from it, especially since we do not feel it will have a significant impact on the sense of place.

Sorry for the length of this, but I wanted to make clear that we are not just widening it for maintenance vehicles. Hope this helps.

Tony Fernandez
afernandez@cityofmadison.com
voice: 608-266-9219
fax: 608-264-9275

From: Satya Rhodes-Conway [mailto:satya.vadia@gmail.com]
Sent: Tuesday, February 19, 2013 10:56 AM
To: Fernandez, Anthony; Maglio, Tom
Cc: Monks, Anne; Phillips, Rob; Briski, Kevin; Rhodes-Conway, Satya
Subject: Fwd: [northsidediscuss] Re: Warner Park Path Reconstruction & Bridge Replacements

FYI.

I remain unconvinced about this project. I do not support widening the paths just for maintenance vehicles. And I certainly don't support removing trees. Please take another look at this project through the lens of placemaking.

Thanks
Satya

----- Forwarded message -----

From: **wildwarnerwilycoyote** <wilycoyote@wildwarner.org>

Date: Tue, Feb 19, 2013 at 8:57 AM

Subject: [northsidediscuss] Re: Warner Park Path Reconstruction & Bridge Replacements

To: northsidediscuss@yahoogroups.com

Please take a look at the proposed new "bike" path proposed for Warner Park. The link to a map is below.

It removes the two most photographed bridges and spots in the park - the arched wood bridges over two inlets -- and the trees around them.

This is done, in part, so that city machines, including trucks and mowers and plows, can "maintain" this path with minimal labor.

Wild Warner asked the bike planner to keep those bridges -- while adding ADA- and machine-friendly and bike-lane-standard bridges elsewhere. But the city has decided they must go.

The bridge between the statue of liberty and the tin-can shelter is a wedding favorite, it is a beautiful spot for walkers and lovers, it is the kind of arched, old-fashioned wood bridge that is built to human scale, representative of a kind of park for people who happen to enjoy walking rather than getting across the park quickly on a 10-foot bicycle freeway.

That bridge and its enveloping cottonwoods is one of the most restful, picturesque and wonderful places in Warner Park. Its picture is blown up on a dividing wall at Warner Park community center. If this plan is not revised, that picture will remain a constant reminder of what Warner Park used to be.

Mark your calenders for March 7, at 6:30, and tell city planners what you think.

> Warner Park Path Reconstruction & Bridge Replacements<

> <https://my.cityofmadison.com/emaillists/redirect.cfm?ID=60322&MID=9996&LID=133>

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>

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