

**APPLICATION FOR
URBAN DESIGN COMMISSION
REVIEW AND APPROVAL**

AGENDA ITEM # _____
Project # _____

DATE SUBMITTED: <u>05.12.10</u>	Action Requested
UDC MEETING DATE: <u>05.19.10</u>	<input type="checkbox"/> Informational Presentation
	<input type="checkbox"/> Initial Approval and/or Recommendation
	<input checked="" type="checkbox"/> Final Approval and/or Recommendation

PROJECT ADDRESS: 1208, 1212, 1214 Spring Street

ALDERMANIC DISTRICT: District 8/ Bryon Eagon

OWNER/DEVELOPER (Partners and/or Principals) <u>Gebhardt Development/ Otto Gebhardt III</u>	ARCHITECT/DESIGNER/OR AGENT: <u>bark design/Christopher Gosch, AIA, NCARB</u>
<u>222 North Street</u>	<u>222 North Street</u>
<u>Madison, WI 53704</u>	<u>Madison, WI 53704</u>

CONTACT PERSON: Christopher Gosch, AIA, NCARB
Address: 222 North Street
Madison, WI 53704
Phone: 608.333.1926
Fax: 608.245.0770
E-mail address: studio@bark-design.com

TYPE OF PROJECT:

(See Section A for:)

- Planned Unit Development (PUD)
- General Development Plan (GDP)
- Specific Implementation Plan (SIP)
- Planned Community Development (PCD)
- General Development Plan (GDP)
- Specific Implementation Plan (SIP)
- Planned Residential Development (PRD)
- New Construction or Exterior Remodeling in an Urban Design District * (A public hearing is required as well as a fee)
- School, Public Building or Space (Fee may be required)
- New Construction or Addition to or Remodeling of a Retail, Hotel or Motel Building Exceeding 40,000 Sq. Ft.
- Planned Commercial Site

(See Section B for:)

- New Construction or Exterior Remodeling in C4 District (Fee required)

(See Section C for:)

- R.P.S.M. Parking Variance (Fee required)

(See Section D for:)

- Comprehensive Design Review* (Fee required)
- Street Graphics Variance* (Fee required)
- Other _____

*Public Hearing Required (Submission Deadline 3 Weeks in Advance of Meeting Date)

Where fees are required (as noted above) they apply with the first submittal for either initial or final approval of a project.

Gebhardt Development, LLC

222 North Street
Madison, WI 53704

Ph. 608 245-0753
Fax: 608 245-0770

1208, 1212, 1214 Spring Street Plan Commission Submittal

Proposed Zoning Text:

Legal Description: The lands subject to this planned unit development shall include those described in the following legal description:

Deeded Description 1: Parcel Number 070922110063

Lot 14, Block 11, Brook's Addition to Madison, City of Madison, Dane County, Wisconsin.

Deeded Description 2: Parcel Number 070922110071

Lot 13, Block 11, Brook's Addition to Madison, City of Madison, Dane County, Wisconsin.

Deeded Description 3: Parcel Number 070922110089

The West 35 Feet of the South 1/2 of Lot 12, Block 11, Brook's Addition to Madison, Dane County, Wisconsin.

A Metes and Bounds description of the following three parcels of land known as Lot 14, Lot 13 and the West 35 feet of the South 1/2 of Lot 12, all in Block 11, Brook's Addition to Madison, located in Section 22, T7N, R9E, in the City of Madison, Dane County, Wisconsin, more particularly described as follows:

Commencing at the Southwest corner of Lot 14, Block 11, Brook's Addition to Madison, said point also lying on the northerly platted Right-of-way line of Spring Street, and is the point of beginning of this description, thence N 00°22'07" W, along the westerly boundary line of said Lot 14, Block 11, 149.10 feet; thence S 88°49'43" E, along the northerly boundary line of said Lots 14 and 13, Block 11, 107.08 feet; thence S 00°22'07" E, along the easterly boundary line of said Lot 13, Block 11, 74.08 feet; thence S 88°49'36" E, 35.00 feet; thence S 00°22'07" E, 75.01 feet to a point lying on said southerly boundary line of said Lot 12, and northerly platted Right-of-way line of Spring Street; thence N 88°50'05" W, along the southerly boundary line of said Lots 12, 13 and 14, Block 11, and said northerly platted Right-of-way line of Spring Street, 142.08 feet to the point of beginning;

This Description has an area of 18,584 square feet, or 0.43 Acres.

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A: Statement of Purpose:

This zoning district is established to allow for the construction of the following:

Total Site Area: 18.584 s.f.

Building Area Breakdown:

Parking level: 11,780 s.f.

1st Floor: 11,600 s.f.

2nd Floor: 11,600 s.f.

3rd Floor: 11,600 s.f.

4th Floor: 10,100 s.f.

5th Floor: 10,100 s.f.

6th Floor: 10,100 s.f.

7th Floor: 10,100 s.f.

8th Floor: 10,100 s.f.

Total: 85,300 s.f. = F.A.R. = 4.58

Total New Construction:

Apartments: 75

Bedrooms: 230 +/-

Auto Parking: 20 interior stalls

Bicycle Parking: 220 +/- interior stalls

Scooter Parking: 30 interior stalls

B. Permitted Uses:

1. Those uses that are stated in all Residential Zoning Districts and as modified by the submitted plans.
2. Uses accessory to permitted uses as listed above.

C. Lot Area: 18,584 square feet/ 0.43 Acres.

D. Floor Area Ratio:

1. Floor area ratios will be 4.58
2. Maximum building height shall be 8 stories or 116 feet, per the RSSC recommendations. Height of proposed structure is identified on submitted plans.

E. Yard Area Requirements: Yard areas will be provided as shown on approved plans.

F. Landscaping: Site Landscaping will be provided as shown on final approved plans.

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G. Accessory Off-Street Parking & Loading: Accessory off street automobile, bicycle and scooter parking will be provided as shown on the submitted plans. A 10'x18' loading zone will be provided as shown on the submitted plans.

H. Lighting: Site Lighting is provided as shown on the submitted plans.

I. Signage: Signage will be provided as approved on the recorded plans.

J. Family Definition: The family definition of this PUD-GDP shall coincide with the definition given in chapter 28.03(2) of the Madison General Ordinances for the R-6 Zoning District.

K. Alterations and Revisions: No alteration or revision to this planned unit development shall be permitted unless approved by the City Plan Commission, however the Zoning Administrator may issue permits for minor alterations or additions which are approved by the Director of Planning and Development and the Alderperson of the district and are compatible with the concept approved by the City Plan Commission.

Residential Parking Permits: No residential parking permits will be issued for the properties included in this PUD-GDP/SIP.

END

Gebhardt Development, LLC

**222 North Street
Madison, WI 53704**

**Ph. 608 245-0753
Fax: 608 245-0770**

1208, 1212, 1214 Spring Street Plan Commission Submittal

Matt Tucker
Zoning Administrator
Madison Municipal Building, LL 100
215 Martin Luther King Jr. Blvd.
Madison, WI 53701

Letter of intent for PUD Rezoning

Mr. Tucker:

Please consider this our formal letter of intent to pursue PUD-GDP/SIP rezoning for 3 parcels of property located at 1208, 1212, and 1214 Spring Street.

Project Summary:

Project involves the demolition of 3 existing structures on contiguous properties which are currently used for student rental housing.

Replacing the removed structures will be a new 8 story, approx. 75 unit apartment building. The use, overall height and massing are consistent with adopted planning guidelines and represent the first major development under these guidelines by a private party in this neighborhood.

Units will consist of a combination of 1-4 bedroom units, with the final unit mix to be determined at a later date. Laundry facilities and vending will be provided at each floor.

Approximately 20 covered automobile parking spaces will be provided. This number is based on the amount of need demonstrated by similar projects in the area which are owned and managed by the same developer, and is more than sufficient to meet the needs of residents. Bicycle and scooter parking will be provided, with the ratio of bicycle parking at or near 1 space/bedroom, as that need has been historically demonstrated.

Proposed Project Schedule:

Demolition of existing structures:	08.20.10
New Construction start:	09.15.10
Project completion and occupancy:	08.10.11

Gebhardt Development, LLC

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Madison, WI 53704

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Project Team:

Owner:

Gebhardt Development
222 North Street
Madison, WI 53704
608.245.0753
Attn.: Otto Gebhardt III
gebhardtdevelopment@gmail.com

Architect:

Bark Design
222 North Street
Madison, WI 53704
608.333.1926
studio@bark-design.com

Structural Engineer:

GKS Engineering
3310 Kingston Drive
Madison, WI 53713
608.277.9520
gksengr@charter.net

Civil Engineer:

Professional Engineering, LLC
818 N. Meadowbrook Lane
Waunakee, WI 53597
608.849.9378
Attn.: Roxanne Johnson, P.E., LEED AP
Rjohnson@pe-wi.com

Landscape Architect:

Thiel Studio
608.286.9474
Attn.: Christopher Thiel, ASLA, LEED AP
cthiel@thielstudio.com

General Contractor:

Ideal Builders
1406 Emil Street
Madison, WI 53713
608.271.8111
Attn.: David Martin
dmmartin@IdealBuildersinc.com

Gebhardt Development, LLC

**222 North Street
Madison, WI 53704**

**Ph. 608 245-0753
Fax: 608 245-0770**

RSSC Neighborhood Plan Compliance:

Refer to attached submittal documentation for information on compliance with RSSC recommendations

Project Breakdown:

Existing number of units on project site: **44**

Existing number of parking stalls on project site: **14**

Proposed Units: **75 +/-**

Net Unit increase: **32 +/-**

Site Area: **18,584 s.f.; 0.43 Acres**

Building Footprint: **11,500 s.f.**

F.A.R. = 4.58

Vehicle and Pedestrian Access: **Off Spring Street, Auto and Scooter acces/parking
not visible from street**

Proposed Auto Parking spaces: **20 +/-**

Proposed scooter parking spaces: **37 +/-**

Proposed bicycle parking spaces: **1 per bedroom +/-**

Amenities:

- Private and public outdoor space- private balconies, public rooftop terrace
- Wireless Internet
- Secure Automobile, Scooter, and Bicycle Parking
- Laundry Facilities
- Energy Efficiency
- Location to UW facilities and retail/commercial establishments

Respectfully Submitted,

Otto Gebhardt III

END

Gebhardt Development, LLC

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Madison, WI 53704**

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1208, 1212, 1214 Spring Street Plan Commission Submittal

RSSC Recommendations Compliance

Applicable excerpts from the RSSC plan are as follows:

“...approximately 35,000 students are seeking off-campus housing each year. The planning area, which is one of the most convenient off-campus locations, is currently capturing less than 10 percent of that population.

If the planning area is able to capitalize on the current trends in student housing — the overall movement to higher-density structures — and leverage its current position as one of the main nodes of dense student housing, then the area north of Regent Street may become one of the central hubs of student life. Increasing the density of student housing north of Regent Street should serve to attract students currently living south of Regent, which could open up the area south of Regent for more owner-occupancy. Further discussion on development trends and opportunities is in Section VII: Economic Development.”

Goals:

Consider the redevelopment of parcels with non-historic buildings that have land values which are higher than improvement values.

Recommendations:

- If developed, high-rise student housing should be constructed north of Regent Street.
- Integrate student housing into the expanding University fabric.
- Increase density north of Regent Street in order to encourage students in converted homes south of Regent Street to move closer to campus.

Conformance with neighborhood plan:

-Provide high rise student housing north of Regent Street

Proposed: Spring Street is two blocks north of Regent Street

Development Location: Complies with neighborhood plan

-Provide additional housing density for the student population

Proposed: 44 existing units to be replaced with 75 +/- units

Higher density: Complies with neighborhood plan

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RSSC Recommendations Compliance (cont.)

-Promote the use of infill sites to unify the district by creating more intensity and consistency of use.

Proposed: Replace 3 underperforming non-historic properties with higher quality housing on existing parcels. Existing resources and infrastructure are used more efficiently.

Releases single family rental housing south of Regent Street for conversion back to owner/occupied properties.

Infill site: Complies with neighborhood plan

Building Height: 8 stories or 116 feet whichever is less

Proposed: 8 stories/86 feet

Height: Complies with neighborhood plan

Building Setback: 10 feet from property line at street frontage for first 3 stories

Proposed: 10 feet from property line at street frontage for first 3 stories

Setback: Complies with neighborhood plan

Building Stepback: 15 feet from building line for additional stories

Proposed: 15 feet from building line for additional stories

Stepback: Complies with neighborhood plan

Height Above Grade Plane: First Floor of solely residential buildings to be a minimum of 36" above grade plane.

Proposed: First habitable floor to be 36" min. above grade plane

First Floor Level: Complies with neighborhood plan

Provide inset and covered building entrances

Proposed: Entry to be at corner of building, inset and articulated

Entry location: Complies with neighborhood plan

Project is not in a Special Design Guideline District

Proposed project and the University of Wisconsin

Property was identified in RSSC plan as a potential site for future UW campus development. Developer, UW staff and City Planning Staff have reviewed and discussed proposed project.

UW has publicly stated that the parcels where the current project is proposed is not in their short to mid term goals for property acquisition and proposed project would not be inconsistent with the University's goals for the neighborhood.

END

DEVELOPER:
 GEBHARDT DEVELOPMENT
 222 NORTH STREET
 MADISON, WI 53704
 ATTN.: OTTO GEBHARDT III
 608.245.0753

SHEET INDEX

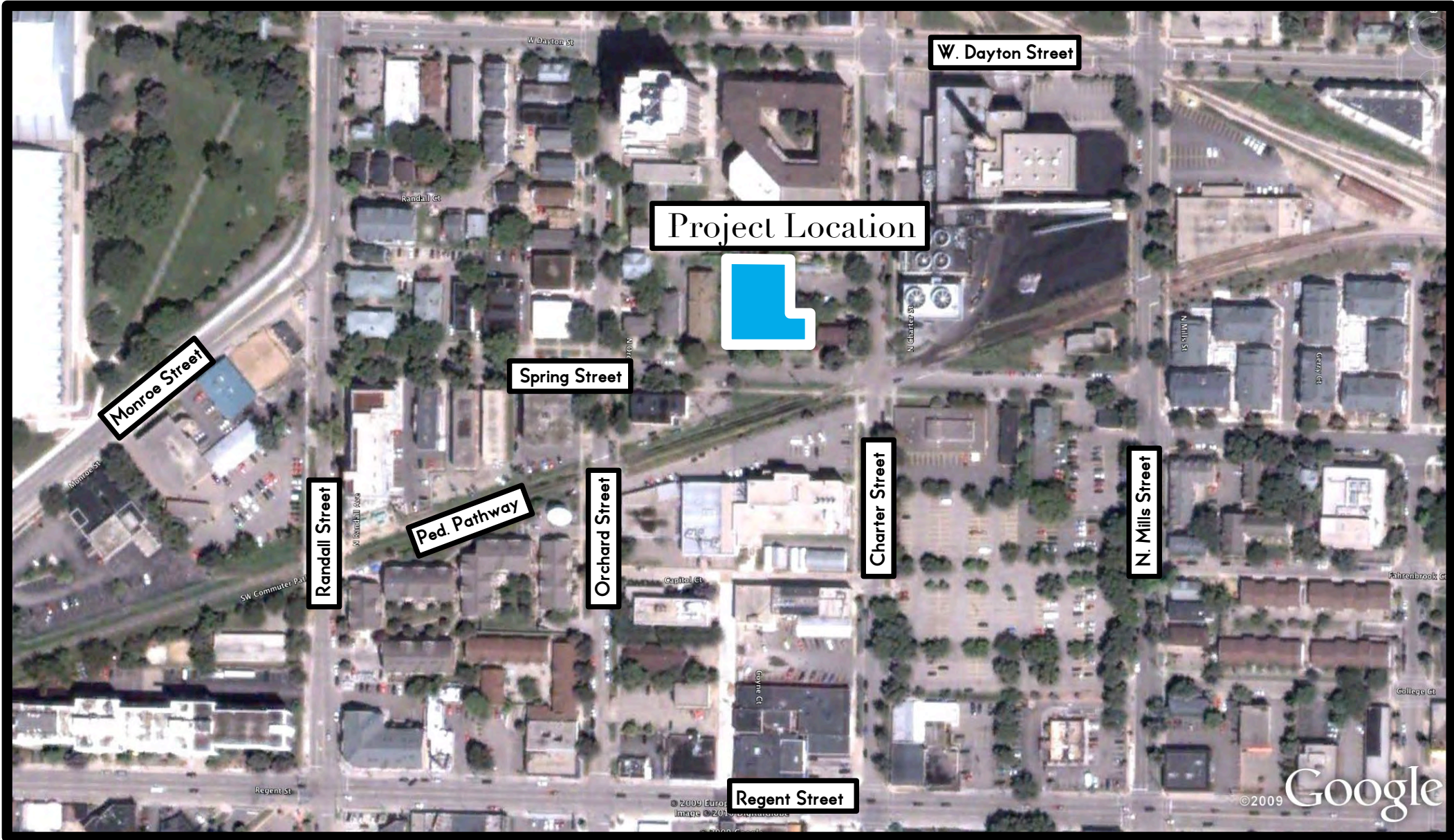
- C100 SURVEY
- C101 SITE PLAN
- C102 GRADING PLAN
- C400 LANDSCAPE PLAN
- C401 LANDSCAPE PLAN
- C402 LIGHTING PLAN
- C700 NEIGHBORHOOD CIRCULATION PLAN
- C701 NEIGHBORHOOD CONTEXT PLAN
- C702 PROPOSED CIRCULATION PATHS PER NEIGHBORHOOD PLAN

- A1.0 PARKING AND LOBBY LEVEL
- A1.1 FIRST FLOOR PLAN
- A1.2 SECOND FLOOR PLAN
- A1.3 THIRD FLOOR PLAN
- A1.4 FOURTH FLOOR PLAN
- A1.5 FIFTH - EIGHTH FLOOR PLAN
- A1.6 PENTHOUSE PLAN

- A4.1 BUILDING ELEVATIONS
- A4.2 BUILDING ELEVATIONS
- A4.3 BUILDING ELEVATIONS
- A4.4 BUILDING ELEVATIONS
- A4.5 BUILDING PERSPECTIVES
- A4.6 BUILDING PERSPECTIVES
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- A6.1 BUILDING DETAILS
- A6.2 BUILDING DETAILS
- A6.3 BUILDING DETAILS

A7.1 NEIGHBORHOOD PLAN COMPLIANCE AND BEST PRACTICES



REVIEW SET
URBAN DESIGN COMMISSION

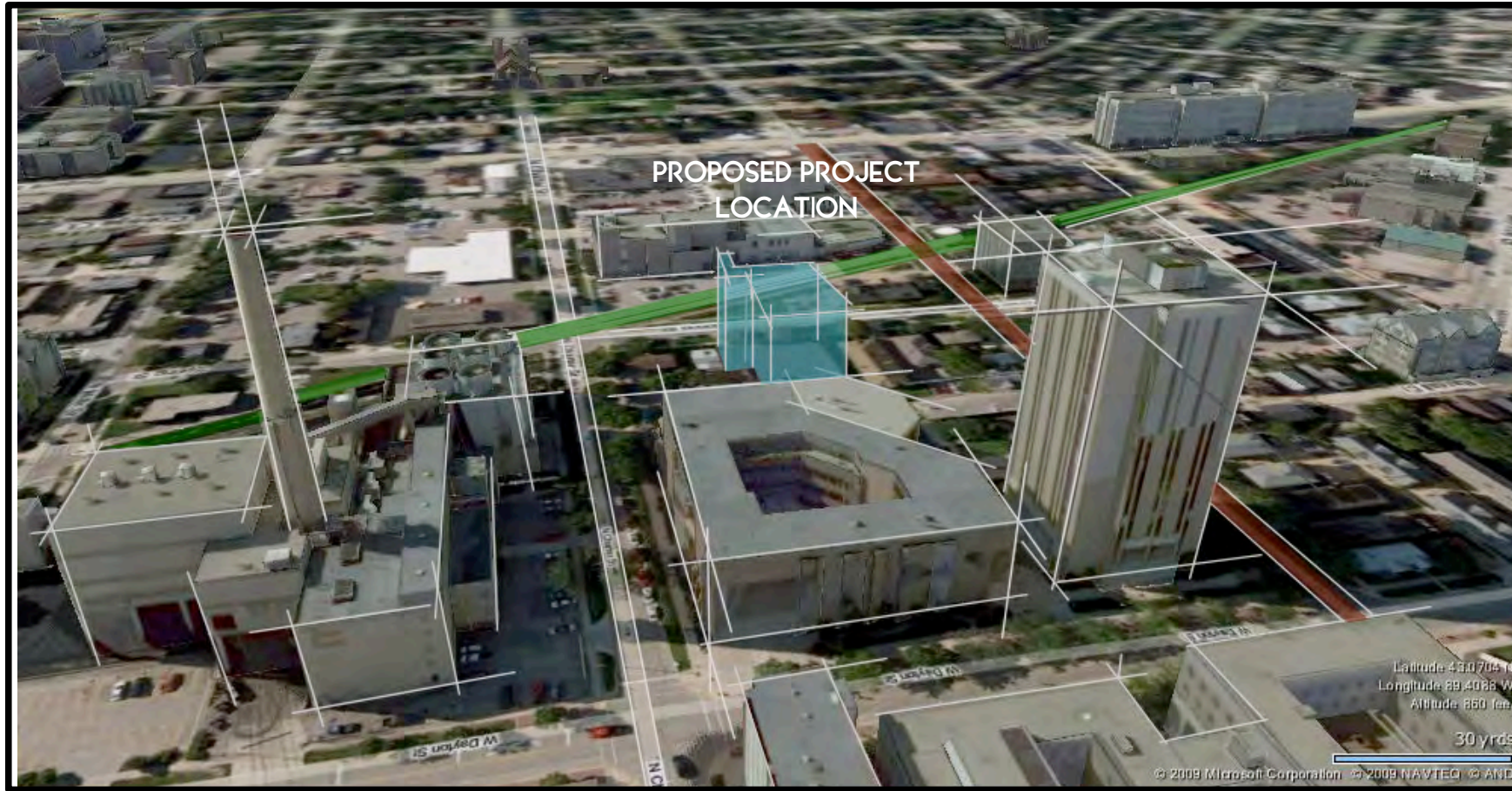


WWW.BARK-DESIGN.COM
 STUDIO@BARK-DESIGN.COM

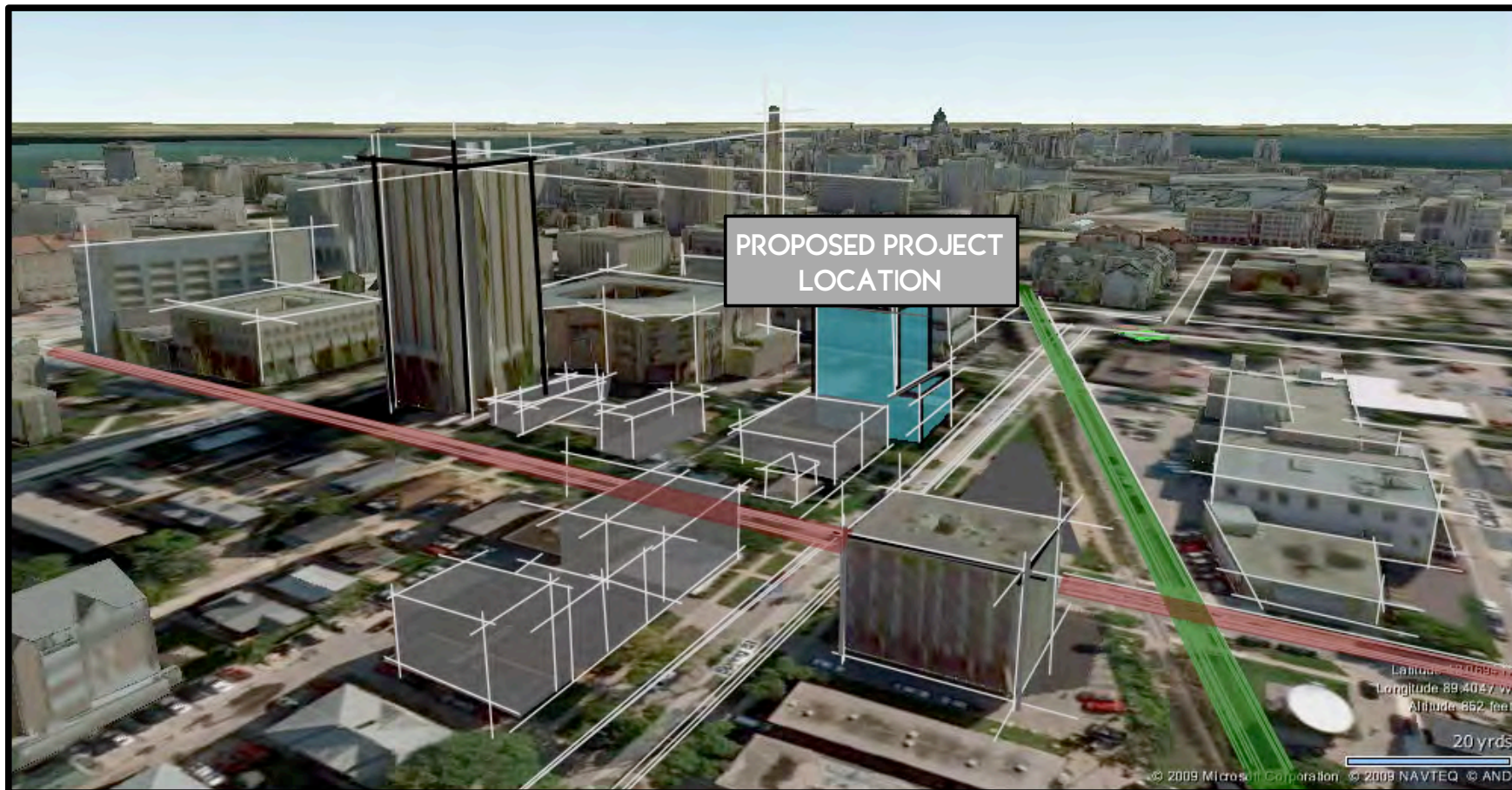
THE "HUMBUCKER APARTMENTS"

COVER SHEET

DATE	05/12/10
DRAWN BY	OTG
REVISIONS	



VIEW TO SOUTH



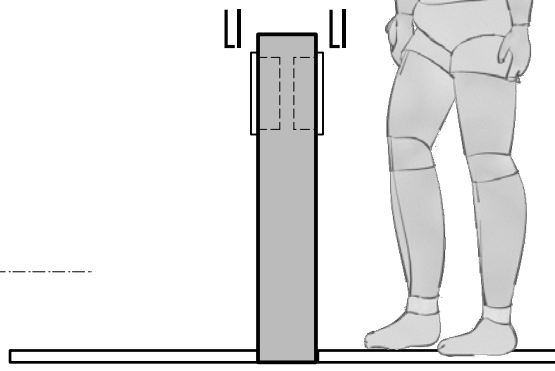
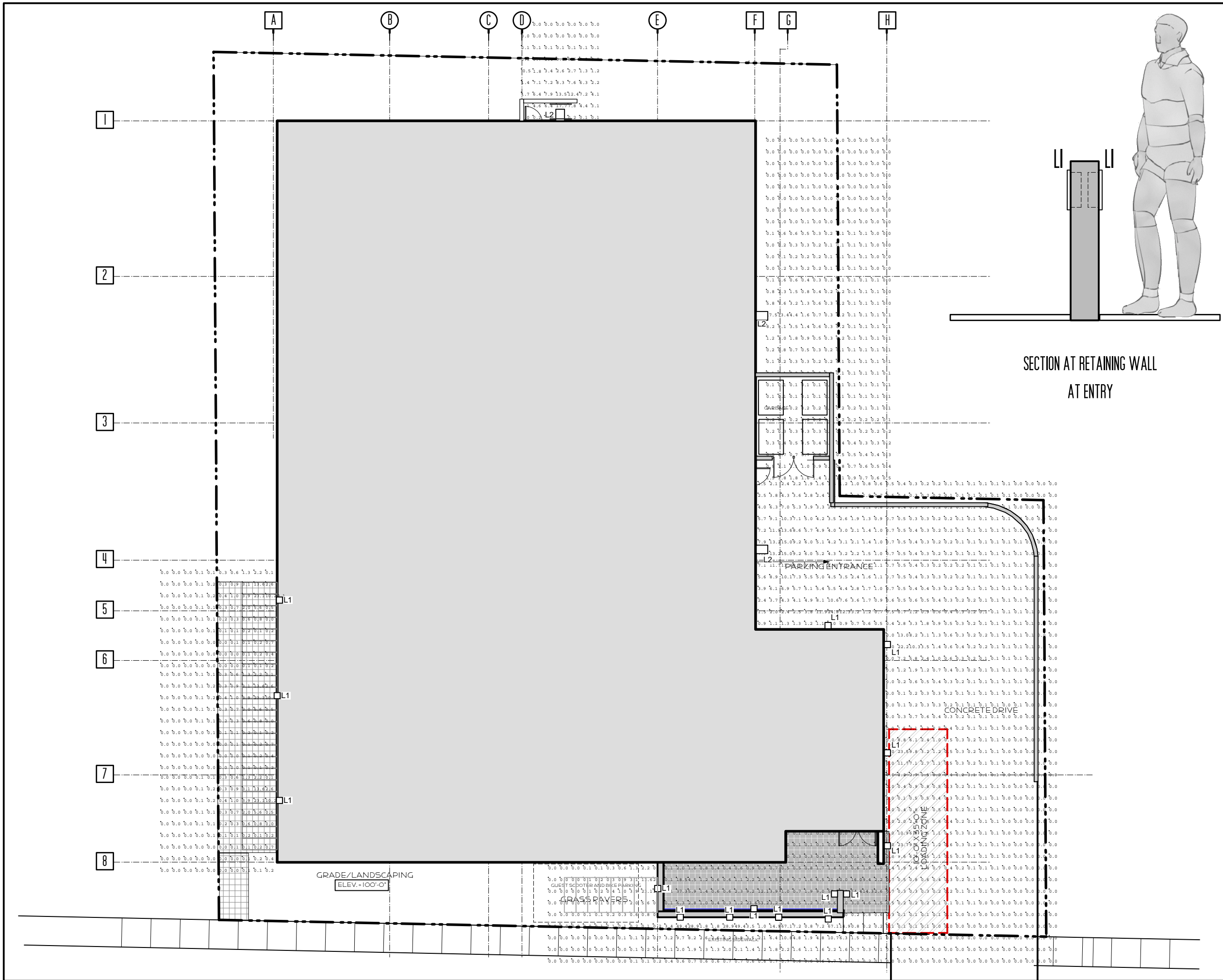
VIEW TO EAST

SITE NOTES:

- PEDESTRIAN WALKWAYS SHOWN IN RED
- EXISTING BIKE PATH SHOWN IN GREEN

REVIEW SET
URBAN DESIGN COMMISSION

	<small>WWW.BARK-DESIGN.COM</small> <small>STUDIO@BARK-DESIGN.COM</small>
	<small>DRAWN BY</small> <small>REVIEWED</small>
<p>THE "HUMBUCKER APARTMENTS"</p>	
<p>NEIGHBORHOOD CONTEXT AND CIRCULATION</p>	
<p>C701</p>	



SECTION AT RETAINING WALL
AT ENTRY



L1:
INVUE SOLAS SERIES
12" DIA.
52 W COMPACT FL



L2:
COOPER LIGHTING-WALLY CUTOFF
9"x9"
70 W METAL HALIDE

REVIEW SET

URBAN DESIGN COMMISSION

	WWW.BARK-DESIGN.COM STUDIO@BARK-DESIGN.COM	
	DRAWN BY REVISED	DATE 05/10/10
THE "HUMBUCKER APARTMENTS"		
LIGHTING PLAN <small>SCALE: 1/8"=1'-0"</small>		C 402

LANDSCAPE PLANT LEGEND

Symbol	Botanical name	Common Name	Size	Root	Quantity	Comments
EVERGREEN TREES/SHRUBS						
JUN SCO	Juniperus scopulorum 'Blue Heaven'	Blue Heaven Juniper	4' Ht.	Cont.		
TAX MED	Taxus x media 'Tauntonii'	Japanese Yew	3 Gal.	Cont.		
DECIDUOUS SHRUBS						
BER THU	Berberis thunbergii	Barberry	5 Gal.	Cont.		
COR ALT	Cornus alternifolia	Pagoda dogwood	5 Gal.	Cont.		
COR BAI	Cornus baileyi	Bailey Redosier Dogwood	5 Gal.	Cont.		
ORNAMENTAL GRASSES						
CAL ACU	Calamagrostis acutiflora 'Stricta'	Feather Reed Grass	1 Gal.	Cont.		Plant 18" O.C.
HEL SEM	Helictotrichon sempervirens	Blue Oat Grass	1 Gal.	Cont.		
PERENNIALS / GROUNDCOVER						
HEM MIX	Hemerocallis 'Baltimore Oriole'	Baltimore Oriole	1 Gal.	Cont.		Red/Orange Flowers
	Hemerocallis 'Cradle of Liberty'	Cradle of Liberty	1 Gal.	B#B		Scarlet Red Flowers
	Hemerocallis 'Demetrius'	Demetrius Daylily	1 Gal.	Cont.		Yellow Flowers
LOB CAR	Lobelia cardinalis	Cardinal Flower	1 Gal.	Cont.		
HOS SIE	Hosta sieboldiana 'Elgans'	Elgans Hosta	1 Gal.	Cont.		Blue/Green Leaves
SAL SYL	Salvia x sylvestris 'May Night'	Perennial Salvia	1 Gal.	Cont.		

TERRACE PLANTERS

Artemisia ludoviciana	Prairie Sage
Calamagrostis acutiflora 'Stricta'	Feather Reed Grass
Coreopsis lanceolata	Sand Coreopsis
Helictotrichon sempervirens	Blue Oat Grass
Hemerocallis Species	Daylily
Lamium maculatum	Dead Nettle
Liatris spicata	Blazing Star
Rudbeckia hirta 'Indian Summer'	Black-eyed Susan
Salvia x sylvestris 'May Night'	Perennial Salvia

1 COR ALT

2 TAX MED

3 HEM MIX

2 LOB CAR

4 LOB CAR

2 COR BAI

8 HEM MIX

24" STONE MULCH
MAINTENANCE EDGE

4 LOB CAR

2 COR BAI

8 HEM MIX

CAL ACU

3 JUN SCO

5 HEM MIX

6 HEL SEM

2 SAL SYL

4 BAR THU

14 HEM MIX

EXISTING PARKING LOT

S 88°49'43" E

107.08'

LAWN

EXISTING TREE

9 HOS SIE

1 COR ALT

3 TAX MED

NOTE:
SEE SHEET C401 FOR
LANDSCAPE LEGEND

N 00°22'07" W

149.10'

S 00°22'07" E 74.08'

S 88°49'36" E

S 00°22'07" E

75.01'

7

(BIKE)

LOADING ZONE

LAWN

(SCOOTER) (BIKE)

EXISTING TREE

EXISTING TREE

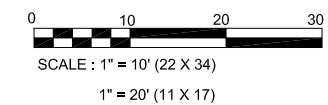
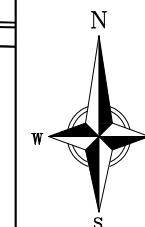
EXISTING TREE

SPRING STREET

4 JUN SCO

9 HEL SEM

4 SAL SYL



DATE	04-28-10
DRAWN BY	CT
DATE	02-22-10
REVISION	1

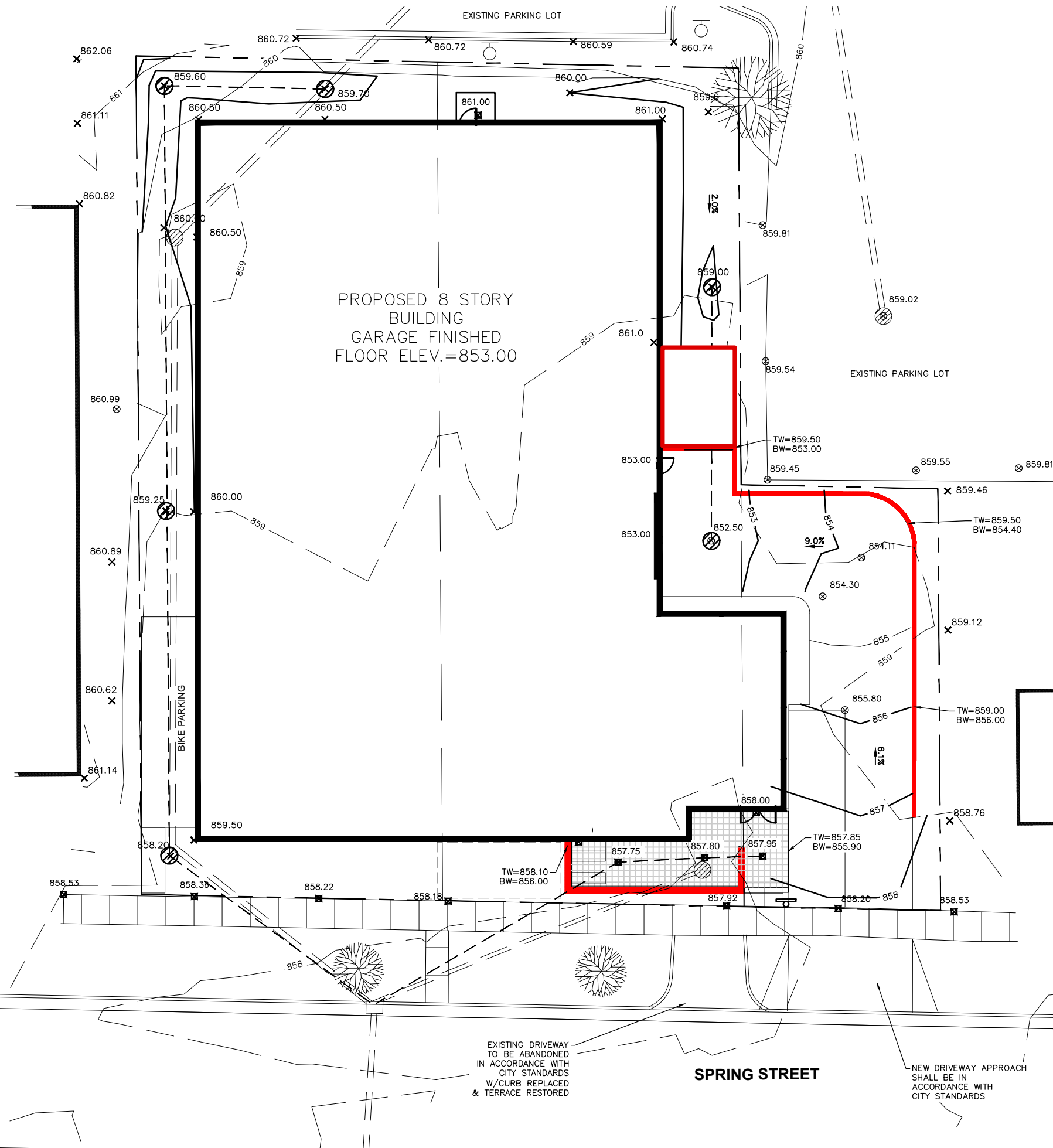
828 S. Brooks St.
Madison, WI 53715
phone (608) 215-6474



THE "HUMBUCKER" LANDSCAPE PLAN

MADISON, WISCONSIN

C400

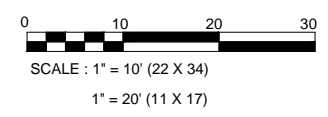
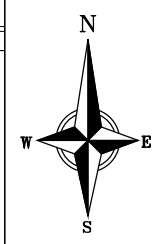


GRADING NOTES

1. CONTRACTOR SHALL CALL DIGGERS HOTLINE PRIOR TO ANY CONSTRUCTION.
 2. ALL MATCHING PAVEMENT AND STORM SEWER ELEVATIONS SHALL BE VERIFIED IN THE FIELD TO ALLOW FOR PROPER DRAINAGE.
 3. CONTOUR AND SPOT ELEVATIONS SHOWN ARE FINISH GRADE ELEVATIONS.
 4. ALL LAWN AND PLANTING AREAS WHICH HAVE BEEN COMPACTED DUE TO CONSTRUCTION SHALL BE LOOSENEED PRIOR TO ADDITION OF TOPSOIL.
 5. SIDEWALK AND PATIO AREA CROSS SLOPES SHALL NOT EXCEED 2% SLOPE.
 6. CONTRACTOR SHALL CALL DIGGERS HOTLINE PRIOR TO ANY CONSTRUCTION.
- UTILITIES MARKED BY DIGGERS HOTLINE INCLUDE:
 -SBC - CITY OF MADISON
 -TDS TELECOM - CENTURYTEL
 -TDS METROCOM - CHARTER COMMUNICATIONS
 -MADISON GAS & ELECTRIC

GRADING LEGEND

- EXISTING CONTOUR
- - - PROPOSED CONTOUR
- ⊗ 840.00 CONCRETE PAVEMENT ELEVATION
- 839.34 SIDEWALK ELEVATION
- × 860.89 GROUND ELEVATION
- 1.8% FINISH GRADE SLOPE
- █ RETAINING WALL



EXISTING DRIVEWAY TO BE ABANDONED IN ACCORDANCE WITH CITY STANDARDS W/CURB REPLACED & TERRACE RESTORED

SPRING STREET

NEW DRIVEWAY APPROACH SHALL BE IN ACCORDANCE WITH CITY STANDARDS

DATE	02-08-10
DRAWN BY	RJJ
REVISION	
DATE	02-22-10
DATE	04-28-10
REVISION	1
REVISION	2

818 N Meadowbrook Ln
 Waunakee, WI 53597
 phone (608) 849-9378
 fax (608) 237-2129

PROFESSIONAL ENGINEERING LLC

"THE HUMBUCKER"
GRADING PLAN
 MADISON, WISCONSIN

C102

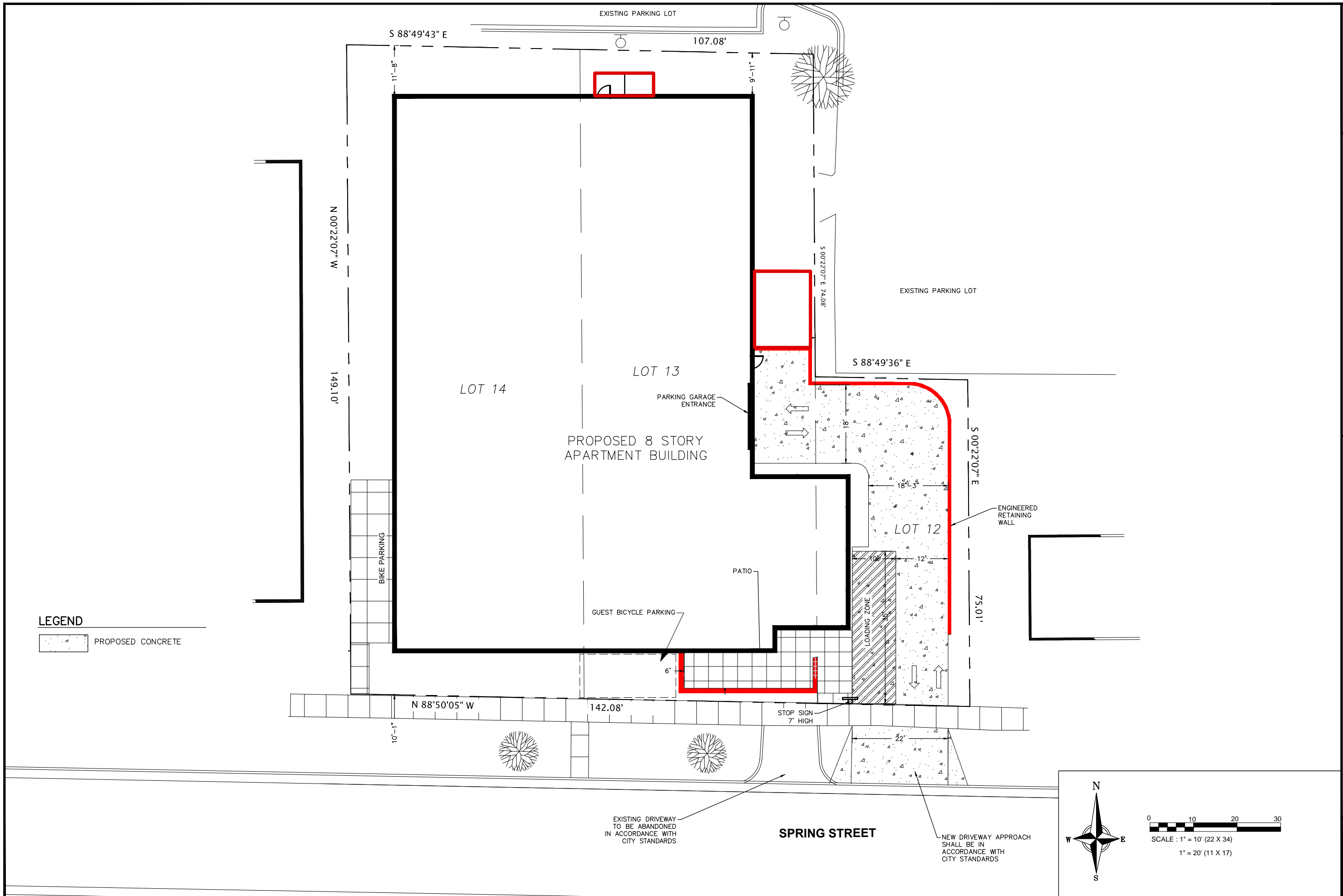
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REVISION	
1	02-22-10
2	04-28-10

818 N Meadowbrook Ln
 Waunakee, WI 53597
 phone (608) 849-9378
 fax (608) 237-2129



THE "HUMBUCKER"
 SITE PLAN
 MADISON, WISCONSIN

C101



DATE	02-08-10
DRAWN BY	RJJ
REVISION	
DATE	
REVISION	

818 N Meadowbrook Ln
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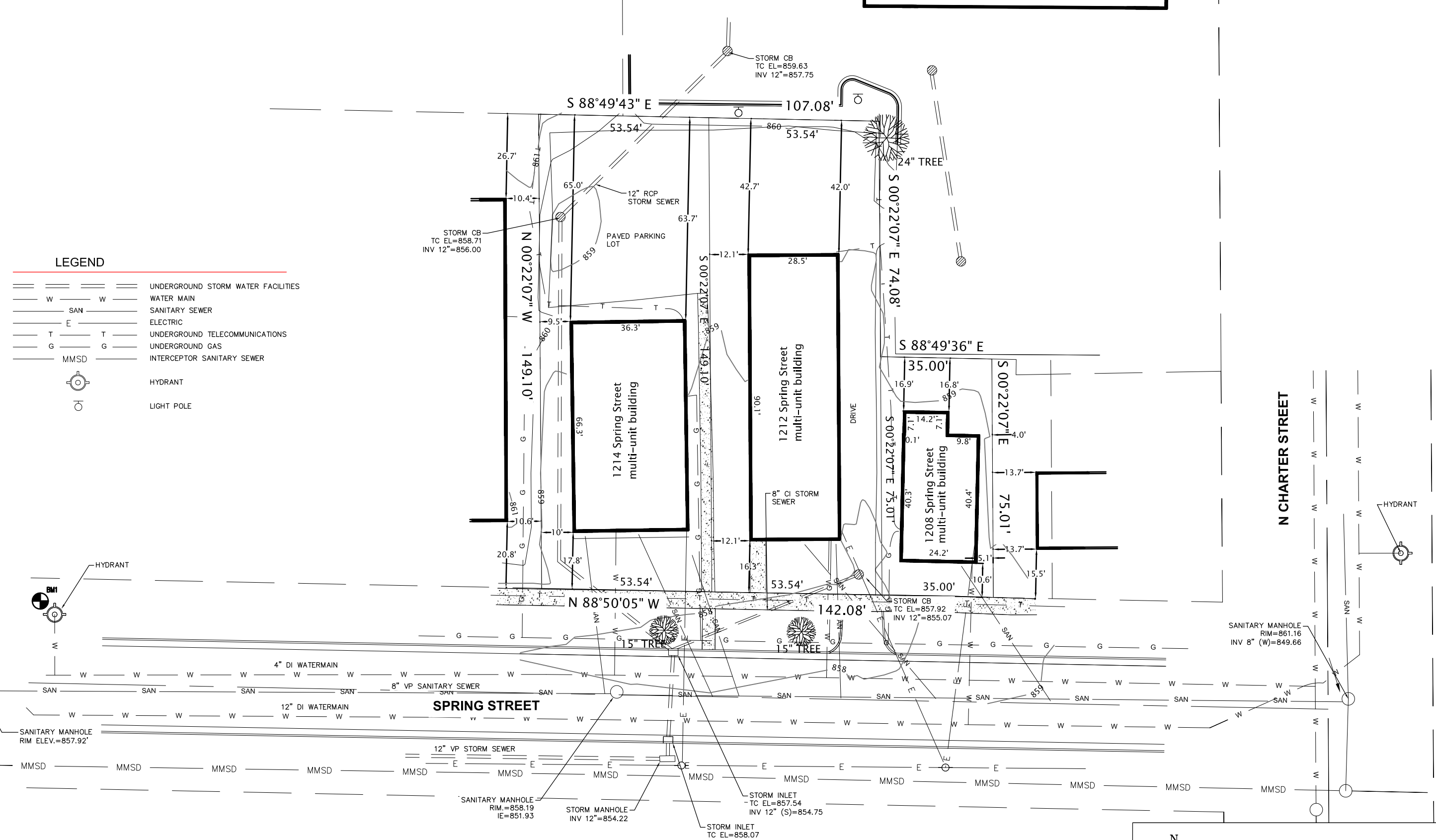
PROFESSIONAL ENGINEERING LLC

THE "HUMBUCKER" TOPOGRAPHIC SURVEY
 MADISON, WISCONSIN

C100

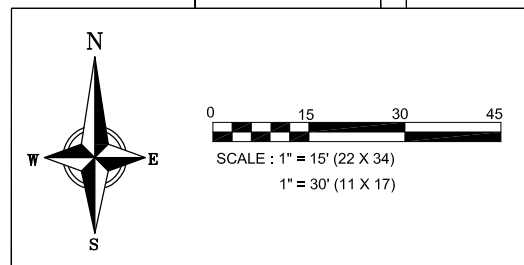
LEGEND

	UNDERGROUND STORM WATER FACILITIES
	WATER MAIN
	SANITARY SEWER
	ELECTRIC
	UNDERGROUND TELECOMMUNICATIONS
	UNDERGROUND GAS
	INTERCEPTOR SANITARY SEWER
	HYDRANT
	LIGHT POLE



BENCHMARKS

No.	ELEVATION	LOCATION
BM1	862.59	TAG BOLT, FIRE HYDRANT, NE CORNER OF INTERSECTION OF N ORCHARD ST AND SPRING STREET
BM2	850.60	1/4 SECTION 22 MON NAVD 88



Compliance with Neighborhood Plan

Applicable excerpts from the RSSC plan are as follows:

"...approximately 35,000 students are seeking off-campus housing each year. The planning area, which is one of the most convenient off-campus locations, is currently capturing less than 10 percent of that population.

If the planning area is able to capitalize on the current trends in student housing - the overall movement to higher-density structures - and leverage its current position as one of the main nodes of dense student housing, then the area north of Regent Street may become one of the central hubs of student life. Increasing the density of student housing north of Regent Street should serve to attract students currently living south of Regent, which could open up the area south of Regent for more owner-occupancy. Further discussion on development trends and opportunities is in Section VII: Economic Development."

Goals:

Consider the redevelopment of parcels with non-historic buildings that have land values which are higher than improvement values.

Recommendations:

- If developed, high-rise student housing should be constructed north of Regent Street.
- Integrate student housing into the expanding University fabric.
- Increase density north of Regent Street in order to encourage students in converted homes south of Regent Street to move closer to campus.

Conformance with neighborhood plan:

-Provide high rise student housing north of Regent Street

Proposed: Spring Street is two blocks north of Regent Street

Development Location: Complies with neighborhood plan

-Provide additional housing density for the student population

Proposed: 44 existing units to be replaced with 75 +/- units

Higher density: Complies with neighborhood plan

-Promote the use of infill sites to unify the district by creating more intensity and consistency of use.

Proposed: Replace 3 underperforming non-historic properties with higher quality housing on existing parcels. Existing resources and infrastructure are used more efficiently.

Releases strain single family rental housing south of Regent Street for conversion back to owner/occupied properties.

Infill site: Complies with neighborhood plan

Building Height: 8 stories or 116 feet whichever is less

Proposed: 8 stories/86 feet

Height: Complies with neighborhood plan

Building Setback: 10 feet from property line at street frontage for first 3 stories

Proposed: 10 feet from property line at street frontage for first 3 stories

Setback: Complies with neighborhood plan

Building Stepback: 15 feet from building line for additional stories

Proposed: 15 feet from building line for additional stories

Stepback: Complies with neighborhood plan

Height Above Grade Plane: First Floor of solely residential buildings to be a minimum of 36" above grade plane.

Proposed: First habitable floor to be 48" +/- above grade plane

First Floor Level: Complies with neighborhood plan

Provide inset and covered building entrances

Proposed: Entry to be at corner of building, inset and articulated

Entry location: Complies with neighborhood plan

Project is not in a Special Design Guideline District

Sustainable Design, Construction, and Building Life-Cycle Goals

Site:

- Urban Infill site- Infrastructure and Utilities in place-no additional strain on existing services.
- Existing buildings highly inefficient- energy use high and quality of life low.
- Close proximity to existing public transportation routes.
- Within walking distance of University resources and supporting commercial/retail properties.

Design/Construction

- Recycling program for existing buildings to be demolished.
- Minimal Site Disruption.
- Outdoor spaces- Semi private and for all residents.
- Local/Regional Finishes/High Air quality/Minimize off-gassing.
- Energy Efficient Appliances.
- Utilize local or regional materials and suppliers.
- Phase construction to minimize waste and unnecessary energy outlays (winter heat).
- Utilize materials that have post consumer content and can be easily recycled.
- Design for a 50 year building life cycle.

Energy

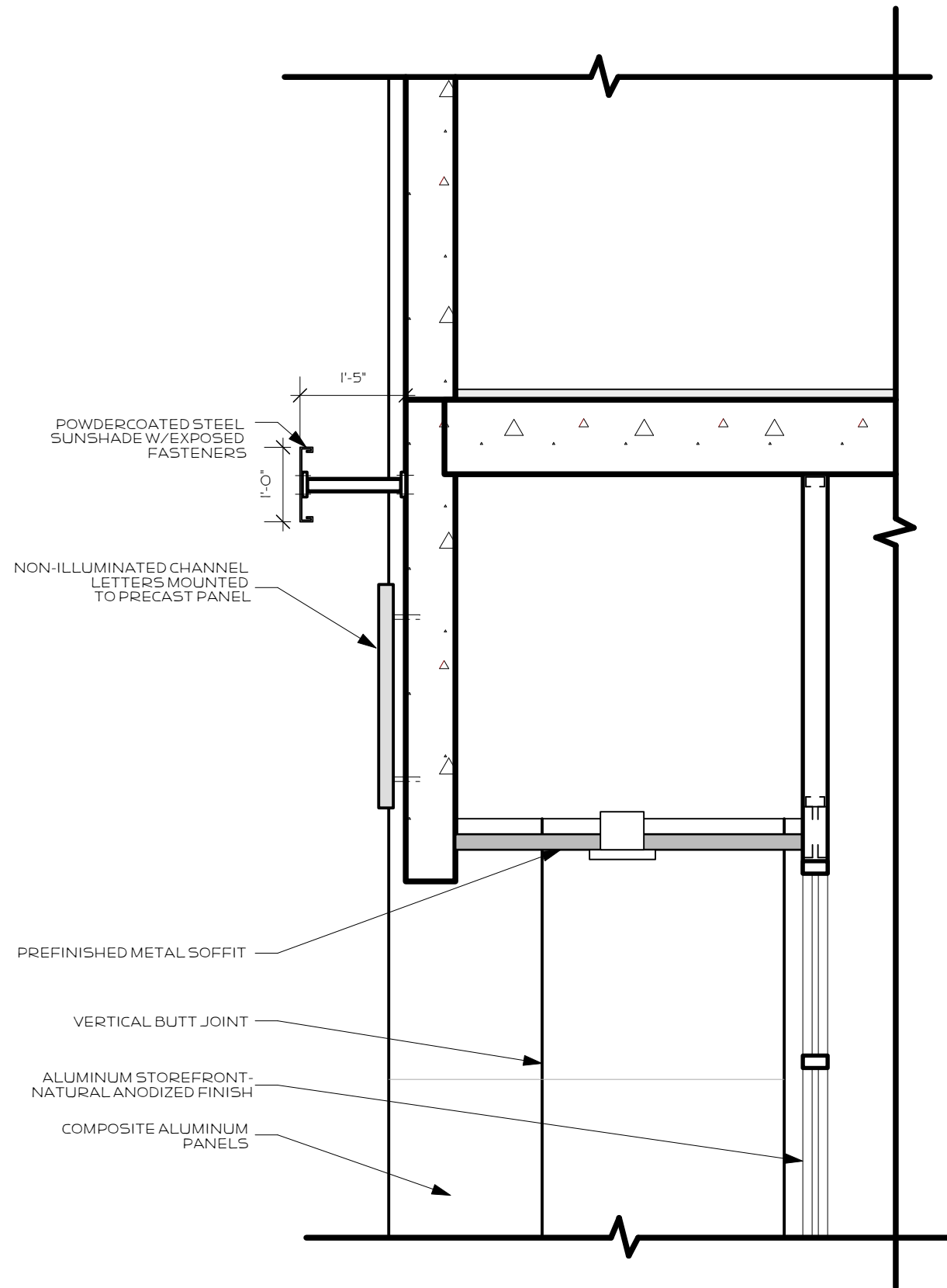
- Implement passive solar elements.
- Attempt to utilize renewable energy sources (solar/wind).
- Efficient mechanical systems- minimal combustion discharges.
- Identify high power items and utilize smart routing of power.
- Focus on Energy involved early in design process

Life Cycle

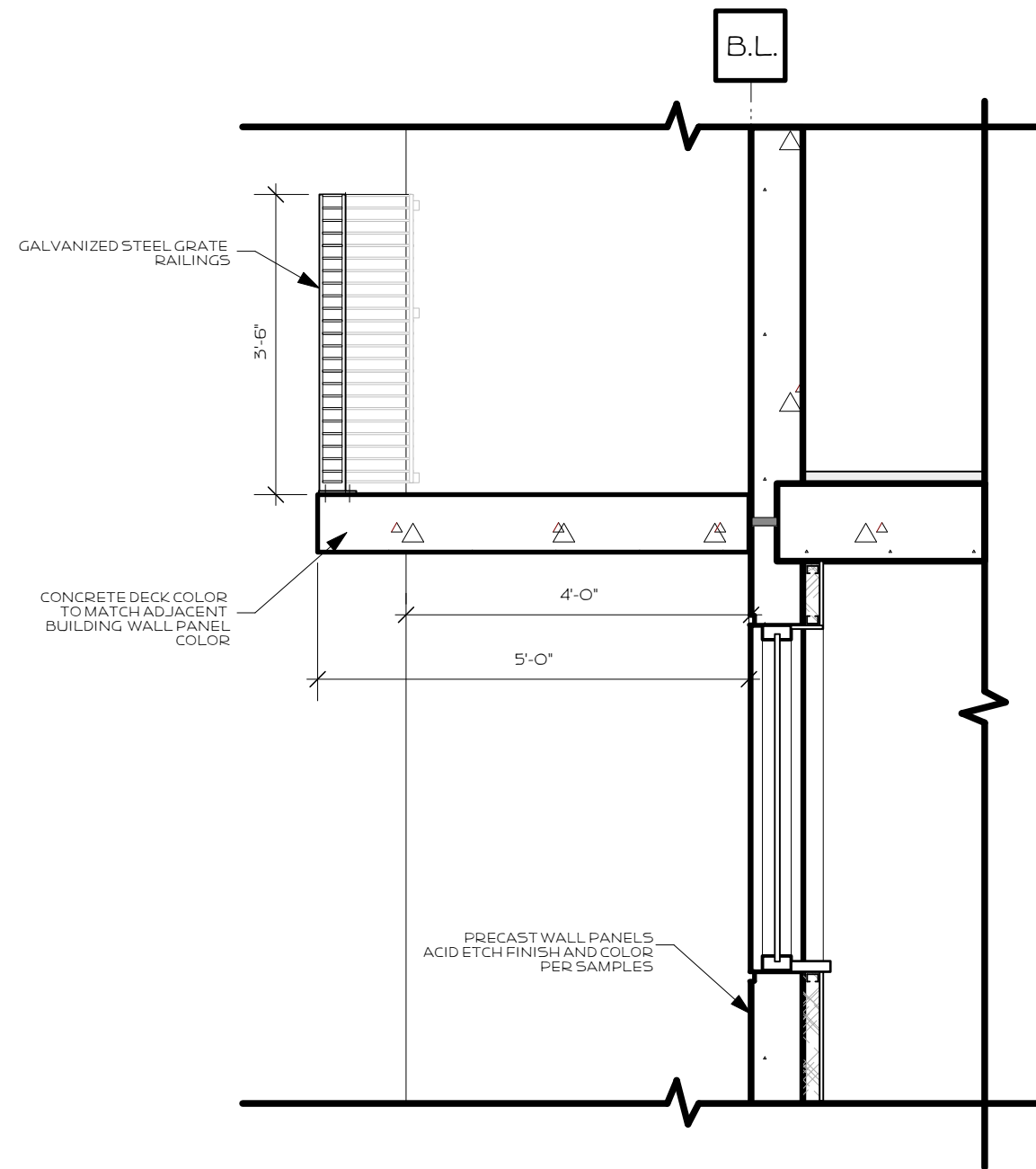
- Minimize maintenance/material replacement.
- High quality of life for residents.
- Efficient heating/cooling for residents- lower cost and higher quality of life.

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	DRAWN BY	DATE	
THE "HUMBUCKER" APARTMENTS		REVISED	05.12.10
COMPLIANCE AND GOALS		A 7.1	



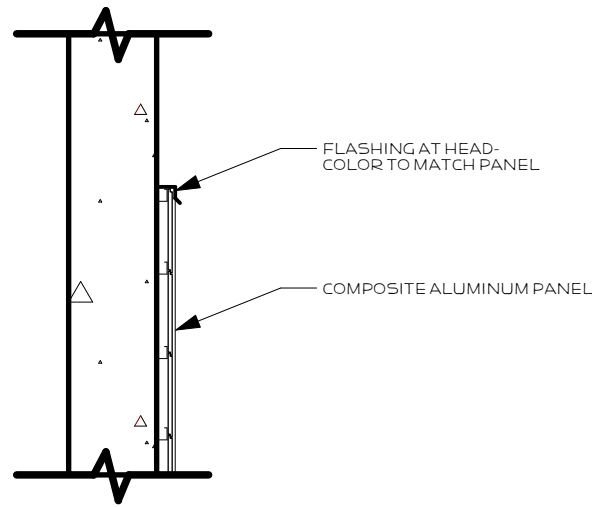
2 WALL SECTION AT ENTRY



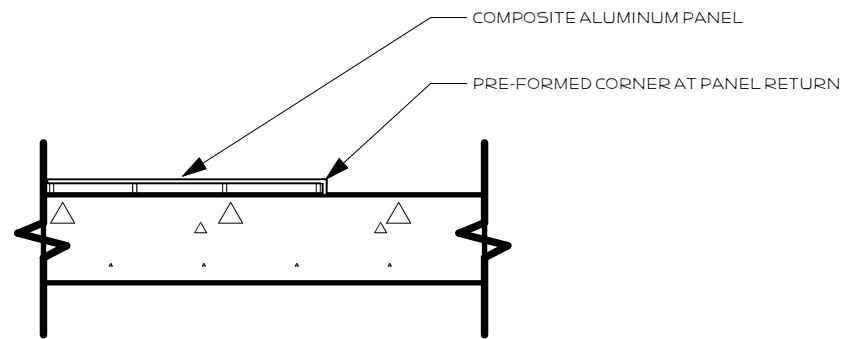
1 WALL SECTION AT BALCONY

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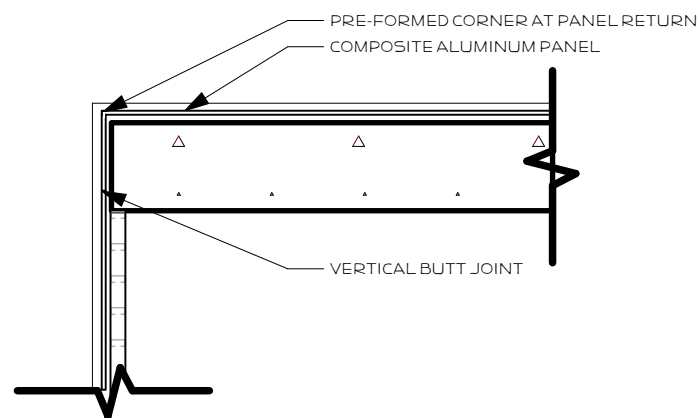
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THE "HUMBUCKER APARTMENTS"		A6.3
BUILDING DETAILS		



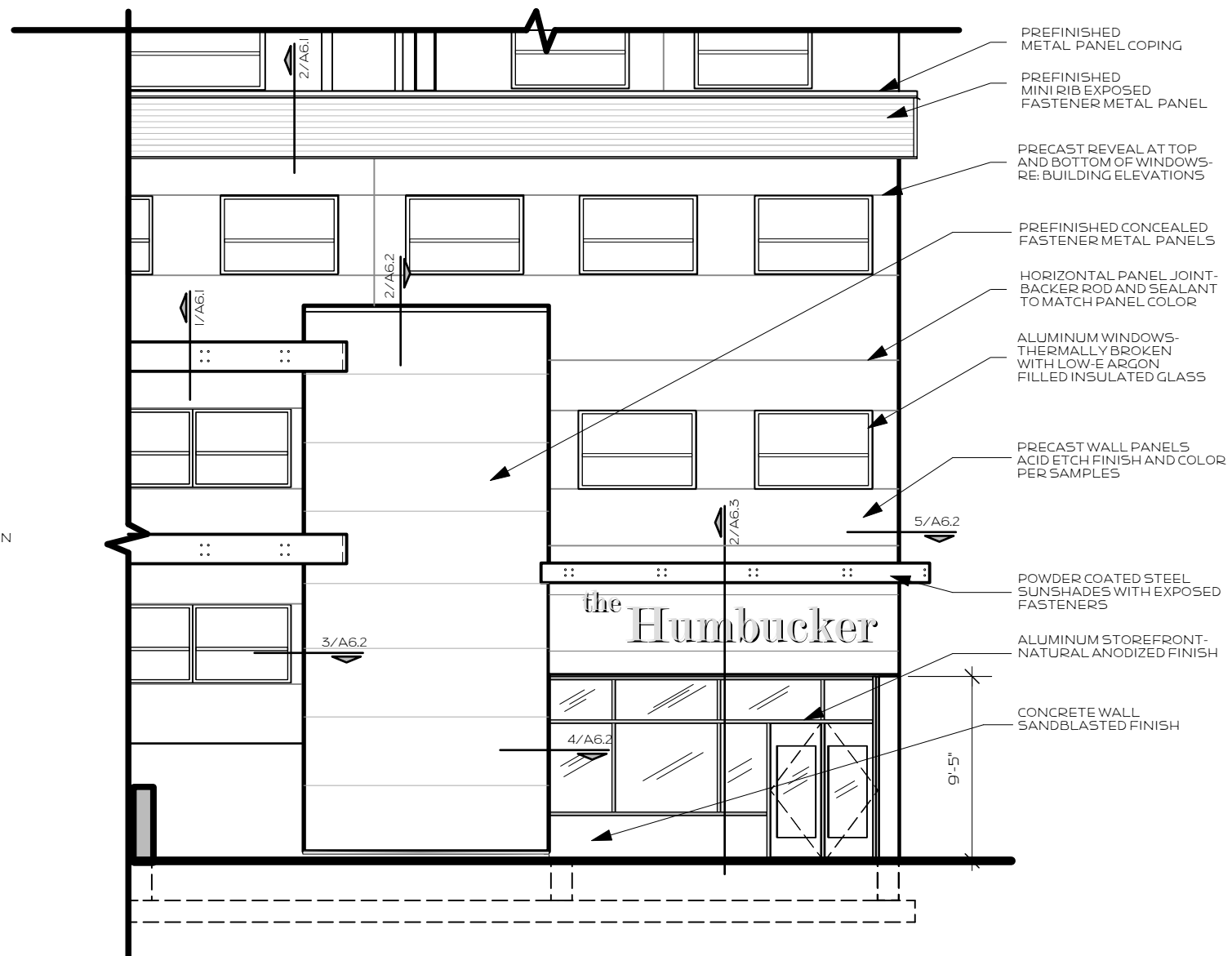
2 PANEL DETAIL



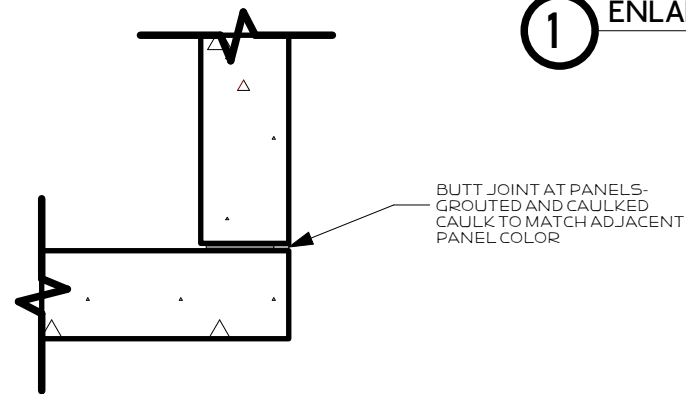
3 PANEL DETAIL



4 PANEL DETAIL



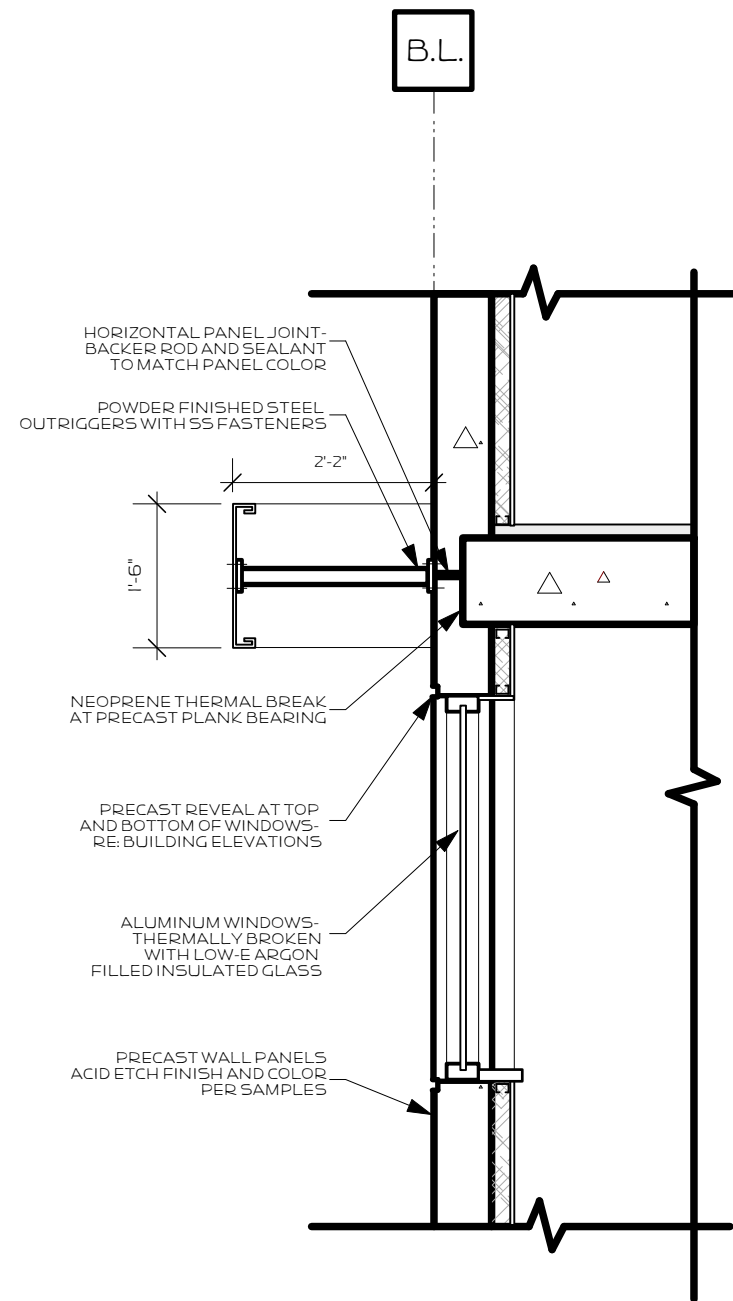
1 ENLARGED ENTRY



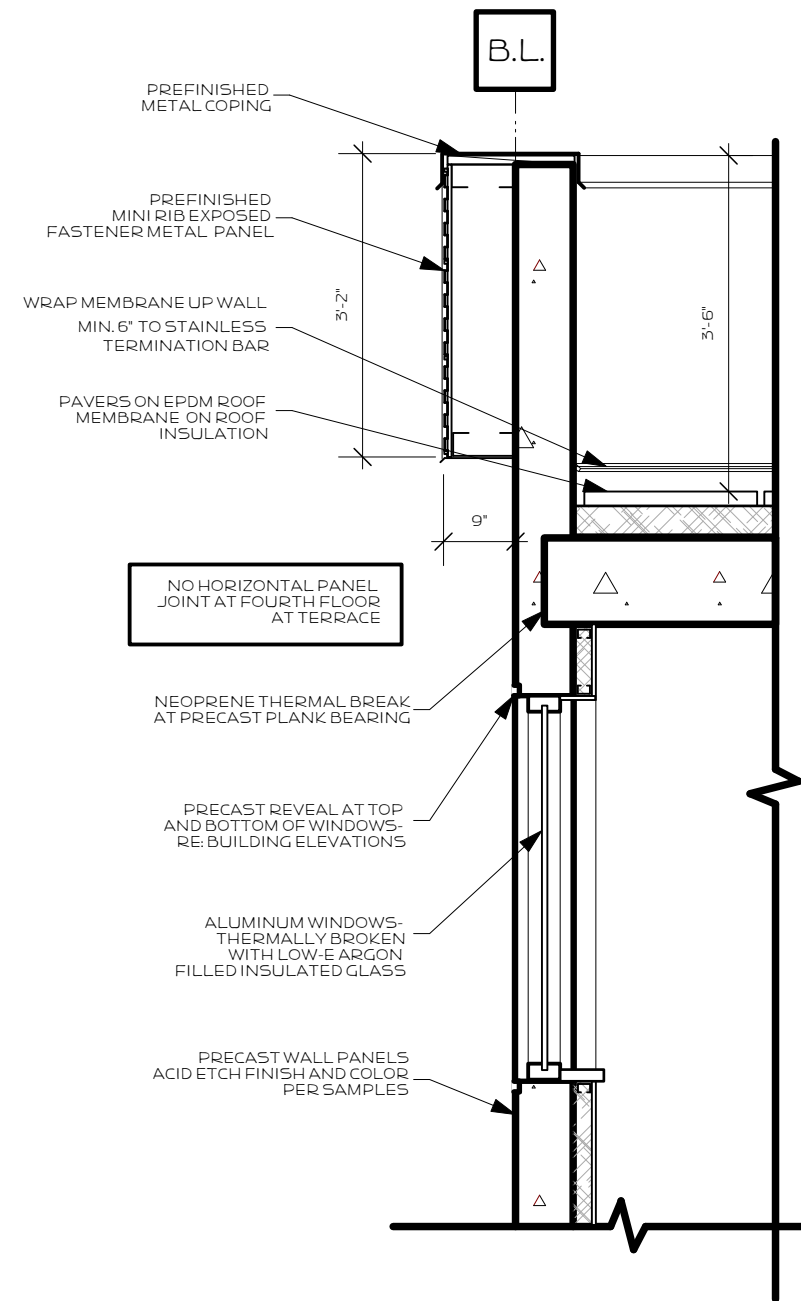
5 PRECAST CORNER DETAIL

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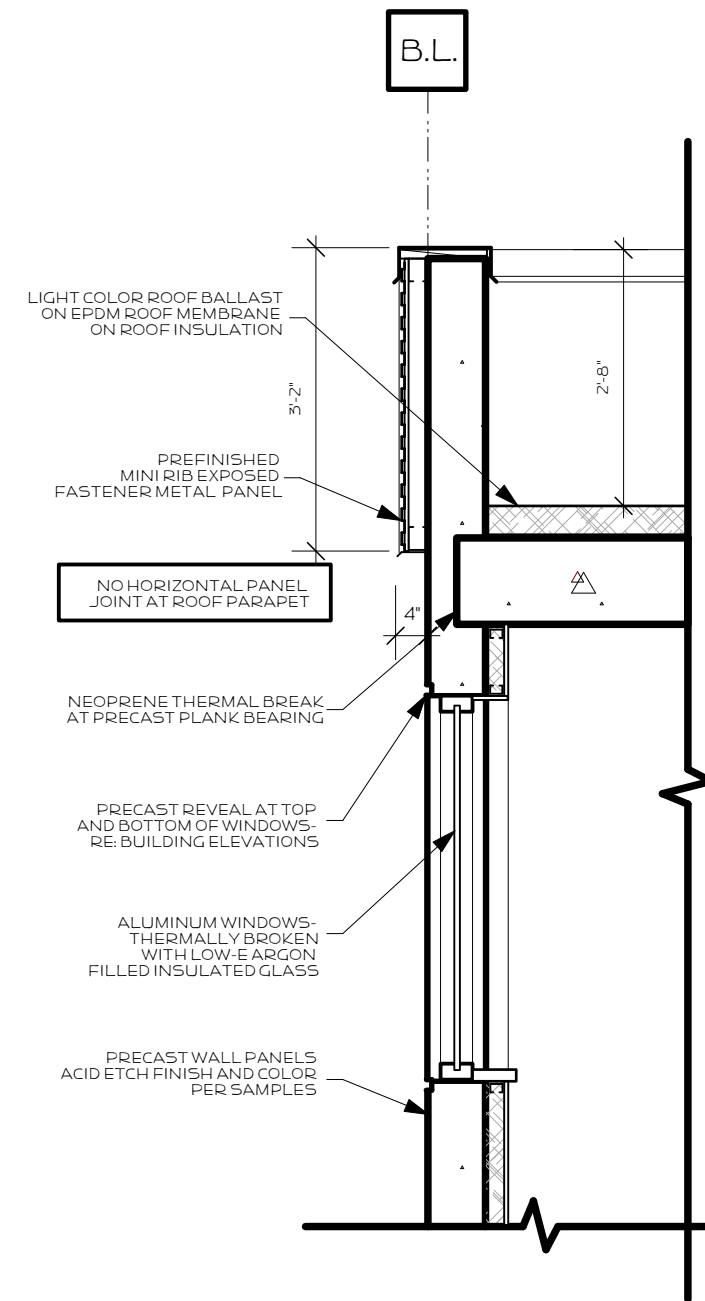
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	<small>DRAWN BY</small> <small>REVISION</small>	<small>DATE</small> <small>05.12.10</small>
THE "HUMBUCKER APARTMENTS"		A6.2
BUILDING DETAILS		



1 WALL SECTION AT FLOORS 2-3



2 WALL SECTION AT TERRACE



3 WALL SECTION AT ROOF

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THE "HUMBUCKER APARTMENTS"		
BUILDING DETAILS		A6.1

NOTE: COLORS SHOWN ARE FOR REFERENCE ONLY. SAMPLES WILL BE PROVIDED AT MEETING.



VIEW FROM SPRING STREET TO NORTH



VIEW FROM SPRING AND ORCHARD STREETS TO EAST

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THE "HUMBUCKER APARTMENTS"

PERSPECTIVES

A4 . 7

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VIEW FROM SPRING AND RANDALL STREETS TO EAST



VIEW FROM SPRING AND MILLS STREETS TO WEST

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THE "HUMBUCKER APARTMENTS"

PERSPECTIVES

A4 . 6

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VIEW FROM SPRING STREET TO NORTHWEST



VIEW FROM SPRING STREET TO NORTH

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THE "HUMBUCKER APARTMENTS"		REVISED	05.12.13
PERSPECTIVES		A4 . 5	

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NORTH ELEVATION

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THE "HUMBUCKER APARTMENTS"		A4 . 4
BUILDING ELEVATIONS <small>SCALE: 3/32"=1'-0"</small>		

NOTE: COLORS SHOWN ARE FOR REFERENCE ONLY. SAMPLES WILL BE PROVIDED AT MEETING.



WEST ELEVATION

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THE "HUMBUCKER APARTMENTS"		A4 . 3
BUILDING ELEVATIONS <small>SCALE: 3/32"=1'-0"</small>		

NOTE: COLORS SHOWN ARE FOR REFERENCE ONLY. SAMPLES WILL BE PROVIDED AT MEETING.

PREFINISHED METAL PANELS-
COLOR TO APPROXIMATE ADJACENT
PRECAST PANELS



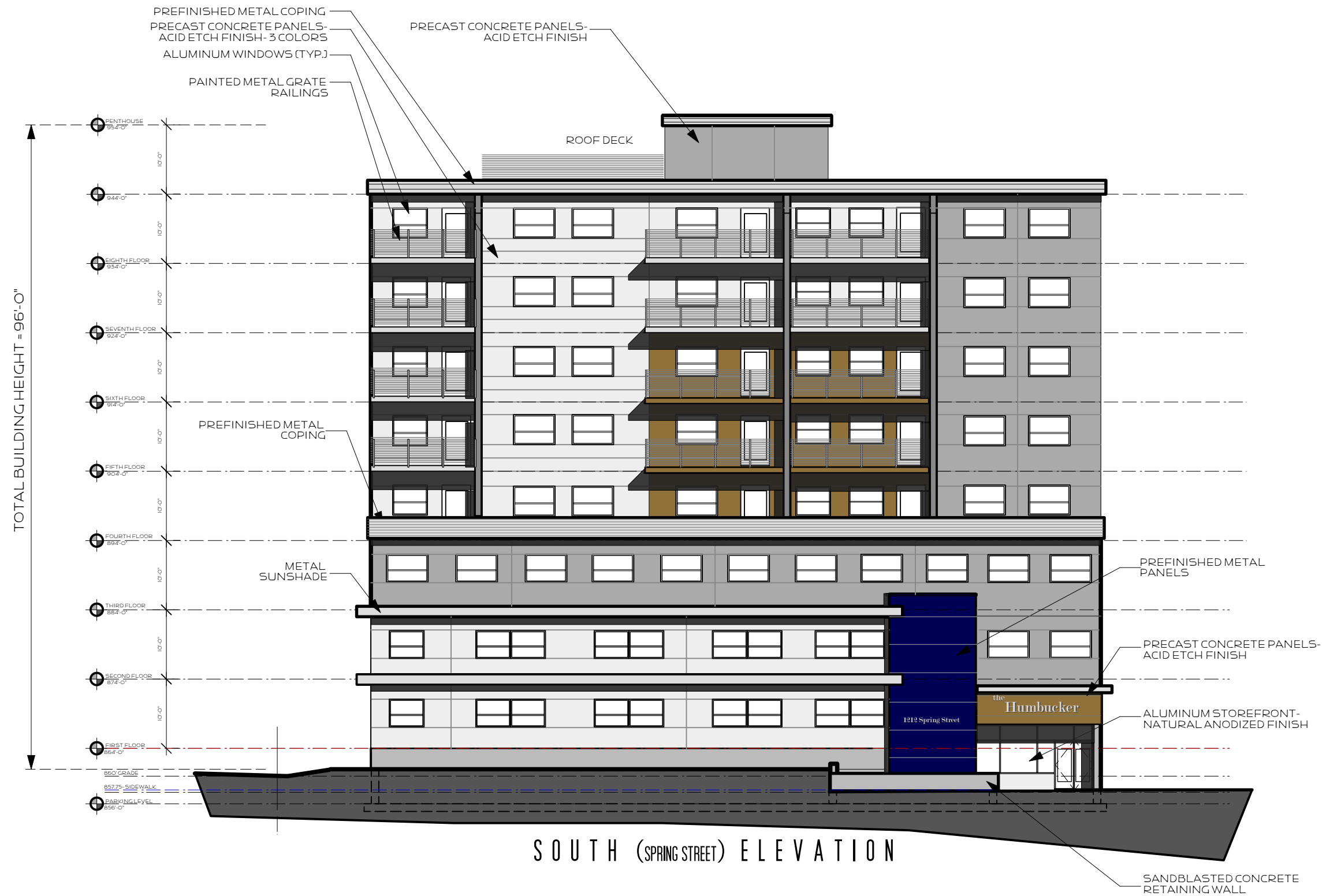
EAST ELEVATION

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THE "HUMBUCKER APARTMENTS"		A4.2
BUILDING ELEVATIONS <small>SCALE: 3/32"=1'-0"</small>		

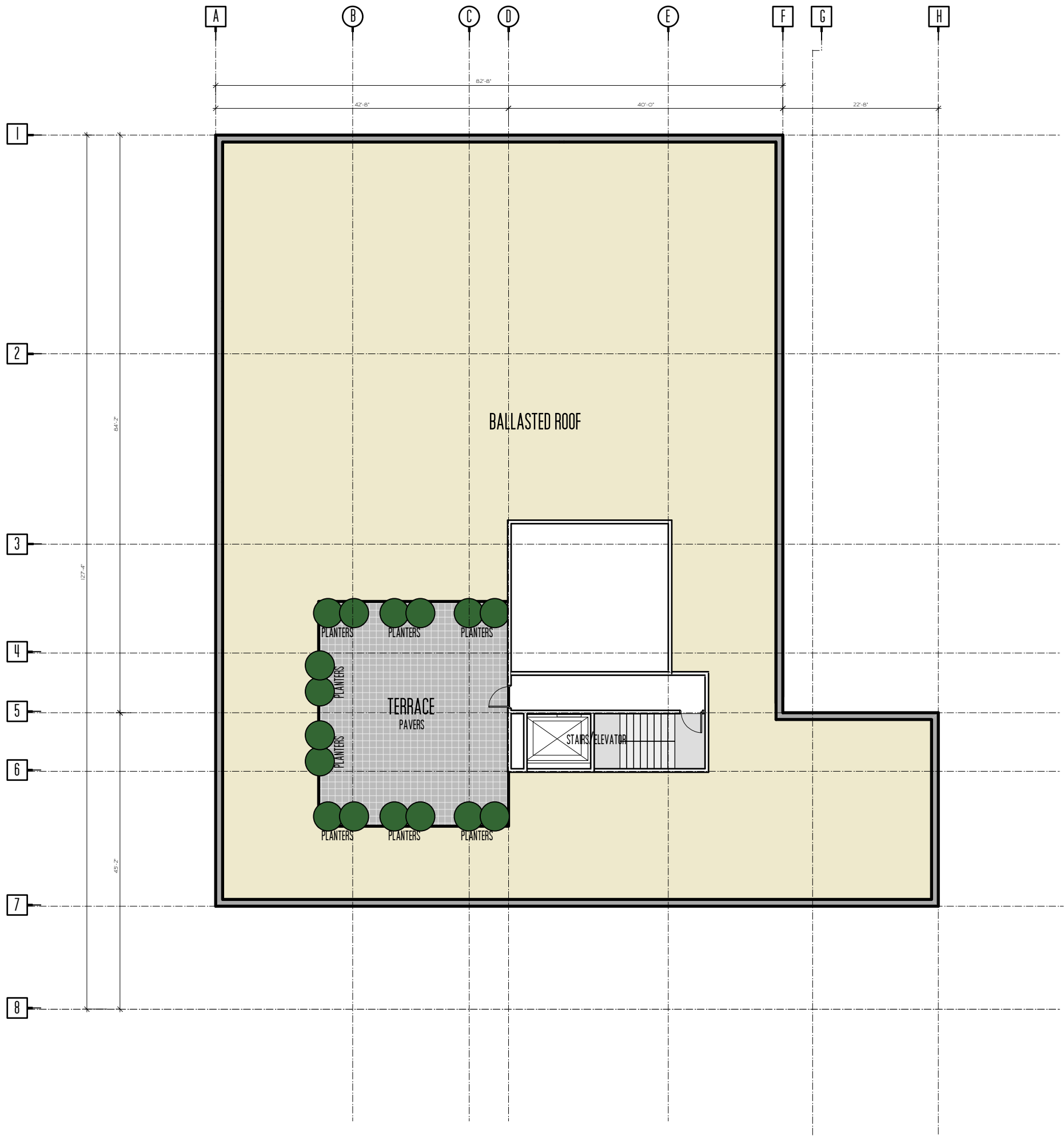
NOTE: COLORS SHOWN ARE FOR REFERENCE ONLY. SAMPLES WILL BE PROVIDED AT MEETING.



SOUTH (SPRING STREET) ELEVATION

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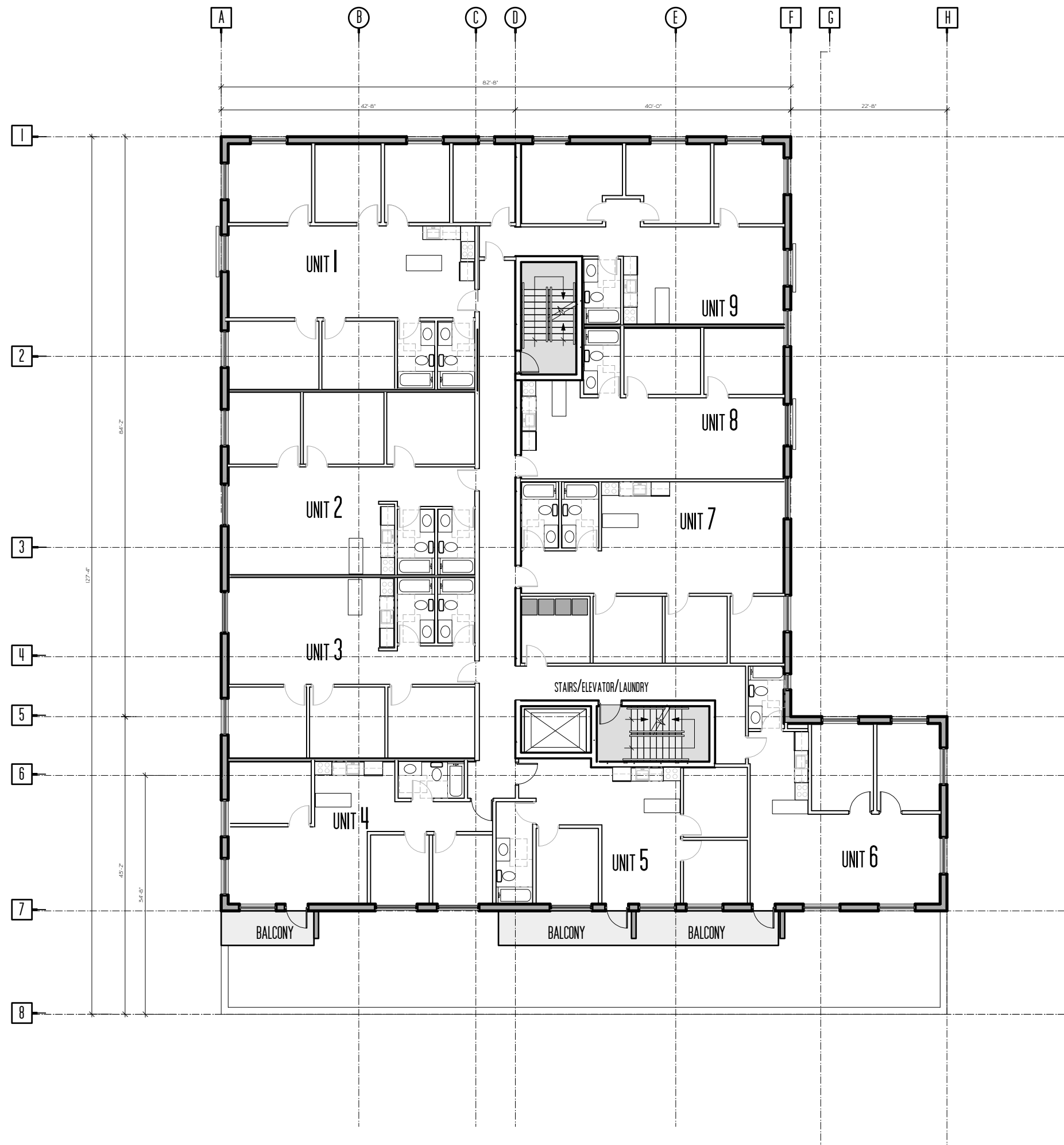
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	<small>DRAWN BY</small> <small>REVISD</small>	<small>DATE</small> <small>05/13</small>
THE "HUMBUCKER APARTMENTS"		A4 . 1
BUILDING ELEVATIONS <small>SCALE: 1/8" = 1'-0"</small>		



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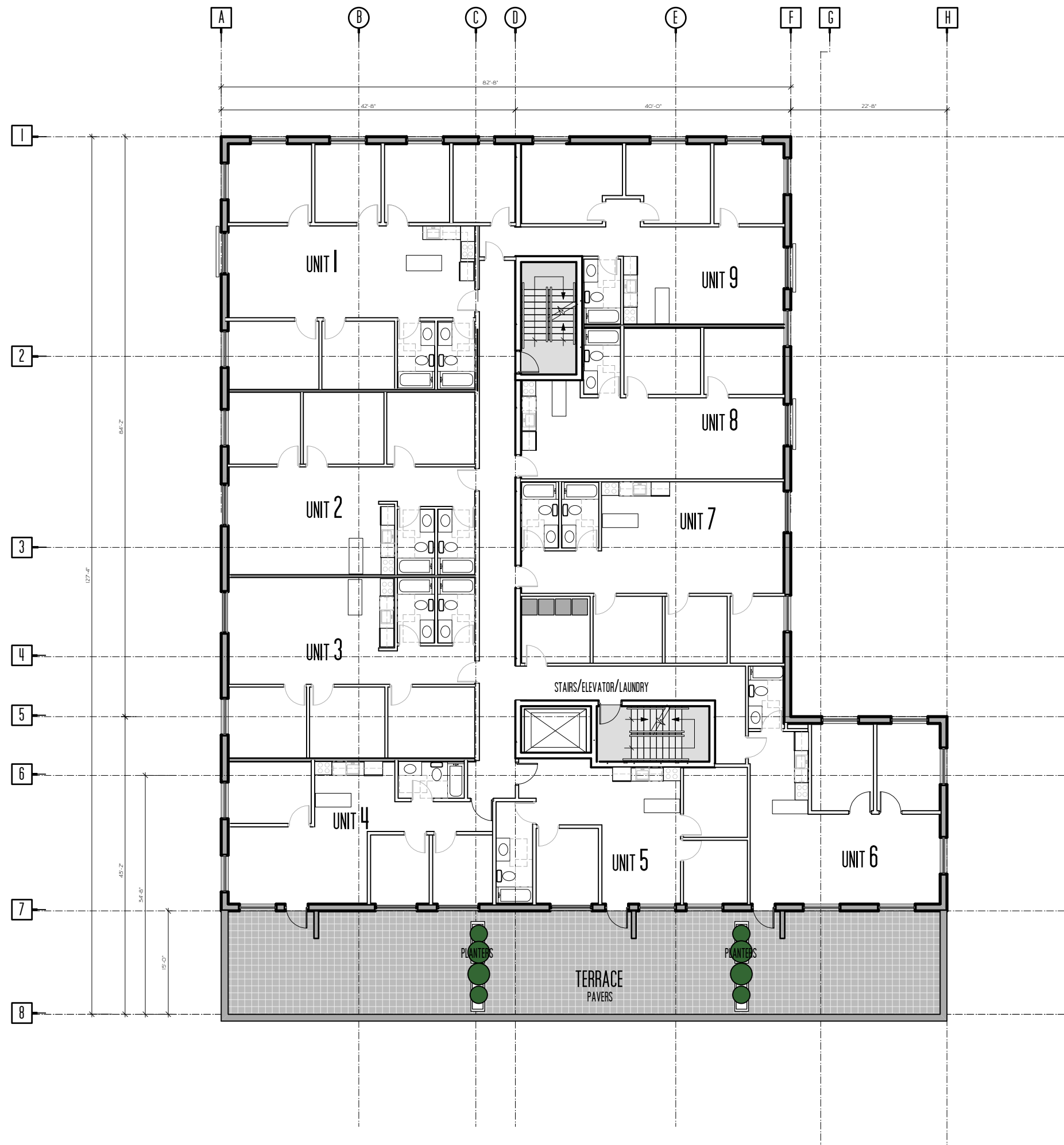
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THE "HUMBUCKER APARTMENTS"		A1.6
PENTHOUSE PLAN <small>SCALE: 1/8"=1'-0"</small>		



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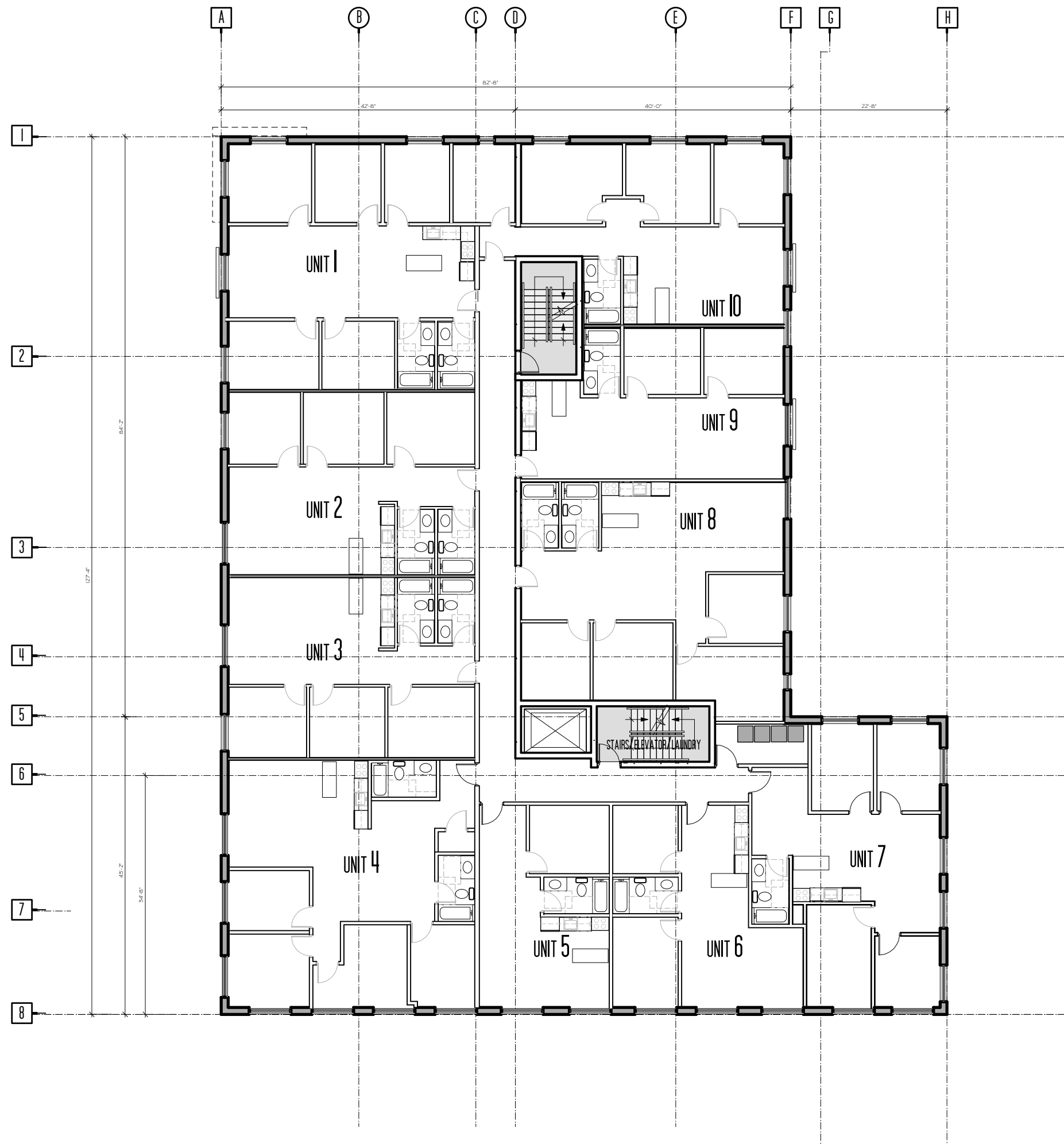
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FIFTH-EIGHTH FLOOR PLAN <small>SCALE: 1/8"=1'-0"</small>		41.5



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THE "HUMBUCKER APARTMENTS"		41.4
FOURTH FLOOR PLAN <small>SCALE: 1/8"=1'-0"</small>		



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THE "HUMBUCKER APARTMENTS"

THIRD FLOOR PLAN
SCALE: 1/8"=1'-0"

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DATE: 03/20/10

REVISED: [blank]

41.3



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THE "HUMBUCKER APARTMENTS"

SECOND FLOOR PLAN

SCALE: 1/8"=1'-0"

DATE: 03.20.10

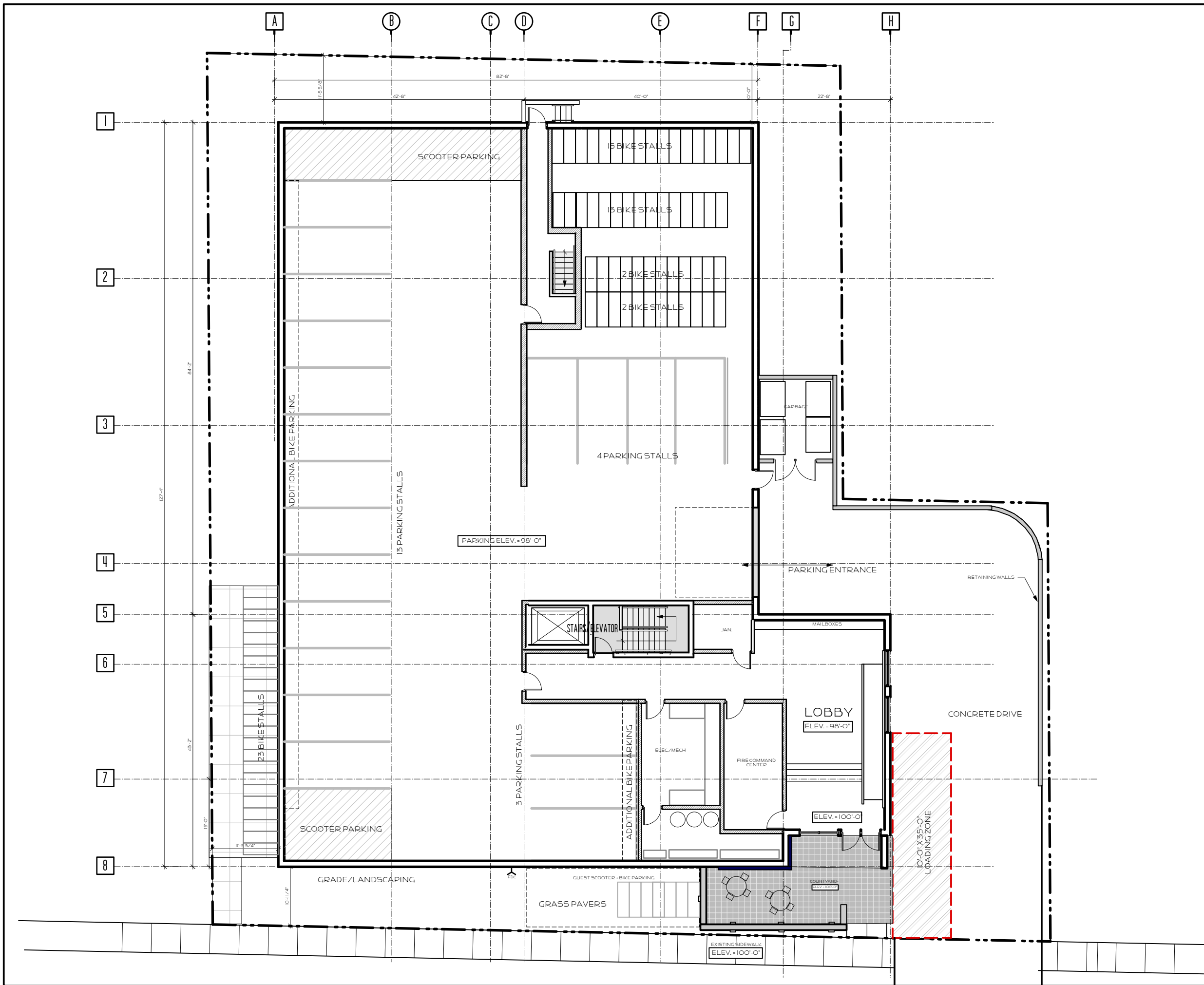
1.2



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THE "HUMBUCKER APARTMENTS"		
FIRST FLOOR PLAN		A1.1
<small>SCALE: 1/8"=1'-0"</small>		



Lock Rod



8 Bike Rack Shown



PROPOSED BIKE PARKING OPTIONS

75 BIKE PARKING STALLS PER CITY OF MADISON ZONING REQUIREMENTS REMAINING THROUGH A COMBINATION OF STORAGE SOLUTIONS THROUGHOUT THE BUILDING

FLOOR SUMMARY

11,780 S.F.

- 20 INTERIOR AUTOMOBILE PARKING STALLS
- 20 INTERIOR SCOOTER PARKING STALLS
- 190 INTERIOR BICYCLE PARKING STALLS
- 7 EXTERIOR GUEST BICYCLE PARKING STALLS
- 4 EXTERIOR GUEST SCOOTER PARKING STALLS

REVIEW SET

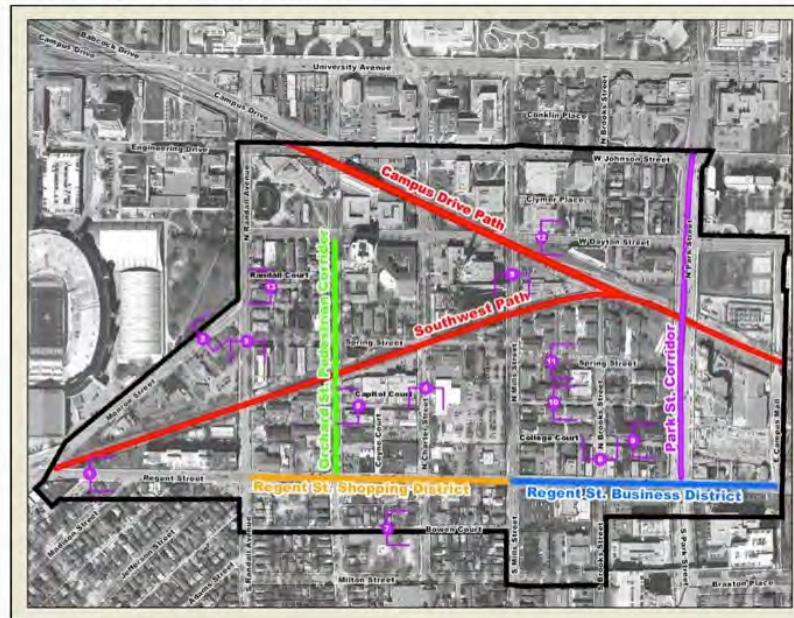
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<p>THE "HUMBUCKER APARTMENTS"</p>		
<p>GROUND LEVEL PLAN</p>		
<small>SCALE: 1/8"=1'-0"</small>		

Other Streets in Planning Area

The following pages contain design guidelines and cross sections of other streets within the planning area. The map to the right shows the location each section is based upon. These design guidelines and cross sections illustrate the recommendations applicable to the streets on which they are located.

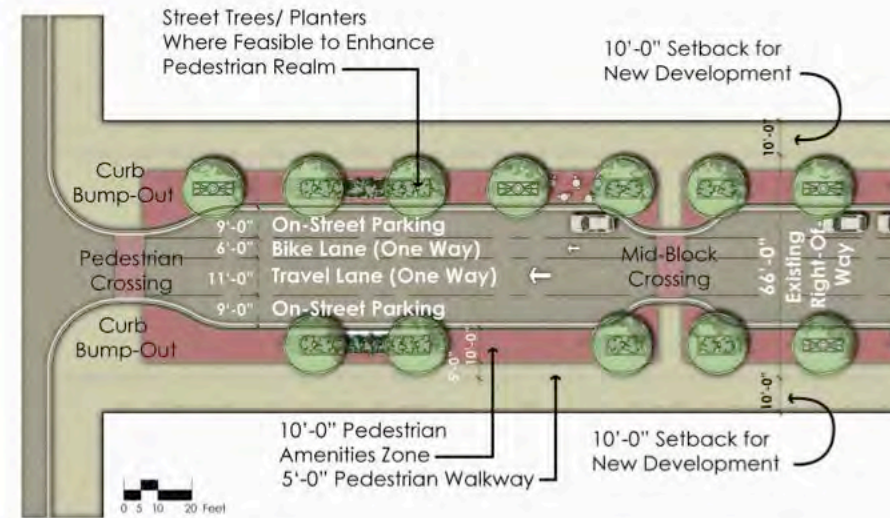
Map 4.6 Key Map to Street Cross-Sections



Design information keyed to Map 4.3 is found on the following pages, 4-33 through 4-37.

Streetscape Guidelines

Proposed configuration of Orchard Street



NOTES:

EXCERPTS FROM THE RSSC PLAN REGARDING CIRCULATION AND STREETScape IN THE IMMEDIATE AREA

into the pedestrian walkway, yet is long enough that the tree receives a sufficient amount of water.

Along the south side of the street underground utilities may prevent the planting of street trees, so planters should be placed in the amenities zone close enough to provide the same sense of safety and separation, that trees would provide. It may even be possible to plant smaller tree species in surface planters, creating an overhead canopy without interfering with access to utilities.

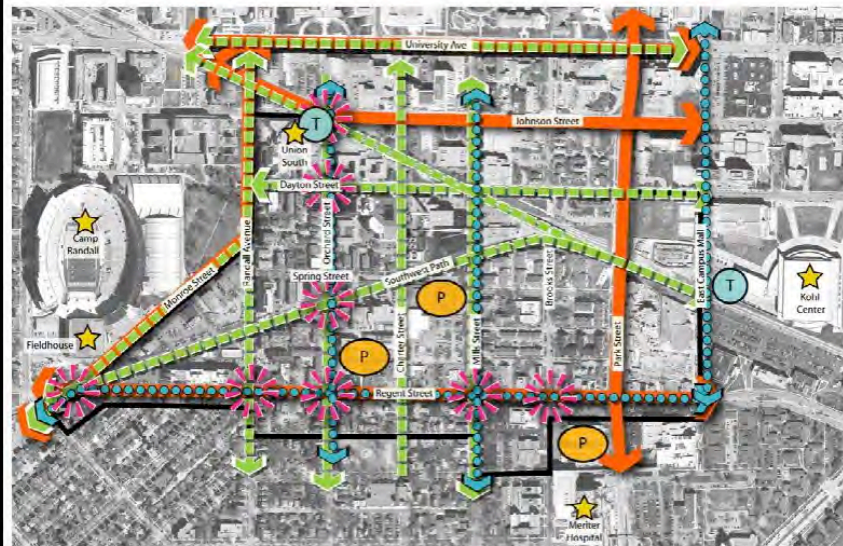
Pedestrian Amenities

Pedestrian-scale lighting is another vertical element which helps define the pedestrian realm. Pedestrian-scale lights should be lower than conventional street lights and provide more illumination of the ground plane. Dark-sky-compliant lighting should be utilized to avoid directing light upwards at night. Pedestrian-scale lighting should be closely spaced down the length of the pedestrian corridor, at marked crosswalks, and within public plaza areas. This type of lighting can take multiple forms, such as traditional pole lighting, lighted bollards, entrance lighting over doorways, and path lighting under benches and along paths.

Special paving can help direct pedestrian traffic by defining active vs. passive pedestrian areas. It can also provide a sense of place to a district. Special paving can take several forms, including stamped and/ or colored concrete, textured concrete, concrete with glass or stone mixed in,

**Map 5.4
Future Circulation**

Regent Street
South Campus
August 2007



- Primary Pedestrian Route
- Primary Bicycle Route
- Primary Auto Route
- Planned Transit Stop
- Key Intersection for Pedestrian Enhancement
- Major Destination
- General Location for Future Parking Facility
- RSSC Project Boundary

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THE "HUMBUCKER APARTMENTS"		
PROPOSED NEIGHBORHOOD CIRCULATION & IMPROVEMENTS PLAN		
		C702