TID 51 (South Madison)

27 January 2023

Joint Review Board Organizational Meeting

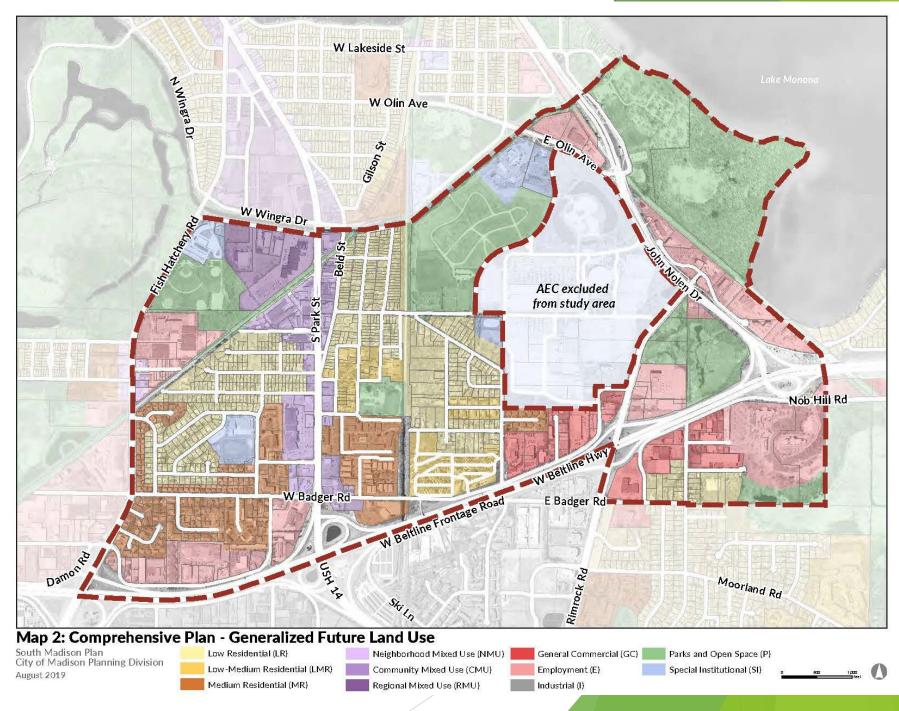
Dan Rolfs, Community Development Project Manager

Agenda:

- South Madison Plan
- Town of Madison attachment
- ► TID 51 Overview

South Madison Planning Area

(Future Land Use Map)



Top Community Issues / Concerns

Displacement/Gentrification

- Preserving single family homes
- Opportunities for community wealth building
- Affordable housing
- Opportunities to age in place in South Madison
- Preserve the mobile home park
- Preserve affordable spaces for businesses, preserve existing local businesses
- Provide spaces for neighborhood serving retail

Community Center

- Need for neighborhood gathering space
- Youth programing
- Senior programing
- Affordable, quality childcare
- Improve bicycle/pedestrian infrastructure
 - Create east/west bike route
 - Create north/south bike route
 - Extend Cannonball Path

Improve Connectivity

- Between neighborhoods
- Railroad and major road corridors barriers

Parks

- Improve access
- Need for amenities/equipment
- Programing for youth and seniors
- Improve lighting for pedestrians
- Transit Service
 - More frequent transit service
 - Service for 3rd shift workers

Guiding Principles

Anti-Displacement and Gentrification

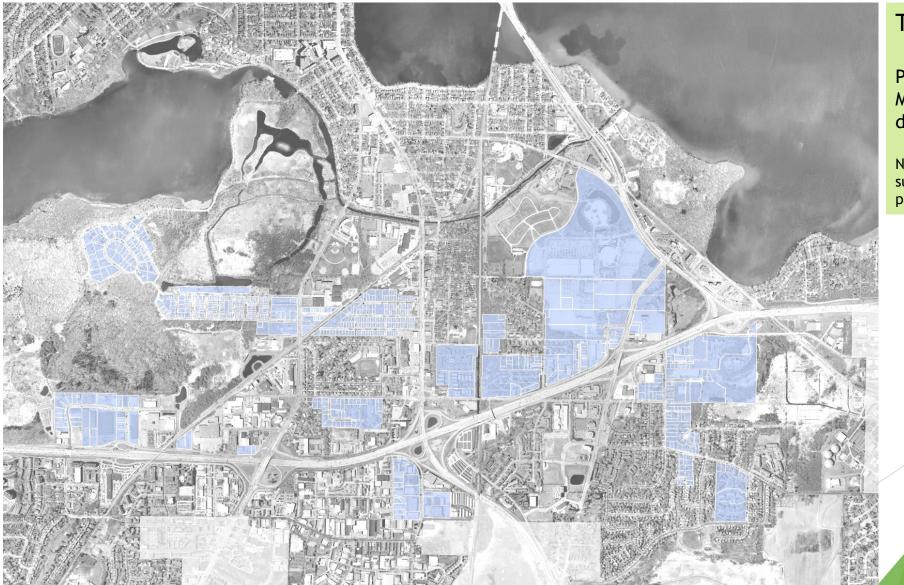
Strategies that aim to retain affordability and welcome the existing diversity of the South Madison area Community Wealth Building

Strategies that support economic growth while prioritizing existing residents and local entrepreneurs so they can successfully invest and stay in South Madison

Opportunities to Thrive

Strategies that address social aspects of succeeding - Social cohesion, Health, Food Accessibility, Education and General Wellbeing

Town of Madison attachment



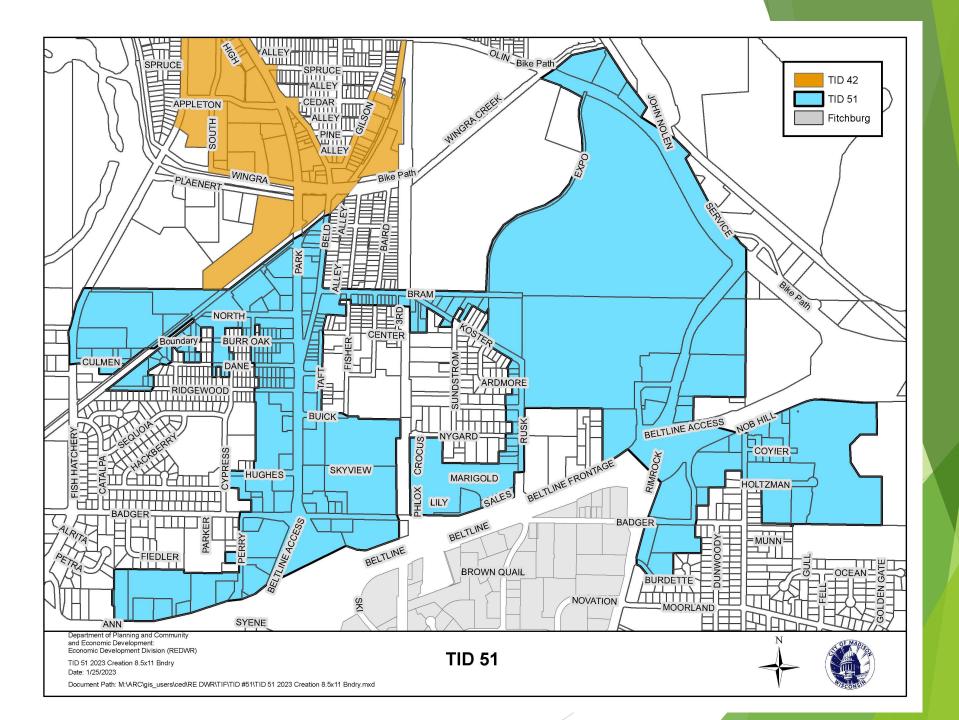
Town Parcels

Parcels attached to City of Madison Oct 31, 2022 following dissolution of Town of Madison

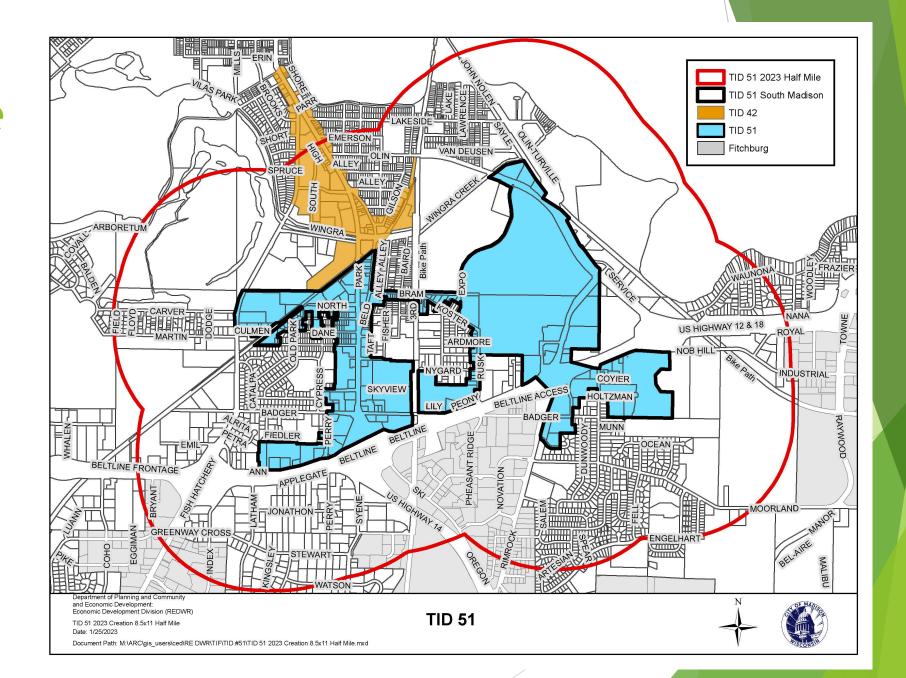
NOTE: Does not include all Town parcels such as Sherman Ave and Schroeder Rd parcels.

TID 51 Overview

TID 51



TID 51 Half Mile



TID 51 Overview

TOTAL COST:	\$115,017,500
TID Administration	\$779,000
CDA	\$15,000,000
Community Development	\$22,113,000
Economic Development	\$19,900,000
Parks	\$6,940,000
Infrastructure (City Eng)	\$50,285,500

(TIF Funds: \$99,480,000)

Major Engineering / Parks Projects

- John Nolen Drive
- Rimrock Rd
- BRT Stations
- Bike / Ped improvements
- Improvements to the following parks:
 - Badger Park
 - Cypress Spray Park
 - Penn Park
 - Bowman Park
 - Heifitz Park
 - Schmidt Park





Major CDA, Economic and Community Development Projects

- Land Banking
- Development Loans
- Commercial Ownership Assistance (COA) Program
- Façade Grant Program
- Building Improvement Grant Program
- Financial Assistance to Affordable Owner Occupied and Rental Housing
 - ► Home Buyer Assistance
- Rental Rehab program

TID 51 Funding

- ▶ \$34,280,000 Funding from TID 51
- \$65,200,000 Donated Increment
- ▶ \$99,480,000 TIF Funding

Donor District	2023	2024	2025	2026	2027	Total
TID 36	\$7,200,000	\$10,000,000	\$10,000,000	\$10,000,000	\$10,000,000	\$47,200,000
TID 37	\$3,600,000	\$3,600,000	\$3,600,000	\$3,600,000	\$3,600,000	<u>\$18,000,000</u>
					Total	\$65,200,000
Recipient TID						
TID 51	\$10,800,000	\$13,600,000	\$13,600,000	\$13,600,000	\$13,600,000	\$65,200,000
					Combined Total	\$65,200,000

Thank you

Dan Rolfs (drolfs@cityofmadison.com)

