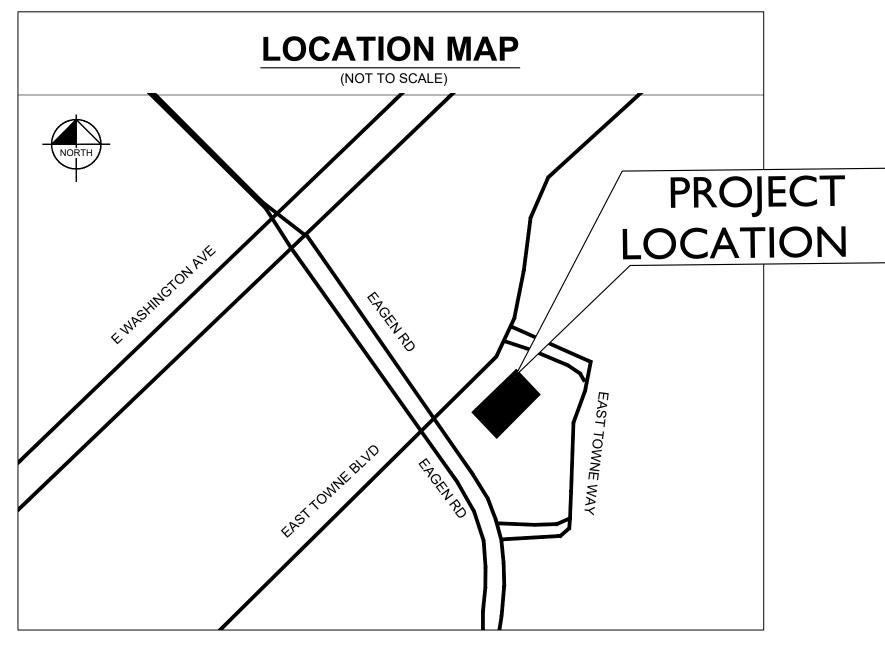
4301 E TOWNE BLVD MADISON, WISCONSIN 53704





BENCHMARKS
SEE SURVEY (SHEETVO.O) FOR LOCATION OF SITE BENCHMARKS.
BENCHMARK:
SBM#1: CUT CROSS ON TOP OF CURB ALONG EAST PROPERTY LINE
ELEVATION = 867.82' (NAVD 88)

LEGAL DESCRIPTION

LOT 3 OF CERTIFIED SURVEY MAP 14486 (VOL. 296-314) IN THE NORTHEAST AND SOUTHEAST 1/4S OF THE SOUTHEAST $\frac{1}{4}$ OF SECTION 28, TOWN 8 NORTH, RANGE 10 EAST, CITY OF MADISON, DANE COUNTY, WISCONSIN.



Kimley » Horn

S

ORIGINAL ISSUE: 08/13/2021 KHA PROJECT NO. 268025002

36037-006

REGISTERED PROFESSIONAL

ENGINEER

SHEET NUMBER C0.0

CERTIFY THAT THIS SUBMISSION, PERTAINING ONLY TO THE "C" SERIES CIVIL SHEETS LISTED ABOVE BUT EXCLUDING DETAILS PREPARED BY OTHERS, WAS PREPARED ON BEHALF OF CULVER'S AND VALLEY FOOD OPS INC. BY KIMLEY-HORN AND ASSOCIATES, INC. UNDER MY PERSONAL DIRECTION. THIS TECHNICAL SUBMISSION IS INTENDED TO BE USED AS AN INTEGRAL PART OF AND

PROFESSIONAL ENGINEER'S CERTIFICATION

I, ANDREW HEINEN, A LICENSED PROFESSIONAL ENGINEER OF WISCONSIN, HEREBY

IN CONJUNCTION WITH THE PROJECT SPECIFICATIONS AND CONTRACT DOCUMENTS.

DATED THIS 13TH DAY OF AUGUST , A.D., 2021.

Sheet List Table

GENERAL NOTES & DETAILS

CONSTRUCTION DETAILS

LANDSCAPE PLAN

TOPOGRAPHIC SURVEY

TITLE SHEET

SITE PLAN

Sheet Title

EXISTING CONDITIONS & DEMO PLAN

GRADING & EROSION CONTROL PLAN

Sheet Number

C1.0

C4.0

C5.0

A-NH.

WISCONSIN LICENSED PROFESSIONAL ENGINEER 36037-006 MY LICENSE EXPIRES ON NOVEMBER 30, 2021 DESIGN FIRM REGISTRATION NUMBER: 184002012-0006

UTILITY AND GOVERNING AGENCY CONTACTS

COMMUNITY DEVELOPMENT AUTH. MADISON MUNICIPAL BUILDING 215 MARTIN LUTHER KING JR. BLVD #161 MADISON, WI 53703 TEL: (608) 266-6520 CONTACT: MATT WACHTER

PUBLIC WORKS DEPT. 2120 FISH HATCHERY RD MADISON, WI 53713 TEL: (601) 856-8958

POWER COMPANY
MADISON GAS & ELECTRIC CO. PO BOX 1231 MADISON, WI 53701-1231 TEL: (608) 252-7222

NATURAL GAS COMPANY
MADISON GAS & ELECTRIC CO. PO BOX 1231 MADISON, WI 53701-1231 TEL: (608) 252-7222

PROJECT TEAM

VALLEY FOODS OPS INC. 4301 E TOWNE BLVD MADISON, WI 53704 TEL: (608)849-6599 CONTACT: DEAN MEIER

<u>CIVIL_ENGINEER</u> KIMLEY-HORN AND ASSOCIATES, INC. 4201 WINFIELD RD, SUITE 600 WARRENVILLE, IL 60555 TEL: (630) 487-5550 EMAIL: JOE.MAYER@KIMLEY-HORN.COM CONTACT: JOE MAYER EMAIL: ANDY.HEINEN@KIMLEY-HORN.COM CONTACT: ANDY HEINEN, P.E.

LANDSCAPE ARCHITECT KIMLEY-HORN AND ASSOCIATES, INC. 4201 WINFIELD RD, SUITE 600 WARRENVILLE, IL 60555 TEL: (630) 487-5550 EMAIL: DANIEL.GROVE@KIMLEY-HORN.COM CONTACT: DANIEL GROVE

TALARCZYK LAND SURVEYS 517 2ND AVE. NEW GLARUS, WI 53574 TEL: (608) 527-5216 CONTACT: BOB TALARCZYK

SURVEYLot 3 of Certified Survey Map 144RF . PLAT

98221 W:55 EAST TOWNE WAY 707 £457 70MME RIM ELEV: 866.76' INV. ELEV: 860.41' I hereby c that I hav correct re, Augu CHANNO THOR STAND TOWN OR TOWN OR THE SHALL SHAL PREPARED FOR: Kimley—Horn and Associate 4201 Winfield Road, Suite 6 Warrenville, IL 60555 (630) 487–5550 JOB NO. 21111 POINTS 21111_3 DRWG. 21111_3 DRAWN BY JMB

GENERAL NOTES

- EXISTING SITE TOPOGRAPHY, UTILITIES, RIGHT-OF-WAY AND HORIZONTAL CONTROL SHOWN ON THE DRAWINGS WERE OBTAINED FROM A SURVEY PREPARED BY:
 - TALARCZYK LAND SURVEYS 517 2ND AVE. NEW GLARUS, WI 53574

INCORPORATED BY REFERENCE

- COPIES OF THE SURVEY ARE AVAILABLE FROM THE ENGINEER. SITE CONDITIONS MAY HAVE CHANGED SINCE THE SURVEY WAS PREPARED. CONTRACTORS TO VISIT SITE TO FAMILIARIZE THEMSELVES WITH THE CURRENT CONDITIONS.
- 2. COPIES OF SOILS INVESTIGATION REPORTS MAY BE OBTAINED FROM THE OWNER. ANY BRACING, SHEETING OR SPECIAL CONSTRUCTION METHODS DEEMED NECESSARY BY THE CONTRACTOR IN ORDER TO INSTALL THE PROPOSED IMPROVEMENTS SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE PROJECT. ANY ADDITIONAL SOILS DATA NEEDED TO CONFIRM THE CONTRACTOR'S OPINIONS OF THE SUBSOIL CONDITIONS SHALL BE DONE AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL OBTAIN THE OWNER'S WRITTEN AUTHORIZATION TO ACCESS THE SITE TO CONDUCT A SUPPLEMENTAL SOILS INVESTIGATION.
- 3. THE CONTRACTOR SHALL PHOTOGRAPH THE WORK AREA PRIOR TO CONSTRUCTION FOR THE PURPOSE OF DOCUMENTING EXISTING CONDITIONS.
- 4. EXCEPT WHERE MODIFIED BY THE CONTRACT DOCUMENTS, ALL PROPOSED WORK SHALL BE IN ACCORDANCE WITH THE FOLLOWING SPECIFICATIONS WHICH ARE HEREBY MADE A PART HEREOF:
- A. "ROADWAY STANDARD SPECIFICATIONS, WISCONSIN DEPARTMENT OF TRANSPORTATION," AS PREPARED BY WISDOT, CURRENT EDITION AND ITS SUPPLEMENTS.
- B. "10 STATE RECOMMENDED STANDARDS FOR WASTEWATER FACILITIES", AS PUBLISHED BY HEALTH RESEARCH INC., LATEST EDITION.
- C. REGULATIONS, STANDARDS AND GENERAL REQUIREMENTS SET FORTH BY THE CITY OF MADISON, UNLESS OTHERWISE NOTED ON THE PLANS.
- D. THE NATIONAL ELECTRIC CODE.E. ALL APPLICABLE PROVISIONS OF THE OCCUPATIONAL SAFETY AND HEALTH ACT ARE HEREIN
- STANDARD SPECIFICATIONS, SUPPLEMENTAL SPECIFICATIONS, AND RECURRING SPECIAL PROVISIONS, CONSTRUCTION PLANS, AND SUBSEQUENT DETAILS ARE ALL TO BE CONSIDERED AS PART OF THE CONTRACT. INCIDENTAL ITEMS OR ACCESSORIES NECESSARY TO COMPLETE THE CONTRACTOR'S WORK MAY NOT BE SPECIFICALLY NOTED, BUT ARE CONSIDERED A PART OF THE CONTRACTOR'S CONTRACT.
- 6. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT ALL ITEMS REQUIRED FOR CONSTRUCTION OF THE PROJECT, AS SHOWN ON THE PLANS, ARE INCLUDED IN THE CONTRACT. ANY ITEM NOT SPECIFICALLY INCLUDED IN THE CONTRACT, BUT SHOWN ON THE PLANS, SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY IN THE EVENT OF A DISCREPANCY WITH THE PLANS AND QUANTITIES.
- 7. THE CONTRACTOR IS RESPONSIBLE FOR HAVING A SET OF "APPROVED" ENGINEERING PLANS WITH THE LATEST REVISION DATE ON THE JOB SITE PRIOR TO THE START OF CONSTRUCTION. IF THERE ARE ANY DISCREPANCIES WITH WHAT IS SHOWN ON THE CONSTRUCTION PLANS, HE MUST IMMEDIATELY REPORT THEM TO THE SURVEYOR OR ENGINEER BEFORE DOING ANY WORK. OTHERWISE, THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, SPECIFICATIONS, AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTION FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTION, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT THE CONTRACTOR'S OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTIONS ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
- 8. THE CONTRACTOR SHALL SUBSCRIBE TO ALL GOVERNING REGULATIONS AND SHALL OBTAIN ALL NECESSARY PUBLIC AGENCY PERMITS PRIOR TO STARTING WORK. THE CONTRACTOR, BY USING THESE PLANS FOR THEIR WORK, AGREE TO HOLD HARMLESS KIMLEY—HORN AND ASSOCIATES, INC, THE CITY OF MADISON, THEIR EMPLOYEES AND AGENTS AND THE OWNER FROM AND AGAINST ANY AND ALL LIABILITY, CLAIMS, DAMAGES, AND THE COST OF DEFENSE ARISING OUT OF CONTRACTOR(S) PERFORMANCE OF THE WORK DESCRIBED HEREIN.
- 9. THE ENGINEER AND OWNER ARE NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS OR FOR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF THE WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND SPECIFICATIONS.
- 10. CONSTRUCTION MATERIALS AND/OR EQUIPMENT MAY NOT BE STORED IN THE RIGHT-OF-WAY, AS DIRECTED BY THE OWNER.
- 11. EASEMENTS FOR THE EXISTING UTILITIES, BOTH PUBLIC AND PRIVATE, AND UTILITIES WITHIN PUBLIC RIGHT-OF-WAYS ARE SHOWN ON THE PLANS ACCORDING TO AVAILABLE RECORDS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE EXACT LOCATION OF THESE UTILITY LINES AND THEIR PROTECTION FROM DAMAGE DUE TO CONSTRUCTION OPERATIONS. IF EXISTING UTILITY LINES OF ANY NATURE ARE ENCOUNTERED WHICH CONFLICT WITH LOCATIONS OF THE NEW CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT MAY BE RESOLVED.
- 12. OWNER SHALL OBTAIN EASEMENTS AND APPROVAL OF PERMITS NECESSARY TO FACILITATE CONSTRUCTION OF THE PROPOSED UTILITIES. THE CONTRACTOR, HOWEVER, SHALL FURNISH ALL REQUIRED BONDS AND EVIDENCE OF INSURANCE NECESSARY TO SECURE THESE PERMITS AND EASEMENTS.
- 13. THE CONTRACTOR SHALL PRESERVE ALL CONSTRUCTION STAKES UNTIL THEY ARE NO LONGER NEEDED.
 ANY STAKES DESTROYED OR DISTURBED BY THE CONTRACTOR PRIOR TO THEIR USE SHALL BE RESET BY
 THE SURVEYOR AT THE CONTRACTOR'S EXPENSE.
- 14. NOTIFICATION OF COMMENCING CONSTRUCTION:
- 14.A. THE CONTRACTOR SHALL NOTIFY AFFECTED GOVERNMENTAL AGENCIES IN WRITING AT LEAST THREE FULL WORKING DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION. IN ADDITION, THE CONTRACTOR SHALL NOTIFY, AS NECESSARY, ALL TESTING AGENCIES, THE CITY OF MADISON, AND THE OWNER SUFFICIENTLY IN ADVANCE OF CONSTRUCTION.
- 14.B. FAILURE OF THE CONTRACTOR TO ALLOW PROPER NOTIFICATION TIME WHICH RESULTS IN THE TESTING COMPANIES TO BE UNABLE TO VISIT THE SITE AND PERFORM TESTING WILL CAUSE THE CONTRACTOR TO SUSPEND THE OPERATION TO BE TESTED UNTIL THE TESTING AGENCY CAN SCHEDULE TESTING OPERATIONS. COST OF SUSPENSION OF WORK SHALL BE BORNE BY THE CONTRACTOR
- 15. ALL CONTRACTORS SHALL KEEP ACCESS AVAILABLE AT ALL TIMES FOR ALL EMERGENCY TRAFFIC, AS DIRECTED BY THE CITY OF MADISON.
- 16. ANY EXISTING SIGNS, LIGHT STANDARDS, AND UTILITY POLES THAT INTERFERE WITH CONSTRUCTION OPERATIONS AND ARE NOT NOTED ON THE PLANS FOR DISPOSAL SHALL BE REMOVED AND RESET BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE, AS DIRECTED BY THE ENGINEER. ANY DAMAGE TO THESE ITEMS SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE TO THE SATISFACTION OF THE OWNER. ANY SIGNS NOT REQUIRED TO BE RESET SHALL BE DELIVERED TO THE RESPECTIVE OWNERS.
- 17. ALL TREES TO BE SAVED SHALL BE IDENTIFIED PRIOR TO CONSTRUCTION BY THE LANDSCAPE ARCHITECT AND SHALL BE PROTECTED PER WISDOT SECTION 202. THE RIGHT—OF—WAY LINE AND LIMITS OF THE CONTRACTOR'S OPERATIONS SHALL BE CLEARLY DEFINED THROUGHOUT THE CONSTRUCTION PERIOD. ALL TREES NOTED TO REMAIN SHALL BE PROTECTED FROM DAMAGE TO TRUNKS, BRANCHES AND ROOTS. NO EXCAVATING, FILLING OR GRADING IS TO BE DONE INSIDE THE DRIP LINE OF TREES UNLESS OTHERWISE INDICATED.
- 18. LIMB PRUNING SHALL BE PERFORMED UNDER THE SUPERVISION OF AN APPROVED LANDSCAPE ARCHITECT FORESTER, OR ARBORIST AND SHALL BE UNDERTAKEN IN A TIMELY FASHION SO AS NOT TO INTERFERE WITH CONSTRUCTION. ALL LIMBS, BRANCHES, AND OTHER DEBRIS RESULTING FROM THE CONTRACTOR'S WORK SHALL BE DISPOSED OF OFF-SITE BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE. ALL CUTS OVER ONE (1) INCH IN DIAMETER SHALL BE PAINTED WITH AN APPROVED TREE PAINT.
- 19. ALL EXISTING PAVEMENT OR CONCRETE TO BE REMOVED SHALL BE SAWCUT ALONG LIMITS OF PROPOSED REMOVAL BEFORE COMMENCEMENT OF PAVEMENT REMOVAL.
- 20. ALL EXISTING UTILITIES OR IMPROVEMENTS, INCLUDING WALKS, CURBS, PAVEMENT, AND PARKWAYS DAMAGED OR REMOVED DURING CONSTRUCTION SHALL BE PROMPTLY RESTORED TO THEIR RESPECTIVE ORIGINAL CONDITION. THE CONTRACTOR'S WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A PAY ITEM IS LISTED ON THE BID LIST.
- 21. REMOVAL OF SPECIFIED ITEMS, INCLUDING BUT NOT LIMITED TO, PAVEMENT, SIDEWALK, CURB, CURB AND GUTTER, CULVERTS, ETC., SHALL BE DISPOSED OF OFF-SITE BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE. THE CONTRACTOR IS RESPONSIBLE FOR ANY PERMITS REQUIRED FOR SUCH DISPOSAL.
- 22. THE CONTRACTOR SHALL COLLECT AND REMOVE ALL CONSTRUCTION DEBRIS, EXCESS MATERIALS, TRASH, OIL AND GREASE RESIDUE, MACHINERY, TOOLS, AND OTHER MISCELLANEOUS ITEMS WHICH WERE NOT PRESENT PRIOR TO PROJECT COMMENCEMENT AT NO ADDITIONAL EXPENSE TO THE OWNER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ACQUIRING ANY AND ALL PERMITS NECESSARY FOR THE HAULING AND DISPOSAL REQUIRED FOR CLEANUP, AS DIRECTED BY THE ENGINEER OR OWNER. BURNING ON THE SITE IS NOT PERMITTED.
- 23. NO UNDERGROUND WORK WITHIN THE PUBLIC RIGHT-OF-WAY SHALL BE COVERED UNTIL IT HAS BEEN APPROVED BY THE CITY OF MADISON. APPROVAL TO PROCEED MUST BE OBTAINED FROM THE CITY OF MADISON PRIOR TO INSTALLING PAVEMENT BASE, BINDER, AND SURFACE, AND PRIOR TO POURING ANY CONCRETE AFTER FORMS HAVE BEEN SET, AS NECESSARY.
- 24. WHERE SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER, EXISTING DRAINAGE STRUCTURES AND PIPE SHALL BE CLEANED OF DEBRIS AND PATCHED AS NECESSARY TO ASSURE INTEGRITY OF THE STRUCTURE. THE CONTRACTOR'S WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE MERGED INTO THE CONTRACT UNIT PRICE EACH FOR STRUCTURES AND CONTRACT UNIT PRICE PER LINEAL FOOT FOR STORM SEWERS, WHICH SHALL BE PAYMENT IN FULL FOR CLEANING, PATCHING, REMOVAL, AND DISPOSAL OF DEBRIS AND DIRT. DRAINAGE STRUCTURES AND STORM SEWERS CONSTRUCTED AS PART OF THE CONTRACTOR'S PROJECT SHALL BE MAINTAINED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE. NO EXTRA PAYMENT WILL BE MADE FOR CLEANING STRUCTURES OR STORM SEWERS CONSTRUCTED AS PART OF THE CONTRACTOR'S PROJECT.
- 25. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING THE UTILITY COMPANIES LOCATE THEIR FACILITIES IN THE FIELD PRIOR TO CONSTRUCTION AND SHALL ALSO BE RESPONSIBLE FOR THE MAINTENANCE AND PRESERVATION OF THESE FACILITIES. THE ENGINEER DOES NOT WARRANT THE LOCATION OF ANY EXISTING UTILITIES SHOWN ON THE PLANS. THE CONTRACTOR SHALL CALL DIGGERS HOTLINE LOCATING SERVICES (1–800–242–8511) AND THE CITY OF MADISON FOR UTILITY LOCATIONS.
- 26. THE GENERAL CONTRACTOR SHALL COORDINATE WITH UTILITY COMPANIES TO PROVIDE CABLE TV, PHONE, ELECTRIC, GAS AND IRRIGATION SERVICES. GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR SECURING SITE LAYOUTS FOR THESE UTILITIES AND SHALL COORDINATE AND PROVIDE CONDUIT CROSSINGS AS REQUIRED. THIS COORDINATION SHALL BE CONSIDERED INCIDENTAL TO GENERAL CONTRACTOR AGREEMENT WITH THE OWNER. ANY CONFLICTS IN UTILITIES SHALL BE CORRECTED BY THE GENERAL CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
- 27. CONTRACTOR IS TO VERIFY ALL EXISTING STRUCTURES AND FACILITIES AT ALL PROPOSED UTILITY CONNECTION LOCATIONS AND NOTIFY ENGINEER OF ANY DISCREPANCIES PRIOR TO ORDERING MATERIAL AND STARTING WORK.
- 28. ANY FIELD TILES ENCOUNTERED SHALL BE INSPECTED BY THE ENGINEER. THE DRAIN TILE SHALL BE CONNECTED TO THE STORM SEWER SYSTEM AND A RECORD KEPT BY THE CONTRACTOR OF THE LOCATIONS AND TURNED OVER TO THE ENGINEER UPON COMPLETION OF THE PROJECT. THE COST OF THE CONTRACTOR'S WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT, AND NO ADDITIONAL COMPENSATION SHALL BE ALLOWED.
- 29. ALL FRAMES AND LIDS FOR STORM AND SANITARY SEWERS, VALVE VAULT COVERS, FIRE HYDRANTS, AND B-BOXES ARE TO BE ADJUSTED TO MEET FINISHED GRADE. THE CONTRACTOR'S ADJUSTMENT IS TO BE MADE BY THE SEWER AND WATER CONTRACTOR, AND THE COST IS TO BE CONSIDERED INCIDENTAL. THESE ADJUSTMENTS TO FINISHED GRADE WILL NOT ALLEVIATE THE CONTRACTOR FROM ANY ADDITIONAL

- ADJUSTMENTS AS REQUIRED BY THE CITY OF MADISON UPON FINAL INSPECTION OF THE PROJECT.

 30. HYDRANTS SHALL NOT BE FLUSHED DIRECTLY ONTO THE ROAD SUBGRADES. WHENEVER POSSIBLE, HOSES SHALL BE USED TO DIRECT THE WATER INTO LOT AREAS OR THE STORM SEWER SYSTEM, IF AVAILABLE. DAMAGE TO THE ROAD SUBGRADE OR LOT GRADING DUE TO EXCESSIVE WATER SATURATION AND/OR FROSION FROM HYDRANT FLUSHING. OR FROM LEAKS IN THE WATER DISTRIBUTION SYSTEM. WILL BE
- DAMAGE TO THE ROAD SUBGRADE OR LOT GRADING DUE TO EXCESSIVE WATER SATURATION AND/OR EROSION FROM HYDRANT FLUSHING, OR FROM LEAKS IN THE WATER DISTRIBUTION SYSTEM, WILL BE REPAIRED BY THE CONTRACTOR FLUSHING OR USING THE HYDRANT AT THE CONTRACTOR'S OWN EXPENSE. LEAKS IN THE WATER DISTRIBUTION SYSTEM SHALL BE THE RESPONSIBILITY OF THE WATER MAIN CONTRACTOR AND SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- 31. TRENCH BACKFILL WILL BE REQUIRED TO THE FULL DEPTH ABOVE SEWERS AND WATERMAIN WITHIN TWO (2) FEET HORIZONTAL OF PROPOSED OR EXISTING PAVEMENT.
- 32. IF SOFT, SPONGY, OR OTHER UNSUITABLE SOILS WITH UNCONFINED COMPRESSIVE STRENGTH LESS THAN 0.5 TSF ARE ENCOUNTERED AT THE BOTTOM OF THE TRENCH, ALL SUCH MATERIAL SHALL BE REMOVED AND REPLACED WITH WELL—COMPACTED, CRUSHED LIMESTONE BEDDING MATERIAL. IF ROCK IS ENCOUNTERED, IT SHALL BE REMOVED TO AT LEAST SIX (6) INCHES BELOW THE BOTTOM OF THE PIPE TO ALLOW PROPER THICKNESS OF BEDDING. ANY UNDERCUTS OF TWO (2) FEET OR LESS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. DEPTHS GREATER THAN TWO (2) FEET SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO PROCEEDING.
- 33. THE TRENCHES FOR PIPE INSTALLATION SHALL BE KEPT DRY AT ALL TIMES DURING PIPE PLACEMENT. APPROPRIATE FACILITIES TO MAINTAIN THE DRY TRENCH SHALL BE PROVIDED BY THE CONTRACTOR, AND THE COST OF SUCH SHALL BE INCIDENTAL TO THE UNIT PRICE BID FOR THE ITEM. PLANS FOR THE SITE DEWATERING, IF EMPLOYED, SHALL BE SUBMITTED TO AND APPROVED BY THE OWNER PRIOR TO IMPLEMENTATION. NO ADDITIONAL COMPENSATION SHALL BE MADE FOR DEWATERING DURING CONSTRUCTION UNLESS APPROVED IN WRITING BY THE OWNER.
- 34. AFTER THE STORM SEWER SYSTEM HAS BEEN CONSTRUCTED, THE CONTRACTOR SHALL PLACE PROPER INLET PROTECTION EROSION CONTROL AT LOCATIONS INDICATED BY THE ENGINEER. THE PURPOSE OF THE INLET PROTECTION WILL BE TO MINIMIZE THE AMOUNT OF SILTATION THAT NORMALLY WOULD ENTER THE STORM SEWER SYSTEM FROM ADJACENT AND/OR UPSTREAM DRAINAGE AREAS.
- 35. AT THE CLOSE OF EACH WORKING DAY AND AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES AND FLOW LINES SHALL BE FREE FROM DIRT AND DEBRIS.
- 36. EROSION CONTROL MEASURES SHALL BE INSTALLED IN ACCORDANCE WITH WISCONSIN DEPARTMENT OF NATURAL RESOURCES REGULATIONS AND WISDOT STANDARDS FOR SOIL EROSION AND SEDIMENTATION CONTROL AND SHALL BE MAINTAINED BY THE CONTRACTOR AND REMAIN IN PLACE UNTIL A SUITABLE GROWTH OF GRASS, ACCEPTABLE TO THE ENGINEER, HAS DEVELOPED.
- 37. THE CONTRACTOR SHALL CONFORM TO ALL EROSION CONTROL REQUIREMENTS AS SET FORTH BY THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES THROUGH THE NPDES PHASE II PERMIT PROGRAM REQUIREMENTS AND GOVERNING MUNICIPALITY. THE CONTRACTOR SHALL INSTALL AND MAINTAIN ALL EROSION CONTROL MEASURES AS INDICATED ON THE EROSION CONTROL DRAWINGS AND SPECIFICATIONS PREPARED BY KIMLEY—HORN AND ASSOCIATES, INC. KIMLEY—HORN AND ASSOCIATES, INC. IS NOT RESPONSIBLE FOR THE ACTS OR OMISSIONS OF THE CONTRACTOR, SUBCONTRACTORS OR SUPPLIERS, WHICH CONTRIBUTE TO DEFICIENCIES IN THE SWPPP OR ANY VIOLATIONS RESULTING FROM INADEQUATE EROSION CONTROL PROTECTION AND/OR DOCUMENTATION.
- 38. THE PAVEMENT SHALL BE KEPT FREE OF MUD AND DEBRIS AT ALL TIMES. IT MAY BE NECESSARY TO KEEP A SWEEPER ON—SITE AT ALL TIMES.
- 39. ALL DISTURBED AREAS OF THE RIGHT-OF-WAY SHALL BE FULLY RESTORED TO PRE-CONSTRUCTION CONDITIONS WITH A MINIMUM OF SIX (6) INCHES OF TOPSOIL, SEEDING, AND MULCH AS PER WISDOT
- 40. ALL PROPOSED GRADES SHOWN ON PLANS ARE FINISHED SURFACE ELEVATIONS, UNLESS NOTED
- 1. ALL TESTING SHALL BE THE RESPONSIBILITY AND EXPENSE OF THE CONTRACTOR. IF REQUESTED BY THE CITY OF MADISON OR ENGINEER, COPIES OF ALL TEST RESULTS SHALL BE PROVIDED TO THE
- 42. PROVIDE SMOOTH VERTICAL CURVES THROUGH HIGH AND LOW POINTS INDICATED BY SPOT ELEVATIONS. PROVIDE UNIFORM SLOPES BETWEEN NEW AND EXISTING GRADES. AVOID RIDGES AND DEPRESSIONS.
 43. WHEN REQUIRED, THE CONTRACTOR SHALL NOTIFY THE OWNER WHEN RECORD DRAWINGS CAN BE PREPARED. RECORD DRAWINGS SHALL INDICATE THE FINAL LOCATION AND LAYOUT OF ALL

IMPROVEMENTS, INCLUDING VERIFICATION OF ALL CONCRETE PADS, INVERT, RIM, AND SPOT GRADE

ELEVATIONS, AND INCORPORATE ALL FIELD DESIGN CHANGES APPROVED BY THE OWNER.

44. BEFORE ACCEPTANCE, ALL WORK SHALL BE INSPECTED BY THE CITY OF MADISON, AS NECESSARY.

EARTHWORK NOTES

ENGINEER FOR REVIEW AND APPROVAL.

- 1. GENERAL
- 1.1. IT IS THE CONTRACTOR'S RESPONSIBILITY TO UNDERSTAND THE SOIL AND GROUNDWATER CONDITIONS AT THE SITE.
- 1.2. ANY QUANTITIES IN THE BID PROPOSAL ARE INTENDED AS A GUIDE FOR THE CONTRACTOR'S USE IN DETERMINING THE SCOPE OF THE COMPLETED PROJECT. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ALL MATERIAL QUANTITIES AND BE KNOWLEDGEABLE OF ALL SITE CONDITIONS.
- 1.3. THE CONTRACTOR WILL NOTE THAT THE ELEVATIONS SHOWN ON THE CONSTRUCTION PLANS ARE FINISHED GRADE AND THAT PAVEMENT THICKNESS, TOPSOIL, ETC., MUST BE ACCOUNTED FOR.
- 1.4. THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE DURING CONSTRUCTION AND PREVENT STORMWATER FROM RUNNING INTO OR STANDING IN EXCAVATED AREAS. THE FAILURE TO PROVIDE PROPER DRAINAGE WILL NEGATE ANY POSSIBLE ADDED COMPENSATION REQUESTED DUE TO DELAYS OR UNSUITABLE MATERIALS CREATED AS A RESULT THEREOF. FINAL GRADES SHALL BE PROTECTED AGAINST DAMAGE FROM EROSION, SEDIMENTATION, AND TRAFFIC.
- 1.5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTATION OF THE SOIL EROSION AND SEDIMENTATION CONTROL MEASURES. THE INITIAL ESTABLISHMENT OF EROSION CONTROL PROCEDURES AND THE PLACEMENT OF SILT AND FILTER FENCING, ETC., TO PROTECT ADJACENT PROPERTY,
- 1.6. PRIOR TO COMMENCEMENT OF GRADING ACTIVITIES, THE CONTRACTOR SHALL ERECT A CONSTRUCTION FENCE AROUND ANY TREE DESIGNATED TO BE PRESERVED. SAID FENCE SHALL BE PLACED IN A CIRCLE CENTERED AROUND THE TREE, THE DIAMETER OF WHICH SHALL BE SUCH THAT THE ENTIRE DRIP ZONE (EXTENT OF FURTHEST EXTENDING BRANCHES) SHALL BE WITHIN THE FENCE LIMITS. THE EXISTING GRADE WITHIN THE FENCED AREA SHALL NOT BE DISTURBED.
- 1.7. EXISTING SUBSURFACE CONDITIONS WERE OBTAINED FROM A GEOTECHNICAL PREPARED BY:
- 1. TOPSOIL EXCAVATION INCLUDES:
- 1.1. EXCAVATION OF TOPSOIL AND OTHER STRUCTURALLY UNSUITABLE MATERIALS WITHIN THOSE AREAS THAT WILL REQUIRE EARTH EXCAVATION OR COMPACTED EARTH FILL MATERIAL. EXISTING VEGETATION SHALL BE REMOVED PRIOR TO STRIPPING TOPSOIL OR FILLING AREAS.
- 1.2. PLACEMENT OF EXCAVATED MATERIAL IN OWNER-DESIGNATED AREAS FOR FUTURE USE WITHIN AREAS TO BE LANDSCAPED AND THOSE AREAS NOT REQUIRING STRUCTURAL FILL MATERIAL. PROVIDE NECESSARY EROSION CONTROL MEASURES FOR STOCKPILE.
- 1.3. TOPSOIL STOCKPILED FOR RESPREAD SHALL BE FREE OF CLAY AND SHALL NOT CONTAIN ANY OF THE TRANSITIONAL MATERIAL BETWEEN THE TOPSOIL AND CLAY. THE TRANSITIONAL MATERIAL SHALL BE USED IN NON-STRUCTURAL FILL AREAS OR DISPOSED OF OFF-SITE.
- 1.4. TOPSOIL RESPREAD SHALL INCLUDE HAULING AND SPREADING SIX (6) INCHES OF TOPSOIL DIRECTLY OVER AREAS TO BE LANDSCAPED WHERE SHOWN ON THE PLANS OR AS DIRECTED BY THE OWNER.

 1.5. MODERATE COMPACTION IS REQUIRED IN NON—STRUCTURAL FILL AREAS.
- 2. EARTH EXCAVATION INCLUDES:
- 2.1. EXCAVATION OF SUBSURFACE MATERIALS WHICH ARE SUITABLE FOR USE AS STRUCTURAL FILL. THE EXCAVATION SHALL BE TO WITHIN A TOLERANCE OF 0.1 FEET OF THE PLAN SUBGRADE ELEVATIONS WHILE MAINTAINING PROPER DRAINAGE. THE TOLERANCE WITHIN PAVEMENT AREAS SHALL BE SUCH THAT THE EARTH MATERIALS SHALL "BALANCE" DURING THE FINE GRADING OPERATION.
- 2.2. PLACEMENT OF SUITABLE MATERIALS SHALL BE WITHIN THOSE AREAS REQUIRING STRUCTURAL FILL IN ORDER TO ACHIEVE THE PLAN SUBGRADE ELEVATIONS TO WITHIN A TOLERANCE OF 0.1 FEET. THE FILL MATERIALS SHALL BE PLACED IN LOOSE LIFTS THAT SHALL NOT EXCEED EIGHT (8) INCHES IN THICKNESS, AND THE WATER CONTENT SHALL BE ADJUSTED IN ORDER TO ACHIEVE REQUIRED COMPACTION.
- 2.3. STRUCTURAL FILL MATERIAL MAY BE PLACED WITHIN THOSE PORTIONS OF THE SITE NOT REQUIRING STRUCTURAL FILL, WITHIN SIX (6) INCHES OF THE PLAN FINISHED GRADE ELEVATION. IN AREAS REQUIRING STRUCTURAL FILL, HOWEVER, THIS MATERIAL SHALL NOT BE PLACED OVER TOPSOIL OR OTHER UNSUITABLE MATERIALS UNLESS SPECIFICALLY DIRECTED BY A SOILS ENGINEER WITH THE CONCURRENCE OF THE OWNER.
- 2.4. COMPACTION OF SUITABLE MATERIALS SHALL BE TO AT LEAST 93% OF THE MODIFIED PROCTOR DRY DENSITY WITHIN PROPOSED PAVEMENT AREAS, SIDEWALK, ETC. COMPACTION SHALL BE AT LEAST 95% OF THE MODIFIED PROCTOR WITHIN PROPOSED BUILDING PAD AREAS.
- 3. UNSUITABLE MATERIAL: UNSUITABLE MATERIALS SHALL BE CONSIDERED MATERIAL THAT IS NOT SUITABLE FOR THE SUPPORT OF PAVEMENT AND BUILDING CONSTRUCTION, AND IS ENCOUNTERED BELOW NORMAL TOPSOIL DEPTHS AND THE PROPOSED SUBGRADE ELEVATION. THE DECISION TO REMOVE SAID MATERIAL AND TO WHAT EXTENT SHALL BE MADE BY THE ENGINEER WITH THE CONCURRENCE OF THE OWNER.
 4. MISCELLANEOUS. THE CONTRACTOR SHALL:
- 4.1. SPREAD AND COMPACT UNIFORMLY TO THE DEGREE SPECIFIED ALL EXCESS TRENCH SPOIL AFTER COMPLETION OF THE UNDERGROUND IMPROVEMENTS.4.2. SCARIFY, DISC, AERATE, AND COMPACT, TO THE DEGREE SPECIFIED, THE UPPER TWELVE (12) INCHES
- OF THE SUITABLE SUBGRADE MATERIAL IN ALL AREAS THAT MAY BE SOFT DUE TO EXCESS MOISTURE CONTENT. THIS APPLIES TO CUT AREAS AS WELL AS FILL AREAS.

 4.3. PROVIDE WATER TO ADD TO DRY MATERIAL IN ORDER TO ADJUST THE MOISTURE CONTENT FOR THE PURPOSE OF ACHIEVING THE SPECIFIED COMPACTION.
- 4.4. BACKFILL THE CURB AND GUTTER AFTER ITS CONSTRUCTION AND PRIOR TO THE PLACEMENT OF THE BASE COURSE MATERIAL.
- 5.1. THE CONTRACTOR SHALL PROVIDE AS A MINIMUM A FULLY LOADED SIX-WHEEL TANDEM AXLE TRUCK FOR PROOF ROLLING THE PAVEMENT SUBGRADE PRIOR TO THE PLACEMENT OF THE CURB AND GUTTER AND THE BASE MATERIAL. THIS SHALL BE WITNESSED BY THE ENGINEER AND THE OWNER. (SEE PAVING
- 5.2. ANY UNSUITABLE AREA ENCOUNTERED AS A RESULT OF PROOF ROLLING SHALL BE REMOVED AND REPLACED WITH SUITABLE MATERIAL OR OTHERWISE CORRECTED AND APPROVED BY THE ENGINEER.

PAVING NOTES

1 CENERAL

- 1.1. PAVING WORK INCLUDES FINAL SUBGRADE SHAPING, PREPARATION, AND COMPACTION; PLACEMENT OF SUBBASE OR BASE COURSE MATERIALS; BITUMINOUS BINDER AND/OR SURFACE COURSES; FORMING, FINISHING, AND CURING CONCRETE PAVEMENT, CURBS, AND WALKS; AND FINAL CLEAN-UP AND ALL
- 1.2. COMPACTION REQUIREMENTS [REFERENCE ASTM D-1557 (MODIFIED PROCTOR)]: SUBGRADE = 95%; SUBBASE = 95%; AGGREGATE BASE COURSE = 95%; BITUMINOUS COURSES = 95% OF MAXIMUM
- DENSITY, PER WISCONSIN DEPARTMENT OF TRANSPORTATION (WISDOT) HIGHWAY STANDARDS.

 1.3. IT SHALL BE THE CONTRACTOR'S SOLE RESPONSIBILITY TO PROVIDE PROPER BARRICADING WARNING DEVICES, AND THE SAFE MANAGEMENT OF TRAFFIC WITHIN THE AREA OF CONSTRUCTION. ALL SUCH DEVICES AND THEIR INSTALLATION SHALL CONFORM TO THE MANUAL OF UNIFORM TRAFFIC CONTROL
- DEVICES AND THEIR INSTALLATION SHALL CONFORM TO THE MANUAL OF UNIFORM TRAFFIC CONT DEVICES (MUTCD), LATEST EDITION, AND IN ACCORDANCE WITH THE CITY OF MADISON CODE.

 2. SUBGRADE PREPARATION
- 2.1. EARTHWORK FOR PROPOSED PAVEMENT SUBGRADE SHALL BE FINISHED TO WITHIN 0.1 FOOT, PLUS OR MINUS, OF PLAN ELEVATION. THE CONTRACTOR SHALL CONFIRM THAT THE SUBGRADE HAS BEEN PROPERLY PREPARED AND THAT THE FINISHED TOP SUBGRADE ELEVATION HAS BEEN GRADED WITHIN TOLERANCES ALLOWED IN THESE SPECIFICATIONS, UNLESS THE CONTRACTOR ADVISES THE ENGINEER IN WRITING PRIOR TO FINE GRADING FOR BASE COURSE CONSTRUCTION. IT IS UNDERSTOOD THAT THE CONTRACTOR HAS APPROVED AND ACCEPTS THE RESPONSIBILITY FOR THE SUBGRADE.
- 2.2. PRIOR TO THE PLACEMENT OF THE BASE COURSE, THE SUBGRADE MUST BE PROOF—ROLLED AND INSPECTED FOR UNSUITABLE MATERIALS AND/OR EXCESSIVE MOVEMENT. IF UNSUITABLE SUBGRADE IS ENCOUNTERED, IT SHALL BE CORRECTED. THIS MAY INCLUDE ONE OR MORE OF THE FOLLOWING
- 2.2.1. SCARIFY, DISC, AND AERATE.
- 2.2.2. REMOVE AND REPLACE WITH STRUCTURAL CLAY FILL.
- 2.2.3. REMOVE AND REPLACE WITH GRANULAR MATERIAL.
- 2.2.4. USE OF GEOTEXTILE FABRIC.
- MAXIMUM DEFLECTION ALLOWED IN ISOLATED AREAS MAY BE ONE-QUARTER (1/4) INCH TO ONE-HALF (1/2) INCH IF NO DEFLECTION OCCURS OVER THE MAJORITY OF THE AREA.
- 2.3. PRIOR TO THE CONSTRUCTION OF THE CURB AND GUTTER AND THE PLACEMENT OF THE BASE MATERIAL, THE PAVEMENT AREA SHALL BE FINE—GRADED TO WITHIN 0.04 FEET (1/2 INCH) OF FINAL SUBGRADE ELEVATION, TO A POINT TWO (2) FEET BEYOND THE BACK OF THE CURB, SO AS TO ENSURE THE PROPER THICKNESS OF PAVEMENT COURSES. NO CLAIMS FOR EXCESS QUANTITY OF BASE MATERIALS DUE TO IMPROPER SUBGRADE PREPARATION WILL BE HONORED.
- 2.4. PRIOR TO PLACEMENT OF THE BASE COURSE, THE SUBGRADE SHALL BE APPROVED BY THE TESTING ENGINEER.3. CONCRETE WORK
- 3.1. ALL EXTERIOR CONCRETE SHALL BE PORTLAND CEMENT CONCRETE WITH AIR ENTRAINMENT OF NOT LESS THAN FIVE (5%) OR MORE THAN EIGHT (8%) PERCENT. CONCRETE SHALL BE A MINIMUM OF SIX (6) BAG MIX AND SHALL DEVELOP A MINIMUM OF 3,500 PSI COMPRESSIVE STRENGTH AT FOURTEEN (14) DAYS AND A MINIMUM OF 4,000 PSI COMPRESSIVE STRENGTH AT TWENTY—EIGHT (28) DAYS. ALL CONCRETE SHALL BE BROOM—FINISHED PERPENDICULAR TO THE DIRECTION OF TRAVEL.
- 3.2. CONCRETE CURB AND/OR COMBINATION CURB AND GUTTER SHALL BE OF THE TYPE SHOWN ON THE PLANS. THE CONTRACTOR IS CAUTIONED TO REFER TO THE CONSTRUCTION STANDARDS AND THE PAVEMENT CROSS SECTION TO DETERMINE THE GUTTER FLAG THICKNESS AND THE AGGREGATE BASE COURSE THICKNESS BENEATH THE CURB AND GUTTER. PRE-MOLDED FIBER EXPANSION JOINTS, WITH TWO 3/4-INCH BY 18-INCH EPOXY-COATED STEEL DOWEL BARS, SHALL BE GREASED AND FITTED WITH
- METAL EXPANSION TUBES

 3.3. CURBS SHALL BE DEPRESSED AND MEET THE SLOPE REQUIREMENTS OF THE FEDERAL ADA STANDARDS FOR ACCESSIBLE DESIGN AT LOCATIONS WHERE PUBLIC WALKS INTERSECT CURB LINES AND OTHER
- LOCATIONS, AS DIRECTED, FOR THE PURPOSE OF PROVIDING ACCESSIBILITY.

 3.4. THE CURBS SHALL BE BACKFILLED AFTER THEIR CONSTRUCTION AND PRIOR TO THE PLACEMENT OF THE BASE COURSE.
- 3.5. CONCRETE SIDEWALK SHALL BE IN ACCORDANCE WITH THE ABOVE AND THE PLANS. PROVIDE SCORED JOINTS AT 5-FOOT INTERVALS AND 1/2-INCH PRE-MOLDED FIBER EXPANSION JOINTS AT 20-FOOT INTERVALS AND ADJACENT TO CONCRETE CURBS, DRIVEWAYS, FOUNDATIONS, AND OTHER STRUCTURES.
- 3.6. CONCRETE CURING AND PROTECTION SHALL BE PER WISDOT STANDARDS. TWO (2) COATS OF WISDOT APPROVED CURING AGENT SHALL BE APPLIED TO ALL EXPOSED CONCRETE SURFACES.
- 3.7. THE COST OF AGGREGATE BASE OR SUBBASE UNDER CONCRETE WORK SHALL BE INCLUDED IN THE COST OF THE RESPECTIVE CONCRETE ITEM.
- 4. FLEXIBLE PAVEMENT
- 4.1. THE PAVEMENT MATERIALS FOR BITUMINOUS STREETS, PARKING LOTS, AND DRIVE AISLES SHALL BE AS DETAILED ON THE PLANS. UNLESS OTHERWISE SHOWN ON THE PLANS, THE FLEXIBLE PAVEMENTS SHALL CONSIST OF AGGREGATE BASE COURSE, TYPE B, BITUMINOUS CONCRETE BINDER COURSE, AND BITUMINOUS CONCRETE SURFACE COURSE, OF THE THICKNESS AND MATERIALS SPECIFIED ON THE PLANS. THICKNESSES SPECIFIED SHALL BE CONSIDERED TO BE THE MINIMUM COMPACTED THICKNESS.
- 4.2. ALL TRAFFIC SHALL BE KEPT OFF THE COMPLETED AGGREGATE BASE UNTIL THE BINDER COURSE IS LAID. THE AGGREGATE BASE SHALL BE UNIFORMLY PRIME COATED AT A RATE OF 0.4 TO 0.5 GALLONS PER SQUARE YARD PRIOR TO PLACING THE BINDER COURSE. PRIME COAT MATERIALS SHALL BE WISDOT APPROVED.
- 4.3. PRIOR TO PLACEMENT OF THE SURFACE COURSE, THE BINDER COURSE SHALL BE CLEANED AND TACK—COATED IF DUSTY OR DIRTY. ALL DAMAGED AREAS IN THE BINDER, BASE, OR CURB SHALL BE REPAIRED TO THE SATISFACTION OF THE OWNER PRIOR TO LAYING THE SURFACE COURSE. THE CONTRACTOR SHALL PROVIDE WHATEVER EQUIPMENT AND STAFF NECESSARY, INCLUDING THE USE OF POWER BROOMS IF REQUIRED BY THE OWNER, TO PREPARE THE PAVEMENT FOR APPLICATION OF THE SURFACE COURSE. THE TACK COAT SHALL BE UNIFORMLY APPLIED TO THE BINDER COURSE AT A RATE
- OF 0.05 TO 0.10 GALLONS PER SQUARE YARD. TACK COAT SHALL BE AS PER WISDOT STANDARDS.

 4.4. SEAMS IN BAM, BINDER, AND SURFACE COURSE SHALL BE STAGGERED A MINIMUM OF 6 INCHES.

 5. TESTING AND FINAL ACCEPTANCE.
- 5.1. THE CONTRACTOR SHALL FOLLOW THE QUALITY CONTROL TESTING PROGRAM FOR CONCRETE AND PAVEMENT MATERIALS ESTABLISHED BY THE MATERIALS/TESTING ENGINEER.
- 5.2. PRIOR TO PLACEMENT OF THE BITUMINOUS CONCRETE SURFACE COURSE, THE CONTRACTOR, WHEN REQUIRED BY THE CITY OF MADISON, SHALL OBTAIN SPECIMENS OF THE BINDER COURSE WITH A CORE DRILL WHERE DIRECTED, FOR THE PURPOSE OF THICKNESS VERIFICATION.
- 5.3. WHEN REQUIRED BY THE CITY OF MADISON, THE CONTRACTOR SHALL OBTAIN SPECIMENS OF THE FULL DEPTH BITUMINOUS CONCRETE PAVEMENT STRUCTURE WITH A CORE DRILL WHERE DIRECTED IN ORDER TO CONFIRM THE PLAN THICKNESS. DEFICIENCIES IN THICKNESS SHALL BE ADJUSTED FOR BY THE METHOD REQUIRED BY WISDOT STANDARDS.
- 5.4. FINAL ACCEPTANCE OF THE TOTAL PAVEMENT INSTALLATION SHALL BE SUBJECT TO THE TESTING AND CHECKING REQUIREMENTS CITED ABOVE.
- ALL MATERIAL AND CONSTRUCTION SHALL CONFORM TO THE CITY OF MADISON CODE. WHEN CONFLICTS
 ARISE BETWEEN MUNICIPAL CODE, GENERAL NOTES AND SPECIFICATIONS, THE MORE STRINGENT SHALL
 TAKE PRECEDENCE.

SIGNAGE AND PAVEMENT MARKING NOTES

- ALL SIGNING AND PAVEMENT MARKING SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE WISCONSIN DEPARTMENT OF TRANSPORTATION (WISDOT)
- 2. SIGNS: SIGNS SHALL BE CONSTRUCTED OF 0.080—INCH THICK FLAT ALUMINUM PANELS WITH REFLECTORIZED LEGEND ON THE FACE. LEGEND SHALL BE IN ACCORDANCE WITH THE MUTCD.
- 5. POSTS: SIGN POSTS SHALL BE A HEAVY-DUTY STEEL "U" SHAPED CHANNEL WEIGHING 3.0 POUNDS/FOOT, SUCH AS A TYPE B METAL POST, AS PER THE WISDOT STANDARDS (OR 2-INCH PERFORATED STEEL
- . SIGNS AND POSTS SHALL BE INSTALLED IN ACCORDANCE WITH WISDOT STANDARDS.

DEGREES FAHRENHEIT AND RISING.

5. PAVEMENT MARKINGS: ALL PAVEMENT MARKINGS IN THE PUBLIC RIGHT-OF-WAY, SUCH AS STOP LINES, CENTERLINES, CROSSWALKS, AND DIRECTIONAL ARROWS, SHALL BE REFLECTORIZED THERMOPLASTIC ON ASPHALT AND EPOXY ON CONCRETE OR AS APPROVED BY WISDOT.

. PAVEMENT MARKINGS ON BIKE PATHS, PARKING LOT STALLS, AND SIMILAR "LOW—WEAR" APPLICATIONS,

- SHALL BE PAINT IN ACCORDANCE WITH WISDOT STANDARDS.

 7. COLOR, WIDTH, STYLE, AND SIZE OF ALL MARKINGS SHALL BE IN ACCORDANCE WITH THE MUTCD AND
- LOCAL CODE. STANDARD PARKING SPACES SHALL BE PAINTED WHITE OR YELLOW PER LOCAL CODE.

 THERMOPLASTIC MARKINGS SHALL BE INSTALLED WHEN THE PAVEMENT TEMPERATURE IS 55 DEGREES FAHRENHEIT AND RISING. PAINT MARKINGS MAY BE INSTALLED WHEN THE AIR TEMPERATURE IS 50



NC.
NO. REVISIONS
DA

JPM KIMLEY-HORN AND ASSOCIATES, ANA 4201 WINFIELD ROAD, SUITE 600

DRAWN BY: ANA



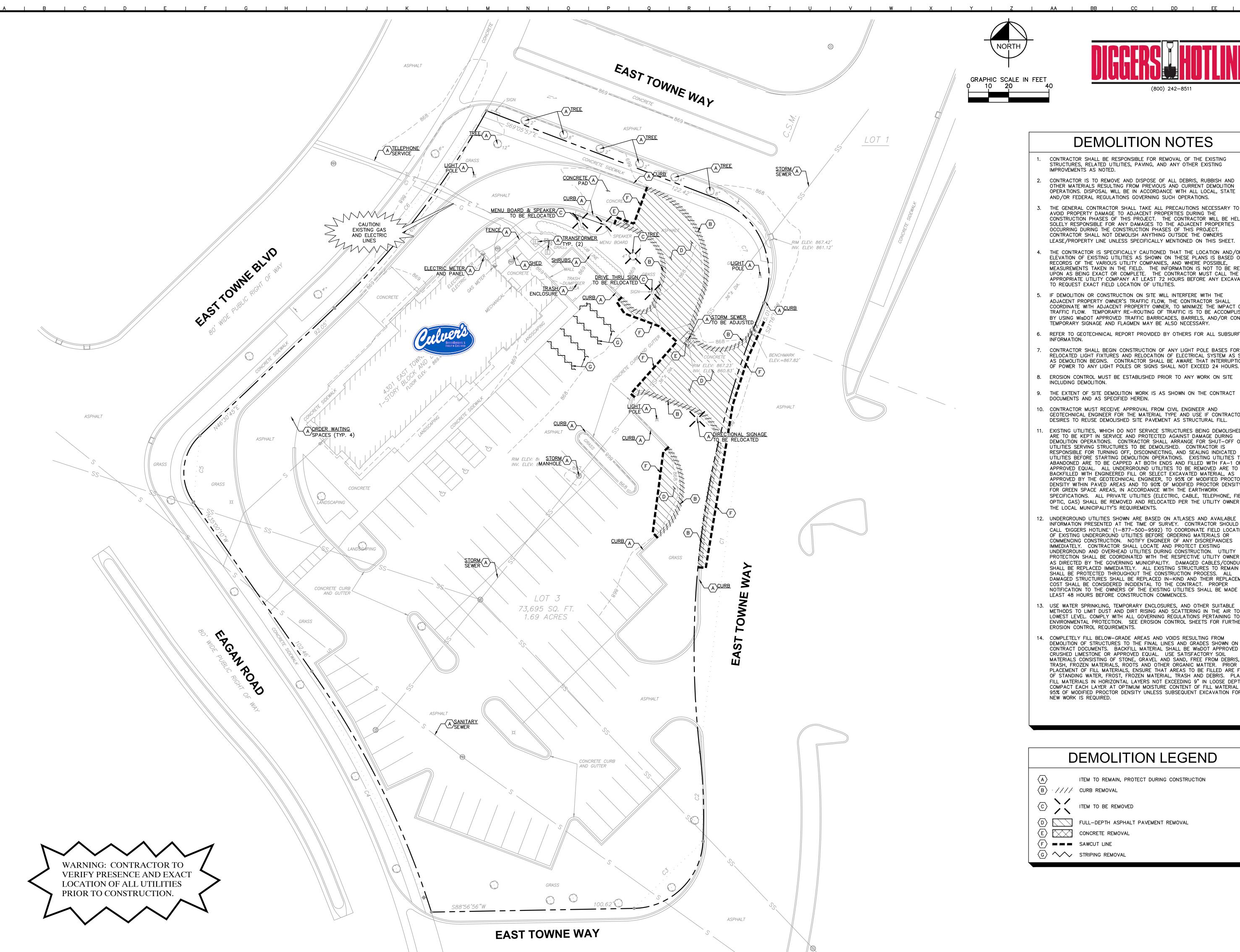
NERAL NOTES & DETAILS

DRIVE-THRU
IPROVEMENTS

ORIGINAL ISSUE: 08/13/2021 KHA PROJECT NO. 268025002

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SHEET NUMBER





DEMOLITION NOTES

- CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVAL OF THE EXISTING STRUCTURES, RELATED UTILITIES, PAVING, AND ANY OTHER EXISTING
- 2. CONTRACTOR IS TO REMOVE AND DISPOSE OF ALL DEBRIS, RUBBISH AND OTHER MATERIALS RESULTING FROM PREVIOUS AND CURRENT DEMOLITION OPERATIONS. DISPOSAL WILL BE IN ACCORDANCE WITH ALL LOCAL, STATE AND/OR FEDERAL REGULATIONS GOVERNING SUCH OPERATIONS.
- THE GENERAL CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT. THE CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR ANY DAMAGES TO THE ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASES OF THIS PROJECT. CONTRACTOR SHALL NOT DEMOLISH ANYTHING OUTSIDE THE OWNERS LEASE/PROPERTY LINE UNLESS SPECIFICALLY MENTIONED ON THIS SHEET.
- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED UPON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES.
- 5. IF DEMOLITION OR CONSTRUCTION ON SITE WILL INTERFERE WITH THE ADJACENT PROPERTY OWNER'S TRAFFIC FLOW, THE CONTRACTOR SHALL COORDINATE WITH ADJACENT PROPERTY OWNER, TO MINIMIZE THE IMPACT ON TRAFFIC FLOW. TEMPORARY RE-ROUTING OF TRAFFIC IS TO BE ACCOMPLISHED BY USING WisDOT APPROVED TRAFFIC BARRICADES, BARRELS, AND/OR CONES. TEMPORARY SIGNAGE AND FLAGMEN MAY BE ALSO NECESSARY.
- 6. REFER TO GEOTECHNICAL REPORT PROVIDED BY OTHERS FOR ALL SUBSURFACE
- RELOCATED LIGHT FIXTURES AND RELOCATION OF ELECTRICAL SYSTEM AS SOON AS DEMOLITION BEGINS. CONTRACTOR SHALL BE AWARE THAT INTERRUPTION OF POWER TO ANY LIGHT POLES OR SIGNS SHALL NOT EXCEED 24 HOURS.
- 8. EROSION CONTROL MUST BE ESTABLISHED PRIOR TO ANY WORK ON SITE INCLUDING DEMOLITION.
- 9. THE EXTENT OF SITE DEMOLITION WORK IS AS SHOWN ON THE CONTRACT DOCUMENTS AND AS SPECIFIED HEREIN.
- 10. CONTRACTOR MUST RECEIVE APPROVAL FROM CIVIL ENGINEER AND GEOTECHNICAL ENGINEER FOR THE MATERIAL TYPE AND USE IF CONTRACTOR DESIRES TO REUSE DEMOLISHED SITE PAVEMENT AS STRUCTURAL FILL.
- 11. EXISTING UTILITIES, WHICH DO NOT SERVICE STRUCTURES BEING DEMOLISHED, ARE TO BE KEPT IN SERVICE AND PROTECTED AGAINST DAMAGE DURING DEMOLITION OPERATIONS. CONTRACTOR SHALL ARRANGE FOR SHUT-OFF OF UTILITIES SERVING STRUCTURES TO BE DEMOLISHED. CONTRACTOR IS RESPONSIBLE FOR TURNING OFF, DISCONNECTING, AND SEALING INDICATED UTILITIES BEFORE STARTING DEMOLITION OPERATIONS. EXISTING UTILITIES TO E ABANDONED ARE TO BE CAPPED AT BOTH ENDS AND FILLED WITH FA-1 OR APPROVED EQUAL. ALL UNDERGROUND UTILITIES TO BE REMOVED ARE TO BE BACKFILLED WITH ENGINEERED FILL OR SELECT EXCAVATED MATERIAL, AS APPROVED BY THE GEOTECHNICAL ENGINEER, TO 95% OF MODIFIED PROCTOR DENSITY WITHIN PAVED AREAS AND TO 90% OF MODIFIED PROCTOR DENSITY FOR GREEN SPACE AREAS, IN ACCORDANCE WITH THE EARTHWORK SPECIFICATIONS. ALL PRIVATE UTILITIES (ELECTRIC, CABLE, TELEPHONE, FIBER OPTIC, GAS) SHALL BE REMOVED AND RELOCATED PER THE UTILITY OWNER AND THE LOCAL MUNICIPALITY'S REQUIREMENTS.
- 12. UNDERGROUND UTILITIES SHOWN ARE BASED ON ATLASES AND AVAILABLE INFORMATION PRESENTED AT THE TIME OF SURVEY. CONTRACTOR SHOULD CALL "DIGGERS HOTLINE" (1-877-500-9592) TO COORDINATE FIELD LOCATIONS OF EXISTING UNDERGROUND UTILITIES BEFORE ORDERING MATERIALS OR COMMENCING CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES IMMEDIATELY. CONTRACTOR SHALL LOCATE AND PROTECT EXISTING UNDERGROUND AND OVERHEAD UTILITIES DURING CONSTRUCTION. UTILITY PROTECTION SHALL BE COORDINATED WITH THE RESPECTIVE UTILITY OWNER AND AS DIRECTED BY THE GOVERNING MUNICIPALITY. DAMAGED CABLES/CONDUITS SHALL BE REPLACED IMMEDIATELY. ALL EXISTING STRUCTURES TO REMAIN SHALL BE PROTECTED THROUGHOUT THE CONSTRUCTION PROCESS. ALL DAMAGED STRUCTURES SHALL BE REPLACED IN-KIND AND THEIR REPLACEMENT COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. PROPER NOTIFICATION TO THE OWNERS OF THE EXISTING UTILITIES SHALL BE MADE AT LEAST 48 HOURS BEFORE CONSTRUCTION COMMENCES.
- 13. USE WATER SPRINKLING, TEMPORARY ENCLOSURES, AND OTHER SUITABLE METHODS TO LIMIT DUST AND DIRT RISING AND SCATTERING IN THE AIR TO TH LOWEST LEVEL. COMPLY WITH ALL GOVERNING REGULATIONS PERTAINING TO ENVIRONMENTAL PROTECTION. SEE EROSION CONTROL SHEETS FOR FURTHER EROSION CONTROL REQUIREMENTS.
- 14. COMPLETELY FILL BELOW-GRADE AREAS AND VOIDS RESULTING FROM DEMOLITION OF STRUCTURES TO THE FINAL LINES AND GRADES SHOWN ON THE CONTRACT DOCUMENTS. BACKFILL MATERIAL SHALL BE WISDOT APPROVED CRUSHED LIMESTONE OR APPROVED EQUAL. USE SATISFACTORY SOIL MATERIALS CONSISTING OF STONE, GRAVEL AND SAND, FREE FROM DEBRIS, TRASH, FROZEN MATERIALS, ROOTS AND OTHER ORGANIC MATTER. PRIOR TO PLACEMENT OF FILL MATERIALS, ENSURE THAT AREAS TO BE FILLED ARE FREE OF STANDING WATER, FROST, FROZEN MATERIAL, TRASH AND DEBRIS. PLACE FILL MATERIALS IN HORIZONTAL LAYERS NOT EXCEEDING 9" IN LOOSE DEPTH. COMPACT EACH LAYER AT OPTIMUM MOISTURE CONTENT OF FILL MATERIAL TO 95% OF MODIFIED PROCTOR DENSITY UNLESS SUBSEQUENT EXCAVATION FOR

DEMOLITION LEGEND

ITEM TO REMAIN, PROTECT DURING CONSTRUCTION

D FULL-DEPTH ASPHALT PAVEMENT REMOVAL

ORIGINAL ISSUE: 08/13/2021 KHA PROJECT NO 268025002

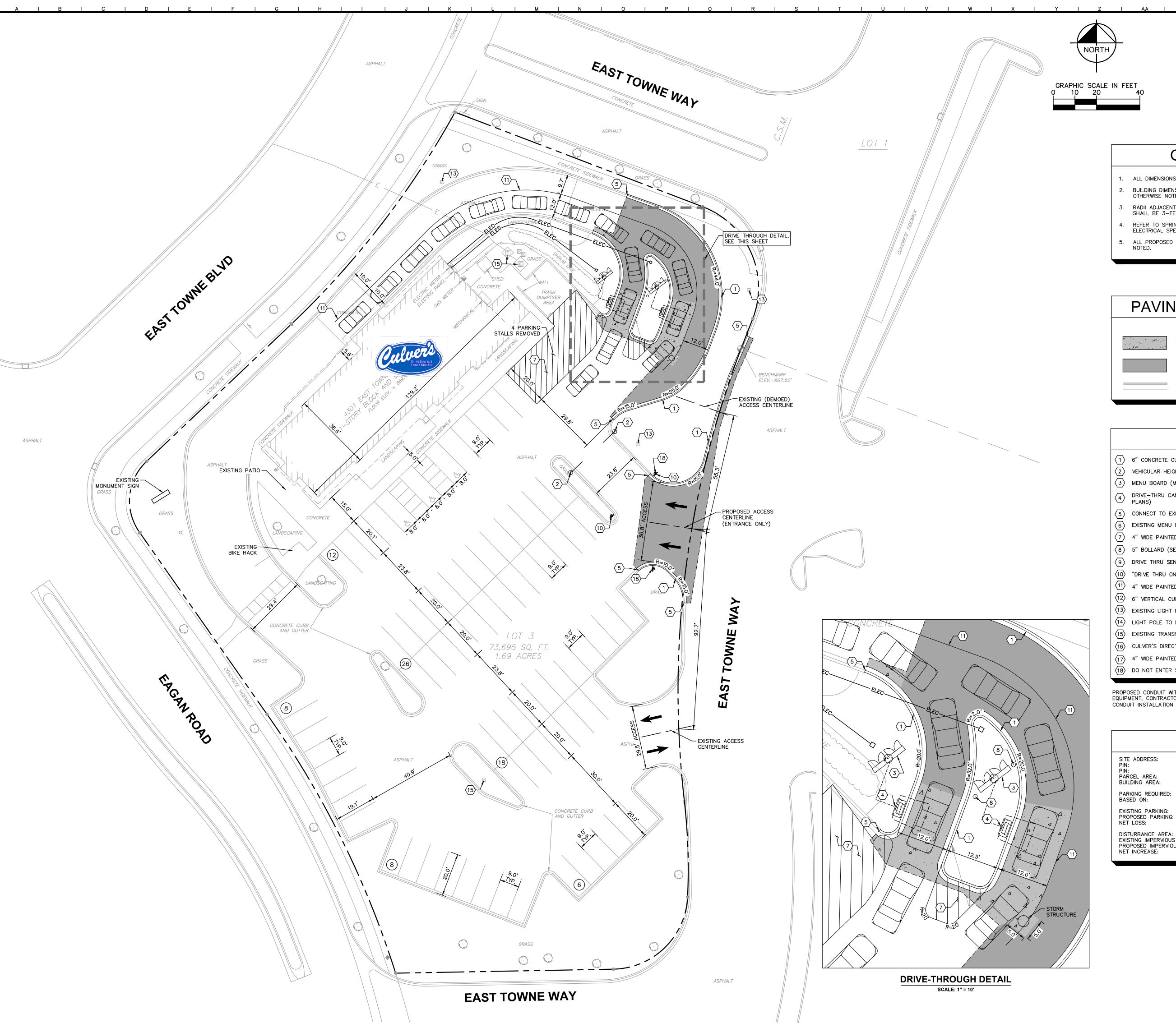
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Kimley»Horn

∞ EXISTING CONDITIONS & DEMO PLAN

CULVER'S DRIVE-THRU IMPROVEMENTS

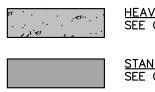
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- 1. ALL DIMENSIONS REFER TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
- 2. BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
- 3. RADII ADJACENT TO PARKING STALL AND NOT DIMENSIONED ON THIS PLAN SHALL BE 3-FEET, TYPICAL.
- 4. REFER TO SPRINGFIELD SIGN PLANS FOR MENU BOARD DETAILS AND ELECTRICAL SPECIFICATIONS
- 5. ALL PROPOSED ON-SITE STRIPING SHALL BE PAINTED UNLESS OTHERWISE

PAVING AND CURB LEGEND



HEAVY DUTY CONCRETE PAVEMENT
SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION

STANDARD DUTY ASPHALT PAVEMENT
SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION

— 6" CURB AND GUTTER

KEY NOTES

- $\langle 1 \rangle$ 6" CONCRETE CURB AND GUTTER (SEE DETAIL, SHEET C5.0)
- $\langle 2 \rangle$ VEHICULAR HEIGHT DETECTOR (VHD, SEE SIGNAGE PLANS)
- $\langle 3 \rangle$ MENU BOARD (MB DT-46, SEE SIGNAGE PLANS)
- DRIVE-THRU CANOPY AND ORDER CONFIRMATION SYSTEM (OCS, SEE SIGNAGE PLANS)
- (5) CONNECT TO EXISTING PAVEMENT, SIDEWALK, CURB, TYP.
- 6 EXISTING MENU BOARD AND SPEAKER TO REMAIN
- 7 4" WIDE PAINTED STRIPE AT 45°, 2' O.C. (YELLOW) $\langle 8 \rangle$ 5" BOLLARD (SEE DETAIL, SHEET C5.0)
- 9 DRIVE THRU SENSOR LOOP (SEE DETAILS, SHEET C5.0) (10) "DRIVE THRU ONLY" SIGN (SEE DETAILS, SHEET C5.0)
- 4" WIDE PAINTED SOLID LINE, TYP.
- 6" VERTICAL CURB AND GUTTER (SEE DETAIL, SHEET C5.0)
- (13) EXISTING LIGHT POLE TO REMAIN
- (14) LIGHT POLE TO BE RELOCATED
- (15) EXISTING TRANSFORMER
- (16) CULVER'S DIRECTIONAL SIGNAGE
- (17) 4" WIDE PAINTED PARKING STRIPE
- (18) DO NOT ENTER SIGN

PROPOSED CONDUIT WITH PULL BOX FOR POWER AND DATA TO NEW DRIVE THRU EQUIPMENT, CONTRACTOR TO TRENCH AND REPAIR PAVEMENT AS NEEDED FOR CONDUIT INSTALLATION (SEE DETAILS, SHEET C1.0)

SITE DATA

4301 E. TOWNE BLVD, MADISON, WI 53704
0810-273-0085-5
CC (COMMERCIAL CENTER DISTRICT) CITY OF MADISON
1.69 ACRES 4,922 SF

50 SPACES 10 SPACES PER 1,000 SF FLOOR AREA

82 SPACES 78 SPACES 4 SPACES

DISTURBANCE AREA: 7,122 SF
EXISTING IMPERVIOUS AREA: 55,148 SF
PROPOSED IMPERVIOUS AREA: 55,826 SF
NET INCREASE: 678 SF

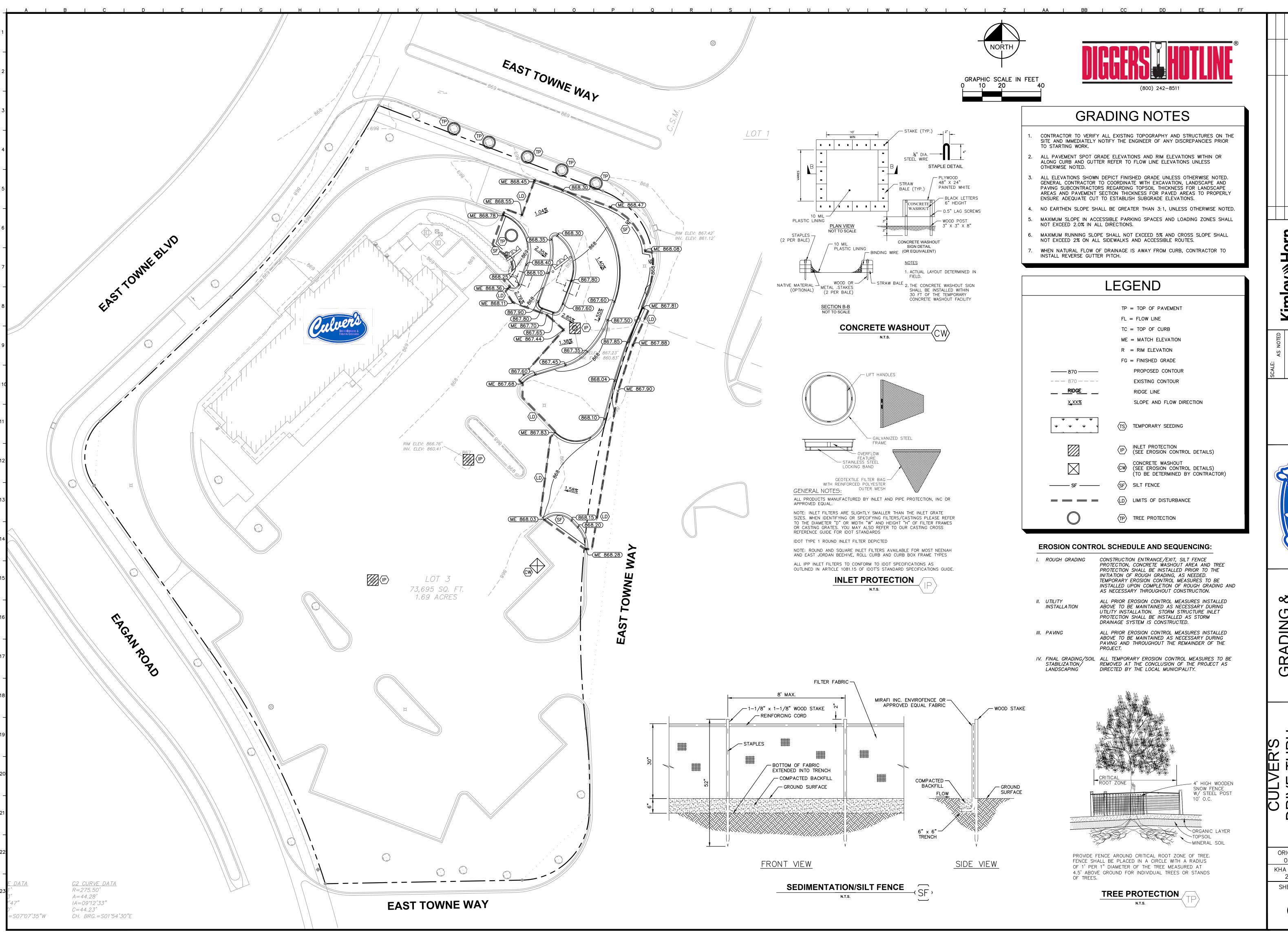
CULV DRIVE-IMPROVE

Kimley » Horn

ORIGINAL ISSUE: 08/13/2021 KHA PROJECT NO. 268025002

SHEET NUMBER

C3.0



REVISIONS

Kimley >>> Horn
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WARRENVILLE, IL 60555
PHONE: 630—487—5550
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DRAWN BY: ANA CHECKED BY: ANH

BUTTERBURGERS & FROZEN GUSTARD

GRADING & EROSION ONTROL PLA

DRIVE-THRU
IMPROVEMENTS
4301 E TOWNE BLVD

ORIGINAL ISSUE: 08/13/2021

KHA PROJECT NO. 268025002

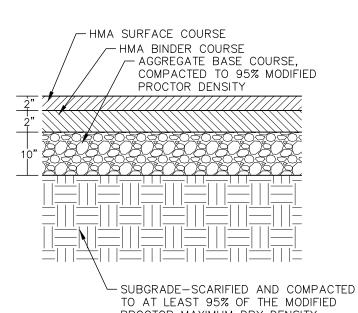
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NOTE:

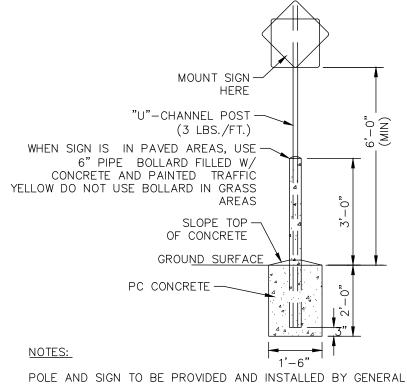
PAVEMENT SECTION BASED ON GENERAL UNDERSTANDING OF PROJECT AREA. CONTRACTOR TO INSTALL RECOMMENDED SECTION OR MATCH EXISTING PAVEMENT SECTION, WHICHEVER IS GREATER. GEOTECHNICAL REPORT NOT PROVIDED BY OWNER

HEAVY DUTY CONCRETE PAVEMENT SECTION



PROCTOR MAXIMUM DRY DENSITY NOTE: PAVEMENT SECTION BASED ON GENERAL UNDERSTANDING OF PROJECT AREA. CONTRACTOR TO INSTALL RECOMMENDED SECTION OR MATCH EXISTING PAVEMENT SECTION, WHICHEVER IS GREATER. GEOTECHNICAL REPORT NOT PROVIDED BY OWNER

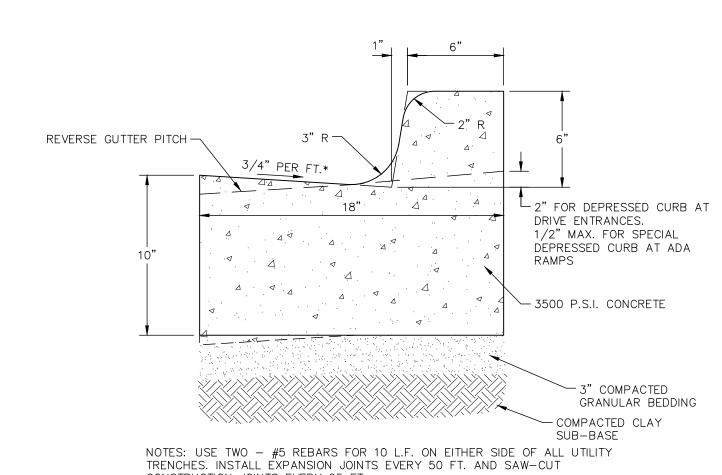
STANDARD DUTY **ASPHALT PAVEMENT SECTION**



CONTRACTOR.

ALL SIGNS SHALL COMPLY WITH U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION'S "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES", LOCAL CODES AND AS SPECIFIED. MOUNT SIGNS TO POST IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS.

> STANDARD SIGN BASE N.T.S.

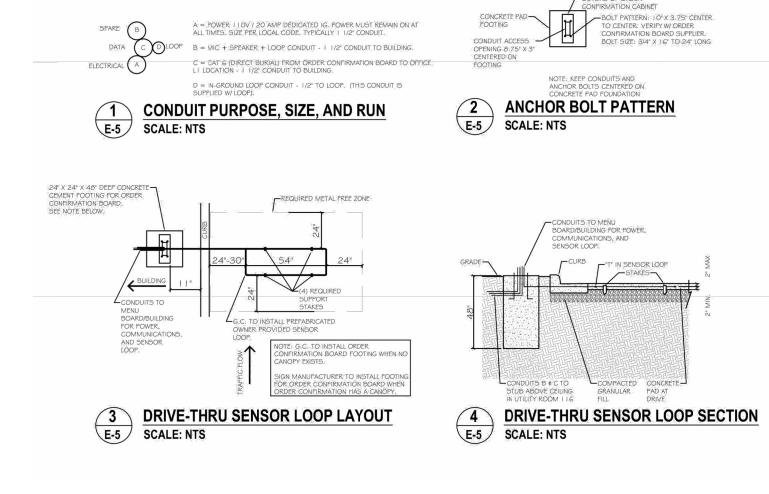


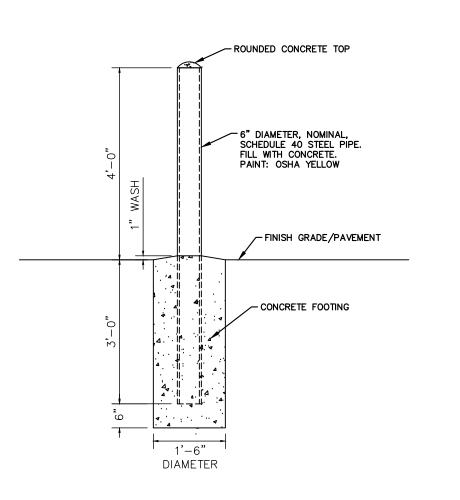
CONSTRUCTION JOINTS EVERY 25 FT. * MAXIMUM GUTTER SLOPE AT ADA RAMPS TO BE 5.0%

6-INCH CURB & GUTTER

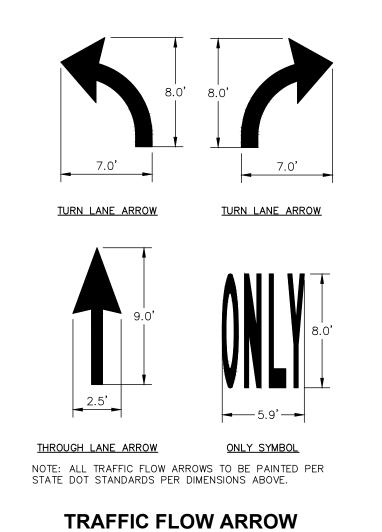
N.T.S.

DATA C DLOOP B = VIC + SPEAKER + LOOP CONDUIT - 1 1/2" CONDUIT TO BUILDING. C= CAT G (DIRECT BURIAL) FROM ORDER CONFIRMATION BOARD TO OFFICE. L1 LOCATION - J 1/2" CONDUIT TO BUILDING. D = IN-GROUND LOOP CONDUIT - 1/2" TO LOOP. (THIS CONDUIT IS SUPPLIED W) LOOP): 2 ANCHOR BOLT PATTERN CONDUIT PURPOSE, SIZE, AND RUN E-5 SCALE: NTS B-5 DRIVE-THRU SENSOR LOOP LAYOUT SCALE: NTS

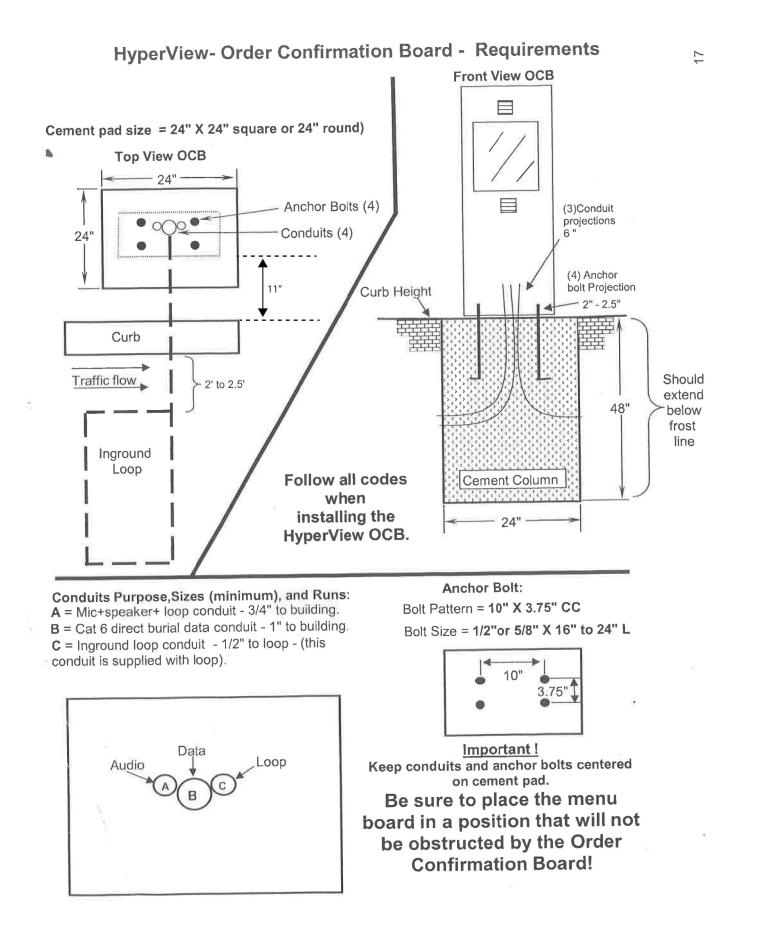




6" BOLLARD DETAIL



N.T.S.



Kimley » Horn

(800) 242-8511



ONSTRUC DETAIL

CULVER'S DRIVE-THRU IMPROVEMENT

ORIGINAL ISSUE: 08/13/2021 KHA PROJECT NO. 268025002

SHEET NUMBER

C5.0



PLANT SCHEDULE

SHRUBS	CODE	QTY	BOTANICAL / COMMON NAME	CONT	<u>SPACING</u>	SIZE	
\odot	AU	5	ARONIA MELANOCARPA `UCONNAM166` / LOW SCAPE HEDGER CHOKEBERRY	-	SEE PLAN	6" HT MIN	
\odot	HL	3	HYDRANGEA PANICULATA `LITTLE LIME` / LITTLE LIME HYDRANGEA	3 GAL	SEE PLAN	18" HT MIN	
\odot	RG	5	RHUS AROMATICA `GRO-LOW` / GRO-LOW FRAGRANT SUMAC	-	SEE PLAN	12" HT MIN	
\odot	VA	5	VIBURNUM DENTATUM `CHRISTOM` / BLUE MUFFIN VIBURNUM	3 GAL	SEE PLAN	48" HT MIN	
EVERGREEN SHRUBS	CODE	<u>QTY</u>	BOTANICAL / COMMON NAME	CONT	SPACING	SIZE	
+	JF	3	JUNIPERUS CHINENSIS 'SEA GREEN' / SEA GREEN JUNIPER	3 GAL	SEE PLAN	24" SPREAD	
<u>GRASSES</u>	CODE	<u>QTY</u>	BOTANICAL / COMMON NAME	CONT	SPACING		
ONLY OF THE STATE	СК	20	CALAMAGROSTIS X ACUTIFLORA `KARL FOERSTER` / FEATHER REED GRASS	1 GAL	SEE PLAN		
PERENNIALS AND GRASSES	CODE	<u>QTY</u>	BOTANICAL / COMMON NAME	CONT	SPACING		
	НО	19	HEMEROCALLIS X 'STELLA DE ORO' / STELLA DE ORO DAYLILY	1 GAL	18" OC		
	SH	3	SPOROBOLUS HETEROLEPIS / PRAIRIE DROPSEED	1 GAL	24" OC		

GROUND COVERS

LANDSCAPE AREAS IN

ELEV.=8

EX. PARKWAY TREES, TYP

TURF SOD TO LIMITS OF -

EX. GRASS GROUND — COVER TO REMAIN

ASPHALT

 \succeq EX. CANOPY TREE

EX. CANOPY TREE

EX. CANOPY TREE

LOT 3

73,695 SQ. FT.

1.69 ACRES

EAST TOWNE WAY

DISTURBANCE, TYP.

PROPERTY LINE,

LANDSCAPE AREA, TYP. EXISTING SHRUBS TO

EX. STONE MULCH

REMAIN, TYP.

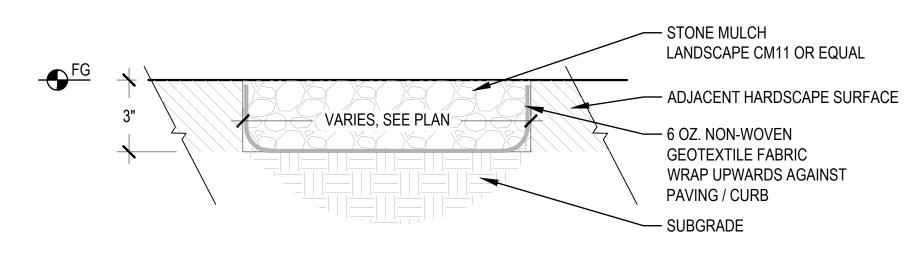
CM11 STONE MULCH

BOTANICAL / COMMON NAME

TURF SOD

LANDSCAPE NOTES

- THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING MATERIALS AND PLANTS SHOWN ON THE LANDSCAPE PLAN. THE CONTRACTOR IS RESPONSIBLE FOR THE COST TO REPAIR UTILITIES, ADJACENT LANDSCAPE, PUBLIC AND PRIVATE PROPERTY THAT MAINTENANCE PERIOD. CALL FOR UTILITY LOCATIONS PRIOR TO ANY EXCAVATION
- THE CONTRACTOR SHALL REPORT ANY DISCREPANCY IN PLAN VS. FIELD CONDITIONS IMMEDIATELY TO THE LANDSCAPE ARCHITECT PRIOR TO CONTINUING WITH THAT PORTION OF WORK.
- NO PLANTING WILL BE INSTALLED UNTIL ALL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.
- 4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPAIR OF ANY OF THEIR TRENCHES OR EXCAVATIONS THAT SETTLE. 5. ALL PLANTS TO BE SPECIMEN GRADE, WELL BRANCHED, HEALTHY, FULL, PRE-INOCULATED AND FERTILIZED. PLANTS SHALL BE FREE FROM DISEASE, PESTS, WOUNDS, AND SCARS. PLANTS SHALL BE FREE FROM NOTICEABLE GAPS, HOLES, OR DEFORMITIES. PLANTS
- 6. THE OWNER'S REPRESENTATIVE MAY REJECT ANY PLANT MATERIALS THAT ARE DISEASED, DEFORMED, OR OTHERWISE NOT EXHIBITING SUPERIOR QUALITY.
- 7. ALL NURSERY STOCK SHALL BE GUARANTEED, BY THE CONTRACTOR, FOR ONE YEAR FROM DATE OF FINAL INSPECTION. THE GUARANTEE BEGINS ON THE DATE OF THE LANDSCAPE ARCHITECT'S OR OWNERS WRITTEN ACCEPTANCE OF THE INITIAL PLANTING. REPLACEMENT PLANT MATERIAL SHALL HAVE A ONE YEAR GUARANTEE COMMENCING UPON PLANTING.
- 8. PLANTS TO MEET AMERICAN STANDARD FOR NURSERY STOCK (ANSI Z60.1-2014 OR MOST CURRENT VERSION) REQUIREMENTS FOR SIZE
- 9. PRUNE PLANTS AS NECESSARY- PER STANDARD NURSERY PRACTICE AND TO CORRECT POOR BRANCHING OF EXISTING AND PROPOSED 10. TOPSOIL SHALL BE PROVIDED AND GRADED BY THE GENERAL CONTRACTOR UP TO 6 INCHES BELOW FINISHED GRADE IN TURF AREAS
- AND 18 INCHES IN PLANTING AREAS. 11. PLANTING AREA TOPSOIL SHALL BE AMENDED WITH 25% SPHAGNUM PEATMOSS, 5% HUMUS AND 70% PULVERIZED SOIL FOR ALL NON
- TURF SEED MIX AREAS, SHRUB, ORNAMENTAL GRASS, PERENNIAL AND ANNUAL BEDS. 12. SEED/SOD LIMIT LINES ARE APPROXIMATE. CONTRACTOR SHALL SEED/SOD ALL AREAS WHICH ARE DISTURBED BY GRADING WITH THE SPECIFIED SEED/SOD MIXES.
- 13. EDGING TO BE A SPADED EDGE UNLESS INDICATED OTHERWISE ON THE PLANS. SPADED EDGE TO PROVIDE V-SHAPED DEPTH AND WIDTH TO CREATE SEPARATION BETWEEN MULCH AND GRASS. A SPADED BED EDGE SHALL SEPARATE MULCH BEDS FROM TURF OR SEEDED AREAS. A SPADED EDGE IS NOT REQUIRED ALONG CURBED EDGES.
- 14. CONTRACTOR SHALL INSTALL SHREDDED HARDWOOD MULCH AT A 3" DEPTH TO ALL TREES, SHRUB, PERENNIAL, AND GROUNDCOVER AREAS. TREES PLACED IN AREA COVERED BY TURF SHALL RECEIVE A 4 FT WIDE MAXIMUM TREE RING WITH 3" DEPTH SHREDDED 15. INSTALLATION OF TREES WITHIN PARKWAYS SHALL BE COORDINATED IN THE FIELD WITH LOCATIONS OF UNDERGROUND UTILITIES.
- TREES SHALL NOT BE LOCATED CLOSER THAN 5' FROM UNDERGROUND UTILITY LINES AND NO CLOSER THAN 10' FROM UTILITY 16. DO NOT DISTURB THE EXISTING PAVING, LIGHTING, OR LANDSCAPING THAT EXISTS ADJACENT TO THE SITE UNLESS OTHERWISE NOTED
- 17. ALL DISTURBED AREAS TO BE SODDED OR SEEDED, UNLESS OTHERWISE NOTED. SOD/SEED SHALL BE LOCAL HARDY TURF GRASS MIX
- UNLESS, OTHERWISE NOTED.
- 18. PLANT QUANTITIES SHOWN ARE FOR THE CONVENIENCE OF THE OWNER AND JURISDICTIONAL REVIEW AGENCIES. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL PLANT QUANTITIES AS DRAWN.



CM11 STONE MULCH

SHALL BE FREE FROM BROKEN OR DEAD BRANCHES. TRUNKS WILL BE WRAPPED IF NECESSARY TO PREVENT SUN SCALD AND INSECT DAMAGE. THE LANDSCAPE CONTRACTOR SHALL REMOVE THE WRAP AT THE PROPER TIME AS PART OF THIS CONTRACT.

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SHEET NUMBER

EX. PARKWAY TREES,

EX. PARKWAY TREES, TYP.

MINIMUM 6" BEYOND ROOT BALL

APPLY CORRECTIVE PRUNING.

PRIOR TO INSTALLATION.

FOR BED LAYOUTS.

REMOVE OR CORRECT GIRDLING ROOTS.

THOROUGHLY WITHIN TWO HOURS.

2. SET ROOT BALL OR CONTAINER ON UNEXCAVATED OR TAMPED SOIL. TOP OF

ROOTBALL (CONTAINER) SHALL BE ONE INCH ABOVE SURROUNDING GRADE.

REMOVE BURLAP FROM TOP HALF THE LENGTH OF ROOTBALL. TWINE AND (IF

CONTAINER GROWN SHRUBS, REMOVE CONTAINER AND LOOSEN ROOTS

5. PLUMB AND BACKFILL WITH AMENDED SOIL PER LANDSCAPE NOTES. WATER

LIMITS FOR SHRUBS EXTEND TO ALL LIMITS OF PLANTING BED, SEE PLANS

6. APPLY MULCH IN EVEN LAYER, KEEPING AWAY FROM ROOT FLARE. MULCH

USED) SYNTHETIC MATERIAL SHALL BE REMOVED FROM PLANTING BED. FOR

FOR LARGER SHRUBS WITHIN PLANTING BED DIG A DEEPER PIT ONLY FOR

NOTES:

SHRUB PLANTING

-EX. CANOPY TREE

CONCRETE CURB

SHREDDED

AMENDED SOIL

HARDWOOD MULCH

EX. CANOPY TREE

←EX. SHRUB

EX. EVERGREEN -

3" = 1'-0"